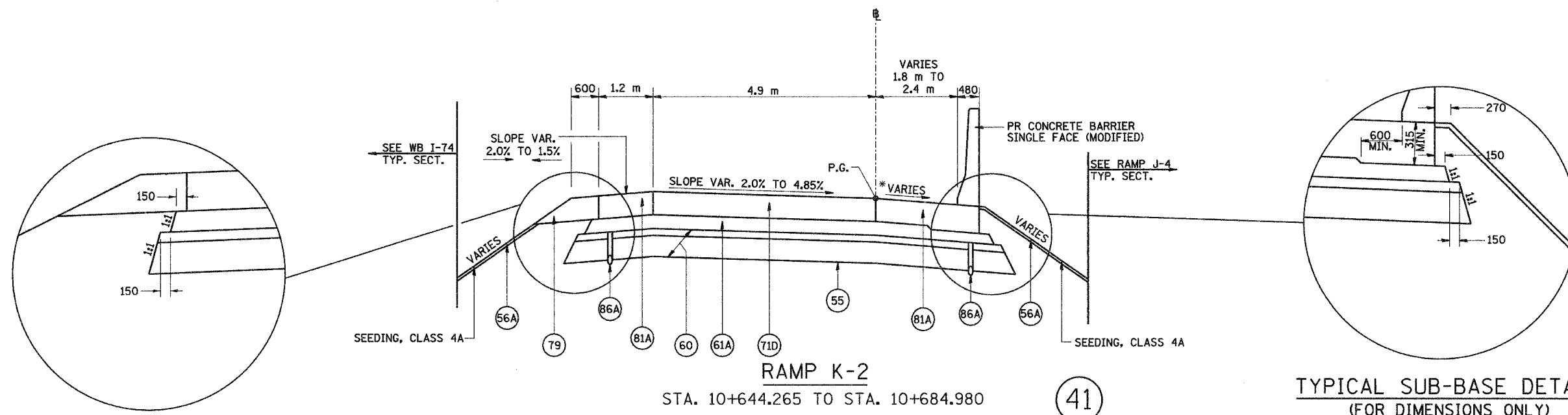


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	*	TAZEWELL	1366	78
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
* (90-IDR-2; 90(3,4,4-DR-1				



TYPICAL SUB-BASE DETAIL
(FOR DIMENSIONS ONLY)

RAMP K-2
STA. 10+644.265 TO STA. 10+684.980

41

TYPICAL SUB-BASE DETAIL
(FOR DIMENSIONS ONLY)

*STA. 10+644.265 TO STA. 10+656.962 - SLOPE VARIES 2.0% TO 4.0%
*STA. 10+656.962 TO STA. 10+684.980 - SLOPE VARIES 4.0% TO 4.85%

LEGEND			
55	PR GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	89K	PR COMB. CONCRETE C&G, TYPE B-15.45
56A	PR FURNISH AND PLACE TOPSOIL, 100 MM	81A	PR PORTLAND CEMENT CONCRETE SHOULDERS 250 MM
60	PR AGGREGATE SUB-BASE, 300 MM	81B	PR PORTLAND CEMENT CONCRETE SHOULDERS, 290 MM
61A	PR BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0L (LOW ESAL), 100MM	82	PR RETAINING WALL (SEE WALL PLANS)
61B	PR BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0L (LOW ESAL), 150MM	86A	PR PIPE UNDERDRAINS, 100 MM
71D	PR PORTLAND CEMENT CONCRETE PAVEMENT 250MM (JOINTED)	86B	PR PIPE UNDERDRAINS, 150 MM
75	PR CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT, 290 MM	88	PR CONCRETE GUTTER, TYPE A (MODIFIED)
79	PR AGGREGATE SHOULDERS, TYPE B	89B	PR COMB. CONCRETE C&G, TYPE B-15.60
		96A	PR LONGITUDINAL CONSTRUCTION JOINT (TYPICAL)
		96B	PR SAWED LONGITUDINAL JOINT (TYPICAL)

NOTES:
SHOULDERS ADJACENT TO THE HIGH SIDE OF SUPERELEVATED PAVEMENT SHALL BE 4% OR 8% MINUS THE RATE OF SUPER, WHICHEVER IS LESS. ADJACENT TO THE LOW SIDE OF SUPERELEVATED PAVEMENT SHOULDERS SHALL HAVE A SLOPE OF 4% OR THE RATE OF SUPER, WHICHEVER IS GREATER.

ALL DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE STATED.
60 IS SHOWN IN TWO LAYERS TO DENOTE THE TWO GRADATIONS THAT MAKE UP THIS PAY ITEM.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
FAI ROUTE 74 (I-74)
PROPOSED TYPICAL SECTIONS
RAMP K-2
NUMBERS 41



DATE 12/20/04
DRAWN BY DER
CHECKED BY MLB