

QUANTITIES NOT OTHERWISE SHOWN ON THE PLANS

X4066514: POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C" N50
APPLIED AT A RATE OF 112 LB PER SQ YD PER INCH

MAINLINE - STA. 366+00.00 TO STA. 529+00.00
 $[(16,300' \times 22.0')/9 = 39,844.4 \text{ SQ YD} \times 112 \times 1.5"]/2000 = 3,346.9 \text{ TON}$
 4 MAILBOX TURNOUTS @ 2.4 TONS EACH = 9.6 TON
 5 SIDE ROAD INTERSECTIONS @ 34.3 TONS EACH = 171.5 TON
 5 PRIVATE ENTRANCES @ 1.6 TONS EACH = 8.0 TON

TOTAL = 3,346.9 TON + 9.6 TON + 171.5 TON + 8.0 TON = 3,536.0 TON
 $3,536.0 \times 1.30 \text{ (ADD 30% FOR RUTTING)} = 4,596.8 \text{ TON}$
 PAY QUANTITY = 4,597.0 TON

X406654: POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N50
APPLIED AT A RATE OF 112 LB PER SQ YD PER INCH

(BRIDGE OMISSION FROM STA. 409+75.00 TO STA. 411+05.00 FOR BINDER COURSE ONLY)
 MAINLINE - STA. 366+00.00 TO STA. 409+75.00 AND FROM STA. 411+05.00 TO STA. 529+00.00
 $[(16,170.0 \times 22.5')/9 = 40,425.0 \text{ SQ YD} \times 112.0 \times 2.50"]/2000 = 5,659.5 \text{ TON}$

TOTAL = 5,659.5 TON $\times 1.50 \text{ (ADD 50% FOR RUTTING)} = 8,489.3 \text{ TON}$
 PAY QUANTITY = 8,489.0 TON

40600100: BITUMINOUS MATERIALS (PRIME COAT)
APPLIED AT A RATE OF 0.10 GAL PER SQ YD

MAINLINE - STA. 366+00.00 TO STA. 529+00.00
 $(16,300' \times 22.0')/9 = 39,844.4 \text{ SQ YD} \times 0.10 \text{ GAL/SQ YD} \times 2 \text{ LIFTS} = 7,968.9 \text{ GALLON}$
 PAY QUANTITY = 7,969.0 GALLON

40600300: AGGREGATE (PRIME COAT)
APPLIED AT A RATE OF 4 LB PER SQ YD

MAINLINE - STA. 366+00.00 TO STA. 529+00.00
 $[(16,300' \times 22.0')/9 = 39,844.4 \text{ SQ YD} \times 4 \text{ LB/SQ YD} \times 2 \text{ LIFTS}]/2000 = 159.4 \text{ TON}$
 PAY QUANTITY = 159.0 TON

48101200: AGGREGATE SHOULDERS, TYPE B
APPLIED AT A RATE OF 2.0 TON PER CU YD

BRIDGE OMISSION FROM STA. 409+75.00 TO STA. 411+05.00
 MAINLINE - STA. 366+00.00 TO STA. 409+75.00 AND FROM STA. 411+05.00 TO STA. 529+00.00
 $[(16,170' \times 3.0' \times 0.3' \text{ DEPTH})/27 = 539 \text{ CU YD} \times 2 \text{ TON/CU YD} = 1,078.0 \text{ TON} \times 2 \text{ SIDES} = 2,156.0 \text{ TON}$
 $2,156.0 \text{ TON} \times 1.30 \text{ (ADD 30% LOW SHOULDERS)} = 2,802.8 \text{ TON}$
 PAY QUANTITY = 2,803.0 TON

40200800: AGGREGATE SURFACE COURSE, TYPE B
30 PRIVATE OR FIELD ENTRANCES @ 10.0 TON EACH = 300.0 TON

PAY QUANTITY = 300.0 TON

70300100: SHORT-TERM PAVEMENT MARKING

MAINLINE - STA. 366+00.00 TO STA. 529+00.00 (USE 4' STRIPE WITH A 36' SPACE)
 $16,300 \text{ FT}/40 \text{ FT} = 407.5 \text{ STRIPES} \times 4' = 1,630.0 \text{ FT} \times 2 \text{ LIFTS} = 3,260.0 \text{ FEET}$
 PAY QUANTITY = 3,260.0 FOOT

78001110: PAINT PAVEMENT MARKING LINE 4
MAINLINE - STA. 366+00.00 TO STA. 529+00.00

NO PASSING ZONES: (4" YELLOW)
 STA. 385+03 LT. TO STA. 396+04 LT. = 1,101.0 FT.
 STA. 403+40 LT. TO STA. 411+14 LT. = 774.0 FT.
 STA. 421+46 LT. TO STA. 431+31 LT. = 985.0 FT.
 STA. 440+80 LT. TO STA. 452+94 LT. = 1,214.0 FT.
 STA. 375+94 RT. TO STA. 387+05 RT. = 1,111.0 FT.
 STA. 394+93 RT. TO STA. 401+73 RT. = 680.0 FT.
 STA. 412+36 RT. TO STA. 422+91 RT. = 1,055.0 FT.
 STA. 431+54 RT. TO STA. 445+15 RT. = 1,361.0 FT.

TOTAL NO PASSING ZONES = 8,281.0 FT.

SKIP-DASH CENTERLINE (4" YELLOW)
 16,300.0 FT. - 893.0 FT. (DOUBLE NO PASSING ZONES) =
 15,407.0 FT. $\times 0.25 \text{ (10' DASH \& 30' SKIP)} = 3,852.0 \text{ FT.}$

TOTAL NO PASSING ZONES = 3,852.0 FT.

EDGE STRIPING (4" WHITE)
 16,300.0 FT. $\times 2 \text{ SIDES} = 32,600 \text{ FT.}$

TOTAL NO PASSING ZONES = 32,600.0 FT.

TOTAL = 8,281.0 + 3,852.0 + 32,600.0 = 44,733.0 FT.

PAY QUANTITY = 44,733.0 FOOT

70301000: WORK ZONE PAVEMENT MARKING REMOVAL
1630.0 FT $\times 0.33 \text{ FT.} = 537.9 \text{ SQ. FT.}$

PAY QUANTITY = 538.0 SQ. FT.

44300300: AREA REFLECTIVE CRACK CONTROL TREATMENT SYSTEM A
STA. 366+00 TO STA. 409+75 = $(4,375.0 \text{ FT.} \times 22')/9 = 10,894.4 \text{ SQ. YD.}$
STA. 411+05 TO STA. 529+00 = $(11,795.0 \text{ FT.} \times 22')/9 = 28,832.2 \text{ SQ. YD.}$

TOTAL = 39,526.6 SQ. YD.

PAY QUANTITY = 39,527.0 SQ. YD.

40600980: BITUMINOUS SURFACE REMOVAL - BUTT JOINT

STA. 366+00 (B.O.P.) TO STA. 367+20 = $(120.0' \times 22.0')/9 = 293.3 \text{ SQ. YD.}$
 STA. 367+45 LT. (INDUSTRY ROAD) = $(50.0' \times 22.0')/9 = 122.2 \text{ SQ. YD.}$
 STA. 367+45 RT. (PLYMOUTH ROAD) = $(50.0' \times 22.0')/9 = 122.2 \text{ SQ. YD.}$
 STA. 409+00 TO STA. 409+75 (BK ABUT) = $(75.0' \times 22.0')/9 = 183.3 \text{ SQ. YD.}$
 STA. 411+05 (BK ABUT) TO STA. 411+80 = $(75.0' \times 22.0')/9 = 183.3 \text{ SQ. YD.}$
 STA. 527+80 TO STA. 529+00 (E.O.P.) = $(120.0' \times 22.0')/9 = 293.3 \text{ SQ. YD.}$

TOTAL = 1197.6 SQ. YD.

PAY QUANTITY = 1,198.0 SQ. YD.

GENERAL NOTES

1. WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL CAREFULLY PROTECT AND PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.

2. BITUMINOUS MIXTURE REQUIREMENTS

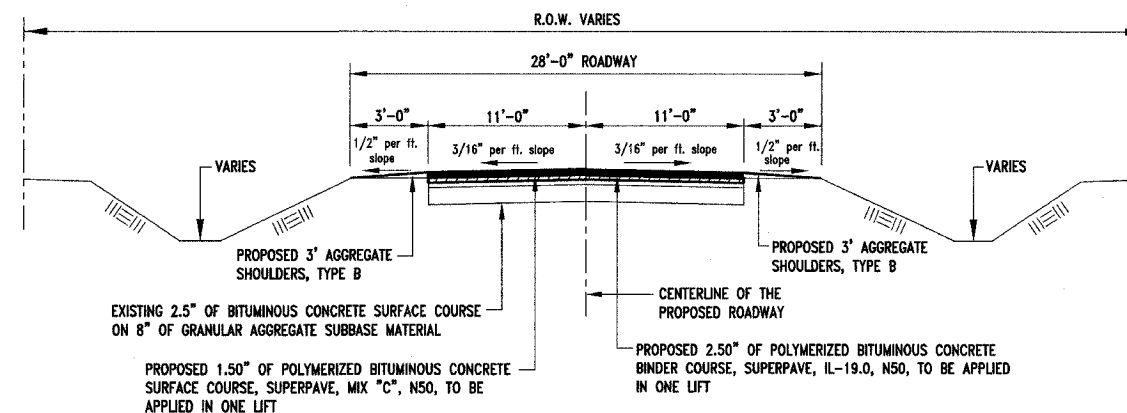
THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT.

MIXTURE USE(S):	SURFACE COURSE	BINDER COURSE
AC/PG:	SBS-PG70-28	SBS-PG70-28
RAP %: (MAX)	0%	0%
DESIGN AIR VOIDS:	4.2% @ Ndes = 50	4.2% @ Ndes = 50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 9.5 or IL 12.5	IL 19.0
FRICTION AGGREGATE:	MIXTURE D (SEE NOTE)	N/A

NOTE: DOLOMITE ONLY IS TO BE USED FOR THE FRICTION AGGREGATE IN THE SURFACE COURSE MATERIAL.

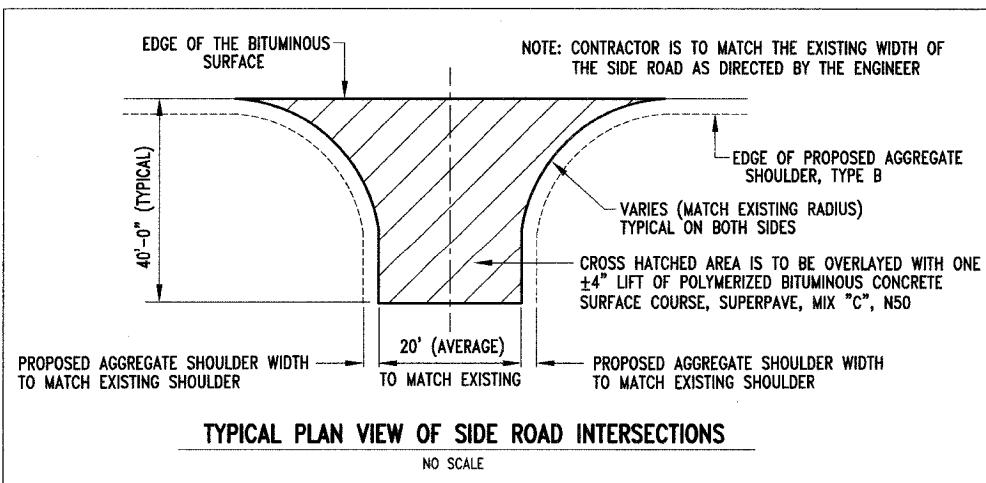
3. PAVEMENT DESIGN: STA 366+00.00 TO STA 529+00.00
 AVERAGE DAILY TRAFFIC (ADT) = 800 (YEAR COUNTED 2002)
 80,000 LB TRUCK ROUTE
 PV = 94.00%, SU = 2.00%, MU = 4.00%
 DESIGN PERIOD = 20 YEARS
 CLASS III ROAD

4. THERE IS TO BE AN OMISSION IN THE PROPOSED BITUMINOUS CONCRETE BINDER CONCRETE FROM STATION 409+75.00 TO STATION 411+05.00, FOR THE EXISTING BRIDGE.



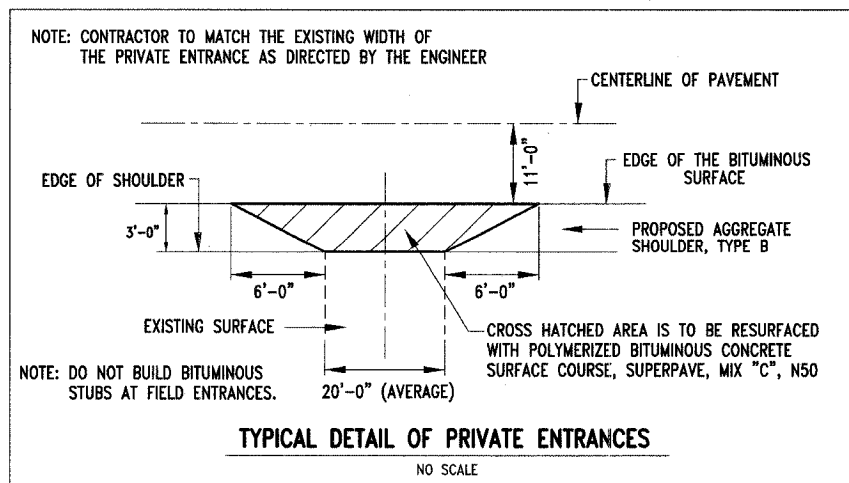
TYPICAL SECTION THROUGH THE PROPOSED ROADWAY FROM STATION 366+00.00 TO STATION 409+75.00 AND FROM STATION 411+05.00 TO STATION 529+00.00

NO SCALE



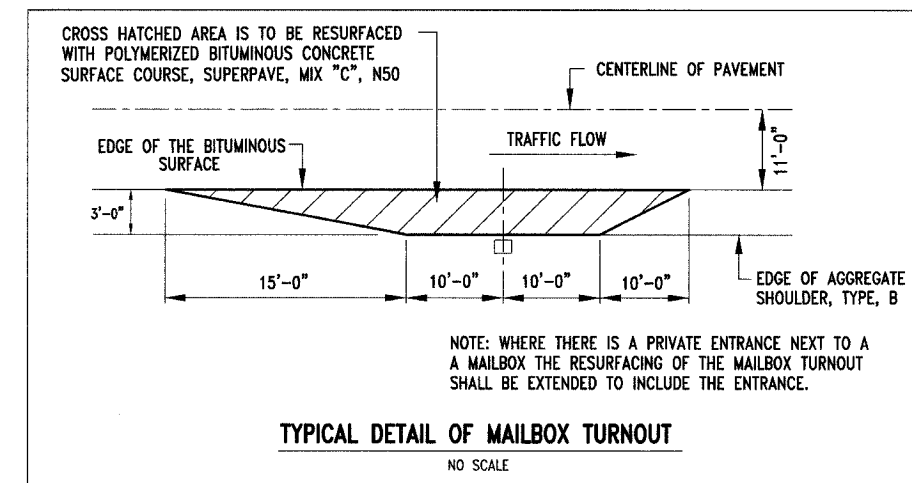
TYPICAL PLAN VIEW OF SIDE ROAD INTERSECTIONS

NO SCALE



TYPICAL DETAIL OF PRIVATE ENTRANCES

NO SCALE



TYPICAL DETAIL OF MAILBOX TURNOUT

NO SCALE