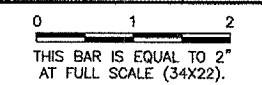


REVISIONS		
NUMBER	BY	DATE

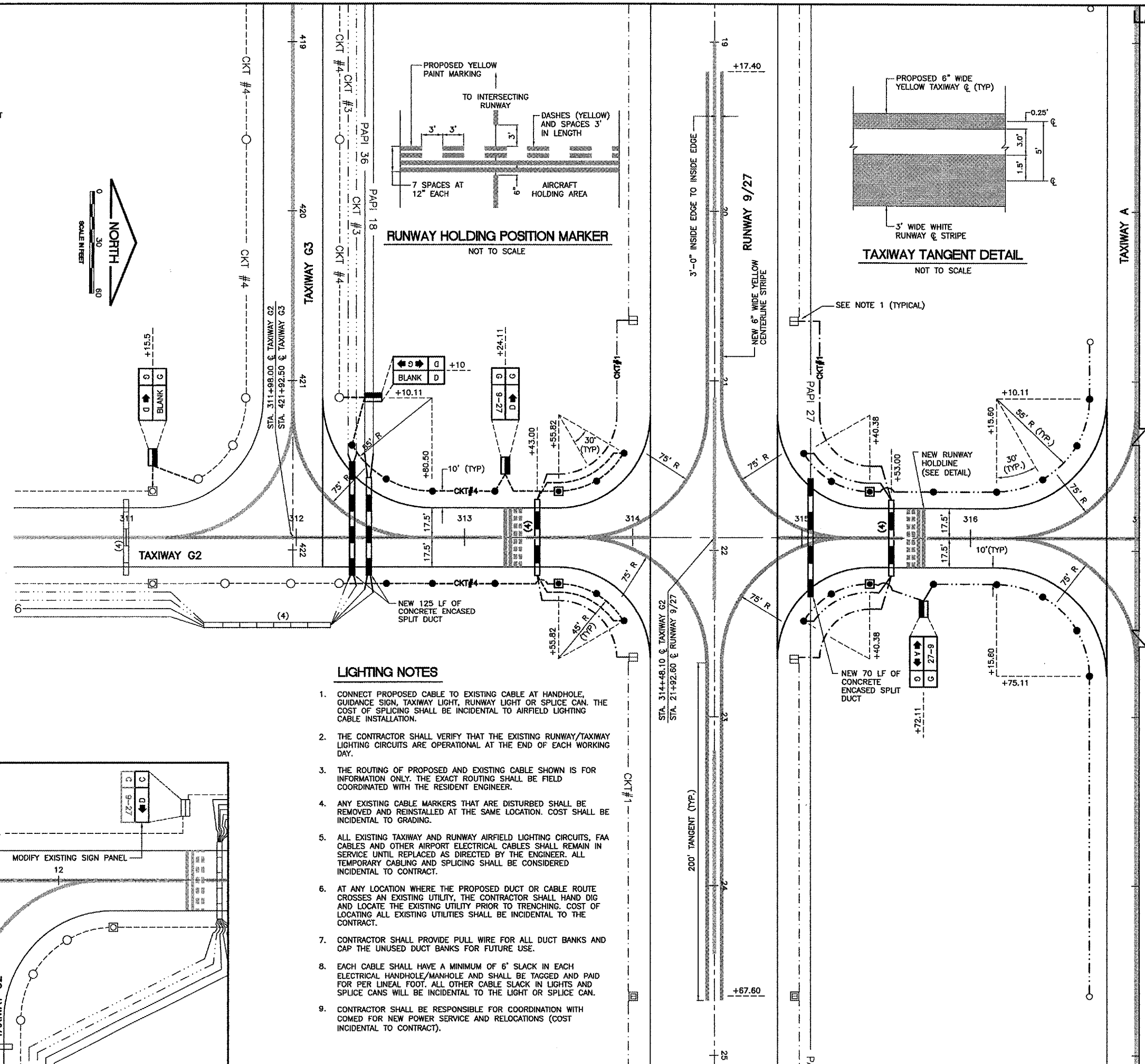
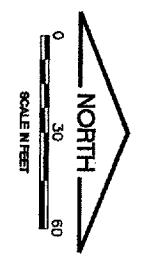


**LANSING MUNICIPAL AIRPORT  
 LANSING, ILLINOIS  
 NORTH QUADRANT SITEWORK - PHASE 1  
 AND TAXIWAY G2 EXTENSION  
 LIGHTING AND PAVEMENT MARKING PLAN  
 TAXIWAY G2 EXTENSION**

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DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	ARM
APPROVED BY:	
DATE:	03/04/05
JOB No:	03297-02
IL PROJECT: IGQ-3329 A.I.P. PROJECT: 3-17-0121-821	
SHEET 33 OF 50 SHEETS	

- LEGEND**
- NEW PAVEMENT
  - NEW STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT (MITL)
  - NEW BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT (MITL)
  - EXISTING STAKE MOUNTED TAXIWAY LIGHT
  - EXISTING BASE MOUNTED TAXIWAY LIGHT
  - NEW AIRFIELD GUIDANCE SIGN
  - EXISTING AIRFIELD GUIDANCE SIGN
  - NEW GRS CONDUIT, DIRECT BURY
  - NEW CONCRETE ENCASED DUCT (NUMBER OF WAYS NOTED)
  - EXISTING CONCRETE ENCASED DUCT (NUMBER OF WAYS SHOWN)
  - NEW POWER POLE (BY COMED)
  - NEW ELECTRICAL HANDHOLE
  - NEW ROADWAY TRANSCLOSURE
  - NEW GATE OPERATOR
  - NEW RETROREFLECTIVE MARKER
  - NEW ROADWAY LIGHT
  - NEW GROUND ROD
  - NEW POWER CABLE (NO. AND SIZE OF CONDUITS AS SHOWN) (DIRECT BURIED)
  - NEW 1/C #8 5KV, L-824 CABLE IN 34" UNIT DUCT CIRCUIT #1 RUNWAY 9/27
  - NEW 1/C #8 5KV, L-824 CABLE IN 34" UNIT DUCT CIRCUIT #1 TAXIWAY A
  - NEW 1/C #8 5KV, L-824 CABLE IN 34" UNIT DUCT CIRCUIT #1 TAXIWAY (G2,G3)
  - EXISTING RUNWAY 9/27 CIRCUIT
  - EXISTING TAXIWAY A CIRCUIT
  - EXISTING RUNWAY 18/36 CIRCUIT
  - EXISTING TAXIWAY CIRCUIT (G2,G3)
  - EXISTING RUNWAY 36 PAPI CIRCUIT
  - EXISTING RUNWAY 18 PAPI CIRCUIT
  - EXISTING RUNWAY 27 PAPI CIRCUIT
  - EXISTING AWOS CIRCUIT
  - NEW DEPRESSED CURB



**LIGHTING NOTES**

1. CONNECT PROPOSED CABLE TO EXISTING CABLE AT HANDHOLE, GUIDANCE SIGN, TAXIWAY LIGHT, RUNWAY LIGHT OR SPLICE CAN. THE COST OF SPLICING SHALL BE INCIDENTAL TO AIRFIELD LIGHTING CABLE INSTALLATION.
2. THE CONTRACTOR SHALL VERIFY THAT THE EXISTING RUNWAY/TAXIWAY LIGHTING CIRCUITS ARE OPERATIONAL AT THE END OF EACH WORKING DAY.
3. THE ROUTING OF PROPOSED AND EXISTING CABLE SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE FIELD COORDINATED WITH THE RESIDENT ENGINEER.
4. ANY EXISTING CABLE MARKERS THAT ARE DISTURBED SHALL BE REMOVED AND REINSTALLED AT THE SAME LOCATION. COST SHALL BE INCIDENTAL TO GRADING.
5. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS DIRECTED BY THE ENGINEER. ALL TEMPORARY CABLING AND SPLICING SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
6. AT ANY LOCATION WHERE THE PROPOSED DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
7. CONTRACTOR SHALL PROVIDE PULL WIRE FOR ALL DUCT BANKS AND CAP THE UNUSED DUCT BANKS FOR FUTURE USE.
8. EACH CABLE SHALL HAVE A MINIMUM OF 6' SLACK IN EACH ELECTRICAL HANDHOLE/MANHOLE AND SHALL BE TAGGED AND PAID FOR PER LINEAL FOOT. ALL OTHER CABLE SLACK IN LIGHTS AND SPLICE CANS WILL BE INCIDENTAL TO THE LIGHT OR SPLICE CAN.
9. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH COMED FOR NEW POWER SERVICE AND RELOCATIONS (COST INCIDENTAL TO CONTRACT).

