





**LEGEND**

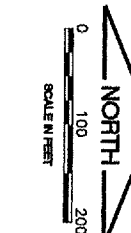
-  NEW PAVEMENT
-  FUTURE HANGAR
-  FUTURE PAVEMENT
-  PROPOSED SOIL BORING

**GEOTECHNICAL LAYOUT TABLE**

BORING/CORE NO.	STATION	OFFSET RUNWAY $\phi$	DEPTH	ELEVATION
B1	315+75 TXY G2	$\phi$	10'	610.83
B2	319+00 TXY G2	$\phi$	10'	613.18
B3	19+00 TXY A	310' LT.	10'	613.19
B4	24+80 TXY A	310' LT.	10'	613.18
B5	20+10 TXY A	515' LT.	10'	613.03
B6	23+00 TXY A	600' LT.	10'	612.95
B7	18+50 TXY A	710' LT.	10'	612.94
B8	319+00 TXY G2	600' RT.	10'	612.99
B9	322+50 TXY G2	810' RT.	10'	613.29

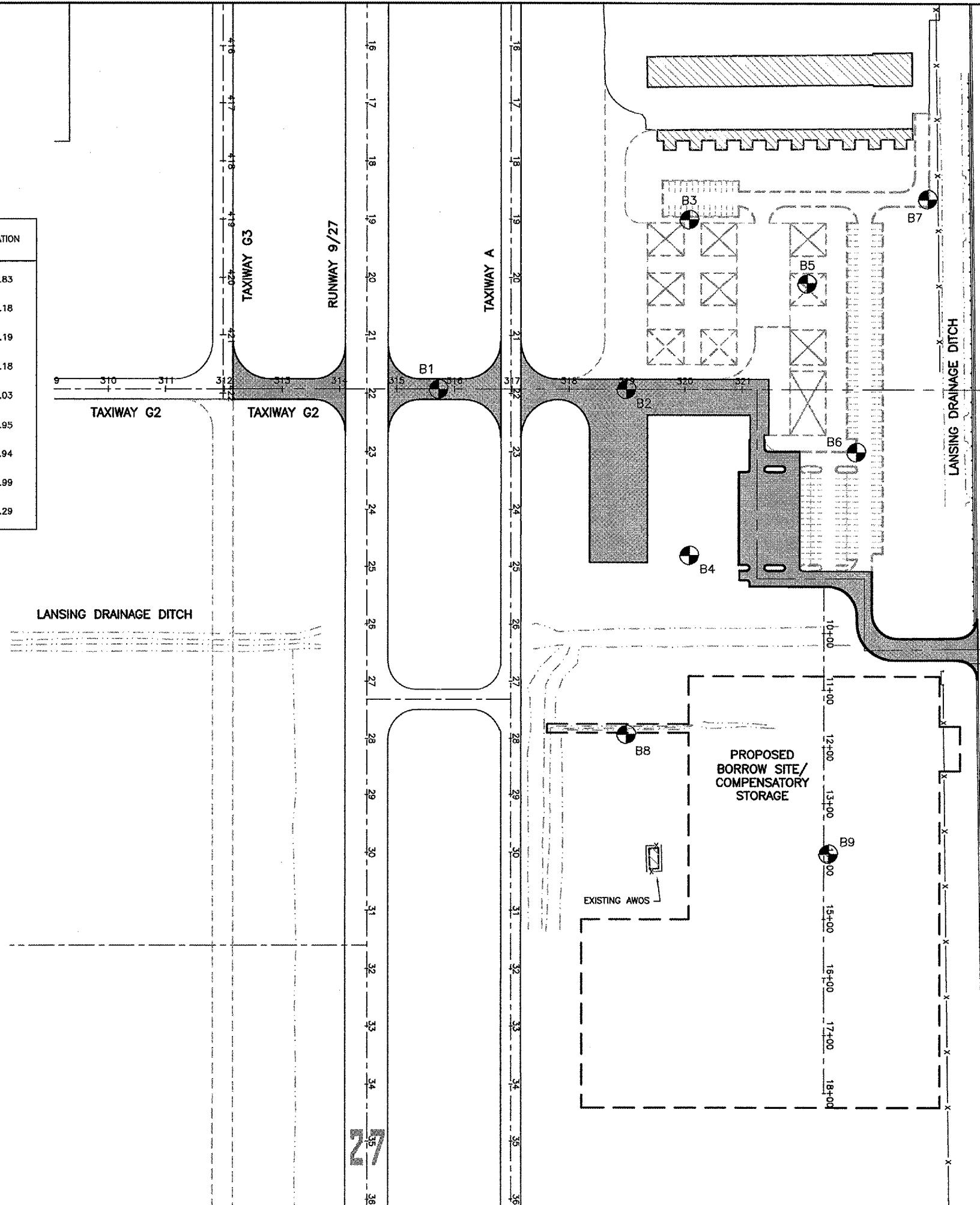
LA034

PATH: K:\0329702\sheets\  
 FILE: enginfo.dwg  
 UPDATE BY: johse  
 SURVEY BOOK #  
 XREF DWG:  
 DATE: Fri 3/26/04 2:18pm



REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2'  
 AT FULL SCALE (34x22).



PROJECT Lansing Municipal Airport, N. Quadrant Hangar Development, Lansing, IL  
 CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois  
 BORING 1 DATE STARTED 11-12-03 DATE COMPLETED 11-12-03 JOB L-59,265

ELEVATIONS  
 GROUND SURFACE 610.8  
 END OF BORING 600.8


WIRE DRILLING  
 AT END OF BORING Dry  
 24 HOURS Dry




LENGTH RECOVERY	SAMPLE NO.	TYPE	H	WC	Q <sub>u</sub>	γ <sub>DRY</sub>	DEPTH	ELEV.	SOIL DESCRIPTIONS
0							0.4	610.4	5" Black SILTY CLAY (topsoil), very moist
1	1	SS	4-5	19.9	3.83	4.25*			Very tough to hard brown and gray CLAY, moist A-6/A-7-6
2	2	SS	4-5	18.4	3.83	3.25*			
3	3	SS	3-4	20.5	2.25*		5.5	605.3	Very tough to tough gray CLAY, moist to very moist A-6/A-7-6
4	4	SS	3-4	20.2	1.29	1.5*			
							10.0		End of Boring at 10.0'

\* Approximate unconfined compressive strength based on measurements with a calibrated pocket penetrometer.

**LANSING MUNICIPAL AIRPORT  
 LANSING, ILLINOIS  
 NORTH QUADRANT SITEWORK - PHASE 1  
 AND TAXIWAY G2 EXTENSION  
 ENGINEERING INFORMATION - SHEET 1**

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 Lansing Municipal Airport

DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	ARM
APPROVED BY:	
DATE:	03/04/05
JOB No:	03297-02
IL PROJECT:	IGQ-3329
A.I.P. PROJECT:	3-17-0121-821
SHEET 45 OF 50 SHEETS	