

|                     |          |              |              |           |
|---------------------|----------|--------------|--------------|-----------|
| F.A. RTE.           | SECTION  | COUNTY       | TOTAL SHEETS | SHEET NO. |
| 347                 | LY-RS-3  | DUPAGE       | 35 #         | 1         |
| FED. ROAD DIST. NO. | ILLINOIS | CONTRACT NO. |              |           |

35 + 1 = 36

FOR INDEX OF SHEETS, SEE SHEET NO. 2

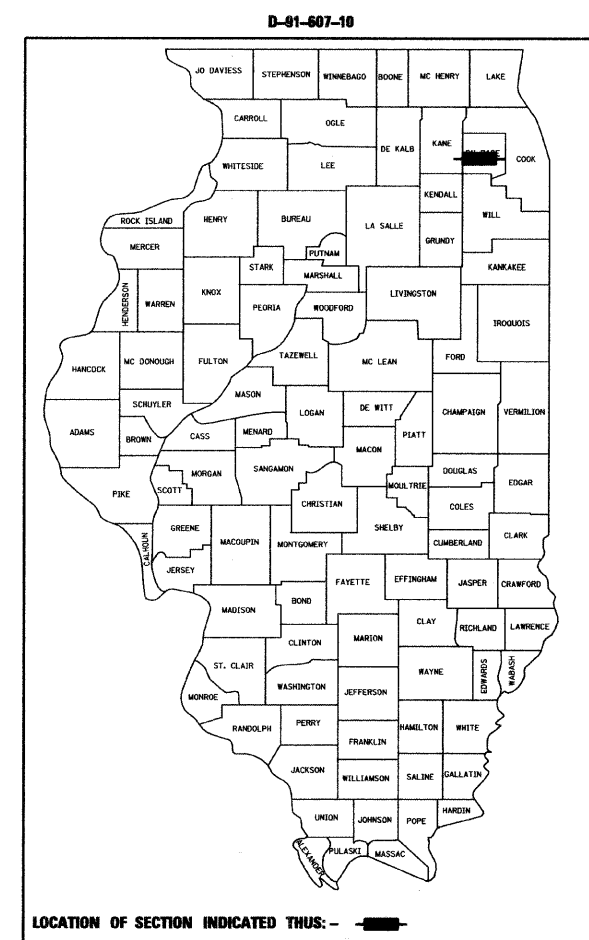
**DESIGN DESIGNATION:**  
**SPEED LIMIT: 45 TO 55 M.P.H.**  
**2010 ADT = 21,600**

**PROJECT LOCATED IN THE CITY OF WEST CHICAGO**

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

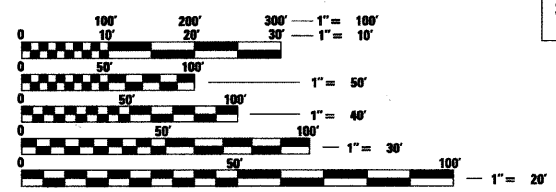
**PROPOSED HIGHWAY PLANS**

**FAP 347 (IL RTE 38)**  
**SECTION LY-RS-3**  
**KAUTZ RD (KANE COUNTY LINE) TO S NELTNOR BLVD (IL RTE 59)**  
**RESURFACING (3P)**  
**PROJECT: NHF-0347(026)**  
**DUPAGE COUNTY**  
**C-91-607-10**



LOCATION OF SECTION INDICATED THUS: - [shaded box] -

**HR Green**  
 420 N. Front Street, Suite 100  
 McHenry, IL 60050-2138  
 L 815.759.2013 F 815.385.1781  
 www.hrgreen.com



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

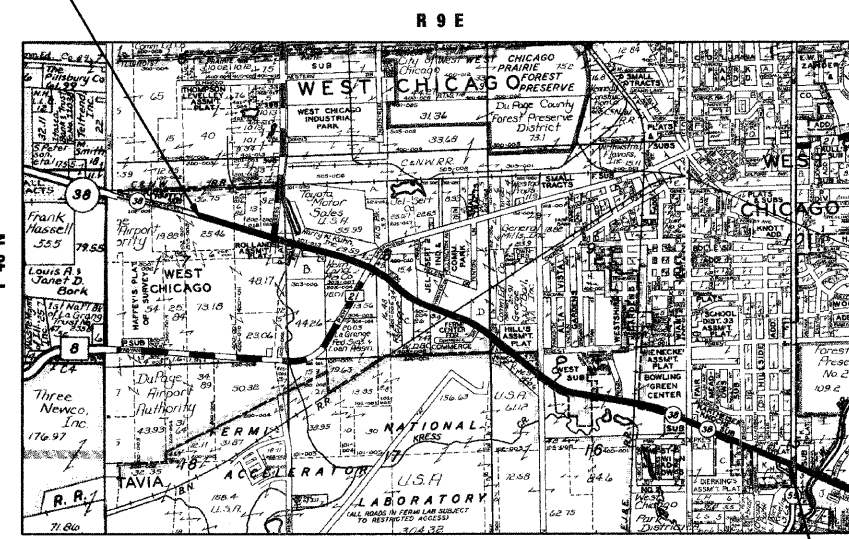
**J.U.L.I.E.**  
**JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION**  
**1-800-892-0123**  
**OR 811**

**DISTRICT ONE - PLAN PREP ENGINEER:**  
**KEN ENG: (847) 705-4247**

**CONTRACT NO. 60K82**

**OMISSIONS:**  
 STA. 169+00 TO STA. 170+62  
 STA. 214+48 TO STA. 215+88  
 STA. 268+25 TO STA. 270+90

**PROJECT BEGINS**  
**STA. 148+78**



**PROJECT ENDS**  
**STA. 312+67**

**GROSS LENGTH OF PROJECT = 16,389 FT. = 3.10 MILE**  
**NET LENGTH OF PROJECT = 15,822 FT. = 3.00 MILE**

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

SUBMITTED FEBRUARY 15, 2011

Diane M. O'Keefe  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 25 2011  
Scott E. Stitt, P.E.  
 acting ENGINEER OF DESIGN AND ENVIRONMENT

March 25 2011  
Christina M. Reed  
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PROFESSIONAL ENGINEER'S SIGN & SEAL**

Christopher M. Heston 3/10/11

EXPIRES: 11/30/2011

**PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS**

COMPANY NAME: #COMPANY/NAME  
 PROJECT CONTACT: #PROJECT/CONTACT  
 CLIENT: #CLIENT  
 DATE PLOTTED: 2/22/2011 3:56:20 PM  
 PLOT DRIVER: #PLOT/DRIVER  
 PEN TABLE: #PEN/TABLE

**INDEX OF SHEETS**

|       |   |
|-------|---|
| 1     | COVER SHEET   |
| 2     | INDEX OF SHEETS, LIST OF STATE STANDARDS, AND GENERAL NOTES   |
| 3     | SUMMARY OF QUANTITIES   |
| 4-9   | TYPICAL SECTIONS  |
| 10-15 | ROADWAY PLAN  |
| 16-21 | PAVEMENT MARKING PLAN   |
| 22-24 | DETECTOR LOOP PLAN  |
| 25    | DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER $\geq 15'$ (4.5 m) (BD-01) |
| 26    | DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)   |
| 27    | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)   |
| 28    | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)   |
| 29    | BUTT JOINT AND HMA TAPER DETAILS (BD-32)  |
| 30    | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)                       |
| 31    | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-FLOW RESISTANT) (TC-11)                     |
| 32    | DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)  |
| 33    | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING  |
| 34    | ARTERIAL ROAD INFORMATION SIGN  |
| 35    | DISTRICT 1 - TRAFFIC SIGNAL DESIGN DETAIL (TS-05)   |
| 35A   | DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-0T)                           |

**LIST OF STATE STANDARDS**

| STANDARD NO.         | DESCRIPTION   |
|----------------------|---|
| 000001-06            | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS                                    |
| 442201-03            | CLASS C AND D PATCHES   |
| 701421-03            | LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS $> 45$ MPH TO 55 MPH   |
| 701426-04            | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS $> 45$ MPH |
| 701602-05            | URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE             |
| 701606-07            | URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL MOUNTABLE MEDIAN           |
| 701701-07            | URBAN LANE CLOSURE, MULTILANE INTERSECTION                                      |
| 701901-01            | TRAFFIC CONTROL DEVICES   |
| 780001-02            | TYPICAL PAVEMENT MARKINGS   |
| <del>781001-03</del> | <del>TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS</del>              |
| 886001-01            | DETECTOR LOOP INSTALLATIONS   |
| 886006-01            | TYPICAL LAYOUTS FOR DETECTION LOOPS   |

**GENERAL NOTES**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 OR (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).
- 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND CONDITIONS IN THE FIELD UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF WEST CHICAGO
- WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- SAW CUTTING OF PAVEMENTS, SIDEWALK, CURB & GUTTER, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1.5 INCHES (40mm) WHERE THE SPEED LIMIT IS 40 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKING ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE TYPE III SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKINGS.
- THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
- THE CONTRACTOR SHALL CONTACT MR. DON CHIARUGI THE AREA TRAFFIC FIELD ENGINEER, AT 847 741-9857 AT LEAST TWO (2) WEEKS PRIOR TO THE BEGINNING OF WORK.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE THE 'BUTT JOINT AND HMA TAPER DETAILS' INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- MATCH EXISTING PAVEMENT MARKINGS AT THE PROJECT LIMITS.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS  
 DATE PLOTTED: 2/12/2011 6:46:54 PM  
 FILE NAME: 86100196.02-gen.dgn  
 PLOT DRIVER: pdf.plt  
 PEN TABLE: standard-trans.tbl


**SEC Group, Inc.**  
 An HR Green Company  
 420 N. Ford Street  
 Midway, IL 60050-2138  
 1 815.365.1778 F 815.365.1781  
 www.secgroup.com  
 Anthony E. Winkler, L. New Lenox, IL Chicago, IL

|                       |                        |           |
|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = N.T.S.   | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/12/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/12/11         | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, LIST OF STATE STANDARDS AND GENERAL NOTES**  
**ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.P. RTE.  | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------|---------|--------|--------------|-----------|
| 347          | LY-RS-3 | DUPAGE | 35           | 2         |
| CONTRACT NO. |         |        | 60K82        |           |

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

Rev.

SUMMARY OF QUANTITIES

URBAN  
80% FED.  
20% STATE

URBAN  
80% FED.  
20% STATE

| CODE NO. | ITEM   | UNIT   | TOTAL QUANTITY | ROADWAY 0005 |
|----------|--|--------|----------------|--------------|
| 40600200 | BITUMINOUS MATERIALS (PRIME COAT)                    | TON    | 44             | 44           |
| 40600300 | AGGREGATE (PRIME COAT)                               | TON    | 220            | 220          |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS           | TON    | 40             | 40           |
| 40600895 | CONSTRUCTING TEST STRIP                              | EACH   | 1              | 1            |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT         | SQ YD  | 585            | 585          |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70         | TON    | 9,515          | 9,515        |
| 42300400 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH   | SQ YD  | 10             | 10           |
| 42001300 | PROTECTIVE COAT                                      | SQ YD  | 192            | 192          |
| 44000159 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"              | SQ YD  | 113,250        | 113,250      |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL                            | SQ YD  | 10             | 10           |
| 44201798 | CLASS D PATCHES, TYPE I, 13 INCH                     | SQ YD  | 29             | 29           |
| 44201803 | CLASS D PATCHES, TYPE II, 13 INCH                    | SQ YD  | 612            | 612          |
| 44201807 | CLASS D PATCHES, TYPE III, 13 INCH                   | SQ YD  | 728            | 728          |
| 44201809 | CLASS D PATCHES, TYPE IV, 13 INCH                    | SQ YD  | 1,544          | 1,544        |
| 48102100 | AGGREGATE WEDGE SHOULDER, TYPE B                     | TON    | 1,281          | 1,281        |
| 60618210 | HOT-MIX ASPHALT MEDIAN SURFACE, 4 INCH               | SQ FT  | 735            | 735          |
| 60618300 | CONCRETE MEDIAN SURFACE, 4 INCH                      | SQ FT  | 1,905          | 1,905        |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A                      | CAL MO | 6              | 6            |
| 67100100 | MOBILIZATION   | L SUM  | 1              | 1            |
| 70100310 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701421      | L SUM  | 1              | 1            |
| 70102625 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701606      | L SUM  | 1              | 1            |
| 70102632 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701602      | L SUM  | 1              | 1            |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701      | L SUM  | 1              | 1            |
| 70300100 | SHORT TERM PAVEMENT MARKING                          | FOOT   | 23,000         | 23,000       |
| 70300210 | TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS     | SQ FT  | 984            | 984          |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4"                 | FOOT   | 64,280         | 64,280       |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6"                 | FOOT   | 2,525          | 2,525        |
| 70300250 | TEMPORARY PAVEMENT MARKING - LINE 8"                 | FOOT   | 330            | 330          |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12"                | FOOT   | 625            | 625          |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24"                | FOOT   | 510            | 510          |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL                   | SQ FT  | 7,670          | 7,670        |
| 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT  | 984            | 984          |
| 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4"             | FOOT   | 64,280         | 64,280       |
| 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6"             | FOOT   | 2,525          | 2,525        |
| 78000500 | THERMOPLASTIC PAVEMENT MARKING - LINE 8"             | FOOT   | 330            | 330          |
| 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12"            | FOOT   | 625            | 625          |
| 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24"            | FOOT   | 510            | 510          |

| CODE NO. | ITEM   | UNIT  | TOTAL QUANTITY | ROADWAY 0005 |
|----------|--|-------|----------------|--------------|
| 78100100 | RAISED REFLECTIVE PAVEMENT MARKER                            | EACH  | 775            | 775          |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL                    | EACH  | 775            | 775          |
| 88600600 | DETECTOR LOOP REPLACEMENT                                    | FOOT  | 1,558          | 1,558        |
| X2020110 | GRADING AND SHAPING SHOULDERS                                | UNIT  | 220            | 220          |
| X4060826 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50   | TON   | 6,342          | 6,342        |
| X4403800 | MEDIAN SURFACE REMOVAL                                       | SQ FT | 2,640          | 2,640        |
| X5539700 | STORM SEWERS TO BE CLEANED                                   | FOOT  | 800            | 800          |
| X6030310 | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)                     | EACH  | 17             | 17           |
| Z0004562 | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT  | 575            | 575          |
| Z0017700 | DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED            | EACH  | 5              | 5            |
| Z0018500 | DRAINAGE STRUCTURES TO BE CLEANED                            | EACH  | 50             | 50           |
| Z0030850 | TEMPORARY INFORMATION SIGNING                                | SQ FT | 129            | 129          |
| Z0048665 | RAILROAD PROTECTIVE LIABILITY INSURANCE                      | L SUM | 1              | 1            |

\* SPECIALTY ITEMS  
Δ = Non-participating

COMPANY NAME: #COMPANY NAME/  
PROJECT CONTACT: #PROJECT CONTACT/  
CLIENT: #CLIENT/  
DATE PLOTTED: 2/14/2011 8:47:09 AM  
FILE NAME: 86100196.02-sum.dgn  
PLOT DRIVER: pdf.plt  
PEN TABLE: standard-trans.tbl

SEC Group, Inc.  
An HR Green Company  
420 N. Ford Street  
Methuen, IL 60050-4138  
T. 815.386.1778 F. 815.386.1781  
www.secgroup.com

|                       |                        |           |
|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = N.T.S.   | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/14/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/14/11         | REVISED - |

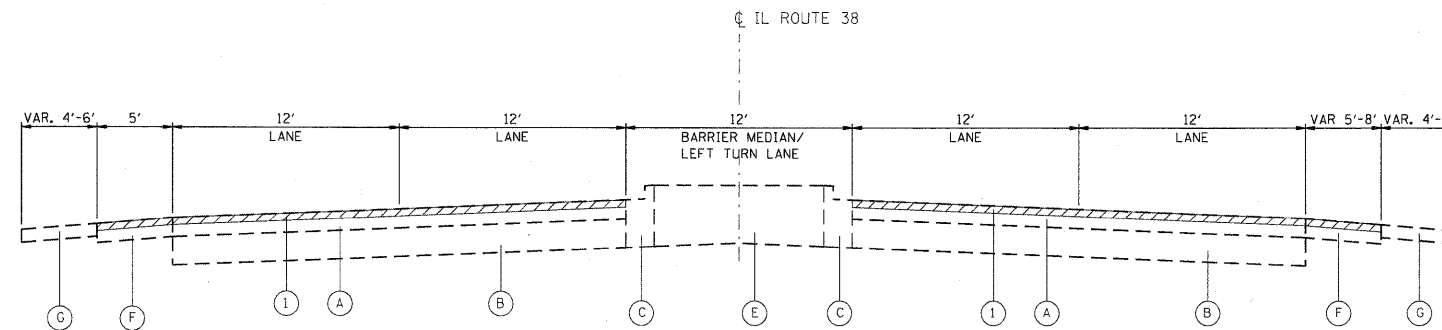
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES  
ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)

|                              |                 |                  |                                      |             |
|------------------------------|-----------------|------------------|--------------------------------------|-------------|
| F.A.P. RTE. 347              | SECTION LY-RS-3 | COUNTY DUPAGE    | TOTAL SHEETS 35                      | SHEET NO. 3 |
| SCALE: N.T.S.                |                 |                  | SHEET NO. 1 OF 1 SHEETS STA. TO STA. |             |
| FED. ROAD DIST. NO. ILLINOIS |                 | FED. AID PROJECT |                                      |             |

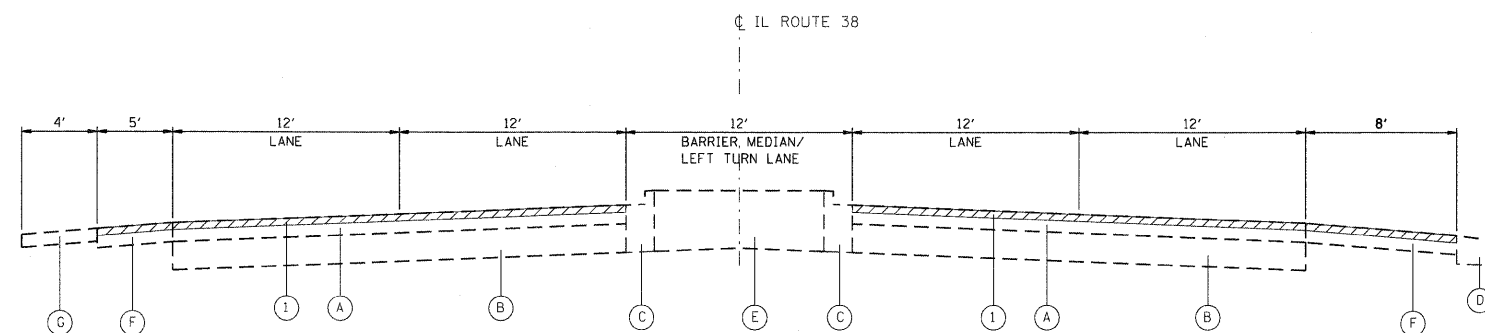
Rev.

CONTRACT NO. 60K82



EXISTING TYPICAL SECTION

STA. 148+78 TO STA. 159+06  
 STA. 197+00 TO STA. 204+00  
 STA. 212+00 TO STA. 214+48  
 STA. 215+88 TO STA. 261+00



EXISTING TYPICAL SECTION

STA. 159+06 TO STA. 169+00  
 STA. 170+62 TO STA. 177+99  
 STA. 204+00 TO STA. 212+00

EXISTING CONDITIONS

- (A) HOT-MIX ASPHALT BINDER SURFACE COURSE, +.6"
- (B) P.C. CONCRETE BASE COURSE, 9" & VARIES
- (C) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (E) BARRIER CONCRETE MEDIAN
- (F) HMA SHOULDERS
- (G) AGGREGATE SHOULDERS

PROPOSED IMPROVEMENTS

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- (4) COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT (AT LOCATIONS AS DETERMINED BY THE ENGINEER)
- (5) AGGREGATE WEDGE SHOULDER, TYPE B (TAPER 3" TO 1" THICKNESS)

COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 2/14/2011 11:41:39 AM  
 FILE NAME: 8610056.02-1-yp-01.dgn  
 PLOT DRIVER: standard.ctb  
 PLOT TABLE:

**SEC Group, Inc.**  
 An HR Green Company  
 600 N. First Street  
 Maitland, FL 32751-2136  
 P: 407.395.1778 F: 407.395.1781  
 www.secgroup.com  
 4400 N. Hennepin Ave., Roseville, IL, Chicago, IL

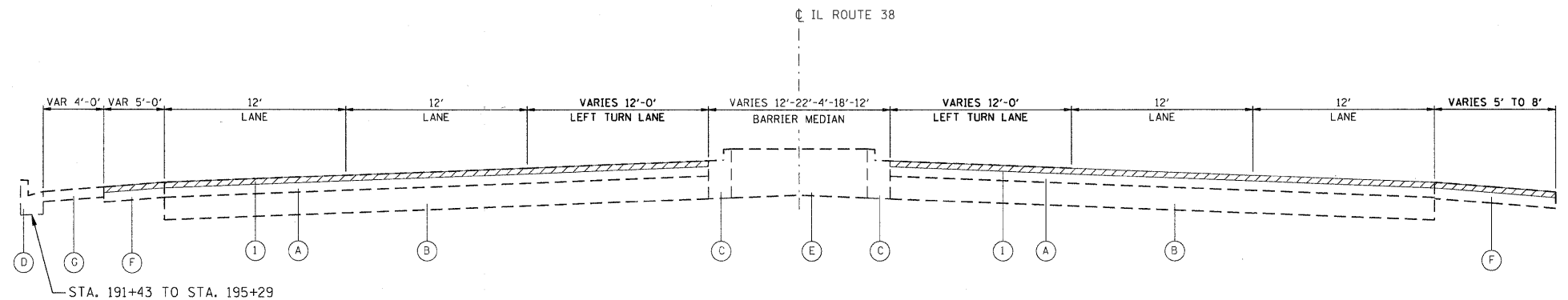
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| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = N.T.S.   | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/14/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/14/11         | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

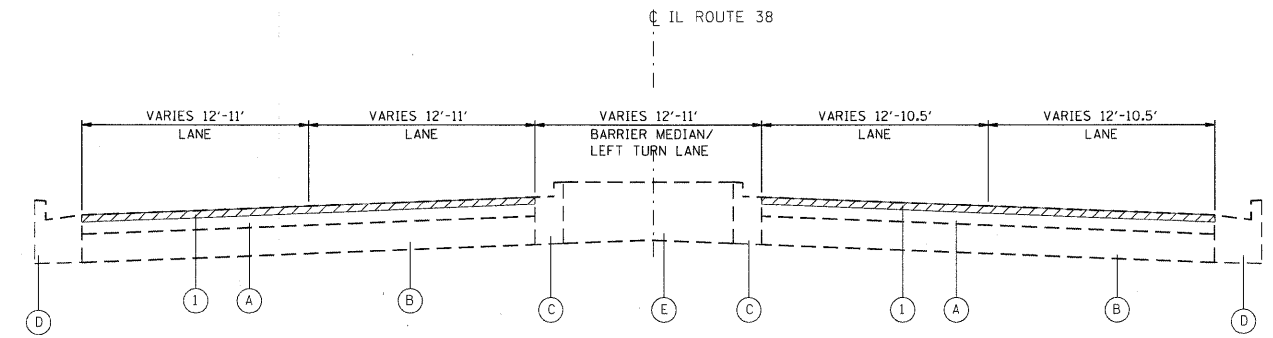
**TYPICAL SECTIONS  
 ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)**

SCALE: N.T.S. SHEET NO. 1 OF 6 SHEETS STA. TO STA.

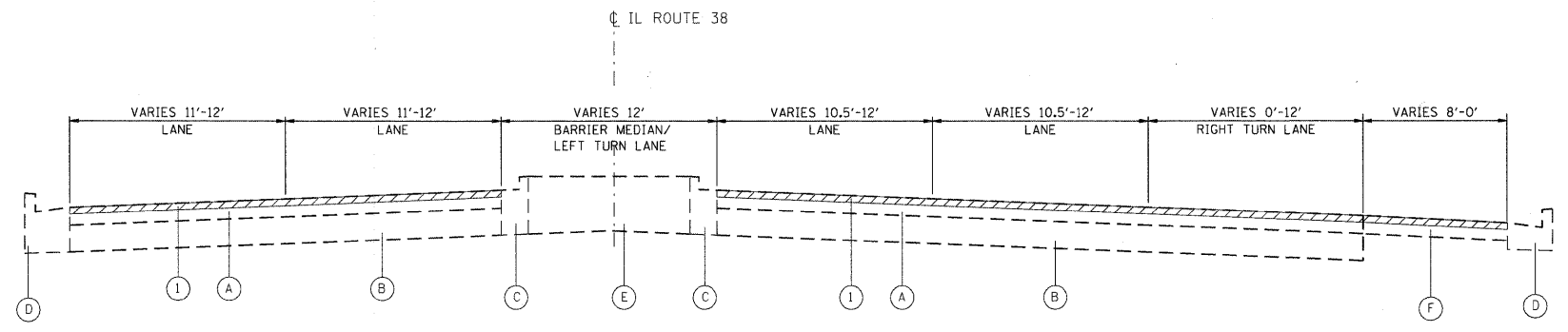
| F.A.P. RTE.         | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
|---------------------|---------|--------|---------------------------|-----------|
| 347                 | LY-RS-3 | DUPAGE | 35                        | 4         |
| FED. ROAD DIST. NO. |         |        | ILLINOIS FED. AID PROJECT |           |
|                     |         |        | CONTRACT NO. 60K82        |           |



EXISTING TYPICAL SECTION  
STA. 177+99 TO STA. 197+00



EXISTING TYPICAL SECTION  
STA. 261+00 TO STA. 268+25



EXISTING TYPICAL SECTION  
STA. 270+90 TO STA. 285+46.10

EXISTING CONDITIONS

- (A) HOT-MIX ASPHALT BINDER SURFACE COURSE, +.6"
- (B) P.C. CONCRETE BASE COURSE, 9" & VARIES
- (C) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (E) BARRIER CONCRETE MEDIAN
- (F) HMA SHOULDERS
- (G) AGGREGATE SHOULDERS

PROPOSED IMPROVEMENTS

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- (4) COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT (AT LOCATIONS AS DETERMINED BY THE ENGINEER)
- (5) AGGREGATE WEDGE SHOULDER, TYPE B (TAPER 3" TO 1" THICKNESS)

COMPANY NAME: #/COMPANY/NAME  
 PROJECT CONTACT: #/PROJECT/CONTACT  
 CLIENT: #/CLIENT/  
 DATE PLOTTED: 2/14/2011 11:41:42 AM  
 FILE NAME: 86100196.02-1yp-02.dgn  
 PLOT DRIVER: pcalp  
 PLOT TABLES: standard-trans.tbl

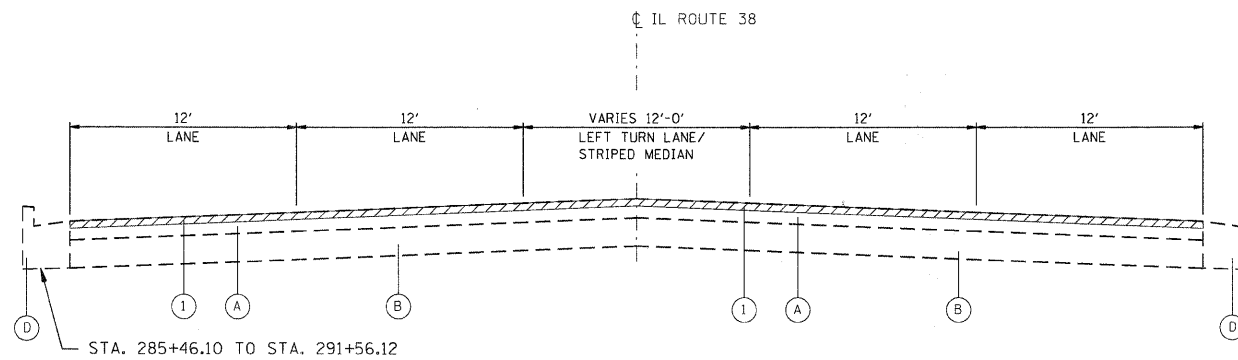
**SBC Group, Inc.**  
 An RBC Group Company  
 420 N. First Street,  
 Moline, IL 61902-2099  
 P 815.385.7778 F 815.385.1781  
 www.sbcgroupinc.com  
 Midland, TX • Houston, TX • New Orleans, LA • Chicago, IL

|                       |                        |           |
|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = N.T.S.   | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/14/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/14/11         | REVISED - |

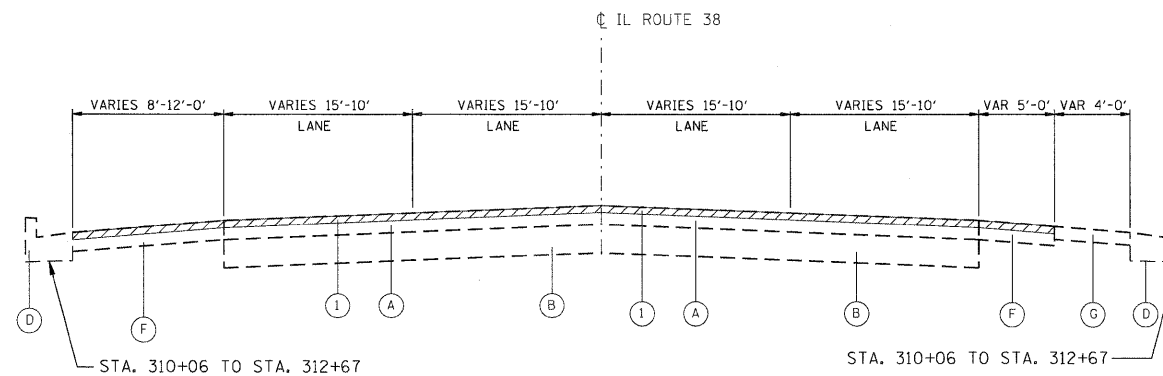
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

|  |                         |
|--|-------------------------|
| <b>TYPICAL SECTIONS</b>                                    |                         |
| <b>ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)</b> |                         |
| SCALE: N.T.S.  | SHEET NO. 2 OF 6 SHEETS |
| STA.   | TO STA.                 |

| F.A.P. RTE.         | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
|---------------------|---------|--------|---------------------------|-----------|
| 347                 | LY-RS-3 | DUPAGE | 35                        | 5         |
| FED. ROAD DIST. NO. |         |        | ILLINOIS FED. AID PROJECT |           |
|                     |         |        | CONTRACT NO. 60K82        |           |



EXISTING TYPICAL SECTION  
STA. 285+46.10 TO STA. 292+42.77



EXISTING TYPICAL SECTION  
STA. 292+42.77 TO STA. 312+67

EXISTING CONDITIONS

- A HOT-MIX ASPHALT BINDER SURFACE COURSE, +.6"
- B P.C. CONCRETE BASE COURSE, 9" & VARIES
- C COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- D COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- E BARRIER CONCRETE MEDIAN
- F HMA SHOULDERS
- G AGGREGATE SHOULDERS

PROPOSED IMPROVEMENTS

- 1 HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- 2 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- 3 POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50, 1"
- 4 COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT (AT LOCATIONS AS DETERMINED BY THE ENGINEER)
- 5 AGGREGATE WEDGE SHOULDER, TYPE B (TAPER 3" TO 1" THICKNESS)

COMPANY NAME: SEC Group, Inc.  
PROJECT CONTACT: J. ROITBURD  
CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
DATE PLOTTED: 2/14/2011 11:41:44 AM  
FILE NAME: 86100196.02-11yp-03.dgn  
PLOT DRIVER: pdt11p  
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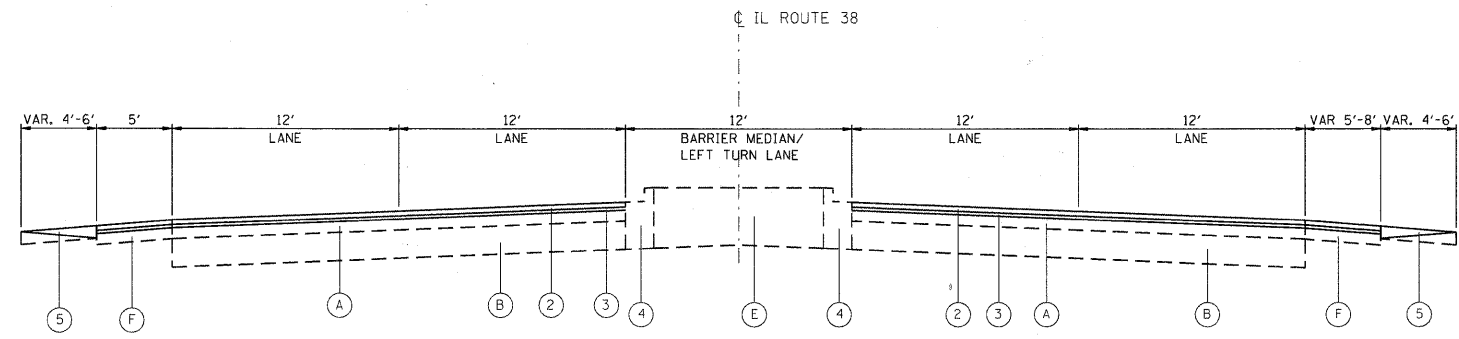
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| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = N.T.S.   | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/14/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/14/11         | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

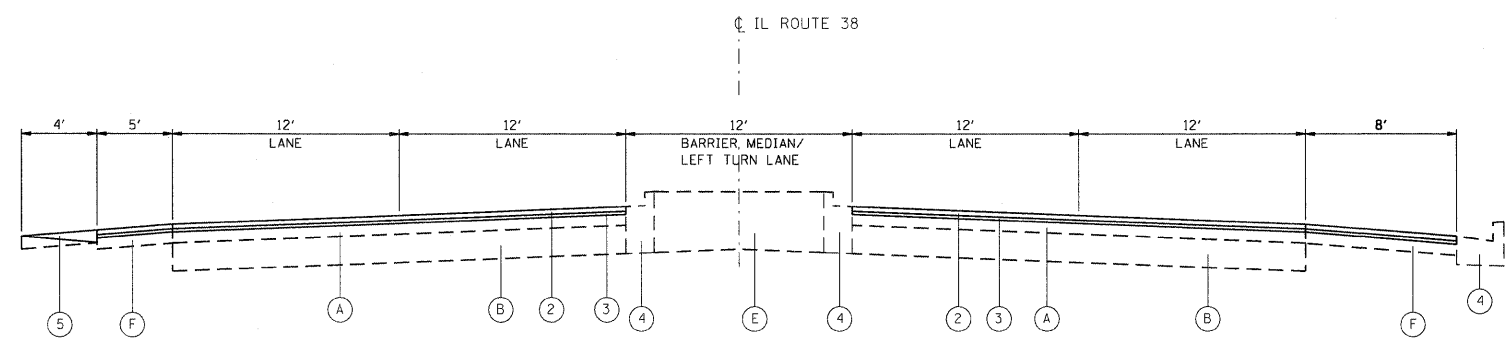
TYPICAL SECTIONS  
ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)

SCALE: N.T.S. SHEET NO. 3 OF 6 SHEETS STA. TO STA.

| F.A.P. RTE.         | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
|---------------------|---------|--------|---------------------------|-----------|
| 347                 | LY-RS-3 | DUPAGE | 35                        | 6         |
| FED. ROAD DIST. NO. |         |        | ILLINOIS FED. AID PROJECT |           |
|                     |         |        | CONTRACT NO. 60K82        |           |



**PROPOSED TYPICAL SECTION**  
 STA. 148+78 TO STA. 159+06  
 STA. 197+00 TO STA. 204+00  
 STA. 212+00 TO STA. 214+48  
 STA. 215+88 TO STA. 261+00



**PROPOSED TYPICAL SECTION**  
 STA. 159+06 TO STA. 169+00  
 STA. 170+62 TO STA. 177+99  
 STA. 204+00 TO STA. 212+00

**NOTES:**  
 THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING. SEE DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT.

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

| MIXTURE TYPE   | AIR VOIDS    |
|--|--------------|
| <b>PAVEMENT RESURFACING</b>                                |              |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm)    | 4% @ 70 GYR. |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | 4% @ 50 GYR. |
| <b>HMA SHOULDERS</b>                                       |              |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm)    | 4% @ 70 GYR. |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | 4% @ 50 GYR. |
| <b>PATCHING</b>  |              |
| CLASS D PATCHES (HMA BINDER, IL-19mm)                      | 4% @ 70 GYR. |
| <b>DRIVEWAYS</b>   |              |
| HMA SURFACE COURSE, MIX "C", N 50 (IL 9.5 mm); 2"          | 4% @ 50 GYR. |
| HMA BASE COURSE (HMA BINDER IL-19mm); PE -6", CE -8"       | 4% @ 50 GYR. |

THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/50 YD<sup>3</sup>/IN.  
 THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.  
 FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

**EXISTING CONDITIONS**

- A HOT-MIX ASPHALT BINDER SURFACE COURSE, +.6"
- B P.C. CONCRETE BASE COURSE, 9" & VARIES
- C COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- D COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- E BARRIER CONCRETE MEDIAN
- F HMA SHOULDERS
- G AGGREGATE SHOULDERS

**PROPOSED IMPROVEMENTS**

- 1 HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- 2 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- 3 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- 4 COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT (AT LOCATIONS AS DETERMINED BY THE ENGINEER)
- 5 AGGREGATE WEDGE SHOULDER, TYPE B (TAPER 3" TO 1" THICKNESS)

COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 2/14/2011 11:41:46 AM  
 FILE NAME: 86100196.02-1yp-04.dgn  
 PLOT DRIVER: pldrvr1  
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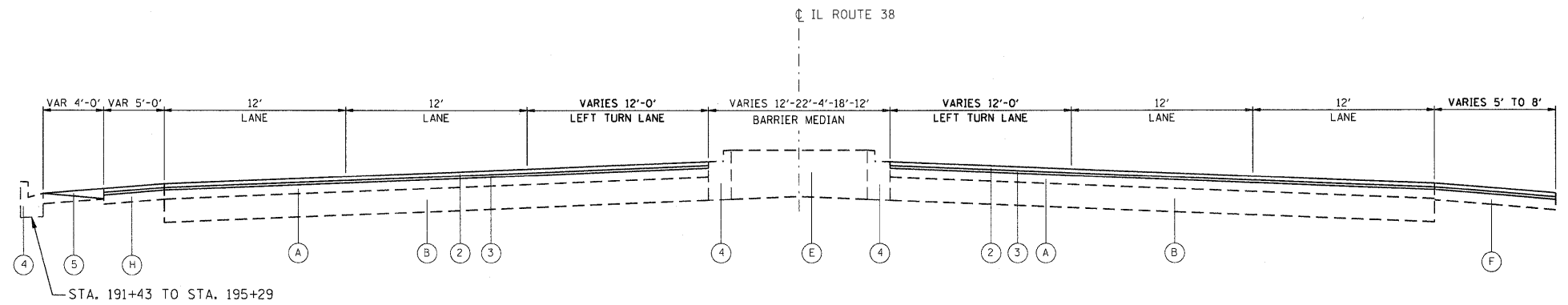


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|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = N.T.S.   | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/14/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/14/11         | REVISED - |

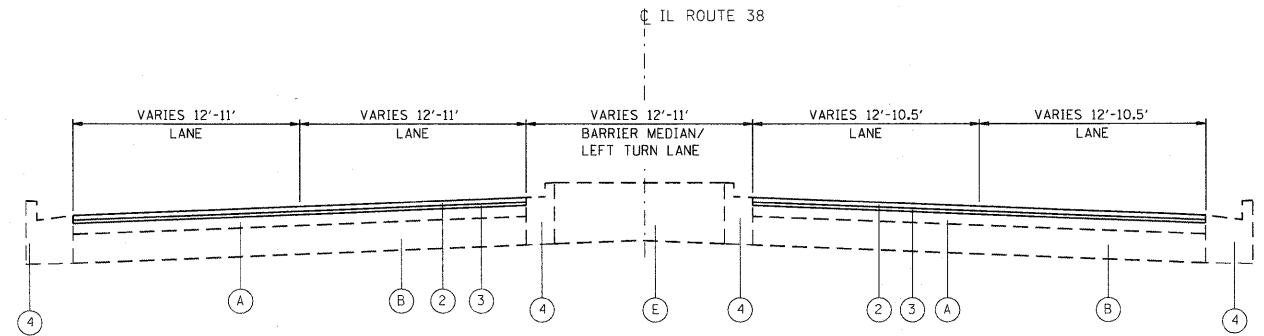
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|  |                         |
|--|-------------------------|
| <b>TYPICAL SECTIONS</b>                                    |                         |
| <b>ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)</b> |                         |
| SCALE: N.T.S.  | SHEET NO. 4 OF 6 SHEETS |
| STA. _____   | TO STA. _____           |

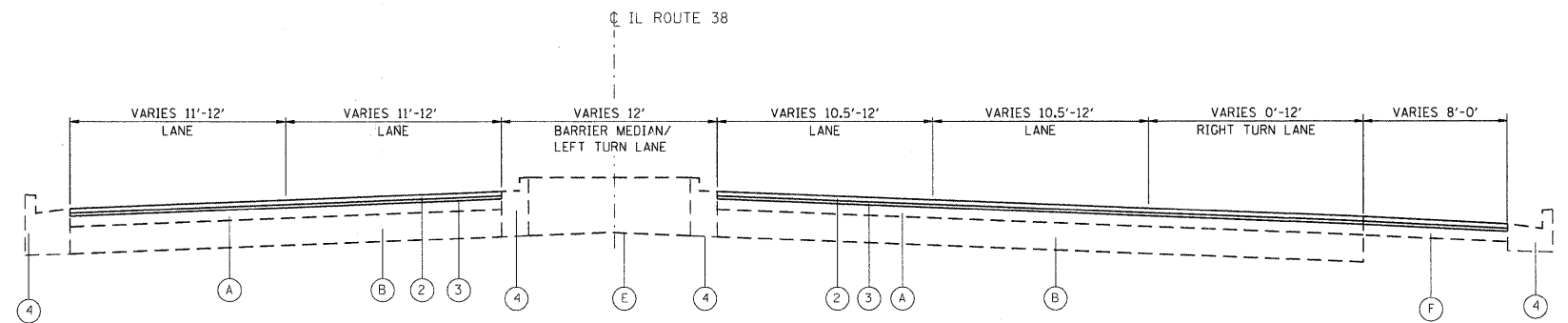
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|---|-----------------|---------------|-----------------|-------------|
| F.A.P. RTE. 347                               | SECTION LY-RS-3 | COUNTY DUPAGE | TOTAL SHEETS 35 | SHEET NO. 7 |
| CONTRACT NO. 60K82                            |                 |               |                 |             |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                 |               |                 |             |



PROPOSED TYPICAL SECTION  
STA. 177+99 TO STA. 197+00



PROPOSED TYPICAL SECTION  
STA. 261+00 TO STA. 268+25



PROPOSED TYPICAL SECTION  
STA. 270+90 TO STA. 285+46.10

EXISTING CONDITIONS

- (A) HOT-MIX ASPHALT BINDER SURFACE COURSE, +.6"
- (B) P.C. CONCRETE BASE COURSE, 9" & VARIES
- (C) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (E) BARRIER CONCRETE MEDIAN
- (F) HMA SHOULDERS
- (G) AGGREGATE SHOULDERS

PROPOSED IMPROVEMENTS

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", NTO, 1 1/2"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- (4) COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT (AT LOCATIONS AS DETERMINED BY THE ENGINEER)
- (5) AGGREGATE WEDGE SHOULDER, TYPE B (TAPER 3" TO 1" THICKNESS)

NOTES:  
THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING. SEE DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT.

COMPANY NAME: SEC Group, Inc.  
PROJECT CONTACT: J. ROITBURD  
CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
DATE PLOTTED: 2/14/2011 11:41:48 AM  
FILE NAME: 86100196.02-1yp-05.dgn  
PLOT DRIVER: pdrlapl  
PLOT TABLE: standard-trns.tbl

**SEC Group, Inc.**  
An HNTB Group Company  
401 N. First Street  
Chicago, IL 60654-2138  
P: 815.386.1778 F: 815.386.1781  
www.secgroup.com  
©2011 SEC Group, Inc. - All Rights Reserved - Chicago, IL

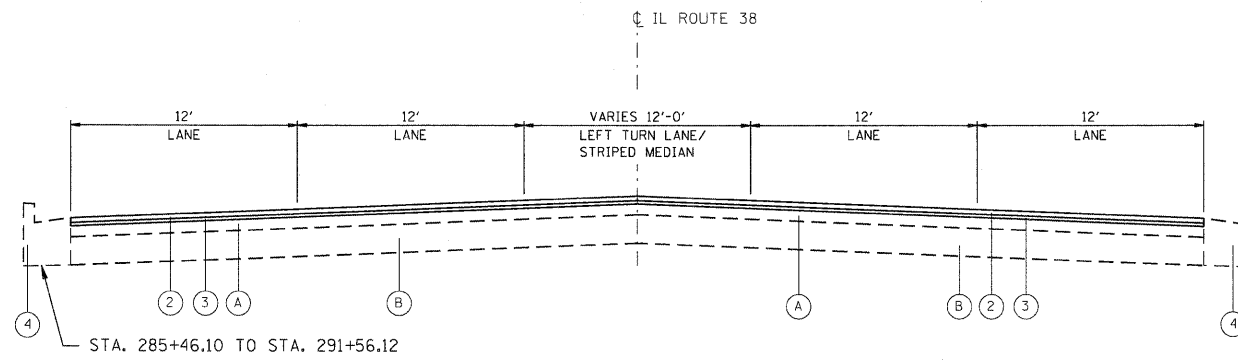
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| USER NAME = SPernell  | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = N.T.S.   | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/14/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/14/11         | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

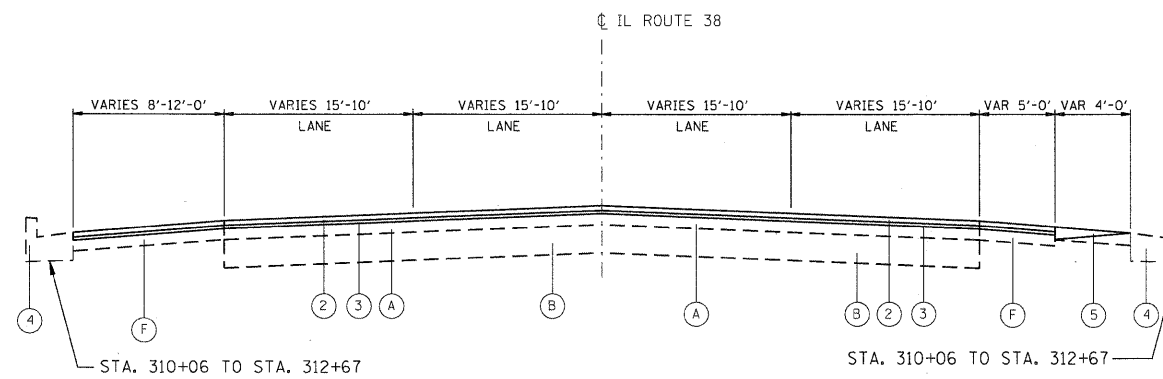
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| <b>TYPICAL SECTIONS</b>                                    |                         |
| <b>ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)</b> |                         |
| SCALE: N.T.S.  | SHEET NO. 5 OF 6 SHEETS |
| STA.   | TO STA.                 |

|                     |         |        |                           |                    |
|---------------------|---------|--------|---------------------------|--------------------|
| F.A.P. RTE.         | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO.          |
| 347                 | LY-RS-3 | DUPAGE | 35                        | 8                  |
| FED. ROAD DIST. NO. |         |        | ILLINOIS FED. AID PROJECT | CONTRACT NO. 60K82 |





PROPOSED TYPICAL SECTION  
STA. 285+46.10 TO STA. 292+42.77



PROPOSED TYPICAL SECTION  
STA. 292+42.77 TO STA. 312+67

**EXISTING CONDITIONS**

- (A) HOT-MIX ASPHALT BINDER SURFACE COURSE, +.6"
- (B) P.C. CONCRETE BASE COURSE, 9" & VARIES
- (C) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (E) BARRIER CONCRETE MEDIAN
- (F) HMA SHOULDERS
- (G) AGGREGATE SHOULDERS

**PROPOSED IMPROVEMENTS**

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (2) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- (4) COMBINATION CONCRETE CURB & GUTTER REMOVAL AND REPLACEMENT (AT LOCATIONS AS DETERMINED BY THE ENGINEER)
- (5) AGGREGATE WEDGE SHOULDER, TYPE B (TAPER 3" TO 1" THICKNESS)

**NOTES:**  
THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING. SEE DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT.

COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 2/14/2011 11:41:51 AM  
 FILE NAME: 86100196.02-Typ-06.dgn  
 PLOT DRIVER: pdtcalif  
 PEN TABLE: Standard.ctb



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|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = N.T.S.   | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/14/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/14/11         | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

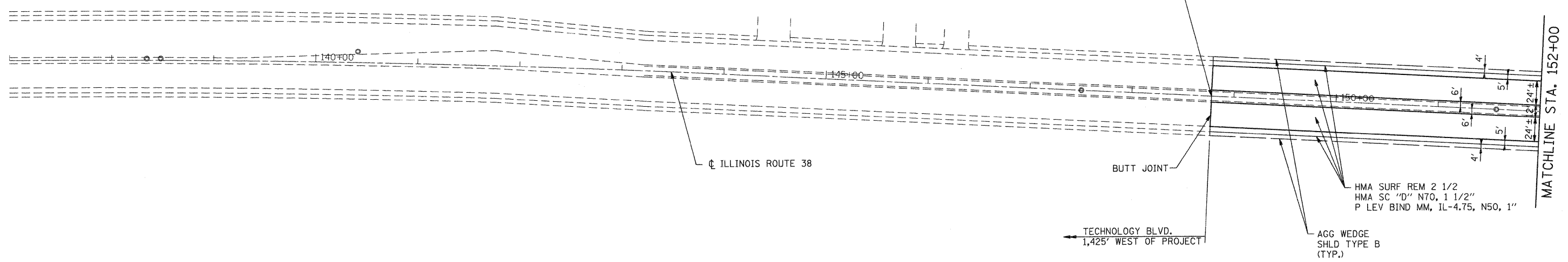
**TYPICAL SECTIONS  
ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)**

SCALE: N.T.S. SHEET NO. 6 OF 6 SHEETS STA. TO STA.

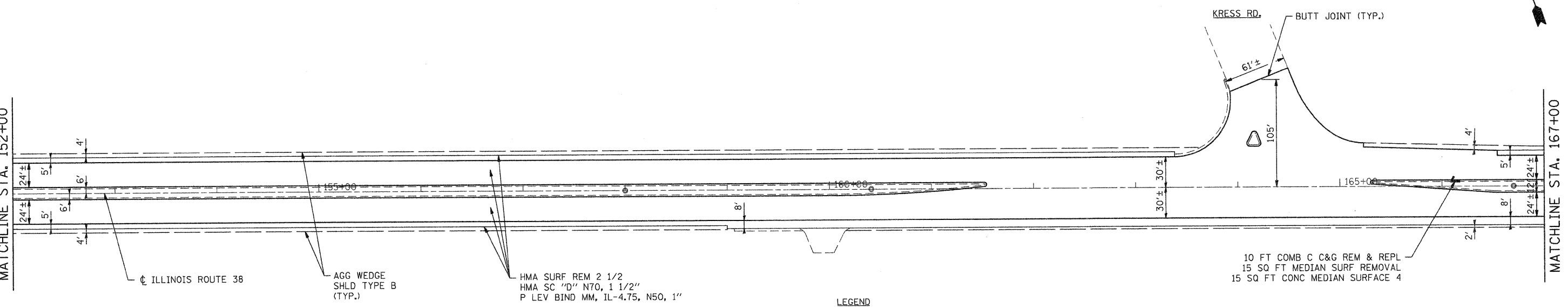
| F.A.P. RTE.         | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
|---------------------|---------|--------|---------------------------|-----------|
| 347                 | LY-RS-3 | DUPAGE | 35                        | 9         |
| FED. ROAD DIST. NO. |         |        | ILLINOIS FED. AID PROJECT |           |
|                     |         |        | CONTRACT NO. 60K82        |           |



PROJECT BEGINS  
STA. 148+78

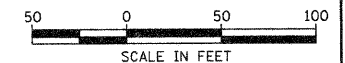


MATCHLINE STA. 152+00



MATCHLINE STA. 167+00

- LEGEND**
- EXISTING INLET
  - EXISTING CATCH BASIN
  - ⊙ EXISTING MANHOLE
  - E ELECTRIC
  - W WATER
  - CE COM ED (ADJUSTMENTS PERFORMED BY OTHERS)
  - T TELEPHONE (ADJUSTMENTS PERFORMED BY OTHERS)
  - G GAS
  - ⊕ FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
  - ⊔ DRAINAGE STRUCTURES TO BE CLEANED
  - ⊕ FRAMES AND LIDS TO BE ADJUSTED
  - ⊔ DRAINAGE STRUCTURES TO BE RECONSTRUCTED
  - ⊕ TREE REMOVAL



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 3/4/2011 10:56:16 PM  
 FILE NAME: 8600936.02-plan-02.dgn  
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 PEN TABLE: standard-trans.tbl

**SEC Group, Inc.**  
 An IRG Group Company  
 420 N. First Street  
 Moline, IL 61704-2138  
 (815) 395-1779 • (815) 395-1791  
 www.secgroup.com

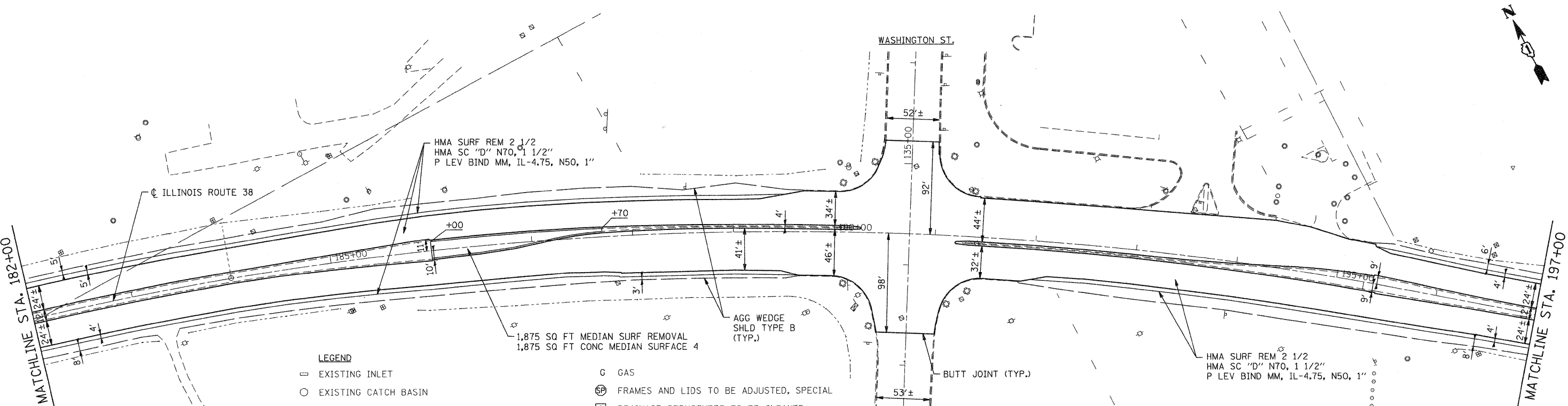
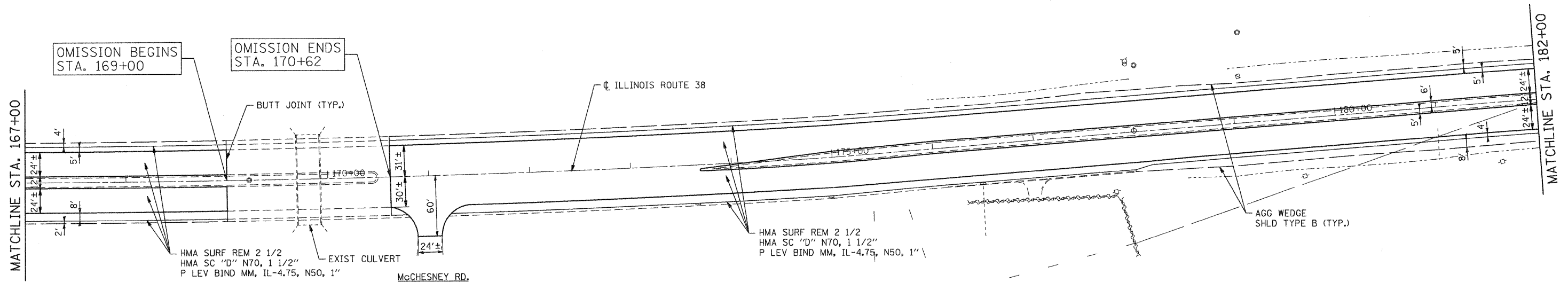
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| USER NAME = SPerno1   | DESIGNED - J. ROITBURD | REVISED - |
|                       | DRAWN - R. BEST        | REVISED - |
| PLOT SCALE = 1" = 50' | CHECKED - T. HAMILTON  | REVISED - |
| PLOT DATE = 3/4/2011  | DATE - 3/4/11          | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)**

SCALE: 1" = 50'    SHEET NO. 1 OF 6 SHEETS    STA. BEGIN TO STA. 167+00

| F.A.P. RTE.         | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
|---------------------|---------|--------|---------------------------|-----------|
| 347                 | LY-RS-3 | DUPAGE | 35                        | 10        |
| FED. ROAD DIST. NO. |         |        | ILLINOIS FED. AID PROJECT |           |
|                     |         |        | CONTRACT NO. 60K82        |           |



- LEGEND**
- EXISTING INLET
  - EXISTING CATCH BASIN
  - ⊙ EXISTING MANHOLE
  - E ELECTRIC
  - W WATER
  - CE COM ED (ADJUSTMENTS PERFORMED BY OTHERS)
  - T TELEPHONE (ADJUSTMENTS PERFORMED BY OTHERS)
  - G GAS
  - ⊕ FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
  - ⊠ DRAINAGE STRUCTURES TO BE CLEANED
  - ADJ FRAMES AND LIDS TO BE ADJUSTED
  - REC DRAINAGE STRUCTURES TO BE RECONSTRUCTED
  - ⊗ UNIT TREE REMOVAL



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 DATE PLOTTED: 3/4/2011 10:52:21 AM  
 FILE NAME: 86100196.02-plan-03.dgn  
 PLOT DRIVER: pdf.plt  
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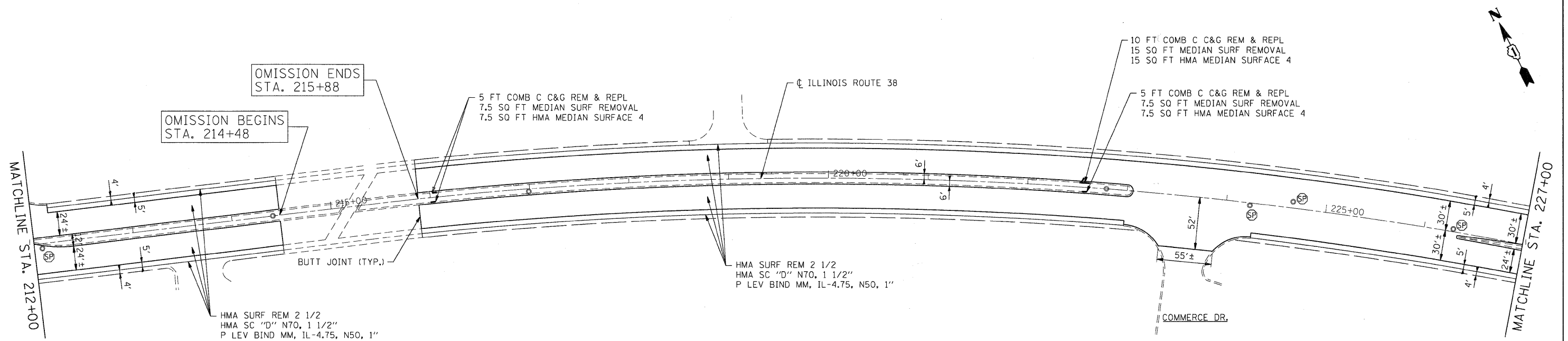
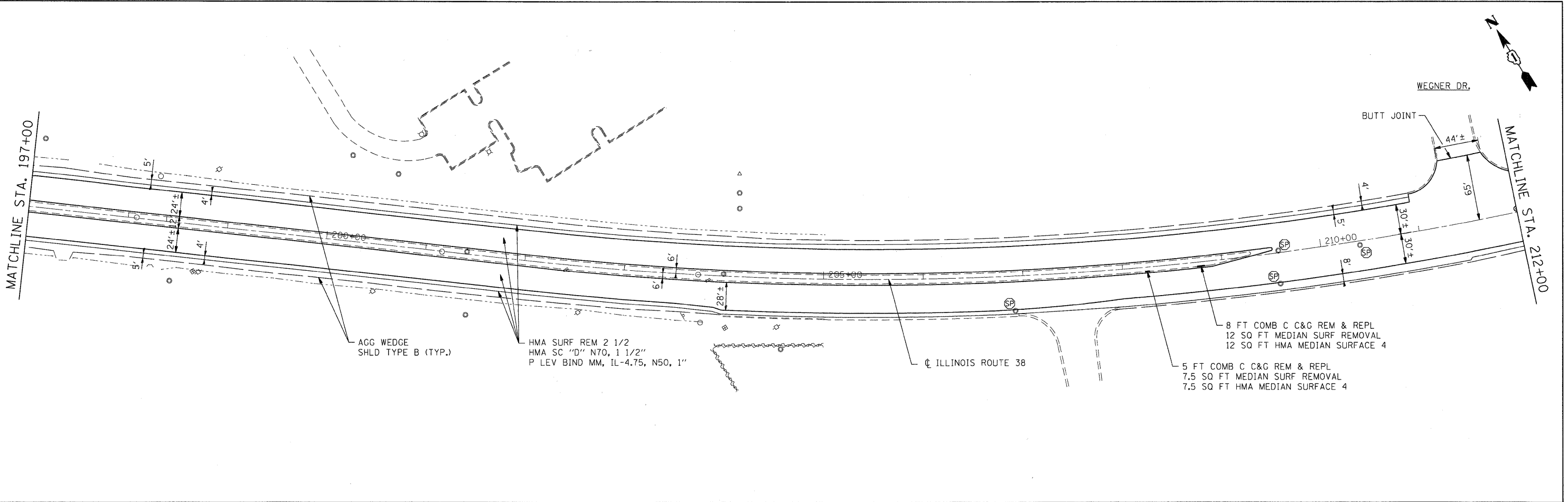


|                       |                        |           |
|-----------------------|------------------------|-----------|
| USER NAME = spernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = 1" = 50' | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 3/4/2011  | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 3/4/11          | REVISED - |

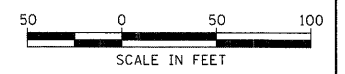
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|  |                         |                            |  |
|--|-------------------------|----------------------------|--|
| <b>ROADWAY PLAN</b>  |                         |                            |  |
| <b>ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)</b> |                         |                            |  |
| SCALE: 1" = 50'  | SHEET NO. 2 OF 6 SHEETS | STA. 167+00 TO STA. 197+00 |  |

|                     |                 |                           |                 |              |
|---------------------|-----------------|---------------------------|-----------------|--------------|
| F.A.P. RTE. 347     | SECTION LY-RS-3 | COUNTY DUPAGE             | TOTAL SHEETS 35 | SHEET NO. 11 |
| FED. ROAD DIST. NO. |                 | ILLINOIS FED. AID PROJECT |                 |              |
| CONTRACT NO. 60K82  |                 |                           |                 |              |



- LEGEND**
- ◻ EXISTING INLET
  - EXISTING CATCH BASIN
  - ⊙ EXISTING MANHOLE
  - E ELECTRIC
  - W WATER
  - CE COM ED (ADJUSTMENTS PERFORMED BY OTHERS)
  - T TELEPHONE (ADJUSTMENTS PERFORMED BY OTHERS)
  - G GAS
  - Ⓢ FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
  - ⓐ DRAINAGE STRUCTURES TO BE CLEANED
  - Ⓜ FRAMES AND LIDS TO BE ADJUSTED
  - Ⓡ DRAINAGE STRUCTURES TO BE RECONSTRUCTED
  - Ⓢ UNIT TREE REMOVAL



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 2/14/2011 11:42:07 AM  
 FILE NAME: 86000196.02-plan-04.dgn  
 PLOT DRIVER: pdfplot  
 PEN TABLE: standard-trans.tbl

**SEC Group, Inc.**  
 An HR Green Company  
 601 N. First Street  
 Moline, IL 61201-2138  
 1.815.385.1778 1.815.385.1781  
 www.secgroupinc.com  
 Moline, IL • Moline, IL • New Lenox, IL • Chicago, IL

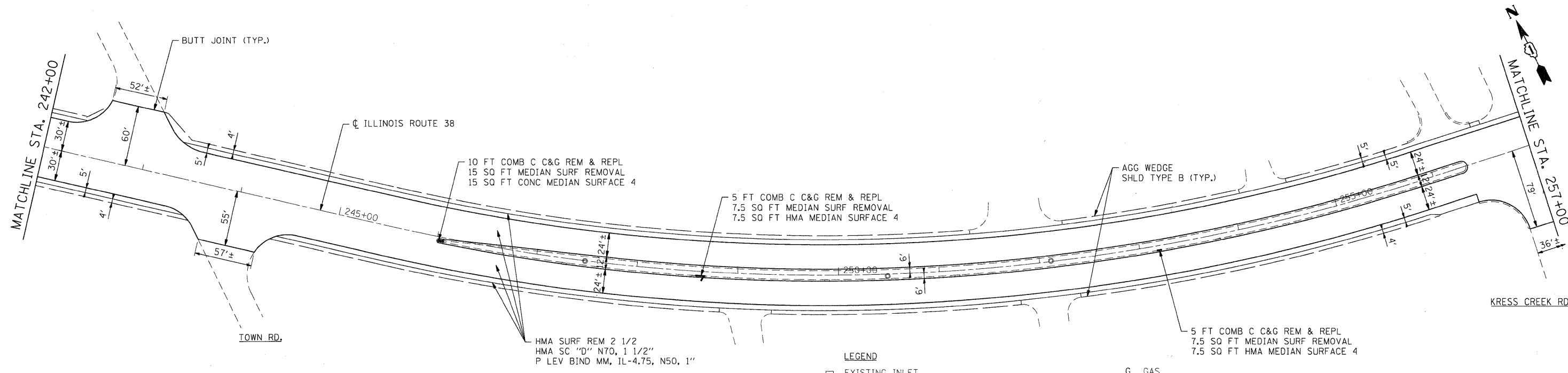
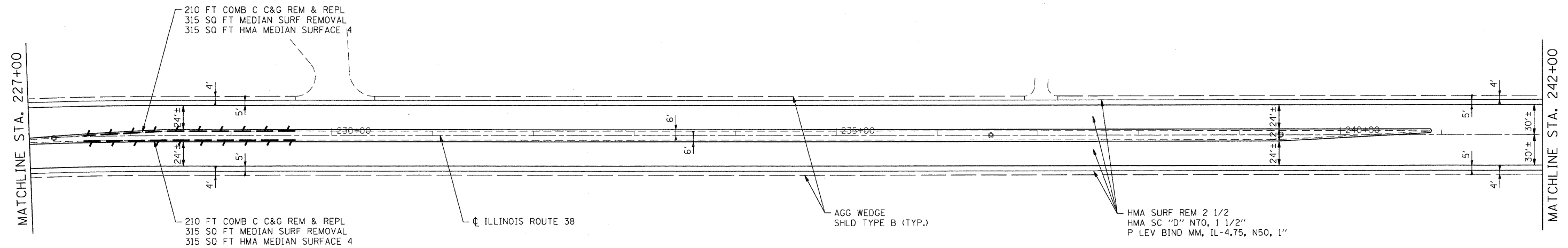
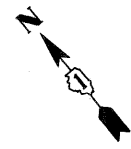
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| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = 1" = 50' | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/14/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/14/11         | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

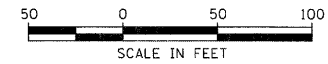
**ROADWAY PLAN  
 ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)**

SCALE: 1" = 50' SHEET NO. 3 OF 6 SHEETS STA. 197+00 TO STA. 227+00

|                    |                 |               |                           |              |
|--------------------|-----------------|---------------|---------------------------|--------------|
| F.A.P. RTE. 347    | SECTION LY-RS-3 | COUNTY DUPAGE | TOTAL SHEETS 35           | SHEET NO. 12 |
| CONTRACT NO. 60K82 |                 |               | ILLINOIS FED. AID PROJECT |              |



- LEGEND**
- ◻ EXISTING INLET
  - EXISTING CATCH BASIN
  - ⊙ EXISTING MANHOLE
  - E ELECTRIC
  - W WATER
  - CE COM ED (ADJUSTMENTS PERFORMED BY OTHERS)
  - T TELEPHONE (ADJUSTMENTS PERFORMED BY OTHERS)
  - G GAS
  - ⊕ FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
  - ⊡ DRAINAGE STRUCTURES TO BE CLEANED
  - ⊞ FRAMES AND LIDS TO BE ADJUSTED
  - ⊞ DRAINAGE STRUCTURES TO BE RECONSTRUCTED
  - ⊞ UNIT TREE REMOVAL



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 2/14/2011 11:42:13 AM  
 FILE NAME: 86100196.02-plan-05.dgn  
 PLOT DRIVER: pdt101  
 PEN TABLE: standard.ctb

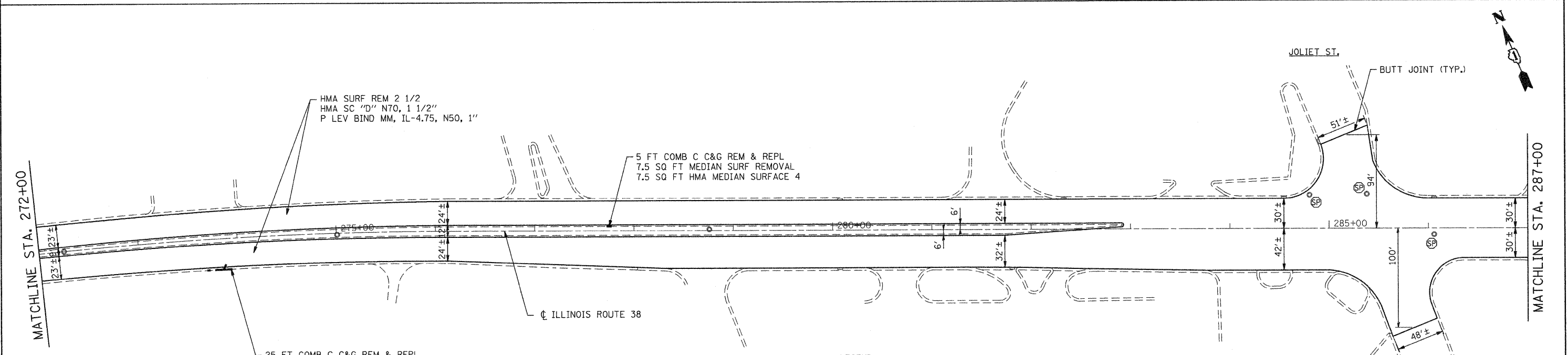
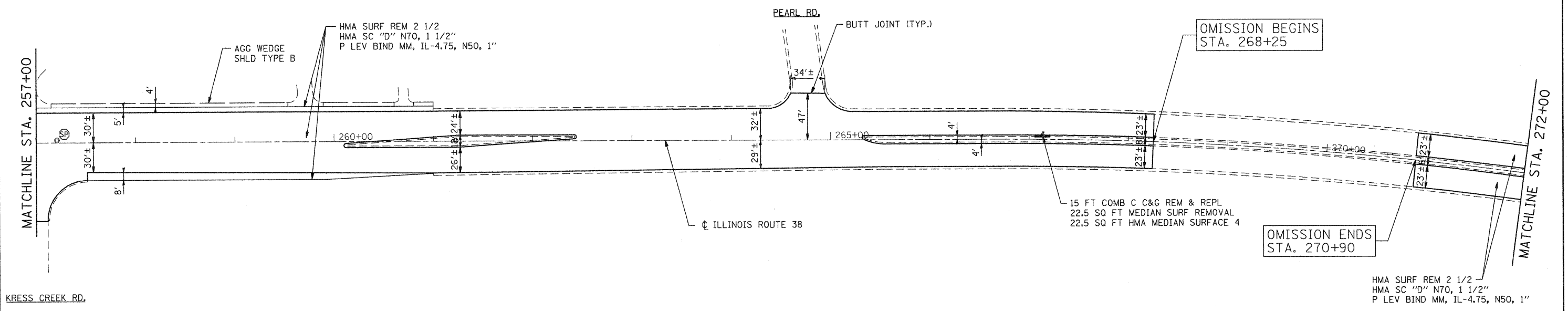
**SEC Group, Inc.**  
 An HNTB Group Company  
 401 N. First Street  
 Naperville, IL 60563-2138  
 P: 630.385.1770 F: 630.386.1781  
 www.secgroup.com

|                       |                        |           |
|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = 1" = 50' | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/14/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/14/11         | REVISED - |

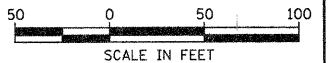
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)**  
 SCALE: 1" = 50' SHEET NO. 4 OF 6 SHEETS STA. 227+00 TO STA. 251+00

| F.A.P. RTE.         | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
|---------------------|---------|--------|---------------------------|-----------|
| 347                 | LY-RS-3 | DUPAGE | 35                        | 13        |
| FED. ROAD DIST. NO. |         |        | ILLINOIS FED. AID PROJECT |           |
|                     |         |        | CONTRACT NO. 60K82        |           |



- LEGEND**
- EXISTING INLET
  - EXISTING CATCH BASIN
  - ⊙ EXISTING MANHOLE
  - E ELECTRIC
  - W WATER
  - CE COM ED (ADJUSTMENTS PERFORMED BY OTHERS)
  - T TELEPHONE (ADJUSTMENTS PERFORMED BY OTHERS)
  - G GAS
  - ⊕ FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
  - ⊔ DRAINAGE STRUCTURES TO BE CLEANED
  - ⊕ FRAMES AND LIDS TO BE ADJUSTED
  - ⊔ DRAINAGE STRUCTURES TO BE RECONSTRUCTED
  - ⊕ TREE REMOVAL



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 3/4/2011 10:53:36 PM  
 FILE NAME: 86100196-02-plan-06.dgn  
 PLOT DRIVER: pdt/dlf  
 PEN TABLE: standard-trans.tbl

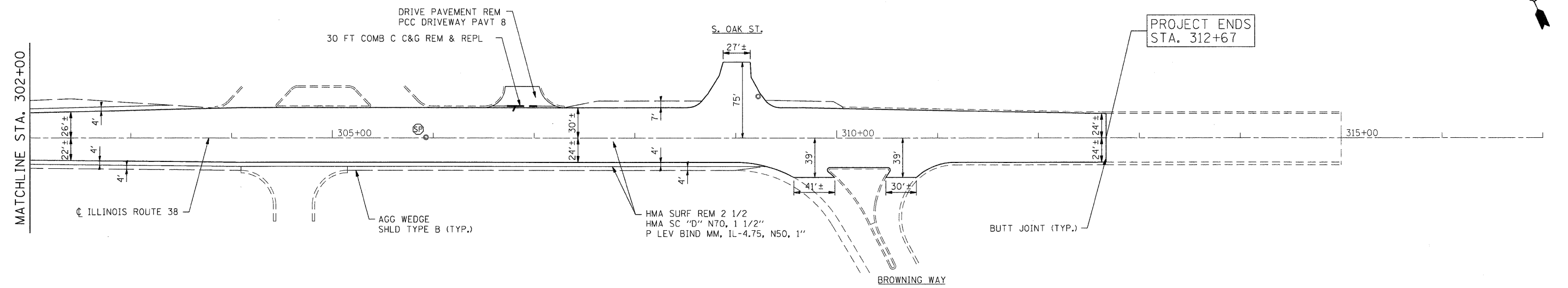
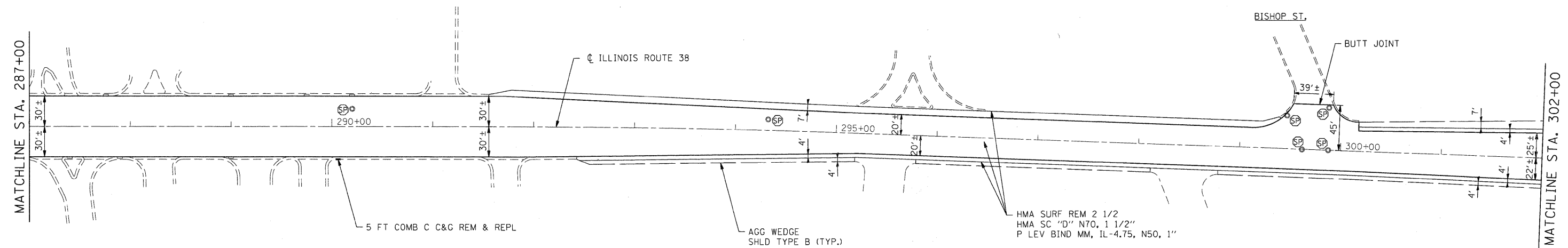
**SEC Group, Inc.**  
 An HR Group Company  
 400 N. First Street  
 Naperville, IL 60563-2138  
 P: 630.355.1778 F: 630.355.1781  
 www.secgroup.com

|                       |                        |           |
|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = 1" = 50' | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 3/4/2011  | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 3/4/11          | REVISED - |

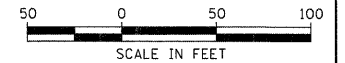
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
 ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)**  
 SCALE: 1" = 50' SHEET NO. 5 OF 6 SHEETS STA. 257+00 TO STA. 287+00

| F.A.P. RTE.                                     | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|--------|--------------|-----------|
| 347   | LY-RS-3 | DUPAGE | 35           | 14        |
| CONTRACT NO. 60K82                              |         |        |              |           |
| FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT |         |        |              |           |

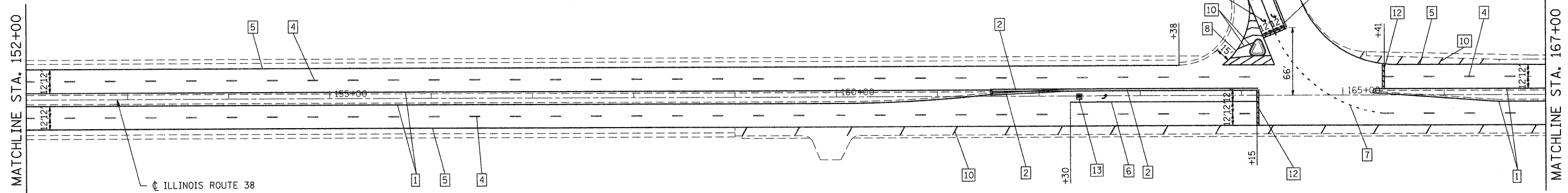
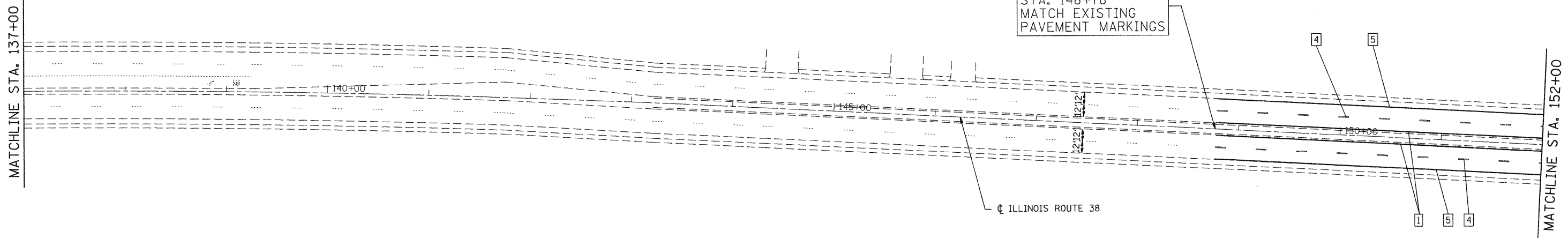


- LEGEND**
- ◻ EXISTING INLET
  - EXISTING CATCH BASIN
  - ⊙ EXISTING MANHOLE
  - E ELECTRIC
  - W WATER
  - CE COM ED (ADJUSTMENTS PERFORMED BY OTHERS)
  - T TELEPHONE (ADJUSTMENTS PERFORMED BY OTHERS)
  - G GAS
  - SP FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
  - C DRAINAGE STRUCTURES TO BE CLEANED
  - ADJ FRAMES AND LIDS TO BE ADJUSTED
  - REC DRAINAGE STRUCTURES TO BE RECONSTRUCTED
  - UNIT TREE REMOVAL



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 2/14/2011 11:42:24 AM  
 FILE NAME: 86100196.02-plan-07.dgn  
 PLOT DRIVER: pofair  
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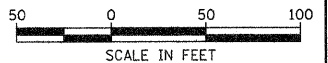
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|--|---|--|--|---|---|--|
|  | USER NAME = SPernel<br>PLOT SCALE = 1" = 50'<br>PLOT DATE = 2/14/2011 | DESIGNED - J. ROITBURD<br>DRAWN - R. BEST<br>CHECKED - T. HAMILTON<br>DATE - 2/14/11 | REVISED -<br>REVISED -<br>REVISED -<br>REVISED - | <b>STATE OF ILLINOIS</b><br><b>DEPARTMENT OF TRANSPORTATION</b> | <b>ROADWAY PLAN</b><br><b>ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)</b> | F.A.P. RTE. 347<br>SECTION LY-RS-3<br>COUNTY DUPAGE<br>TOTAL SHEETS 35<br>SHEET NO. 15<br>CONTRACT NO. 60K82 |
|  |   |  |  |   | SCALE: 1" = 50'    SHEET NO. 6 OF 6 SHEETS    STA. 287+00 TO STA. END             | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT  |



| LEGEND |  | NOTES: |                                 |
|--------|--|--------|---------------------------------|
| 1      | LINE 4" (YELLOW SOLID LINE)                    | 10     | LINE 12" (WHITE DIAGONAL LINE)  |
| 2      | LINE 4" (DOUBLE YELLOW SOLID LINES)            | 11     | LINE 12" (YELLOW DIAGONAL LINE) |
| 3      | LINE 4" (YELLOW - 10' DASH, 30' SKIP)          | 12     | LINE 24" (WHITE SOLID LINE)     |
| 4      | LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP) | 13     | LETTERS AND SYMBOLS (TYP.)      |
| 5      | LINE 4" (WHITE SOLID LINE)                     |        |                                 |
| 6      | LINE 6" (WHITE SOLID LINE)                     |        |                                 |
| 7      | LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)   |        |                                 |
| 8      | LINE 8" (WHITE SOLID LINE)                     |        |                                 |
| 9      | LINE 12" (WHITE SOLID LINE)                    |        |                                 |

1. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SPECIFIED.

2. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE PROJECT LIMITS. SEE DISTRICT DETAIL TC-11.



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 3/4/2011 10:54:48 PM  
 FILE NAME: 86100196-02-pm-02.dgn  
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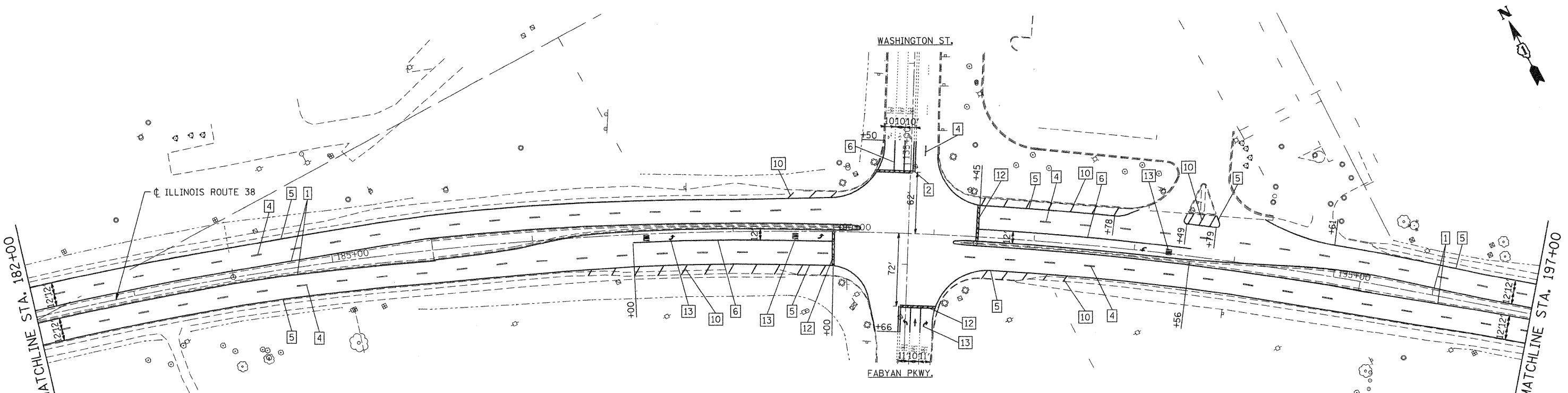
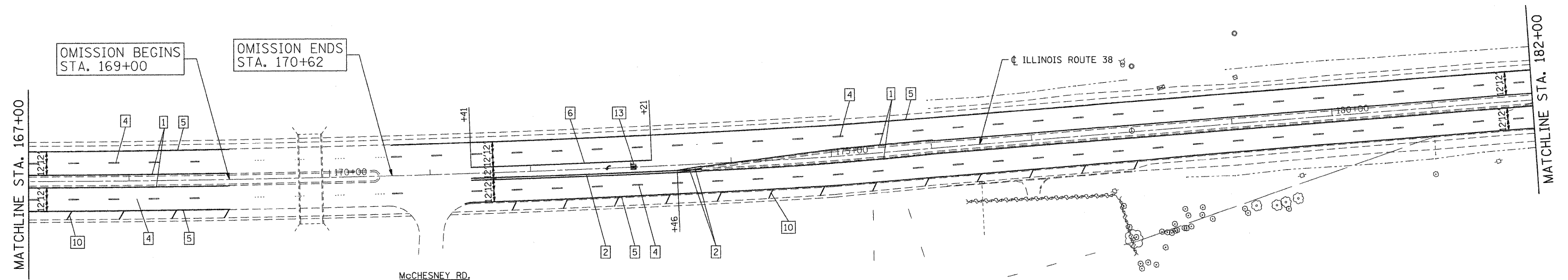
|                       |                        |           |
|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = 1" = 50' | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 3/4/2011  | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 3/4/11          | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

| PAVEMENT MARKING PLAN                                      |                         |
|--|-------------------------|
| <b>ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)</b> |                         |
| SCALE: 1" = 50'  | SHEET NO. 1 OF 6 SHEETS |
| STA. BEGIN   | TO STA. 167+00          |

|   |                 |               |                 |              |
|---|-----------------|---------------|-----------------|--------------|
| F.A.P. RTE. 347                               | SECTION LY-RS-3 | COUNTY DUPAGE | TOTAL SHEETS 35 | SHEET NO. 16 |
| CONTRACT NO. 60K82                            |                 |               |                 |              |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                 |               |                 |              |

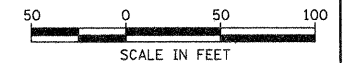




| LEGEND |  | NOTES: |                                 |
|--------|--|--------|---------------------------------|
| 1      | LINE 4" (YELLOW SOLID LINE)                    | 10     | LINE 12" (WHITE DIAGONAL LINE)  |
| 2      | LINE 4" (DOUBLE YELLOW SOLID LINES)            | 11     | LINE 12" (YELLOW DIAGONAL LINE) |
| 3      | LINE 4" (YELLOW - 10' DASH, 30' SKIP)          | 12     | LINE 24" (WHITE SOLID LINE)     |
| 4      | LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP) | 13     | LETTERS AND SYMBOLS (TYP.)      |
| 5      | LINE 4" (WHITE SOLID LINE)                     |        |                                 |
| 6      | LINE 6" (WHITE SOLID LINE)                     |        |                                 |
| 7      | LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)   |        |                                 |
| 8      | LINE 8" (WHITE SOLID LINE)                     |        |                                 |
| 9      | LINE 12" (WHITE SOLID LINE)                    |        |                                 |

1. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SPECIFIED.

2. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE PROJECT LIMITS. SEE DISTRICT DETAIL TC-11.



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 3/4/2011 10:55:4 PM  
 FILE NAME: 86100196.02-pm-03.dgn  
 PLOT DRIVER: acadplot  
 PEN TABLE: standard-trans.tbl



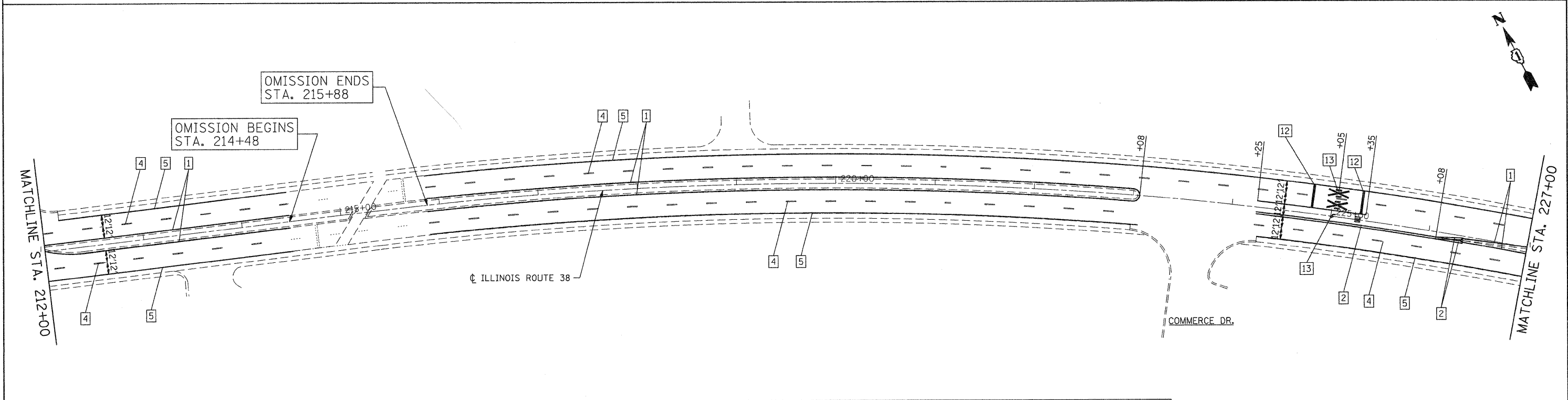
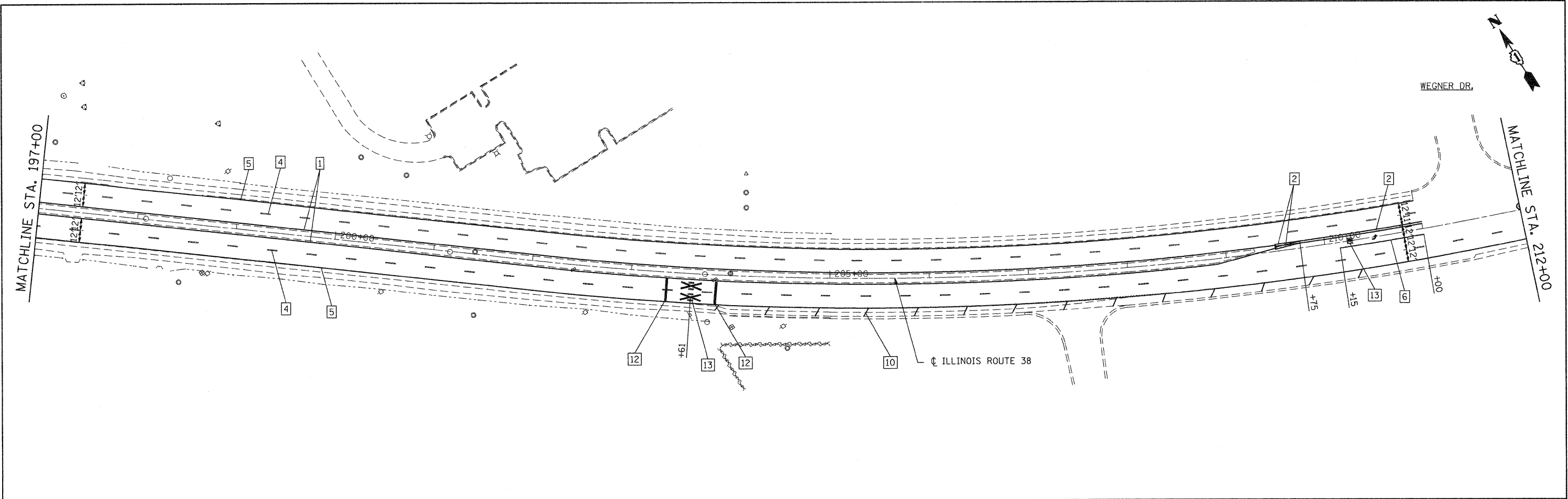
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|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = 1" = 50' | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 3/4/2011  | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 3/4/11          | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN  
 ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)**

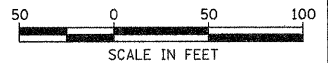
SCALE: 1" = 50'    SHEET NO. 2 OF 6 SHEETS    STA. 167+00 TO STA. 197+00

| F.A.P. RTE.                                   | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|---------|--------|--------------------|-----------|
| 347   | LY-RS-3 | DUPAGE | 35                 | 17        |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |         |        | CONTRACT NO. 60K82 |           |



| LEGEND |  | NOTES: |                                 |
|--------|--|--------|---------------------------------|
| 1      | LINE 4" (YELLOW SOLID LINE)                    | 10     | LINE 12" (WHITE DIAGONAL LINE)  |
| 2      | LINE 4" (DOUBLE YELLOW SOLID LINES)            | 11     | LINE 12" (YELLOW DIAGONAL LINE) |
| 3      | LINE 4" (YELLOW - 10' DASH, 30' SKIP)          | 12     | LINE 24" (WHITE SOLID LINE)     |
| 4      | LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP) | 13     | LETTERS AND SYMBOLS (TYP.)      |
| 5      | LINE 4" (WHITE SOLID LINE)                     |        |                                 |
| 6      | LINE 6" (WHITE SOLID LINE)                     |        |                                 |
| 7      | LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)   |        |                                 |
| 8      | LINE 8" (WHITE SOLID LINE)                     |        |                                 |
| 9      | LINE 12" (WHITE SOLID LINE)                    |        |                                 |

1. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SPECIFIED.  
 2. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE PROJECT LIMITS. SEE DISTRICT DETAIL TC-11.



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 3/4/2011 10:55:59 PM  
 FILE NAME: 86100196.02-pm-04.dgn  
 PLOT DRIVER: pdt.plt  
 PEN TABLE: standard-trans.tbl

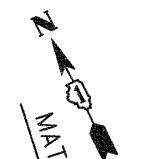
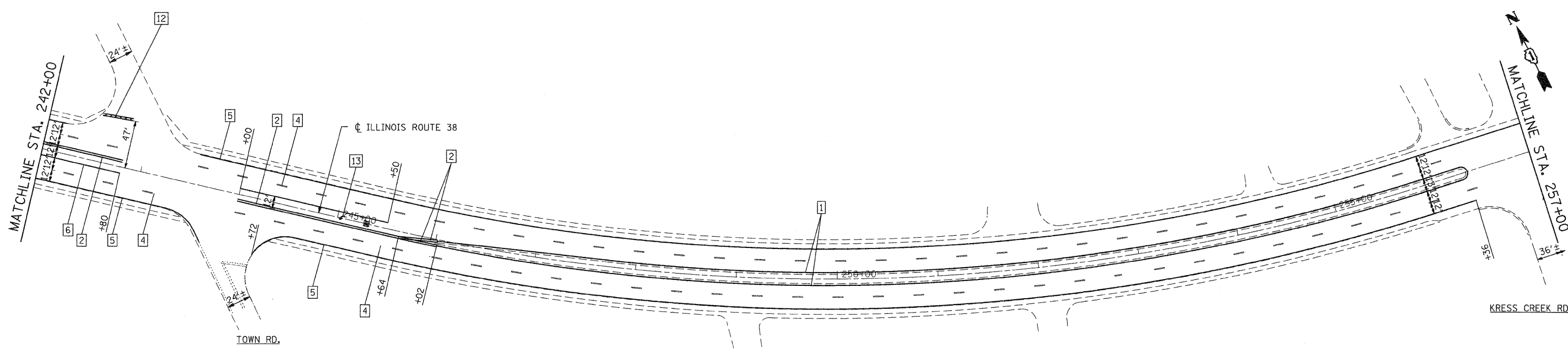
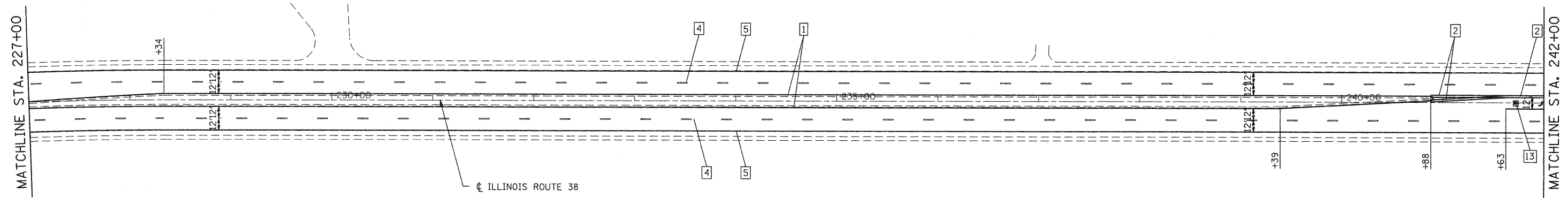
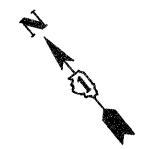
**SEC Group, Inc.**  
 An IRI Group Company  
 450 N. First Street  
 Melrose, IL 60050-2128  
 630.356.1779 • 630.356.1791  
 www.secgroupinc.com  
 Chicago, St. Louis, St. Paul, & Chicago, IL

|                       |                        |           |
|-----------------------|------------------------|-----------|
| USER NAME = SPernel   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = 1" = 50' | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 3/4/2011  | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 3/4/11          | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

| PAVEMENT MARKING PLAN                               |                         |                            |  |
|---|-------------------------|----------------------------|--|
| ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59) |                         |                            |  |
| SCALE: 1" = 50'                                     | SHEET NO. 3 OF 6 SHEETS | STA. 197+00 TO STA. 227+00 |  |

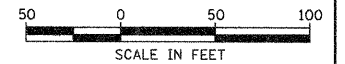
| F.A.P. RTE.                                   | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|---------|--------|--------------------|-----------|
| 347   | LY-RS-3 | DUPAGE | 35                 | 18        |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |         |        | CONTRACT NO. 60K82 |           |



| LEGEND |  |
|--------|--|
| 1      | LINE 4" (YELLOW SOLID LINE)                    |
| 2      | LINE 4" (DOUBLE YELLOW SOLID LINES)            |
| 3      | LINE 4" (YELLOW - 10' DASH, 30' SKIP)          |
| 4      | LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP) |
| 5      | LINE 4" (WHITE SOLID LINE)                     |
| 6      | LINE 6" (WHITE SOLID LINE)                     |
| 7      | LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)   |
| 8      | LINE 8" (WHITE SOLID LINE)                     |
| 9      | LINE 12" (WHITE SOLID LINE)                    |
| 10     | LINE 12" (WHITE DIAGONAL LINE)                 |
| 11     | LINE 12" (YELLOW DIAGONAL LINE)                |
| 12     | LINE 24" (WHITE SOLID LINE)                    |
| 13     | LETTERS AND SYMBOLS (TYP.)                     |

**NOTES:**

- ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SPECIFIED.
- RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE PROJECT LIMITS. SEE DISTRICT DETAIL TC-11.



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: #PROJECT\_CONTACT  
 CLIENT: #CLIENT  
 FILE NAME: 86100196.02-pm-05.dgn  
 PLOT DRIVER: pefplot  
 PEN TABLE: standard-trans.tbl



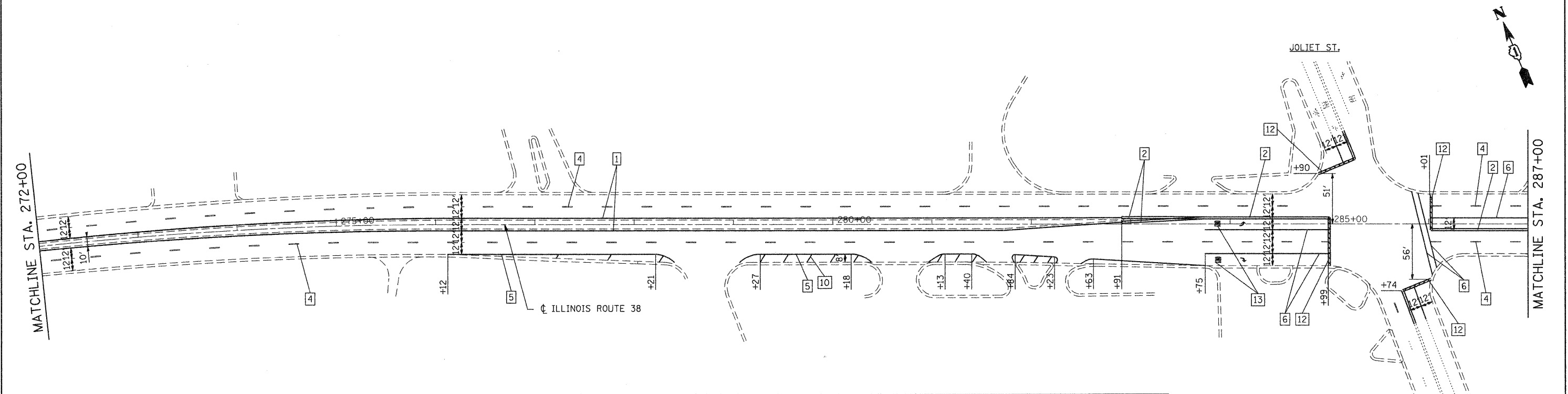
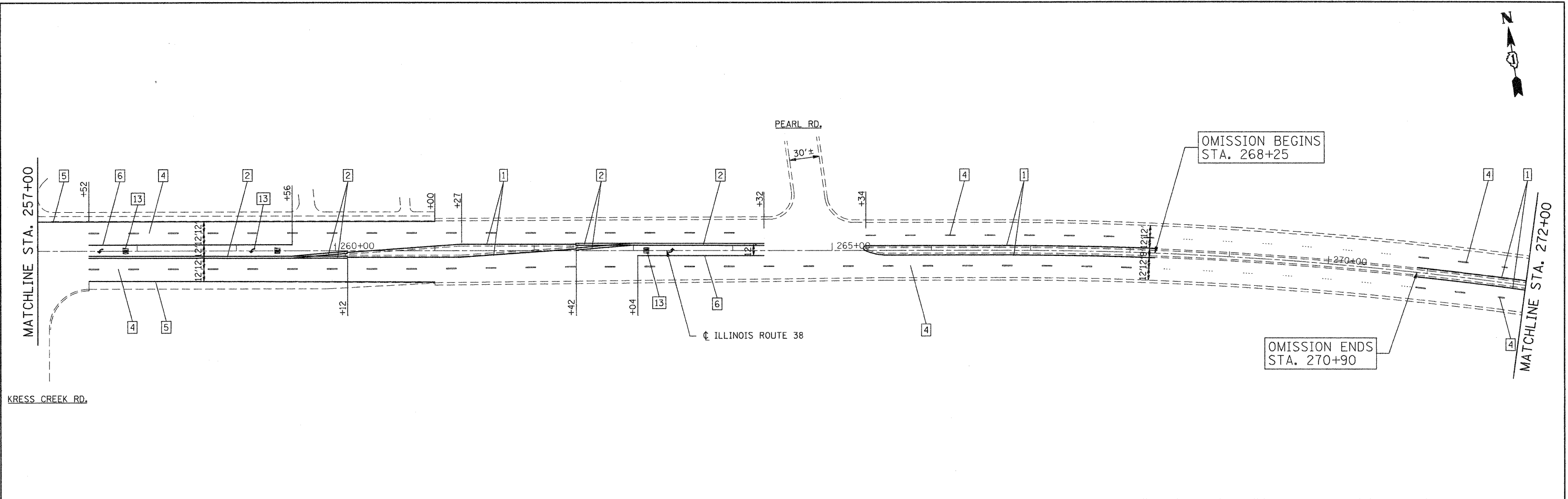
|                       |                        |           |
|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = 1" = 50' | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 3/4/2011  | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 3/4/11          | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN**  
**ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)**  
 SCALE: 1" = 50'    SHEET NO. 4 OF 6 SHEETS    STA. 227+00 TO STA. 257+00

|                              |                 |                  |                 |              |
|------------------------------|-----------------|------------------|-----------------|--------------|
| F.A.P. RTE. 347              | SECTION LY-RS-3 | COUNTY DUPAGE    | TOTAL SHEETS 35 | SHEET NO. 19 |
| FED. ROAD DIST. NO. ILLINOIS |                 | FED. AID PROJECT |                 |              |

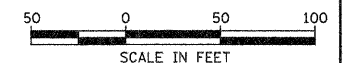
CONTRACT NO. 60K82



| LEGEND |  |
|--------|--|
| 1      | LINE 4" (YELLOW SOLID LINE)                    |
| 2      | LINE 4" (DOUBLE YELLOW SOLID LINES)            |
| 3      | LINE 4" (YELLOW - 10' DASH, 30' SKIP)          |
| 4      | LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP) |
| 5      | LINE 4" (WHITE SOLID LINE)                     |
| 6      | LINE 6" (WHITE SOLID LINE)                     |
| 7      | LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)   |
| 8      | LINE 8" (WHITE SOLID LINE)                     |
| 9      | LINE 12" (WHITE SOLID LINE)                    |
| 10     | LINE 12" (WHITE DIAGONAL LINE)                 |
| 11     | LINE 12" (YELLOW DIAGONAL LINE)                |
| 12     | LINE 24" (WHITE SOLID LINE)                    |
| 13     | LETTERS AND SYMBOLS (TYP.)                     |

**NOTES:**

- ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SPECIFIED.
- RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE PROJECT LIMITS. SEE DISTRICT DETAIL TC-11.



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 3/4/2011 10:06:10 PM  
 FILE NAME: 8600095602-pm-06.dgn  
 PLOT DRIVER: pd4.plt  
 PEN TABLE: standard-trans.tbl

**SEC Group, Inc.**  
 430 N. Foy Street  
 Moline, IL 61901-0108  
 815.365.1778 & 815.365.1781  
 www.secgroupinc.com

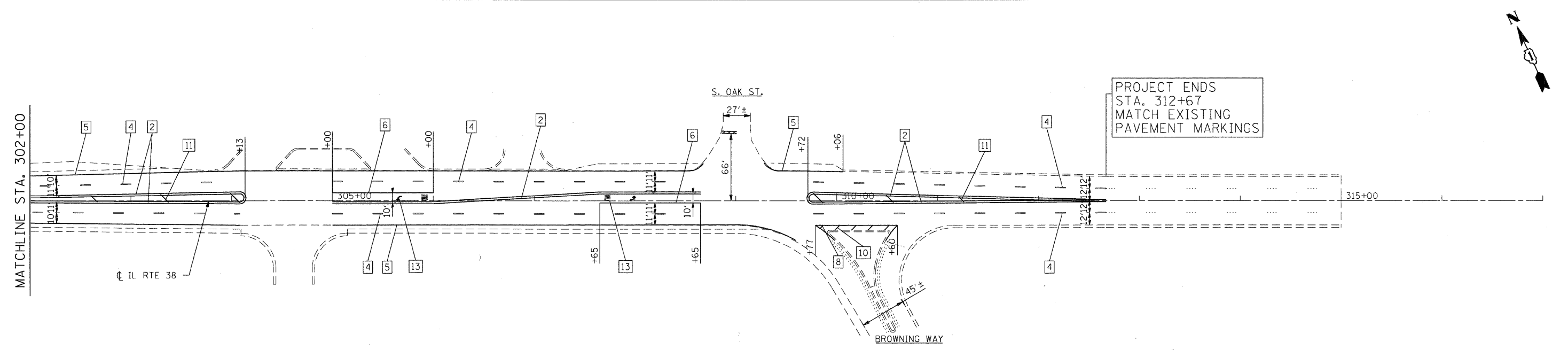
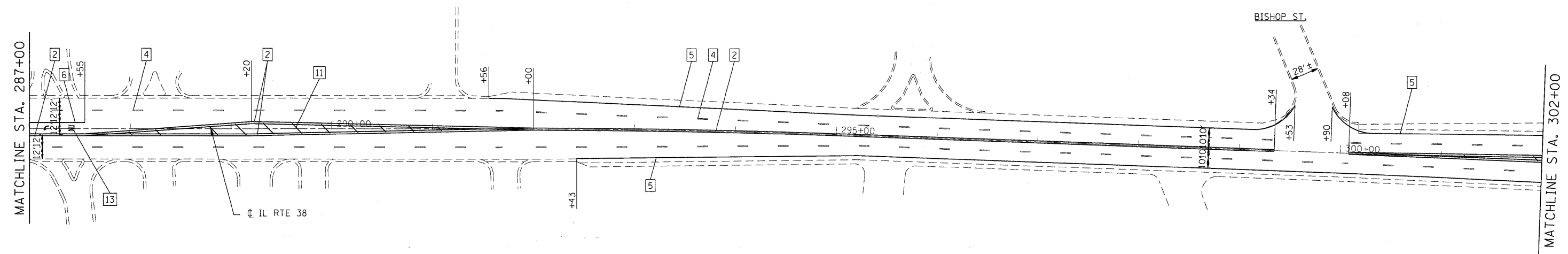
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|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = 1" = 50' | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 3/4/2011  | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 3/4/11          | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN  
 ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)**

SCALE: 1" = 50'    SHEET NO. 5 OF 6 SHEETS    STA. 257+00 TO STA. 287+00

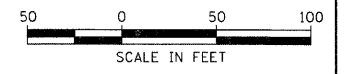
| F.A.P. RTE.                                   | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|--------|--------------|-----------|
| 347   | LY-RS-3 | DUPAGE | 35           | 20        |
| CONTRACT NO. 60K82                            |         |        |              |           |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |         |        |              |           |



| LEGEND |  | NOTES: |                                 |
|--------|--|--------|---------------------------------|
| 1      | LINE 4" (YELLOW SOLID LINE)                    | 10     | LINE 12" (WHITE DIAGONAL LINE)  |
| 2      | LINE 4" (DOUBLE YELLOW SOLID LINES)            | 11     | LINE 12" (YELLOW DIAGONAL LINE) |
| 3      | LINE 4" (YELLOW - 10' DASH, 30' SKIP)          | 12     | LINE 24" (WHITE SOLID LINE)     |
| 4      | LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP) | 13     | LETTERS AND SYMBOLS (TYP.)      |
| 5      | LINE 4" (WHITE SOLID LINE)                     |        |                                 |
| 6      | LINE 6" (WHITE SOLID LINE)                     |        |                                 |
| 7      | LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)   |        |                                 |
| 8      | LINE 8" (WHITE SOLID LINE)                     |        |                                 |
| 9      | LINE 12" (WHITE SOLID LINE)                    |        |                                 |

1. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SPECIFIED.

2. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE PROJECT LIMITS. SEE DISTRICT DETAIL TC-11.



COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: J. ROITBURD  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 2/14/2011 11:42:58 AM  
 FILE NAME: 86100136.02-pm-01.dgn  
 PLOT DRIVER: pdt404f  
 PEN TABLE: standard-tran.tbl

**SEC Group, Inc.**  
 An HR Green Company  
 430 W. Forest Street  
 Naperville, IL 60563-2138  
 630.256.1778 • 630.256.1791  
 www.secgroup.com

|                       |                        |           |
|-----------------------|------------------------|-----------|
| USER NAME = SPernal   | DESIGNED - J. ROITBURD | REVISED - |
| PLOT SCALE = 1" = 50' | DRAWN - R. BEST        | REVISED - |
| PLOT DATE = 2/14/2011 | CHECKED - T. HAMILTON  | REVISED - |
|                       | DATE - 2/14/11         | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

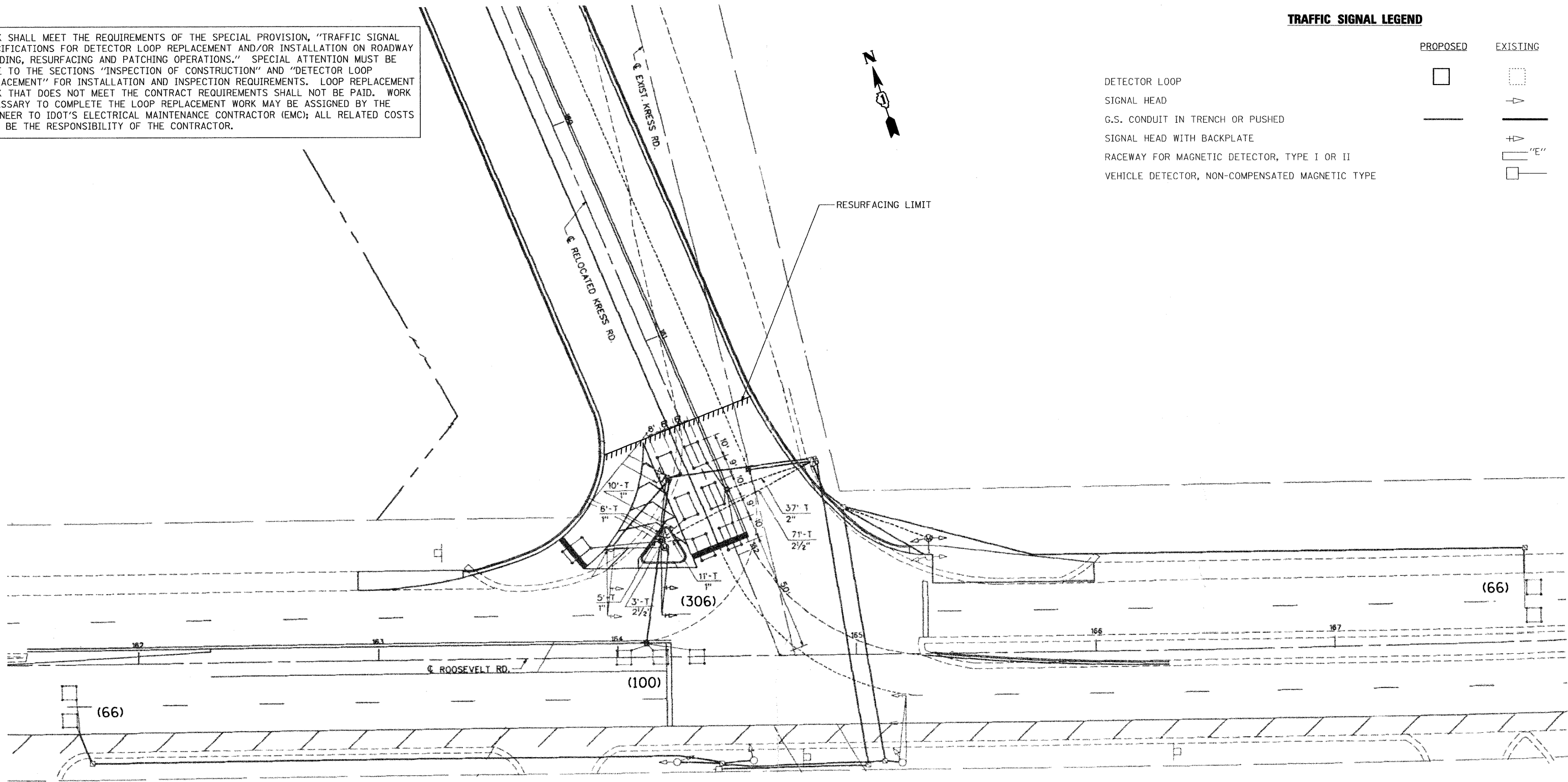
|  |                         |             |             |
|--|-------------------------|-------------|-------------|
| <b>PAVEMENT MARKING PLAN</b>                               |                         |             |             |
| <b>ILLINOIS ROUTE 38 (KAUTZ ROAD TO ILLINOIS ROUTE 59)</b> |                         |             |             |
| SCALE: 1" = 50'  | SHEET NO. 6 OF 6 SHEETS | STA. 287+00 | TO STA. END |

|                     |                 |                           |                 |              |
|---------------------|-----------------|---------------------------|-----------------|--------------|
| F.A.P. RTE. 347     | SECTION LY-RS-3 | COUNTY DUPAGE             | TOTAL SHEETS 35 | SHEET NO. 21 |
| FED. ROAD DIST. NO. |                 | ILLINOIS FED. AID PROJECT |                 |              |
| CONTRACT NO. 60K82  |                 |                           |                 |              |

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS." SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

**TRAFFIC SIGNAL LEGEND**

|   | PROPOSED | EXISTING |
|---|----------|----------|
| DETECTOR LOOP                                   |          |          |
| SIGNAL HEAD                                     |          |          |
| G.S. CONDUIT IN TRENCH OR PUSHED                |          |          |
| SIGNAL HEAD WITH BACKPLATE                      |          |          |
| RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II     |          |          |
| VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE |          |          |



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

| CODE     | QUANTITY | UNIT | ITEM                      |
|----------|----------|------|---------------------------|
| 86600600 | 538      | FOOT | DETECTOR LOOP REPLACEMENT |

COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: SPernal  
 CLIENT: ILLINOIS  
 DATE PLOTTED: 3/4/2011 10:06:24 PM  
 FILE NAME: 86600600-02-dloop-02.dgn  
 PLOT DRIVER: pdt.plt  
 PEN TABLE: standard-trans.tbl

**SEC Group, Inc.**  
 An IHS Green Company  
 400 N. First Street  
 Moline, IL 61704-1700  
 T 815.365.1778 F 815.365.1781  
 www.secgroup.com

|                       |               |           |
|-----------------------|---------------|-----------|
| USER NAME = SPernal   | DESIGNED -    | REVISED - |
| PLOT SCALE = 1" = 20' | DRAWN -       | REVISED - |
| PLOT DATE = 3/4/2011  | CHECKED -     | REVISED - |
|                       | DATE - 3/4/11 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP REPLACEMENT  
ILLINOIS ROUTE 38 @ KRESS RD.**

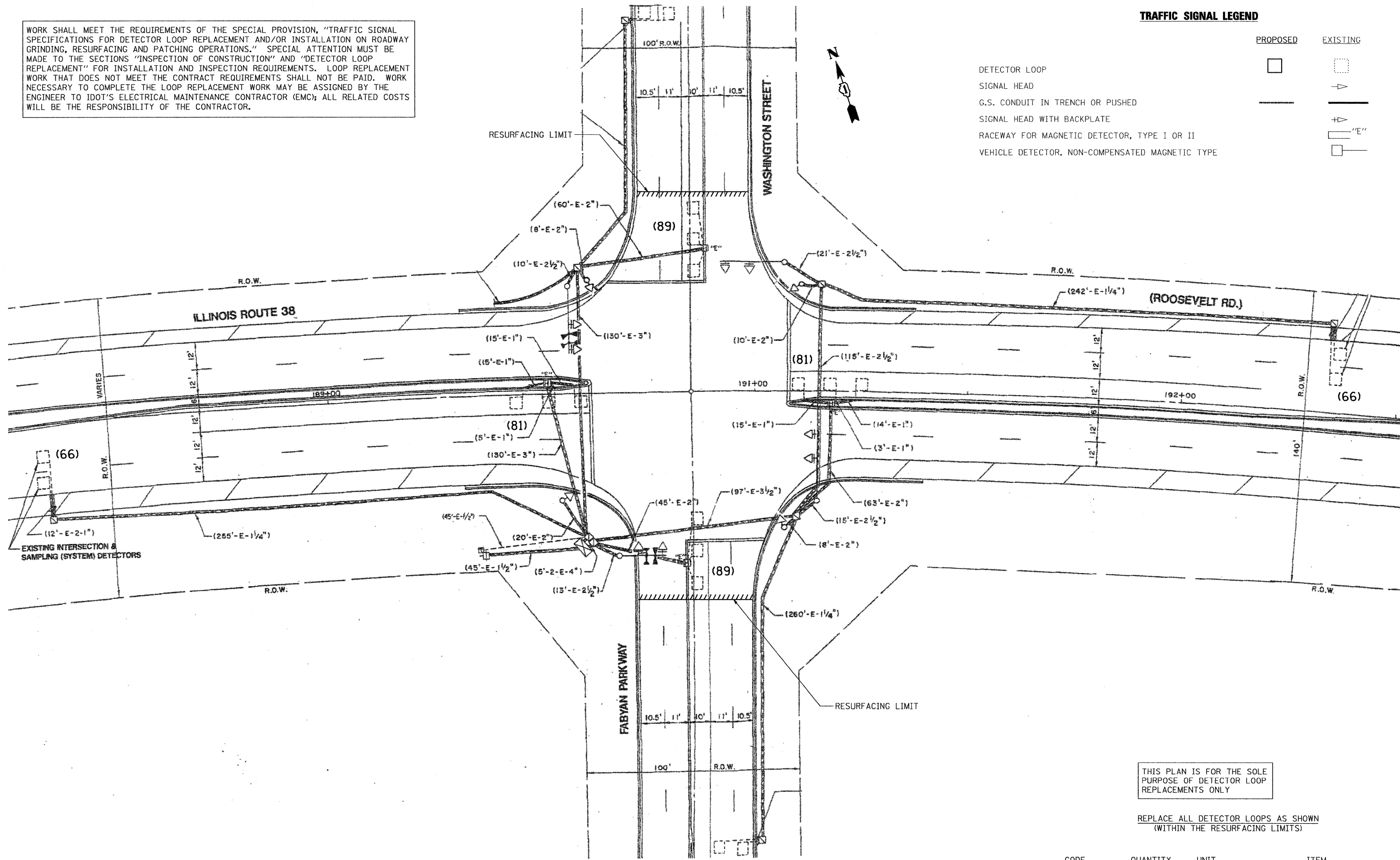
SCALE: 1" = 20'    SHEET NO. 1 OF 3 SHEETS    STA. NA    TO STA. NA

|                     |         |                           |              |           |
|---------------------|---------|---------------------------|--------------|-----------|
| F.A.P. RTE.         | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| 347                 | LY-RS-3 | DUPAGE                    | 35           | 22        |
| FED. ROAD DIST. NO. |         | ILLINOIS FED. AID PROJECT |              |           |
|                     |         | CONTRACT NO. 60K82        |              |           |

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS." SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

**TRAFFIC SIGNAL LEGEND**

|   | PROPOSED | EXISTING |
|---|----------|----------|
| DETECTOR LOOP                                   |          |          |
| SIGNAL HEAD                                     |          |          |
| G.S. CONDUIT IN TRENCH OR PUSHED                |          |          |
| SIGNAL HEAD WITH BACKPLATE                      |          |          |
| RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II     |          |          |
| VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE |          |          |



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

| CODE     | QUANTITY | UNIT | ITEM                      |
|----------|----------|------|---------------------------|
| 86600600 | 472      | FOOT | DETECTOR LOOP REPLACEMENT |

COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: [Name]  
 DATE PLOTTED: 3/4/2011 10:06:29 AM  
 FILE NAME: 86100196.02-decp-03.dgn  
 PLOT DRIVER: pdf.plt  
 PEN TABLE: standard-trans.tbl



|                       |               |           |
|-----------------------|---------------|-----------|
| USER NAME = SPernal   | DESIGNED -    | REVISED - |
| PLOT SCALE = 1" = 20' | DRAWN -       | REVISED - |
| PLOT DATE = 3/4/2011  | CHECKED -     | REVISED - |
|                       | DATE = 3/4/11 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP REPLACEMENT  
ILLINOIS ROUTE 38 @ FABYAN PKWY/WASHINGTON ST.**

SCALE: 1" = 20' SHEET NO. 2 OF 3 SHEETS STA. NA TO STA. NA

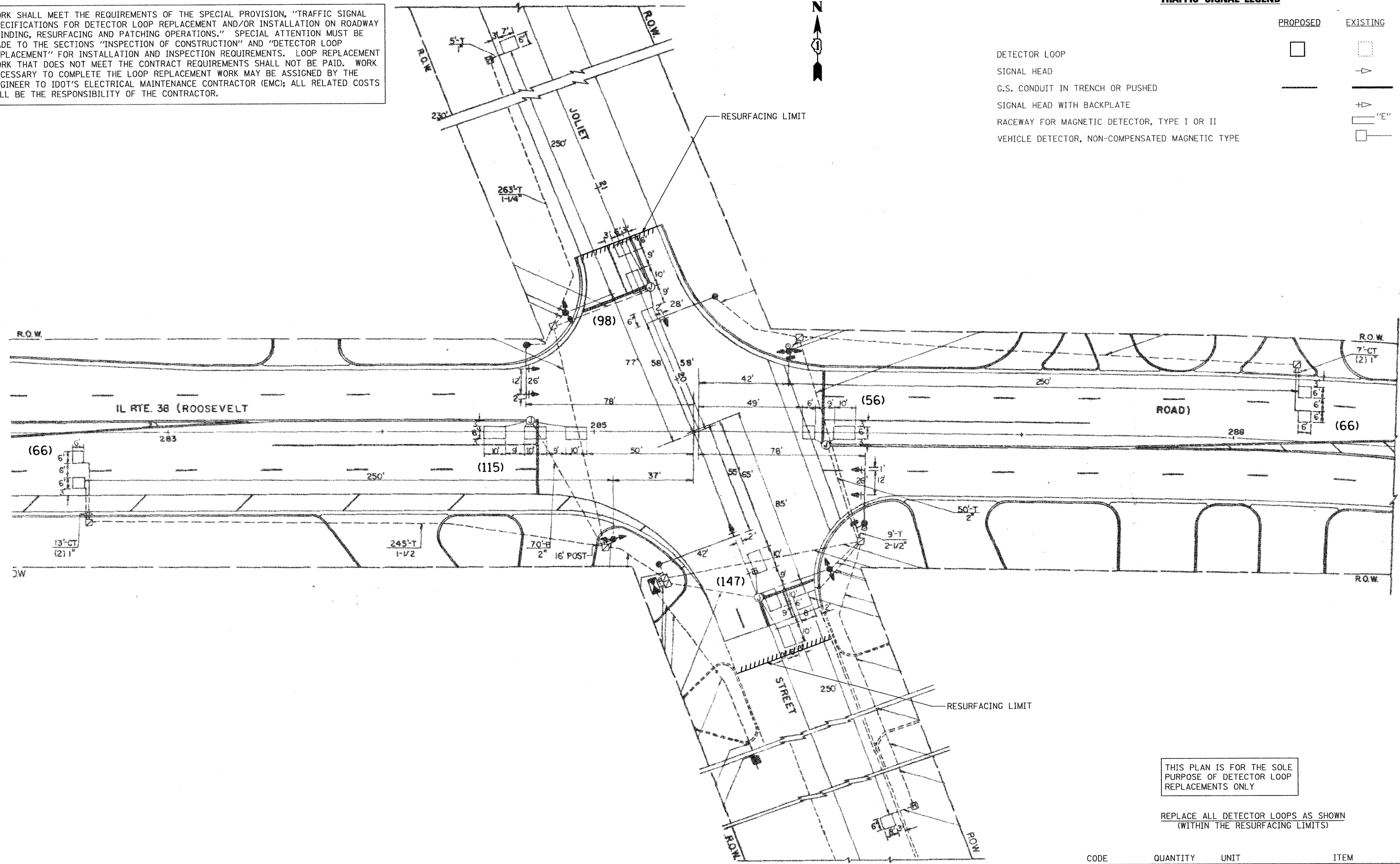
| F.A.P. RTE.        | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|---------|--------|---------------------------|-----------|
| 347                | LY-RS-3 | DUPAGE | 35                        | 23        |
| CONTRACT NO. 60K82 |         |        | ILLINOIS FED. AID PROJECT |           |



WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS." SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

**TRAFFIC SIGNAL LEGEND**

|   | PROPOSED | EXISTING |
|---|----------|----------|
| DETECTOR LOOP                                   |          |          |
| SIGNAL HEAD                                     |          |          |
| G.S. CONDUIT IN TRENCH OR PUSHED                |          |          |
| SIGNAL HEAD WITH BACKPLATE                      |          |          |
| RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II     |          |          |
| VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE |          |          |



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

| CODE     | QUANTITY | UNIT | ITEM                      |
|----------|----------|------|---------------------------|
| 86600600 | 548      | FOOT | DETECTOR LOOP REPLACEMENT |

COMPANY NAME: SEC Group, Inc.  
 PROJECT CONTACT: SPernal  
 CLIENT: IDOT  
 DATE PLOTTED: 3/4/2011 10:06:37 PM  
 FILE NAME: 86100196.02-dloop-04.dgn  
 PLOT DRIVER: pd7.plt  
 PEN TABLE: standard-trans.tbl

**SEC Group, Inc.**  
 An IES Group Company  
 420 N. First Street  
 Moline, IL 61201-2100  
 815.326.1778 • 815.326.1781  
 www.secgroup.com

|                       |               |           |
|-----------------------|---------------|-----------|
| USER NAME = SPernal   | DESIGNED -    | REVISED - |
| PLOT SCALE = 1" = 20' | DRAWN -       | REVISED - |
| PLOT DATE = 3/4/2011  | CHECKED -     | REVISED - |
|                       | DATE = 3/4/11 | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

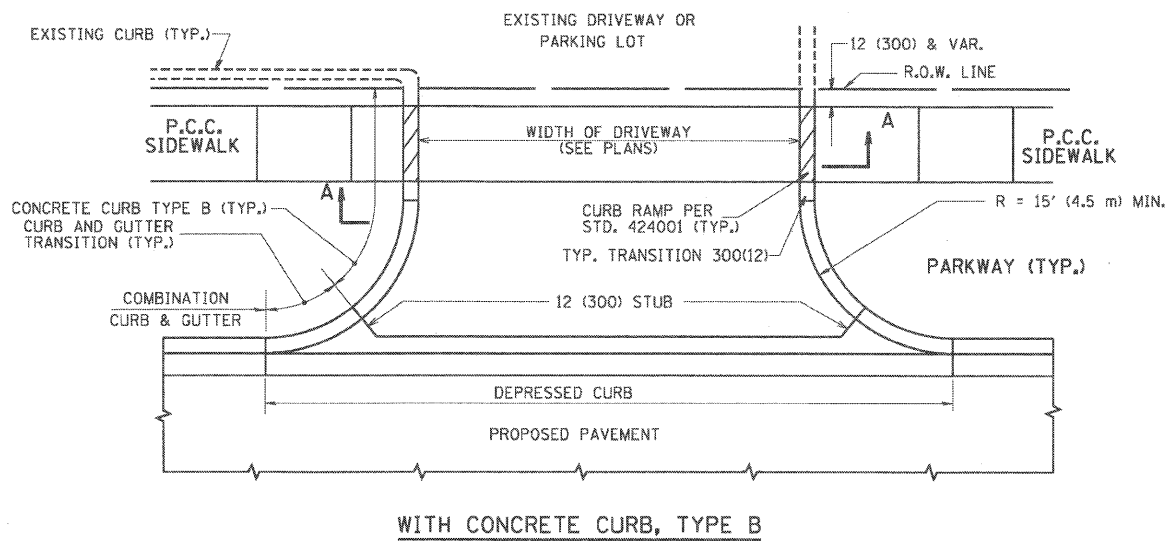
**DISTRICT 1 - DETECTOR LOOP REPLACEMENT  
 ILLINOIS ROUTE 38 @ JOLIET ST.**

SCALE: 1" = 20' SHEET NO. 3 OF 3 SHEETS STA. NA TO STA. NA

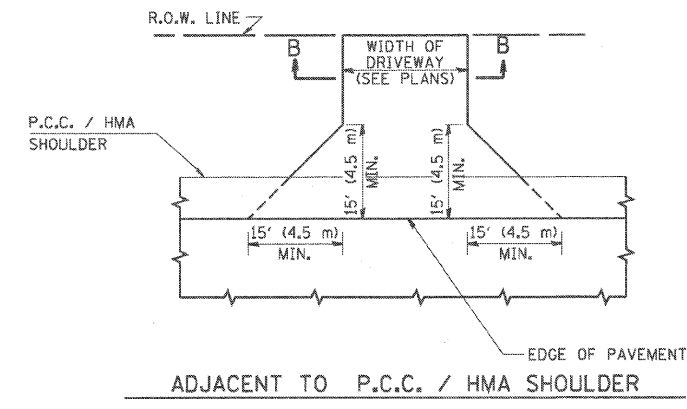
| F.A.P. RTE.         | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------------------|--------------|-----------|
| 347                 | LY-RS-3 | DUPAGE                    | 35           | 24        |
| FED. ROAD DIST. NO. |         | ILLINOIS FED. AID PROJECT |              |           |

CONTRACT NO. 60K82

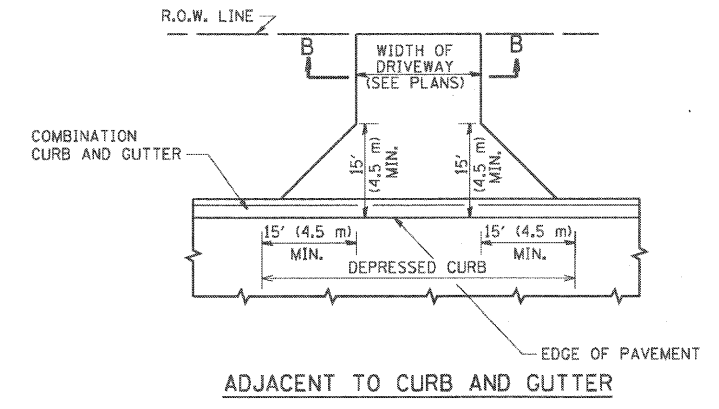




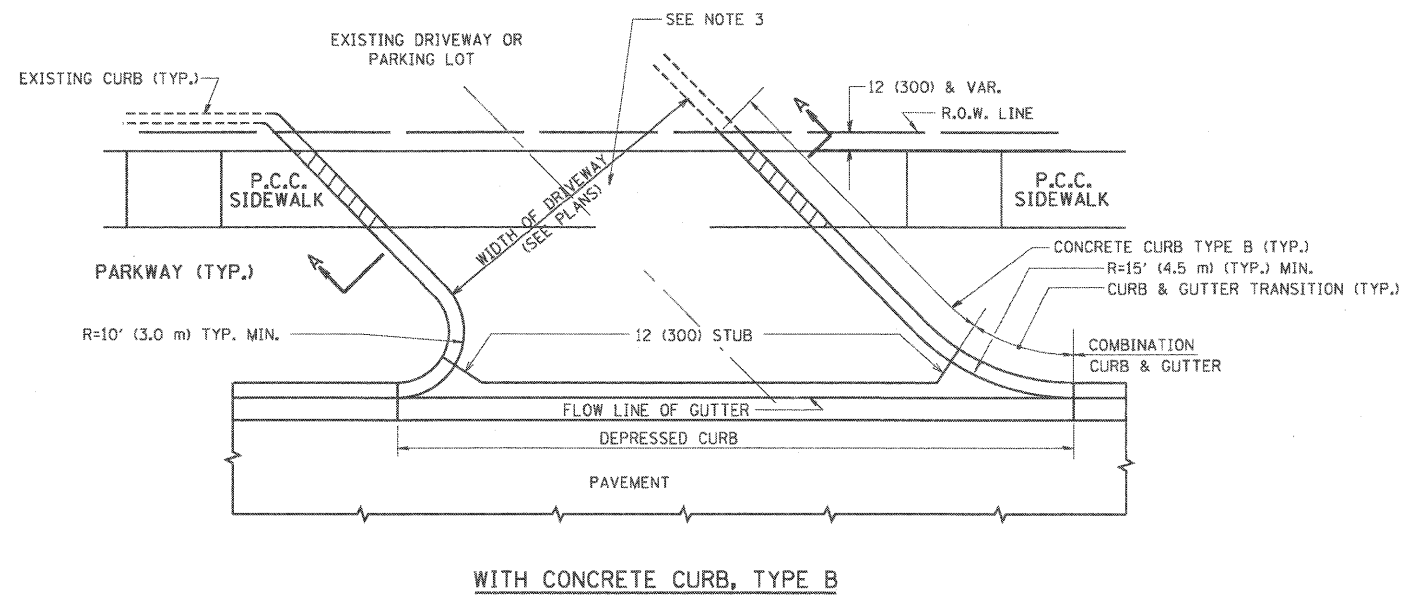
WITH CONCRETE CURB, TYPE B



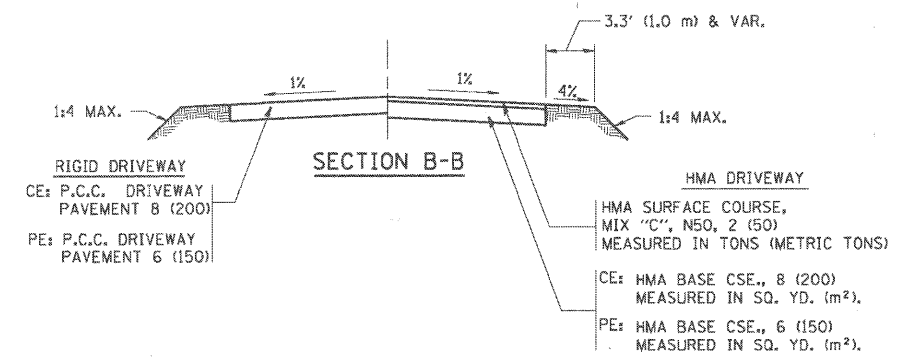
ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



WITH CONCRETE CURB, TYPE B



**RIGID DRIVEWAY**  
 CE: P.C.C. DRIVEWAY PAVEMENT 8 (200)  
 PE: P.C.C. DRIVEWAY PAVEMENT 6 (150)

**HMA DRIVEWAY**  
 HMA SURFACE COURSE, MIX "C", N50, 2 (50) MEASURED IN TONS (METRIC TONS)  
 CE: HMA BASE CSE., 8 (200) MEASURED IN SQ. YD. (m<sup>2</sup>).  
 PE: HMA BASE CSE., 6 (150) MEASURED IN SQ. YD. (m<sup>2</sup>).

**RURAL FIELD ENTRANCE (FE)**  
 HMA SURFACE COURSE, MIX "C", N50, 2 (50) MEASURED IN TONS (METRIC TONS)  
 AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m<sup>2</sup>).

**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

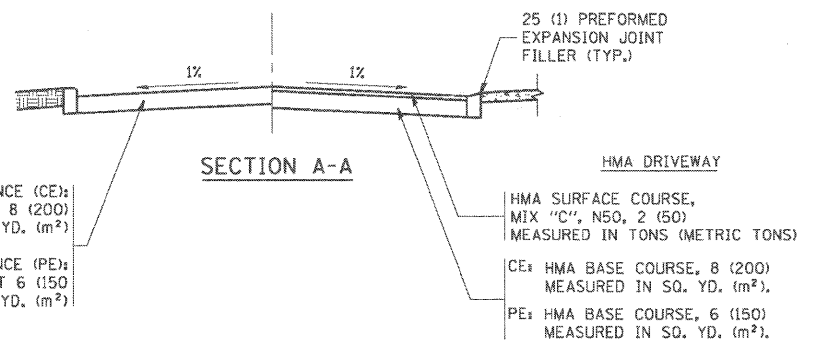
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



**RIGID DRIVEWAY**  
 COMMERCIAL ENTRANCE (CE): P.C.C. DRIVEWAY PAVEMENT 8 (200) MEASURED IN SQ. YD. (m<sup>2</sup>)  
 NON-COMMERCIAL ENTRANCE (PE): P.C.C. DRIVEWAY PAVEMENT 6 (150) MEASURED IN SQ. YD. (m<sup>2</sup>)

**HMA DRIVEWAY**  
 HMA SURFACE COURSE, MIX "C", N50, 2 (50) MEASURED IN TONS (METRIC TONS)  
 CE: HMA BASE COURSE, 8 (200) MEASURED IN SQ. YD. (m<sup>2</sup>).  
 PE: HMA BASE COURSE, 6 (150) MEASURED IN SQ. YD. (m<sup>2</sup>).

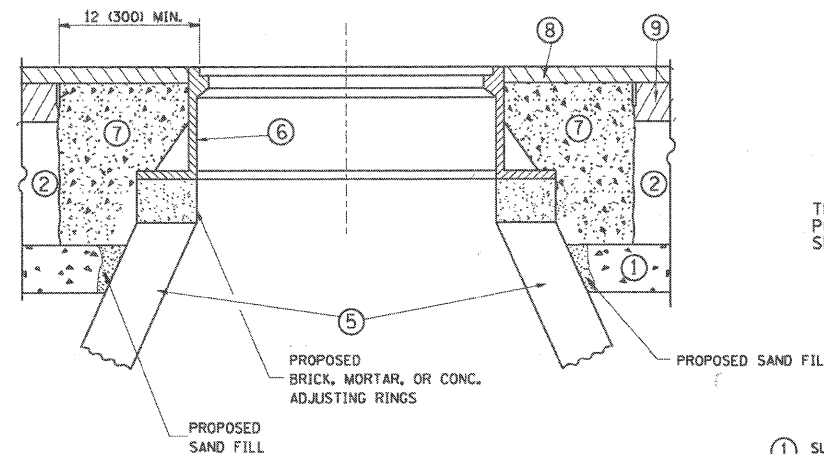
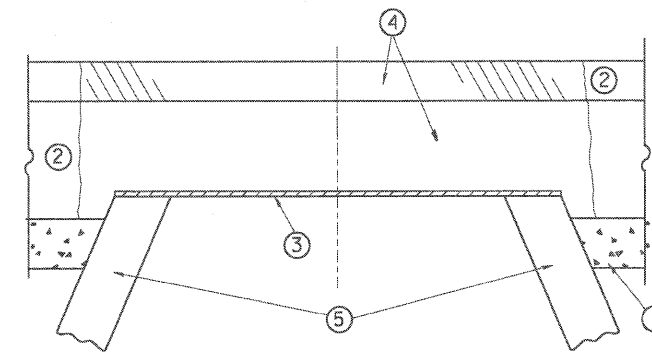
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 PROJECT CONTACT: #PROJECT CONTACT#  
 CLIENT: #CLIENT#  
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 PLOT DRIVER: pdtcalif  
 PEN TABLE: standard-trans.tbl

|                                  |                             |                    |                               |
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| c:\projects\dststd22x34\bd81.dgn |                             | DRAWN -            | REVISED - P. LOFLUER 04-15-03 |
|                                  | PLOT SCALE = 49.9999' / IN. | CHECKED -          | REVISED - R. BORO 01-01-07    |
|                                  | PLOT DATE = 6/12/2009       | DATE - 11-04-95    | REVISED - R. BORO 06-11-08    |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|   |                         |
|---|-------------------------|
| <b>DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.<br/>AND FACE OF CURB &amp; EDGE OF SHOULDER &gt;= 15' (4.5 m)</b> |                         |
| SCALE: NONE   | SHEET NO. 1 OF 1 SHEETS |
| STA.  | TO STA.                 |

|   |                 |               |                    |              |
|---|-----------------|---------------|--------------------|--------------|
| F.A. RTE. 347                                   | SECTION LY-RS-3 | COUNTY DUPAGE | TOTAL SHEETS 35    | SHEET NO. 25 |
| BD0156-07 (BD-01)                               |                 |               | CONTRACT NO. 60K82 |              |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                 |               |                    |              |



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

COMPANY NAME: #COMPANY NAME/ PROJECT CONTACT: #PROJECT CONTACT/ CLIENT: #CLIENT/ DATE PLOTTED: 2/14/2011 11:45:16 AM FILE NAME: 06100196.02-dst-02.dgn PLOT DRIVER: pdtcalif STANDARD TRANS:.tbl

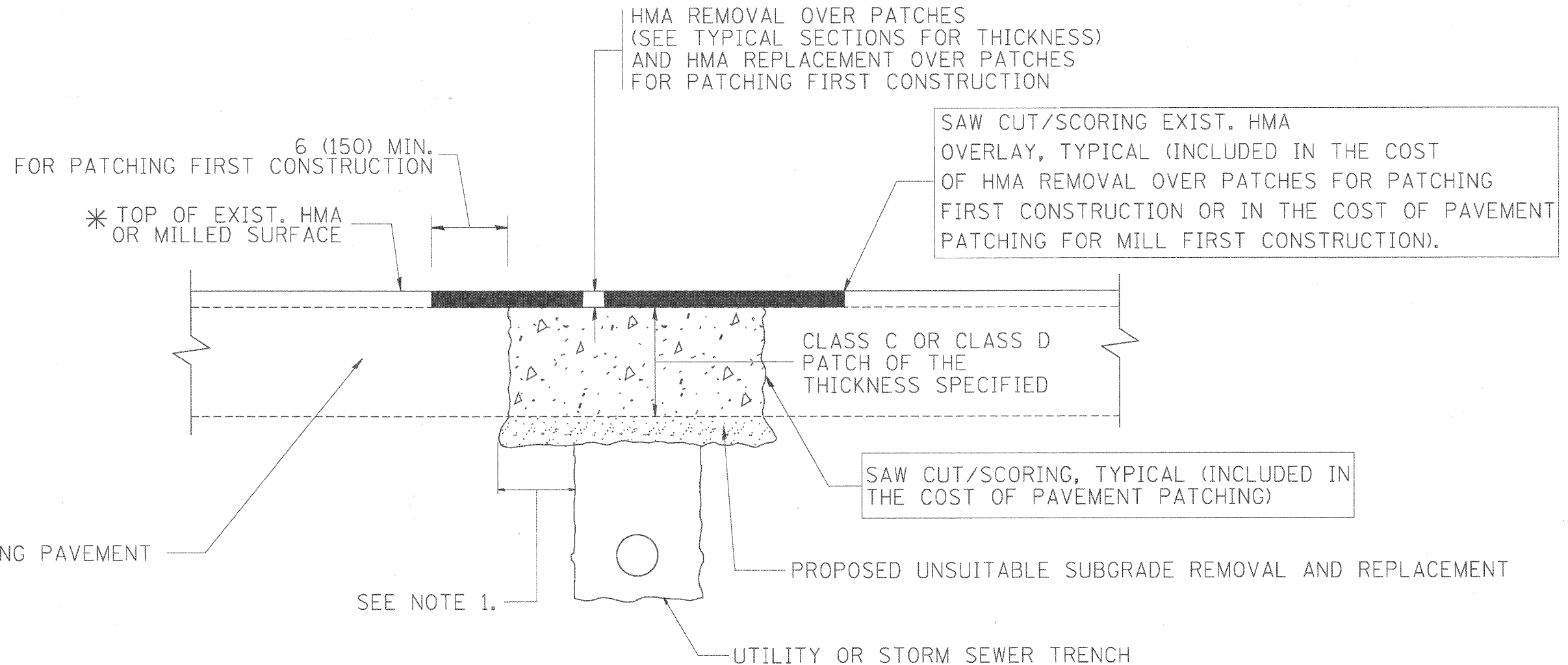
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|--------------------------------------|------------------------|--------------------|--------------------------------|
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|                                      |                        | DRAWN -            | REVISED - A. ABBAS 03-21-97    |
|                                      |                        | CHECKED -          | REVISED - R. WIEDEMAN 05-14-04 |
|                                      |                        | DATE - 10-25-94    | REVISED - R. BORO 01-01-07     |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A. RTE.                                       | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|---------|--------|--------------------|-----------|
| 347   | LY-RS-3 | DUPAGE | 35                 | 26        |
| BD600-03 (BD-8)                                 |         |        | CONTRACT NO. 60K82 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |                    |           |



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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 PROJECT CONTACT: #PROJECT CONTACT  
 CLIENT: #CLIENT  
 DATE PLOTTED: 2/14/2011 11:43:18 AM  
 FILE NAME: 86100196.02-def-03.dgn  
 PLOT DRIVER: pdf24r  
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|   |                     | DRAWN -            | REVISED - R. BORO 01-01-07  |
| PLOT SCALE = 50.000 ' / IN.                   |                     | CHECKED -          | REVISED - R. BORO 09-04-07  |
| PLOT DATE = 10/27/2008                        |                     | DATE - 10-25-94    | REVISED - K. ENG 10-27-08   |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

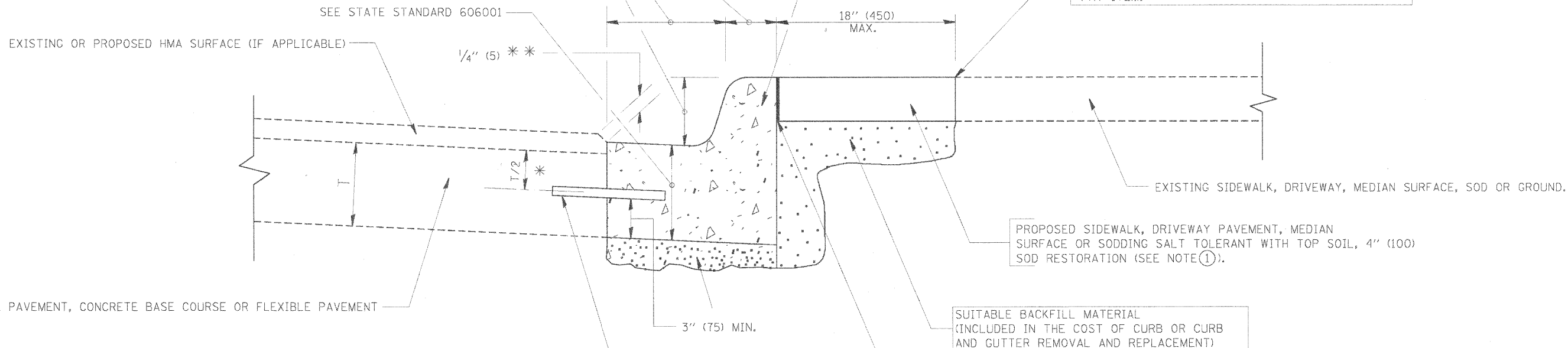
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| <b>PAVEMENT PATCHING FOR<br/>HMA SURFACED PAVEMENT</b> |                                      |
| SCALE: NONE  | SHEET NO. 1 OF 1 SHEETS STA. TO STA. |

| F.A. RTE.                                       | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|---------|--------|--------------------|-----------|
| 347   | LY-RS-3 | DUPAGE | 35                 | 27        |
| <b>BD400-04 (BD-22)</b>                         |         |        | CONTRACT NO. 60K82 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |                    |           |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PROJECT CONTACT: #/PROJECT-CONTACT  
CLIENT: #/CLIENT  
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PLOT DRIVER: pdfplot  
PEN TABLE: standard-trns.tbl

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88100136.02-dst-04.dgn

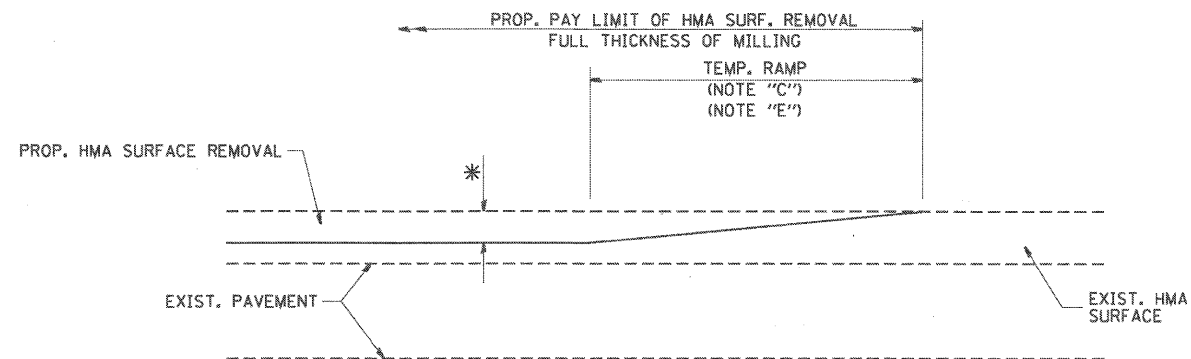
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|  | PLOT DATE = 12/15/2009      | DATE 03-11-94        | REVISED - R. BORO 12-15-09  |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

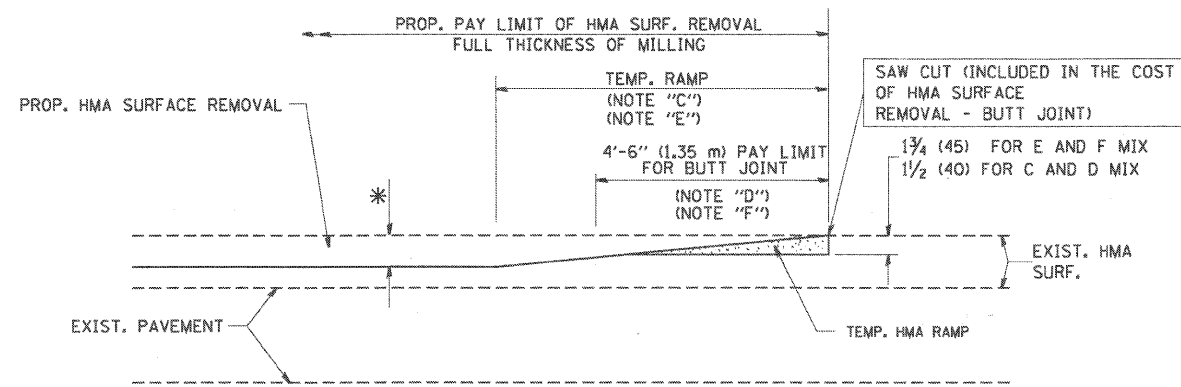
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|   |         |                    |              |           |
|---|---------|--------------------|--------------|-----------|
| F.A. RTE.                                       | SECTION | COUNTY             | TOTAL SHEETS | SHEET NO. |
| 347   | LY-RS-3 | DUPAGE             | 35           | 28        |
| BD600-06 (BD-24)                                |         | CONTRACT NO. 60K82 |              |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |                    |              |           |



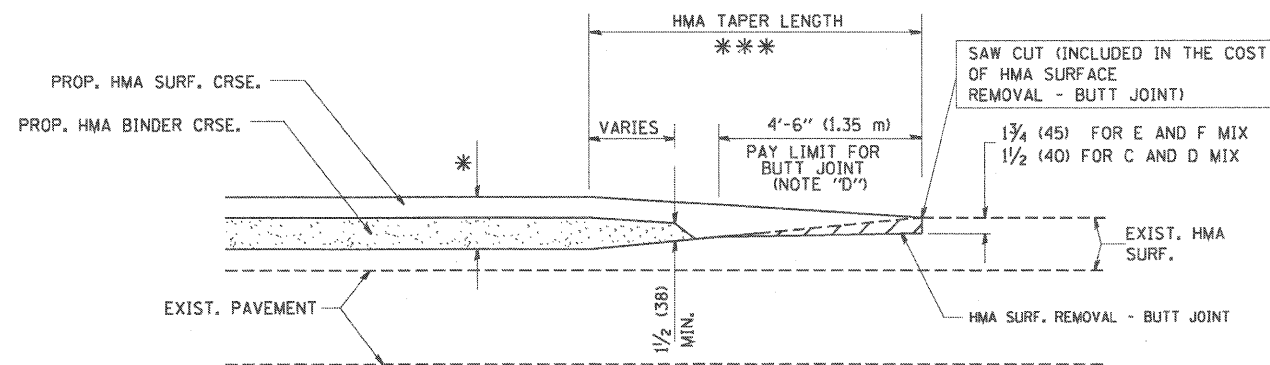
MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

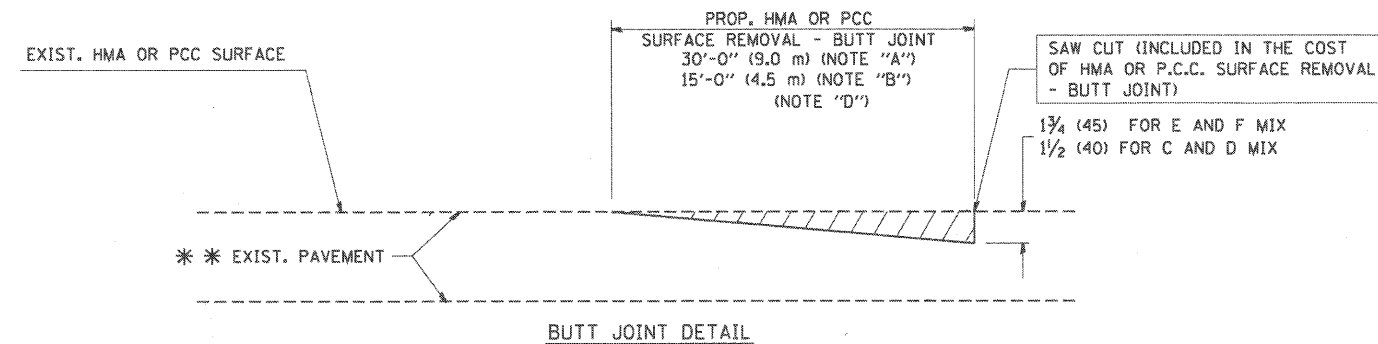


HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

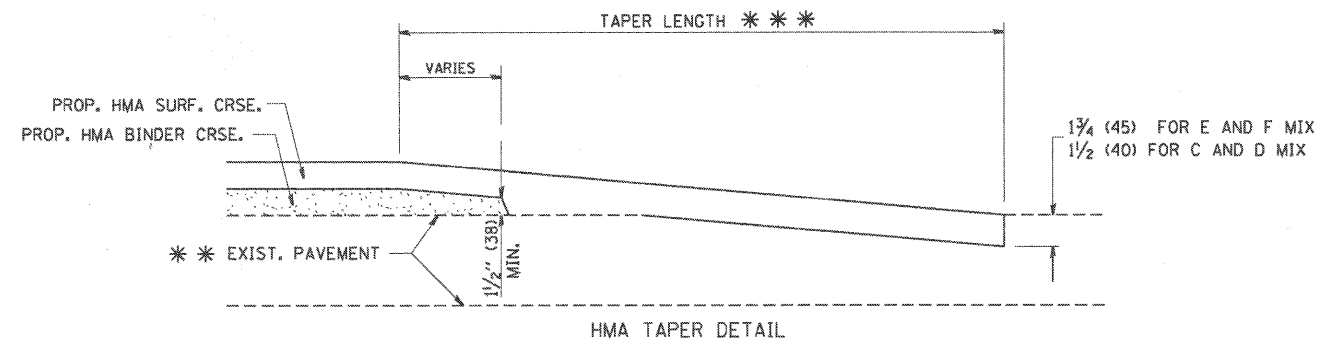
**OPTION 2**  
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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CLIENT: #CLIENT  
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PEN TABLE: Standard.ctb

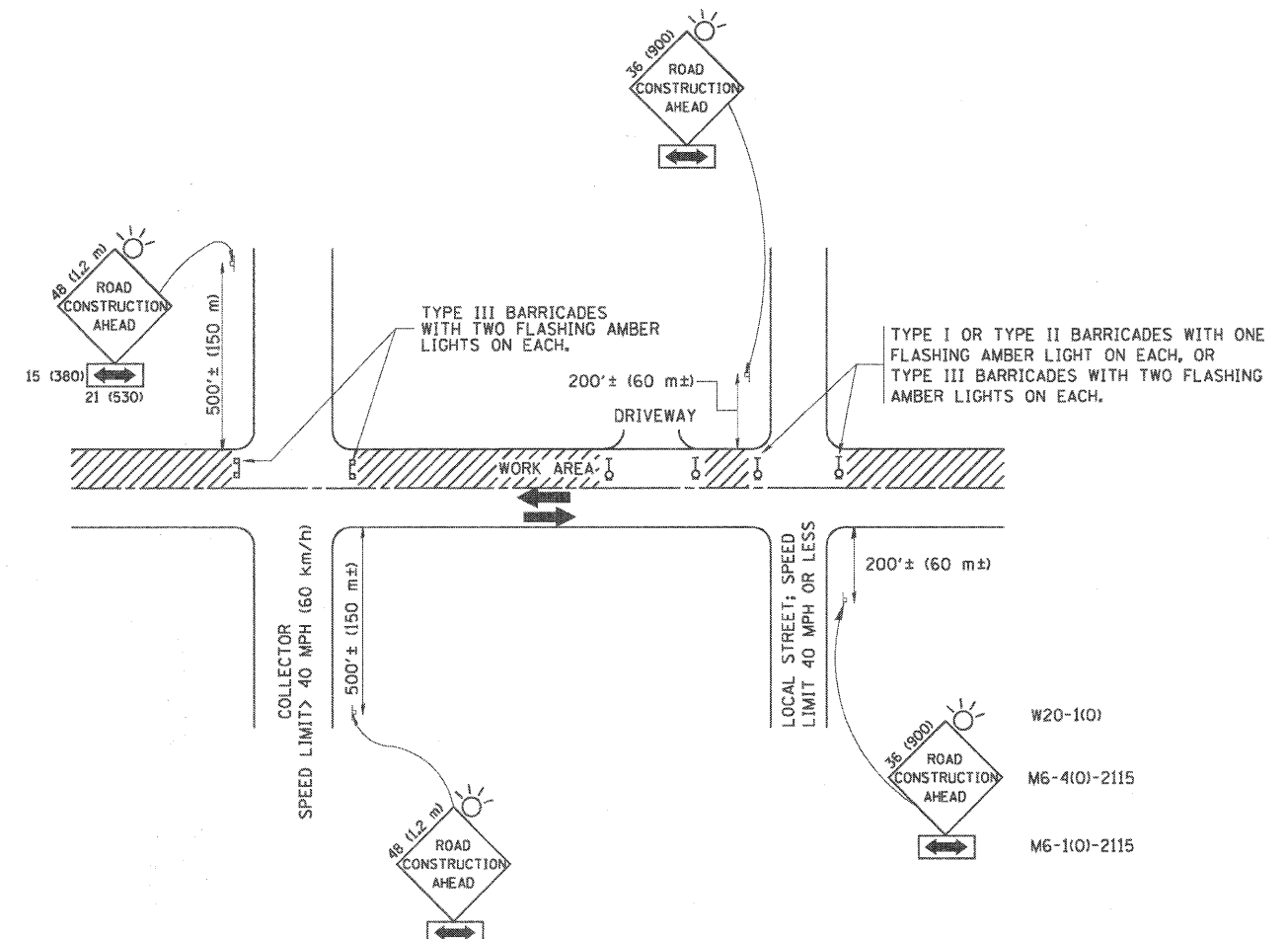
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| W:\dist\std\22x34\bd32.dgn |                        | DRAWN -               | REVISED - A. ABBAS 03-21-97 |
|                            |                        | CHECKED -             | REVISED - M. GOMEZ 04-06-01 |
|                            |                        | DATE - 06-13-90       | REVISED - R. BORO 01-01-07  |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A. RTE.                                       | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|---------|--------|--------------------|-----------|
| 347   | LY-RS-3 | DUPAGE | 35                 | 29        |
| BD400-05 BD32                                   |         |        | CONTRACT NO. 60K82 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |                    |           |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

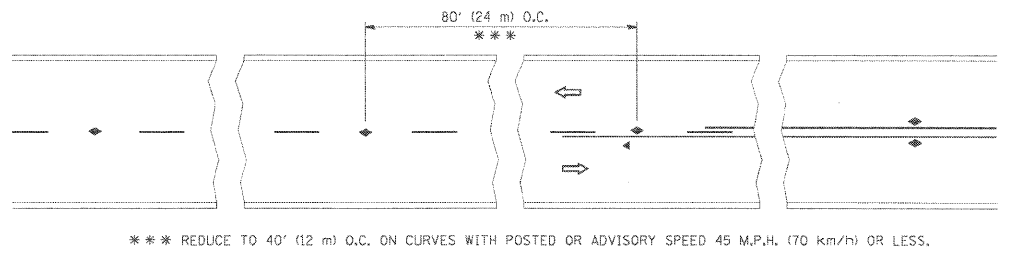
All dimensions are in millimeters (inches) unless otherwise shown.

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 PEN TABLE: StandardTrans.tbl

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|   |                            | DRAWN -           | REVISED -<br>A. HOUSEH 03-06-96    |
|   | PLOT SCALE = 60.000' / IN. | CHECKED -         | REVISED -<br>A. HOUSEH 10-16-96    |
|   | PLOT DATE = 1/4/2008       | DATE -<br>06-89   | REVISED -<br>T. RAMMACHER 01-06-00 |

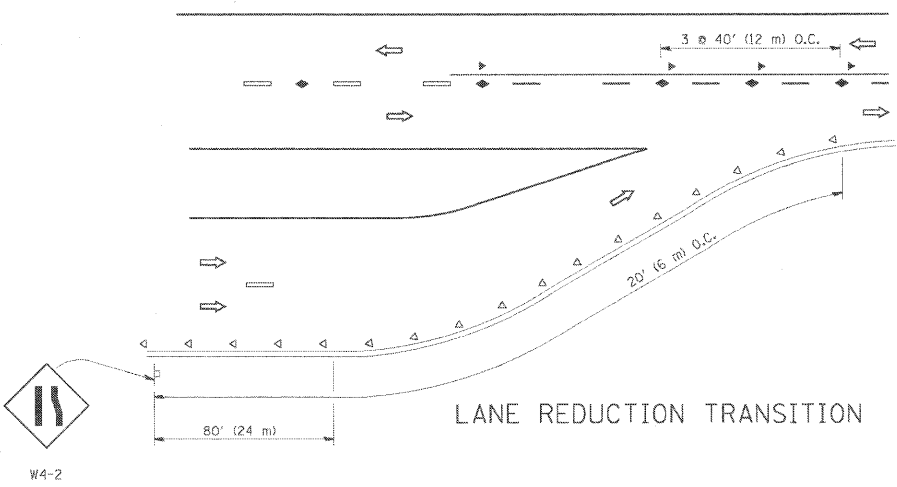
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

|  |  |   |         |              |                 |              |
|--|--|---|---------|--------------|-----------------|--------------|
| TRAFFIC CONTROL AND PROTECTION FOR<br>SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS |  | F.A.<br>RTE.                                    | SECTION | COUNTY       | TOTAL<br>SHEETS | SHEET<br>NO. |
| SCALE: NONE  |  | 347   | LY-RS-3 | DUPAGE       | 35              | 30           |
| SHEET NO. 1 OF 1 SHEETS  |  | TC-10   |         | CONTRACT NO. |                 | 60K82        |
| STA. TO STA.   |  | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |              |                 |              |

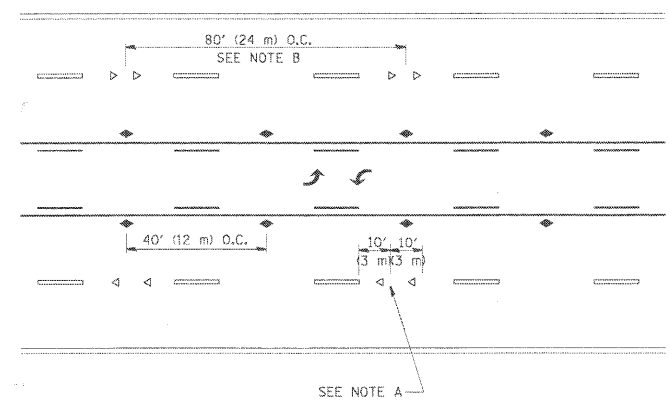


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

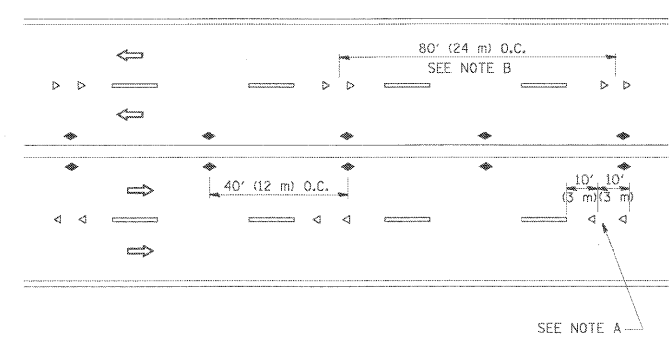
TWO-LANE/TWO-WAY



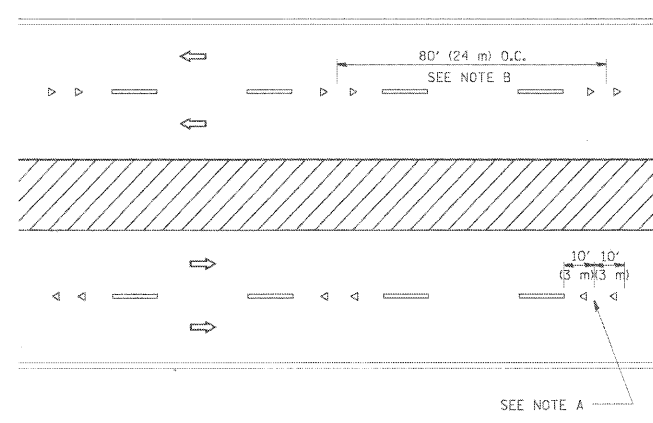
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

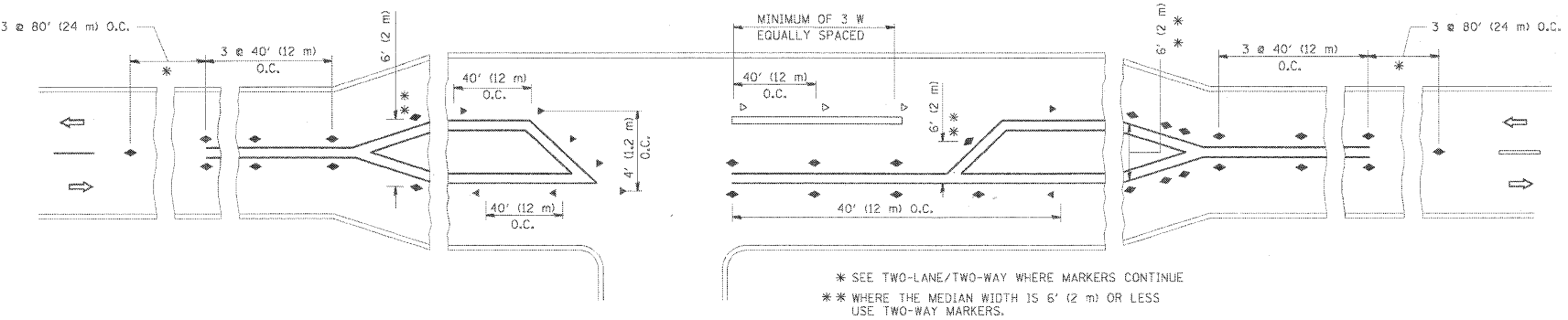
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in Inches (millimeters) unless otherwise shown.

COMPANY NAME: #COMPANY NAME/  
 PROJECT CONTACT: #PROJECT CONTACT/  
 CLIENT: 2/14/2011 11:45:28 AM  
 DATE PLOTTED: 8/10/2009 02:06:07-07.dgn  
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 PLOT DRIVER: pdcalif  
 PER TABLE: Standard Trans.tbl

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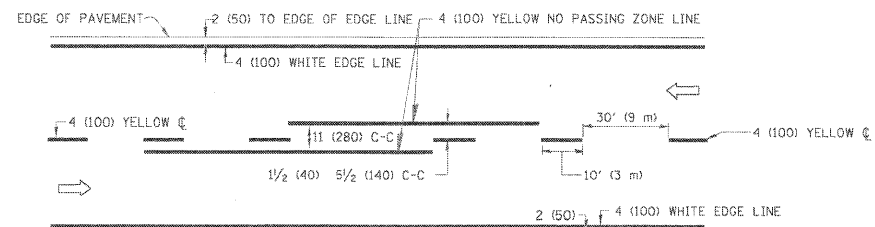
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

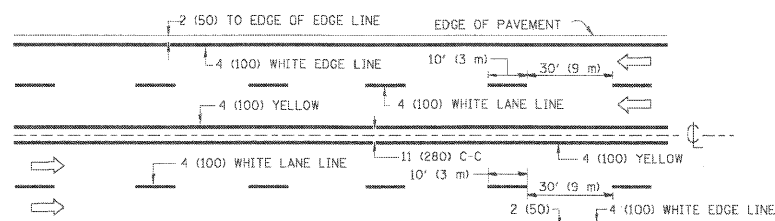
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |           |             |                    |           |
|---|-----------|-------------|--------------------|-----------|
| F.A. RTE.                                       | SECTION   | COUNTY      | TOTAL SHEETS       | SHEET NO. |
| DUPAGE  | IL RTE 38 | D-91-607-10 | 35                 | 31        |
| TC-11   |           |             | CONTRACT NO. 60K82 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |           |             |                    |           |

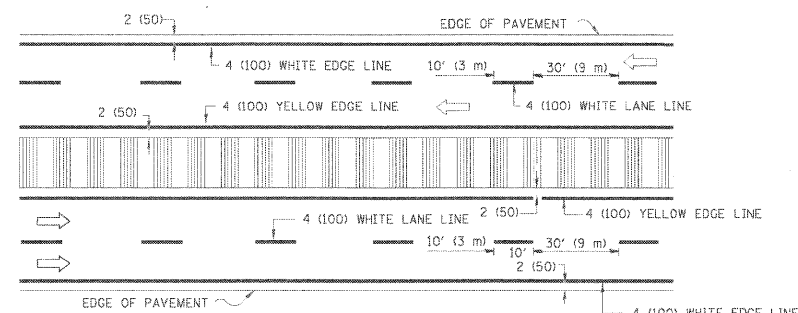




2-LANE ROADWAY



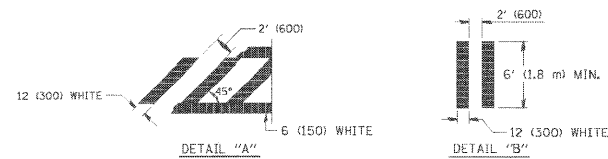
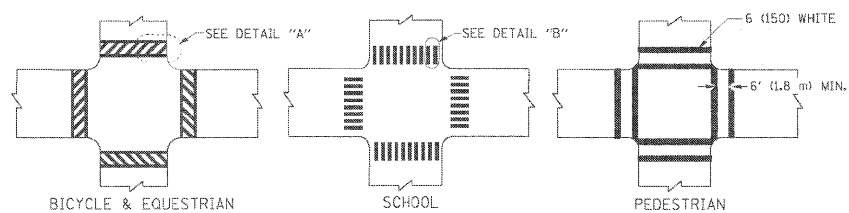
MULTI-LANE UNDIVIDED



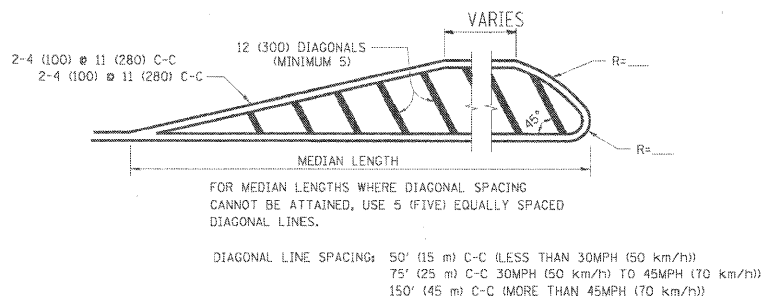
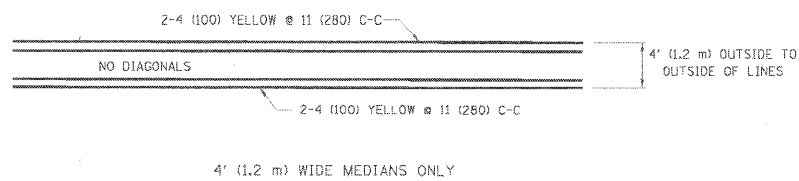
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

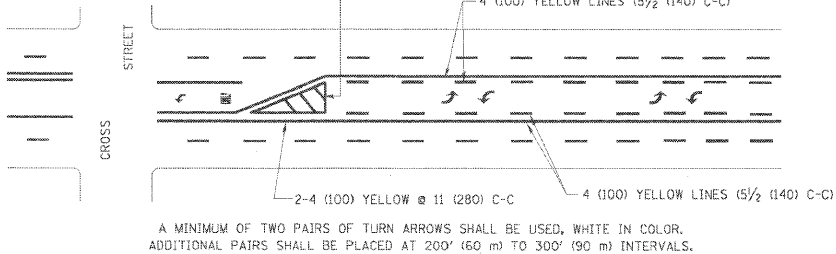
TYPICAL LANE AND EDGE LINE MARKING



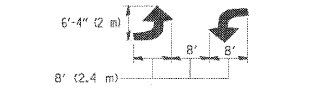
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE

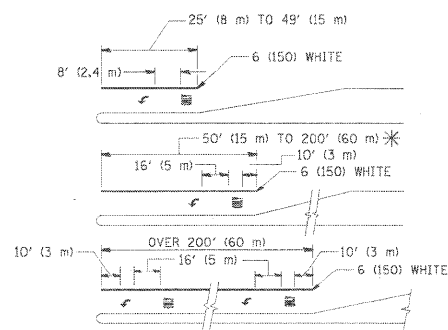


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

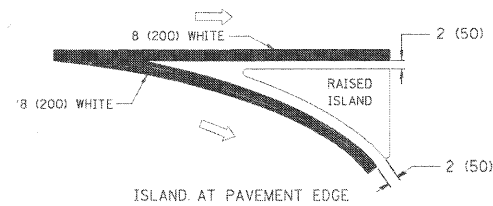
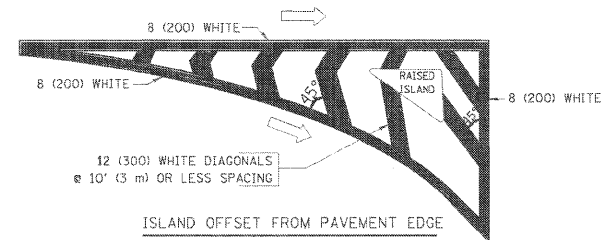


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED. \* AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING   | WIDTH OF LINE   | PATTERN             | COLOR  | SPACING / REMARKS  |
|---|---|---------------------|--|--|
| CENTERLINE ON 2 LANE PAVEMENT   | 4 (100)   | SKIP-DASH           | YELLOW   | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT   | 2 @ 4 (100)   | SOLID               | YELLOW   | 11 (280) C-C   |
| NO PASSING ZONE LINES FOR ONE DIRECTION   | 4 (100)   | SOLID               | YELLOW   | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE  |
| NO PASSING ZONE LINES FOR BOTH DIRECTIONS   | 2 @ 4 (100)   | SOLID               | YELLOW   | 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN   |
| LANE LINES  | 4 (100) 5 (125) ON FREEWAYS   | SKIP-DASH SKIP-DASH | WHITE WHITE                                    | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)                             | SAME AS LINE BEING EXTENDED   | SKIP-DASH           | SAME AS LINE BEING EXTENDED                    | 2' (600) LINE WITH 6' (1.8 m) SPACE  |
| EDGE LINES  | 4 (100)   | SOLID               | YELLOW-LEFT WHITE-RIGHT                        | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB  |
| TURN LANE MARKINGS  | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))                                   | SOLID               | WHITE  | SEE TYPICAL TURN LANE MARKING DETAIL   |
| TWO WAY LEFT TURN MARKING   | 2 @ 4 (100) EACH DIRECTION  | SKIP-DASH AND SOLID | YELLOW   | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE   |
|   | 8" (2.4m) LEFT ARROW  | IN PAIRS            | WHITE  | SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL   |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°   | SOLID SOLID SOLID   | WHITE WHITE WHITE                              | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.   |
| STOP LINES  | 24 (600)  | SOLID               | WHITE  | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSWALK CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS   | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID               | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.   |
| GORE MARKING AND CHANNELIZING LINES   | 8 (200) WITH 12 (300) DIAGONALS @ 45°   | SOLID               | WHITE  | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))                       |
| RAILROAD CROSSING   | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"            | SOLID               | WHITE  | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )  |
| SHOULDER DIAGONALS  | 12 (300) @ 45°  | SOLID               | WHITE - RIGHT YELLOW - LEFT                    | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))                             |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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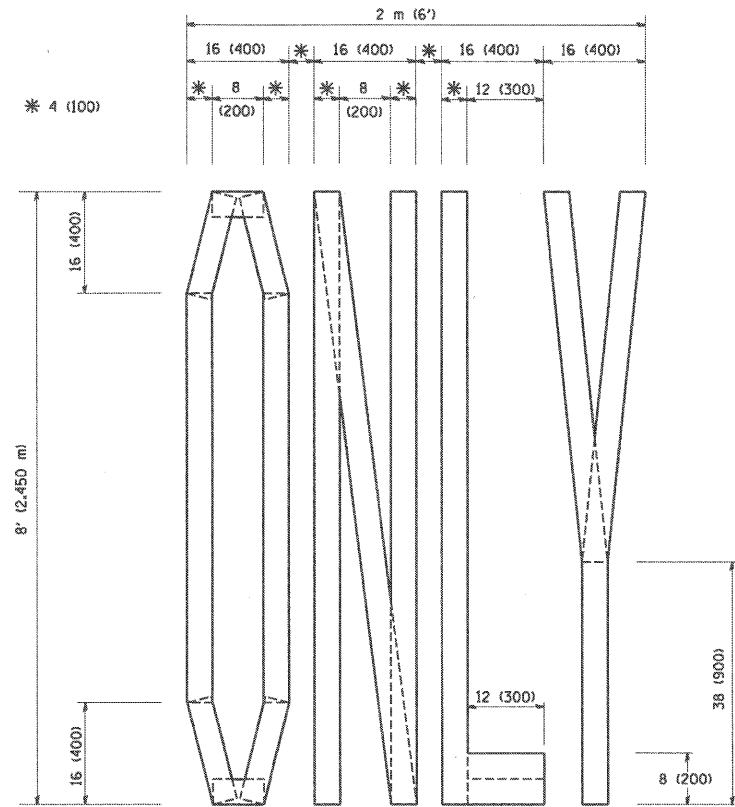
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

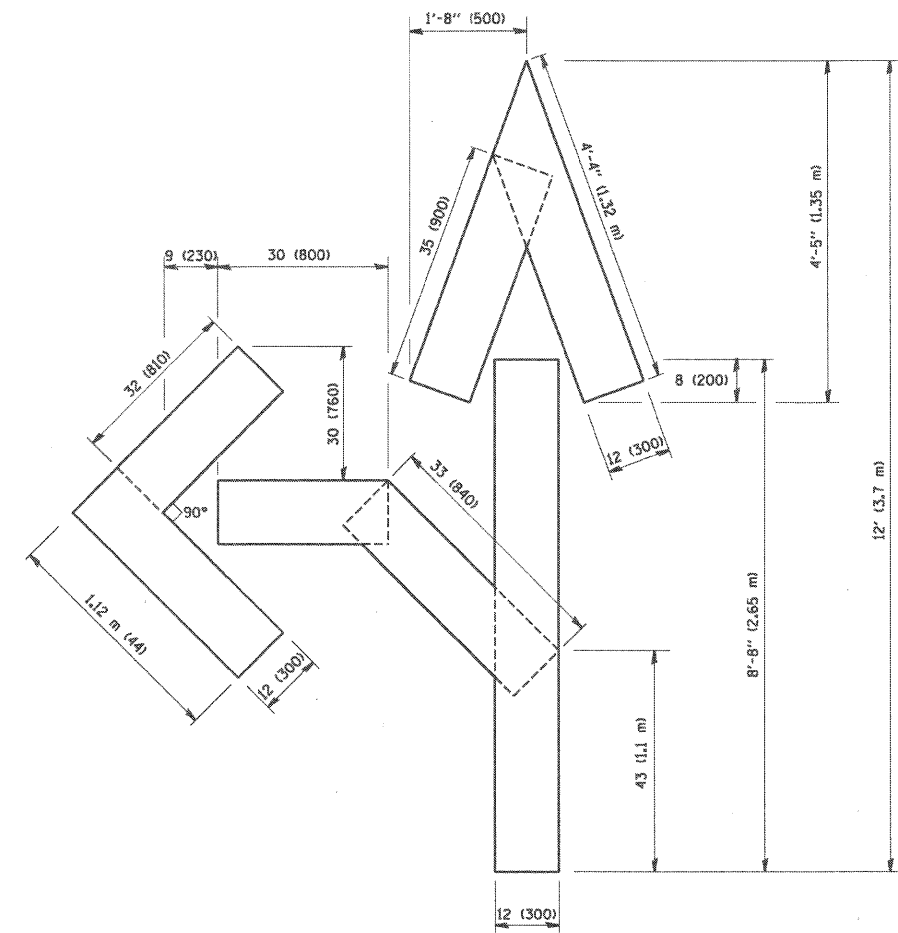
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| F.A. RTE. 347                                   | SECTION LY-RS-3 | COUNTY DUPAGE | TOTAL SHEETS 35    | SHEET NO. 32 |
| TC-13   |                 |               | CONTRACT NO. 60K82 |              |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                 |               |                    |              |

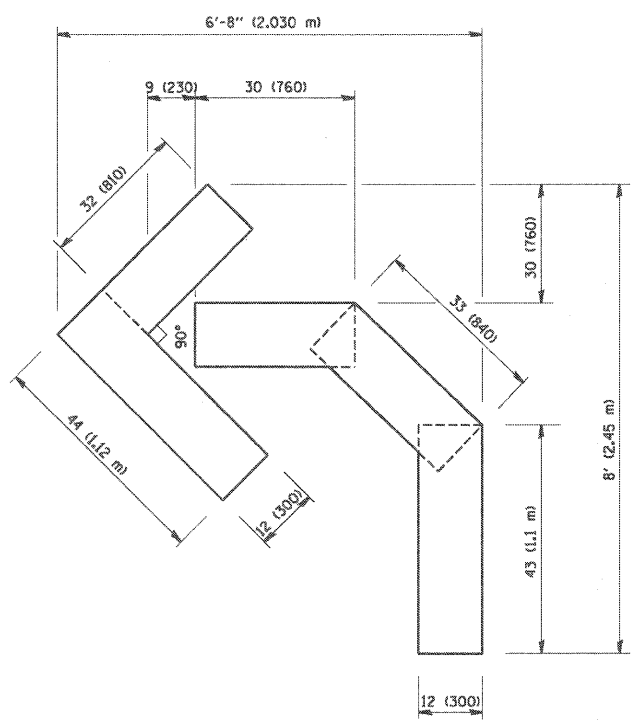




QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

COMPANY NAME: #/COMPANY\_NAME/  
 PROJECT CONTACT: #/PROJECT\_CONTACT/  
 CLIENT: #/CLIENT/  
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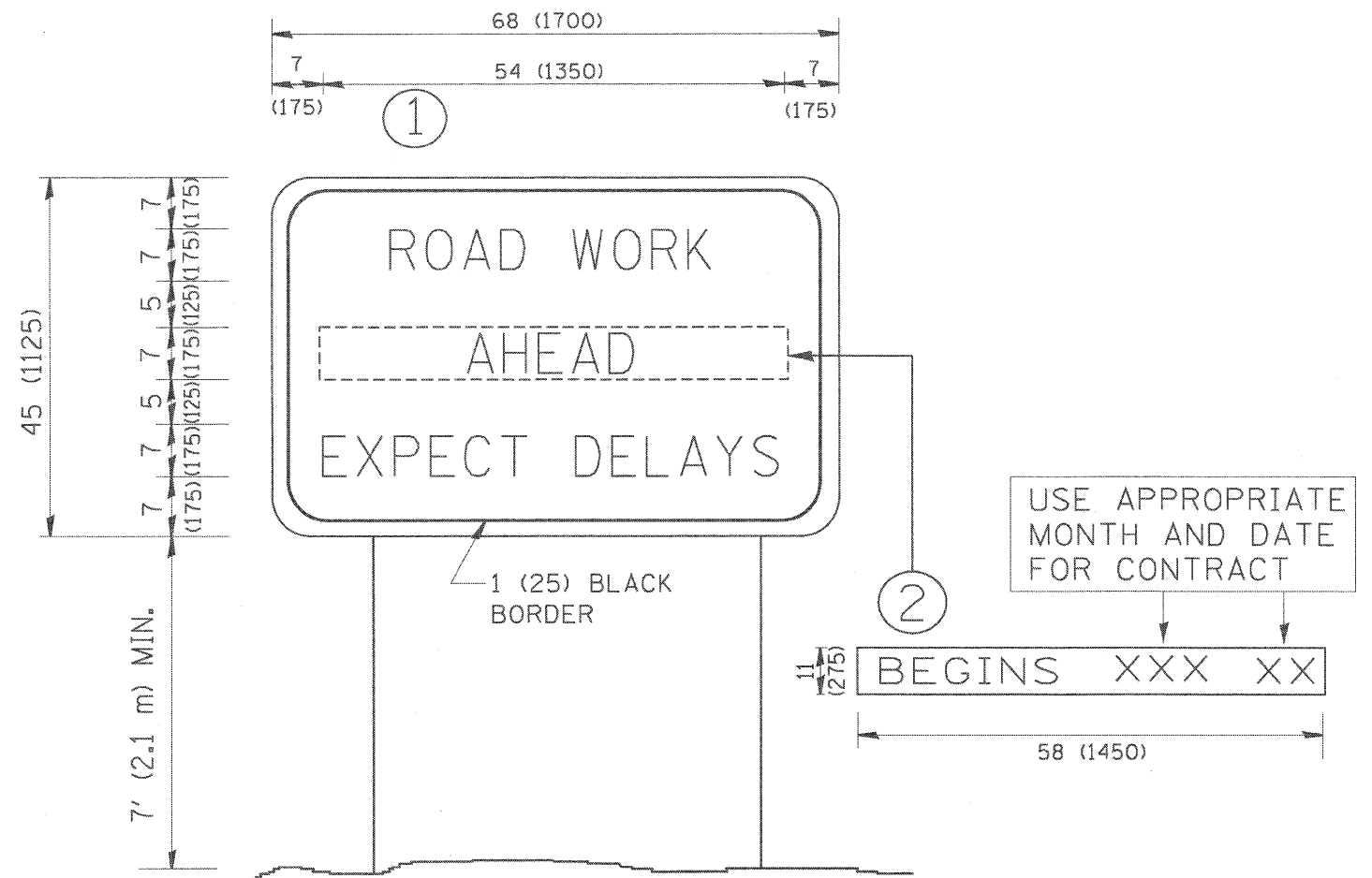
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| DRAWN -         | REVISOR - |
| CHECKED -       | REVISOR - |
| DATE - 09-18-94 | REVISOR - |

|                                 |
|---------------------------------|
| REVISOR - T. RAMMACHER 06-05-96 |
| REVISOR - T. RAMMACHER 11-04-97 |
| REVISOR - T. RAMMACHER 03-02-98 |
| REVISOR - E. GOMEZ 08-28-00     |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

| PAVEMENT MARKING LETTERS AND SYMBOLS<br>FOR TRAFFIC STAGING |                         |      |         |
|---|-------------------------|------|---------|
| SCALE: NONE   | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| F.A. RTE.                                       | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|---------|--------|--------------------|-----------|
| 347   | LY-RS-3 | DUPAGE | 35                 | 33        |
| TC-16   |         |        | CONTRACT NO. 60K82 |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |                    |           |



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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 CLIENT: #CLIENT#  
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|   | PLOT DATE = 1/4/2009        | DATE -     | REVISED - C. JUCIUS 01-31-07    |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
 INFORMATION SIGN**

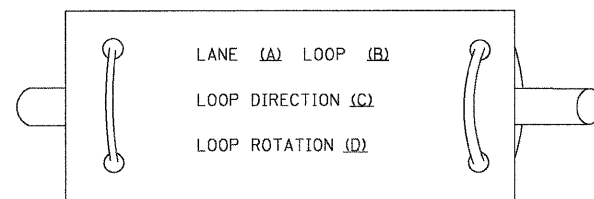
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| F.A. RTE.                                       | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
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| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |         |        |                    |           |

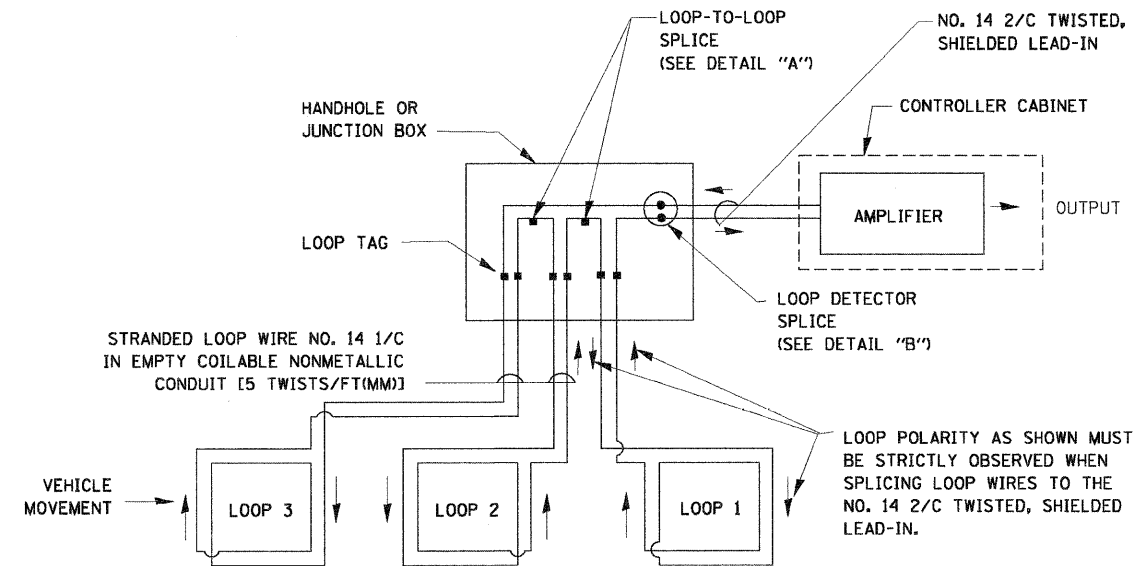
## LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

### LOOP LEAD-IN CABLE TAG

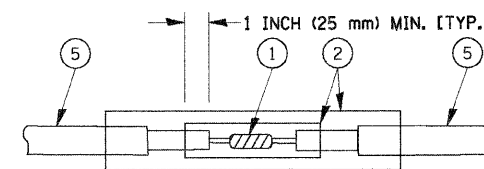


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

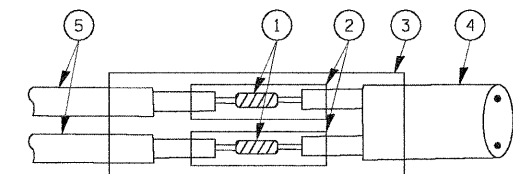


### DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

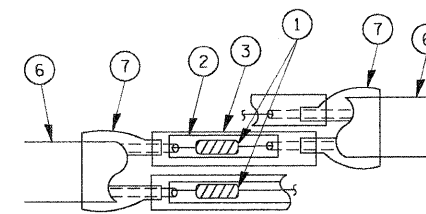


DETAIL "A"  
LOOP-TO-LOOP SPLICE

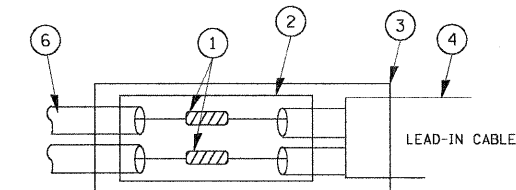


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

### TYPE I LOOP



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

### LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

COMPANY NAME: #COMPANY NAME  
PROJECT CONTACT: #PROJECT CONTACT  
DESIGNER: #DESIGNER  
CHECKER: #CHECKER  
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

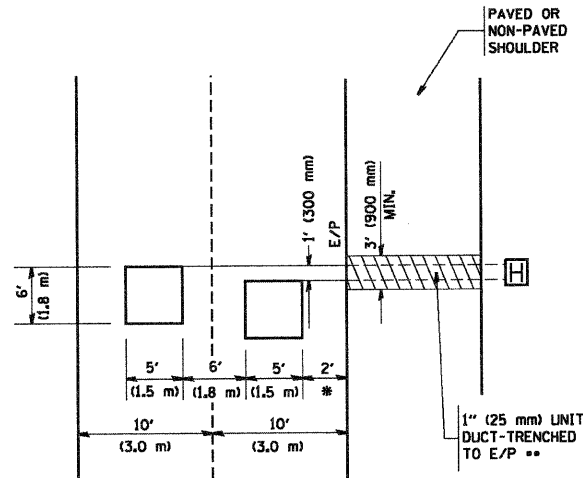
DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                 |               |                    |              |
|---|-----------------|---------------|--------------------|--------------|
| F.A. RTE. 347                                   | SECTION LY-RS-3 | COUNTY DUPAGE | TOTAL SHEETS 35    | SHEET NO. 35 |
| TS-05   |                 |               | CONTRACT NO. 60K82 |              |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                 |               |                    |              |

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

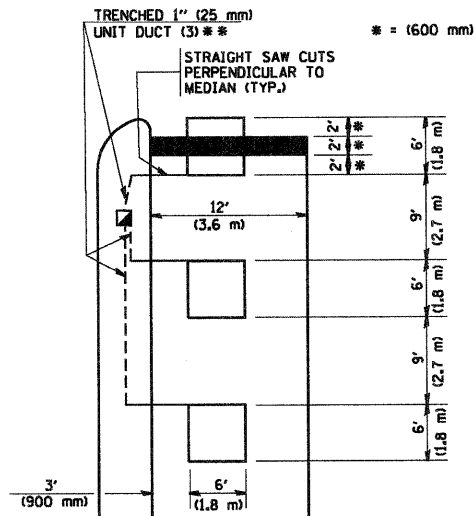


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**

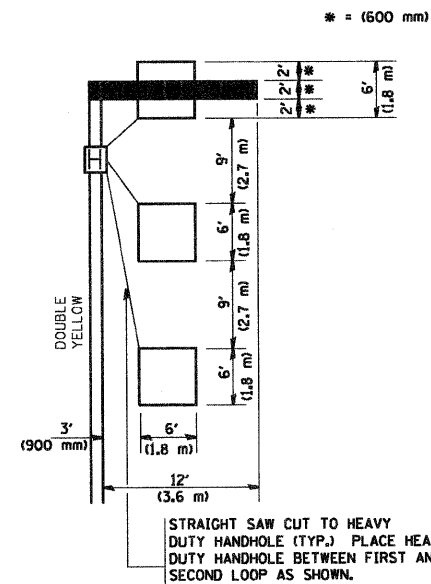
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

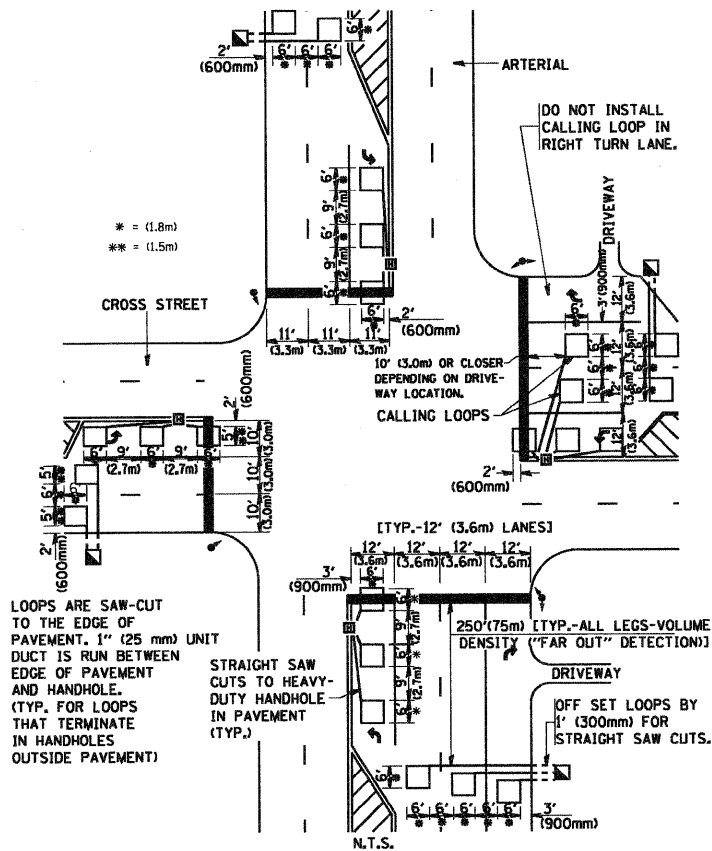
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**



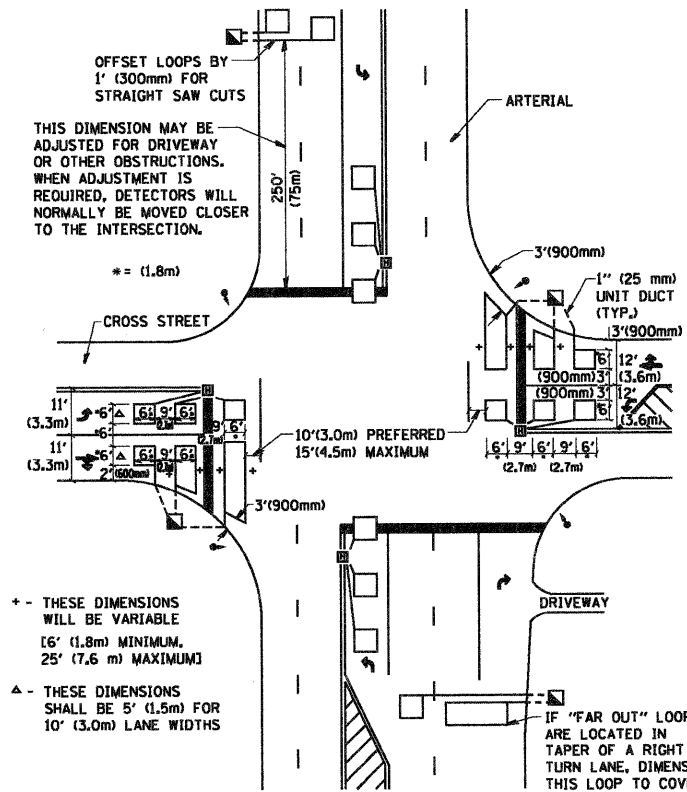
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1  
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2  
N.T.S.**

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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FILE NAME: pdf:pdf  
PLOT DRIVER: #trns:tbl

|                             |                       |                  |           |   |  |                         |              |   |         |        |              |           |
|-----------------------------|-----------------------|------------------|-----------|---|--|-------------------------|--------------|---|---------|--------|--------------|-----------|
| FILE NAME =                 | USER NAME = geglionbt | DESIGNED -       | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DISTRICT 1 - DETECTOR LOOP INSTALLATION<br/>DETAILS FOR ROADWAY RESURFACING</b> |                         |              | F.A. RTE. =   | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| W:\distatd\22x34\ts07.dgn   |                       | DRAWN -          | REVISED - |   | 347  | LY-RS-3                 | DUPAGE       | 35  | 35A     |        |              |           |
| PLOT SCALE = 50.0000' / IN. |                       | CHECKED - R.K.F. | REVISED - |   | <b>TS-07</b>   |                         |              | <b>CONTRACT NO. 60K82</b>                           |         |        |              |           |
| PLOT DATE = 1/4/2000        |                       | DATE -           | REVISED - |   | SCALE: NONE  | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT |         |        |              |           |