

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
LOCAL AGENCY PROJECT

FAU ROUTE 9247 (BELLEVILLE STREET)
SECTION 08-00018-00-PV
PROJECT NO. M-5011(310)
CITY OF LEBANON, ILLINOIS
ST. CLAIR COUNTY
JOB NO. C-98-347-10

| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|-----------|----------------|--------------|
| 9247 | 08-00018-00-PV | ST. CLAIR | 19 | 1 |
| STA 200+88.23 | | TO | STA. 215+72.98 | |
| FEDERAL AID PROJECT | | | CONTRACT 97454 | |

INDEX OF SHEETS

1. COVER SHEET
2. SUMMARY OF QUANTITIES, GENERAL NOTES & HORIZONTAL CONTROL TIES
3. TYPICAL SECTIONS
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9. STORM SEWER PROFILES
10. TRAFFIC CONTROL PLAN
11. ENTRANCE DETAILS
12. MISCELLANEOUS DETAILS
- 13.-19. CROSS SECTIONS

APPLICABLE IDOT HIGHWAY STANDARDS:

| | | |
|-----------|-----------|-----------|
| 424001-05 | 602701-02 | 72000F-02 |
| 602301-03 | 604066-02 | 720011-01 |
| 602306-03 | 606001-04 | 729001-01 |
| 602401-03 | 701501-06 | 780001-02 |
| 602801-02 | 701901-01 | BLR 22-6 |

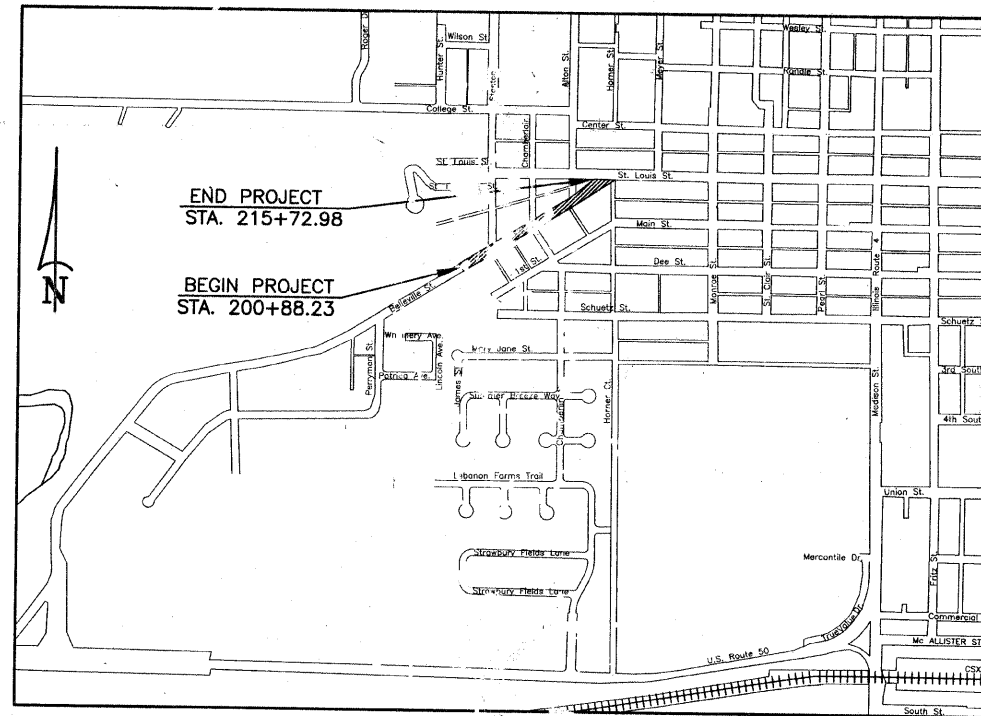
SCALES:

| | |
|---------------------|---------|
| PLAN | 0 10 20 |
| PROFILE HORZ. | 0 10 20 |
| PROFILE VERT. | 0 2.5 5 |
| CROSS SECTION HORZ. | 0 5 40 |
| CROSS SECTION VERT. | 0 2.5 5 |

| UTILITY TYPE | NAME OF UTILITY | PHONE NUMBER |
|----------------|------------------------|----------------|
| ELECTRIC | AMEREN IP | 1-800-755-5000 |
| GAS | AMEREN IP | 1-800-755-5000 |
| WATER | CITY OF LEBANON | 1-618-537-4976 |
| TELEPHONE | AT&T | 1-800-480-8088 |
| CABLE | CHARTER COMMUNICATIONS | 1-888-436-2427 |
| SANITARY SEWER | CITY OF LEBANON | 1-618-537-4976 |

BELLEVILLE STREET

ROADWAY DESIGNATION: URBAN COLLECTOR
DESIGN SPEED: 30 M.P.H.
DESIGN YEAR: (2020) AAT: 6540



LOCATION MAP
NOT TO SCALE

LENGTH OF IMPROVEMENT 1,484.75 FT. = 0.2812 MI.

▨ = IMPROVEMENT AREA

PREPARED BY:



RHUTASEL and ASSOCIATES, INC.
CONSULTING ENGINEERS AND LAND SURVEYORS

CORPORATE OFFICE
4 INDUSTRIAL COURT
FREEBURG, ILLINOIS 62243
(618) 539-3178

REGIONAL OFFICE
201 SOUTH LOCUST STREET
CENTRALIA, ILLINOIS 62801
(618) 532-1992



LOCATION OF PROJECT INDICATED THIS:

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

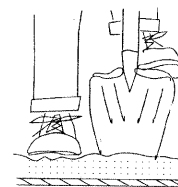
APPROVED 3/3/11 DATE
Scott Abner
SCOTT ABNER, MAYOR

PASSED 3-18-11 DATE
Mary C. Lamie
DISTRICT 8/ENGINEER OF LOCAL ROADS & STREETS

RELEASED FOR BID
BASED ON LIMITED REVIEW 3-18-11 DATE
Mary C. Lamie
MARY C. LAMIE, P.E.
DEPUTY DIRECTOR OF HIGHWAYS
REGION FIVE ENGINEER

PRINTED BY AUTHORITY OF THE STATE OF ILLINOIS

HOLD IT! CALL BEFORE YOU DIG!
Phone J.U.L.I.E.



Underground Cable

The owner would like to remind you that underground utility facilities can be damaged by shovel blades or other digging equipment. You can prevent damage or possible personal danger by phoning 800/892-0123 at least 48 hours before you dig. That's the toll-free number for "J.U.L.I.E.", which stands for Joint Utilities Location Information for Excavators.

If there are any pipes, cables, lines or mains in the excavation area, utility personnel will be out to mark the facilities so you can work around them.

Toll Free 800/892-0123



Sidney W. Legrand
3/1/11
Date of Signing
11/30/11
Date of License Expiration

SUMMARY OF QUANTITIES

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY |
|-----------|---|--------|----------------|
| 20200100 | EARTH EXCAVATION | CU YD | 501 |
| 20800150 | TRENCH BACKFILL | CU YD | 100 |
| 25000100 | SEEDING, CLASS 1 | ACRE | 0.70 |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 63 |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 63 |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 63 |
| 25100115 | MULCH, METHOD 2 | ACRE | 0.70 |
| 25100630 | EROSION CONTROL BLANKET | SQ YD | 758 |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 65 |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 9 |
| 30200650 | PROCESSING MODIFIED SOIL 12" | SQ YD | 5143 |
| 30201500 | LIME | TON | 99 |
| 35100100 | AGGREGATE BASE COURSE, TYPE A | TON | 186 |
| 40201000 | AGGREGATE FOR TEMPORARY ACCESS | TON | 150 |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 321 |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 7 |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL, BUTT-JOINT | SQ YD | 117 |
| 40603085 | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N/0 | TON | 1021 |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N/0 | TON | 481 |
| 48800050 | INCIDENTAL HOT-MIX ASPHALT SURFACING | TON | 72 |
| 42091300 | PROTECTIVE COAT | SQ YD | 824 |
| 42300200 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH | SQ YD | 200 |
| 42400100 | PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH | SQ FT | 3793 |
| 42400800 | DETECTABLE WARNINGS | SQ FT | 56 |
| 44000100 | PAVEMENT REMOVAL | SQ YD | 4861 |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL | SQ YD | 125 |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 2992 |
| 44000600 | SIDEWALK REMOVAL | SQ FT | 3684 |
| 50105220 | PIPE CURVE REMOVAL | FOOT | 229 |
| 550A2320 | STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 12" | FOOT | 688 |
| 56109210 | WATER VALVES TO BE ADJUSTED | EACH | 1 |
| 56500300 | DOMESTIC METER VAULTS TO BE ADJUSTED | EACH | 1 |
| 50218400 | MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 1 |
| 60237000 | INLETS, TYPE A, TYPE 15 FRAME AND LID | EACH | 4 |
| 60240320 | INLETS, TYPE B, TYPE 15 FRAME AND LID | EACH | 4 |
| 60262700 | INLETS TO BE RECONSTRUCTED | EACH | 1 |
| 60500060 | REMOVING INLETS | EACH | 4 |
| 60605000 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 | FOOT | 2965 |
| 67100100 | MOBILIZATION | L SUM | 1 |
| 72000100 | SIGN PANEL-TYPE 1 | SQ FT | 39.25 |
| 72900100 | METAL POST-TYPE A | FOOT | 60 |
| *78000200 | THERMOPLASTIC PAVEMENT MARKING-LINE 4" | FOOT | 2912 |
| *78000400 | THERMOPLASTIC PAVEMENT MARKING-LINE 6" | FOOT | 269 |
| *78000650 | THERMOPLASTIC PAVEMENT MARKING-LINE 24" | FOOT | 43 |
| LR430040 | PAVING BRICK SIDEWALK | SQ YD | 114 |
| X0324864 | DRAIN CONNECTIONS | FOOT | 25 |
| X2010400 | STUMP REMOVAL ONLY | UNIT | 74 |
| X2110100 | TOPSOIL FURNISHED AND PLACED, SPECIAL | CU YD | 70 |
| X4404700 | SIDEWALK REMOVAL (SPECIAL) | SQ FT | 898 |
| X7010216 | TRAFFIC CONTROL AND PROTECTION, (SPECIAL) | L SUM | 1 |
| Z0007125 | HANDRAIL REMOVAL | EACH | 3 |
| Z0012455 | CONCRETE STEP REMOVAL | EACH | 8 |
| Z0056608 | STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH | FOOT | 104 |

*SPECIALTY ITEM

GENERAL NOTES

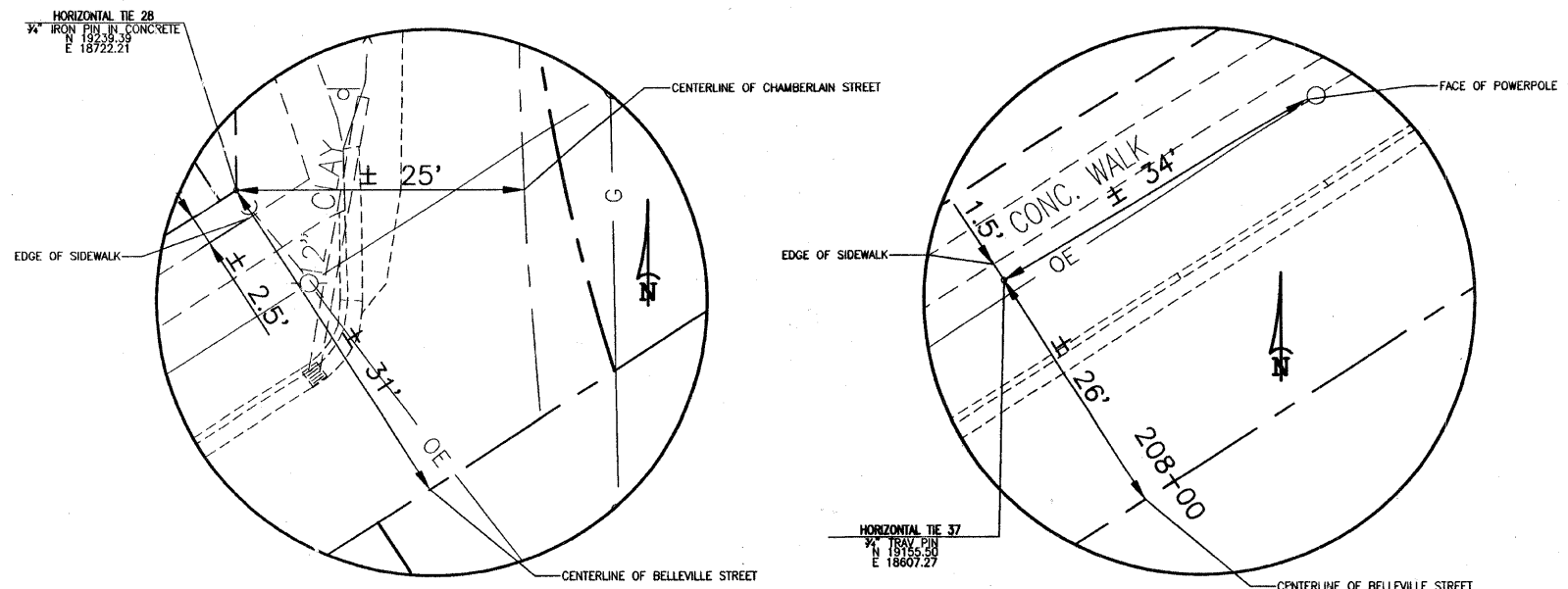
- ALL CONSTRUCTION SHALL BE DONE ACCORDING TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2007.
- THE PROPOSED EMBANKMENT SHALL BE BENCHMARKED INTO THE EXISTING SLOPES TO THE SATISFACTION OF THE ENGINEER.
- "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE PLACED AT THE BEGINNING AND END OF THE PROJECT PLUS THE INTERSECTING SIDE ROADS AND WILL BE INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS. ALL CONSTRUCTION SIGNS SHALL BE FLUORESCENT ORANGE 48"x48".
- ALL AREAS DISTURBED FOR ANY REASON WITHIN THE CONSTRUCTION LIMITS SHALL BE SEEDDED WITH CLASS 1 SEEDING. NUTRIENTS SHALL CONFORM TO ARTICLE 250.04. AREAS DISTURBED BEYOND THE CONSTRUCTION LIMITS SHALL BE SEEDDED BY THE CONTRACTOR AT HIS/HER OWN EXPENSE.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL COORDINATE WITH RESIDENTS PRIOR TO REMOVAL AND REPLACEMENT OF DRIVEWAYS.
- PROTECTIVE COAT SHALL BE APPLIED TO ALL GUTTER FLAGS AND FACE OF CURBS.
- THE EXISTING PROPERTY LINES SHOWN HEREON HAS BEEN PROTRACTED FROM EXISTING RECORDS AND IS TO BE USED FOR REFERENCE PURPOSES ONLY. FURTHERMORE, NO COMPLETE SURVEY OF SAID PROPERTY LINES IS IMPLIED BY THESE DRAWINGS.
- AN ESTIMATED QUANTITY OF 150 TONS OF AGGREGATE FOR TEMPORARY ACCESS HAS BEEN INCLUDED IN THE PLANS FOR THE PURPOSE OF MAINTAINING ACCESS TO PRIVATE PROPERTY AND LOCAL TRAFFIC THROUGHOUT CONSTRUCTION OPERATIONS.
- FACTORS USED FOR APPLICATION RATES AND QUANTITY CALCULATIONS ARE AS FOLLOWS:
PAVING
 ALL HOT-MIX ASPHALT 0.056 TONS/SQ. YD./INCH
 BITUMINOUS MATERIALS (PRIME COAT) 0.075 GAL./SQ. YD.
 AGGREGATE (PRIME COAT) 0.0015 TONS/SQ. YD.
 AGGREGATE BASE COURSE 0.0555 TONS/SQ. YD./INCH
SEEDING
 NITROGEN FERTILIZER NUTRIENT 90 LBS/ACRE
 PHOSPHORUS FERTILIZER NUTRIENT 90 LBS/ACRE
 POTASSIUM FERTILIZER NUTRIENT 90 LBS/ACRE
 MULCH 2 TONS/ACRE
LIME MODIFIED SOILS
 SOIL 1.4445 TONS/CU. YD.
 LIME (4% OF SOIL) 0.01926 TONS/SQ. YD./12 IN.
 WATER 15 GAL./SQ. YD./12 IN.
- CHANGES IN THE VERTICAL OR HORIZONTAL DIMENSIONS OF GUTTERS, CURBS, OR CURB AND GUTTERS, WHEN TRANSITIONS FROM ONE TYPE TO ANOTHER AS SHOWN IN THE PLANS, WILL BE PAID FOR AT THE CONTRACT PRICE PER FOOT FOR THE TYPE SPECIFIED IN THE PLAN SCHEDULE.
- THE REMOVAL OF ANY CURB AND GUTTER SHALL ALSO INCLUDE THE DISCONNECTION OF THE EXISTING CURB AND GUTTER AND GUTTER FROM ANY REINFORCEMENT OR COLLATERAL MATERIAL THAT MAY BE PRESENTLY ATTACHED. THERE SHALL NOT BE ANY ADDITIONAL REMUNERATION BUT THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE REMOVAL ITEM.
- IF ASH TREES ARE REMOVED ON THE PROJECT, THE CONTRACTOR SHALL BECOME FAMILIAR WITH AND COMPLY WITH MEASURES SPECIFIED BY THE ILLINOIS DEPARTMENT OF AGRICULTURE (IDOA) TO PREVENT THE SPREAD OF THE EMERALD ASH BORER. THE IDOA INFORMATION FOR ASH TREE REMOVAL CAN BE FOUND ON THE IDOA WEBSITE AT WWW.AGR.STATE.ILL.US/EAB.
- THE TOP 6 INCHES OF ALL NECESSARY BACKFILL SHALL BE FREE FROM ALL ROCKS OR FOREIGN DEBRIS.
- ALL PIPE DRAINS/ROOF DRAINS, BOTH EXPOSED AND UNEXPOSED, THAT DISCHARGE OVER TOP THE EXISTING CURB SHALL BE DISCHARGED INTO THE NEAREST INLET OR STORM SEWER PIPE. THE PIPE SHALL BE INSTALLED AT A MINIMUM GRADIENT OF 0.50%. IN AREAS WHERE NO STORM SEWER IS PRESENT PIPE DRAINS/ROOF DRAINS SHALL BE PLACED UNDER PROPOSED SIDEWALK AND DAYLIGHTED. ALL EQUIPMENT, MATERIALS, AND LABOR REQUIRED TO COMPLETE THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR DRAIN CONNECTIONS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- LANDSCAPING ITEMS WITHIN THE LIMITS OF CONSTRUCTION WHICH MUST BE REMOVED SHALL BE SALVAGED FOR RE-USE OR DISPOSAL AS DIRECTED BY THE ENGINEER. REMOVAL AND/OR SALVAGE OF LANDSCAPING ITEMS SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- BRICK PAVERS ALONG SIDEWALKS, INDIVIDUAL WALKWAYS AND DRIVEWAYS, SHALL BE RETAINED OR REPLACED IN KIND TO MATCH IN COLOR, SIZE AND HERRINGBONE PATTERN AT INTERSECTIONS WHERE BRICK REPLACEMENT IS NECESSARY TO ENSURE PEDESTRIAN ACCESSIBILITY. STAMPED CONCRETE OR PAVERS MAY BE SUBSTITUTED AS LONG AS THE SUBSTITUTE MATCHES THE ADJACENT BRICK EXACTLY IN PATTERN, COLOR AND DIMENSIONS.

COMMITMENTS

AT THE REQUEST OF THE ILLINOIS HISTORICAL PRESERVATION AGENCY THE CITY OF LEBANON HAS COMMITTED TO REPLACE CURB RAMP WITH STAMPED COLORED CONCRETE OR PAVING BRICKS TO MATCH EXISTING BRICK SIDEWALK WHICH ADJUTS THE RAMPS. ALSO, AT IHPA'S REQUEST ALL BRICKS REMOVED FROM THE PROJECT FOR ANY REASON SHALL BE SALVAGED FOR FUTURE USE IN REPAIR OF EXISTING SIDEWALKS.

| MIXTURE (LBS/SQ. YD.) | SURFACE COURSE | BINDER COURSE | INCIDENTAL |
|---------------------------------------|-----------------|-----------------|-----------------|
| PG. 34-22 | PG. 34-22 | PG. 64-22 | PG. 64-22 |
| DESIGN AIR VELOCITY | 4.0K @ Nodes=70 | 4.0K @ Nodes=70 | 4.0K @ Nodes=70 |
| MAX. COMPOSITION (SEPARATION MIXTURE) | IL-19.0 | | |
| FUNCTION APP. | MIXTURE D | MIXTURE B | MIXTURE D |

HORIZONTAL CONTROL TIES



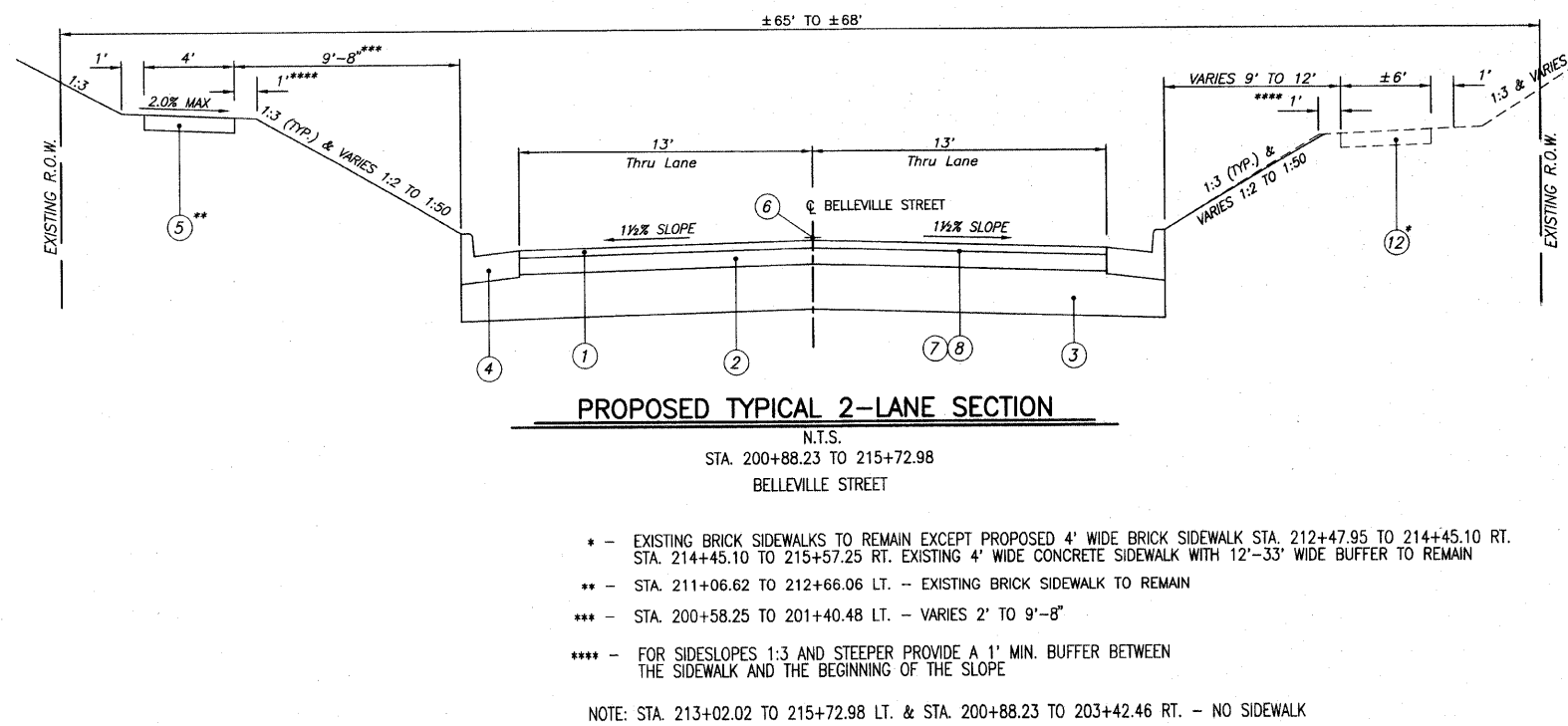
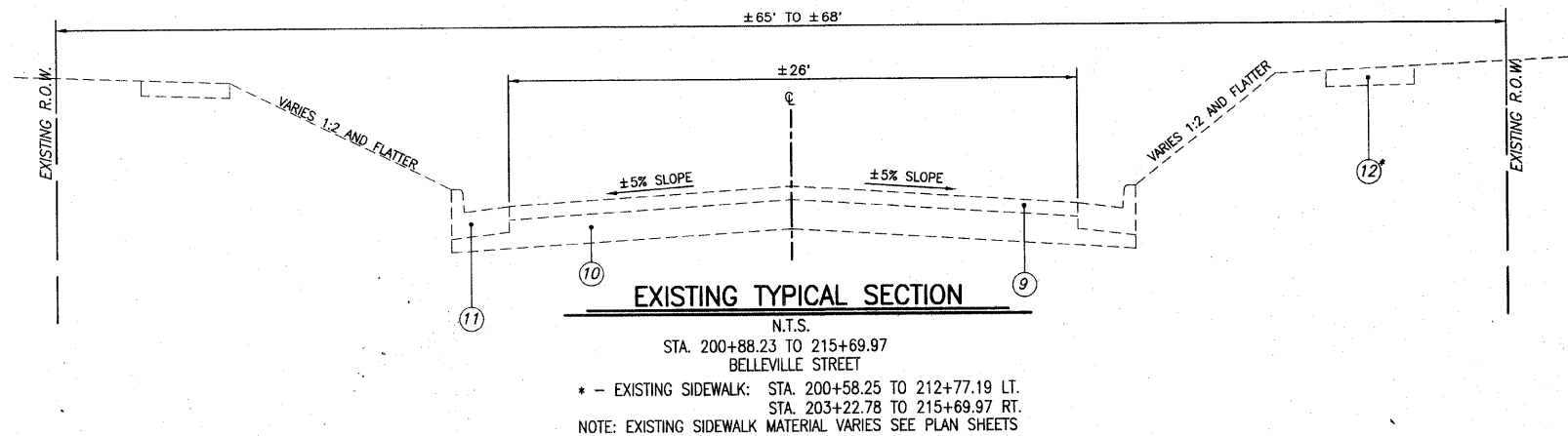
| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|----------------|--------------|--------------|
| 9247 | 08-00018-00-PV | ST. CLAIR | 19 | 3 |
| STA 200+88.23 TO | | STA. 215+72.98 | | |
| FEDERAL AID PROJECT | | CONTRACT 97454 | | |

LEGEND

- ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ② HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 4 1/4"
- ③ LIME MODIFIED SOIL, 12"
- ④ COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- ⑤ P.C.C. SIDEWALK 4"
- ⑥ THERMOPLASTIC PAVEMENT MARKING
- ⑦ BITUMINOUS MATERIALS (PRIME COAT)
- ⑧ AGGREGATE (PRIME COAT)
- ⑨ EXISTING OIL AND CHIP PAVEMENT, VARIES 1" TO 2"
- ⑩ EXISTING AGGREGATE BASE, VARIES 4" TO 10"
- ⑪ EXISTING CURB AND GUTTER
- ⑫ EXISTING SIDEWALK

PAVEMENT DESIGN

| | | | | | |
|--|-----------------|-------------|-------------|-------|----|
| STRUCTURAL DESIGN TRAFFIC: | YEAR | 2020 | | | |
| PV= | 6,536 | SU= | 131 | MU= | 65 |
| ROAD/STREET CLASSIFICATION: | URBAN COLLECTOR | CLASS ROAD: | II | | |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | | | | | |
| P= | 50 | S= | 50 | M= | 50 |
| TRAFFIC FACTOR: | Actual TF= | 0.33 | Minimum TF= | N/A | |
| PG GRADE: | Binder= | 64-22 | Surface= | 64-22 | |
| THICKNESS | - 6 1/4" | | | | |
| SUBGRADE SUPPORT RATING: | POOR | | | | |



COMBINATION CURB & GUTTER REMOVAL

| LOCATION | LENGTH |
|--|-------------|
| | FOOT |
| STA 200+88.23 TO 206+07.01, LT. | 526 |
| STA 201+37.96 TO 209+17.95, RT. | 789 |
| STA 206+42.16 TO 209+48.68, LT. | 334 |
| STA 209+47.89 TO 215+83.18, RT. | 677 |
| STA 209+83.62 TO 212+71.37, LT. | 307 |
| STA 212+79.04 TO 212+89.28, LT. | 20 |
| STA 213+31.89 LT. AROUND RADIUS TO 103+20.02 RT. | 339 |
| TOTAL | 2992 |

STUMP REMOVAL SCHEDULE

| LOCATION | STUMP REMOVAL ONLY |
|----------------------|--------------------|
| | UNIT |
| STA. 201+55, 21' LT. | 18 |
| STA. 207+15, 23' RT. | 18 |
| STA. 207+17, 21' LT. | 20 |
| STA. 207+37, 20' LT. | 18 |
| TOTAL | 74 |

DRAINAGE REMOVAL SCHEDULE

| LOCATION | DESCRIPTION | PIPE CULVERT REMOVAL | REMOVING INLETS |
|----------------------|-------------|----------------------|-----------------|
| | | FOOT | EACH |
| STA. 201+29, 15' RT. | 8" CLAY | 16 | — |
| STA. 209+34, 14' LT. | — | — | 1 |
| STA. 209+45, 23' LT. | 12" CLAY | 25 | — |
| STA. 212+55, 16' RT. | — | — | 1* |
| STA. 212+64, C | 12" CLAY | 32 | — |
| STA. 212+71, 13' LT. | — | — | 1 |
| STA. 212+74, 17' LT. | 12" CLAY | 3** | — |
| STA. 212+99, 13' LT. | 12" CLAY | 51 | — |
| STA. 213+24, 14' LT. | — | — | 1 |
| STA. 215+68, C | 8" CLAY | 102 | — |
| TOTAL | | 229 | 4 |

*ADJACENT BALLARD TO BE REMOVED. INCLUDED IN COST OF INLET REMOVAL.

**PIPE TO BE CUT OFF AT INSIDE FACE OF PROPOSED INLET, COST TO BE INCLUDED IN REMOVING INLETS.

EARTHWORK SCHEDULE

| LOCATION | EARTH EXCAVATION | EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (25%) | ROADBED EMBANKMENT | EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) | TOPSOIL EMBANKMENT | SPOILS FROM STORM* SEWER CONSTRUCTION | TOPSOIL FURNISHED & PLACED, SPECIAL |
|------------------------------------|------------------|---|--------------------|---|--------------------|---------------------------------------|-------------------------------------|
| | CU YD | CU YD | CU YD | CU YD | CU YD | CU YD | CU YD |
| STA. 200+88.23 TO STA. 215+72.98 C | 501 | 125 | 341 | +35 | 248 | 178 | 70 |
| TOTAL | 501 | 125 | 341 | +35 | 248 | 178 | 70 |

* - ESTIMATED AVERAGE EXCAVATION OF 3'-4". SPOILS TO BE USED IN BACKFILL SHALL BE APPROVED BY THE ENGINEER.

ENTRANCE / SIDEROAD SCHEDULE

| STATION | EXISTING SURFACE | TYPE | "L" | "W" | DRIVEWAY PVM/T REMOVAL | INCIDENTAL HOT-MIX ASPHALT SURFACING | AGG. BASE COURSE, TY A, 8" | PCC DRIVEWAY PAVEMENT, 6" | COMMENTS |
|----------------|------------------|------|------|------|------------------------|--------------------------------------|----------------------------|---------------------------|-----------------------------|
| | | | FOOT | FOOT | SQ. YD. | TON | TON | SQ. YD. | |
| 201+29.71, RT. | ASPHALT | P.E. | 16.0 | 12.0 | 26.8 | 4.0 | 10.7 | — | |
| 202+76.37, RT. | ASPHALT | P.E. | 6.1 | 12.0 | 9.8 | 2.0 | 5.4 | — | |
| 203+88.91, LT. | AGGREGATE | P.E. | 28.1 | 12.0 | — | — | 1.4 | 22.2 | |
| 204+12.54, LT. | AGG./ASPHALT | P.E. | 16.2 | 12.0 | 13.2 | 0.5 | 1.4 | 22.2 | |
| 205+82.90, RT. | TURF | S.R. | 14.3 | 12.0 | 13.1 | — | — | — | ALLEY |
| 206+44.13, LT. | OIL & CHIP | S.R. | 31.5 | 29.5 | — | 8.1 | 34.1 | — | STANTON STREET |
| 208+45.90, LT. | ASPHALT | P.E. | 15.9 | 12.0 | 24.4 | 0.4 | 1.1 | 22.1 | |
| 209+33.02, RT. | OIL & CHIP | S.R. | 28.2 | 30.0 | — | 14.0 | 30.7 | — | CHAMBERLAIN STREET |
| 209+58.40, LT. | OIL & CHIP | S.R. | 28.1 | 30.0 | — | 12.9 | 27.8 | — | CHAMBERLAIN STREET |
| 210+18.88, RT. | CONCRETE | P.E. | 10.0 | 12.0 | 14.3 | — | — | 17.5 | 2% MAX 25.58' TO 31.58' RT. |
| 211+12.35, RT. | TURF | P.E. | 10.0 | 12.0 | — | — | — | 17.9 | |
| 212+26.20, RT. | AGGREGATE | P.E. | 17.2 | 12.0 | — | — | — | 28.3 | |
| 212+42.74, RT. | AGGREGATE | S.R. | 17.2 | 12.0 | — | — | — | 28.3 | ALLEY |
| 213+01.13, LT. | OIL & CHIP | S.R. | 16.7 | 28.5 | — | 17.1 | 37.9 | — | ALTON STREET |
| 213+62.80, RT. | ASPHALT | P.E. | 12.0 | 12.0 | 10.3 | — | — | 20.3 | |
| 213+92.60, RT. | ASPHALT | P.E. | 12.0 | 13.0 | 13.1 | — | — | 21.2 | 2% MAX 25.58' TO 31.58' RT. |
| 104+38.01, LT. | OIL & CHIP | S.R. | 10.7 | 14.7 | — | 13.3 | 35.2 | — | HORNER STREET |
| TOTAL | | | | | 125.0 | 72.3 | 185.7 | 200.0 | |

NOTE: PLEASE REFER TO ENTRANCE DETAILS ON SHEET 11 TO SEE APPLICATION OF THE DIMENSIONS.

P.E. - PRIVATE ENTRANCE

S.R. - SIDEROAD ("L" IS MEASURED FROM EDGE OF PAVEMENT ALONG CENTERLINE TO LIMIT OF CONSTRUCTION)

SIDEWALK REMOVAL

| LOCATION | SIDEWALK REMOVAL | SIDEWALK REMOVAL (SPECIAL) |
|----------------------------------|------------------|----------------------------|
| | SQ FT | SQ FT |
| STA. 200+62.27 TO 203+82.27, LT. | 1069 | |
| STA. 203+95.31 TO 204+18.07, LT. | 107 | |
| STA. 205+19.48 TO 206+05.74, LT. | 316 | |
| STA. 206+39.50 TO 208+37.40, LT. | 873 | |
| STA. 208+54.67 TO 209+51.13, LT. | 427 | |
| STA. 208+92.59 TO 209+17.37, RT. | | 108 |
| STA. 209+48.40 TO 209+56.07, RT. | 44 | |
| STA. 209+81.13 TO 211+06.62, LT. | 554 | |
| STA. 212+47.59 TO 214+45.10, RT. | | 790 |
| STA. 212+69.07 TO 212+83.46, LT. | 136 | |
| STA. 212+73.78 TO 212+77.78, RT. | 38 | |
| STA. 215+58.89 TO 215+82.91, RT. | 120 | |
| TOTAL | 3684 | 898 |

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STORM SEWER & DRAINAGE STRUCTURE SCHEDULE

| PT | LOCATION STATION / OFFSET | DESCRIPTION | BACK OF CURB ELEVATION | FLOWLINE | | | |
|----|---------------------------|---|------------------------|----------|--------|--------|--------|
| | | | | N | S | E | W |
| 1 | 209+14.01, 21.85' RT. | INLETS, TYPE A, TYPE 15 F & L | 507.89 | 504.56 | | | |
| 2 | 209+29.55, 16.48' LT. | * INLETS, TYPE B, TYPE 15 F & L | 507.47 | | 504.16 | 503.96 | |
| 3 | 210+70.00, 16.00' LT. | INLETS, TYPE A, TYPE 15 F & L | 504.26 | | | 500.48 | 500.68 |
| 4 | 212+12.67, 16.00' RT. | INLETS, TYPE A, TYPE 15 F & L | 502.67 | 500.08 | | | |
| 5 | 212+17.88, 16.48' LT. | * INLETS, TYPE B, TYPE 15 F & L | 502.67 | | 499.76 | 499.76 | 499.76 |
| 6 | 212+71.90, 13.53' LT. | * MANHOLES, TYPE A, 4' DIA., TYPE 1 FRAME, CLOSED LID | 502.38 (1) | 499.60 | | 499.80 | 499.60 |
| 7 | 213+45.25, 15.50' RT. | * INLETS, TYPE B, TYPE 15 F & L | 503.83 | 500.71 | | 500.91 | |
| 8 | 213+45.25, 16.48' LT. | * INLETS, TYPE B, TYPE 15 F & L | 503.83 | | 500.42 | | 500.32 |
| 9 | 215+00.00, 16.00' RT. | INLETS, TYPE A, TYPE 15 F & L | 506.35 | | | 503.70 | 503.50 |
| 10 | 104+51.56, 21.14' RT. | EXISTING INLET TO BE RE-CONSTRUCTED | | | | | 507.16 |

(*) - DENOTES FLAT SLAB TOP
(1) - ELEVATION FOR TOP OF LID

NOTE: OFFSETS TO ALL INLETS ARE TO THE CENTER OF THE DRAINAGE STRUCTURE.

STORM SEWER SCHEDULE

| No. | STRUCTURE PT. - PT. | RUBBER GASKET, CLASS A TYPE 1 1/2" (FOOT) | WATER MAIN REQUIREMENTS 12" (FOOT) | TRENCH BACKFILL (CU YD) |
|-------|---------------------|---|------------------------------------|-------------------------|
| 1 | 1-2 | 39 | | 7 |
| 2 | 2-3 | 138 | | 24 |
| 3 | 3-5 | 145 | | 19 |
| 4 | 4-5 | | 32 | 4 |
| 5 | 5-6 | 53 | | 7 |
| 6 | 8-6 | | 72 | 9 |
| 7 | 7-8 | 29 | | 5 |
| 8 | 9-7 | 155 | | 20 |
| 9 | 10-9 | 129 | | 6* |
| TOTAL | | 688 | 104 | 100 |

* TRENCH BACKFILL TO BE USED WHEN PIPE IS WITHIN 2 FEET OF PAVEMENT OR CURB AND GUTTER.

MISCELLANEOUS REMOVAL SCHEDULE

| LOCATION | CONCRETE STEP REMOVAL EACH | HANDRAIL REMOVAL* EACH |
|-------------|----------------------------|------------------------|
| 202+57, RT. | 1 | |
| 202+78, LT. | 1 | 1 |
| 203+48, RT. | 1 | |
| 204+47, RT. | 2 | |
| 205+44, RT. | 1 | 2 |
| 206+41, RT. | 1 | |
| 207+26, RT. | 1 | |
| TOTAL | 8 | 3 |

* TO BE SALVAGED FOR RE-USE OR DISPOSED OF AS DIRECTED BY ENGINEER.

MANHOLE AND INLET SCHEDULE

| PT | LOCATION STATION / OFFSET | INLETS TYPE A, TYPE 15 F & G | INLETS TYPE B, TYPE 15 F & G | MANHOLES, TYPE A 4' DIAMETER TYPE 1 FRAME, CLOSED LID | INLETS TO BE RECONSTRUCTED |
|-------|---------------------------|------------------------------|------------------------------|---|----------------------------|
| | | EACH | EACH | EACH | EACH |
| 1 | 209+14.01, 21.85' RT. | 1 | | | |
| 2 | 209+29.55, 16.48' LT. | | 1 | | |
| 3 | 210+70.00, 16.00' LT. | 1 | | | |
| 4 | 212+12.67, 16.00' RT. | 1 | | | |
| 5 | 212+17.88, 16.48' LT. | | 1 | | |
| 6 | 212+71.90, 13.53' LT. | | | 1 | |
| 7 | 213+45.25, 15.50' RT. | | 1 | | |
| 8 | 213+45.25, 16.48' LT. | | 1 | | |
| 9 | 215+00.00, 16.00' RT. | 1 | | | |
| 10 | 104+51.56, 21.14' RT. | | | | 1 |
| TOTAL | | 4 | 4 | 1 | 1 |

SEEDING / EROSION CONTROL SCHEDULE

| LOCATION | SEEDING CLASS 1 | MULCH METHOD 2 | FERTILIZER NUTRIENTS | | | TEMPORARY EROSION CONTROL SEEDING | EROSION BLANKET* | INLET AND PIPE PROTECTION |
|----------------------------------|-----------------|----------------|----------------------|------------------|-----------------|-----------------------------------|------------------|---------------------------|
| | ACRE | ACRE | NITROGEN POUND | PHOSPHORUS POUND | POTASSIUM POUND | POUND | SQ. YD. | EACH |
| STA. 200+88.23 TO STA. 215+72.98 | 0.70 | 0.70 | 63 | 63 | 63 | 65 | 758 | 9 |
| TOTAL | 0.70 | 0.70 | 63 | 63 | 63 | 65 | 758 | 9 |

* SEE PLAN SHEET FOR LIMITS

THERMOPLASTIC PAVEMENT MARKING SCHEDULE

| LOCATION | DOUBLE YELLOW LINE - 4" | SOLID WHITE BAR LINE - 24" | SOLID WHITE LINE - 6" | COMMENTS |
|----------------------------------|-------------------------|----------------------------|-----------------------|----------------------|
| | FOOT | FOOT | FOOT | |
| STA. 200+88.23 TO 212+73.06, C | 2370 | | | DOUBLE YELLOW C |
| STA. 206+09.14 TO 206+40.37, LT. | | | 58 | CROSSWALK LINES |
| STA. 209+18.63 TO 209+46.75, RT. | | | 55 | CROSSWALK LINES |
| STA. 209+53.27 TO 209+87.33, LT. | | | 61 | CROSSWALK LINES |
| STA. 212+73.06 TO 212+79.06, C | | 30 | 57 | CROSSWALK LINES/BARS |
| STA. 212+79.06 TO 215+50.00, C | 542 | | | DOUBLE YELLOW C |
| STA. 215+50.00, RT. | | 13 | | STOP BAR |
| STA. 104+27.74 TO 104+49.51, LT. | | | 38 | CROSSWALK LINES |
| TOTAL | 2912 | 43 | 269 | |

PAVEMENT REMOVAL SCHEDULE

| LOCATION | PAVEMENT REMOVAL SQ. YD. | HMA SURFACE REMOVAL BUTT-JOINT SQ. YD. |
|----------------------------------|--------------------------|--|
| STA. 200+88.23 TO 215+72.98, C | 4861 | |
| STA. 206+12.22 TO 206+40.90, LT. | | 28 |
| STA. 209+19.39 TO 209+46.32, RT. | | 28 |
| STA. 209+56.01 TO 209+86.15, LT. | | 27 |
| STA. 212+90.46 TO 213+27.50, LT. | | 34 |
| TOTAL | 4861 | 117 |

NOTE: SAW CUTS TO BE MADE AT LIMITS OF CONSTRUCTION TO PROVIDE SMOOTH TRANSITION FROM EXISTING TO PROPOSED PAVEMENT.

SIDEWALK SCHEDULE

| LOCATION | P.C.C. SIDEWALK, 4" | PAVING BRICK SIDEWALK | DETECTABLE WARNINGS |
|------------------------------------|---------------------|-----------------------|---------------------|
| | SQ FT | SQ YD | SQ FT |
| STA. 200+62.27 TO 203+82.91, LT. | 1302 | | |
| STA. 203+94.92 TO 204+06.54, LT. | 47 | | |
| STA. 205+19.37 TO 206+09.17, LT. | 356 | | |
| STA. 206+05.33, LT. | | | 8 |
| STA. 206+41.94 TO 208+40.25, LT. | 814 | | |
| STA. 206+43.83, LT. | | | 8 |
| STA. 208+51.23 TO 209+53.23, LT. | 478 | | |
| * STA. 208+92.59 TO 209+17.37, RT. | | 13 | |
| * STA. 209+48.40 TO 209+56.07, RT. | | 5 | |
| STA. 209+49.34, LT. | | | 8 |
| STA. 209+86.66 TO 211+06.62, LT. | 516 | | |
| STA. 209+87.03, LT. | | | 8 |
| STA. 212+47.59 TO 214+45.10, RT. | | 88 | |
| STA. 212+69.07 TO 212+83.46, LT. | 110 | | |
| STA. 212+75.88, LT. | | | 8 |
| STA. 212+76.09, RT. | | | 8 |
| * STA. 212+73.03 TO 212+79.06, RT. | | 8 | |
| STA. 215+57.25 TO 215+87.90, RT. | 170 | | |
| STA. 215+87.90, RT. | | | 8 |
| TOTAL | 3793 | 114 | 56 |

* PAVERS WITH TRUNCATED DOMES INCLUDED IN THIS QUANTITY. (SEE DETAIL)

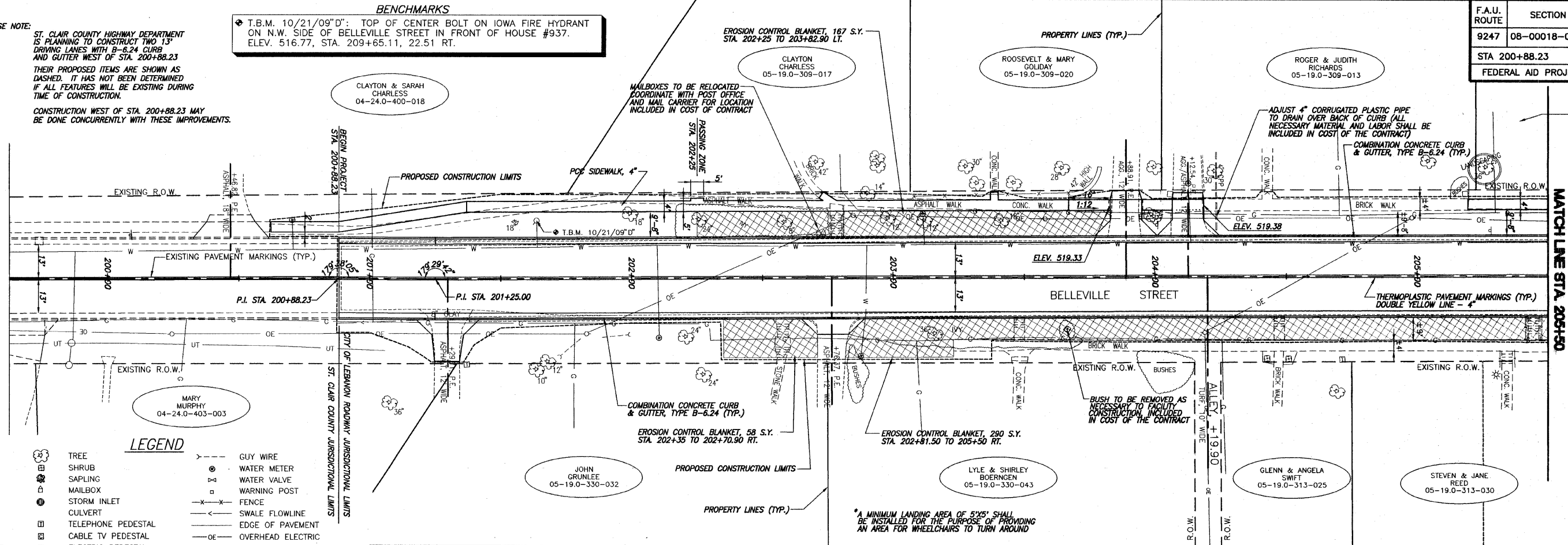
PAVING SCHEDULE

| LOCATION | PROCESSING MODIFIED SOIL, 12" | LIME | HMA BINDER CRSE. IL-19.0, N70, 4 1/4" | BITUMINOUS MATERIALS (PRIME COAT) | AGGREGATE (PRIME COAT) | HMA SURFACE CRSE., MIX "D", N70, 2" | COMB. CONC. CURB & GUTTER TY. B-6.24 | PROTECTIVE COAT |
|----------------------------------|-------------------------------|------|---------------------------------------|-----------------------------------|------------------------|-------------------------------------|--------------------------------------|-----------------|
| | SQ YD | TON | TON | GALLON | TON | TON | FOOT | SQ YD |
| STA. 200+88.23 TO STA. 215+72.98 | 5143 | 99 | 1021 | 321 | 7 | 481 | 2965 | 824 |
| TOTAL | 5143 | 99 | 1021 | 321 | 7 | 481 | 2965 | 824 |

PLEASE NOTE:
 ST. CLAIR COUNTY HIGHWAY DEPARTMENT IS PLANNING TO CONSTRUCT TWO 13" DRIVING LANES WITH B-6.24 CURB AND GUTTER WEST OF STA. 200+88.23. THEIR PROPOSED ITEMS ARE SHOWN AS DASHED. IT HAS NOT BEEN DETERMINED IF ALL FEATURES WILL BE EXISTING DURING TIME OF CONSTRUCTION.
 CONSTRUCTION WEST OF STA. 200+88.23 MAY BE DONE CONCURRENTLY WITH THESE IMPROVEMENTS.

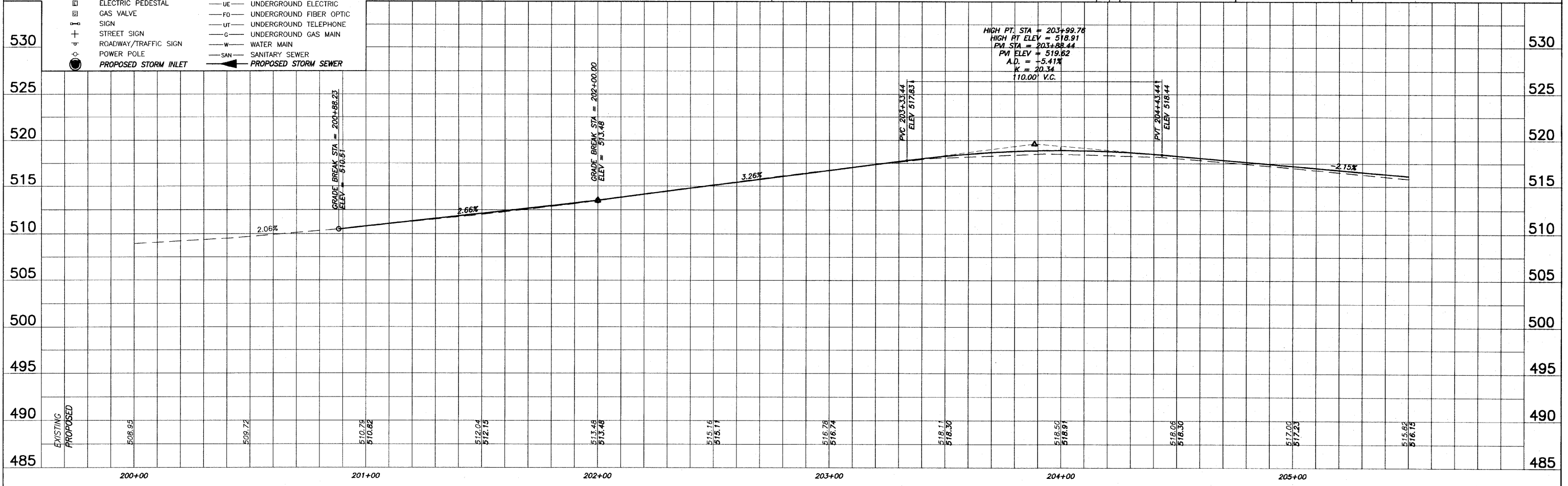
BENCHMARKS
 T.B.M. 10/21/09"D": TOP OF CENTER BOLT ON IOWA FIRE HYDRANT ON N.W. SIDE OF BELLEVILLE STREET IN FRONT OF HOUSE #937. ELEV. 516.77, STA. 209+65.11, 22.51 RT.

| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|----------------|----------------|--------------|
| 9247 | 08-00018-00-PV | ST. CLAIR | 19 | 6 |
| STA 200+88.23 | | TO | STA. 215+72.98 | |
| FEDERAL AID PROJECT | | CONTRACT 97454 | | |

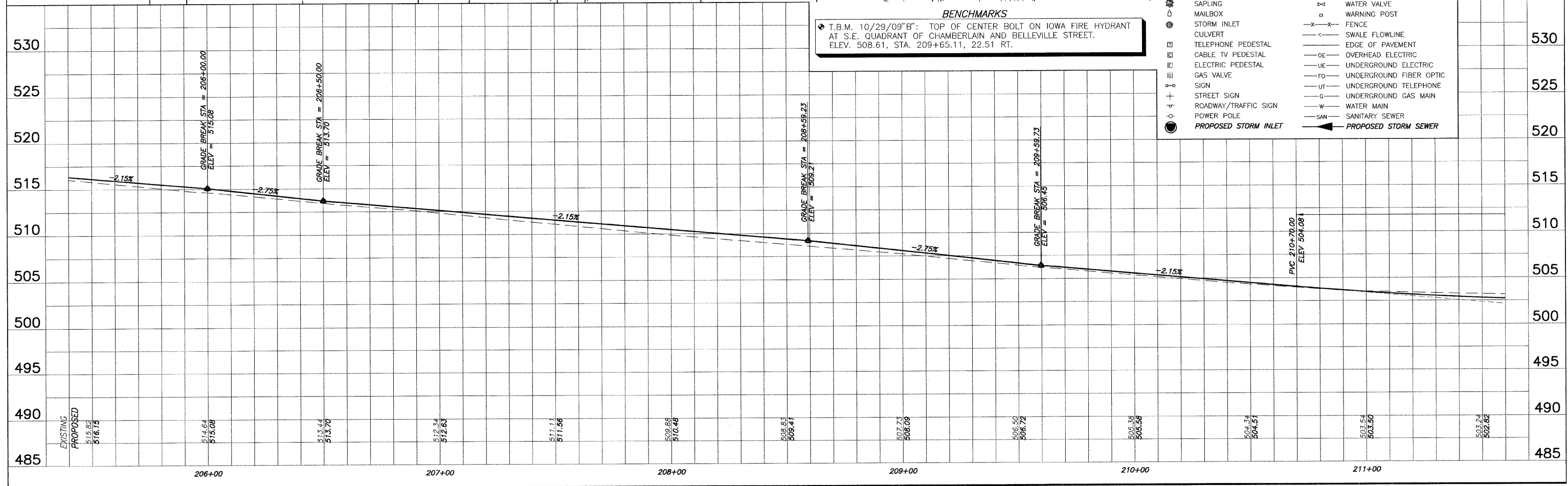
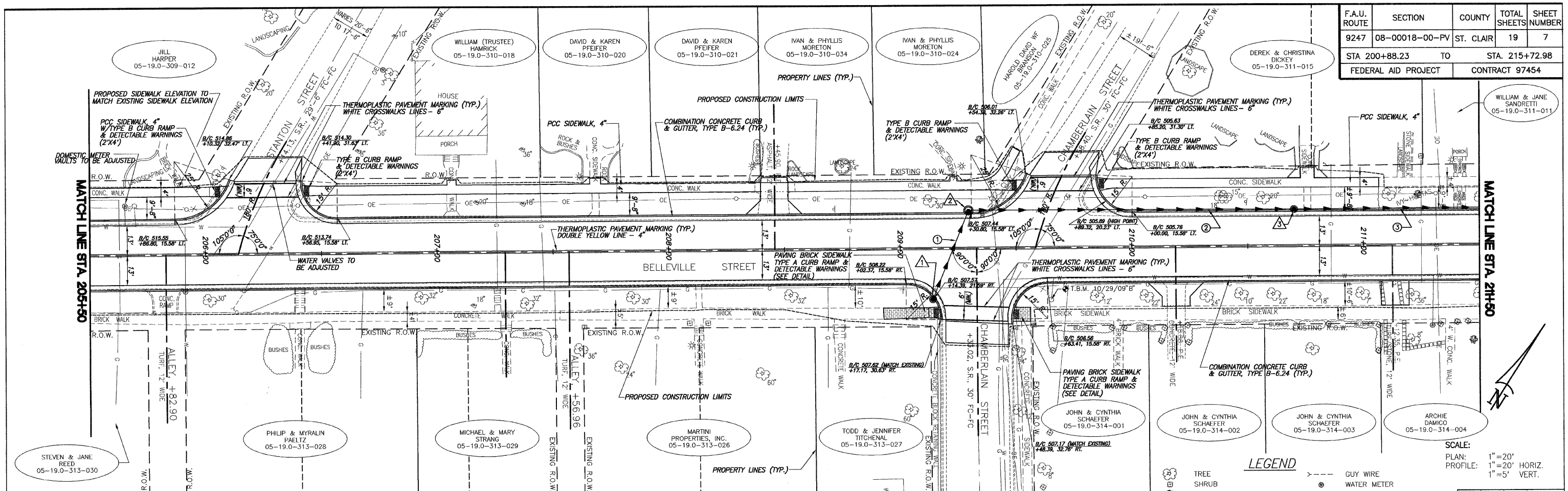


LEGEND

| | | | |
|--|----------------------|--|-------------------------|
| | TREE | | GUY WIRE |
| | SHRUB | | WATER METER |
| | SAPLING | | WATER VALVE |
| | MAILBOX | | WARNING POST |
| | STORM INLET | | FENCE |
| | CULVERT | | SWALE FLOWLINE |
| | TELEPHONE PEDESTAL | | EDGE OF PAVEMENT |
| | CABLE TV PEDESTAL | | OVERHEAD ELECTRIC |
| | ELECTRIC PEDESTAL | | UNDERGROUND ELECTRIC |
| | GAS VALVE | | UNDERGROUND FIBER OPTIC |
| | SIGN | | UNDERGROUND TELEPHONE |
| | STREET SIGN | | UNDERGROUND GAS MAIN |
| | ROADWAY/TRAFFIC SIGN | | WATER MAIN |
| | POWER POLE | | SANITARY SEWER |
| | PROPOSED STORM INLET | | PROPOSED STORM SEWER |



| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------------------|----------------|------------------------------------|--------------|--------------|
| 9247 | 08-00018-00-PV | ST. CLAIR | 19 | 7 |
| STA 200+88.23 TO STA. 215+72.98 | | FEDERAL AID PROJECT CONTRACT 97454 | | |

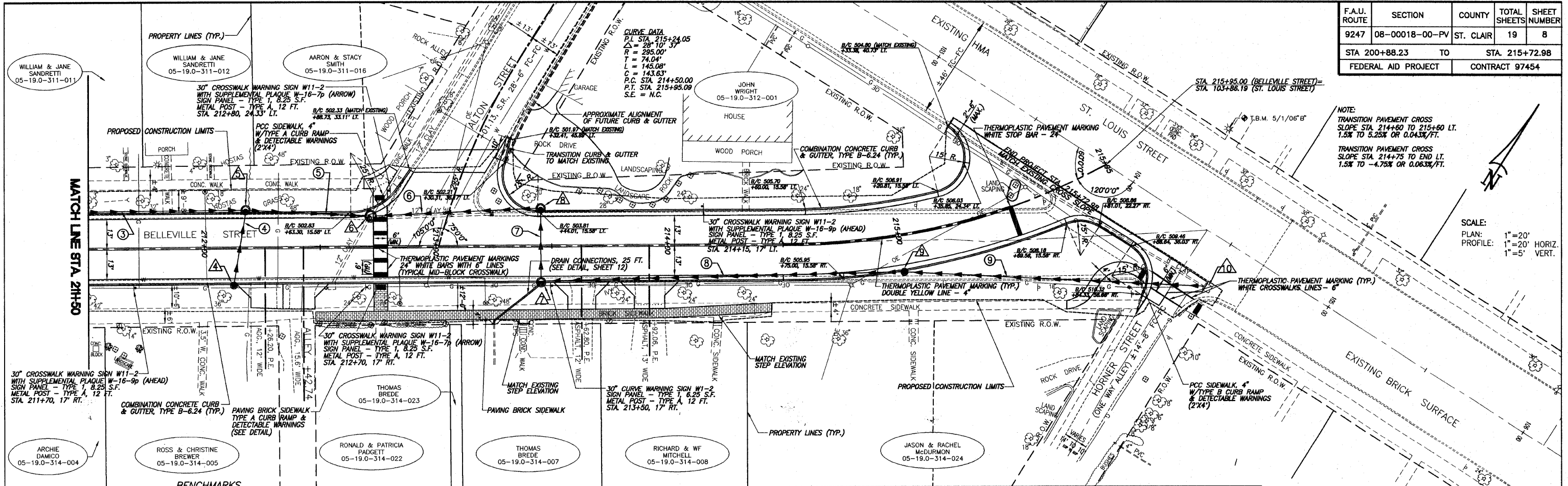


BENCHMARKS
 T.B.M. 10/29/09"B": TOP OF CENTER BOLT ON IOWA FIRE HYDRANT AT S.E. QUADRANT OF CHAMBERLAIN AND BELLEVILLE STREET. ELEV. 508.61, STA. 209+65.11, 22.51 RT.

- LEGEND**
- TREE
 - SHRUB
 - SAPLING
 - MAILBOX
 - STORM INLET
 - CULVERT
 - TELEPHONE PEDESTAL
 - CABLE TV PEDESTAL
 - ELECTRIC PEDESTAL
 - GAS VALVE
 - SIGN
 - STREET SIGN
 - ROADWAY/TRAFFIC SIGN
 - POWER POLE
 - PROPOSED STORM INLET
 - GUY WIRE
 - WATER METER
 - WATER VALVE
 - WARNING POST
 - FENCE
 - SWALE FLOWLINE
 - EDGE OF PAVEMENT
 - OVERHEAD ELECTRIC
 - UNDERGROUND ELECTRIC
 - UNDERGROUND FIBER OPTIC
 - UNDERGROUND TELEPHONE
 - UNDERGROUND GAS MAIN
 - WATER MAIN
 - SANITARY SEWER
 - PROPOSED STORM SEWER

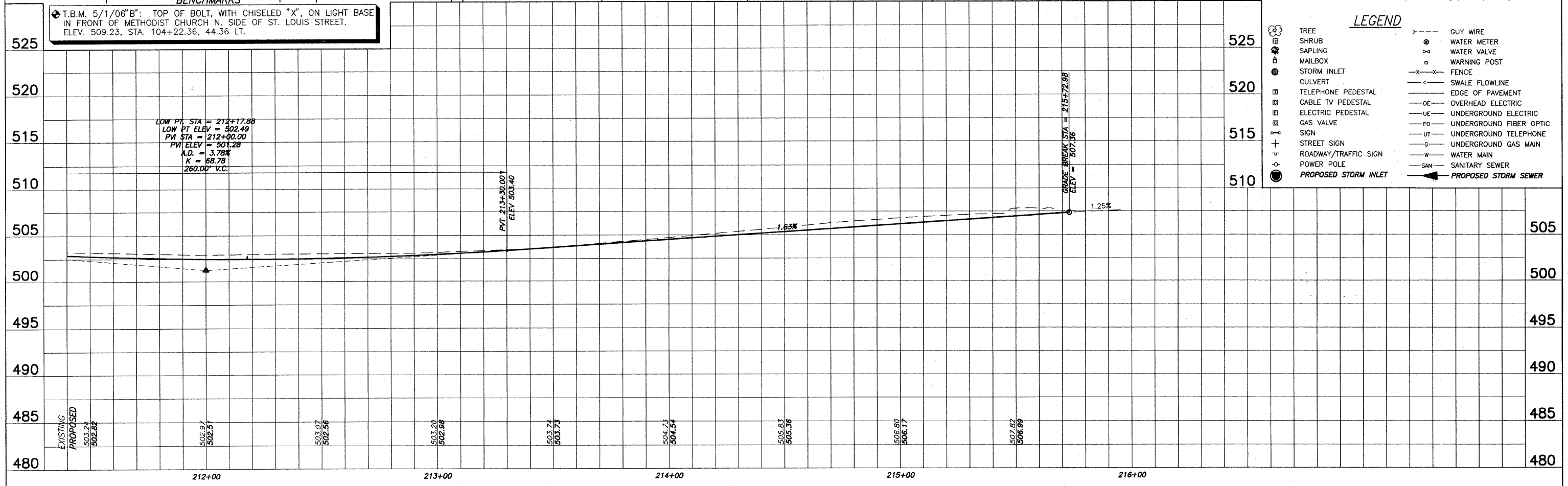
SCALE:
 PLAN: 1" = 20'
 PROFILE: 1" = 20' HORIZ.
 1" = 5' VERT.

| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|----------------|--------------|----------------|
| 9247 | 08-00018-00-PV | ST. CLAIR | 19 | 8 |
| STA 200+88.23 | | TO | | STA. 215+72.98 |
| FEDERAL AID PROJECT | | CONTRACT 97454 | | |



NOTE:
 TRANSITION PAVEMENT CROSS SLOPE STA. 214+60 TO 215+60 LT. 1.5% TO 5.25% OR 0.043%/FT.
 TRANSITION PAVEMENT CROSS SLOPE STA. 214+75 TO END LT. 1.5% TO -4.75% OR 0.063%/FT.

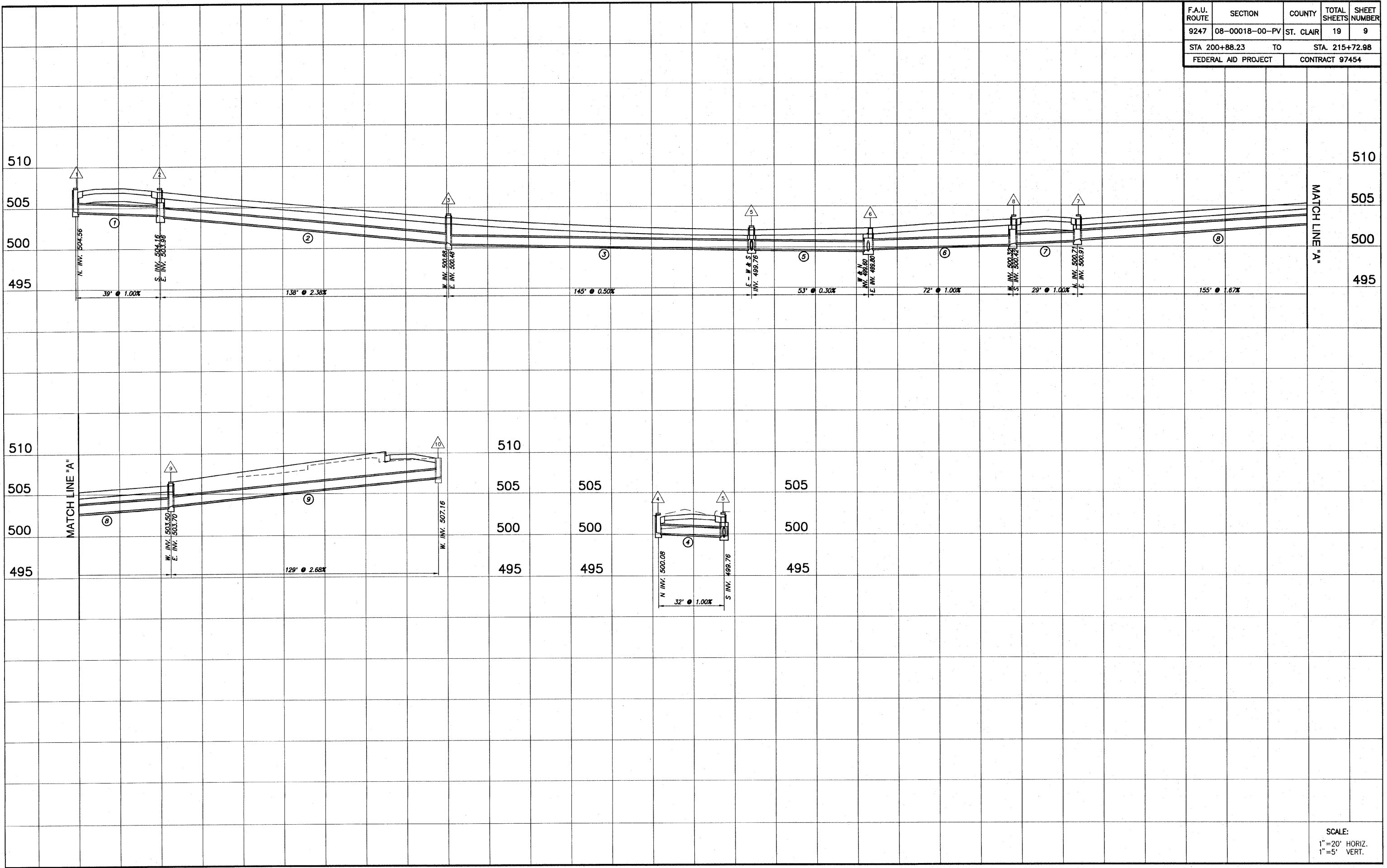
SCALE:
 PLAN: 1"=20'
 PROFILE: 1"=20' HORIZ.
 1"=5' VERT.



LEGEND

| | | | |
|-------------------------------|----------------------|---------------------------------|-------------------------|
| (Tree symbol) | TREE | (Dashed line symbol) | GUY WIRE |
| (Shrub symbol) | SHRUB | (Circle with dot symbol) | WATER METER |
| (Sapling symbol) | SAPLING | (Circle with cross symbol) | WATER VALVE |
| (Mailbox symbol) | MAILBOX | (Square with dot symbol) | WARNING POST |
| (Storm inlet symbol) | STORM INLET | (X-X symbol) | FENCE |
| (Culvert symbol) | CULVERT | (Dashed line with arrow symbol) | SWALE FLOWLINE |
| (Telephone pedestal symbol) | TELEPHONE PEDESTAL | (Solid line symbol) | EDGE OF PAVEMENT |
| (Cable TV pedestal symbol) | CABLE TV PEDESTAL | (Line with 'OE' symbol) | OVERHEAD ELECTRIC |
| (Electric pedestal symbol) | ELECTRIC PEDESTAL | (Line with 'UE' symbol) | UNDERGROUND ELECTRIC |
| (Gas valve symbol) | GAS VALVE | (Line with 'FO' symbol) | UNDERGROUND FIBER OPTIC |
| (Sign symbol) | SIGN | (Line with 'UT' symbol) | UNDERGROUND TELEPHONE |
| (Street sign symbol) | STREET SIGN | (Line with 'G' symbol) | UNDERGROUND GAS MAIN |
| (Roadway/Traffic sign symbol) | ROADWAY/TRAFFIC SIGN | (Line with 'W' symbol) | WATER MAIN |
| (Power pole symbol) | POWER POLE | (Line with 'SAN' symbol) | SANITARY SEWER |
| (Proposed storm inlet symbol) | PROPOSED STORM INLET | (Arrow symbol) | PROPOSED STORM SEWER |

| | | | | |
|---------------------|----------------|-----------|----------------|----------------|
| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
| 9247 | 08-00018-00-PV | ST. CLAIR | 19 | 9 |
| STA 200+88.23 | | TO | | STA. 215+72.98 |
| FEDERAL AID PROJECT | | | CONTRACT 97454 | |



SCALE:
 1" = 20' HORIZ.
 1" = 5' VERT.

| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------------------|----------------|-----------|----------------|--------------|
| 9247 | 08-00018-00-PV | ST. CLAIR | 19 | 10 |
| STA 200+88.23 TO STA. 215+72.98 | | | | |
| FEDERAL AID PROJECT | | | CONTRACT 97454 | |

MAINTENANCE OF TRAFFIC & GENERAL NOTES

GENERAL
 THE CONTRACTOR SHALL USE TYPE III BARRICADES WITH FLASHING LIGHTS AND SIGNS "ROAD CLOSED" (R11-2) AT ALL CLOSURE POINTS. "ROAD CONSTRUCTION AHEAD" (W20-1(O)) AND "ROAD CLOSED AHEAD" (W20-3(O)) WARNING SIGNS WITH FLASHING LIGHTS SHALL BE PLACED IN PERFORMANCE WITH STANDARD BLR 22-6 AND AS DIRECTED BY THE ENGINEER TO ACCOMMODATE FIELD CONDITIONS. INITIAL WARNING SIGNS ON IL. ROUTE 4 AND US ROUTE 50 SHALL HAVE FLASHING LIGHTS.

IT IS ANTICIPATED THAT CONSTRUCTION ON THE ADJACENT SEGMENT OF BELLEVILLE STREET BETWEEN US ROUTE 50 AND STA. 200+88.23 WILL BE TAKING PLACE CONCURRENTLY WITH THIS PROJECT. THE CONTRACTOR SHALL COORDINATE WITH THE CONTRACTOR OF THAT PROJECT TO ENSURE ALL PROPERTIES ARE ACCESSIBLE ALONG THE ENTIRE LENGTH OF BELLEVILLE STREET FOR THE DURATION OF CONSTRUCTION.

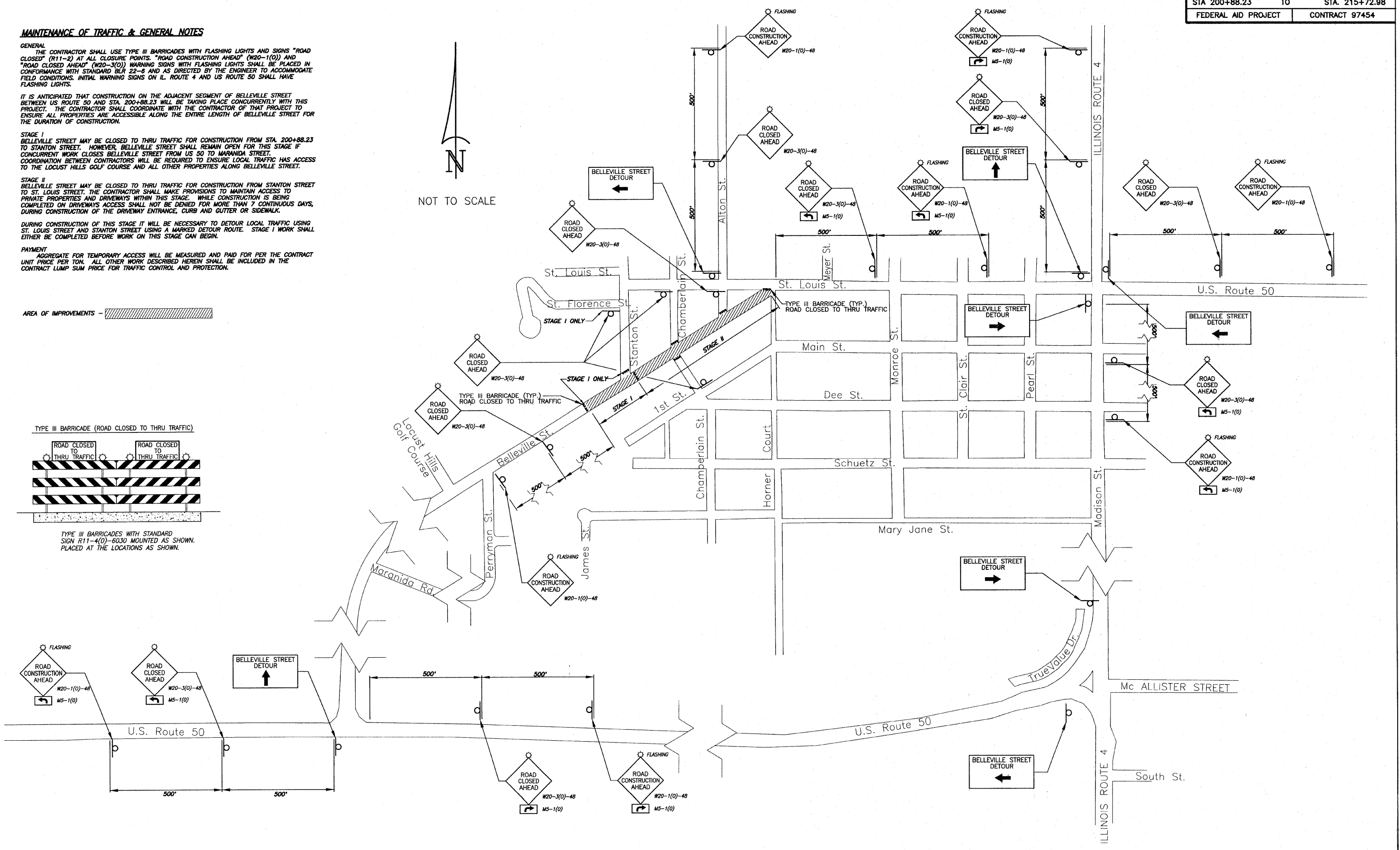
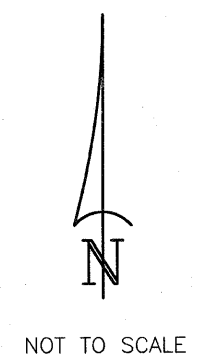
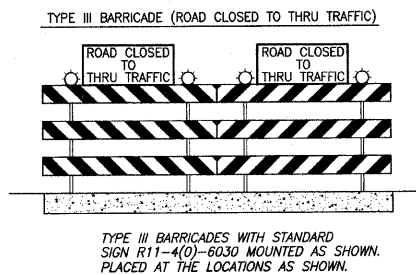
STAGE I
 BELLEVILLE STREET MAY BE CLOSED TO THRU TRAFFIC FOR CONSTRUCTION FROM STA. 200+88.23 TO STANTON STREET. HOWEVER, BELLEVILLE STREET SHALL REMAIN OPEN FOR THIS STAGE IF CONCURRENT WORK CLOSURES BELLEVILLE STREET FROM US 50 TO MARANIDA STREET. COORDINATION BETWEEN CONTRACTORS WILL BE REQUIRED TO ENSURE LOCAL TRAFFIC HAS ACCESS TO THE LOCUST HILLS GOLF COURSE AND ALL OTHER PROPERTIES ALONG BELLEVILLE STREET.

STAGE II
 BELLEVILLE STREET MAY BE CLOSED TO THRU TRAFFIC FOR CONSTRUCTION FROM STANTON STREET TO ST. LOUIS STREET. THE CONTRACTOR SHALL MAKE PROVISIONS TO MAINTAIN ACCESS TO PRIVATE PROPERTIES AND DRIVEWAYS WITHIN THIS STAGE. WHILE CONSTRUCTION IS BEING COMPLETED ON DRIVEWAYS ACCESS SHALL NOT BE DENIED FOR MORE THAN 7 CONTINUOUS DAYS, DURING CONSTRUCTION OF THE DRIVEWAY ENTRANCE, CURB AND GUTTER OR SIDEWALK.

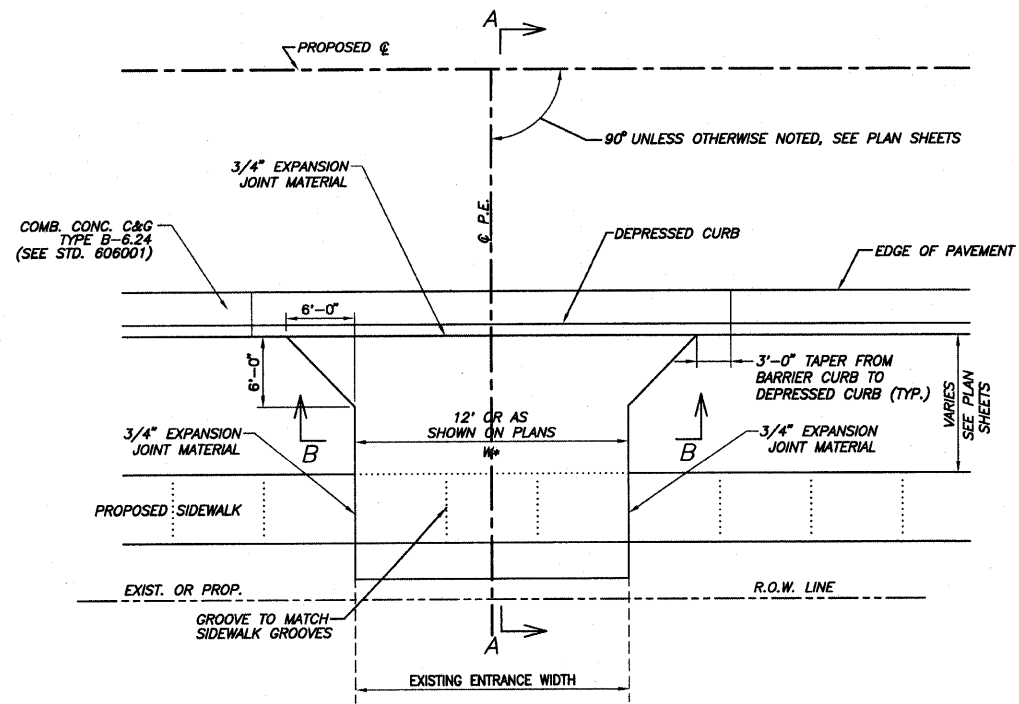
DURING CONSTRUCTION OF THIS STAGE IT WILL BE NECESSARY TO DETOUR LOCAL TRAFFIC USING ST. LOUIS STREET AND STANTON STREET USING A MARKED DETOUR ROUTE. STAGE I WORK SHALL EITHER BE COMPLETED BEFORE WORK ON THIS STAGE CAN BEGIN.

PAYMENT
 AGGREGATE FOR TEMPORARY ACCESS WILL BE MEASURED AND PAID FOR PER THE CONTRACT UNIT PRICE PER TON. ALL OTHER WORK DESCRIBED HEREIN SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION.

AREA OF IMPROVEMENTS - [Hatched Box]

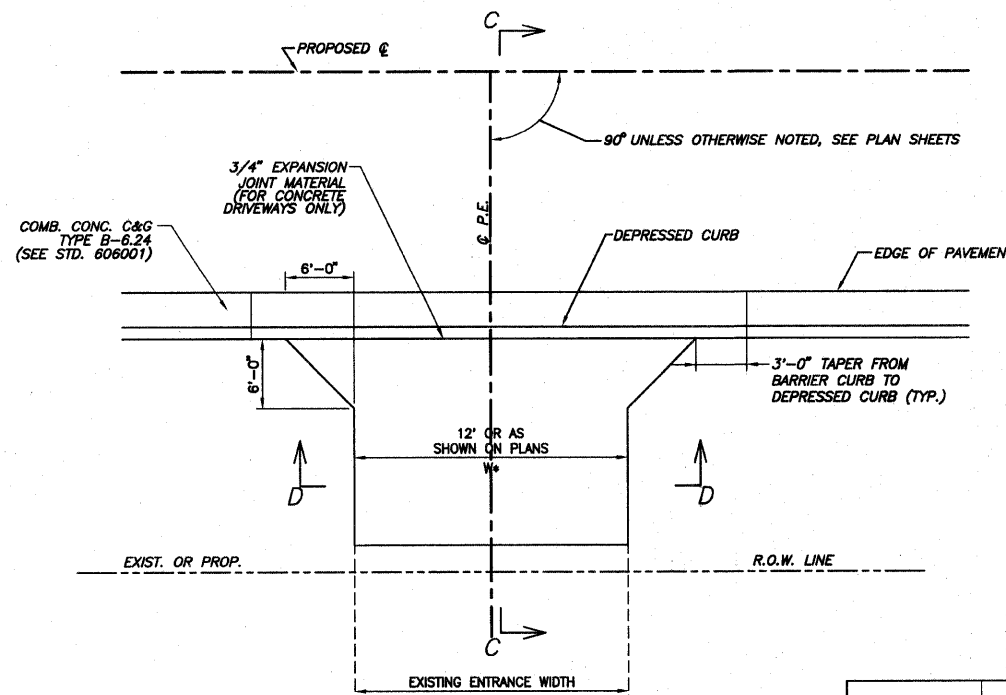


| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|----------------|--------------|--------------|
| 9247 | 08-00018-00-PV | ST. CLAIR | 19 | 11 |
| STA 200+88.23 TO | | STA. 215+72.98 | | |
| FEDERAL AID PROJECT | | CONTRACT 97454 | | |



P.E. DETAIL "A"

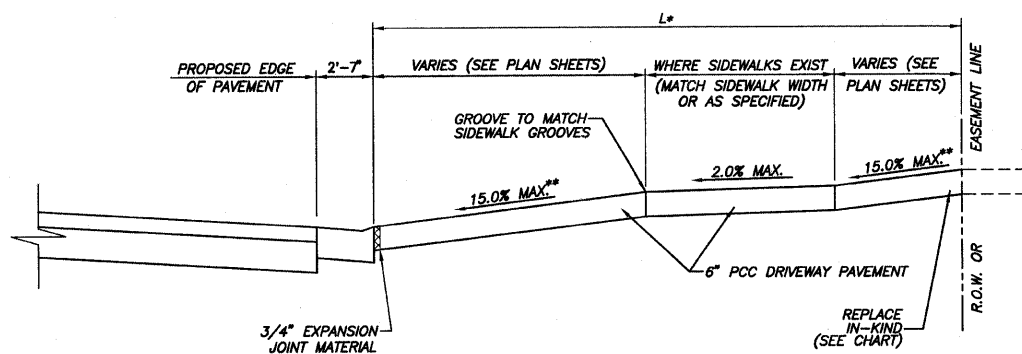
PRIVATE ENTRANCE WITH SIDEWALK AND CONCRETE CURB & GUTTER TYPE B-6.24 N.T.S.



P.E. DETAIL "B"

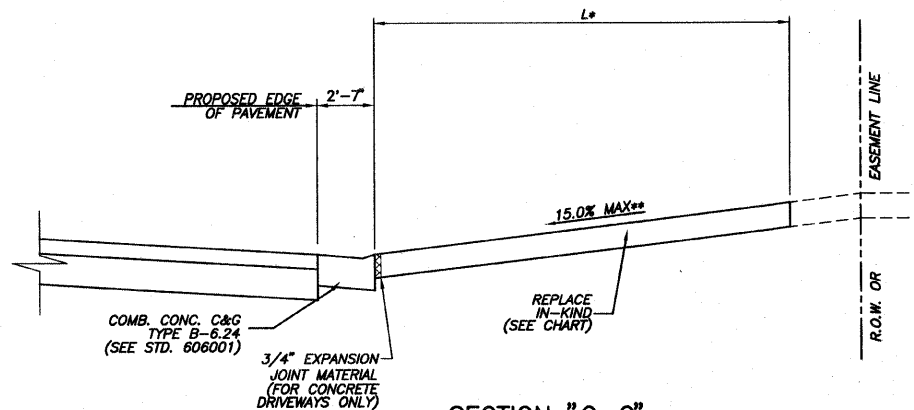
PRIVATE ENTRANCE WITHOUT SIDEWALK N.T.S.

| TYPE OF ENTRANCE | EXISTING MATERIAL | NEW ENTRANCE MATERIAL (BEYOND TAPERS) | | | |
|----------------------|-------------------|---------------------------------------|--------------------------------------|------------------------------|---------------------------------|
| | | PCC DRIVEWAY PAVEMENT | INCIDENTAL HOT-MIX ASPHALT SURFACING | AGGREGATE BASE COURSE TYPE B | AGGREGATE SURFACE COURSE TYPE B |
| PRIVATE & COMMERCIAL | CONCRETE | X | | | |
| | ASPHALT | | X | X | |
| | AGGREGATE | | | | X |



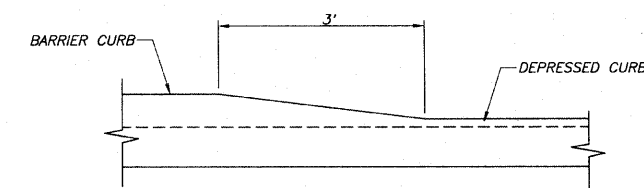
SECTION "A-A"

* SEE ENTRANCE SCHEDULE FOR DISTANCES
** SEE CROSS SECTIONS FOR SLOPES



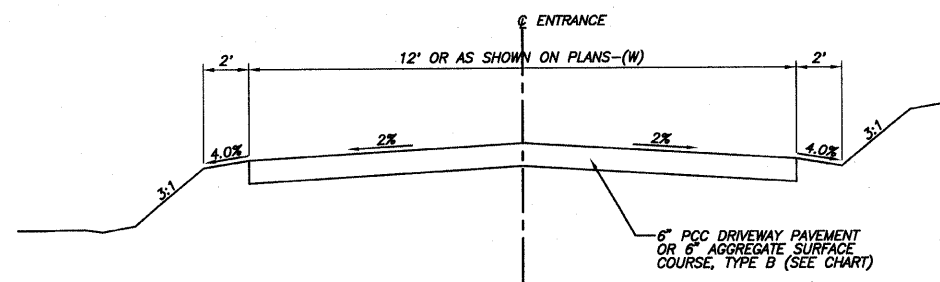
SECTION "C-C"

* SEE ENTRANCE SCHEDULE FOR DISTANCES
** SEE CROSS SECTIONS FOR SLOPES

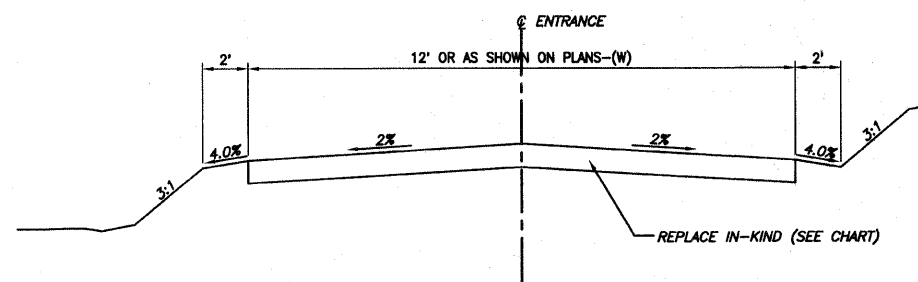


DETAIL OF CURB HEIGHT TRANSITION

N.T.S.

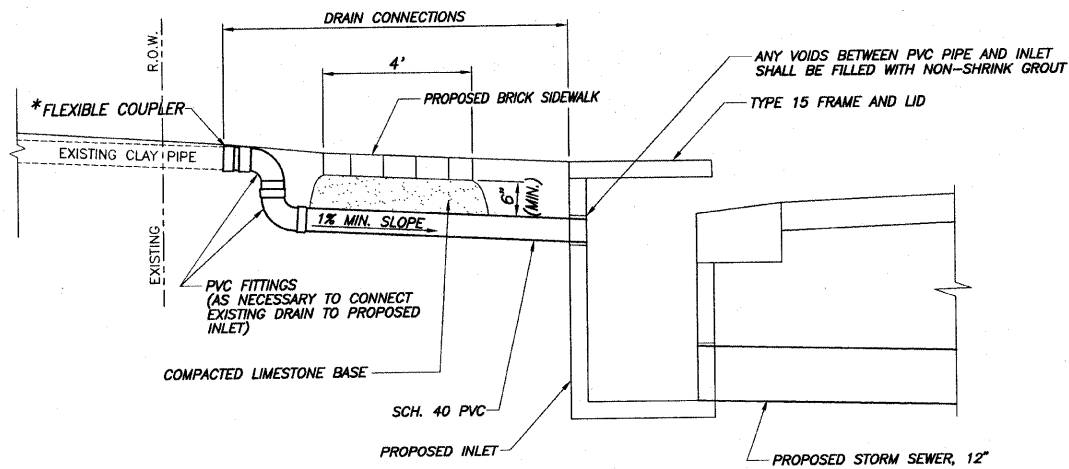


SECTION "B-B"



SECTION "D-D"

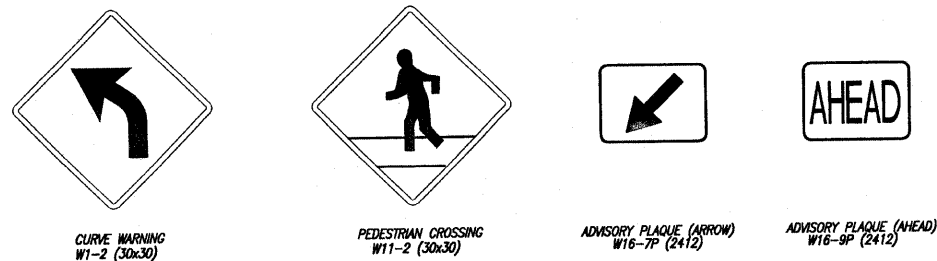
| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|----------------|--------------|--------------|
| 9247 | 08-00018-00-PV | ST. CLAIR | 19 | 12 |
| STA 200+88.23 TO | | STA. 215+72.98 | | |
| FEDERAL AID PROJECT | | CONTRACT 97454 | | |



* COUPLER SHALL PROVIDE A WATER-TIGHT SEAL BETWEEN EXISTING CLAY PIPE AND PROPOSED PVC PIPE.

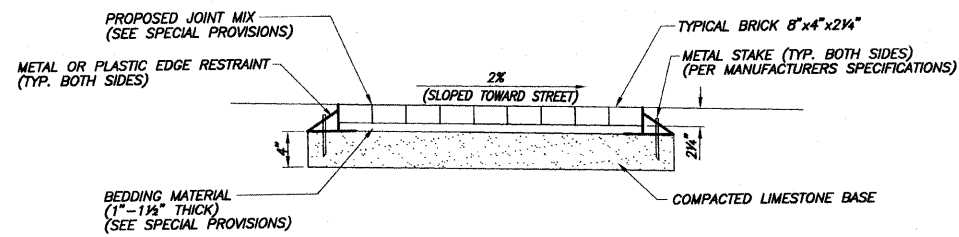
PROPOSED DRAIN PIPE CONNECTION DETAIL

N.T.S.



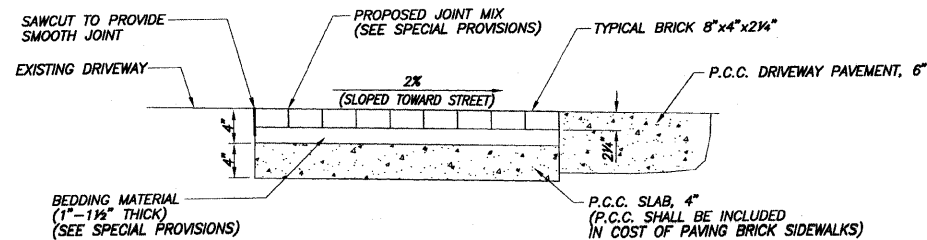
SIGNAGE DETAILS

N.T.S.



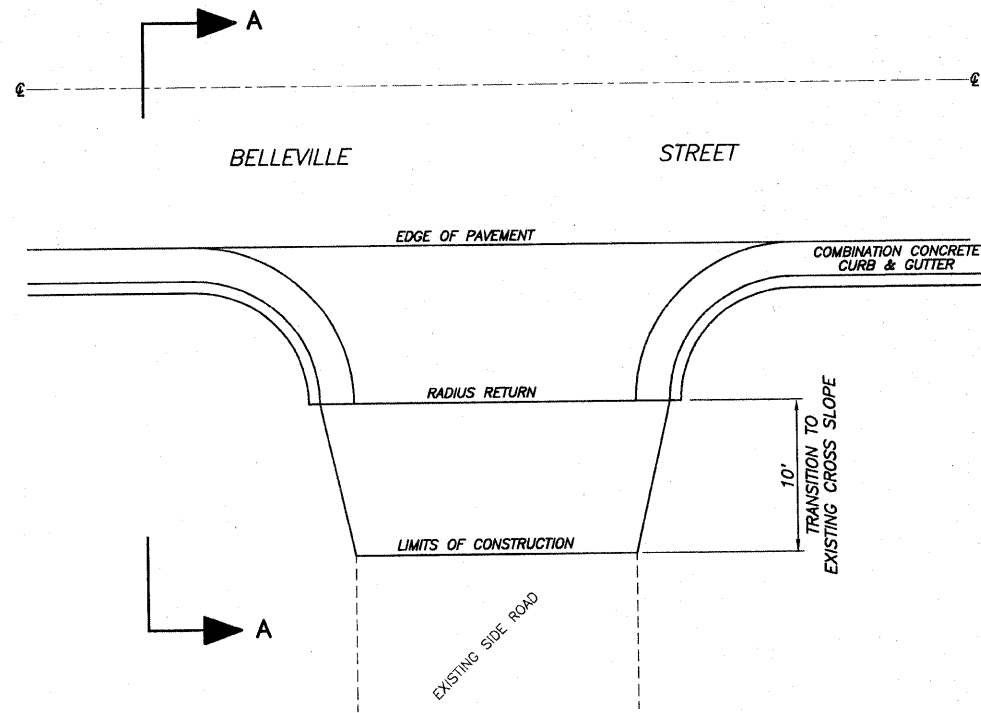
PAVING BRICK SIDEWALK TYPICAL SECTION

N.T.S.

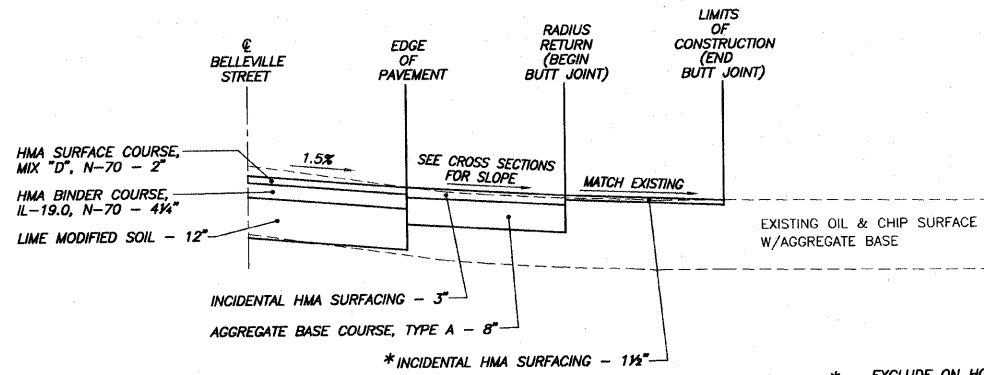


PAVING BRICK SIDEWALK TYPICAL SECTION (THROUGH DRIVEWAYS)

N.T.S.



PLAN VIEW

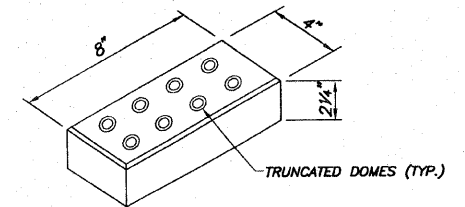


SECTION A-A

* - EXCLUDE ON HORNER STREET

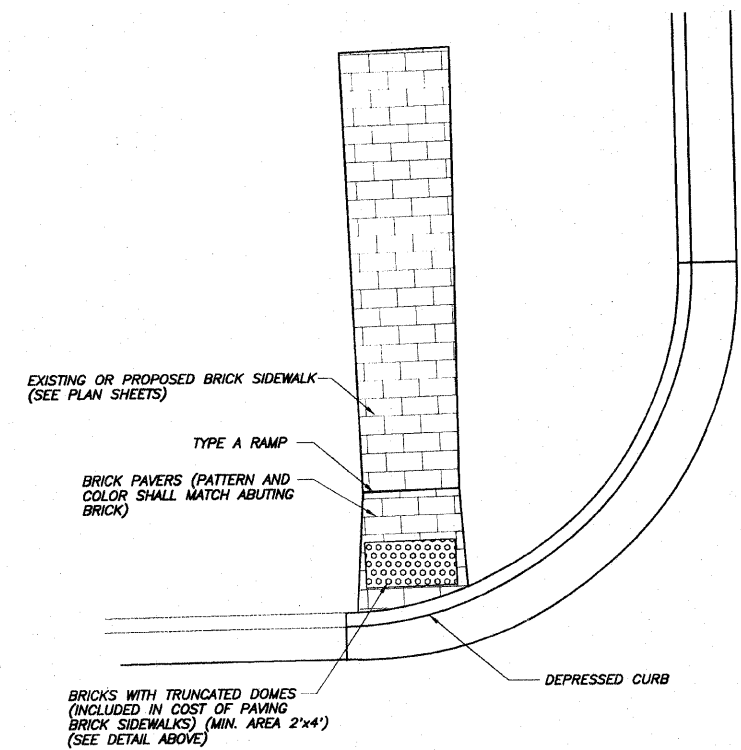
PAVEMENT TRANSITION FOR SIDE ROADS

N.T.S.



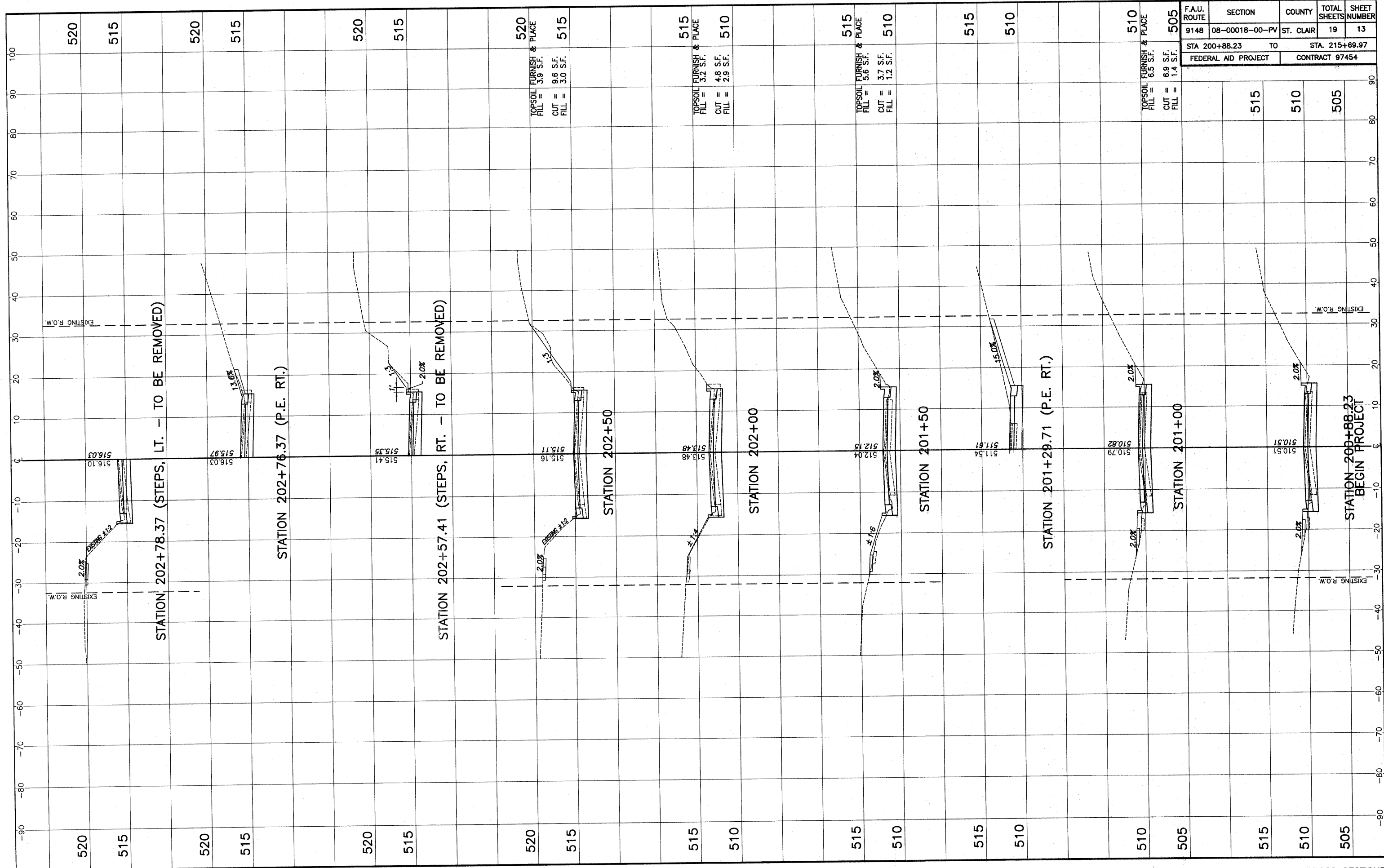
NOTE: FOR SPACING AND PATTERN OF TRUNCATED DOMES, SEE I.D.O.T. STD. 424001-05

BRICKS WITH TRUNCATED DOMES DETAIL



PAVING BRICK SIDEWALK

N.T.S.



520 TOPSOIL FURNISH & PLACE
 FILL = 3.9 S.F.
 515 CUT = 9.6 S.F.
 FILL = 3.0 S.F.

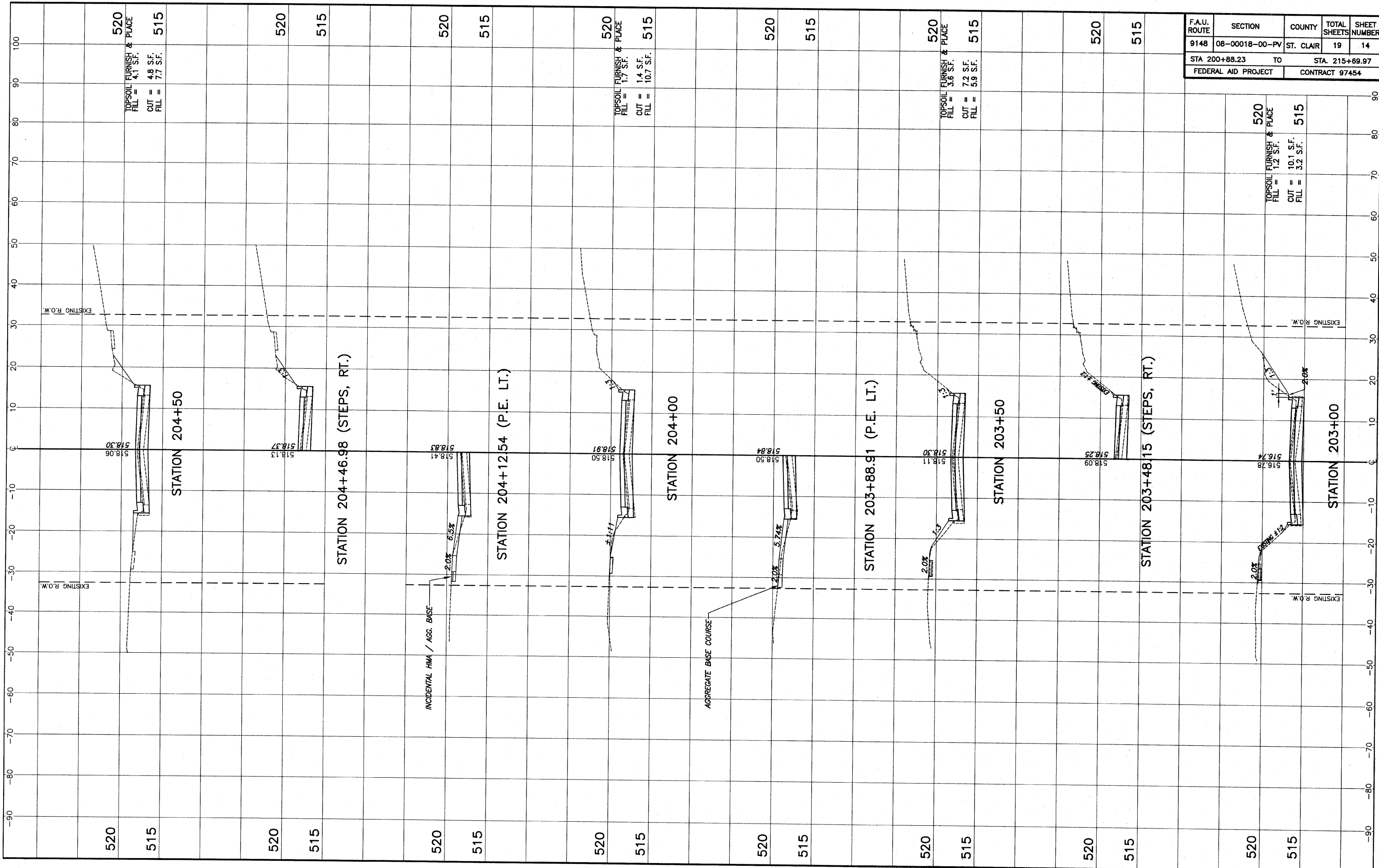
515 TOPSOIL FURNISH & PLACE
 FILL = 3.2 S.F.
 510 CUT = 4.8 S.F.
 FILL = 2.9 S.F.

515 TOPSOIL FURNISH & PLACE
 FILL = 5.6 S.F.
 510 CUT = 3.7 S.F.
 FILL = 1.2 S.F.

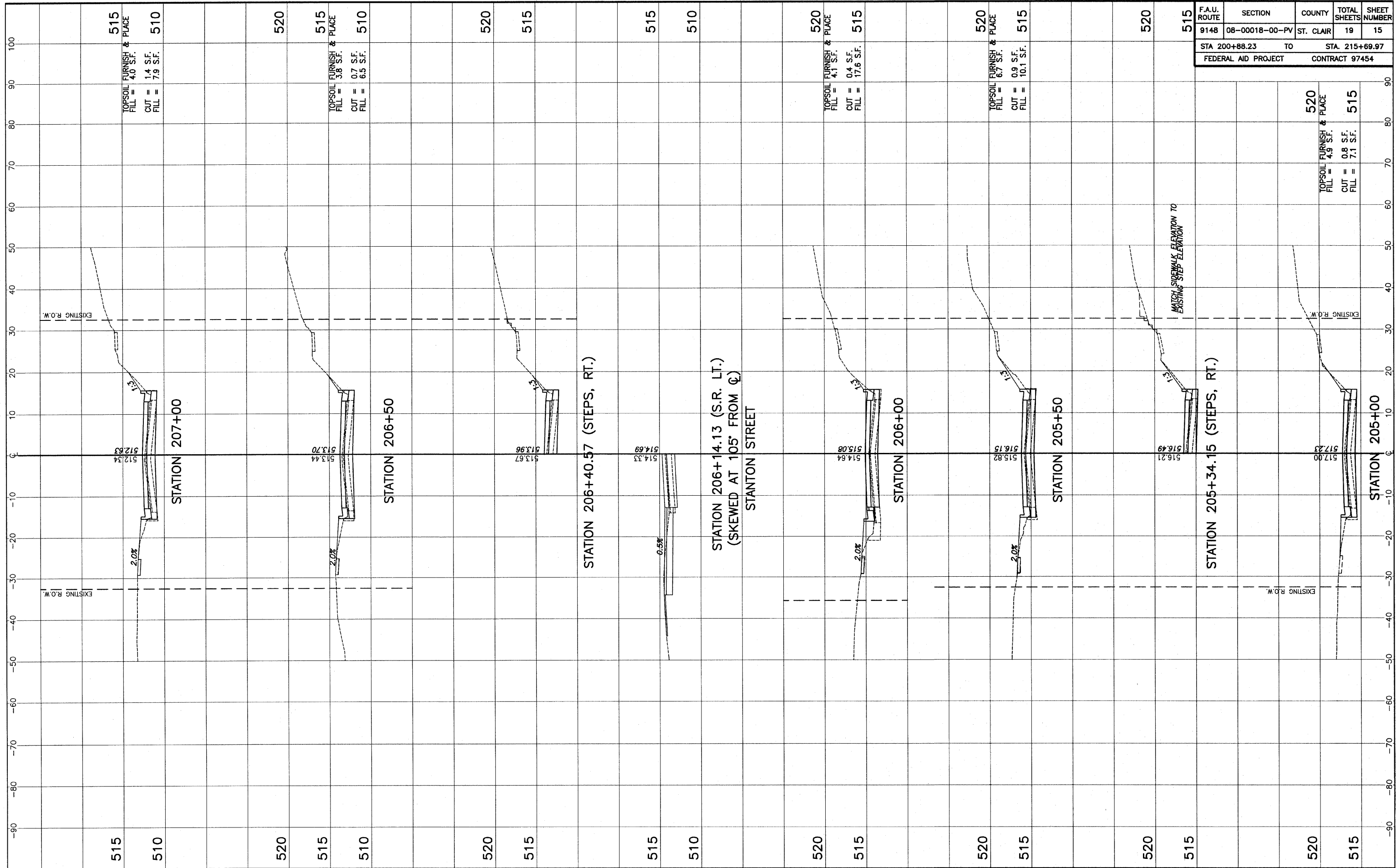
510 TOPSOIL FURNISH & PLACE
 FILL = 6.5 S.F.
 505 CUT = 6.9 S.F.
 FILL = 1.4 S.F.

| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|----------------|--------------|--------------|
| 9148 | 08-00018-00-PV | ST. CLAIR | 19 | 13 |
| STA 200+88.23 TO | | STA. 215+69.97 | | |
| FEDERAL AID PROJECT | | CONTRACT 97454 | | |

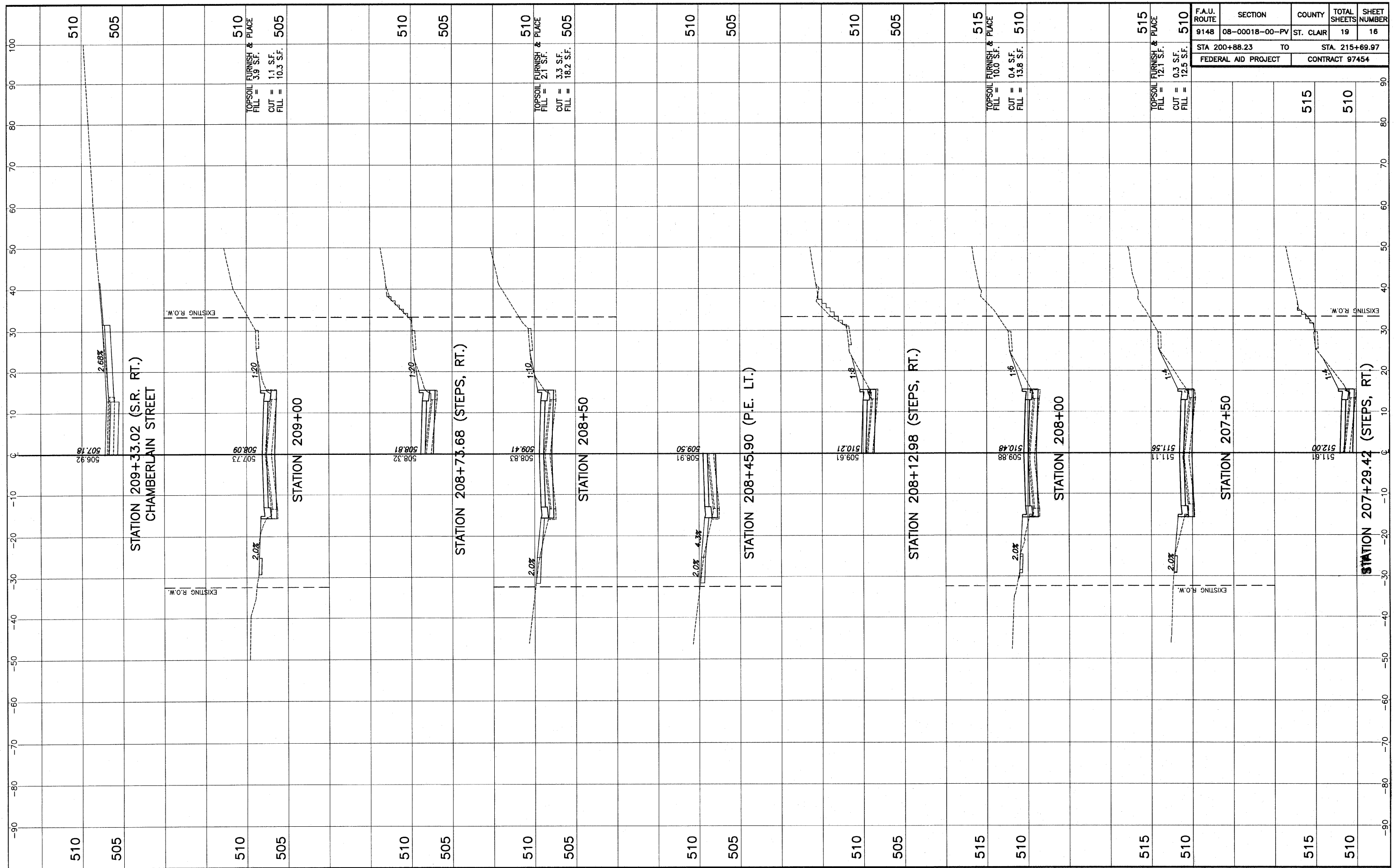
515
 510
 505



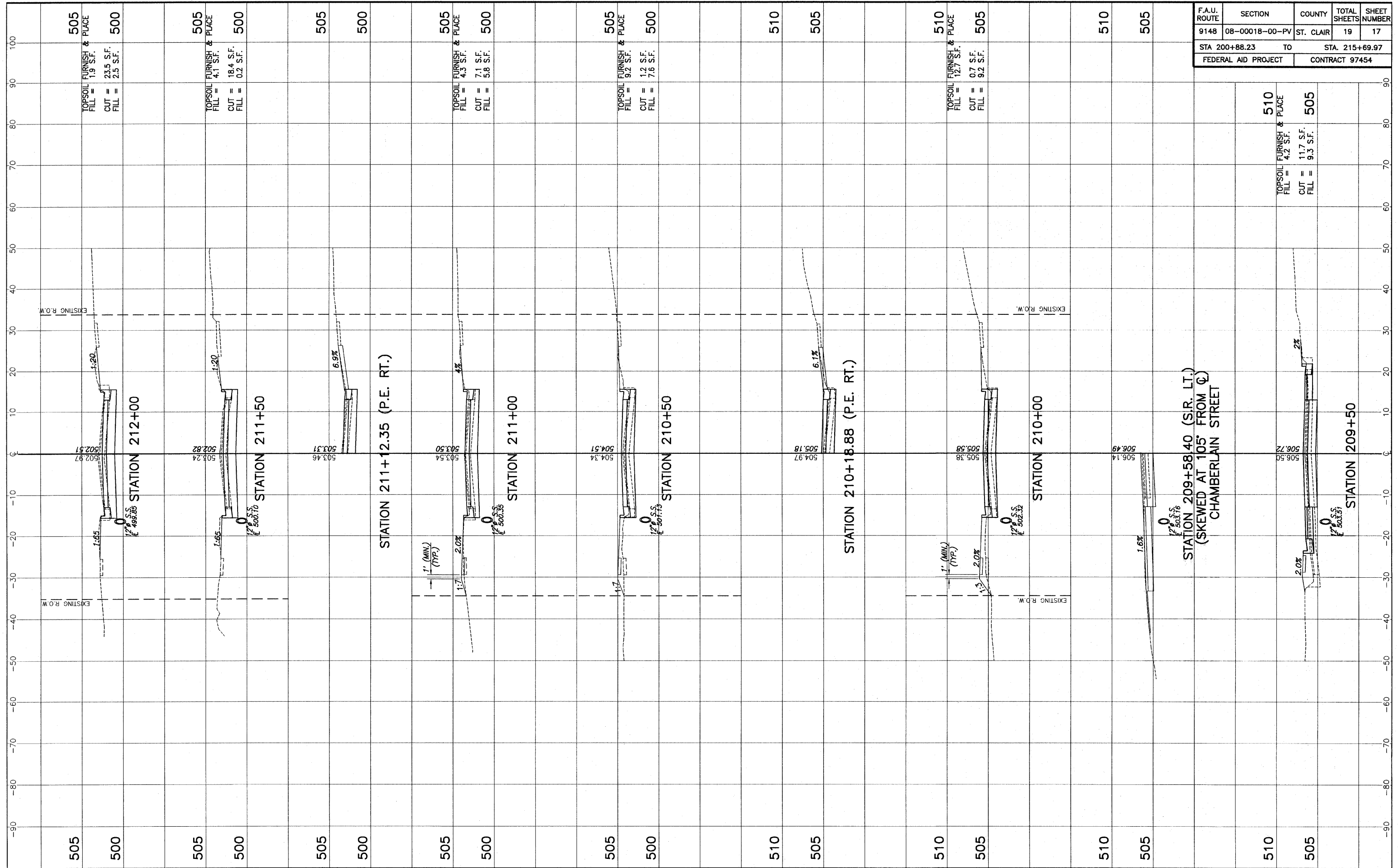
| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|----------------|--------------|--------------|
| 9148 | 08-00018-00-PV | ST. CLAIR | 19 | 14 |
| STA 200+88.23 TO | | STA. 215+69.97 | | |
| FEDERAL AID PROJECT | | CONTRACT 97454 | | |



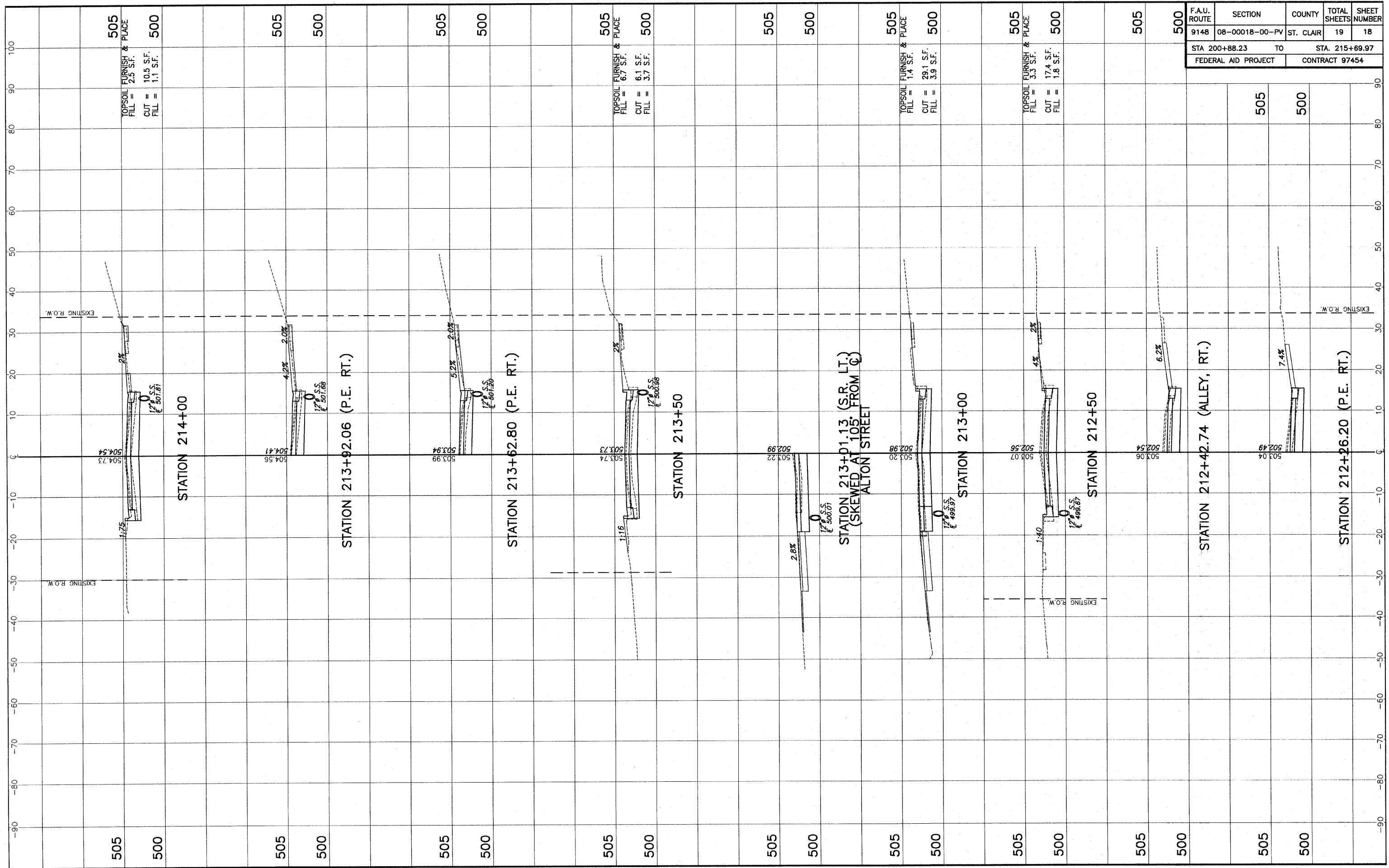
| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|-----------|----------------|----------------|
| 9148 | 08-00018-00-PV | ST. CLAIR | 19 | 15 |
| STA 200+88.23 | | TO | | STA. 215+69.97 |
| FEDERAL AID PROJECT | | | CONTRACT 97454 | |



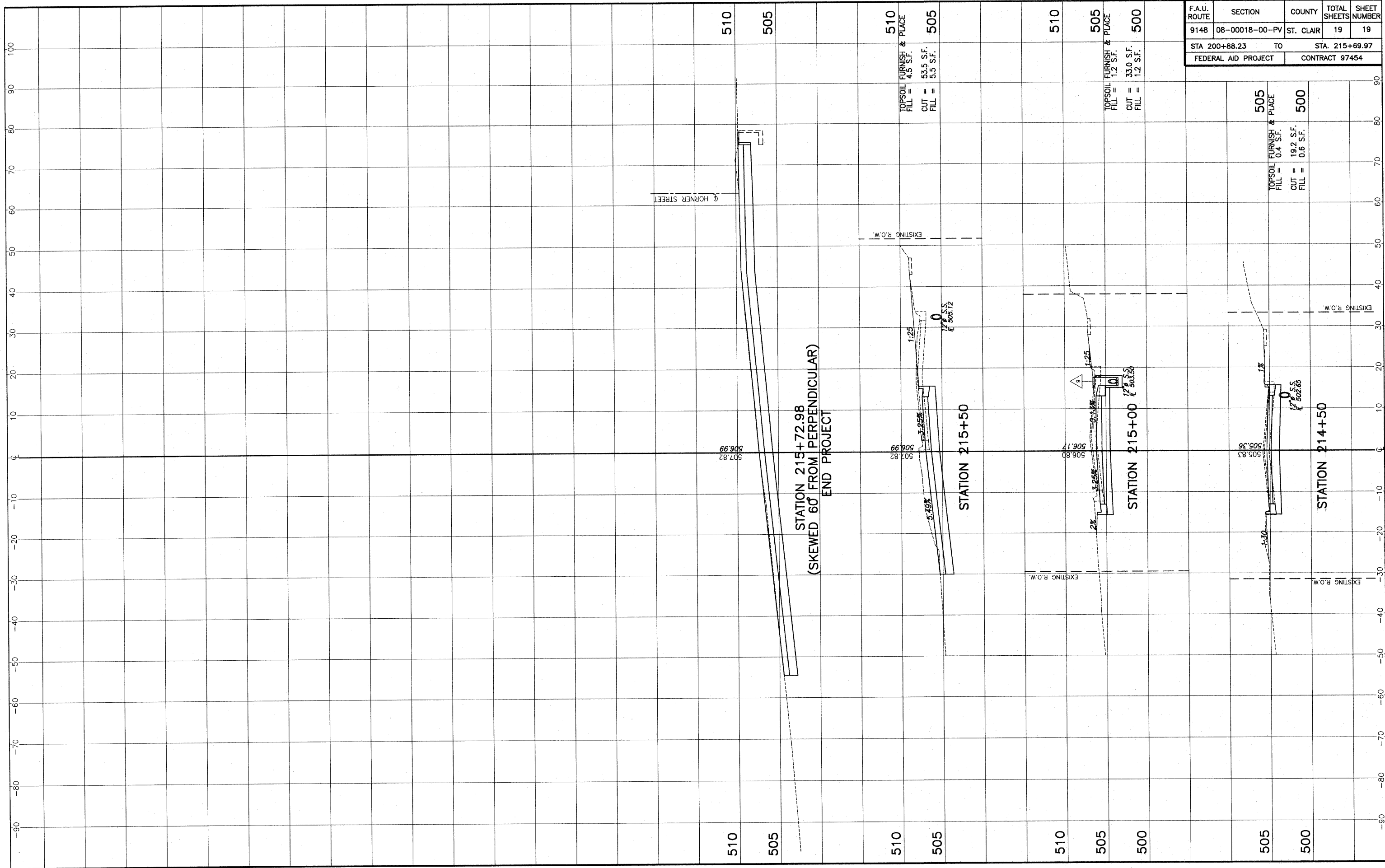
| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|----------------|--------------|--------------|
| 9148 | 08-00018-00-PV | ST. CLAIR | 19 | 16 |
| STA 200+88.23 TO | | STA. 215+69.97 | | |
| FEDERAL AID PROJECT | | CONTRACT 97454 | | |



| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------------------|----------------|------------------------------------|--------------|--------------|
| 9148 | 08-00018-00-PV | ST. CLAIR | 19 | 17 |
| STA 200+88.23 TO STA. 215+69.97 | | FEDERAL AID PROJECT CONTRACT 97454 | | |



| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|----------------|--------------|--------------|
| 9148 | 08-00018-00-PV | ST. CLAIR | 19 | 18 |
| STA 200+88.23 TO | | STA. 215+69.97 | | |
| FEDERAL AID PROJECT | | CONTRACT 97454 | | |



| F.A.U. ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NUMBER |
|---------------------|----------------|-----------|----------------|--------------|
| 9148 | 08-00018-00-PV | ST. CLAIR | 19 | 19 |
| STA 200+88.23 | | TO | STA. 215+69.97 | |
| FEDERAL AID PROJECT | | | CONTRACT 97454 | |

510
505

STATION 215+72.98
(SKEWED 60° FROM PERPENDICULAR)
END PROJECT

510
505
TOPSOIL FURNISH & PLACE
FILL = 4.5 S.F.
CUT = 53.5 S.F.
FILL = 5.5 S.F.

STATION 215+50

510
505
TOPSOIL FURNISH & PLACE
FILL = 1.2 S.F.
CUT = 33.0 S.F.
FILL = 1.2 S.F.

STATION 215+00

505
500
TOPSOIL FURNISH & PLACE
FILL = 0.4 S.F.
CUT = 19.2 S.F.
FILL = 0.6 S.F.

STATION 214+50