



# CITY OF FREEPORT FREEPORT, ILLINOIS



**FREEPORT - ALBERTUS AIRPORT**  
FREEPORT, ILLINOIS

FREEPORT-ALBERTUS AIRPORT

APPROVED BY   
HONORABLE GEORGE W. GAULRAPPA, MAYOR

DATE MARCH 4, 2011

## Rehabilitate North Terminal Apron and T-Hangar Taxiways / Rehabilitate Taxiway Electrical Circuit

FINAL SUBMITTAL

ILLINOIS PROJECT: FEP-3997  
AIP PROJECT: 3-17-0045-B23

MARCH 4, 2011

### INDEX TO SHEETS

1. COVER SHEET (CVR1)
2. SUMMARY OF QUANTITIES - 1 (QTY1)
3. CONSTRUCTION PHASING PLAN (CPP1)
4. CONSTRUCTION PHASING NOTES AND DETAILS (CPN1)

### REHABILITATE NORTH TERMINAL APRON AND T-HANGAR TAXIWAYS

5. SITE PLAN AND PROJECT CONTROL (SPL1)
6. TYPICAL SECTIONS (TYP1)
7. EXISTING CONDITIONS / PROPOSED REMOVALS (REM1)
8. CONSTRUCTION PHASING - 1 (CPS1)
9. CONSTRUCTION PHASING - 2 (CPS2)
10. GRADING PLAN - 1 (GRD1)
11. GRADING PLAN - 2 (GRD2)
12. ELECTRICAL, MARKING, AND LANDSCAPING PLAN - 1 (EML1)
13. ELECTRICAL, MARKING, AND LANDSCAPING PLAN - 2 (EML2)
14. ENGINEERING INFORMATION (ENG1)
15. MISCELLANEOUS DETAILS (DTL1)

### REHABILITATE TAXIWAY ELECTRICAL CIRCUIT

16. SITE PLAN AND PROJECT CONTROL (SPL2)
17. CONSTRUCTION PHASING - 3 (CPS3)
18. EXISTING CONDITIONS / PROPOSED REMOVALS (REM2)
19. ELECTRICAL, MARKING, AND LANDSCAPING PLAN - 3 (EML3)
20. ELECTRICAL, MARKING, AND LANDSCAPING PLAN - 4 (EML4)
21. ELECTRICAL DETAILS - 1 (ELD1)
22. ELECTRICAL DETAILS - 2 (ELD2)

**DESIGN INFORMATION**

DESIGN AIRCRAFT APPROACH CATEGORY D  
DESIGN AIRCRAFT GROUP 2

TOWNSHIP: 26 N RANGE: 8 E TOWNSHIP: SILVER CREEK  
SECTION: 21 STEPHENSON COUNTY  
MAX. EQUIPMENT HT.: 25 FT.

CALL J.U.L.I.E  
BEFORE EXCAVATING  
1-800-892-0123

Illinois Professional Engineering Practice Act of 1989

 **CMT**  
http://www.cmtengr.com  
Springfield, Illinois  
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Rockford, Illinois  
St. Louis, Mo.

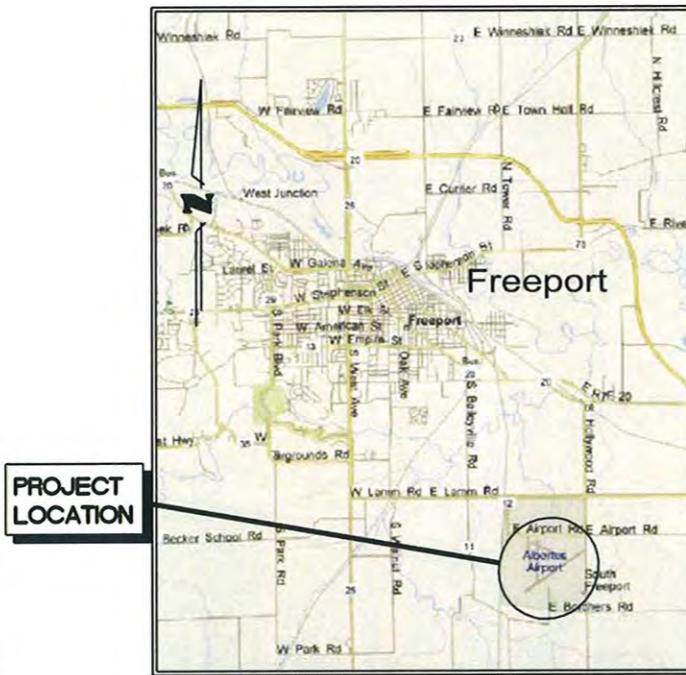
3/4/2011



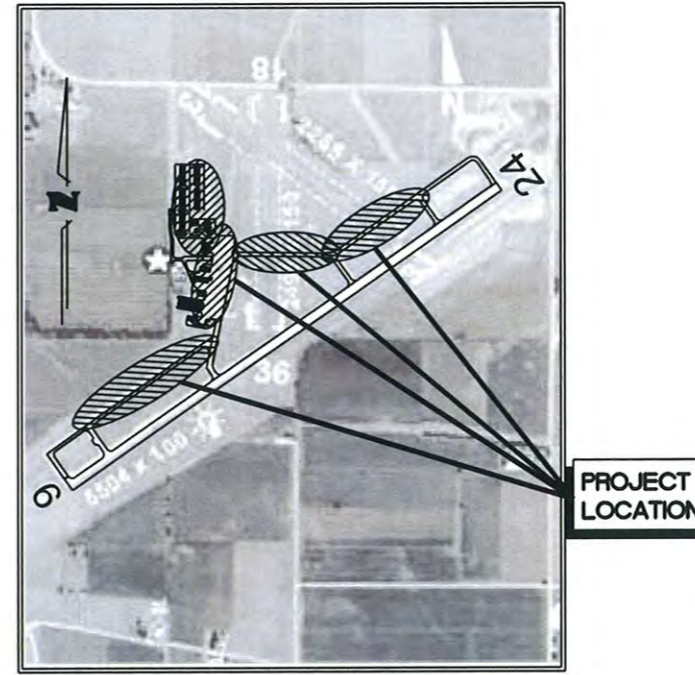
Exp. 11/2011

SUBMITTED BY   
TYLER V. NELSON, P.E.

DATE MARCH 4, 2011  
license expires 11-30-2011



LOCATION MAP



SITE PLAN



# SUMMARY OF QUANTITIES

SEQ NO	ITEM NO	DESCRIPTION	UNIT	TOTAL QUANTITY	
				TOTAL QUANTITY	RECORD QUANTITY
1	AR108108	1/C #8 5 KV UG CABLE	LF	1,000.00	
2	AR108158	1/C #8 5 KV UG CABLE IN UD	LF	14,100.00	
3	AR109210	VAULT MODIFICATIONS	LS	1.00	
4	AR109332	15 KW REGULATOR, STYLE 2	EACH	1.00	
5	AR125410	MITL-STAKE MOUNTED	EACH	94.00	
6	AR125415	MITL-BASE MOUNTED	EACH	52.00	
7	AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	116.00	
8	AR125902	REMOVE BASE MOUNTED LIGHT	EACH	22.00	
9	AR150510	ENGINEER'S FIELD OFFICE	LS	1.00	
10	AR150520	MOBILIZATION	LS	1.00	
11	AR152410	UNCLASSIFIED EXCAVATION	CY	755.00	
12	AR152442	OFFSITE BORROW EXCAVATION	CY	200.00	
13	AR152540	SOIL STABILIZATION FABRIC	SY	2,005.00	
14	AR156510	SILT FENCE	LF	512.00	
15	AR156512	BALES	EACH	64.00	
16	AR201670	CRACK CONTROL FABRIC	SY	15,000.00	
17	AR208515	POROUS GRANULAR EMBANKMENT	CY	95.00	
18	AR209608	CRUSHED AGG. BASE COURSE - 8"	SY	2,005.00	
19	AR401610	BITUMINOUS SURFACE COURSE	TON	3,400.00	
20	AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1.00	
21	AR401650	BITUMINOUS PAVEMENT MILLING	SY	10,200.00	
22	AR401655	BUTT JOINT CONSTRUCTION	SY	1,440.00	
23	AR401900	REMOVE BITUMINOUS PAVEMENT	SY	1,750.00	
24	AR401910	REMOVE & REPLACE BITUMINOUS PAVEMENT	SY	100.00	
25	AR510510	TIEDOWN	EACH	33.00	
26	AR510515	GROUND ROD	EACH	7.00	
27	AR510900	REMOVE TIEDOWN	EACH	30.00	
28	AR602510	BITUMINOUS PRIME COAT	GAL	1,070.00	
29	AR603510	BITUMINOUS TACK COAT	GAL	2,090.00	
30	AR620520	PAVEMENT MARKING - WATERBORNE	SF	2,000.00	
31	AR620525	PAVEMENT MARKING - BLACK BORDER	SF	2,000.00	
33	AR625510	TAR EMULSION SEAL COAT	SY	460.00	
34	AR800813	REFURBISH & GROUND GUIDANCE SIGN	EACH	3.00	
35	AR901510	SEEDING	ACRE	0.95	
36	AR905530	TOPSOILING	SY	4,600.00	
37	AR908510	MULCHING	ACRE	0.95	

SUMMARY OF QUANTITIES

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Revisions

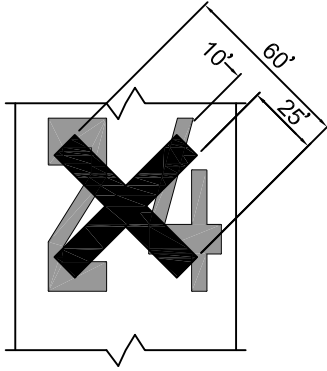
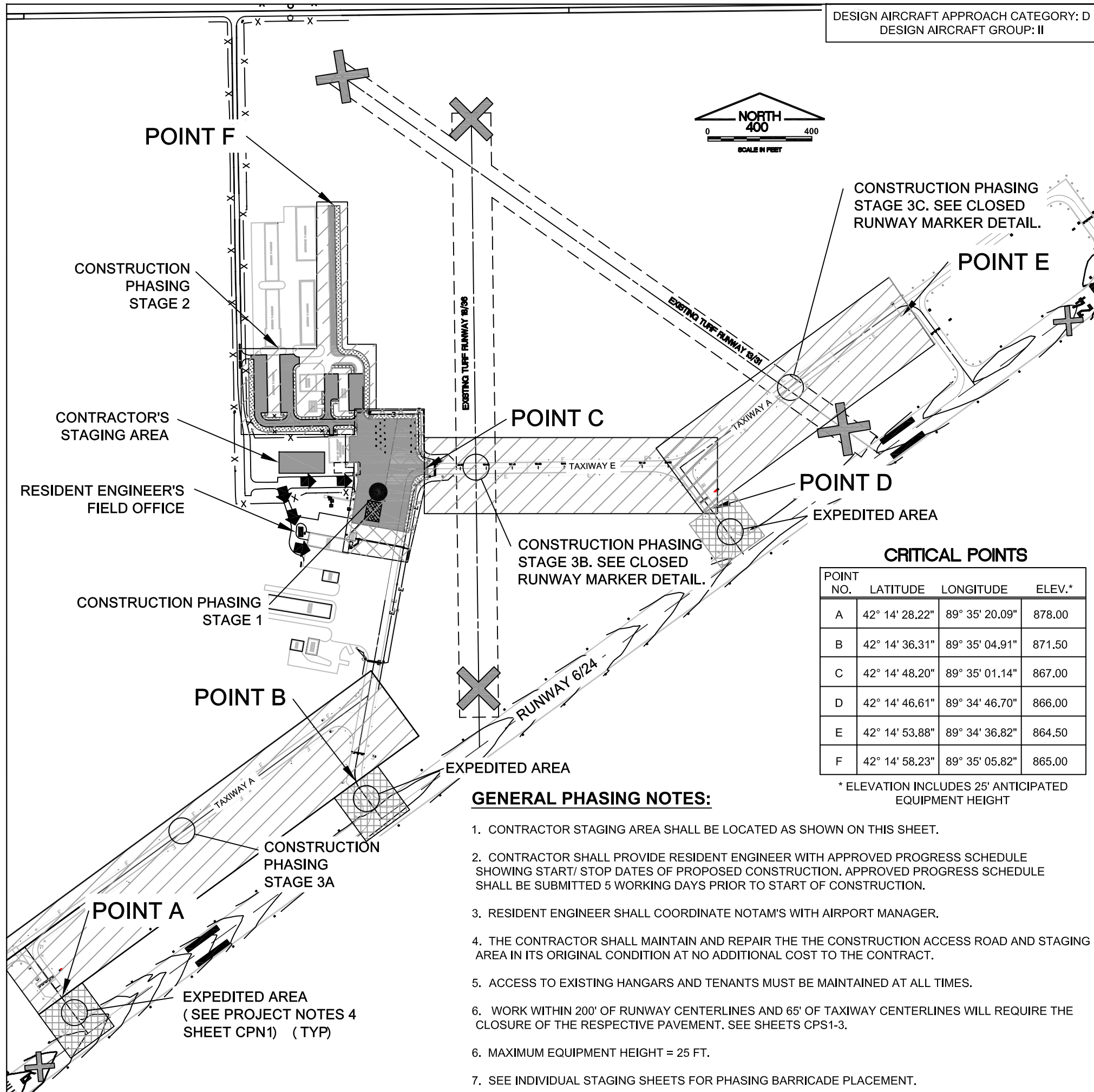
Date	Description

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THIS BAR IS EQUAL TO 1"  
AT FULL SCALE (17 X 11).

DESIGN BY:	CMT- RFD
DRAWN BY:	CMT- RFD
CHECKED BY:	CMT- RFD
APPROVED BY:	CMT- RFD
DATE:	3-4-2011
JOB No:	09294-02-00/06 08294-02-00/06

SUMMARY OF  
QUANTITIES  
(QTY1)

DESIGN AIRCRAFT APPROACH CATEGORY: D  
DESIGN AIRCRAFT GROUP: II



**CLOSED RUNWAY MARKER DETAIL**  
ON PAVEMENT - NO SCALE

1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER AND THE AIRPORT.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
4. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER AND AIRPORT. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED CONSTRUCTION PHASING PLAN.

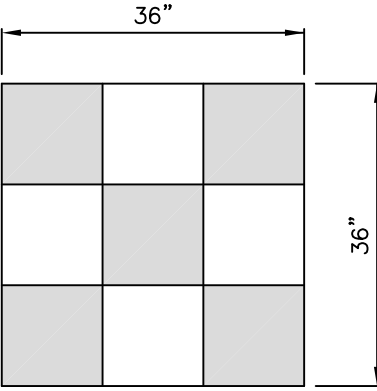
**CRITICAL POINTS**

POINT NO.	LATITUDE	LONGITUDE	ELEV.*
A	42° 14' 28.22"	89° 35' 20.09"	878.00
B	42° 14' 36.31"	89° 35' 04.91"	871.50
C	42° 14' 48.20"	89° 35' 01.14"	867.00
D	42° 14' 46.61"	89° 34' 46.70"	866.00
E	42° 14' 53.88"	89° 34' 36.82"	864.50
F	42° 14' 58.23"	89° 35' 05.82"	865.00

\* ELEVATION INCLUDES 25' ANTICIPATED EQUIPMENT HEIGHT

**GENERAL PHASING NOTES:**

1. CONTRACTOR STAGING AREA SHALL BE LOCATED AS SHOWN ON THIS SHEET.
2. CONTRACTOR SHALL PROVIDE RESIDENT ENGINEER WITH APPROVED PROGRESS SCHEDULE SHOWING START/ STOP DATES OF PROPOSED CONSTRUCTION. APPROVED PROGRESS SCHEDULE SHALL BE SUBMITTED 5 WORKING DAYS PRIOR TO START OF CONSTRUCTION.
3. RESIDENT ENGINEER SHALL COORDINATE NOTAM'S WITH AIRPORT MANAGER.
4. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE THE CONSTRUCTION ACCESS ROAD AND STAGING AREA IN ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT.
5. ACCESS TO EXISTING HANGARS AND TENANTS MUST BE MAINTAINED AT ALL TIMES.
6. WORK WITHIN 200' OF RUNWAY CENTERLINES AND 65' OF TAXIWAY CENTERLINES WILL REQUIRE THE CLOSURE OF THE RESPECTIVE PAVEMENT. SEE SHEETS CPS1-3.
6. MAXIMUM EQUIPMENT HEIGHT = 25 FT.
7. SEE INDIVIDUAL STAGING SHEETS FOR PHASING BARRICADE PLACEMENT.



**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**

NOT TO SCALE

**CONSTRUCTION PHASING PLAN**

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APPROVED BY: CMT-RFD  
DATE: 3-4-2011  
JOB No: 09294-02-00/06  
08294-02-00/06

**CONSTRUCTION PHASING PLAN (CPP1)**

3

SHEET 3 OF 22 SHEETS

# GENERAL NOTES

## 1. SUGGESTED SEQUENCE OF CONSTRUCTION:

THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED ONLY AND MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.

## 2. HAUL ROAD / STAGING AREA RESTORATION:

ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION TO THE SATISFACTION OF THE AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

## 3. AIRPORT APPROVAL OF PHASING:

THE RESIDENT ENGINEER AND AIRPORT MANAGER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC / CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS.

## 4. AIRFIELD PAVEMENT / SITE DEBRIS REMOVAL:

THE CONTRACTOR SHALL RESTRICT ACCESS TO ALL TRUCKS, EQUIPMENT AND MATERIALS FROM THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS SHOWN. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEE USE. FACILITIES WITHIN THE HANGARS / AIRPORT BUILDINGS SHALL NOT BE USED.

## 5. PROJECT LIGHTING OUTSIDE OF DAYLIGHT HOURS:

WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND / OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND AIRPORT MANAGER, AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

## 6. EXISTING UTILITY COORDINATION:

COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED PRIOR TO CONSTRUCTION. SEE SECTION 50 - 17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE AIRPORT NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED, ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY / OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE AIRPORT AND THE RESIDENT ENGINEER. ANY SUCH UTILITIES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT, ENGINEER AND HIS CONSULTANT.

## 7. TRAFFIC CONTROL PAYMENT:

PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, ETC. SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO ASSOCIATED CONTRACT ITEM. TYPE 2 BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE PLACED ON 15' CENTERS. TYPE 2 BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE ENGINEER FOR WORK ADJACENT TO THE EXPEDITED WORK AREAS. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "ACTIVE AIRFIELD AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL TYPE II AND TYPE III BARRICADES SHALL CONFORM TO IDOT STANDARD DETAIL 702001. ALL PAVEMENT DROP-OFFS GREATER THAN 24" REQUIRE TYPE II BARRICADES WITH EXTENDED LEGS.

## 8. DUST CONTROL REQUIREMENTS:

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATIONS GENERATING SIGNIFICANT DUST DURING WORKING HOURS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE ASSOCIATED CONTRACT ITEM.

## 9. OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION (AC 150/5370-2(Latest edition):

ALL WORK SHALL CONFORM TO FAA ADVISORY CIRCULAR (AC) AC 150/5370-2(LATEST EDITION) OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION. THIS AC IS AVAILABLE AT, [www.faa.gov/arp/pdf/5370-2x.pdf](http://www.faa.gov/arp/pdf/5370-2x.pdf)

## 10. STAGING AREA:

THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

## 11. AIRFIELD LIGHTING COORDINATION:

THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT MANAGER REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY.

## 12. WEEKLY COORDINATION MEETINGS

WEEKLY COORDINATION MEETINGS SHALL BE REQUIRED TO DISCUSS PROJECT PROGRESS. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY.

## 13. AIRFIELD FENCING / GATE SECURITY:

ALL EXISTING AND PROPOSED FENCE LINES, EXCEPT AS OTHERWISE NOTED, SHALL BE MAINTAINED AND SHALL SERVE AS CONSTRUCTION FENCING AROUND THE PERIMETER OF THE PROJECT. ALL EXISTING GATES SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT OWNER'S REPRESENTATIVE. SHOULD THE CONTRACTOR CHOOSE TO KEEP A GATE OPEN FOR CONSTRUCTION OPERATIONS, A COMPETENT SECURITY GUARD SHALL MONITOR THE OPEN GATE. ANY COSTS SHALL NOT BE PAID FOR SEPARATELY, BUT THEY WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

## 14. AIRPORT RADIO COMMUNICATION:

THE CONTRACTOR SHALL SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO MONITORING AIRPORT TRAFFIC. THE OPERATOR OF THE AIRPORT RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND BE TUNED INTO THE APPLICABLE GROUND CONTROL FREQUENCY.

# PROJECT NOTES

## 1. UNATTENDED CONSTRUCTION ACCESS:

THE OWNER WILL ASSESS THE CONTRACTOR A \$1,000.00 FINE FOR CLOSING AND/OR LOCKING EACH UNATTENDED ACCESS DOOR, GATE OR FENCE WHICH HAS BEEN IDENTIFIED AS A CONTRACTOR RESPONSIBILITY. AN ACCESS DOOR, GATE OR FENCE IS DEFINED AS "UNATTENDED" ANY TIME IT IS OPEN, UNLOCKED OR OTHERWISE RENDERED INEFFECTIVE IN PROVIDING SECURITY AND CONTRACTOR PERSONNEL ARE NOT PHYSICALLY IN A POSITION TO DETECT AND PREVENT UNAUTHORIZED ENTRY THROUGH IT. IN THE EVENT AIRPORT PERSONNEL ARE UNABLE TO SECURE AN UNATTENDED ACCESS DOOR, GATE OR FENCE, AIRPORT SECURITY PERSONNEL WILL BE POSTED AND CONTRACTORS WILL BE NOTIFIED. CONTRACTORS WILL BE CHARGED THE COST OF PROVIDING SECURITY PERSONNEL UNTIL THE AREA IS AGAIN SECURE.

## 2. UNAUTHORIZED ACCESS TO AIRFIELD:

THE CONTRACTOR SHALL RESTRICT ALL ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE PHASING PLAN. ANY UNAUTHORIZED ACCESS, PEDESTRIAN OR VEHICULAR, TO ACTIVE AIRFIELD PAVEMENTS SHOWN SHALL BE CONSIDERED AIRFIELD INCURSIONS. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DIRECTOR OF OPERATIONS, MAY BE FINED \$1,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND TAKEN FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

## 3. CONSTRUCTION SITE ACCESS:

THE CONTRACTOR SHALL UTILIZE THE HAUL ROAD AS SHOWN ON THE CONSTRUCTION PHASING PLAN. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY.

## 4. EXPEDITED WORK AREAS:

WORK PERFORMED WITHIN THE EXPEDITED WORK AREAS WILL REQUIRE THE CLOSURE OF RUNWAY 6/24. ONLY DAILY CLOSURES BETWEEN THE HOURS OF 7:00 AM AND 7:00 PM WILL BE ALLOWED TO COMPLETE THE WORK WITHIN THE DESIGNATED EXPEDITED WORK AREAS. OVERNIGHT CLOSURES WILL NOT BE ALLOWED.SEE RUNWAY CLOSED MARKER DETAIL SHEET CPP1.

# FR038



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### Revisions

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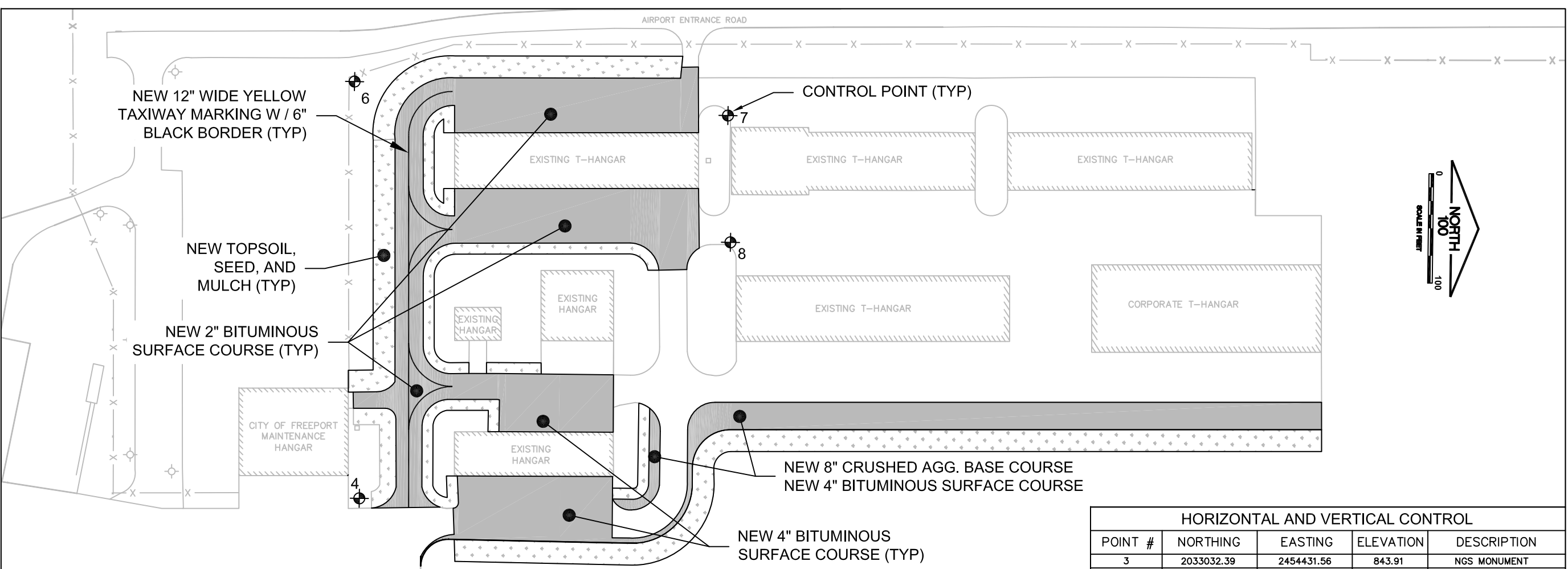
## CONSTRUCTION PHASING NOTES (CPN1)

# 4

SHEET 4 OF 22 SHEETS

# CONSTRUCTION PHASING NOTES





**FRO38**



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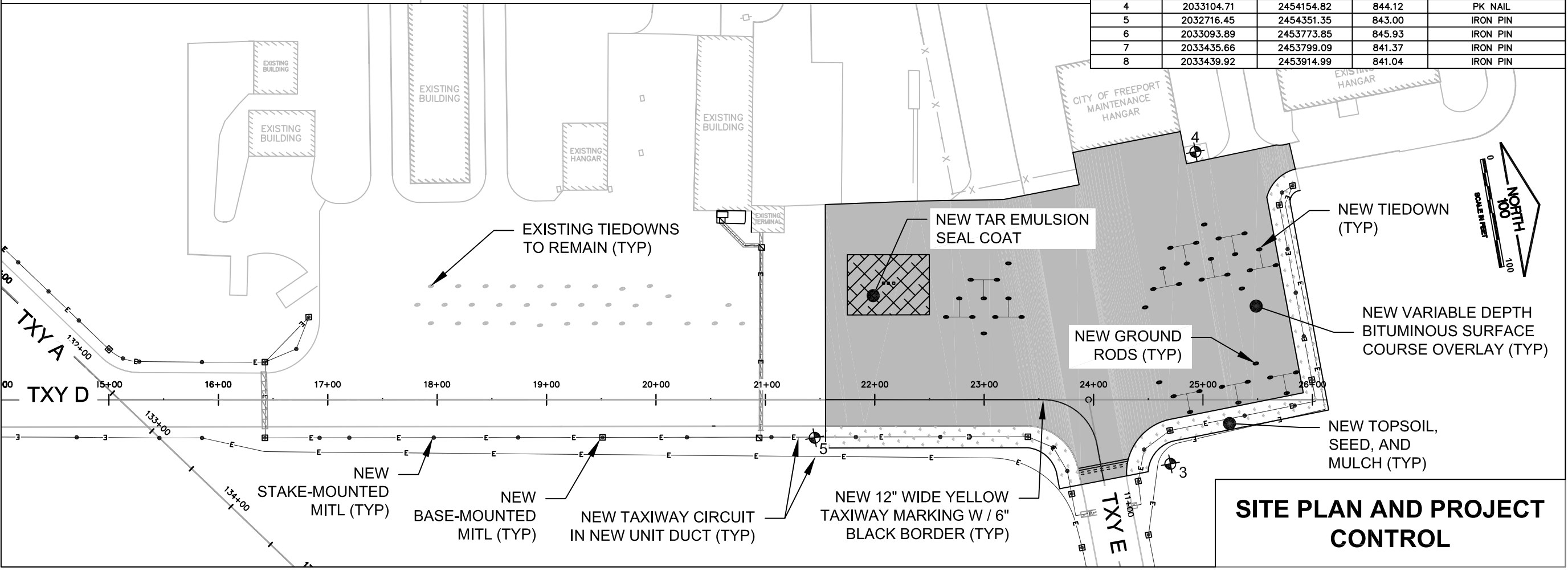
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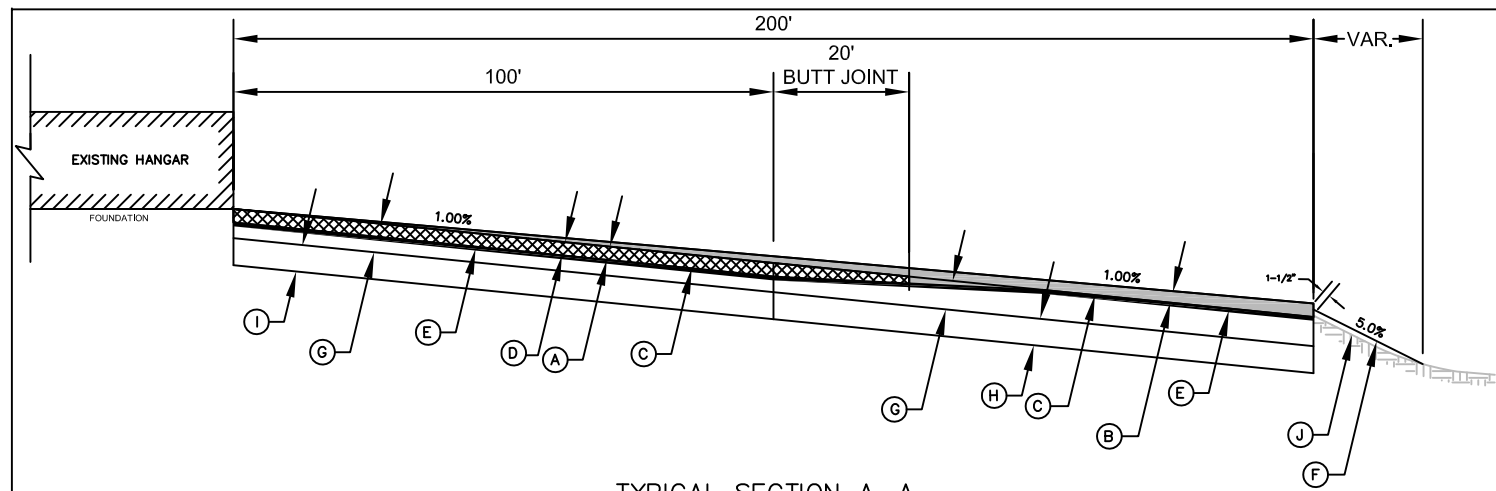
Date	Description

POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
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4	2033104.71	2454154.82	844.12	PK NAIL
5	2032716.45	2454351.35	843.00	IRON PIN
6	2033093.89	2453773.85	845.93	IRON PIN
7	2033435.66	2453799.09	841.37	IRON PIN
8	2033439.92	2453914.99	841.04	IRON PIN



**SITE PLAN AND PROJECT CONTROL**

**5**

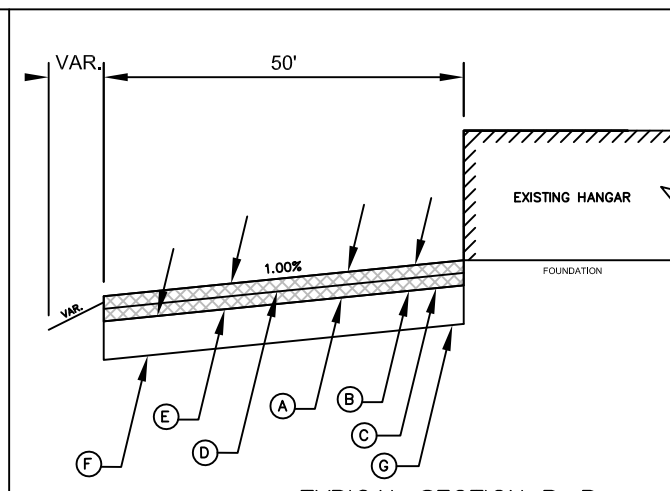


TYPICAL SECTION A-A  
(TERMINAL APRON)

TYPICAL SECTION A-A LEGEND

- |   |  |
|---|--|
| (A) PROPOSED 2-1 1/2" LIFTS BITUMINOUS SURFACE COURSE (401) | (F) PROPOSED TOPSOILING (905)                        |
| (B) PROPOSED VAR. DEPTH BITUMINOUS SURFACE COURSE (401)     | (G) EXISTING 5 1/2"-7 1/2" BITUMINOUS SURFACE COURSE |
| (C) PROPOSED BITUMINOUS TACK COAT (603)                     | (H) EXISTING 6" BITUMINOUS BASE COURSE               |
| (D) PROPOSED 3" BITUMINOUS MILLING (401)                    | (I) EXISTING 6" SOIL CEMENT BASE                     |
| (E) PROPOSED CRACK CONTROL FABRIC (201)                     | (J) EXISTING GROUND LINE                             |

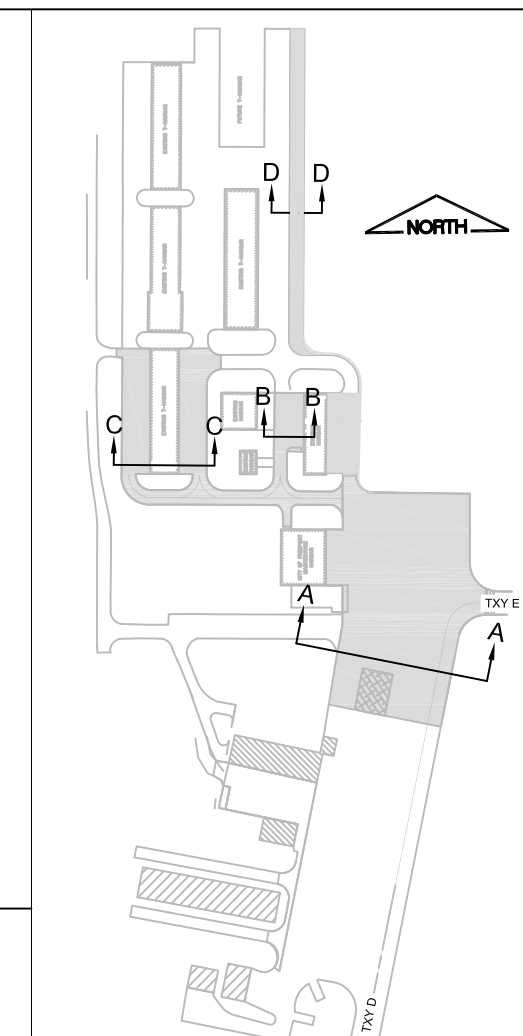
NOTE: A BITUMINOUS TACK COAT (603) IS REQUIRED BETWEEN ALL LIFTS OF BITUMINOUS SURFACE COURSE (401)



TYPICAL SECTION B-B  
(T-HANGAR TAXIWAYS)

TYPICAL SECTION LEGEND

- |   |
|---|
| (A) PROPOSED 2-2" LIFTS BITUMINOUS SURFACE COURSE (401) |
| (B) PROPOSED 4" BITUMINOUS PAVEMENT REMOVAL (401)       |
| (C) PROPOSED BITUMINOUS PRIME COAT (602)                |
| (D) PROPOSED BITUMINOUS TACK COAT (603)                 |
| (E) EXISTING 4" BITUMINOUS SURFACE COURSE               |
| (F) EXISTING 6" CRUSHED AGGREGATE BASE COURSE           |
| (G) EXISTING SUBGRADE                                   |



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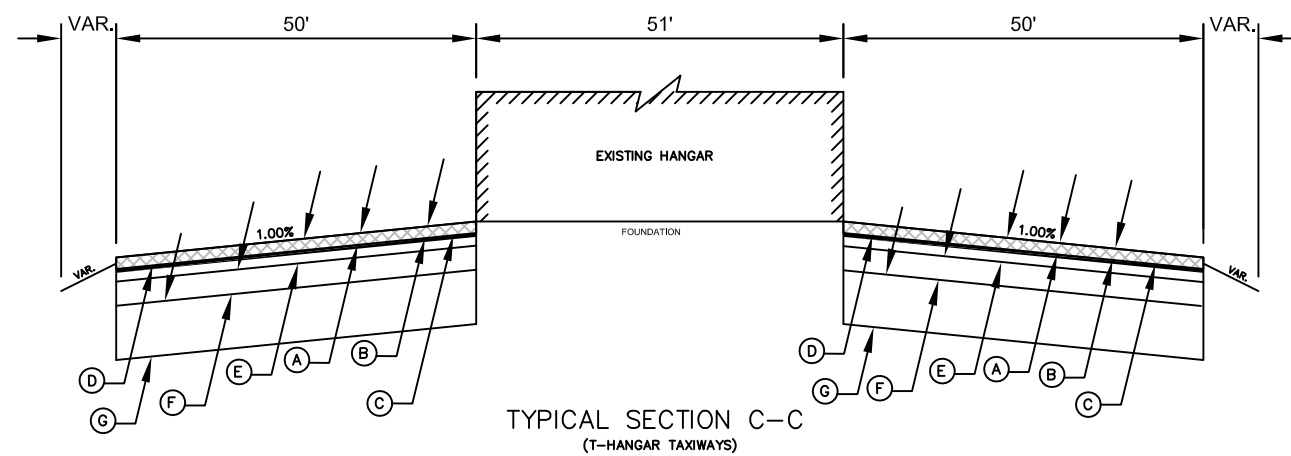
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APPROVED BY:	CMT-RFD
DATE:	3-4-2011
JOB No:	09294-02-00/06 08294-02-00/06

TYPICAL  
SECTIONS  
(TYP1)

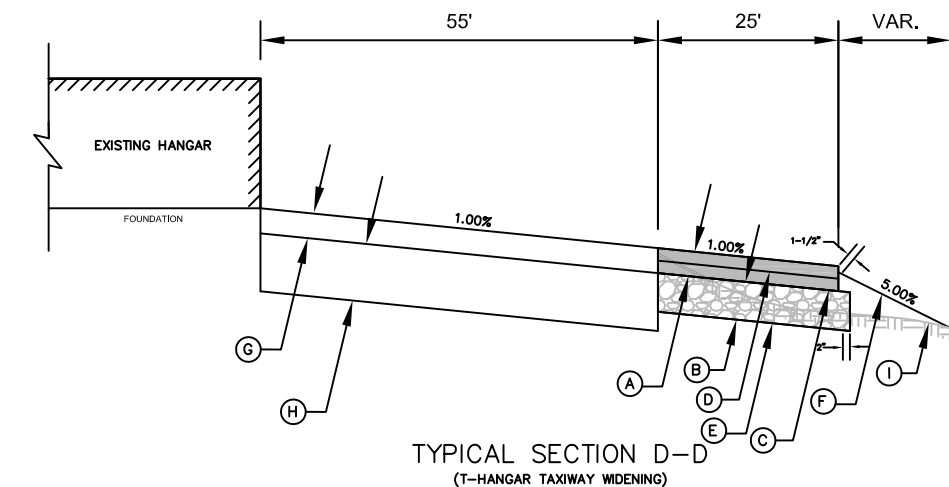
6



TYPICAL SECTION C-C  
(T-HANGAR TAXIWAYS)

TYPICAL SECTION LEGEND

- |   |   |
|---|---|
| (A) PROPOSED 2" BITUMINOUS SURFACE COURSE (401) | (E) EXISTING 4" BITUMINOUS SURFACE COURSE     |
| (B) PROPOSED 2" BITUMINOUS MILLING (401)        | (F) EXISTING 4" CRUSHED AGGREGATE BASE COURSE |
| (C) PROPOSED BITUMINOUS TACK COAT (603)         | (G) EXISTING 9" POROUS GRANULAR EMBANKMENT    |
| (D) PROPOSED CRACK CONTROL FABRIC (201)         |   |



TYPICAL SECTION D-D  
(T-HANGAR TAXIWAY WIDENING)



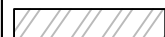
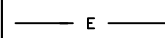
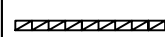
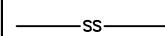
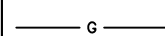
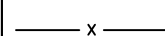




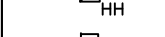


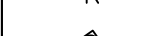
TYPICAL SECTION LEGEND

- |   |   |
|---|---|
| (A) PROPOSED 2-2" LIFTS BITUMINOUS SURFACE COURSE (401) | (F) PROPOSED TOPSOIL SHOULDER FILL            |
| (B) PROPOSED 8" CRUSHED AGGREGATE BASE COURSE (209)     | (G) EXISTING 4" BITUMINOUS SURFACE COURSE     |
| (C) PROPOSED BITUMINOUS PRIME COAT (602)                | (H) EXISTING 9" CRUSHED AGGREGATE BASE COURSE |
| (D) PROPOSED BITUMINOUS TACK COAT (603)                 | (I) EXISTING GROUND                           |
| (E) PROPOSED SOIL STABILIZATION FABRIC (152)            |   |

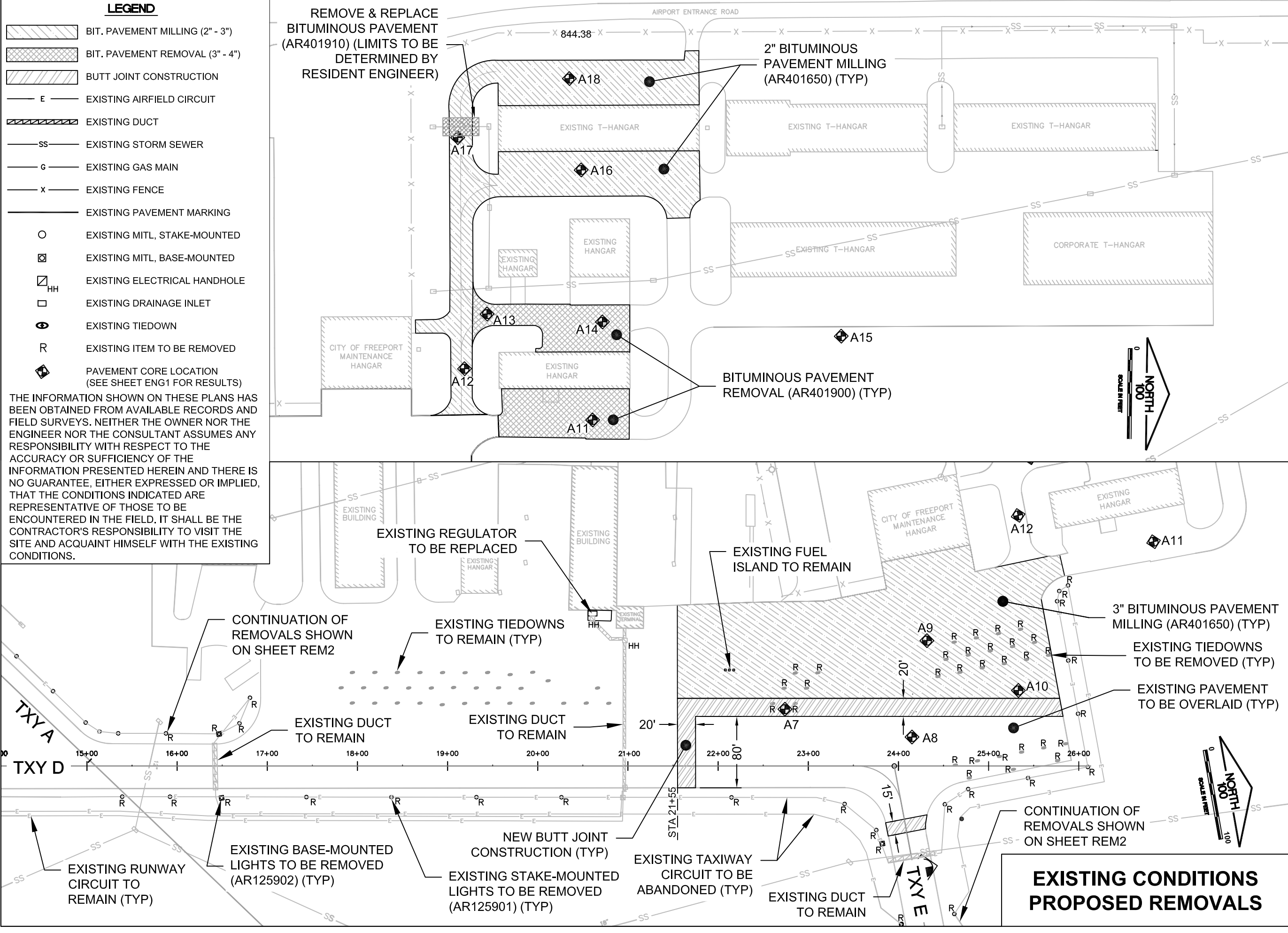
TYPICAL SECTIONS



**LEGEND**

-  BIT. PAVEMENT MILLING (2" - 3")
-  BIT. PAVEMENT REMOVAL (3" - 4")
-  BUTT JOINT CONSTRUCTION
-  EXISTING AIRFIELD CIRCUIT
-  EXISTING DUCT
-  EXISTING STORM SEWER
-  EXISTING GAS MAIN
-  EXISTING FENCE
-  EXISTING PAVEMENT MARKING
-  EXISTING MITL, STAKE-MOUNTED
-  EXISTING MITL, BASE-MOUNTED
-  EXISTING ELECTRICAL HANDHOLE
-  EXISTING DRAINAGE INLET
-  EXISTING TIEDOWN
-  EXISTING ITEM TO BE REMOVED
-  PAVEMENT CORE LOCATION (SEE SHEET ENG1 FOR RESULTS)

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER NOR THE CONSULTANT ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



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Rehab. North Terminal Apron and T-Hangar Taxiways

Rehab. Taxiway Electrical Circuit

Revisions

Date	Description

THIS BAR IS EQUAL TO 1" AT FULL SCALE (17 X 11).

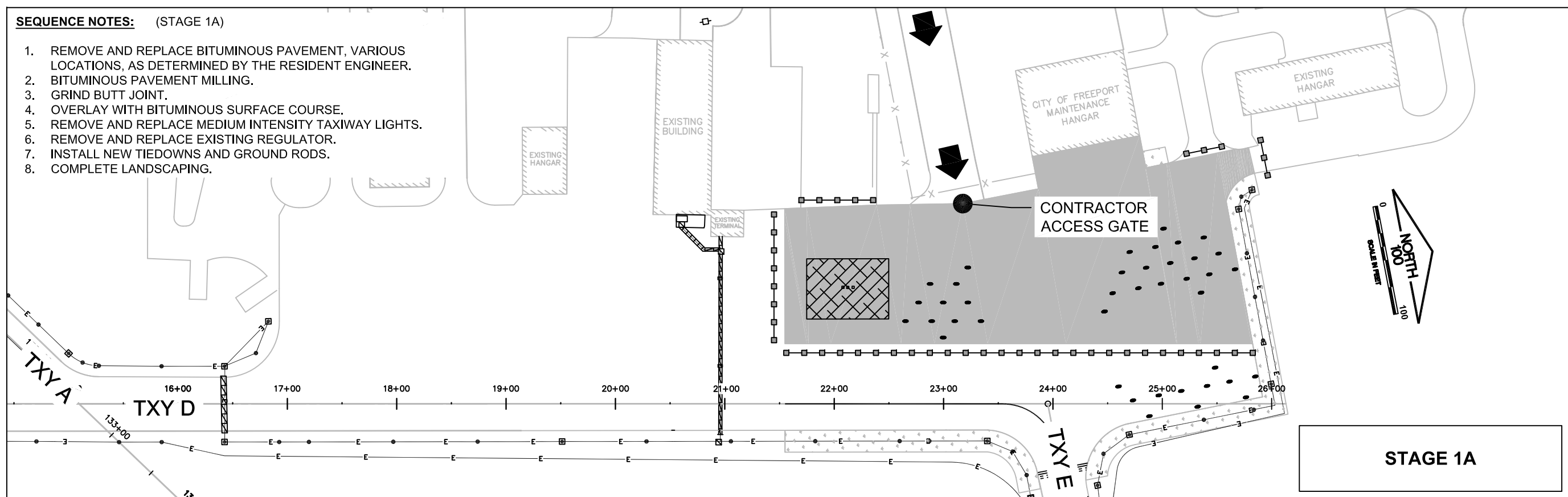
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**EXISTING CONDITIONS / PROPOSED REMOVALS (REM1)**

**7**

**SEQUENCE NOTES:** (STAGE 1A)

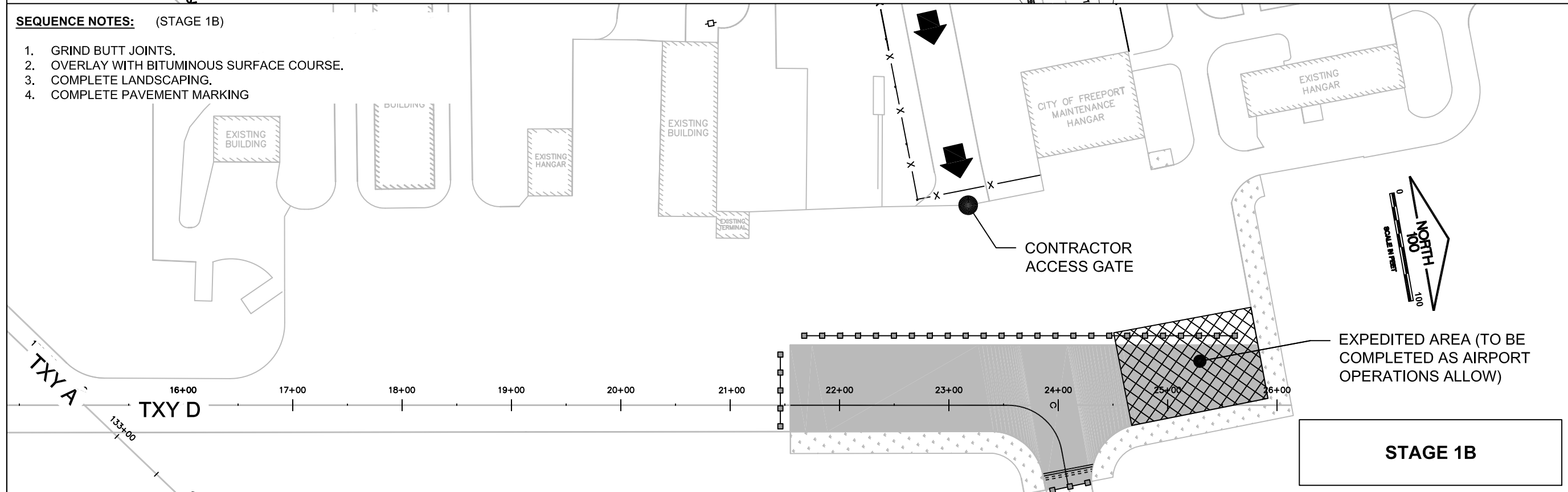
1. REMOVE AND REPLACE BITUMINOUS PAVEMENT, VARIOUS LOCATIONS, AS DETERMINED BY THE RESIDENT ENGINEER.
2. BITUMINOUS PAVEMENT MILLING.
3. GRIND BUTT JOINT.
4. OVERLAY WITH BITUMINOUS SURFACE COURSE.
5. REMOVE AND REPLACE MEDIUM INTENSITY TAXIWAY LIGHTS.
6. REMOVE AND REPLACE EXISTING REGULATOR.
7. INSTALL NEW TIEDOWNS AND GROUND RODS.
8. COMPLETE LANDSCAPING.



**STAGE 1A**

**SEQUENCE NOTES:** (STAGE 1B)

1. GRIND BUTT JOINTS.
2. OVERLAY WITH BITUMINOUS SURFACE COURSE.
3. COMPLETE LANDSCAPING.
4. COMPLETE PAVEMENT MARKING



EXPEDITED AREA (TO BE COMPLETED AS AIRPORT OPERATIONS ALLOW)

**STAGE 1B**

PHASE	WORK AREA	ALLOWABLE WORK PERIODS	AIRPORT OPERATIONAL RESTRICTIONS	APPROXIMATE SCHEDULE
1A	TERMINAL APRON - PAVEMENT MILLING AREA	7:00 A.M. - 7:00 P.M.	NO RESTRICTIONS	17 CALENDAR DAYS
1B	TERMINAL APRON - PAVEMENT OVERLAY AREA	7:00 A.M. - 7:00 P.M.	NO RESTRICTIONS	5 CALENDAR DAYS
				22 CAL DAYS TOTAL

**LEGEND**

- WORK AREA
- LIGHTED BARRICADES
- CONTRACTOR'S CONSTRUCTION ACCESS/HAUL ROAD
- AIRCRAFT MOVEMENT AREA

**CONSTRUCTION PHASING STAGE 1**

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**CONSTRUCTION PHASING STAGE 1 (CPS1)**

**8**





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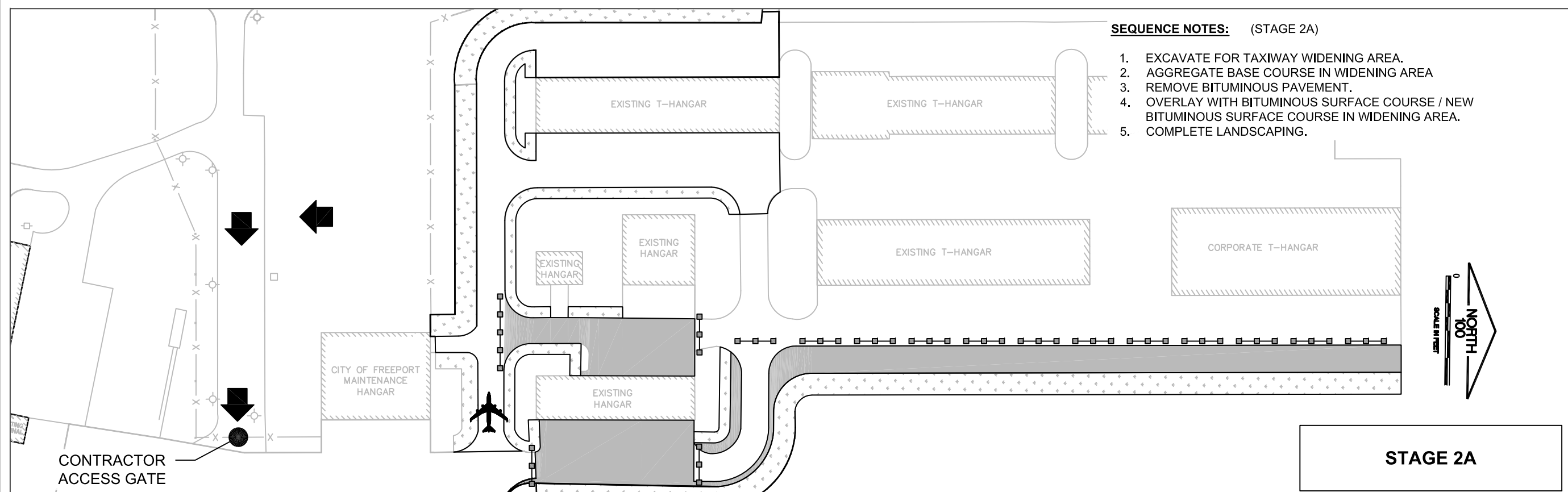
**CONSTRUCTION PHASING STAGE 2 (CPS2)**

**SEQUENCE NOTES: (STAGE 2A)**

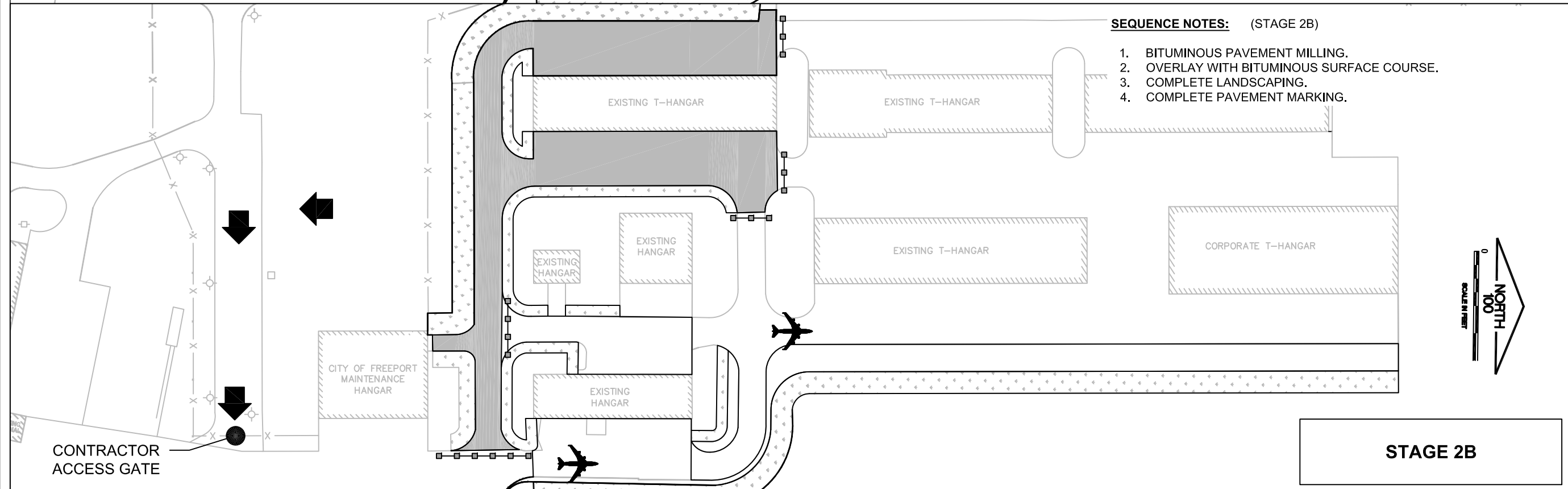
1. EXCAVATE FOR TAXIWAY WIDENING AREA.
2. AGGREGATE BASE COURSE IN WIDENING AREA
3. REMOVE BITUMINOUS PAVEMENT.
4. OVERLAY WITH BITUMINOUS SURFACE COURSE / NEW BITUMINOUS SURFACE COURSE IN WIDENING AREA.
5. COMPLETE LANDSCAPING.

**SEQUENCE NOTES: (STAGE 2B)**

1. BITUMINOUS PAVEMENT MILLING.
2. OVERLAY WITH BITUMINOUS SURFACE COURSE.
3. COMPLETE LANDSCAPING.
4. COMPLETE PAVEMENT MARKING.



STAGE 2A



STAGE 2B

PHASE	WORK AREA	ALLOWABLE WORK PERIODS	AIRPORT OPERATIONAL RESTRICTIONS	APPROXIMATE SCHEDULE
2A	T-HANGAR TAXIWAYS - FULL DEPTH PAVEMENT REMOVAL AREA / TAXIWAY WIDENING AREA	7:00 A.M. - 7:00 P.M.	NO RESTRICTIONS	17 CALENDAR DAYS
2B	T-HANGAR TAXIWAYS - PAVEMENT MILLING AREA	7:00 A.M. - 7:00 P.M.	NO RESTRICTIONS	7 CALENDAR DAYS
				24 CAL DAYS TOTAL

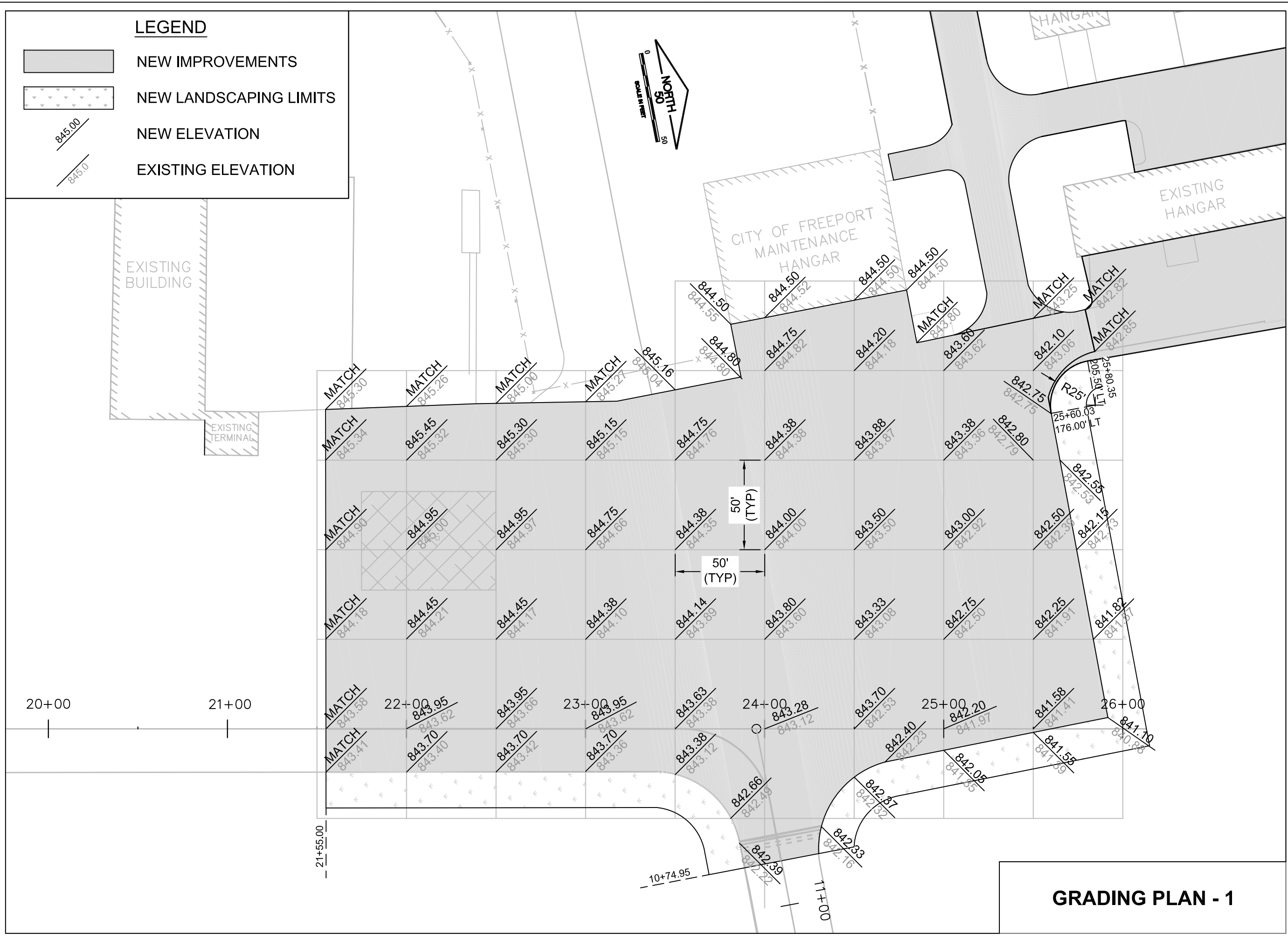
**LEGEND**

- WORK AREA
- LIGHTED BARRICADES
- CONTRACTOR'S CONSTRUCTION ACCESS/HAUL ROAD
- AIRCRAFT MOVEMENT AREA

**CONSTRUCTION PHASING STAGE 2**

**LEGEND**

- NEW IMPROVEMENTS
- NEW LANDSCAPING LIMITS
- NEW ELEVATION
- EXISTING ELEVATION



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**GRADING PLAN - 1**

**GRADING  
PLAN - 1  
(GRD1)**

**10**



AIRPORT ENTRANCE ROAD

**LEGEND**



NEW IMPROVEMENTS



NEW LANDSCAPING LIMITS



NEW ELEVATION



EXISTING ELEVATION

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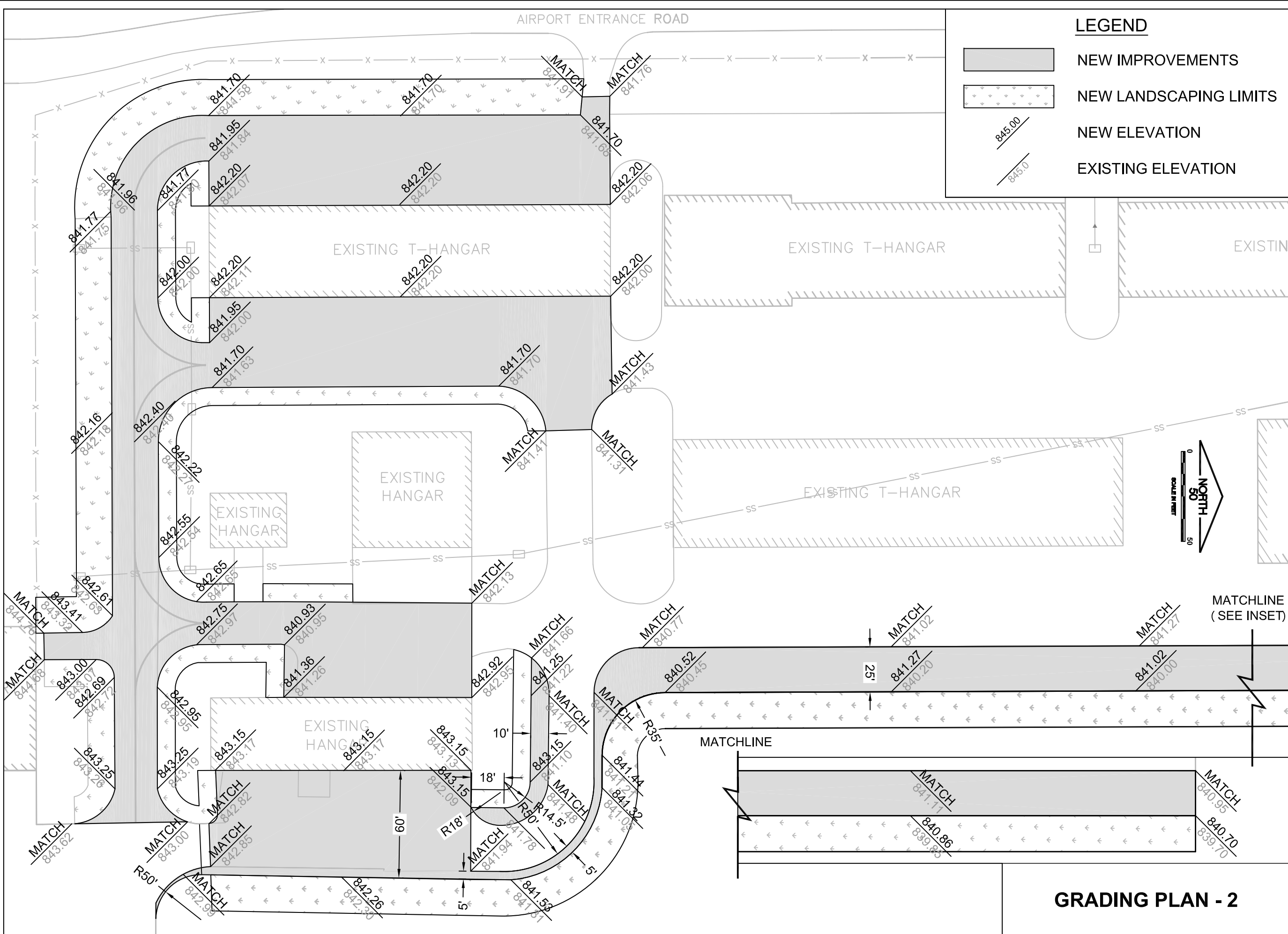
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**GRADING  
PLAN - 2  
(GRD2)**

**11**

SHEET 11 OF 22 SHEETS



**GRADING PLAN - 2**



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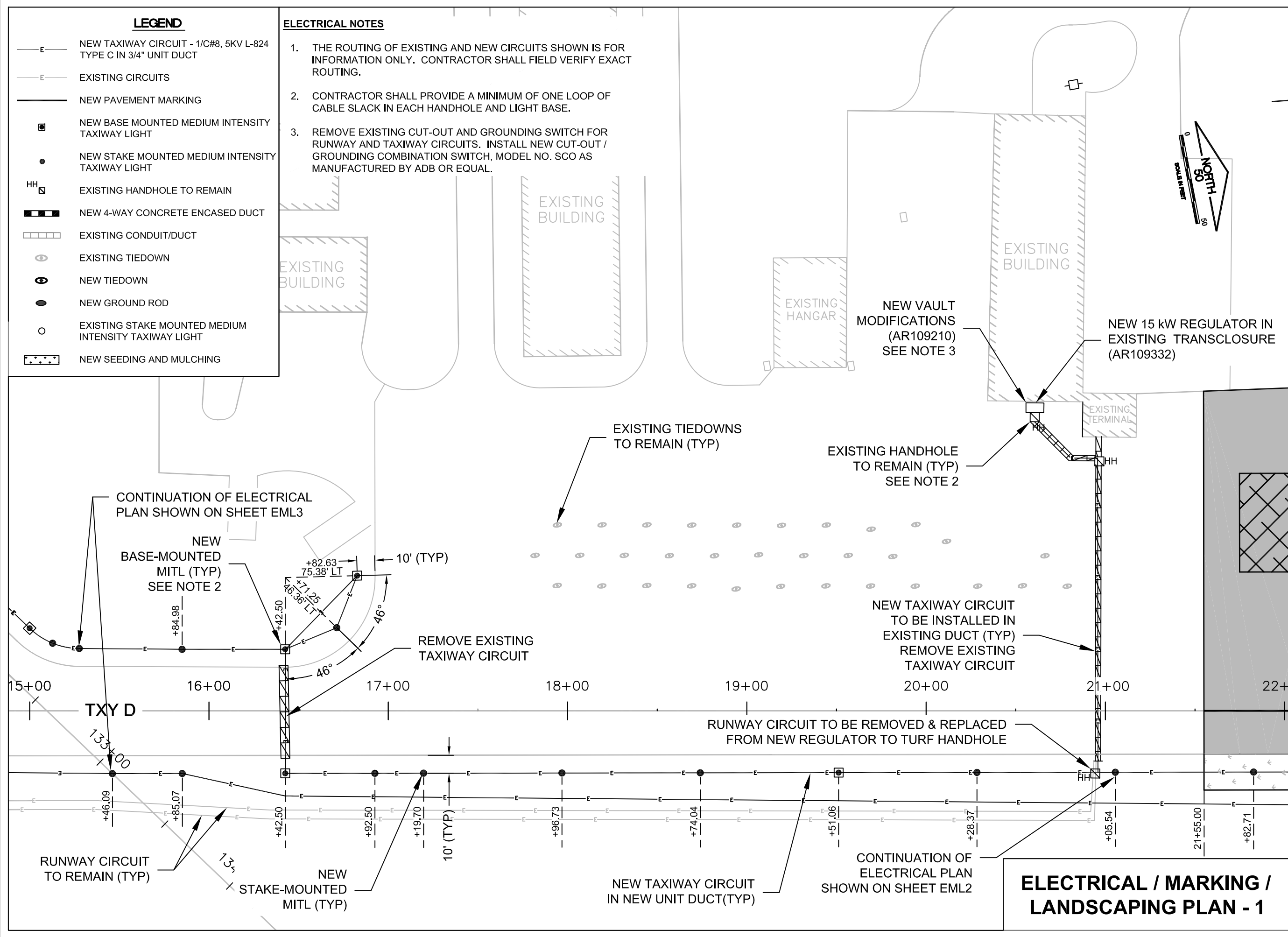
ELECTRICAL /  
MARKING /  
LANDSCAPING  
PLAN - 1  
(EML1)

LEGEND

- E— NEW TAXIWAY CIRCUIT - 1/C#8, 5KV L-824 TYPE C IN 3/4" UNIT DUCT
- E— EXISTING CIRCUITS
- NEW PAVEMENT MARKING
- NEW BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- NEW STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- HH □ EXISTING HANDHOLE TO REMAIN
- NEW 4-WAY CONCRETE ENCASED DUCT
- EXISTING CONDUIT/DUCT
- EXISTING TIEDOWN
- ⊙ NEW TIEDOWN
- NEW GROUND ROD
- EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- NEW SEEDING AND MULCHING

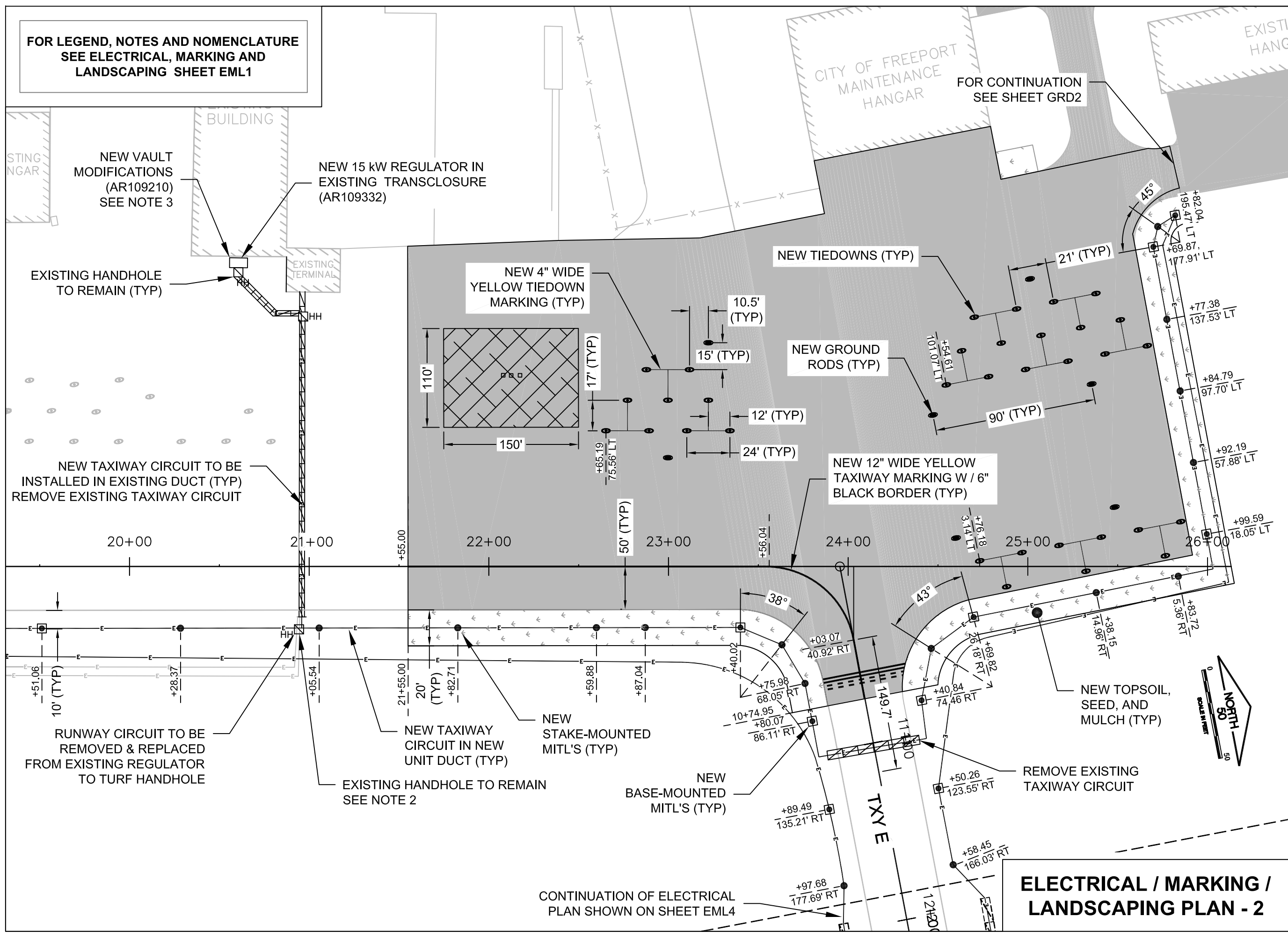
ELECTRICAL NOTES

1. THE ROUTING OF EXISTING AND NEW CIRCUITS SHOWN IS FOR INFORMATION ONLY. CONTRACTOR SHALL FIELD VERIFY EXACT ROUTING.
2. CONTRACTOR SHALL PROVIDE A MINIMUM OF ONE LOOP OF CABLE SLACK IN EACH HANDHOLE AND LIGHT BASE.
3. REMOVE EXISTING CUT-OUT AND GROUNDING SWITCH FOR RUNWAY AND TAXIWAY CIRCUITS. INSTALL NEW CUT-OUT / GROUNDING COMBINATION SWITCH, MODEL NO. SCO AS MANUFACTURED BY ADB OR EQUAL.



ELECTRICAL / MARKING /  
LANDSCAPING PLAN - 1

FOR LEGEND, NOTES AND NOMENCLATURE  
SEE ELECTRICAL, MARKING AND  
LANDSCAPING SHEET EML1



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**ELECTRICAL /  
MARKING /  
LANDSCAPING  
PLAN - 2  
(EML2)**

**13**

SHEET 13 OF 22 SHEETS





**Freeport - Albertus Airport**



**Freeport, Illinois**

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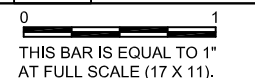
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**Rehab. North  
Terminal Apron and  
T-Hangar Taxiways**

**Rehab. Taxiway  
Electrical Circuit**

**Revisions**

Date	Description



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**ENGINEERING  
INFORMATION  
(ENG1)**

Pavement Core No.	Location	Pavement Material	Thickness, inches
A-13	Middle South T-Hangar, East Side Taxiway, South End	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	4.5  3.5
A-14	Middle South T-Hangar, East Side Taxiway, North End	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	3.0  6
A-15	Northeast T-Hangar Taxiway, South Half	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	4.2  9.3
A-16	Southwest T-Hangar, East Side Taxiway	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	4.2  13.8
A-17	South T-Hangar Taxiway, West End	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	4.0  12

Pavement Core No.	Location	Pavement Material	Thickness, inches
A-18	Southwest T-Hangar, West Side Taxiway	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	5.2  11.8

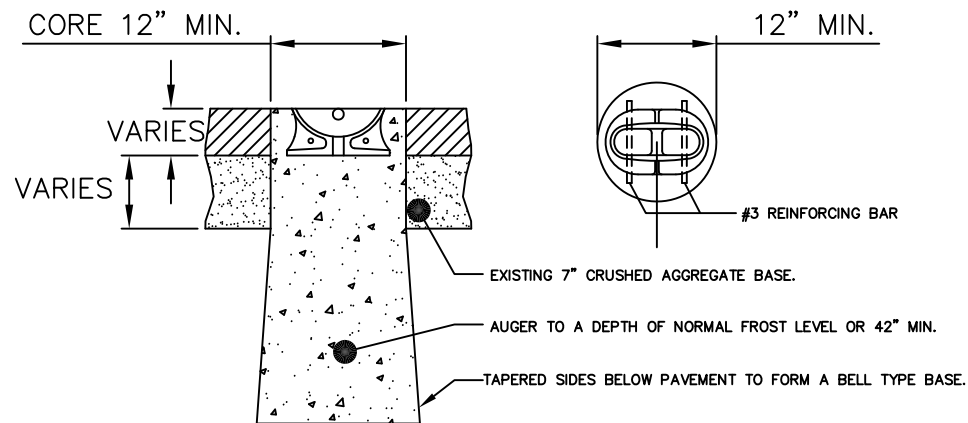
Pavement Core No.	Location	Pavement Material	Thickness, inches
A-8	Terminal Apron, North Half	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	4.2  11.8
A-9	Terminal Apron, North Half, North End	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	5.2  7.8
A-10	Terminal Apron, North Half	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	4.6  5.4
A-11	Southeast T-Hangar Taxiway, East Side, North End	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	2.8  5.2
A-12	South T-Hangar Taxiway, East End	Hot-Mix Asphalt Surface Course  Crushed Stone Granular Base Course	3.8  4.2

**NOTE: SEE SHEET REM1 FOR PAVEMENT CORE LOCATIONS**

Pavement Core No.	Location	Pavement Material	Thickness, inches
A-1	South End of Terminal Apron	Hot-Mix Asphalt Surface & Binder Courses  No Granular Base Course Encountered	8.1  -
A-2	Terminal Apron, South Half	Hot-Mix Asphalt Surface Course  No Granular Base Course Encountered	5.8  -

Pavement Core No.	Location	Pavement Material	Thickness, inches
A-3	Terminal Apron, South Half	Hot-Mix Asphalt Surface & Binder Courses  Crushed Stone Granular Base Course	8.2  5.8
A-4	Terminal Apron, South Half	Hot-Mix Asphalt Surface Course  No Granular Base Course Encountered	5.5  -
A-5	Terminal Apron, South Half, West End	Hot-Mix Asphalt Surface & Binder Courses  No Granular Base Course Encountered	6.2  -
A-6	Terminal Apron, North Half	Hot-Mix Asphalt Surface & Binder Courses  No Granular Base Course Encountered	8.0  -
A-7	Terminal Apron, North Half	Hot-Mix Asphalt Surface Course  No Granular Base Course Encountered	9.0  -

**ENGINEERING INFORMATION**

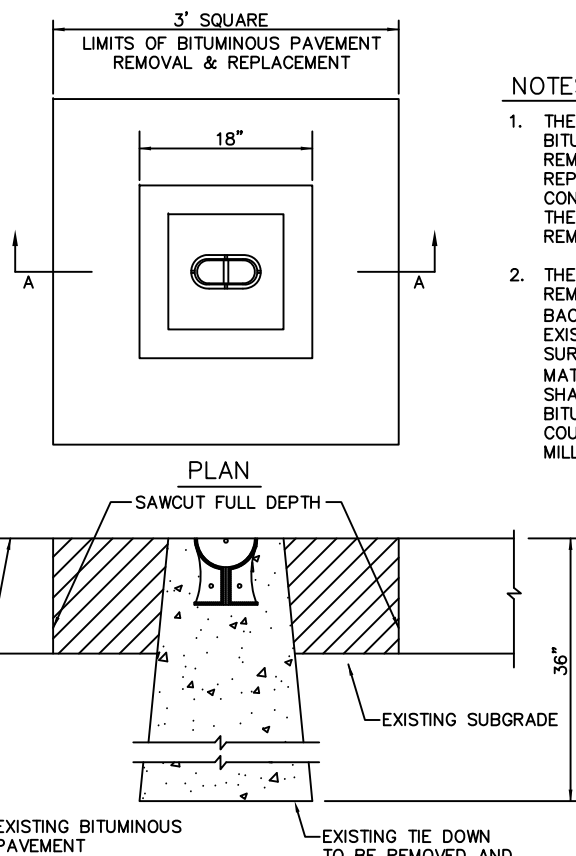


**MOORING EYE DETAIL**

N.T.S.

**NOTES**

- 1.) MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
- 2.) ANCHOR RODS SHALL BE #3 DEFORMED REBAR, 15" LONG AND SHALL BE BENT DOWNWARD AT 45°.
- 3.) MOORING EYES IN PROPOSED P.C.C. PAVEMENT SHALL BE INSTALLED DURING THE PAVING OPERATION WHILE CONCRETE IS STILL PLASTIC.
- 4.) FINISH SHALL SHOW NO VOIDS AND HAVE A BROOM FINISH.

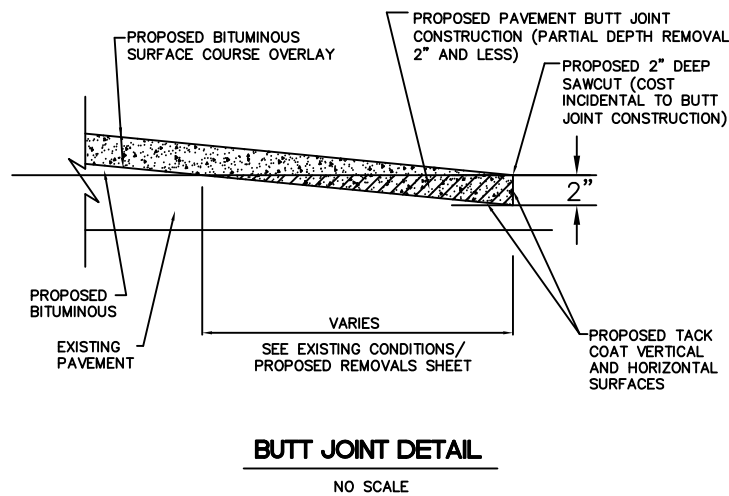


**TIE DOWN REMOVAL DETAIL**

NOT TO SCALE

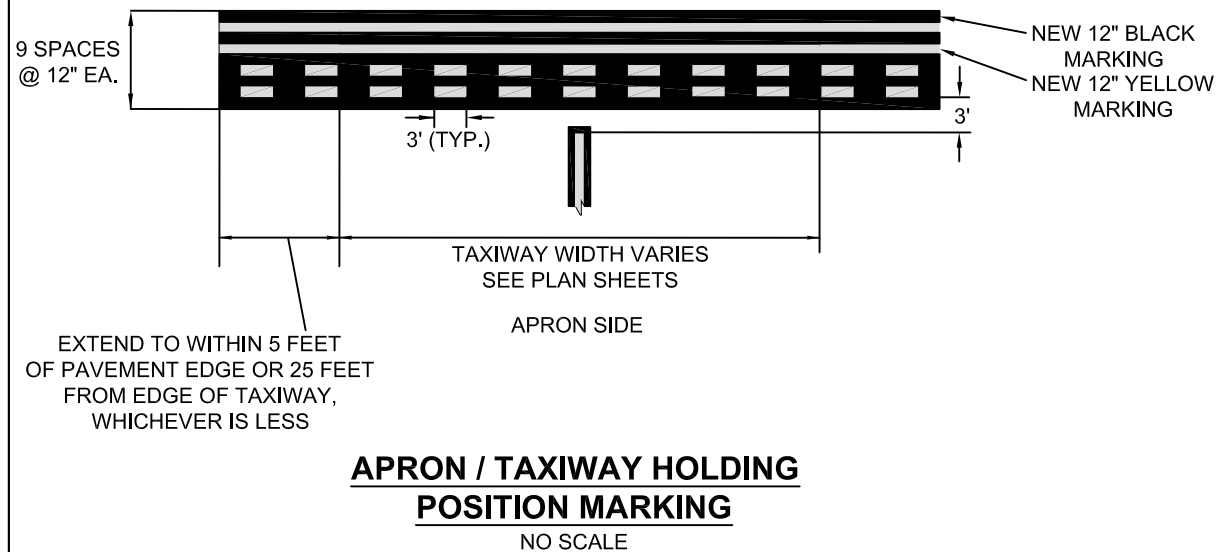
**NOTES:**

1. THE SAWCUT AND BITUMINOUS PAVEMENT REMOVAL AND REPLACEMENT SHALL BE CONSIDERED INCIDENTAL TO THE MOORING EYE REMOVAL.
2. THE VOID LEFT BY THE REMOVAL SHALL BE BACKFILLED TO 9" BELOW EXISTING PAVEMENT SURFACE WITH CLSM MATERIAL. THE FINAL 9" SHALL BE FILLED WITH BITUMINOUS SURFACE COURSE TO MATCH THE MILLED SURFACE.



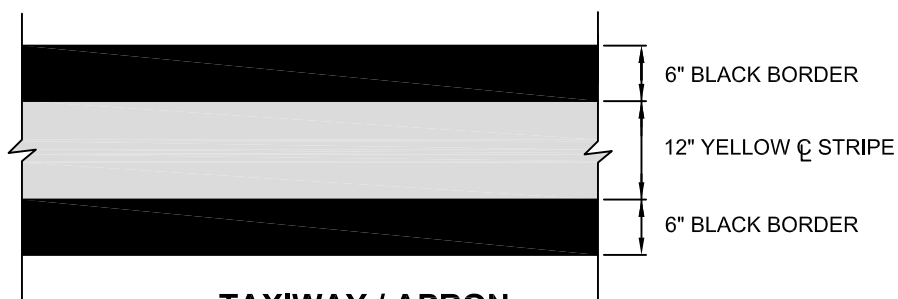
**BUTT JOINT DETAIL**

NO SCALE



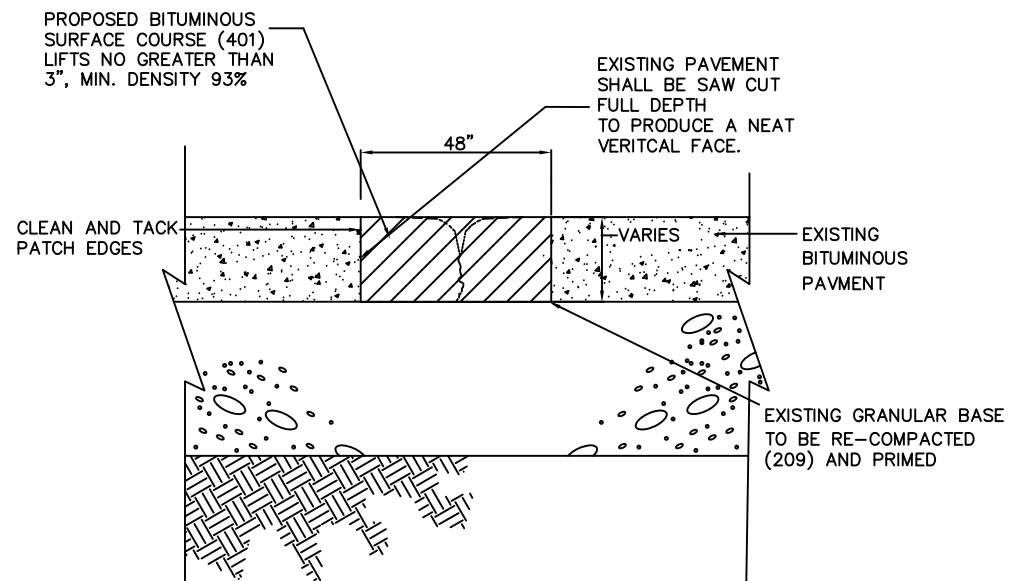
**APRON / TAXIWAY HOLDING POSITION MARKING**

NO SCALE



**TAXIWAY / APRON CENTERLINE MARKING DETAIL**

NO SCALE



**PROPOSED REPAIR METHOD FOR FULL DEPTH PATCHING (REMOVE AND REPLACE BITUMINOUS PAVEMENT)**

NO SCALE

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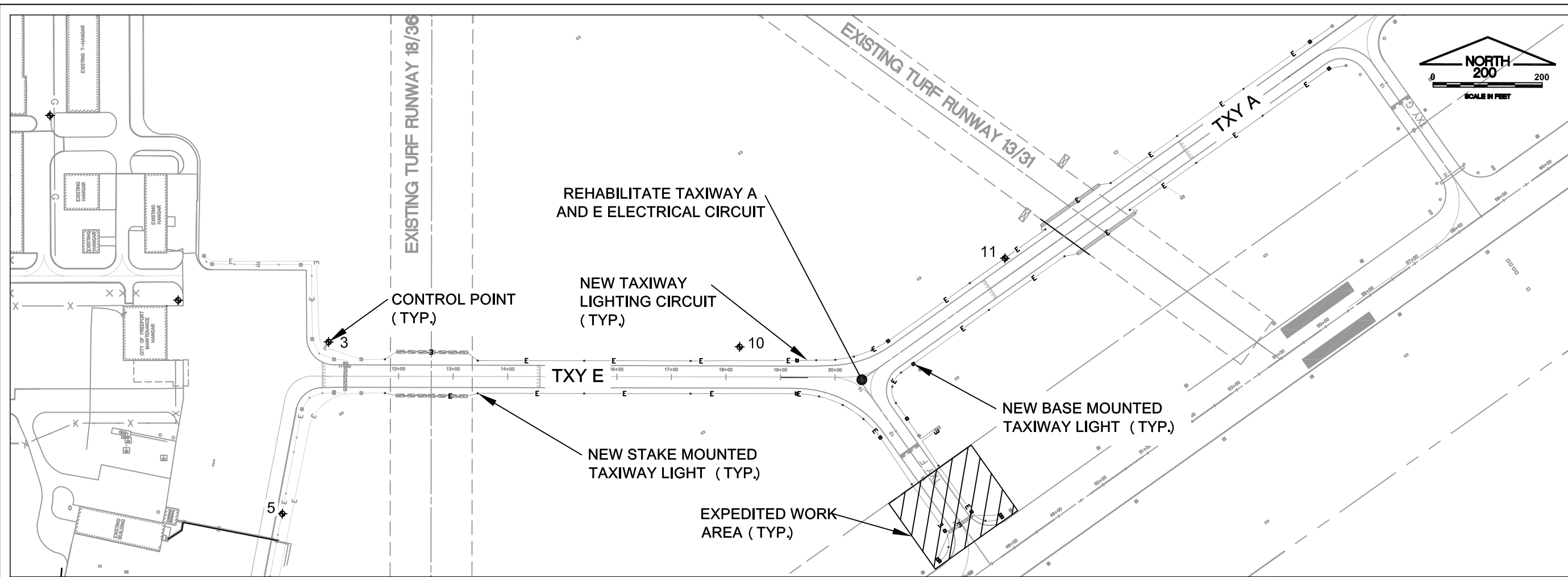
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MISCELLANEOUS DETAILS (DTL1)

15

**MISCELLANEOUS DETAILS**



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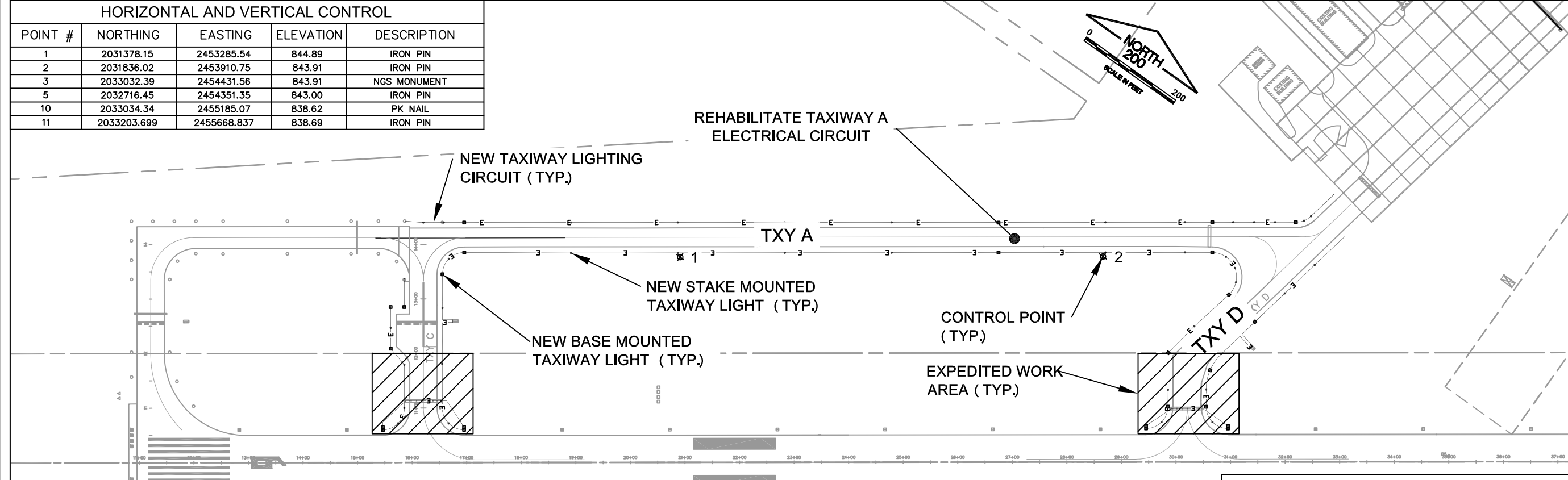
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Rehab. Taxiway Electrical Circuit

HORIZONTAL AND VERTICAL CONTROL				
POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	2031378.15	2453285.54	844.89	IRON PIN
2	2031836.02	2453910.75	843.91	IRON PIN
3	2033032.39	2454431.56	843.91	NGS MONUMENT
5	2032716.45	2454351.35	843.00	IRON PIN
10	2033034.34	2455185.07	838.62	PK NAIL
11	2033203.699	2455668.837	838.69	IRON PIN



Revisions

Date	Description

0 1  
THIS BAR IS EQUAL TO 1" AT FULL SCALE (17 X 11).

DESIGN BY:	CMT-RFD
DRAWN BY:	CMT-RFD
CHECKED BY:	CMT-RFD
APPROVED BY:	CMT-RFD
DATE:	3-4-2011
JOB No:	09294-02-00/06 08294-02-00/06

SITE PLAN AND PROJECT CONTROL (SPL2)

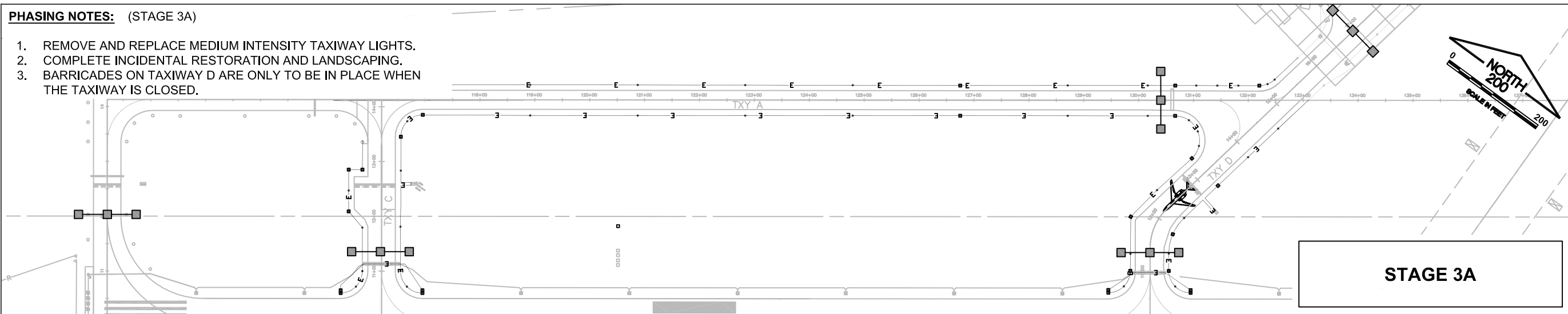
16

SITE PLAN AND PROJECT CONTROL



**PHASING NOTES:** (STAGE 3A)

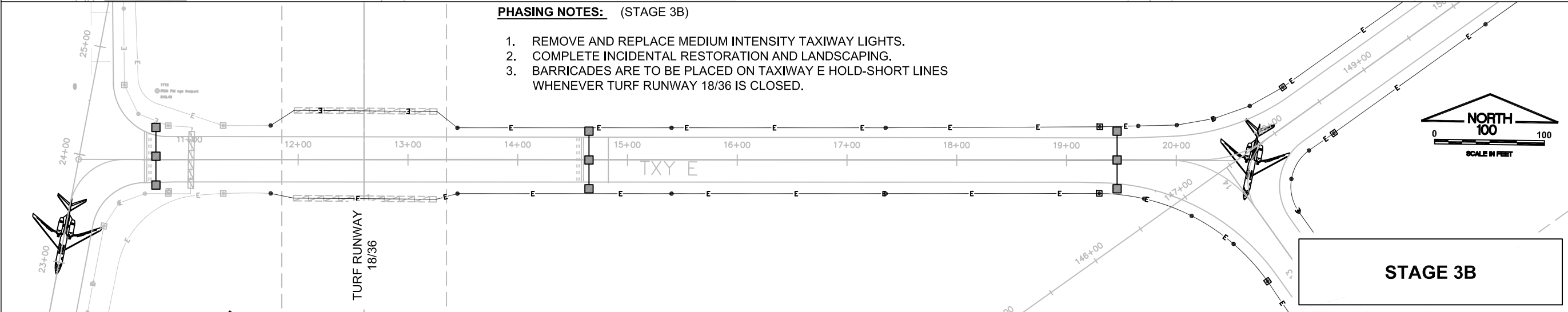
1. REMOVE AND REPLACE MEDIUM INTENSITY TAXIWAY LIGHTS.
2. COMPLETE INCIDENTAL RESTORATION AND LANDSCAPING.
3. BARRICADES ON TAXIWAY D ARE ONLY TO BE IN PLACE WHEN THE TAXIWAY IS CLOSED.



**STAGE 3A**

**PHASING NOTES:** (STAGE 3B)

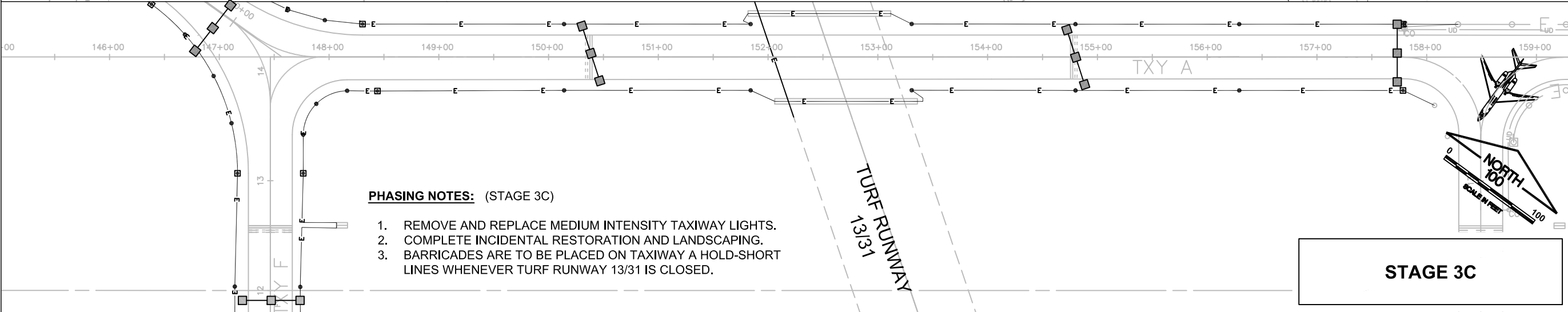
1. REMOVE AND REPLACE MEDIUM INTENSITY TAXIWAY LIGHTS.
2. COMPLETE INCIDENTAL RESTORATION AND LANDSCAPING.
3. BARRICADES ARE TO BE PLACED ON TAXIWAY E HOLD-SHORT LINES WHENEVER TURF RUNWAY 18/36 IS CLOSED.



**STAGE 3B**

**PHASING NOTES:** (STAGE 3C)

1. REMOVE AND REPLACE MEDIUM INTENSITY TAXIWAY LIGHTS.
2. COMPLETE INCIDENTAL RESTORATION AND LANDSCAPING.
3. BARRICADES ARE TO BE PLACED ON TAXIWAY A HOLD-SHORT LINES WHENEVER TURF RUNWAY 13/31 IS CLOSED.



**STAGE 3C**

PHASE	WORK AREA	ALLOWABLE WORK PERIODS	AIRPORT OPERATIONAL RESTRICTIONS	APPROXIMATE SCHEDULE
3	TAXIWAY A FROM CONNECTOR C TO TAXIWAY D	DAILY CLOSURES OF TAXIWAY "A" WILL BE PERMITTED 7:00 A.M. - 7:00 P.M.	NO RESTRICTIONS	7 CALENDAR DAYS
3A	TAXIWAY E FROM THE TERMINAL APRON TO CONNECTOR TAXIWAY F	DAILY CLOSURES OF TAXIWAY "E" AND TURF RUNWAY 18/36 WILL BE PERMITTED 7:00 A.M. - 7:00 P.M.	NO RESTRICTIONS	7 CALENDAR DAYS
3B	TAXIWAY A FROM CONNECTOR TAXIWAY F TO CONNECTOR TAXIWAY G	DAILY CLOSURES OF TAXIWAY "A" AND TURF RUNWAY 13/31 WILL BE PERMITTED 7:00 A.M. - 7:00 P.M.	NO RESTRICTIONS	7 CALENDAR DAYS
				21 CAL DAYS TOTAL

**PHASING NOTES:**

1. CONTRACTOR STAGING AREA SHALL BE LOCATED ON SHEET CPP1. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE STAGING AREA AND SITE ACCESS ROAD IN ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONTRACTOR SHALL PROVIDE RESIDENT ENGINEER WITH APPROVED PROGRESS SCHEDULE SHOWING START/ STOP DATES OF PROPOSED CONSTRUCTION. APPROVED PROGRESS SCHEDULE SHALL BE SUBMITTED 5 WORKING DAYS PRIOR TO START OF CONSTRUCTION. RESIDENT ENGINEER SHALL COORDINATE NOTAM'S WITH AIRPORT MANAGER.
3. ACCESS TO EXISTING HANGARS MUST BE MAINTAINED AT ALL TIMES. SEE INDIVIDUAL STAGING SHEETS FOR PHASING BARRICADE PLACEMENT. COST OF BARRICADE'S, BARRICADE REMOVAL AND REPLACEMENT FOR THE INDIVIDUAL STAGES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

**LEGEND**

- WORK AREA
- LIMITS OF PROPOSED WORK
- LIGHTED BARRICADES
- AIRCRAFT OPERATIONS AREA (A.O.A.)
- AIRCRAFT MOVEMENT AREA

**CONSTRUCTION PHASING  
STAGE 3**

**FR038**



Freeport - Albertus Airport



Freeport, Illinois

City of Freeport

AIP: 3-17-0045-B23  
FEP-3997

Rehab. North  
Terminal Apron and  
T-Hangar Taxiways

Rehab. Taxiway  
Electrical Circuit

Revisions

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**CONSTRUCTION  
PHASING  
STAGE 3  
(CPS3)**

**17**









Freeport - Albertus Airport



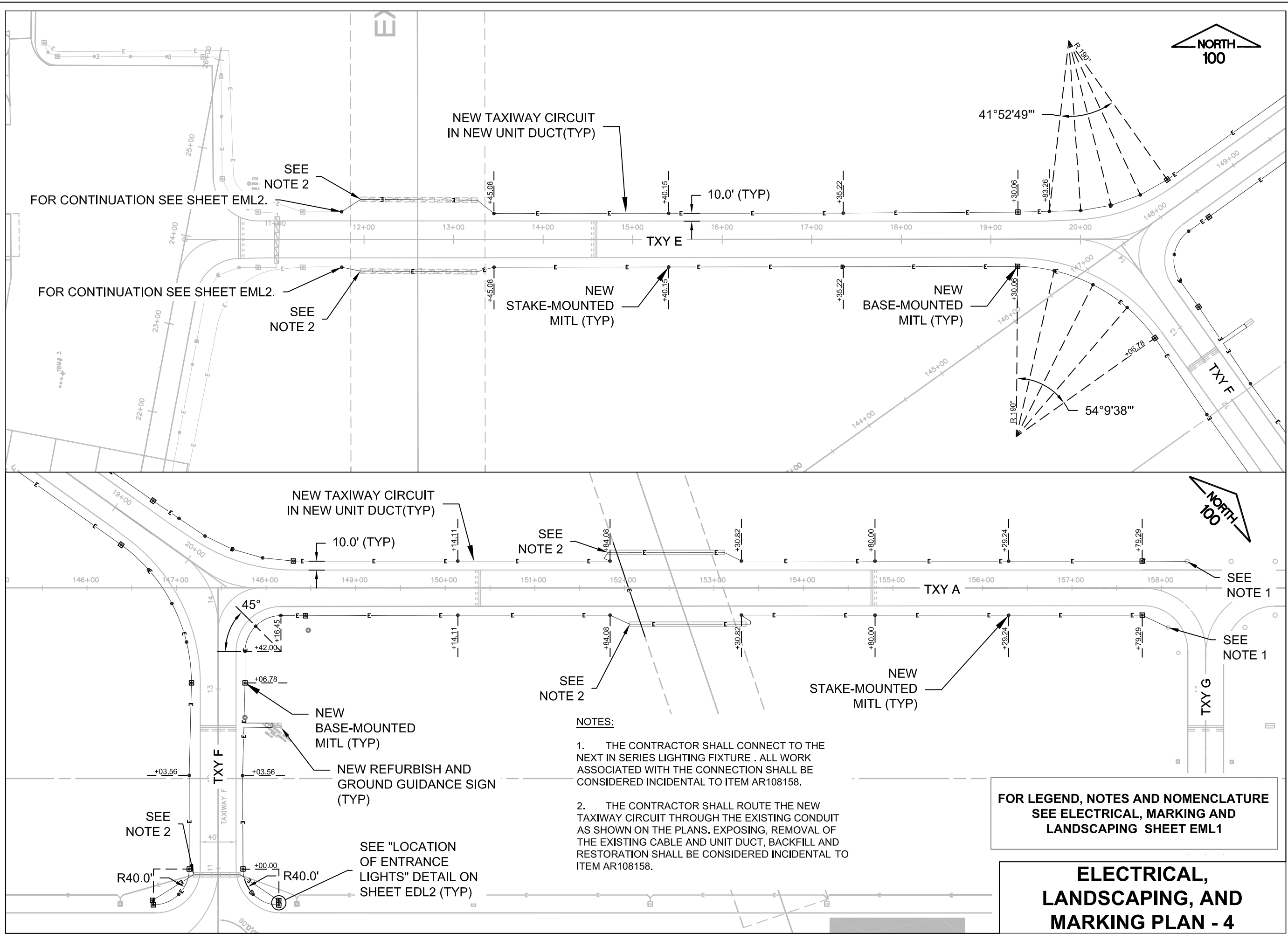
Freeport, Illinois

City of Freeport

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Rehab. North Terminal Apron and T-Hangar Taxiways

Rehab. Taxiway Electrical Circuit



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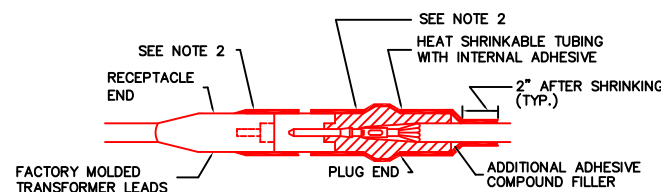
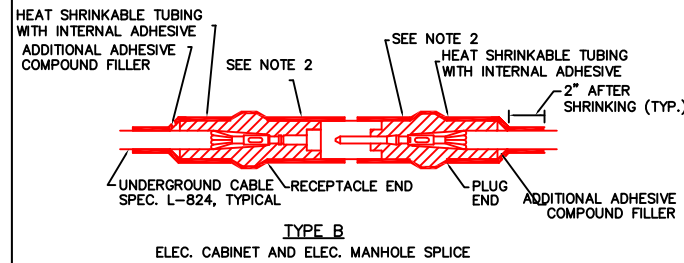
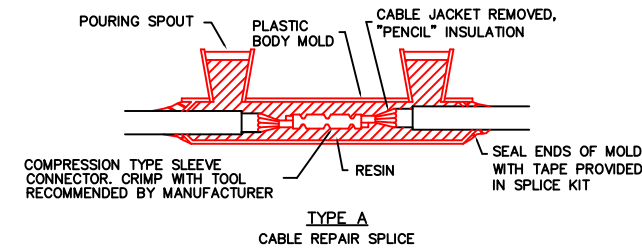
**ELECTRICAL, MARKING, AND LANDSCAPING PLAN - 4 (EML4)**

**NOTES:**

1. THE CONTRACTOR SHALL CONNECT TO THE NEXT IN SERIES LIGHTING FIXTURE. ALL WORK ASSOCIATED WITH THE CONNECTION SHALL BE CONSIDERED INCIDENTAL TO ITEM AR108158.
2. THE CONTRACTOR SHALL ROUTE THE NEW TAXIWAY CIRCUIT THROUGH THE EXISTING CONDUIT AS SHOWN ON THE PLANS. EXPOSING, REMOVAL OF THE EXISTING CABLE AND UNIT DUCT, BACKFILL AND RESTORATION SHALL BE CONSIDERED INCIDENTAL TO ITEM AR108158.

**FOR LEGEND, NOTES AND NOMENCLATURE SEE ELECTRICAL, MARKING AND LANDSCAPING SHEET EML1**

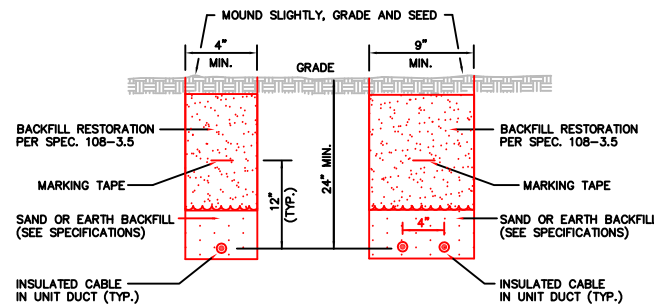
**ELECTRICAL, LANDSCAPING, AND MARKING PLAN - 4**



**CABLE SPLICES**  
NOT TO SCALE

**NOTES**

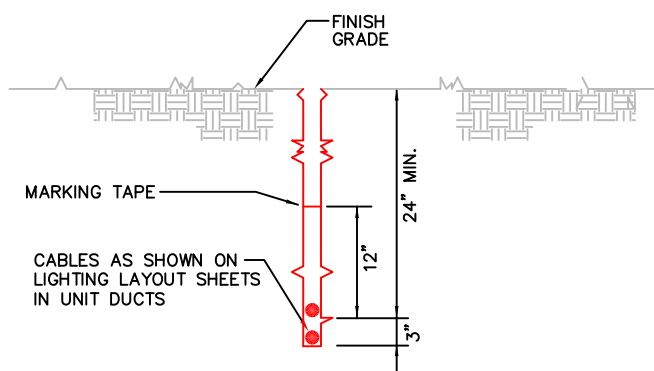
- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE OF THE CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINK TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE.
- SPLICE CAN SHALL BE REQUIRED FOR ALL TYPE A SPLICE LOCATIONS. THE COST SHALL BE INCIDENTAL TO THE CABLE REPAIR.



**TRENCH DETAIL FOR CABLE IN UNIT DUCT**  
NOT TO SCALE

**NOTES**

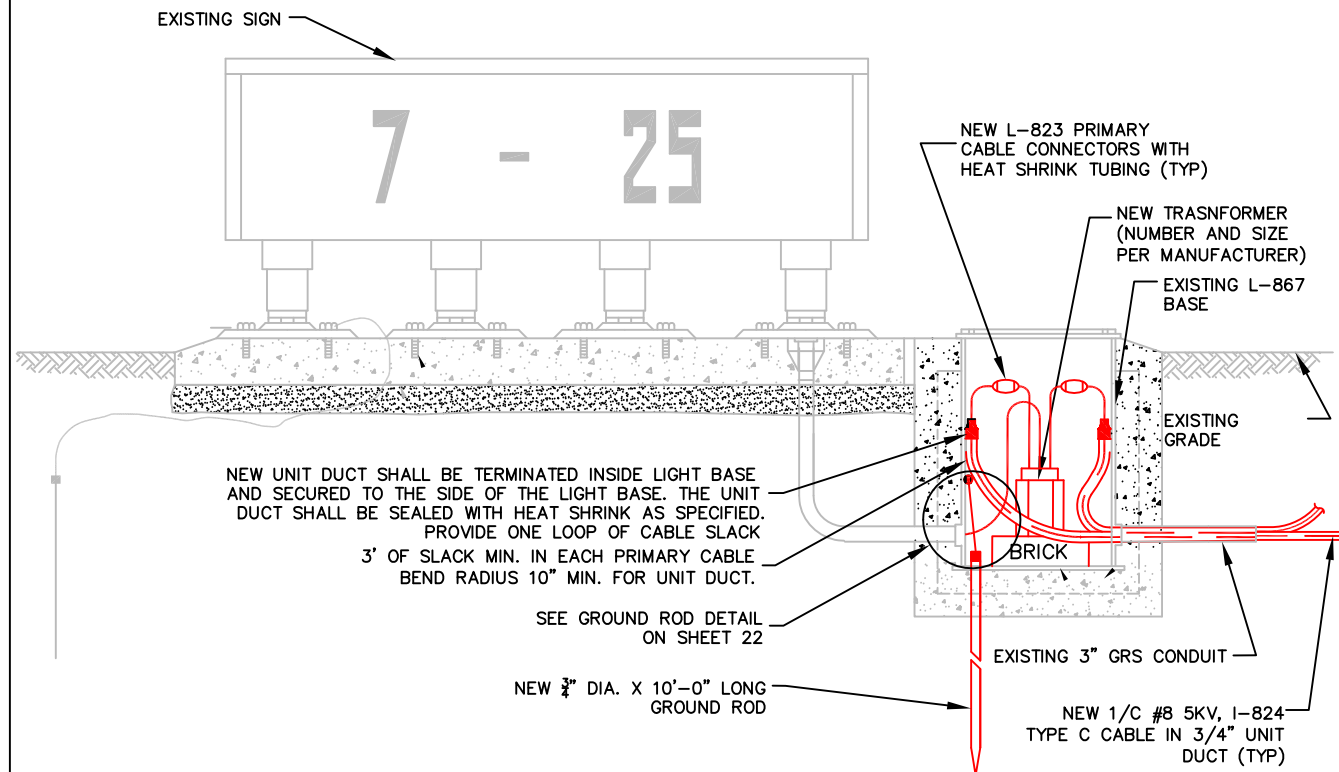
- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES FOR AIRFIELD LIGHTING SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS. DEPTH OF FAA CABLES SHALL BE 36" UNLESS OTHERWISE SHOWN.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH RETURNING MATERIALS.
- THE CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.



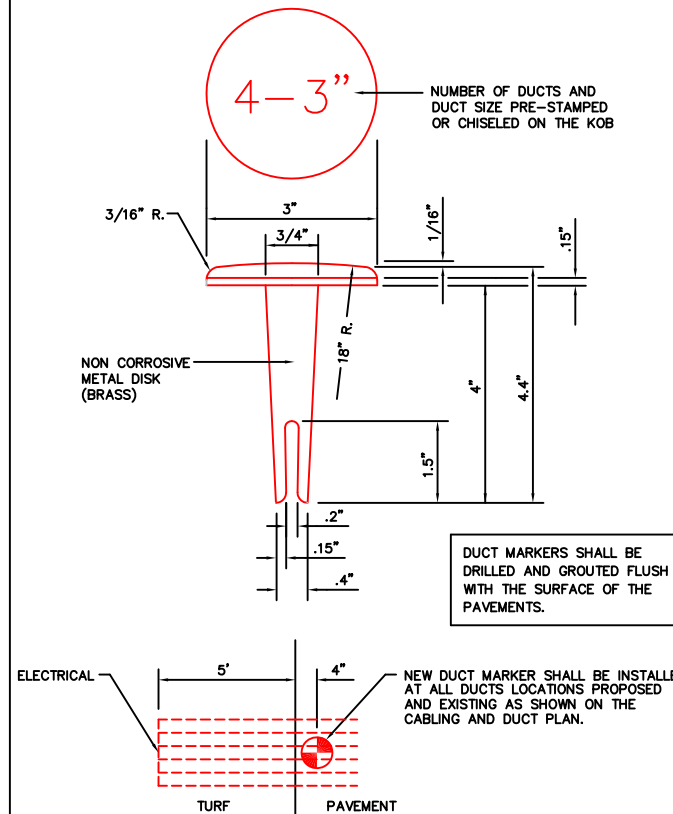
**PLOWED CABLE IN UNIT DUCT**  
NOT TO SCALE

**NOTES**

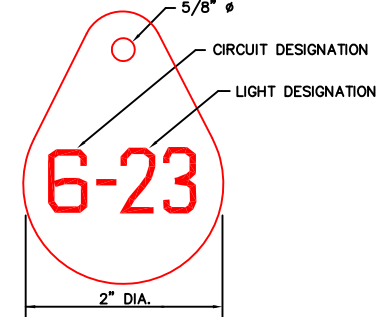
- ONLY CABLES OF THE SAME CIRCUIT WILL BE ALLOWED TO BE PLOWED IN TOGETHER.
- THE CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.



**AR800813 REFURB. AND GROUND GUIDANCE SIGN**  
NOT TO SCALE



**DUCT MARKER DETAIL**  
NOT TO SCALE



**LIGHT IDENTIFICATION DETAIL**  
NOT TO SCALE

**NOTES**

- INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (PROPOSED OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

**ELECTRICAL DETAILS - 1**

**FRO38**



**Freeport - Albertus Airport**



**Freeport, Illinois**

**City of Freeport**

**AIP: 3-17-0045-B23  
FEP-3997**

**Rehab. North  
Terminal Apron and  
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**Rehab. Taxiway  
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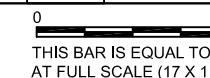
**ELECTRICAL  
DETAILS - 1  
(ELD1)**

**21**



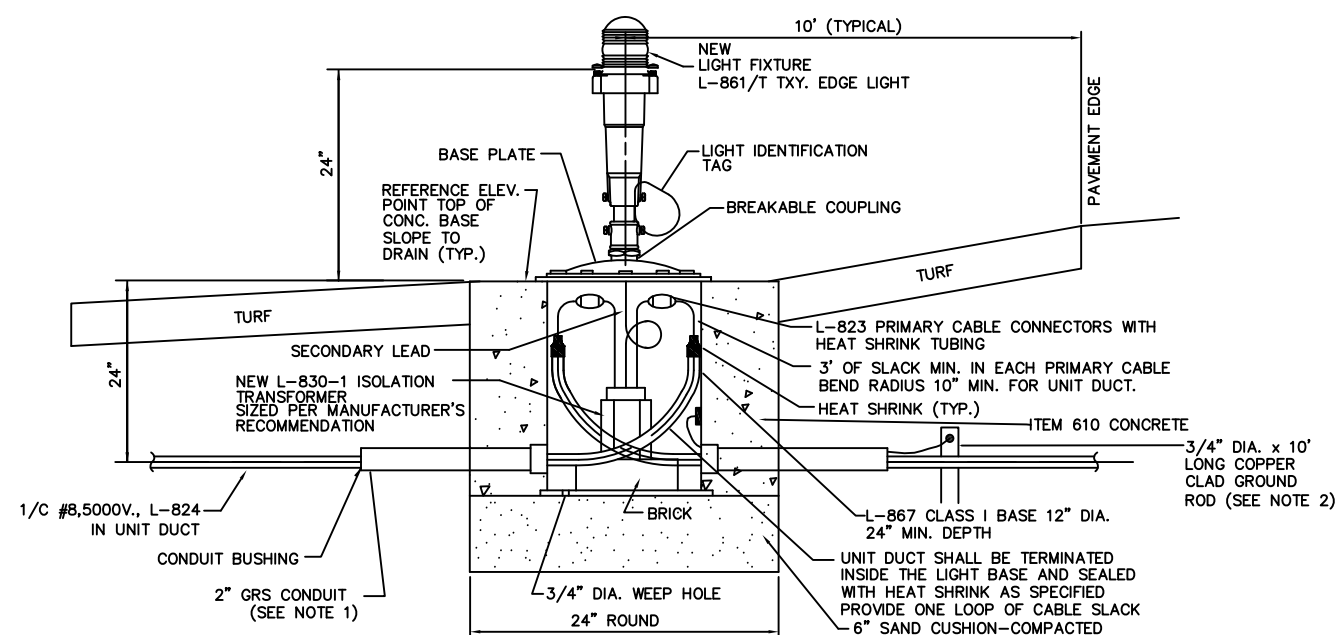
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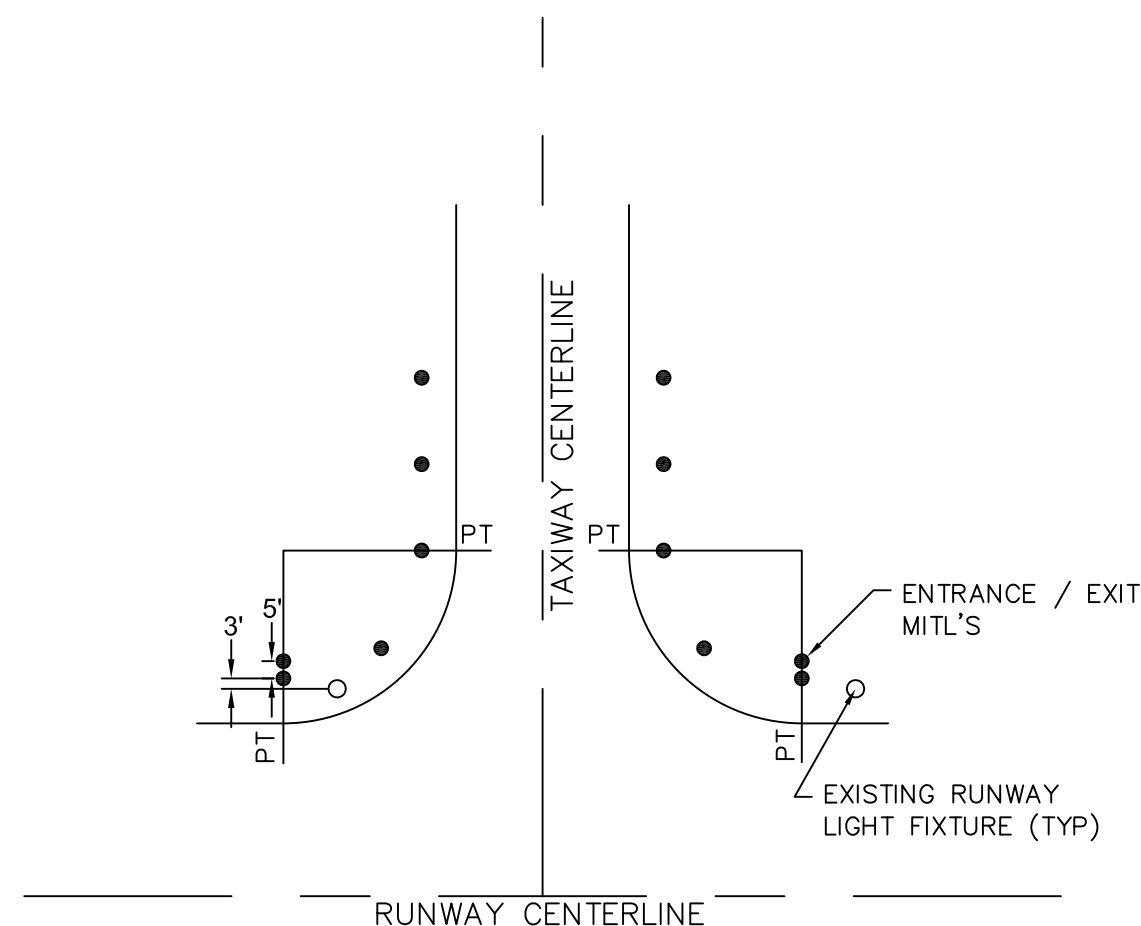
## ELECTRICAL DETAILS - 2 (EDL2)



### NEW BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT

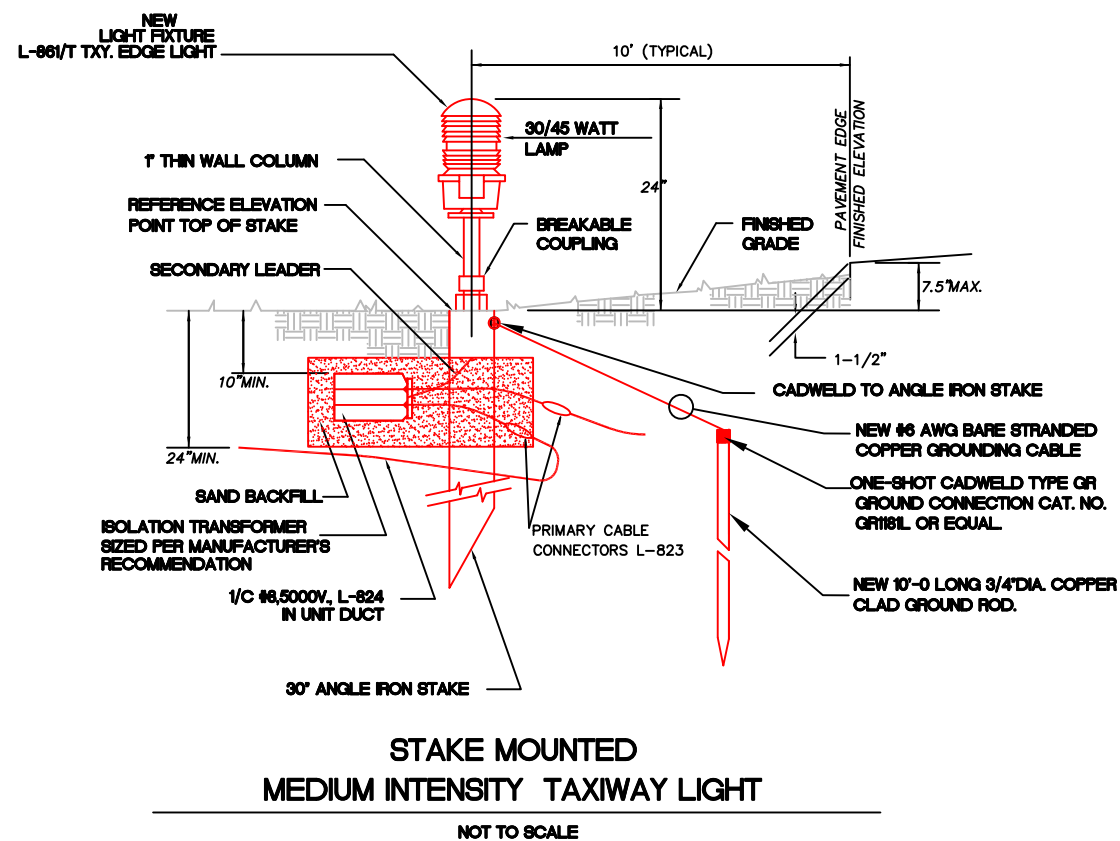
NOT TO SCALE

- INSTALL 2" PVC TO GRS ADAPTER WHERE PVC CONCRETE WILL BE INSTALLED UNDER PAVED SHOULDER.
- INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELD TO GROUND ROD. INSTALL GROUND LUG INSIDE EXISTING CAN.
- NEW/RETROFIT TAXIWAY EDGE LIGHT SHALL BE FAA APPROVED L-861 T



### LOCATION OF ENTRANCE EXIT LIGHTS (IN LIEU OF GUIDANCE SIGNS)

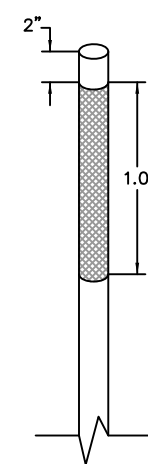
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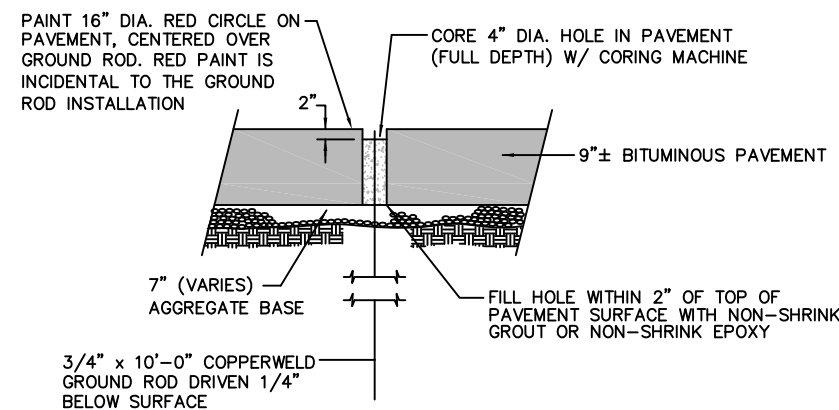
### STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT

NOT TO SCALE

- NEW/RETROFIT TAXIWAY EDGE LIGHT SHALL BE FAA APPROVED L-861 T



GROUND ROD KNURL  
NOT TO SCALE



GROUND ROD DETAIL  
NOT TO SCALE

## ELECTRICAL DETAILS - 2