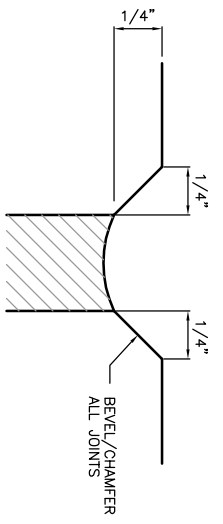
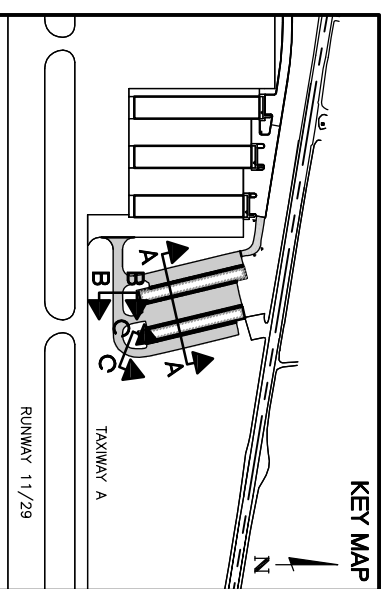
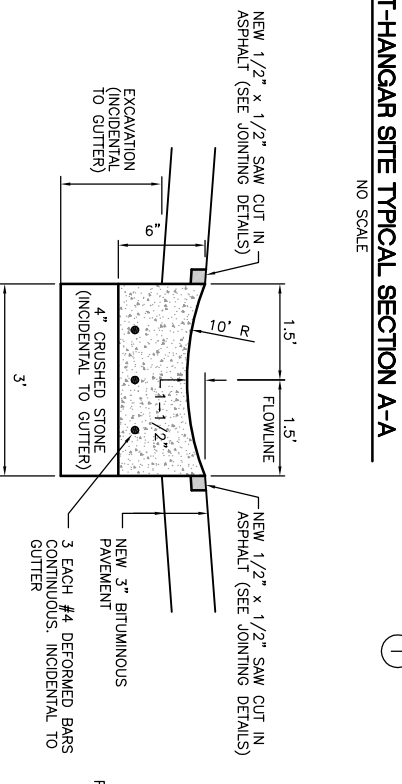


**LEGEND**

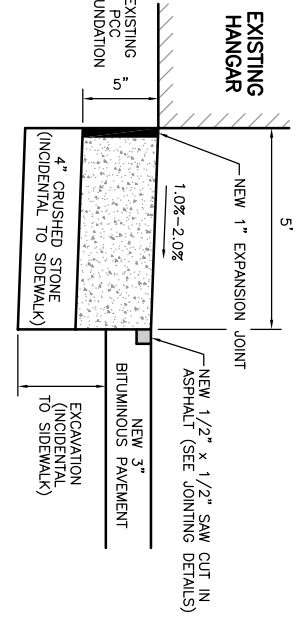
- (A) EXISTING 3" BITUMINOUS SURFACE COURSE TO BE REMOVED AND REPLACED WITH 1-1/2" BITUMINOUS BASE COURSE (403) AND 1-1/2" BITUMINOUS SURFACE COURSE (401)
- (B) NEW BITUMINOUS PRIME COAT (602)
- (C) NEW BITUMINOUS TACK COAT (603)
- (D) EXISTING GROUND LINE
- (E) EXISTING 8" AGGREGATE BASE COURSE (208) TO BE REGRADED AND RECOMPACTED AS NECESSARY (COST INCIDENTAL)
- (F) NEW VARIABLE DEPTH CRUSHED AGGREGATE BASE COURSE (PAID FOR AS 209)
- (G) NEW PCC CURB/FLUME (6") AND 4" CRUSHED STONE (SEE DETAIL)
- (H) NEW 5" WIDE 5" THICK PCC SIDEWALK/PAVEMENT AND 4" CRUSHED STONE (SEE DETAIL)
- (I) EXISTING BASE REPAIR (AS REQUIRED)
  - 20" UNCLASSIFIED EXCAVATION (152)
  - NEW 12" POROUS GRANULAR EMBANKMENT (P.G.C.E.) (208)
  - NEW 8" CRUSHED AGGREGATE BASE COURSE (209)
- (J) NEW TOPSOIL PLACEMENT (905) SEEDING (901) AND MULCHING (908)
- (K) 1" EXPANSION JOINT



**CHAMFER/BEVEL JOINT DETAIL**  
NO SCALE



**T-HANGAR SITE TYPICAL SECTION A-A**  
NO SCALE

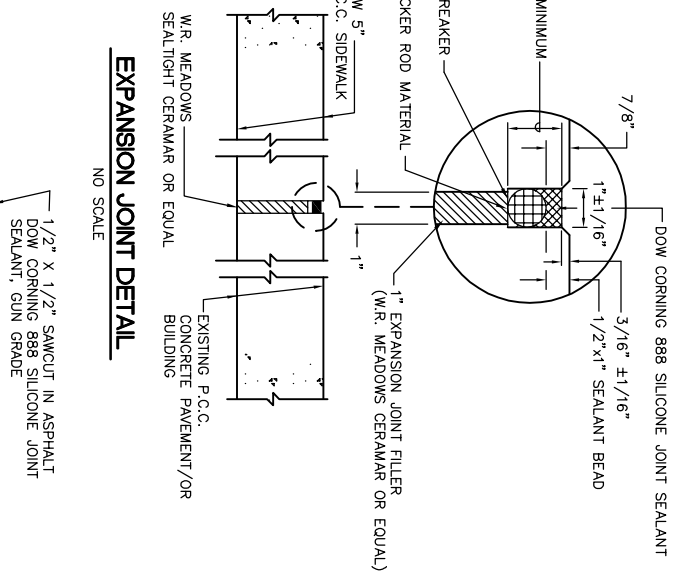


**CONCRETE SIDEWALK DETAIL**  
NO SCALE

**PCC CURB/FLUME DETAIL**  
NO SCALE

DIMENSION TABLE	
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T <sub>1</sub> INCHES
5	1.25"
6	1.50"
7	1.75"
8	2.00"
9	2.25"
10	2.50"
11	2.75"
12	3.00"

**EXPANSION JOINT DETAIL**  
NO SCALE



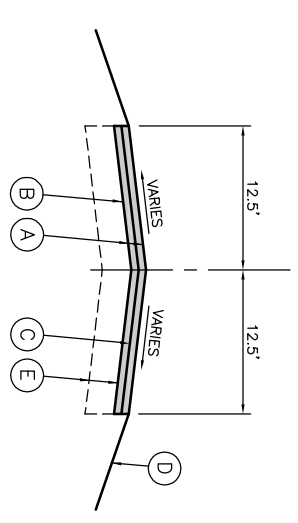
**JOINT SEALING AT CONCRETE ASPHALT INTERFACE**  
NO SCALE



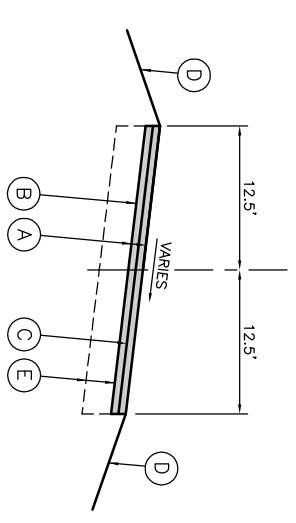
NOTE: JOINT DIMENSIONS SHALL MEET MANUFACTURER'S RECOMMENDATION

- NOTES**
- CONTRACTOR SHALL PROVIDE CONTRACTION JOINTS EVERY FIVE (5) FEET FOR CONCRETE SIDEWALK AND CONCRETE GUTTER/FLUME. JOINTS SHALL BE SAWN AND SHIMMED NOT THE CONSTRUCTED USING THE TOOLING METHOD. THE COST OF THE JOINTING AND SEALING SHALL BE CONSIDERED INCIDENTAL TO THE SIDEWALK AND GUTTER.

**TAXIWAY TYPICAL SECTION B-B**  
NO SCALE



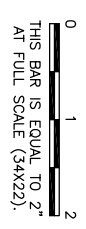
**TAXIWAY TYPICAL SECTION C-C**  
NO SCALE



**JOINTING NOTES**

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN SEPARATE TOOLING HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL THE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- THE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS. BARS ARE TO BE BENT OR RESTRANGHTENED DURING CONSTRUCTION REGARDING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF OTHER TEMPORARY HEADERS, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS AT NO ADDITIONAL COST.

SURVEY BOOK #	BOOK #
REVISIONS	
NUMBER	BY
	DATE



**SCHAUMBURG AIRPORT  
SCHAUMBURG, ILLINOIS  
REHABILITATE T-HANGAR PAVEMENT**

**TYPICAL SECTIONS/MISCELLANEOUS DETAILS**

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A.I.P. PROJECT:	3-17-012A-B29
ILLINOIS PROJECT:	06C-4050

SHEET 4 OF 6 SHEETS