

VILLAGE OF SCHAUMBURG SCHAUMBURG, ILLINOIS

CONSTRUCTION PLANS FOR SCHAUMBURG REGIONAL AIRPORT REHABILITATE T-HANGAR PAVEMENTS

ILLINOIS PROJECT: 06C-4050
AIP PROJECT: 3-17-0124-B29

MARCH 4, 2011

811
Know what's below.
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JULIE
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH UTILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES COMPANIES OF HIS OPERATIONAL PLANS. OBTAIN FROM RESPECTIVE UTILITIES COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION, SIZE AND TYPE OF UTILITIES AND THE DEPTH OF THE UTILITIES. AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN AIRCRAFT APPROACH CATEGORY: B
DESIGN AIRCRAFT GROUP: I

SCHAUMBURG REGIONAL AIRPORT
TOWNSHIP: 41 NORTH
RANGE: 10 EAST
COOK COUNTY
SCHAUMBURG TOWNSHIP
(SECTION: 33)
BLOOMINGDALE TOWNSHIP
(SECTION: 4)

CMT
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS

10256-02

PROFESSIONAL ENGINEER # 012-000000
MARC L. KATZ
02-20-0000

SUBMITTED BY: *Marc L. Katz* P.E.
MARC L. KATZ, P.E.

DATE: 3/10/2011

VILLAGE OF SCHAUMBURG
SCHAUMBURG REGIONAL AIRPORT

APPROVED BY: *Steven R. Weinstein*
DIRECTOR OF ENGINEERING AND PUBLIC WORKS

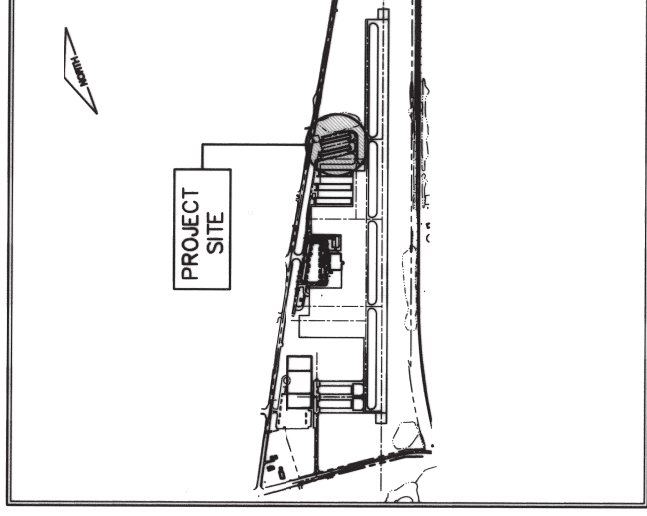
DATE: 3-09-11

SUMMARY OF QUANTITIES

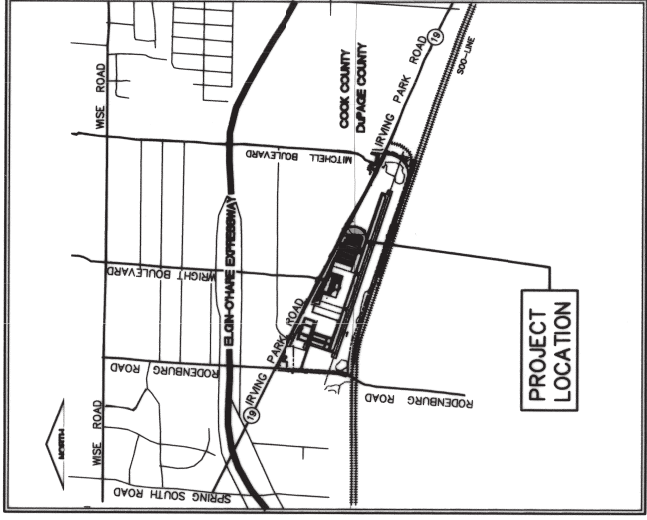
| ITEM NUMBER | DESCRIPTION | UNIT | TOTAL QUANTITY | RECORD QUANTITY |
|-------------|----------------------------------|------|----------------|-----------------|
| AR150200 | MOBILIZATION | LS | 1 | |
| AR152410 | UNCLASSIFIED EXCAVATION | CY | 1,400 | |
| AR200615 | POROUS GRANULAR EMBANKMENT | CY | 720 | |
| AR200610 | CRUSHED AGGREGATE BASE COURSE | TON | 1,305 | |
| AR401610 | BITUMINOUS SURFACE COURSE | TON | 630 | |
| AR401900 | REMOVE BITUMINOUS PAVEMENT | SY | 6,580 | |
| AR403610 | BITUMINOUS BASE COURSE | TON | 530 | |
| AR501605 | 5" PCC SIDEWALK | SF | 5,235 | |
| AR501900 | REMOVE PCC PAVEMENT | SY | 65 | |
| AR602510 | BITUMINOUS PRIME COAT | GAL | 2,960 | |
| AR603510 | BITUMINOUS TACK COAT | GAL | 865 | |
| AR602520 | PAVEMENT MARKING - WATERBORNE | SF | 750 | |
| AR602525 | PAVEMENT MARKING - BLACK BORDER | SF | 750 | |
| AR602900 | PAVEMENT MARKING REMOVAL | SF | 105 | |
| AR701512 | 12" RCP CLASS IV | LF | 35 | |
| AR751411 | INLET - TYPE A | EACH | 1 | |
| AR752412 | PRECAST REINFORCED CONC. FES 12" | EACH | 1 | |
| AR752512 | GRATING FOR CONC. FES 12" | EACH | 1 | |
| AR754710 | CONCRETE FLOUM | LF | 265 | |
| AR800189 | AIRCRAFT RELOCATION | EACH | 30 | |
| AR901510 | SEEDING | ACRE | 0.15 | |
| AR905520 | TOPSOILING (FROM OFF SITE) | CY | 65 | |
| AR908510 | MULCHING | ACRE | 0.15 | |

INDEX TO SHEETS

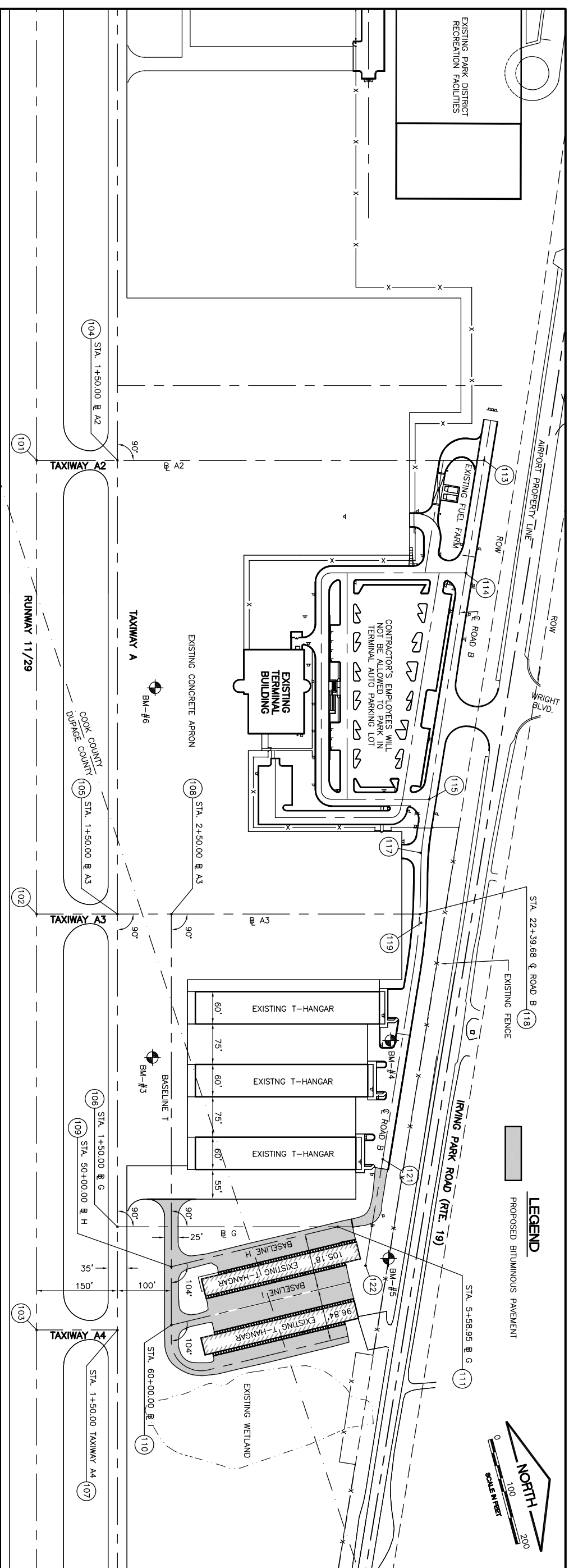
- COVER SHEET
- SITE PLAN/HORIZONTAL AND VERTICAL CONTROL
- SEQUENCE OF CONSTRUCTION PER AC 150/5370-2E (LATEST EDITION)
- TYPICAL SECTIONS/MISCELLANEOUS DETAILS
- EXISTING CONDITIONS/PROPOSED REMOVALS
- PROPOSED IMPROVEMENTS/GRADING PLAN



SITE PLAN



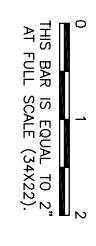
LOCATION MAP



| HORIZONTAL CONTROL | | | | | |
|--------------------|---|---------------|---------------|--------------------------|--------|
| POINT NUMBER | DESCRIPTION | NORTHING | EASTING | STATION | OFFSET |
| 101 | RUNWAY 11/29 AND TAXIWAY A2 | 1,939,016.834 | 1,046,503.336 | 110+27.88 @ RUNWAY 11/29 | 0.00' |
| 102 | RUNWAY 11/29 AND TAXIWAY A3 | 1,938,799.016 | 1,047,316.674 | 118+69.88 @ RUNWAY 11/29 | 0.00' |
| 103 | RUNWAY 11/29 AND TAXIWAY A4 | 1,938,599.394 | 1,048,062.067 | 126+41.54 @ RUNWAY 11/29 | 0.00' |
| 104 | TAXIWAY A AND TAXIWAY A2 | 1,939,161.728 | 1,046,542.139 | 210+27.88 @ TAXIWAY A | 0.00' |
| 105 | TAXIWAY A AND TAXIWAY A3 | 1,938,943.909 | 1,047,355.478 | 218+69.88 @ TAXIWAY A | 0.00' |
| 106 | TAXIWAY A AND TAXIWAY A4 | 1,938,793.839 | 1,047,915.847 | 224+50.00 @ TAXIWAY A | 0.00' |
| 107 | TAXIWAY A AND TAXIWAY A2 | 1,938,744.288 | 1,048,100.871 | 226+41.54 @ TAXIWAY A | 0.00' |
| 108 | T AND @ A3 | 1,939,040.506 | 1,047,381.347 | 318+69.88 @ T | 0.00' |
| 109 | T AND @ H | 1,938,871.236 | 1,048,013.405 | 325+24.22 @ T | 0.00' |
| 110 | T AND @ I | 1,938,843.252 | 1,048,117.898 | 326+32.39 @ T | 0.00' |
| 111 | G AND @ H | 1,939,188.870 | 1,048,021.639 | 53+17.74 @ H | 0.00' |
| 113 | PERIMETER ROAD B AND EXTENDED BASELINE A2 | 1,939,819.458 | 1,046,718.284 | 13+88.38 @ ROAD B | 0.00' |
| 114 | PERIMETER ROAD B AND ROAD C (WEST) | 1,939,732.670 | 1,046,911.290 | 16+00.00 @ ROAD B | 0.00' |
| 115 | PERIMETER ROAD B AND ROAD C (EAST) | 1,939,558.169 | 1,047,299.358 | 20+25.50 @ ROAD B | 0.00' |
| 117 | P.I. CURVE #1, PERIMETER ROAD B | 1,939,519.987 | 1,047,384.772 | 21+18.60 @ ROAD B | 0.00' |
| 118 | @ PERIMETER ROAD AND BASELINE A3 | 1,939,486.617 | 1,047,500.819 | 7+11.83 @ A3 | 0.00' |
| 119 | P.I. CURVE #2, PERIMETER ROAD B | 1,939,484.464 | 1,047,516.914 | 22+55.74 @ ROAD B | 0.00' |
| 121 | PERIMETER ROAD B | 1,939,302.127 | 1,047,922.410 | 27+00.00 @ ROAD B | 0.00' |
| 122 | EXTENDED CENTERLINE ROAD B | 1,939,220.105 | 1,048,104.818 | 29+00.00 @ ROAD B | 0.00' |

| VERTICAL CONTROL | | |
|------------------|--|-----------|
| BENCHMARK | DESCRIPTION | ELEVATION |
| BM - #1 | GPS SURVEY MONUMENT MARKED "1034" 2" ALUMINUM CAP GROUTED IN CONCRETE PAD ON SOUTHWEST CORNER OF RONDENBURG ROAD AND SOO LINE RAILROAD INTERSECTION. BENCHMARK "1034" IS A MONUMENT WHICH IS PART OF THE VILLAGE OF SCHAUMBURG (GEONEX BENCHMARK SYSTEM. (NOT SHOWN ON THIS SHEET) | 796.19 |
| BM - #2 | NOS FAA OGC POINT "A" IRON ROD STA. 21+36, 298 RT @ TXV. A (NOT SHOWN ON THIS SHEET) | 793.81 |
| BM - #3 | CHISELED "x" ON CONCRETE PAD AT FUEL FARM STA. 221+36, 65' LT @ TXV. A | 796.96 |
| BM - #4 | CHISELED "□" NORTH SIDE OF CONCRETE BASE FOR BEACON STA. 24+80.09, 20.84' RT @ ROAD B | 804.41 |
| BM - #5 | RAILROAD SPIKE IN SOUTH FACE OF POWER POLE SOUTH SIDE OF ROUTE 19 AT EXISTING HANGAR STA. 28+80, 40' LT @ ROAD B (EXTENDED) | 796.05 |
| BM - #6 | NORTH RIM INLET AT EXISTING TERMINAL APRON STA. 21+4+50, 70' LT @ TXV. A | 800.17 |

| | |
|----------------------|-----------|
| SURVEY BOOK # BOOK # | |
| NUMBER | REVISIONS |
| BY | DATE |
| | |
| | |



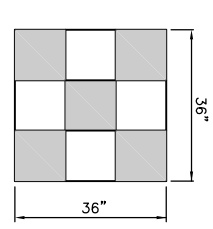
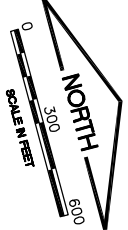
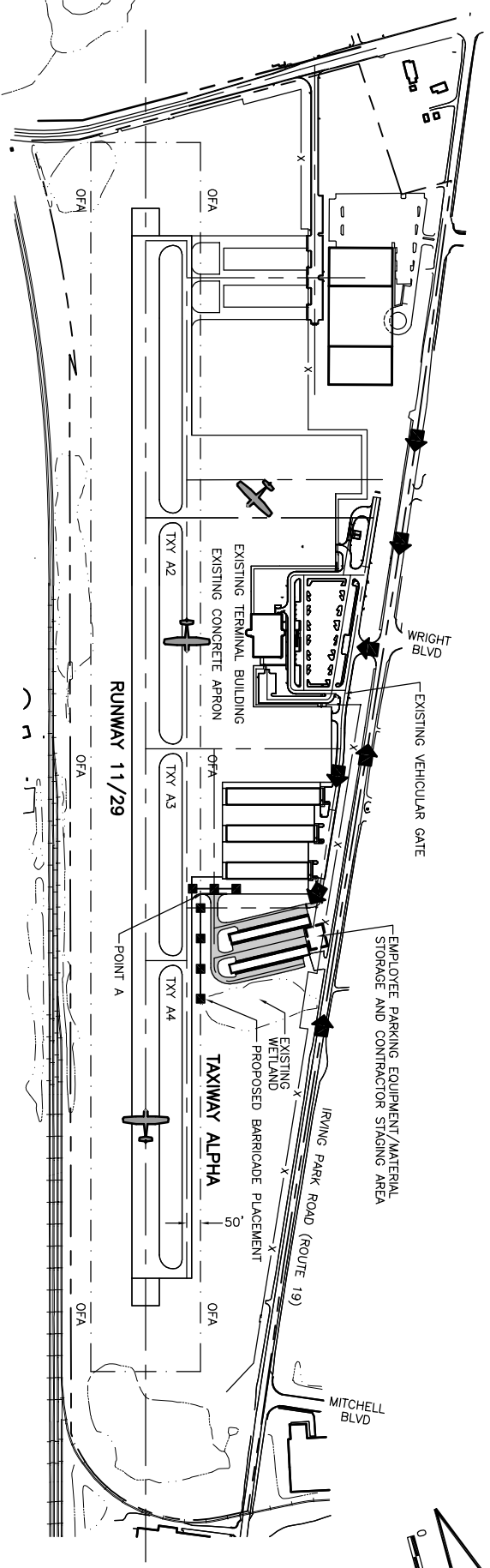
**SCHAUMBURG AIRPORT
SCHAUMBURG, ILLINOIS
REHABILITATE T-HANGAR PAVEMENT**

**SITE PLAN/
HORIZONTAL AND VERTICAL CONTROL**

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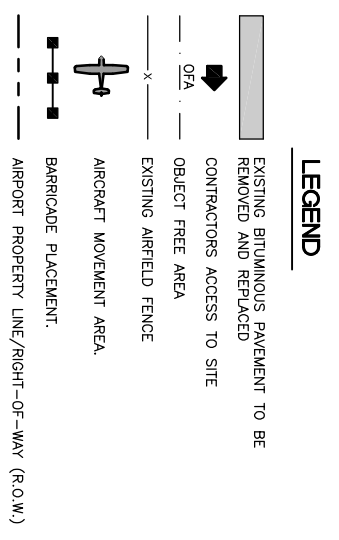
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DESIGN BY: MLK
DRAWN BY: JRO
CHECKED BY: MLK
APPROVED BY: DLP
DATE: 03/04/2011
JOB No.: 10256-02
A.I.P. PROJECT: 3-17-0124-B29
ILLINOIS PROJECT: 06C-4050
SHEET 2 OF 6 SHEETS



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25 FEET



SUGGESTED SEQUENCE OF CONSTRUCTION

1. SUBMIT MATERIAL SOURCES AND APPROPRIATE SHOP DRAWINGS.
2. LOCATE ALL UTILITIES.
3. MOBILIZE EQUIPMENT AND MATERIALS.
4. REMOVE EXISTING BITUMINOUS PAVEMENT AND PCC PAVEMENT AS DETAILED.
5. REPLACE BASE MATERIAL AS REQUIRED.
6. FRAME 5" PCC SIDEWALK AND CONCRETE FLUME.
7. POUR 5" PCC SIDEWALK AND CONCRETE FLUME.
8. INSTALL DRAINAGE AND GRADE INFLELD AREA TO DRAIN.
9. REGRADE AND RECOMPACT AGGREGATE BASE AS REQUIRED.
10. PLACE AGGREGATE BASE COURSE AS NEEDED.
11. PLACE PRIME COAT.
12. PLACE BITUMINOUS BASE COURSE.
13. PLACE TACK COAT.
14. PLACE BITUMINOUS SURFACE COURSE.
15. INSTALL PAVEMENT MARKINGS.
16. RESTORE DISTURBED AREAS TO ORIGINAL CONDITION; PLACE TOPSOIL ALONG EDGES WHERE DETAILED AND SEED AND MULCH.
17. OPEN PAVEMENT TO AIRCRAFT.

LIMITATIONS ON CONSTRUCTION WITHIN OBJECT FREE AREA (O.F.A.)

1. WORK WITHIN THE OBJECT FREE AREA (O.F.A.) SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE R.S.A.
2. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO ADDITIONAL COST TO THE CONTRACT.
3. CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER TWO (2) WORKING DAYS IN ADVANCE OF ANY STAGING CHANGES, WHICH WILL REQUIRE CHANGES IN AIRCRAFT MOVEMENT.
4. RUNWAY AND/OR TAXIWAY WILL BE TEMPORARILY CLOSED WHEN CONSTRUCTION EQUIPMENT OR WORK IS WITHIN THE RUNWAY/TAXIWAY O.F.A. AT THE END OF EACH WORKING DAY THE RUNWAY/TAXIWAY SHALL BE REOPENED. THE COST OF REMOVING AND REPLACING BARRICADES AND CLOSED RUNWAY MARKERS IS INCIDENTAL.

PROJECT OVERVIEW

1. THE PROPOSED PROJECT INVOLVES THE REHABILITATION OF BITUMINOUS PAVEMENTS AT THE T-HANGARS LOCATED ON THE EAST SIDE OF THE AIRPORT.
2. NO CONSTRUCTION WILL BE UNDERTAKEN IN ANY RUNWAY SAFETY AREAS OR OBJECT FREE AREAS.
3. THERE ARE NO RUNWAY CLOSURES ANTICIPATED DURING THIS PROJECT.

GENERAL NOTES

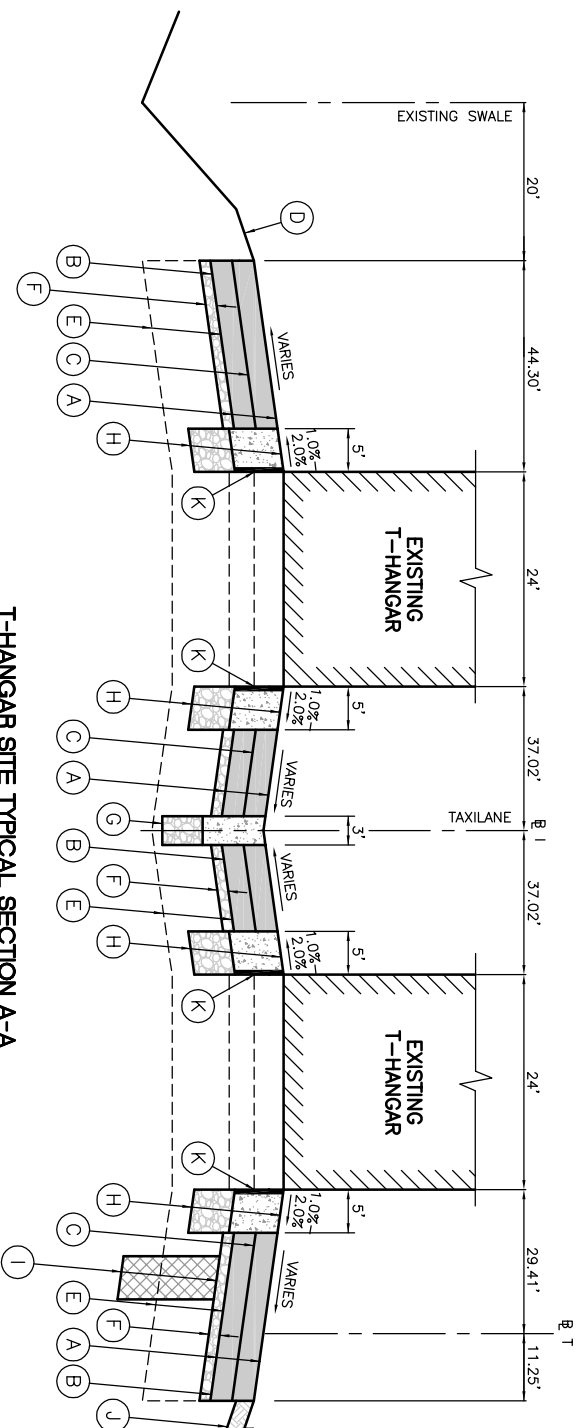
1. ALL CONSTRUCTION SEQUENCING AND OPERATIONS SHALL CONFORM TO THE APPLICABLE PROVISIONS OF AC 150/5370-2E (LATEST EDITION). SAFETY ON AIRPORTS DURING CONSTRUCTION (LATEST EDITION). PAVEMENT FOR MAINTENANCE OF TRAFFIC AND TRAFFIC CONTROL, INCLUDING, BUT NOT LIMITED TO TEMPORARY PAVEMENT MARKING, TEMPORARY PAVEMENT MARKING REMOVALS, THE MOVING AND MAINTENANCE OF BARRICADES, SIGNING, OBJECT FREE AREAS (O.F.A.) LAINE AND RIBBON, ETC. SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
2. THE CONTRACTOR MUST STAGE CONSTRUCTION AROUND AIRPORT OPERATIONS. STAGING SHOWN IS SUGGESTED AND IS INTENDED TO PROVIDE THE CONTRACTOR WITH MAJOR WORK AREAS WHILE MINIMIZING DISRUPTIONS TO AIRPORT OPERATIONS. THE CONTRACTOR MAY USE ALTERNATE STAGING PLANS; HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE VILLAGE OF SCHUMBURG AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS.
3. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER TWO WORKING DAYS IN ADVANCE OF ANY STAGING CHANGES WHICH WILL REQUIRE CHANGES IN AIRCRAFT MOVEMENT OR AIRCRAFT RELOCATION. THE RESIDENT ENGINEER SHALL THEN NOTIFY THE AIRPORT, WHO WILL ISSUE APPROPRIATE NOTAMS. THE RELOCATION OF AIRCRAFT SHALL BE DONE IN ACCORDANCE WITH ITEM AR800189 – AIRCRAFT RELOCATION.
4. BARRICADES AT 15' CENTERS SHALL BE PLACED AT THE LOCATIONS SHOWN OR AS DIRECTED BY THE VILLAGE OF SCHUMBURG IN CONSULTATION WITH THE RESIDENT ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. HAVE A FLASHING RED LIGHT AND CONFORM TO DOT STANDARD 701901-01, TYPE II. ROPE WITH HIGH VISIBILITY ORANGE FLAGGING SHALL BE INSTALLED BETWEEN ALL BARRICADES.
5. THE VILLAGE OF SCHUMBURG, IN CONSULTATION WITH THE RESIDENT ENGINEER, SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
6. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING PAVEMENTS, EXCEPT AS SHOWN OR WITH THE PRIOR APPROVAL OF THE VILLAGE OF SCHUMBURG.
7. EXISTING AREAS BEYOND PROJECT LIMITS INCLUDING THE HAUL ROAD(S) AND STAGING AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO PRE-CONSTRUCTION CONDITION AT NO ADDITIONAL COST TO THE OWNER AND TO THE SATISFACTION OF THE RESIDENT ENGINEER AND VILLAGE OF SCHUMBURG.
8. EACH DAY AT THE COMPLETION OF WORK, OR MORE FREQUENTLY AS DETERMINED BY THE VILLAGE OF SCHUMBURG, THE CONTRACTOR SHALL BE REQUIRED TO USE A PICKUP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AREAS AND AT EXISTING AIRPORT ROADS BEING USED FOR SITE ACCESS AND HAULING OPERATIONS. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. THE COST OF SWEEPING SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
9. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WITHIN THE LIMITS OF EACH PHASE AS DESIGNATED BY THE VILLAGE OF SCHUMBURG WHEN CONSTRUCTION IS NOT IN PROGRESS. CONTRACTOR SHALL PLACE EQUIPMENT/MATERIAL STORAGE AND EMPLOYEE PARKING AREA WITHIN THE CONSTRUCTION SITE BOUNDARIES AT NO TIME WILL THIS AREA BE ALLOWED ON EXISTING PAVEMENTS OR NEWLY CONSTRUCTED PROPOSED PAVEMENT.
10. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE PERFORMED USING METHOD AND LIGHTING TO SHOW THE PROPOSED CONSTRUCTION AREA AND EXISTING LIGHTS SHALL CONSIST OF MOVABLE POLE MOUNTED ECOLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
11. MATERIALS REMOVED FROM THE PROJECT SHALL BE DISPOSED OF AT AN APPROVED SITE OFF THE AIRPORT PROPERTY.
12. THE CONTRACTOR WILL BE REQUIRED TO PLACE A LINE OF LATHE AND RIBBON ALONG THE RUNWAY SAFETY AREA (R.S.A.) LIMIT, RIGHT OF WAY LIMIT AND ALONG DESIGNATED LIMITS OF CONSTRUCTION AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. (COST INCIDENTAL TO THE CONTRACT)
13. THE CONTRACTOR SHALL CONTACT THE RESIDENT ENGINEER AND THE VILLAGE OF SCHUMBURG A MINIMUM OF FIVE (5) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
14. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGAR/TERMINAL BUILDING SHALL NOT BE USED.
15. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A STONE/ASPHALT DELIVERY DUMP TRUCK, WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
16. ALL EXISTING ROADS USED AS A HAUL ROAD BY THE CONTRACTOR SHALL BE RESTORED TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER. REPAIRING OR CONSTRUCTING HAUL ROADS SHALL BE INCIDENTAL TO THE CONTRACT.
17. WEEKLY JOBSITE MEETINGS SHALL BE HELD TO COORDINATE THE WORK, PARTICULARLY PERTAINING TO ANY ACTIVITIES THAT MAY IMPACT OR INTERFERE WITH OTHER CONTRACTORS AND AIRPORT OPERATIONS.
18. OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH ORANGE FLAGS AND BARRICADES THAT SHALL BE WEIGHTED TO PREVENT BLOWING OVER AND HAVE A FLASHING RED LIGHT, CONFORMING TO DOT STANDARD 701901-01, TYPE II DURING THE HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURRENCE) DUE TO UNAUTHORIZED AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND OR AGENTS.
20. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS.
21. THE CONTRACTOR SHALL COORDINATE WORK ON ALL CONCURRENT PROJECTS, WHICH MAY ARISE. NO CLAIMS FOR ADDITIONAL COMPENSATION FOR ADDITIONAL COORDINATION OR CHANGES IN MAINTENANCE OF TRAFFIC OR WORK CONFLICTS WILL BE CONSIDERED.

DESIGN AIRCRAFT APPROACH CATEGORY: B
DESIGN AIRCRAFT GROUP: I

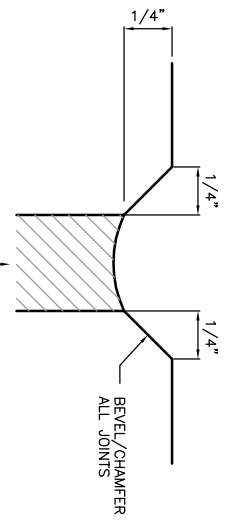
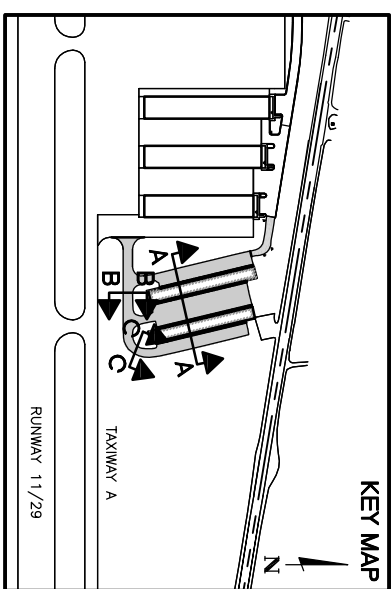
CLOSEST POINT ON CONSTRUCTION SITE
POINT "A"
LATITUDE: 41°59'22.72326"
LONGITUDE: 88°05'57.23294"

RUNWAY/TAXIWAY CLOSURE NOTE
WORK HOURS WITHIN THE AIR OPERATIONS AREA (200' FROM CENTERLINE RUNWAY AND 27.5' FROM CENTERLINE TAXIWAY) MUST BE SUBMITTED FOR APPROVAL TO THE OWNER 14 DAYS BEFORE CONSTRUCTION BEGINS. WORK SHALL BE EXPEDITED WITHIN THE AIR OPERATIONS AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW RUNWAY/TAXIWAY TO BE REOPENED TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

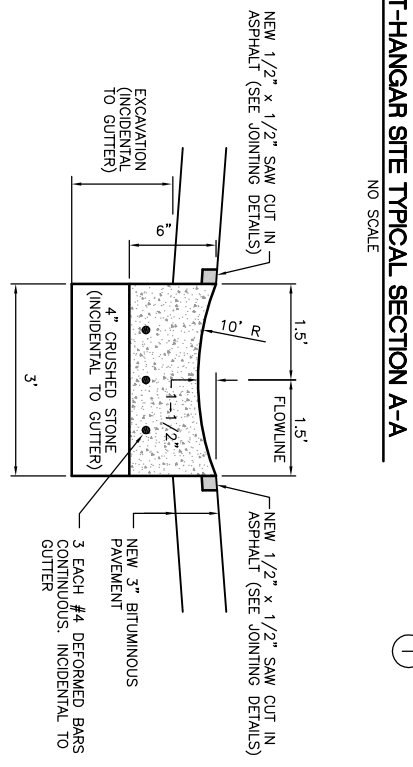
| <p>SURVEY BOOK # BOOK #</p> <p>REVISIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table> | NUMBER | BY | DATE | | | | | | | <p>SCHAUMBURG AIRPORT SCHAUMBURG, ILLINOIS REHABILITATE T-HANGAR PAVEMENT</p> <p>SEQUENCE OF CONSTRUCTION PER AC 150/5370-2E (LATEST EDITION)</p> | <p>0 1 2</p> <p>THIS BAR IS EQUAL TO 2" AT FULL SCALE (3/4X22).</p> |
|---|---|------|------|--|--|--|--|--|--|---|---|
| NUMBER | BY | DATE | | | | | | | | | |
| | | | | | | | | | | | |
| | | | | | | | | | | | |
| <p>DESIGN BY: MLK</p> <p>DRAWN BY: JRO</p> <p>CHECKED BY: MLK</p> <p>APPROVED BY: DLP</p> <p>DATE: 03/04/2011</p> <p>JOB No.: 10256-02</p> <p>A.I.P. PROJECT: 3-17-0124-B29</p> <p>ILLINOIS PROJECT: 06C-4050</p> <p>SHEET 3 OF 6 SHEETS</p> | <p>© Copyright CMT, Inc.</p> <p>CMT CRAWFORD, MURPHY & TILLY, INC. CONSULTING ENGINEERS License No. 184-000613</p> | | | | | | | | | | |



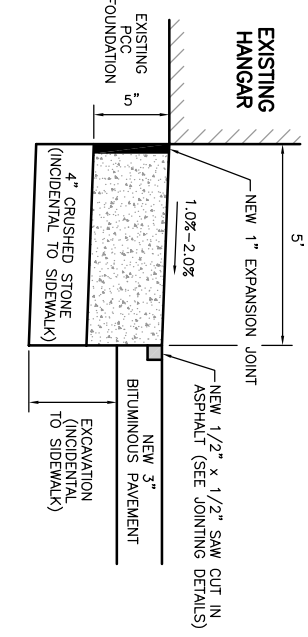
- ### LEGEND
- (A) EXISTING 3" BITUMINOUS SURFACE COURSE TO BE REMOVED AND REPLACED WITH 1-1/2" BITUMINOUS BASE COURSE (403) AND 1-1/2" BITUMINOUS SURFACE COURSE (401)
 - (B) NEW BITUMINOUS PRIME COAT (602)
 - (C) NEW BITUMINOUS TACK COAT (603)
 - (D) EXISTING GROUND LINE
 - (E) EXISTING 8" AGGREGATE BASE COURSE (208) TO BE REGRADED AND RECOMPACTED AS NECESSARY (COST INCIDENTAL)
 - (F) NEW VARIABLE DEPTH CRUSHED AGGREGATE BASE COURSE (PAID FOR AS 209)
 - (G) NEW PCC CURB/FLUME (6") AND 4" CRUSHED STONE (SEE DETAIL)
 - (H) NEW 5" WIDE 5" THICK PCC SIDEWALK/PAVEMENT AND 4" CRUSHED STONE (SEE DETAIL)
 - (I) EXISTING BASE REPAIR (AS REQUIRED)
 - 20" UNCLASSIFIED EXCAVATION (152)
 - NEW 12" POROUS GRANULAR EMBANKMENT (P.G.C.E.) (208)
 - NEW 8" CRUSHED AGGREGATE BASE COURSE (209)
 - (J) NEW TOPSOIL PLACEMENT (905) SEEDING (901) AND MULCHING (908)
 - (K) 1" EXPANSION JOINT



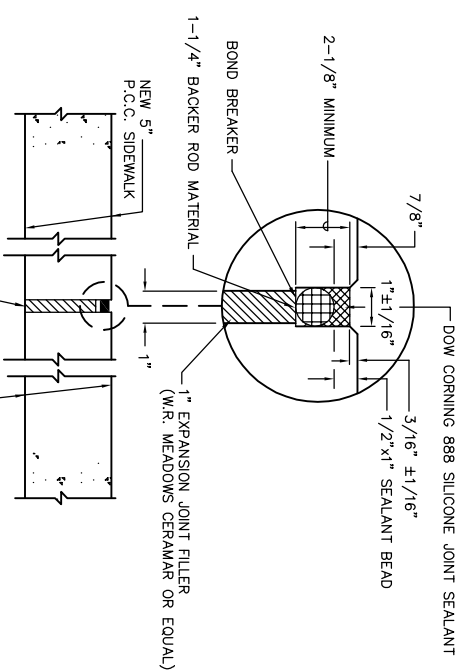
CHAMFER/BEVEL JOINT DETAIL
NO SCALE



T-HANGAR SITE TYPICAL SECTION A-A
NO SCALE



CONCRETE SIDEWALK DETAIL
NO SCALE

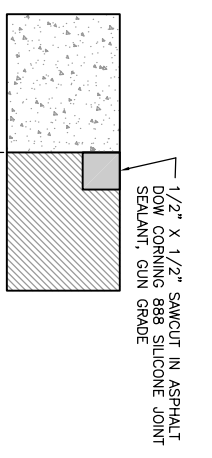


EXPANSION JOINT DETAIL
NO SCALE

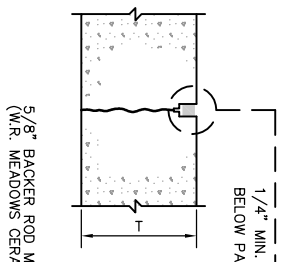
DIMENSION TABLE

| PAVEMENT THICKNESS T - INCHES | DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T INCHES |
|-------------------------------|---|
| 5 | 1.25" |
| 6 | 1.50" |
| 7 | 1.75" |
| 8 | 2.00" |
| 9 | 2.25" |
| 10 | 2.50" |
| 11 | 2.75" |
| 12 | 3.00" |

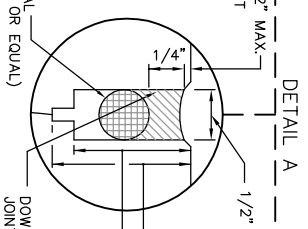
PCC CURB/FLUME DETAIL
NO SCALE



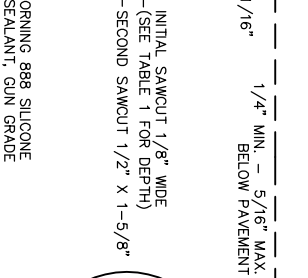
JOINT SEALING AT CONCRETE ASPHALT INTERFACE
NO SCALE



TYPE H DUMMY SYMBOL
NO SCALE



CONTRACTION JOINT DETAIL
NO SCALE

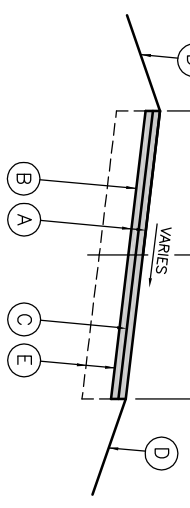


- ### NOTES
- CONTRACTOR SHALL PROVIDE CONTRACTION JOINTS EVERY FIVE (5) FEET FOR CONCRETE SIDEWALK AND CONCRETE GUTTER/FLUME. JOINTS SHALL BE SAWN AND SHIMMED NOT THE CONSTRUCTED USING THE TOOLING METHOD. THE COST OF THE JOINTING AND SEALING SHALL BE CONSIDERED INCIDENTAL TO THE SIDEWALK AND GUTTER.

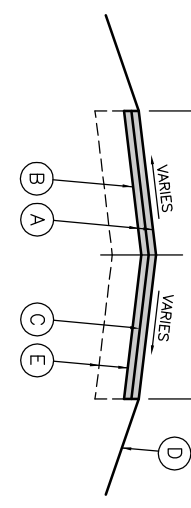
JOINTING NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN SEPARATE TOOLING HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL THE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- THE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS. BARS ARE TO BE BENT OR RESTRANGHTENED DURING CONSTRUCTION REGARDING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF OTHER APPROVED HEADERS, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS AT NO ADDITIONAL COST.

TAXIWAY TYPICAL SECTION C-C
NO SCALE

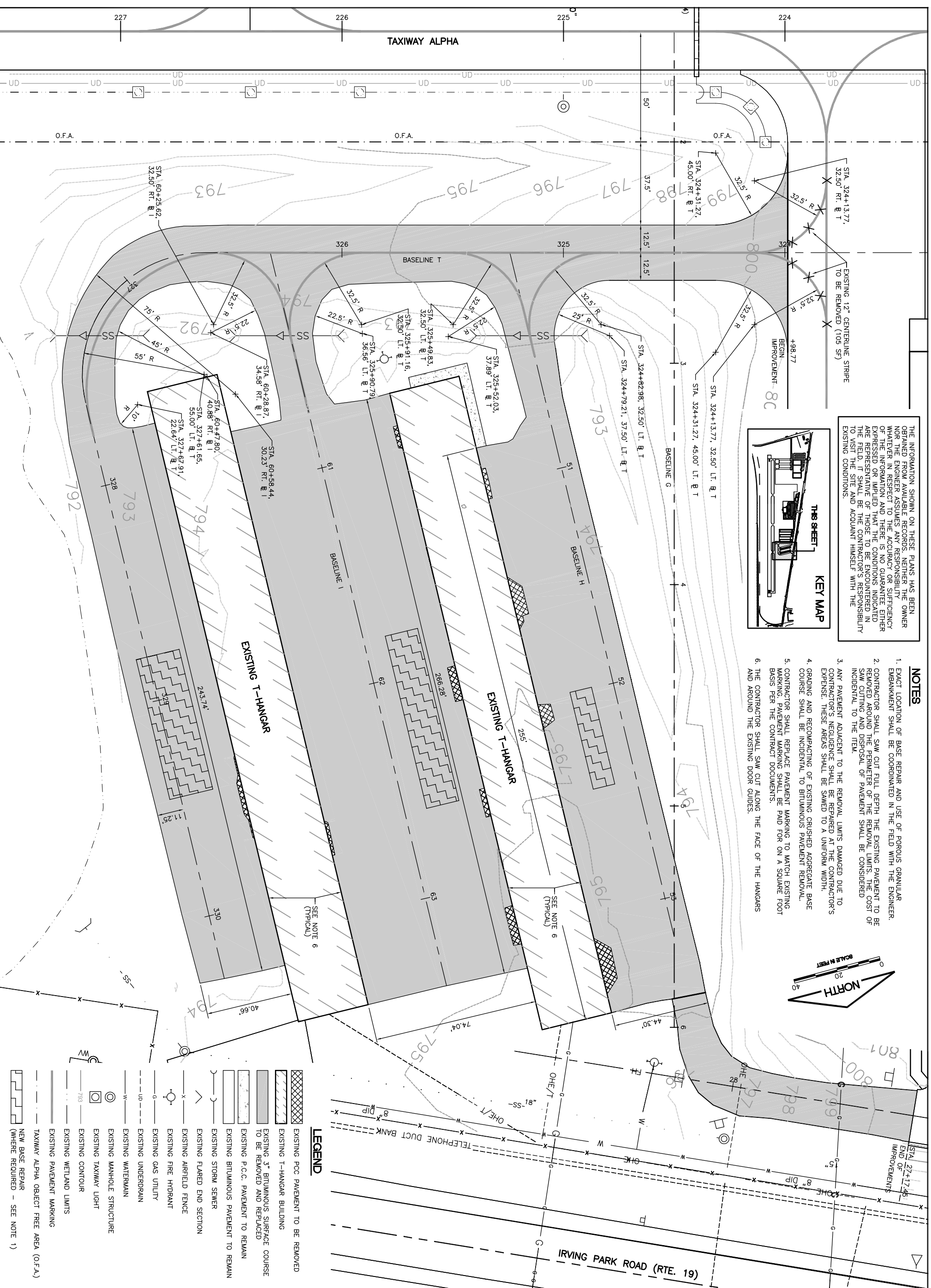


TAXIWAY TYPICAL SECTION B-B
NO SCALE

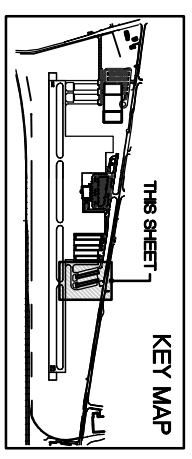


| <p>DESIGN BY: MLK DRAWN BY: JRO CHECKED BY: MLK APPROVED BY: DLP DATE: 03/04/2011 JOB NO.: 10256-02 A.I.P. PROJECT: 3-17-0124-B29 ILLINOIS PROJECT: 06C-4050</p> | <p>SCHAUMBURG AIRPORT SCHAUMBURG, ILLINOIS REHABILITATE T-HANGAR PAVEMENT</p> <p>TYPICAL SECTIONS/MISCELLANEOUS DETAILS</p> | <p>SURVEY BOOK # BOOK #</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NUMBER</th> <th>BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table> | NUMBER | BY | DATE | | | | <p>0 1 2 THIS BAR IS EQUAL TO 2" AT FULL SCALE (3/4X22).</p> |
|--|---|--|--------|----|------|--|--|--|--|
| NUMBER | BY | DATE | | | | | | | |
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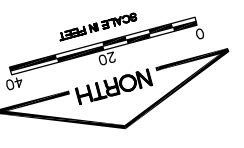
NOTE: JOINT DIMENSIONS SHALL MEET MANUFACTURER'S RECOMMENDATION



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUANT HIMSELF WITH THE EXISTING CONDITIONS.



- NOTES**
1. EXACT LOCATION OF BASE REPAIR AND USE OF POROUS GRANULAR EMBANKMENT SHALL BE COORDINATED IN THE FIELD WITH THE ENGINEER.
 2. CONTRACTOR SHALL SAW CUT FULL DEPTH THE EXISTING PAVEMENT TO BE REMOVED AROUND THE PERIMETER OF THE REMOVAL LIMITS. THE COST OF SAW CUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
 3. ANY PAVEMENT ADJACENT TO THE REMOVAL LIMITS DAMAGED DUE TO CONTRACTOR'S NEGLIGENCE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THESE AREAS SHALL BE SAWED TO A UNIFORM WIDTH.
 4. GRADING AND RECOMPACTING OF EXISTING CRUSHED AGGREGATE BASE COURSE SHALL BE INCIDENTAL TO BITUMINOUS PAVEMENT REMOVAL.
 5. CONTRACTOR SHALL REPLACE PAVEMENT MARKING TO MATCH EXISTING MARKING. PAVEMENT MARKING SHALL BE PAID FOR ON A SQUARE FOOT BASIS PER THE CONTRACT DOCUMENTS.
 6. THE CONTRACTOR SHALL SAW CUT ALONG THE FACE OF THE HANGARS AND AROUND THE EXISTING DOOR GUIDES.



LEGEND

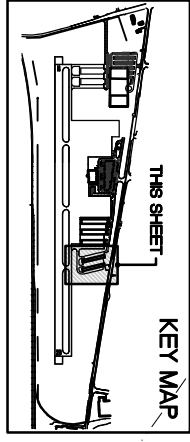
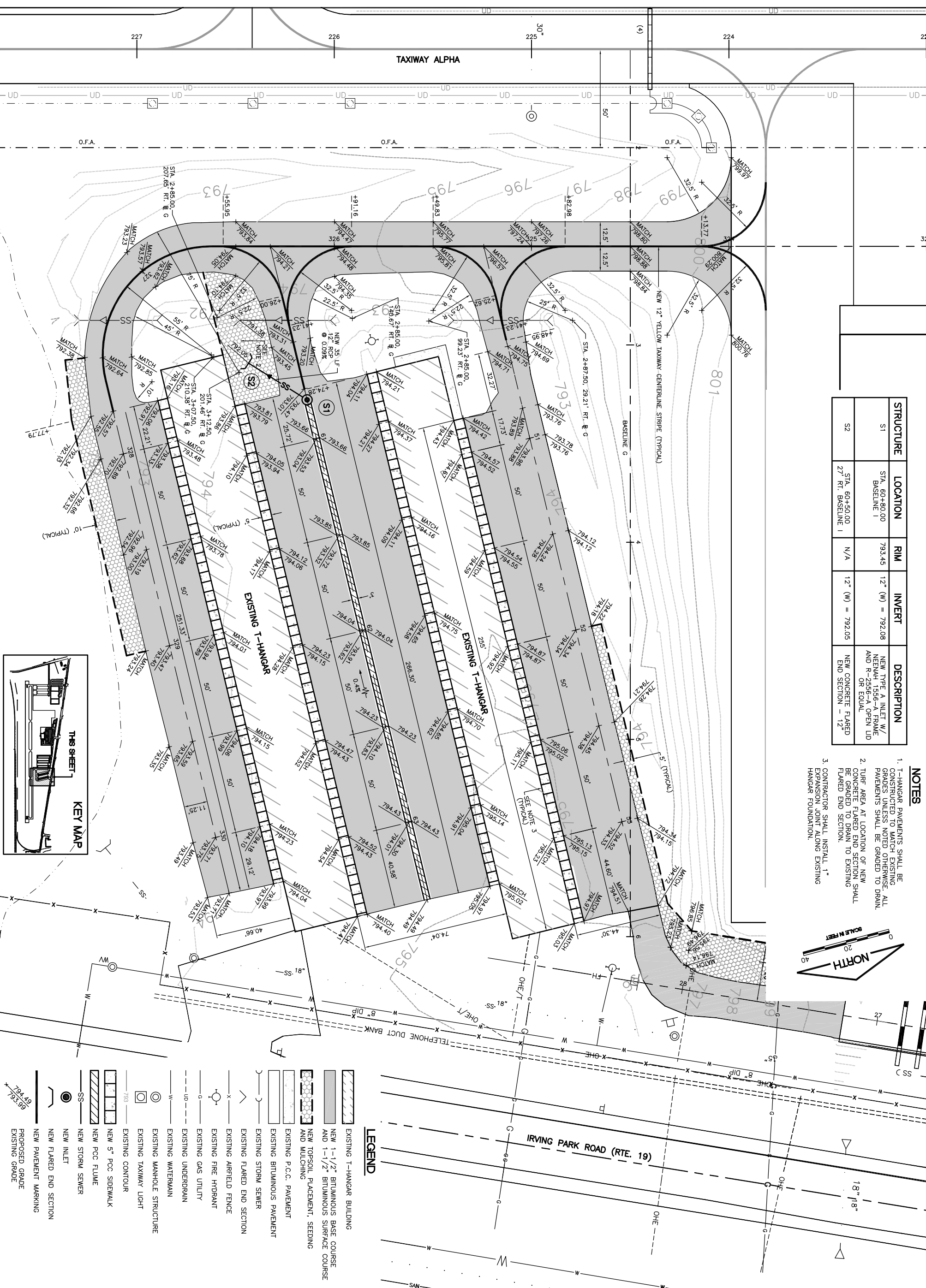
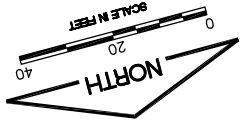
- EXISTING PCC PAVEMENT TO BE REMOVED
- EXISTING T-HANGAR BUILDING
- EXISTING 3" BITUMINOUS SURFACE COURSE TO BE REMOVED AND REPLACED
- EXISTING P.C.C. PAVEMENT TO REMAIN
- EXISTING BITUMINOUS PAVEMENT TO REMAIN
- EXISTING STORM SEWER
- EXISTING FLARED END SECTION
- EXISTING AIRFIELD FENCE
- EXISTING FIRE HYDRANT
- EXISTING GAS UTILITY
- EXISTING UNDERDRAIN
- EXISTING WATERMAIN
- EXISTING MANHOLE STRUCTURE
- EXISTING TAXIWAY LIGHT
- EXISTING CONTOUR
- EXISTING WETLAND LIMITS
- EXISTING PAVEMENT MARKING
- TAXIWAY ALPHA OBJECT FREE AREA (O.F.A.)
- NEW BASE REPAIR (WHERE REQUIRED - SEE NOTE 1)

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| <p>DESIGN BY: MLK</p> <p>DRAWN BY: JRO</p> <p>CHECKED BY: MLK</p> <p>APPROVED BY: DLP</p> <p>DATE: 03/04/2011</p> <p>JOB No.: 10256-02</p> <p>A.I.P. PROJECT: 3-17-0124-B29 ILLINOIS PROJECT: 06C-4050</p> | <p>SHEET 5 OF 6 SHEETS</p> | | | | | | | | |

| STRUCTURE | LOCATION | RIM | INVERT | DESCRIPTION |
|-----------|-------------------------------------|--------|------------------|--|
| S1 | STA. 60+80.00 BASELINE 1 | 793.45 | 12" (W) = 792.08 | NEW TYPE A INLET W/ NEMA# 1556-A FRAME AND R-2556-A OPEN LID OR EQUAL |
| S2 | STA. 60+50.00 27' RT. BASELINE 1 | N/A | 12" (W) = 792.05 | NEW CONCRETE FLARED END SECTION - 12' |

NOTES

1. T-HANGAR PAVEMENTS SHALL BE CONSTRUCTED TO MATCH EXISTING GRADES UNLESS NOTED OTHERWISE. ALL PAVEMENTS SHALL BE GRADED TO DRAIN.
2. TURF AREA AT LOCATION OF NEW CONCRETE FLARED END SECTION SHALL BE GRADED TO DRAIN TO EXISTING FLARED END SECTION.
3. CONTRACTOR SHALL INSTALL 1" EXPANSION JOINT ALONG EXISTING HANGAR FOUNDATION.



LEGEND

- EXISTING T-HANGAR BUILDING
- NEW 1-1/2" BITUMINOUS BASE COURSE AND 1-1/2" BITUMINOUS SURFACE COURSE
- NEW TOPSOIL PLACEMENT, SEEDING AND MULCHING
- EXISTING P.C.C. PAVEMENT
- EXISTING BITUMINOUS PAVEMENT
- EXISTING FLARED END SECTION
- EXISTING STORM SEWER
- EXISTING AIRFIELD FENCE
- EXISTING FIRE HYDRANT
- EXISTING GAS UTILITY
- EXISTING UNDERDRAIN
- EXISTING WATERMAIN
- EXISTING MANHOLE STRUCTURE
- EXISTING TAXIWAY LIGHT
- EXISTING CONTOUR
- NEW 5" P.C.C. SIDEWALK
- NEW P.C.C. FLUME
- NEW STORM SEWER
- NEW INLET
- NEW FLARED END SECTION
- NEW PAVEMENT MARKINGS
- PROPOSED GRADE
- EXISTING GRADE

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