

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
866	3 RS-3	LAKE	35	1
		ILLINOIS	CONTRACT NO. 60J94	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP 866 ILL 83 (BARRON BLVD.)

SOUTH OF ROLLINS RD. TO IL 137 (ANTIOCH RD.)

SECTION: 3 RS-3

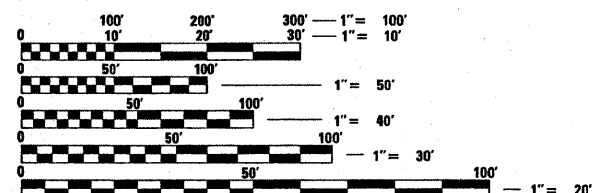
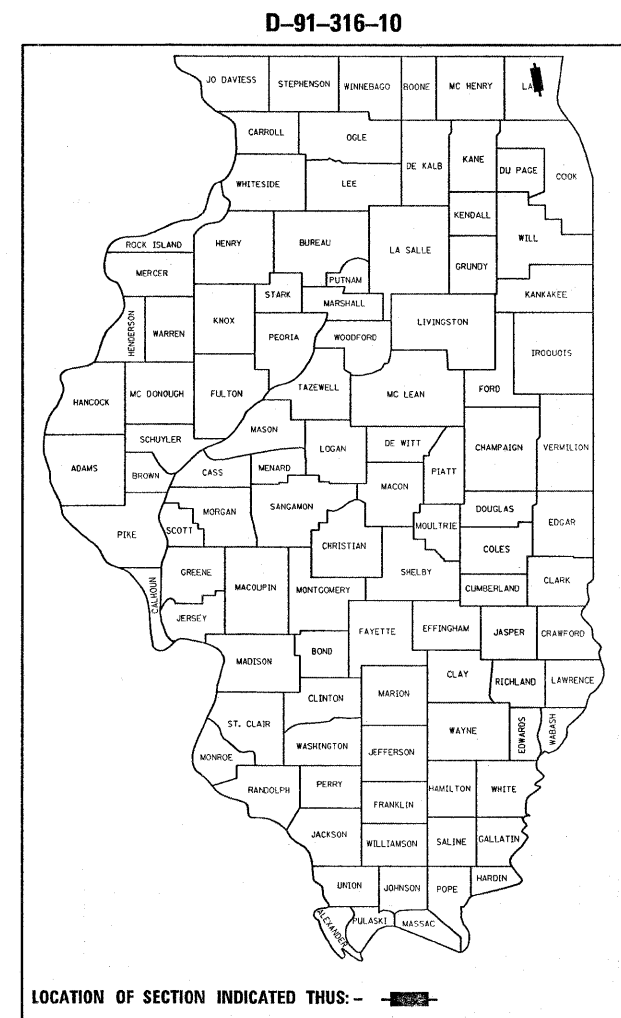
RESURFACING (3P)

LAKE COUNTY

C-91-316-10

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE VILLAGES OF GRAYSLAKE, ROUND LAKE BEACH AND IN UNINCORPORATED VENETIAN VILLAGE.

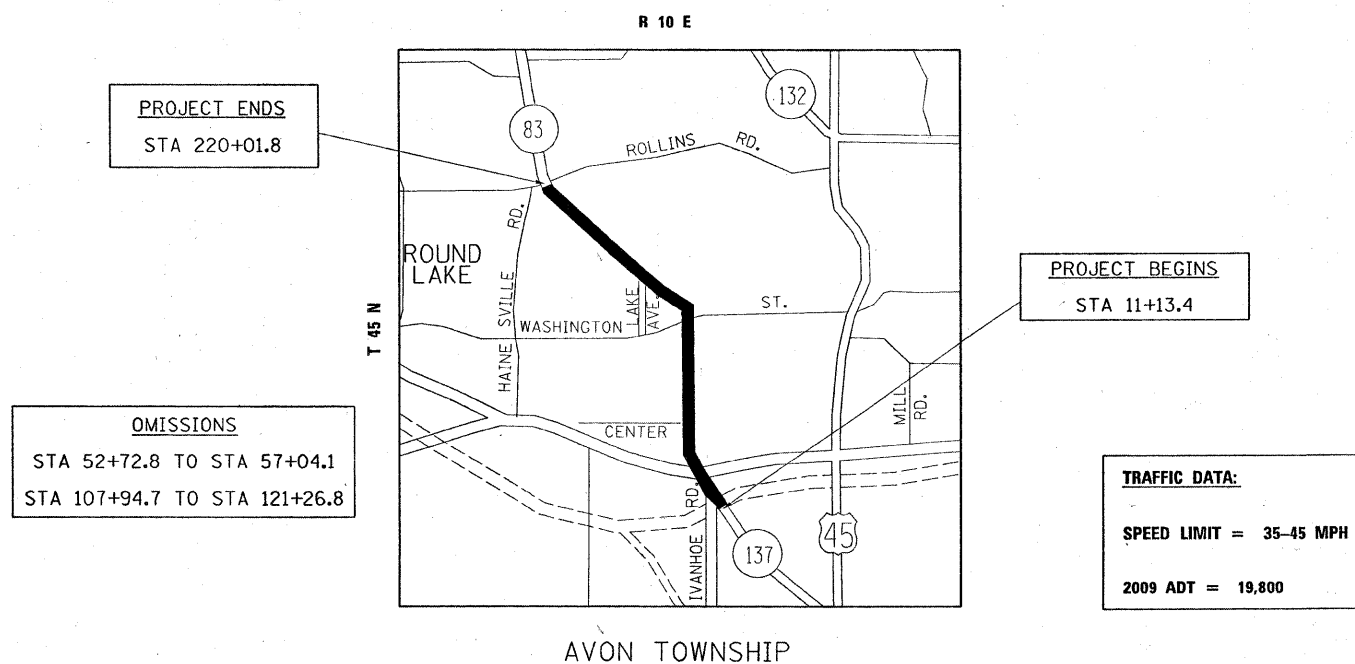


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: JENPAI CHANG (847) 705-4432
PROJECT MANAGER: KEN ENG

CONTRACT NO. 60J94



GROSS LENGTH = 20,888.4 FT. = 3.96 MILE
NET LENGTH = 19,125 FT. = 3.62 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED JANUARY 29, 2010

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 19, 2010
Scott E. Still, P.E.
Acting ENGINEER OF DESIGN AND ENVIRONMENT

March 19, 2010
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

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STATE STANDARDS

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS, & PATTERNS
- 442201-03 CLASS C & D PATCHES
- 606001-04 CONCRETE CURB TYPE B AND COMBINATION
CONCRETE CURB AND GUTTER
- 701301-03 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701306-02 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS
DAY ONLY, FOR SPEEDS > 45 MPH
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
- 701336-05 LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES,
FOR SPEEDS >45 MPH
- 701501-05 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701606-06 URBAN LANE CLOSURE, MULTILANE, 2W, WITH MOUNTABLE
MEDIAN
- 701701-06 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701901-01 TRAFFIC CONTROL DEVICES
- 886001-01 DETECTOR LOOP INSTALLATION

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

10' (3M) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF GRAYSLAKE ROUND LAKE BEACH, AND UNINCORPORATED VENETIAN VILLAGE.

CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2" (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1" (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3" (75MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED AT A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET, WHICH IS INCLUDED IN THE PLANS. UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

CONTACT AREA TRAFFIC FIELD TECHNICIAN DEBBIE HANLON AT (847) 438-2300 AT LEAST TWO WEEKS PRIOR TO INSTALLING FINAL PAVEMENT MARKINGS.

LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT OF THE TYPE SPECIFIED ON THE PLANS, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION WILL BE DETERMINED IN HE FIELD BY THE ENGINEER.

FRAMES AND GRATES ADJUSTMENT SHALL BE COORDINATED WITH THE VILLAGES OF GRAYSLAKE, ROUND LAKE BEACH, AND IN UNINCORPORATED VENETIAN VILLAGE WITHIN THE LIMITS OF THE PROJECT SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

A FULL DEPTH, PERPENDICULAR, AND LONGITUDINAL STRAIGHT JOINT SHALL BE SAWN AT THE ENDS AND ALL EDGES OF CURB AND GUTTER TO BE REMOVED AND WILL NOT BE PAID FOR SEPARATELY. THE COST OF THE SAW CUT WILL BE INCLUDED IN THE BID PRICE FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT PAY ITEM.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE MARKINGS) IN ORDER THAT THESE LOCATIONS CAN BE REESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

FILE NAME =	USER NAME = mosse	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCALE:	SHEET NO. OF SHEETS STA. TO STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwork\FW100T\MOSSE\0177698\01316\0\ght-plan.dgn	DRAWN -	REVISED -	866				3 RS - 3	LAKE	35	2	
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PLOT DATE = 3/11/2010	DATE -	REVISED -	ILLINOIS FED. AID PROJECT								

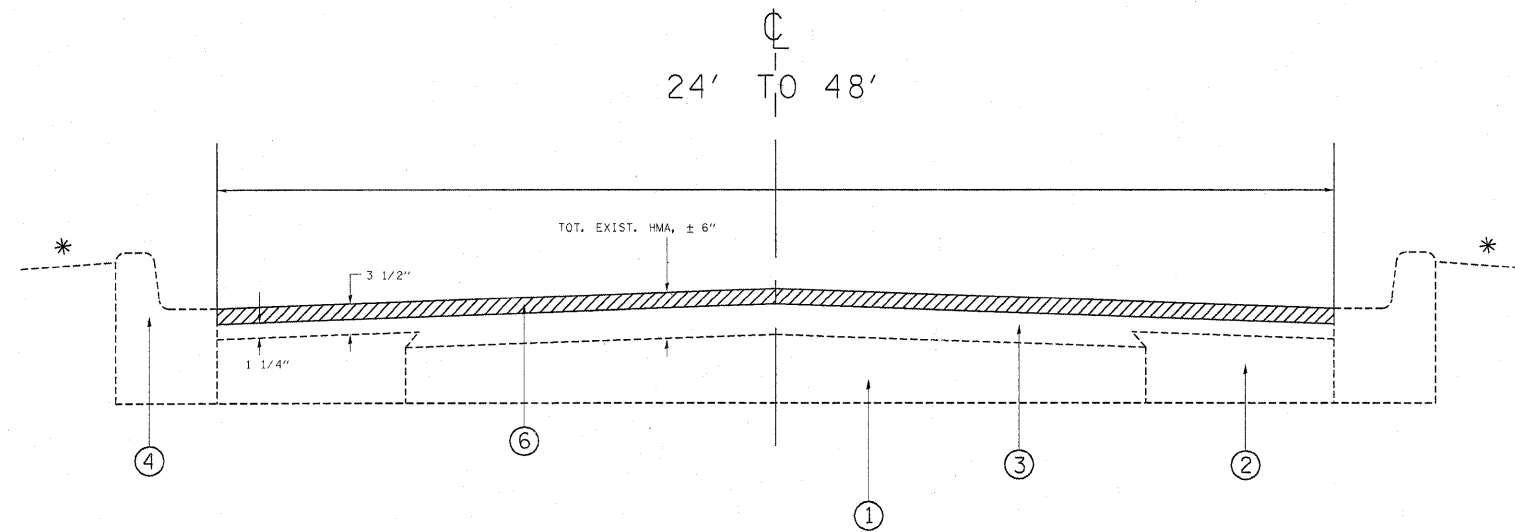
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000-2A					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000-2A				
20201006	GRADING AND SHAPING SHOULDERS	UNIT	230	230					70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	18300	18300				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	390	390					*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1070	1070				
25200110	SODDING, SALT TOLERANT	SO YD	390	390					*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	35200	35200				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	65	65					*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	4400	4400				
40600300	AGGREGATE (PRIME COAT)	TON	326	326					*78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	420	420				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	122	122					*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2100	2100				
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	3365	3365					*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1060	1060				
40600895	CONSTRUCTING TEST STRIP	EACH	2	2					*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	890	890				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	500	500					78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	800	800				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	6850	6850					*88600600	DETECTOR LOOP REPLACEMENT	FOOT	4516	4516				
42001300	PROTECTIVE COAT	SO YD	650	650					X0322256	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	79210	79210					Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	19	19				
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	2850	2850					Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1				
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	2140	2140					70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	/	/				
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	1902	1902					70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	/	/				
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	714	714					70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	/	/				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	455	455					70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	/	/				
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	10	10					* SPECIALTY ITEMS								
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	30	30													
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6													
67100100	MOBILIZATION	L SUM	1	1													
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1													
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	54900	54900													
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1070	1070													
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	35200	35200													
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	4400	4400													
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	420	420													
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	2100	2100													
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1060	1060													

LEGEND

- ① EXISTING PCC PAVEMENT
- ② EXISTING PCC BASE COURSE 9"
- ③ EXISTING HMA SURFACE ± 3 3/4" (AFTER MILLING)
- ④ EXISTING COMB. CONCRETE CURB & GUTTER, VARIOUS TYPES
- ⑤ EXISTING AGGREGATE & HMA SHOULDER
- ⑥ PROP. HMA SURFACE REMOVAL, 2 1/4"
- ⑦ PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑧ PROP. HMA SURFACE COURSE, MIX "D", N70 1 1/2"
- ⑨ PROP. GRADING & SHAPING SHOULDERS

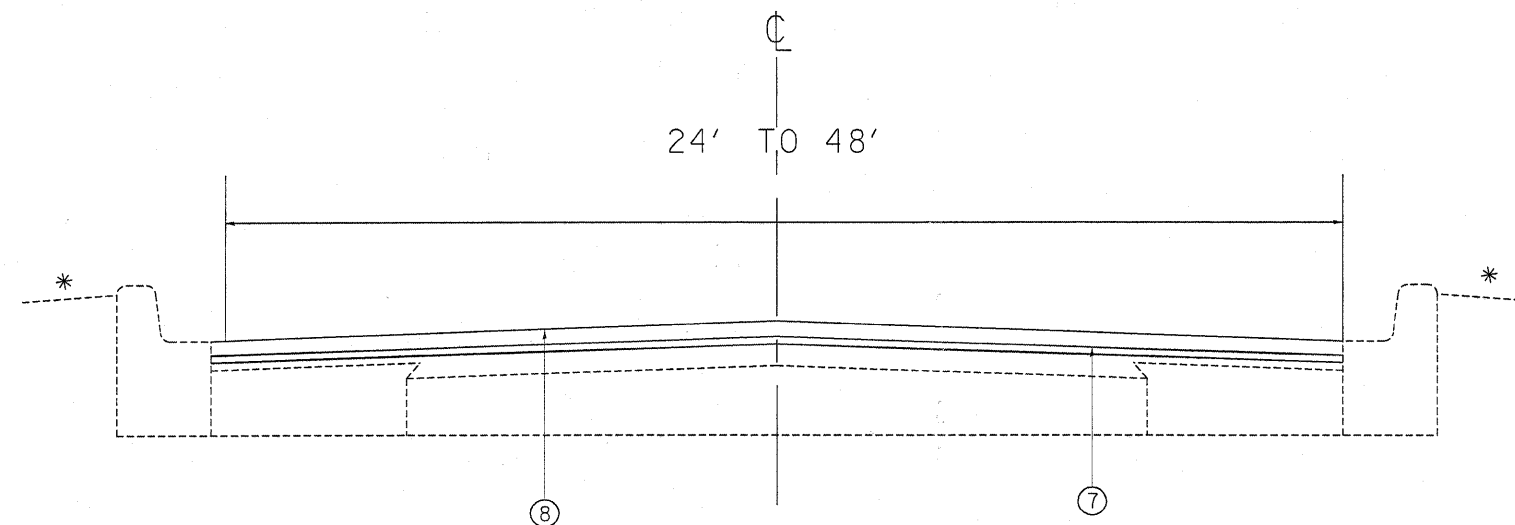
REMOVAL ITEM

* WIDTHS OF HMA AND AGGREGATE SHOULDERS MAY VARY



EXISTING TYPICAL CROSS SECTION

VARIOUS LOCATIONS WHERE CURB & GUTTER PRESENT



PROPOSED TYPICAL CROSS SECTION

HMA MIXTURE REQUIREMENTS

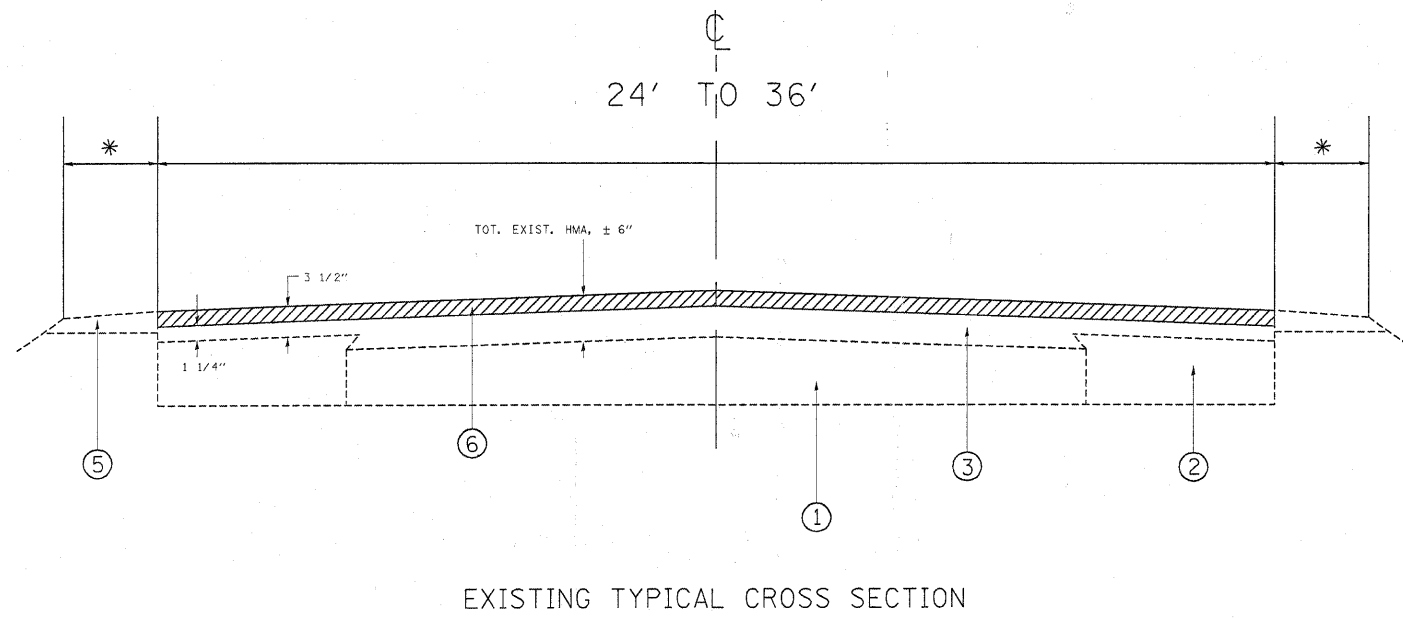
MIXTURE USES	% VOIDS @ Ndes
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% @ 70 GYR.
POLY. LEVELING BINDER (MM), IL-4.75, N50	4% @ 50 GYR.
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURES IS 112 LBS/SQ YD/IN
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

NOTE:

THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

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EXISTING TYPICAL CROSS SECTION

LEGEND

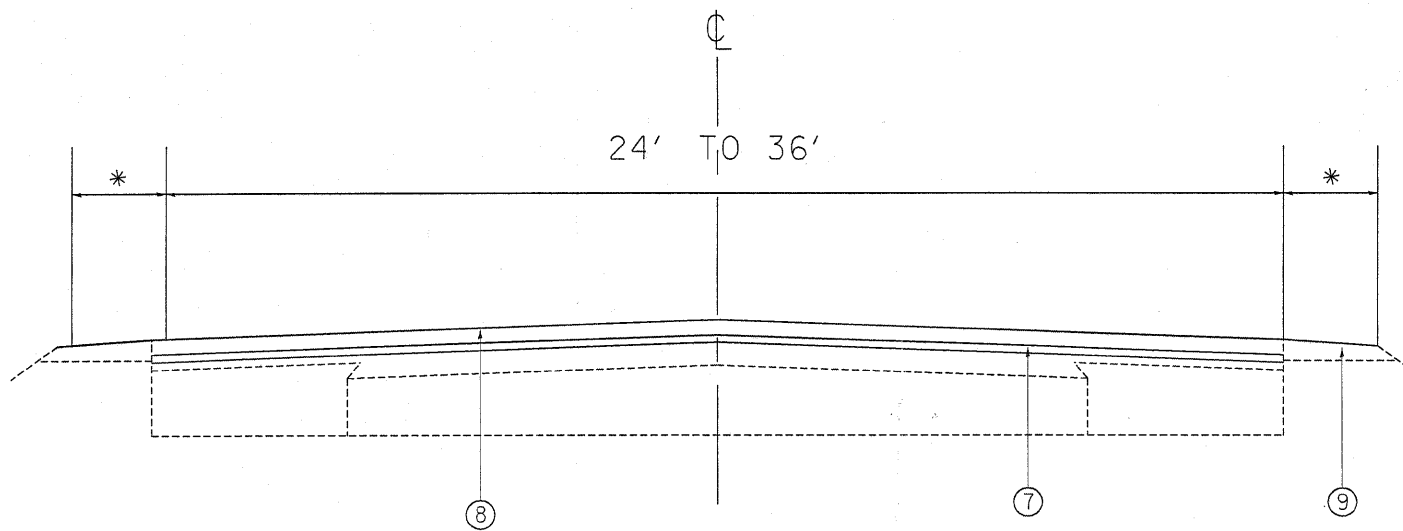
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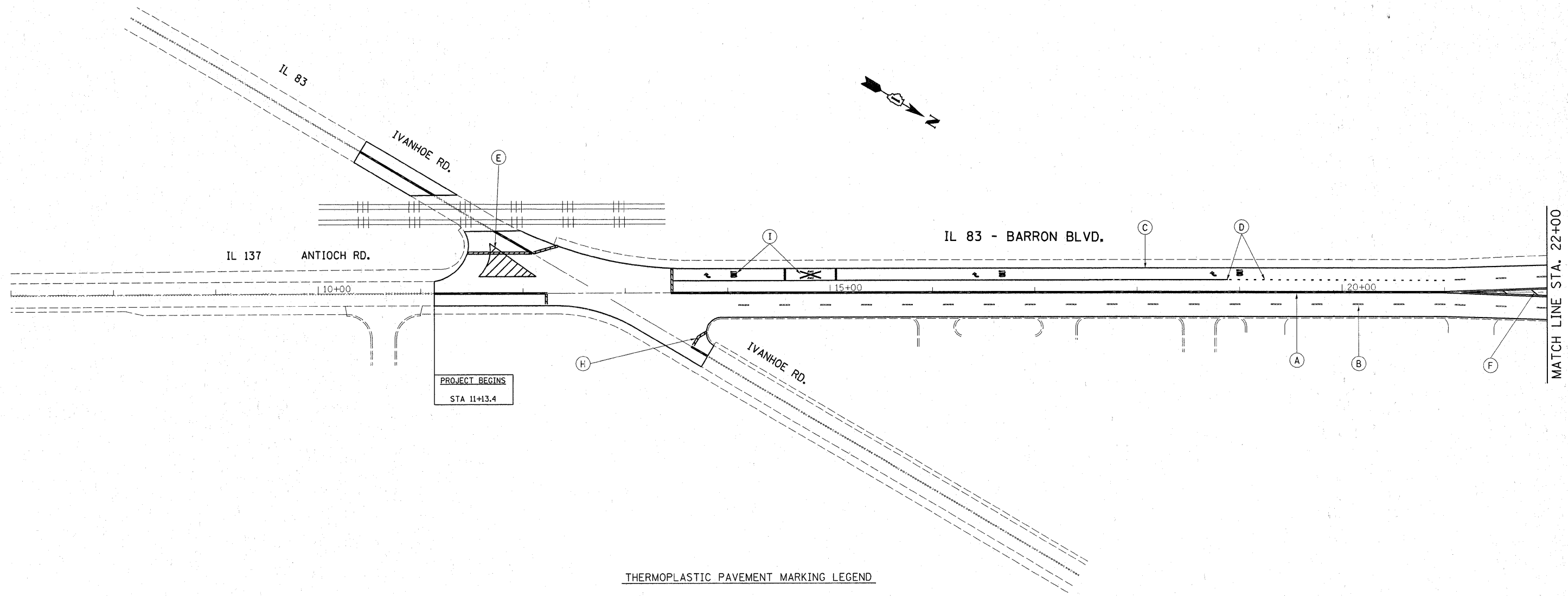
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PROPOSED TYPICAL CROSS SECTION

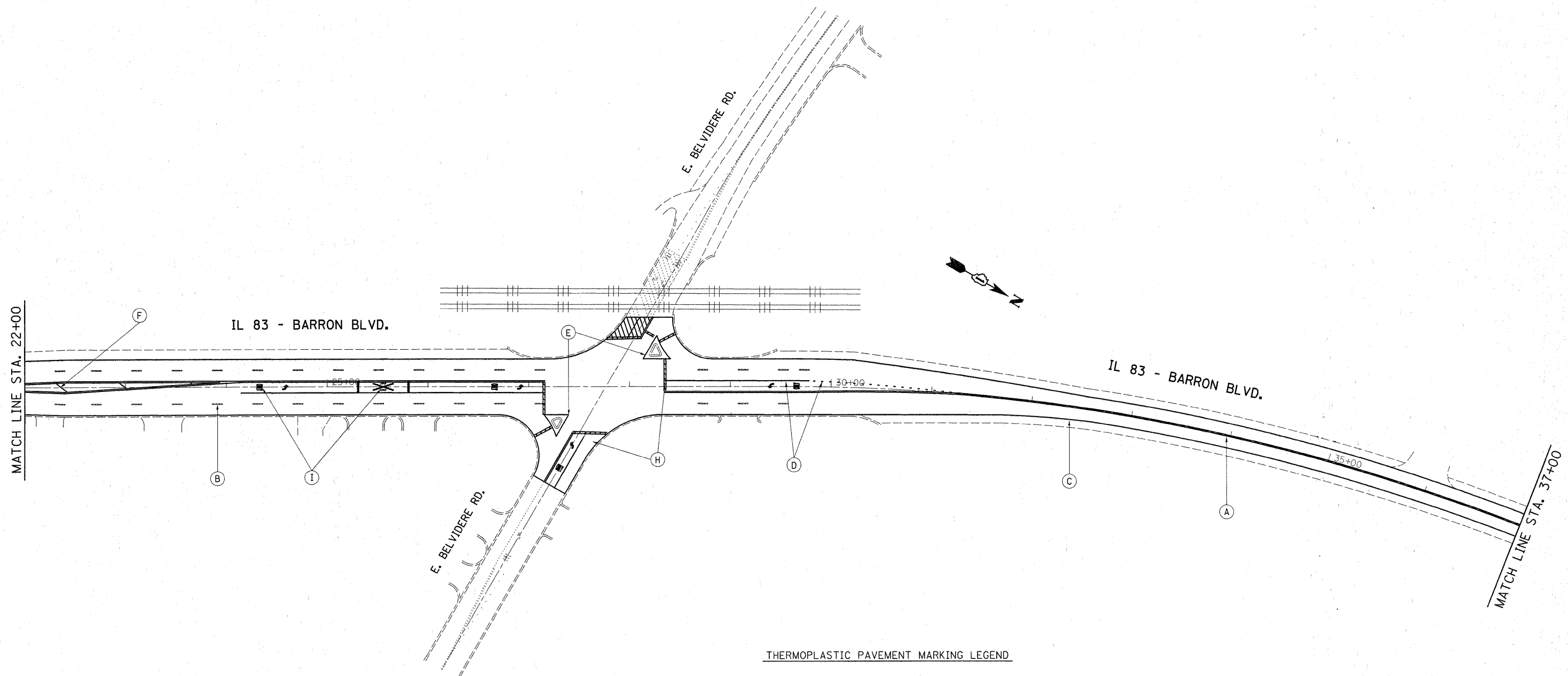
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THERMOPLASTIC PAVEMENT MARKING LEGEND

- (A) 4" DOUBLE SOLID YELLOW, CENTERLINE (TYPICAL)
- (B) 4" WHITE SKIP-DASH, LANELINE (10' DASH, 30' SKIP) (TYPICAL)
- (C) 4" SOLID WHITE, EDGE LINES (TYPICAL)
- (D) 6" SOLID/DOTTED WHITE, TURN LANE LINE (TYPICAL)
- (E) 8" SOLID WHITE, ISLAND OUTLINE (TYPICAL)
- (F) 12" SOLID YELLOW DIAGONAL @ 45° (20-75' C-C MINIMUM OF 5) (TYPICAL)
- (G) 12" SOLID WHITE DIAGONAL @ 45° (75' SPACING) (TYPICAL)
- (H) 24" SOLID WHITE, STOP BAR (TYPICAL)
- (I) LETTERS & SYMBOLS, WHITE (TYPICAL)

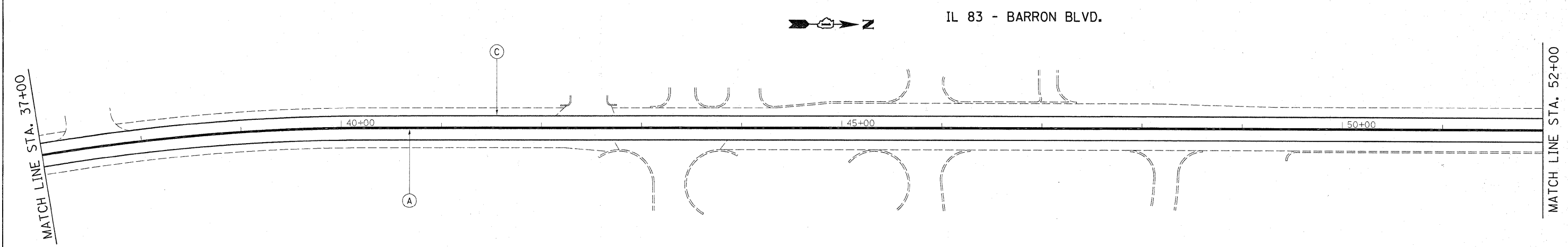
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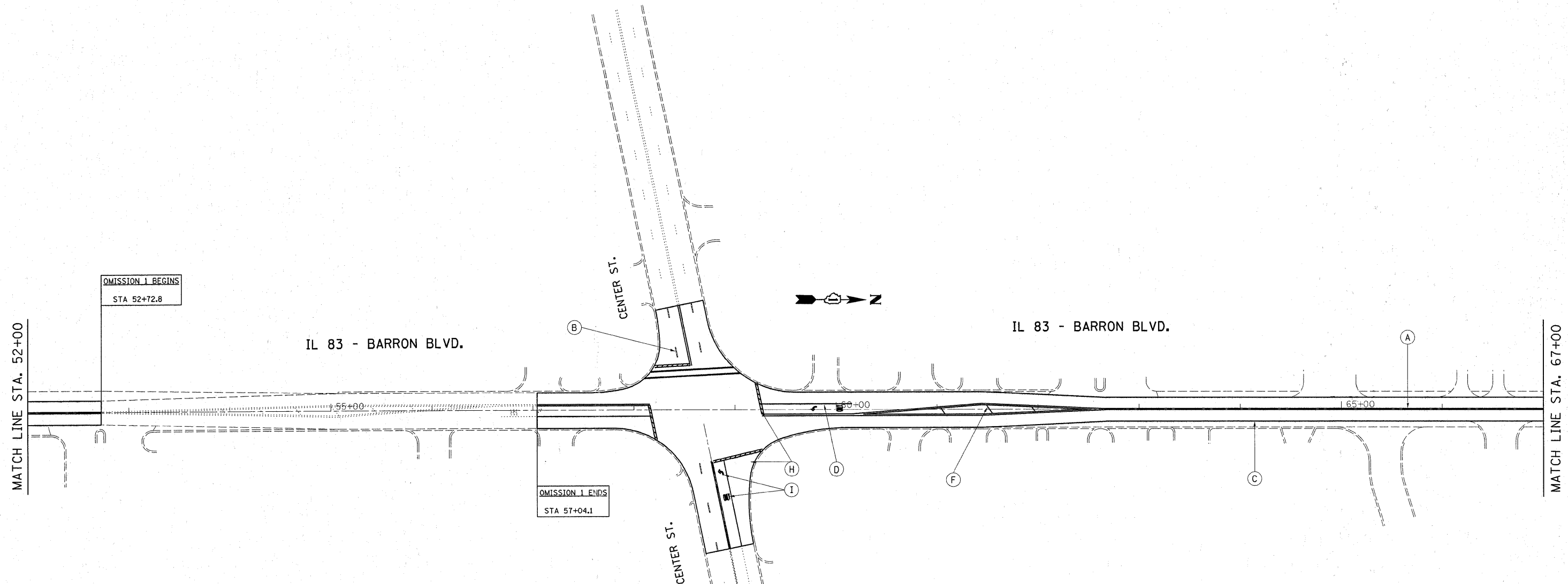


IL 83 - BARRON BLVD.

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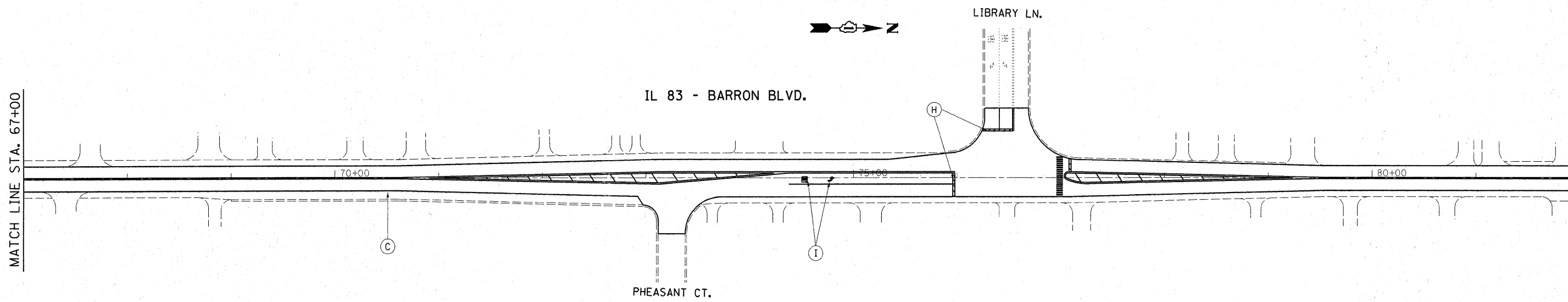
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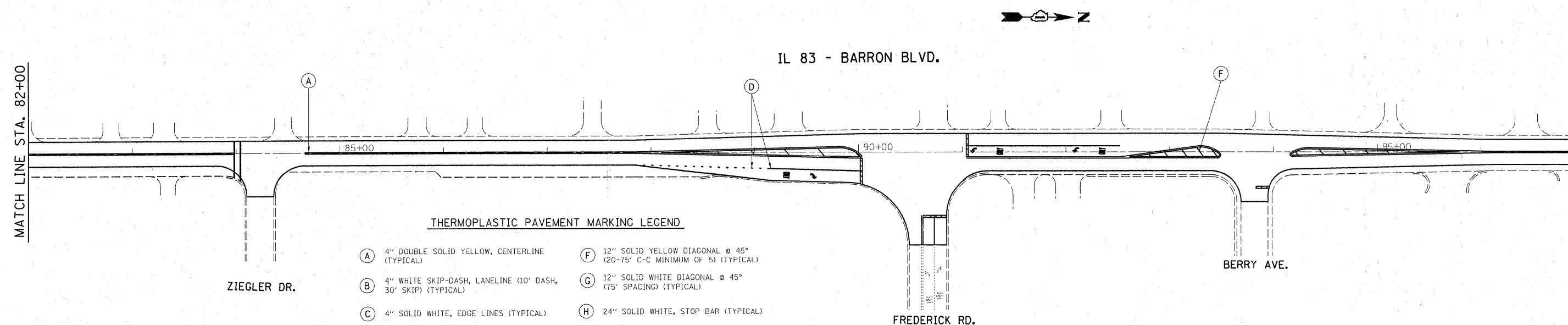
MATCH LINE STA. 67+00

MATCH LINE STA. 82+00



MATCH LINE STA. 82+00

MATCH LINE STA. 97+00



THERMOPLASTIC PAVEMENT MARKING LEGEND

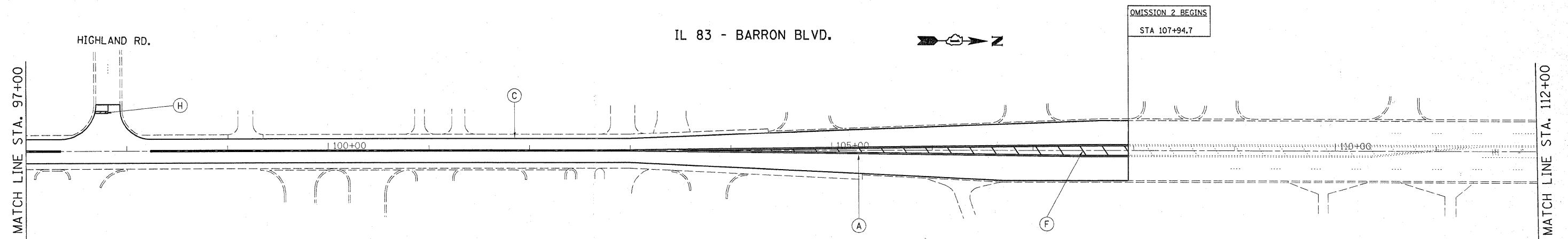
- (A) 4" DOUBLE SOLID YELLOW, CENTERLINE (TYPICAL)
- (B) 4" WHITE SKIP-DASH, LANELINE (10' DASH, 30' SKIP) (TYPICAL)
- (C) 4" SOLID WHITE, EDGE LINES (TYPICAL)
- (D) 6" SOLID/DOTTED WHITE, TURN LANE LINE (TYPICAL)
- (E) 8" SOLID WHITE, ISLAND OUTLINE (TYPICAL)
- (F) 12" SOLID YELLOW DIAGONAL @ 45° (20-75" C-C MINIMUM OF 5) (TYPICAL)
- (G) 12" SOLID WHITE DIAGONAL @ 45° (75' SPACING) (TYPICAL)
- (H) 24" SOLID WHITE, STOP BAR (TYPICAL)
- (I) LETTERS & SYMBOLS, WHITE (TYPICAL)

FILE NAME =	USER NAME = mosse	DESIGNED -	REVISED -
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 3/11/2010	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.

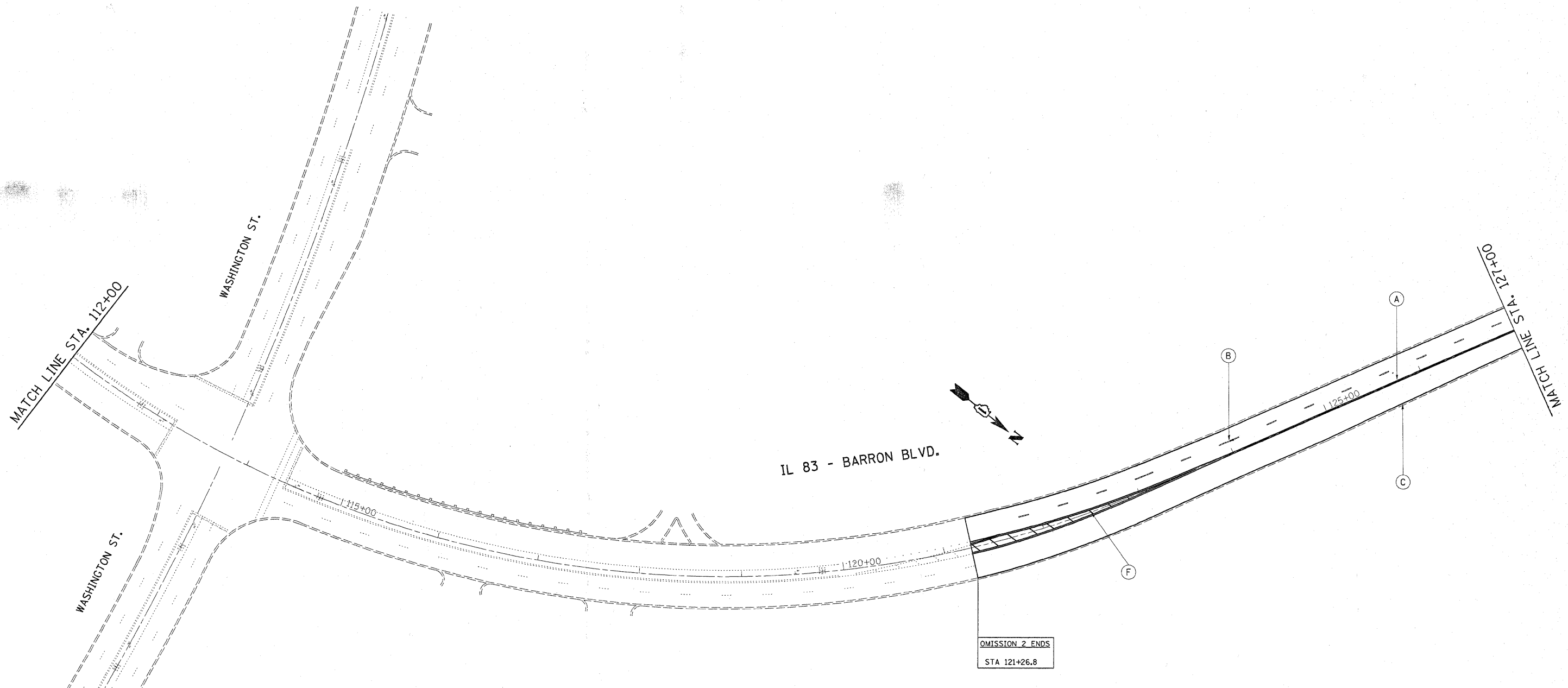
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
866	3 RS-3	LAKE	35	10
CONTRACT NO. 60J94				
ILLINOIS FED. AID PROJECT				



THERMOPLASTIC PAVEMENT MARKING LEGEND

- (A) 4" DOUBLE SOLID YELLOW, CENTERLINE (TYPICAL)
- (B) 4" WHITE SKIP-DASH, LANELINE (10' DASH, 30' SKIP) (TYPICAL)
- (C) 4" SOLID WHITE, EDGE LINES (TYPICAL)
- (D) 6" SOLID/DOTTED WHITE, TURN LANE LINE (TYPICAL)
- (E) 8" SOLID WHITE, ISLAND OUTLINE (TYPICAL)
- (F) 12" SOLID YELLOW DIAGONAL @ 45° (20-75' C-C MINIMUM OF 5) (TYPICAL)
- (G) 12" SOLID WHITE DIAGONAL @ 45° (75' SPACING) (TYPICAL)
- (H) 24" SOLID WHITE, STOP BAR (TYPICAL)
- (I) LETTERS & SYMBOLS, WHITE (TYPICAL)

FILE NAME =	USER NAME = mcsse	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct\pwork\PWIDOT\MOSSSE\d0177698\01316	@-sht-plan.dgn	DRAWN -	REVISED -		866	3 RS-3	LAKE	35	11			
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 60J94							
	PLOT DATE = 2/3/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



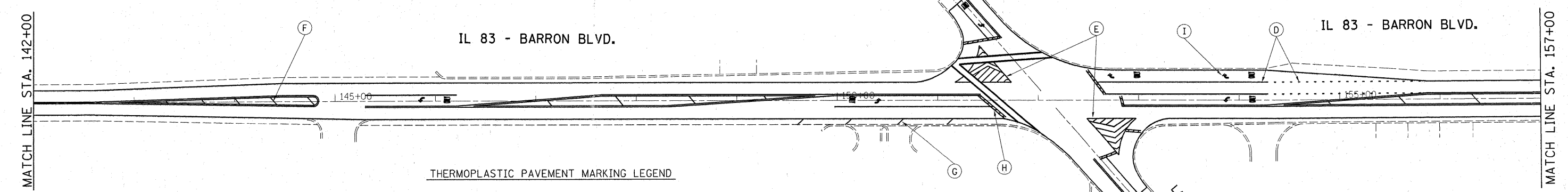
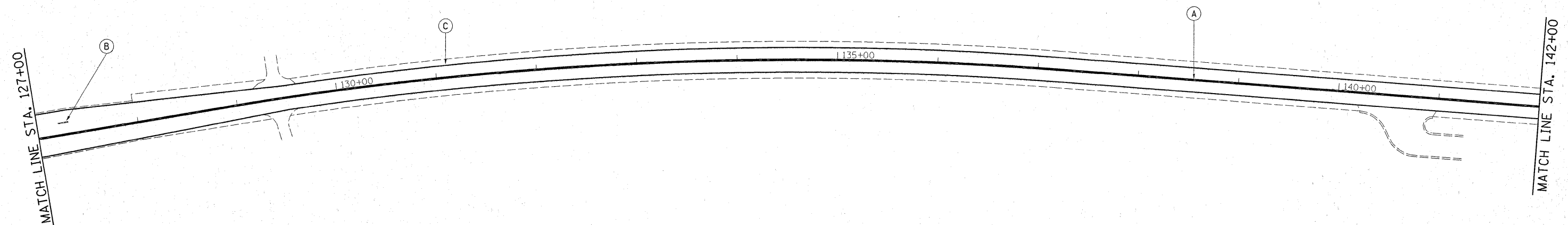
THERMOPLASTIC PAVEMENT MARKING LEGEND

- (A) 4" DOUBLE SOLID YELLOW, CENTERLINE (TYPICAL)
- (B) 4" WHITE SKIP-DASH, LANELINE (10' DASH, 30' SKIP) (TYPICAL)
- (C) 4" SOLID WHITE, EDGE LINES (TYPICAL)
- (D) 6" SOLID/DOTTED WHITE, TURN LANE LINE (TYPICAL)
- (E) 8" SOLID WHITE, ISLAND OUTLINE (TYPICAL)
- (F) 12" SOLID YELLOW DIAGONAL @ 45° (20-75' C-C MINIMUM OF 5) (TYPICAL)
- (G) 12" SOLID WHITE DIAGONAL @ 45° (75' SPACING) (TYPICAL)
- (H) 24" SOLID WHITE, STOP BAR (TYPICAL)
- (I) LETTERS & SYMBOLS, WHITE (TYPICAL)

OMISSION 2 ENDS
STA 121+26.8

FILE NAME =	USER NAME = m0555e	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\pw_work\PWIDOT\M0555E\cd0177698\DI318	@-sht-plan.dgn	DRAWN -	REVISED -		866	3 RS-3	LAKE	35	12				CONTRACT NO. 60J94			
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT											
	PLOT DATE = 2/3/2010	DATE -	REVISED -													

IL 83 - BARRON BLVD.



THERMOPLASTIC PAVEMENT MARKING LEGEND

- (A) 4" DOUBLE SOLID YELLOW, CENTERLINE (TYPICAL)
- (B) 4" WHITE SKIP-DASH, LANELINE (10' DASH, 30' SKIP) (TYPICAL)
- (C) 4" SOLID WHITE, EDGE LINES (TYPICAL)
- (D) 6" SOLID/DOTTED WHITE, TURN LANE LINE (TYPICAL)
- (E) 8" SOLID WHITE, ISLAND OUTLINE (TYPICAL)
- (F) 12" SOLID YELLOW DIAGONAL @ 45° (20'-75' C-C MINIMUM OF 5) (TYPICAL)
- (G) 12" SOLID WHITE DIAGONAL @ 45° (75' SPACING) (TYPICAL)
- (H) 24" SOLID WHITE, STOP BAR (TYPICAL)
- (I) LETTERS & SYMBOLS, WHITE (TYPICAL)

FILE NAME =	USER NAME = mossse	DESIGNED -	REVISED -
c:\pwwork\pwwid\MOSSSE\d0177698\01316	0-sh-t-plan.dgn	DRAWN -	REVISED -
PLOT SCALE = 50.0000 "/ IN.	CHECKED -	REVISED -	REVISED -
PLOT DATE = 3/11/2010	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
866	3 RS-3	LAKE	35	13
CONTRACT NO. 60J94				
ILLINOIS FED. AID PROJECT				

MATCH LINE STA. 157+00

MATCH LINE STA. 172+00

LEXINGTON LN.

IL 83 - BARRON BLVD.

MATCH LINE STA. 172+00

MATCH LINE STA. 187+00

IL 83 - BARRON BLVD.

W. SHOREWOOD RD.

IL 83 - BARRON BLVD.

THERMOPLASTIC PAVEMENT MARKING LEGEND

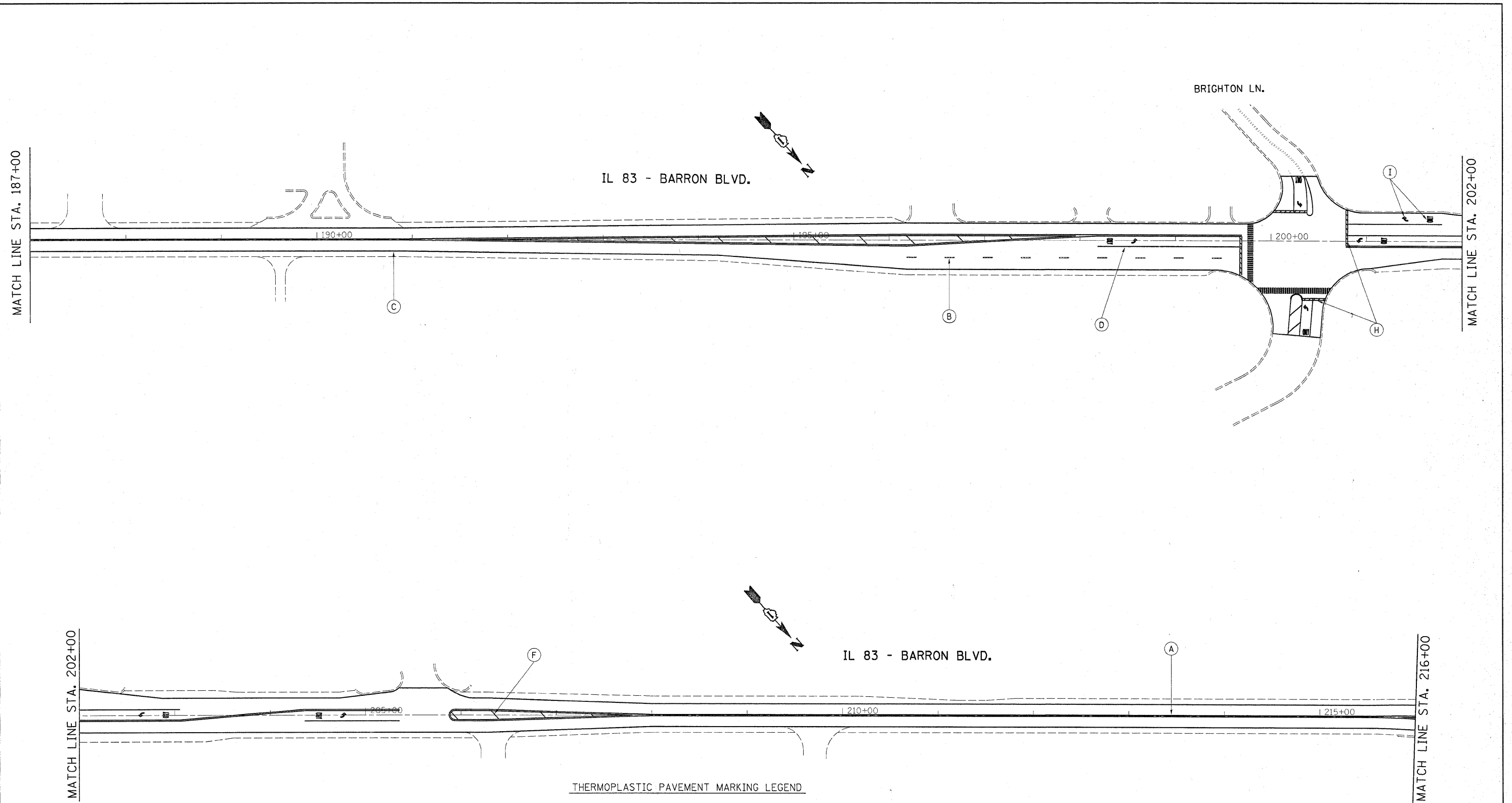
- (A) 4" DOUBLE SOLID YELLOW, CENTERLINE (TYPICAL)
- (B) 4" WHITE SKIP-DASH, LANELINE (10' DASH, 30' SKIP) (TYPICAL)
- (C) 4" SOLID WHITE, EDGE LINES (TYPICAL)
- (D) 6" SOLID/DOTTED WHITE, TURN LANE LINE (TYPICAL)
- (E) 8" SOLID WHITE, ISLAND OUTLINE (TYPICAL)
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- (G) 12" SOLID WHITE DIAGONAL @ 45° (75' SPACING) (TYPICAL)
- (H) 24" SOLID WHITE, STOP BAR (TYPICAL)
- (I) LETTERS & SYMBOLS, WHITE (TYPICAL)

FILE NAME =	USER NAME = mossse	DESIGNED -	REVISED -
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	PLOT DATE = 2/3/2010	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: SHEET NO. OF SHEETS STA. TO STA.

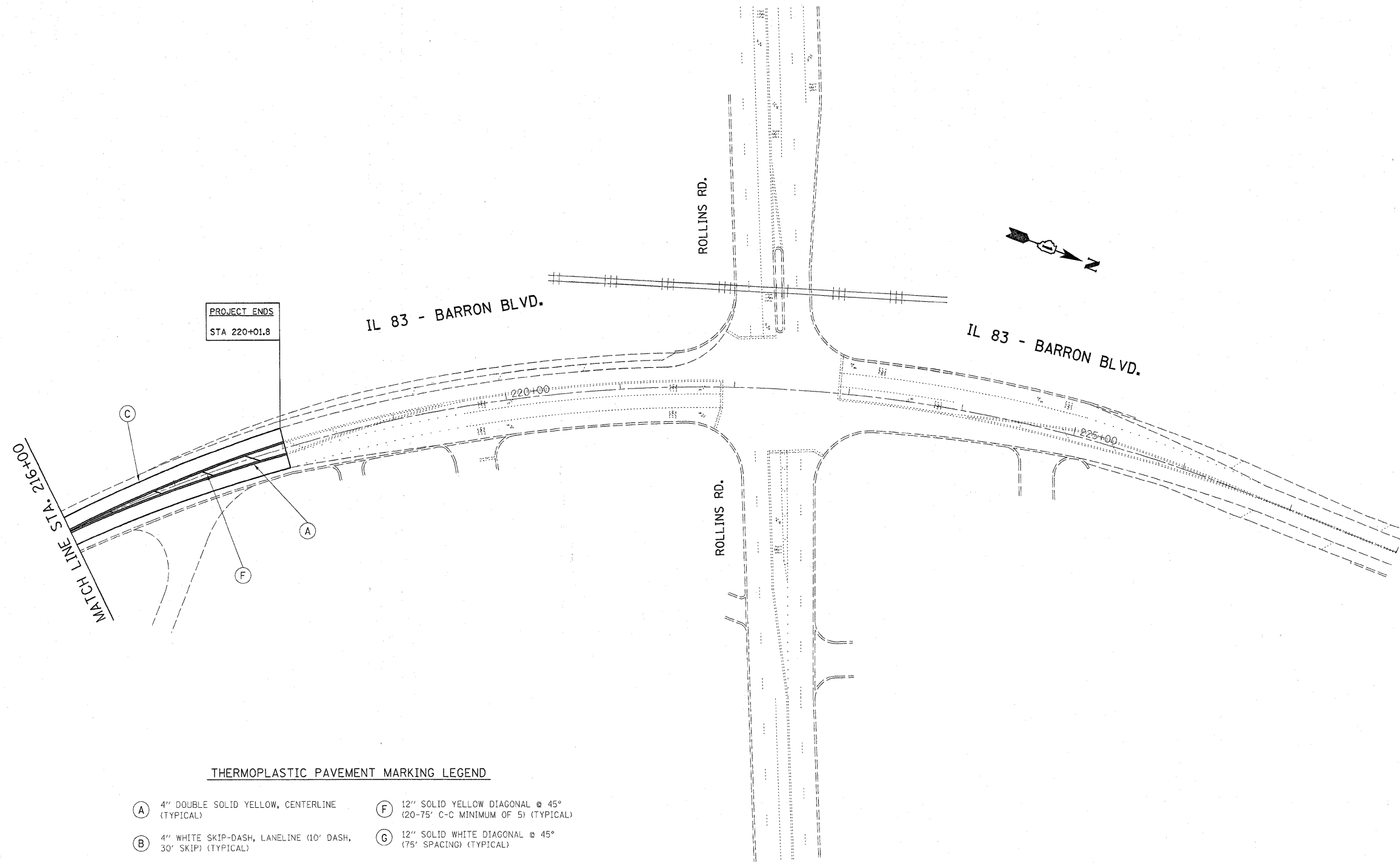
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
866	3 RS-3	LAKE	35	14
CONTRACT NO. 60J94				
ILLINOIS FED. AID PROJECT				



THERMOPLASTIC PAVEMENT MARKING LEGEND

- (A) 4" DOUBLE SOLID YELLOW, CENTERLINE (TYPICAL)
- (B) 4" WHITE SKIP-DASH, LANELINE (10' DASH, 30' SKIP) (TYPICAL)
- (C) 4" SOLID WHITE, EDGE LINES (TYPICAL)
- (D) 6" SOLID/DOTTED WHITE, TURN LANE LINE (TYPICAL)
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- (H) 24" SOLID WHITE, STOP BAR (TYPICAL)
- (I) LETTERS & SYMBOLS, WHITE (TYPICAL)

FILE NAME =	USER NAME = mossse	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cs:\pw_work\PW1001\MOSSSE\177698\01318	@-sht-plan.dgn	DRAWN -	REVISED -		866	3	RS-3	LAKE	35	15					
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 60J94										
	PLOT DATE = 2/3/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT										



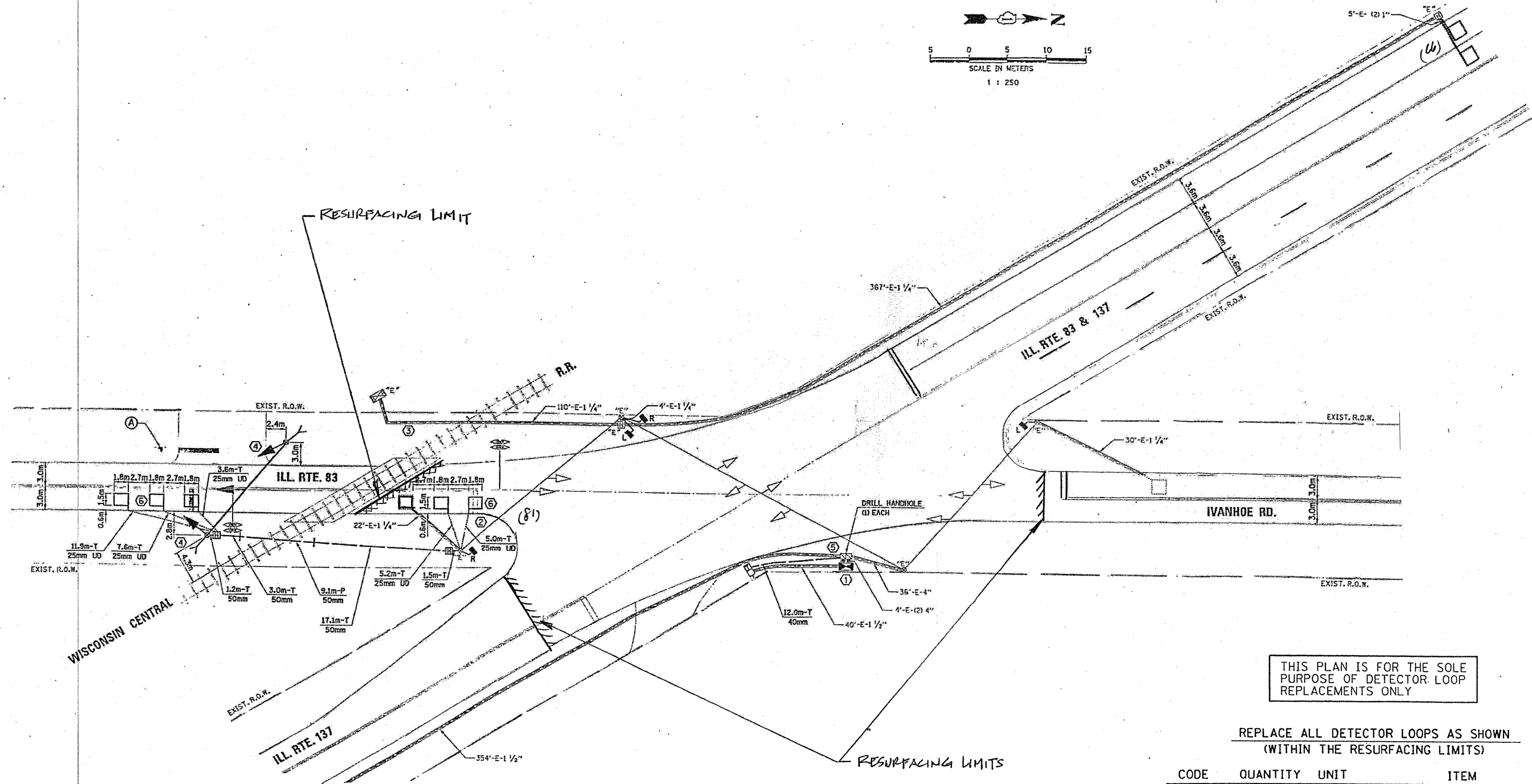
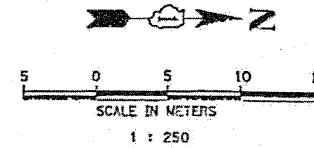
THERMOPLASTIC PAVEMENT MARKING LEGEND

- (A) 4" DOUBLE SOLID YELLOW, CENTERLINE (TYPICAL)
- (B) 4" WHITE SKIP-DASH, LANELINE (10' DASH, 30' SKIP) (TYPICAL)
- (C) 4" SOLID WHITE, EDGE LINES (TYPICAL)
- (D) 6" SOLID/DOTTED WHITE, TURN LANE LINE (TYPICAL)
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- (H) 24" SOLID WHITE, STOP BAR (TYPICAL)
- (I) LETTERS & SYMBOLS, WHITE (TYPICAL)

FILE NAME =	USER NAME = mosse	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION																
c:\pw_work\PWIDOT\MOSSE\d0177698\01318	0-shr-plan.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.										
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		<table border="1" style="font-size: small;"> <tr> <th>F.A.P. RTE.</th> <th>SECTION</th> <th>COUNTY</th> <th>TOTAL SHEETS</th> <th>SHEET NO.</th> </tr> <tr> <td>866</td> <td>3 RS-3</td> <td>LAKE</td> <td>35</td> <td>16</td> </tr> </table>						F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	866	3 RS-3	LAKE	35	16
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS		SHEET NO.															
866	3 RS-3	LAKE	35	16																
	PLOT DATE = 2/3/2010	DATE -	REVISED -	<table border="1" style="font-size: x-small;"> <tr> <td colspan="5" style="text-align: center;">CONTRACT NO. 60J94</td> </tr> <tr> <td colspan="5" style="text-align: center;">ILLINOIS FED. AID PROJECT</td> </tr> </table>						CONTRACT NO. 60J94					ILLINOIS FED. AID PROJECT					
CONTRACT NO. 60J94																				
ILLINOIS FED. AID PROJECT																				

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	147	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kenthaphixoybc	DESIGNED - BCK	REVISED -
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	PLOT DATE = 4/3/2009	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

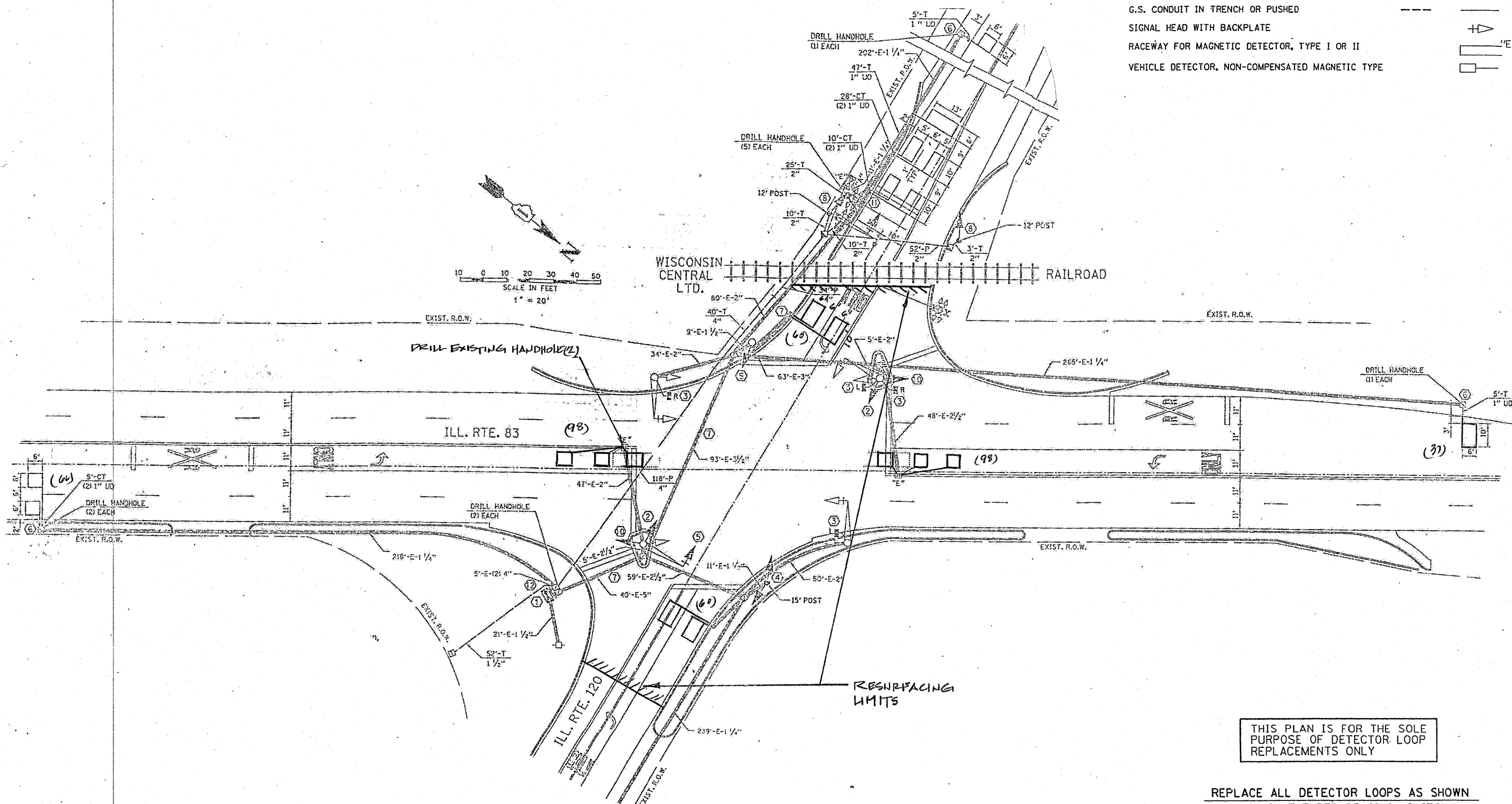
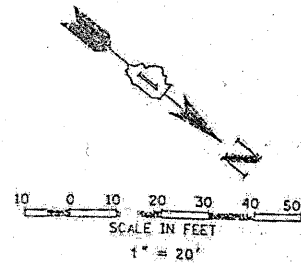
DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. RTE. 83/137 @ IVANHOE ROAD

SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.
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F.A.P. RTE. 866	SECTION 3RS-3	COUNTY LAKE	TOTAL SHEETS 35	SHEET NO. 17
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	419	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =
 c:\pwwork\pwwid\KANTHAPHIXAYBC\d01126

USER NAME = kanthaphixaybc
 \traffic.legend.v7.dgn
 PLOT SCALE = 39.9360 "/>

DESIGNED - BCK	REVISED -
DRAWN - BCK	REVISED -
CHECKED - DAD	REVISED -
DATE	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

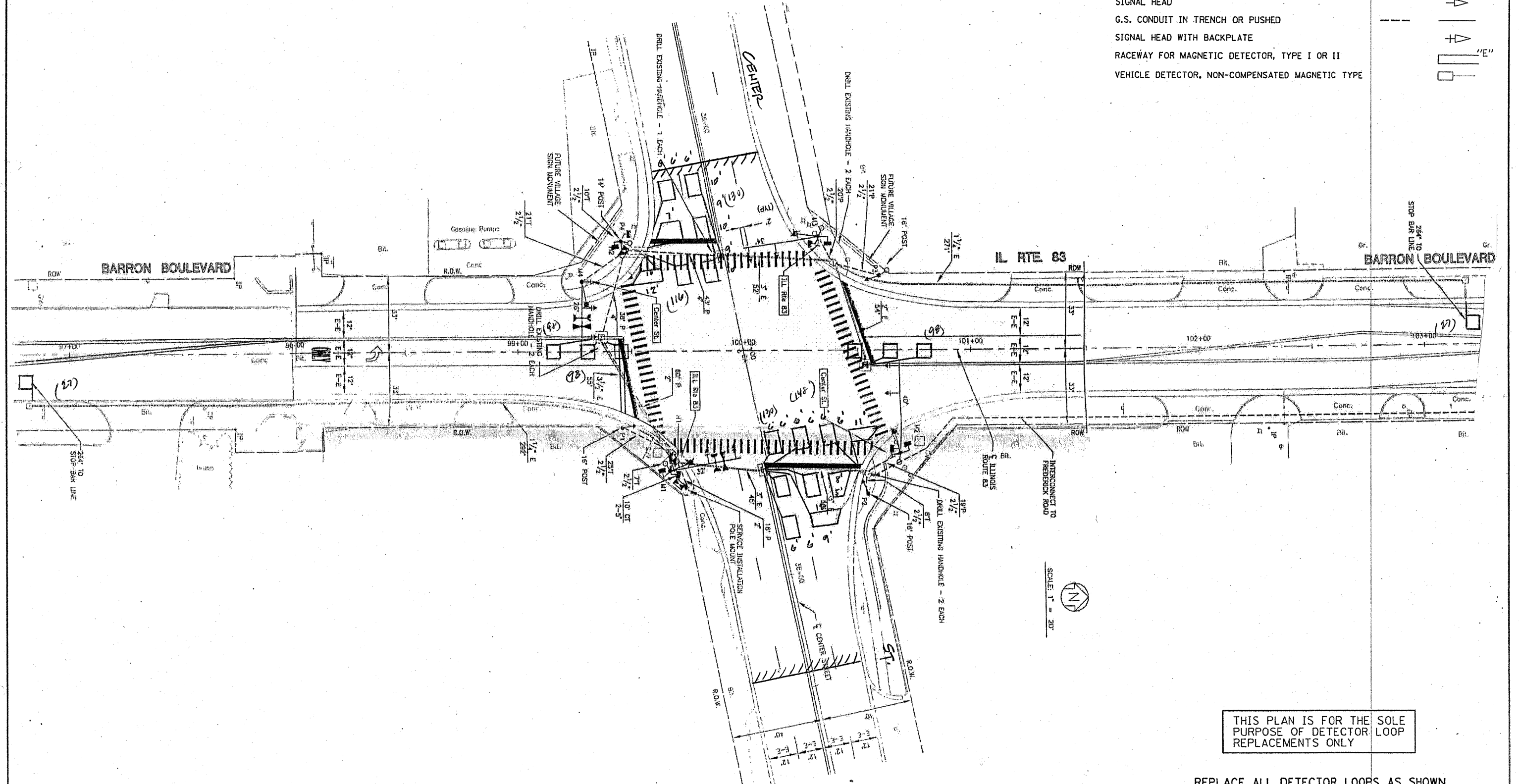
**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
 ILL. ROUTE 83 @ ILL. ROUTE 120**

F.A.V. RTE. 860	SECTION 3 RS-3	COUNTY LAKE	TOTAL SHEETS 35	SHEET NO. 18
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO.	

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	774	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kenthphixojbc	DESIGNED - BCK	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

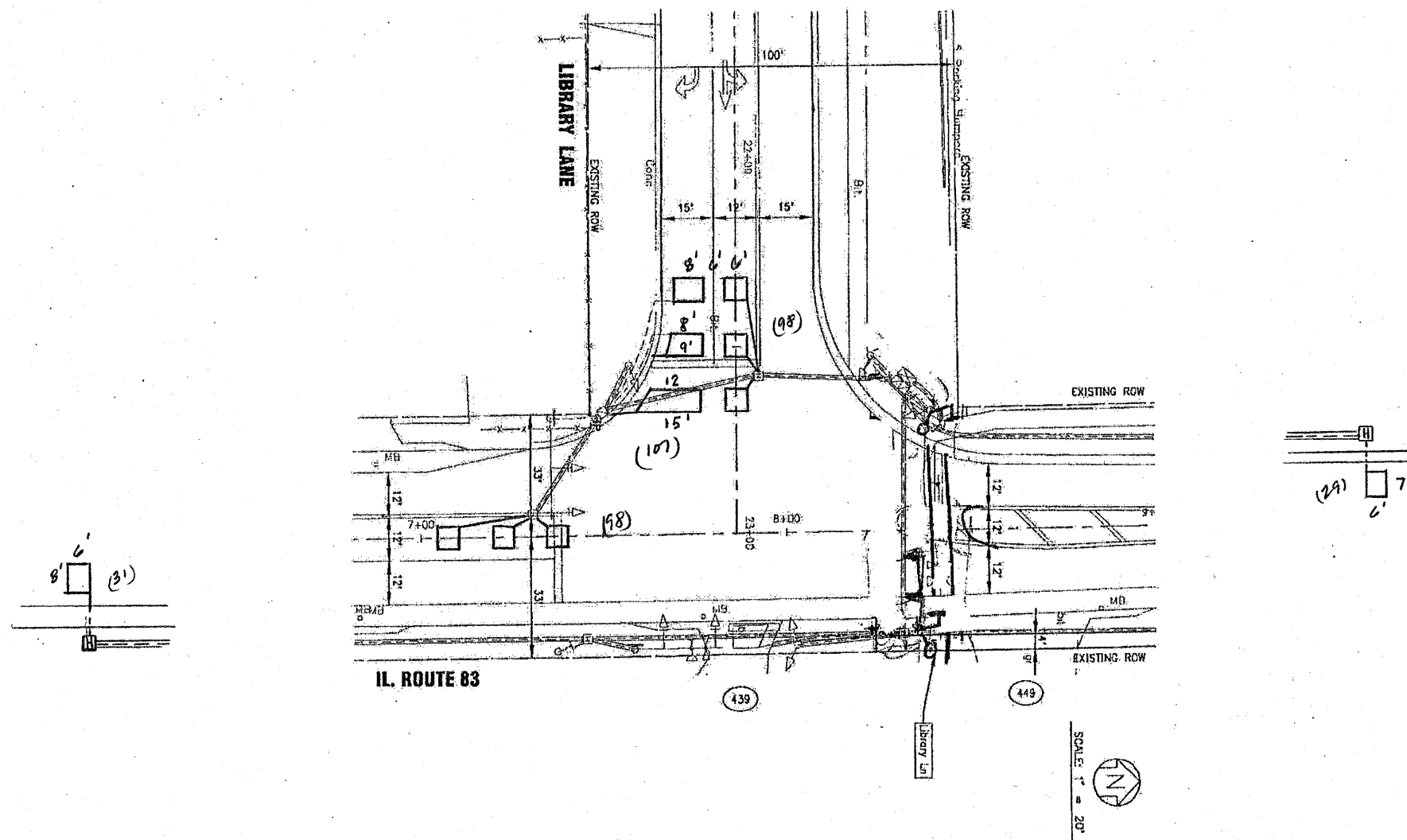
**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 83 @ CENTER STREET**

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
866	3 RS - 3	LAKE	35	19
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO.	

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

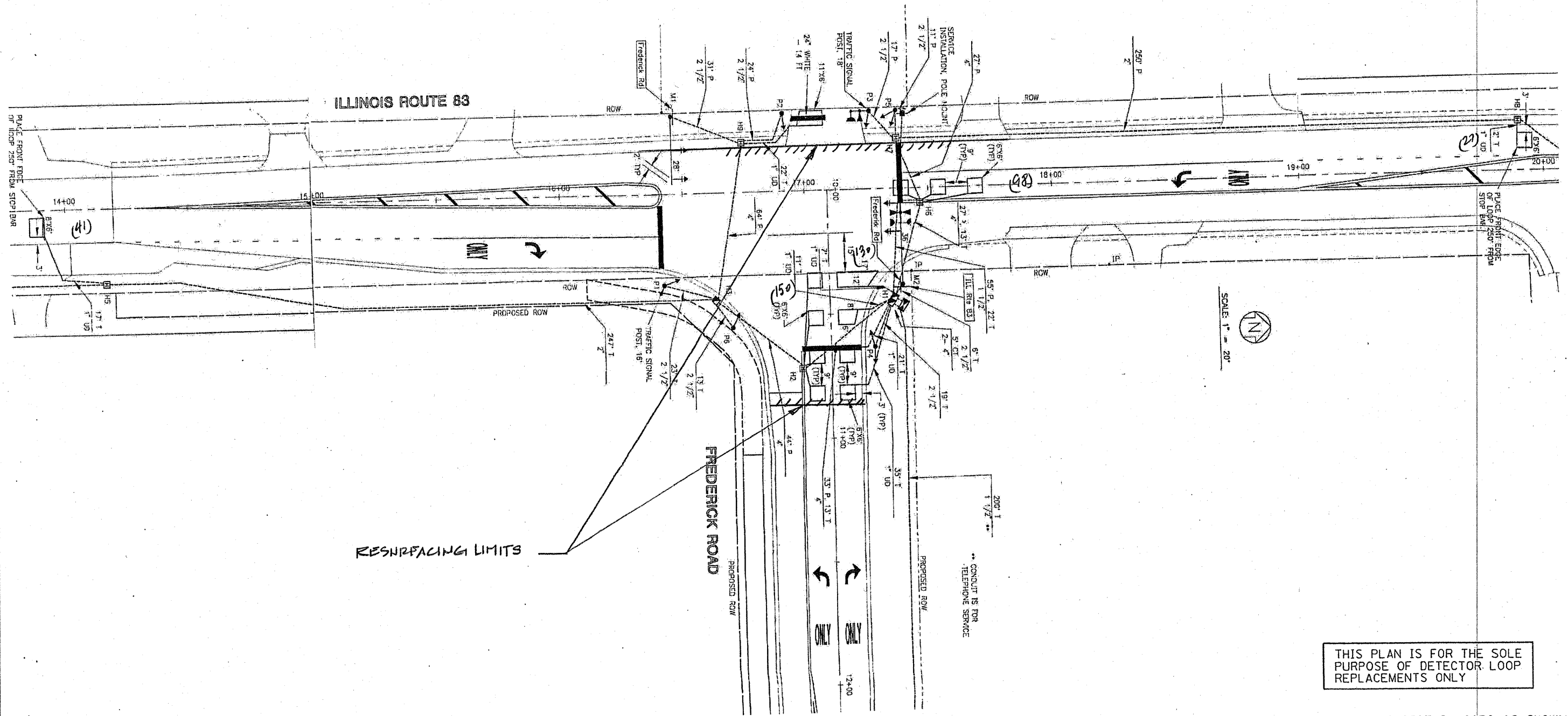
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	363	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kenthaphixaybc	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. ROUTE 83 @ LIBRARY LANE	F.A.V. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwidot\KANTHAPHIXAYBC\01126	truffic.legend.v7.dgn	DRAWN - BCK	REVISED -			060	3 RS-3	LAKE	35	20	
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -			CONTRACT NO.					
	PLOT DATE = 4/3/2009	DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					
				SCALE: NONE		SHEET NO. OF SHEETS		STA. TO STA.			

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	446	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = konthaphixaybc	DESIGNED - BCK	REVISED -
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	PLOT SCALE = 39.9360 1/1 IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 4/3/2009	DATE	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

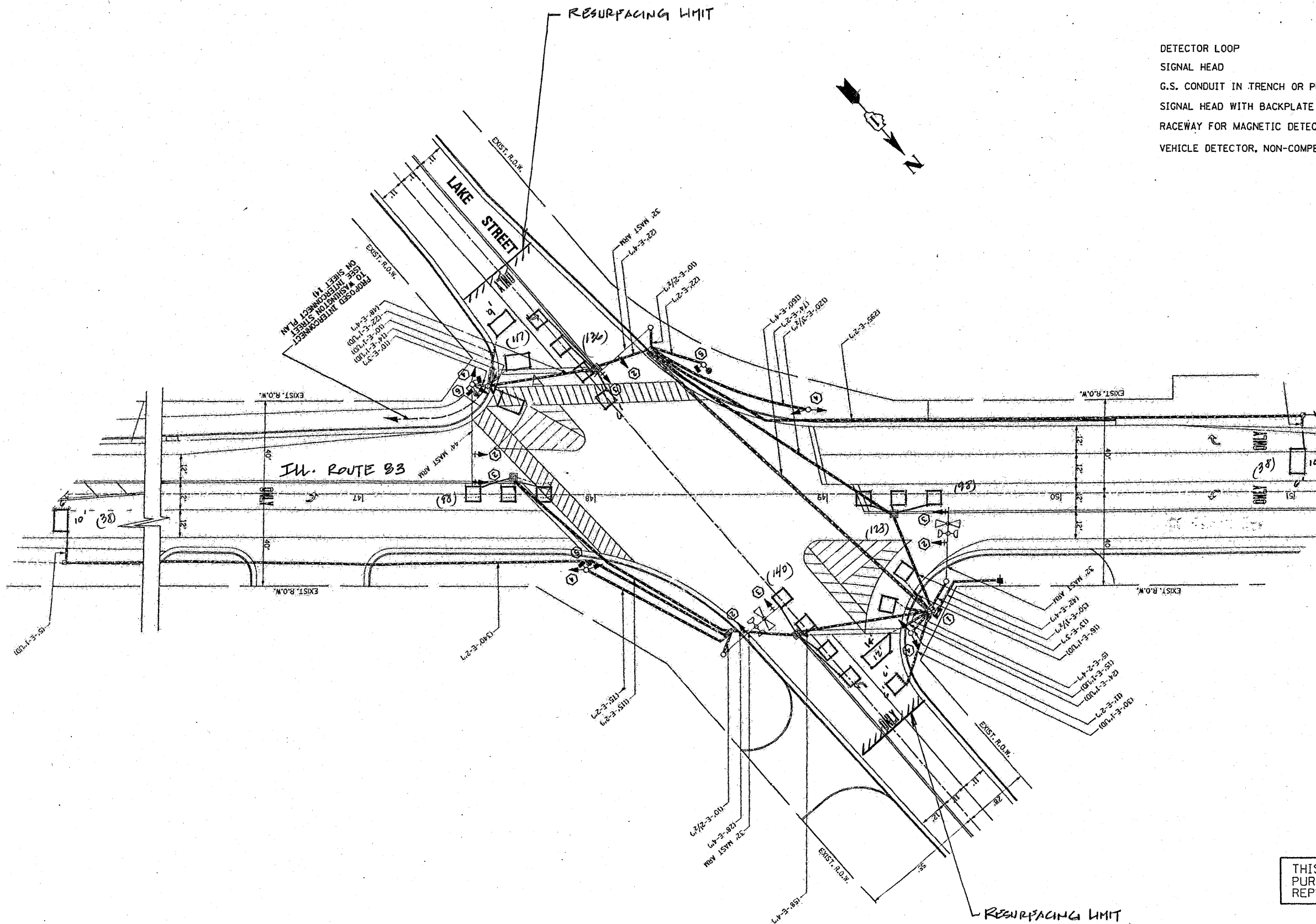
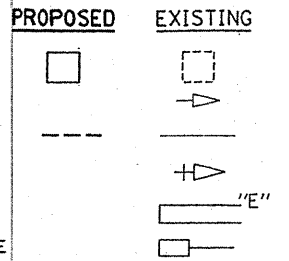
**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 83 @ FREDERICK RD.**

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
860	3 RS-3	LAKE	35	21
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO.	

TRAFFIC SIGNAL LEGEND

- DETECTOR LOOP
- SIGNAL HEAD
- G.S. CONDUIT IN TRENCH OR PUSHED
- SIGNAL HEAD WITH BACKPLATE
- RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II
- VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

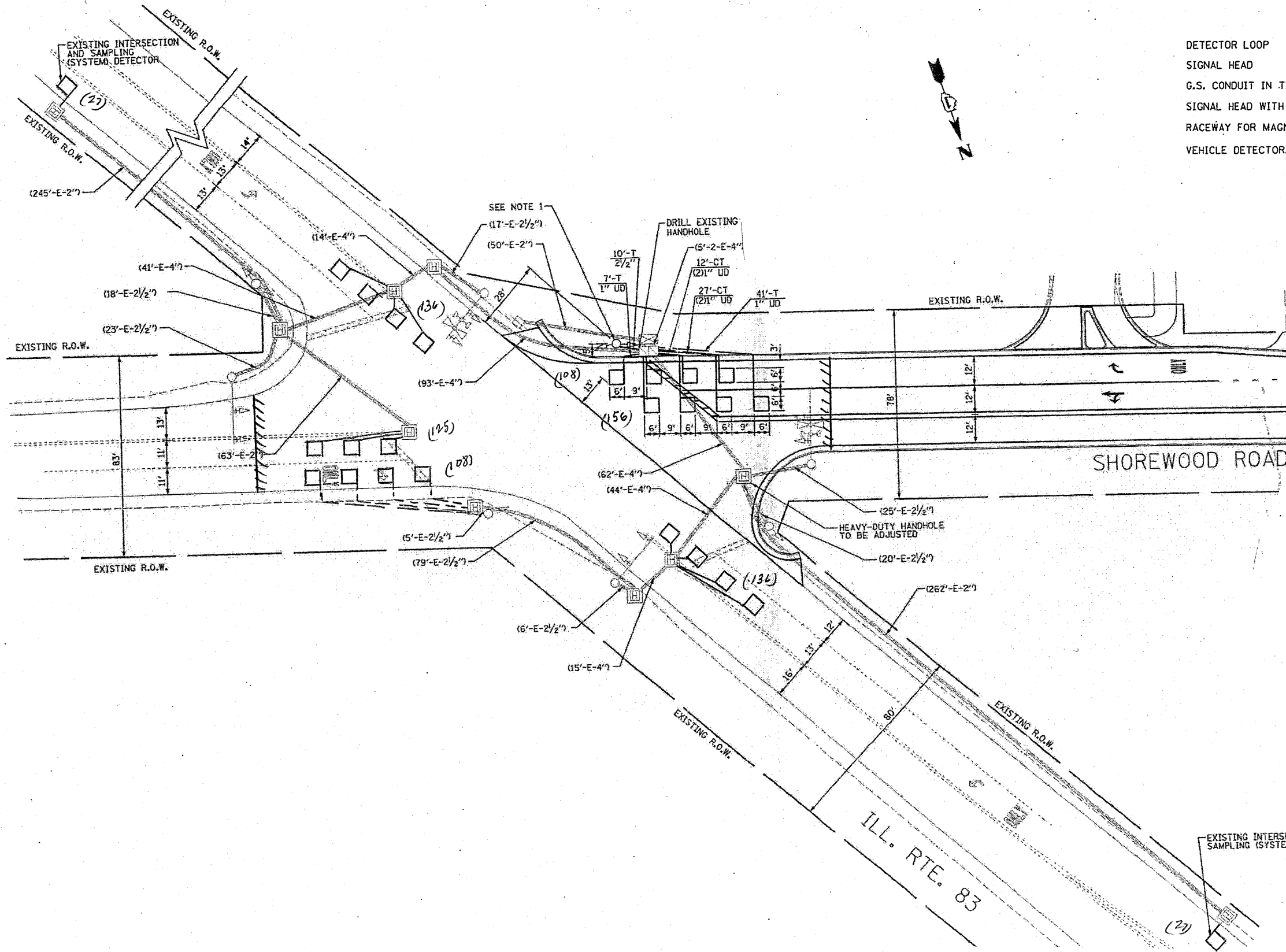
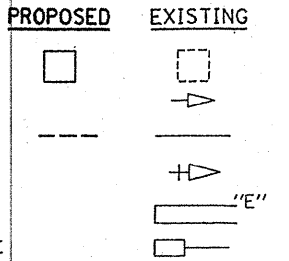
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	788	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kenthphixeybc	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. ROUTE 83 @ LAKE STREET	F.A.P. RTE. 8660	SECTION 3 RS-3	COUNTY LAKE	TOTAL SHEETS 35	SHEET NO. 22
CONTRACT NO.	TRAFFIC.Legend.v7.dgn	DRAWN - BCK	REVISED -			SCALE: NONE	SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -							
	PLOT DATE = 4/3/2009	DATE	REVISED -							

TRAFFIC SIGNAL LEGEND

DETECTOR LOOP
 SIGNAL HEAD
 G.S. CONDUIT IN TRENCH OR PUSHED
 SIGNAL HEAD WITH BACKPLATE
 RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II
 VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE

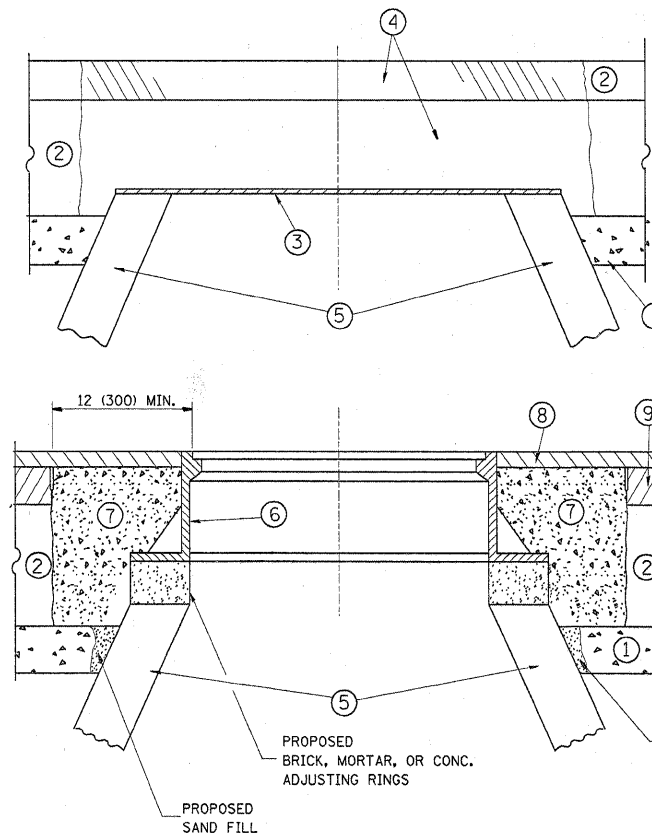


THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	823	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kanthapixayba	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT ILL. ROUTE 83 @ SHOREWOOD ROAD	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
o:\p\work\PIW1001\KANTHAPHIXAYBA\081126	traffic.legend.v7.dgn	DRAWN - BCK	REVISED -			866	3 RS-3	LAKE	35	23	
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -			CONTRACT NO.					
	PLOT DATE = 4/3/2009	DATE	REVISED -			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

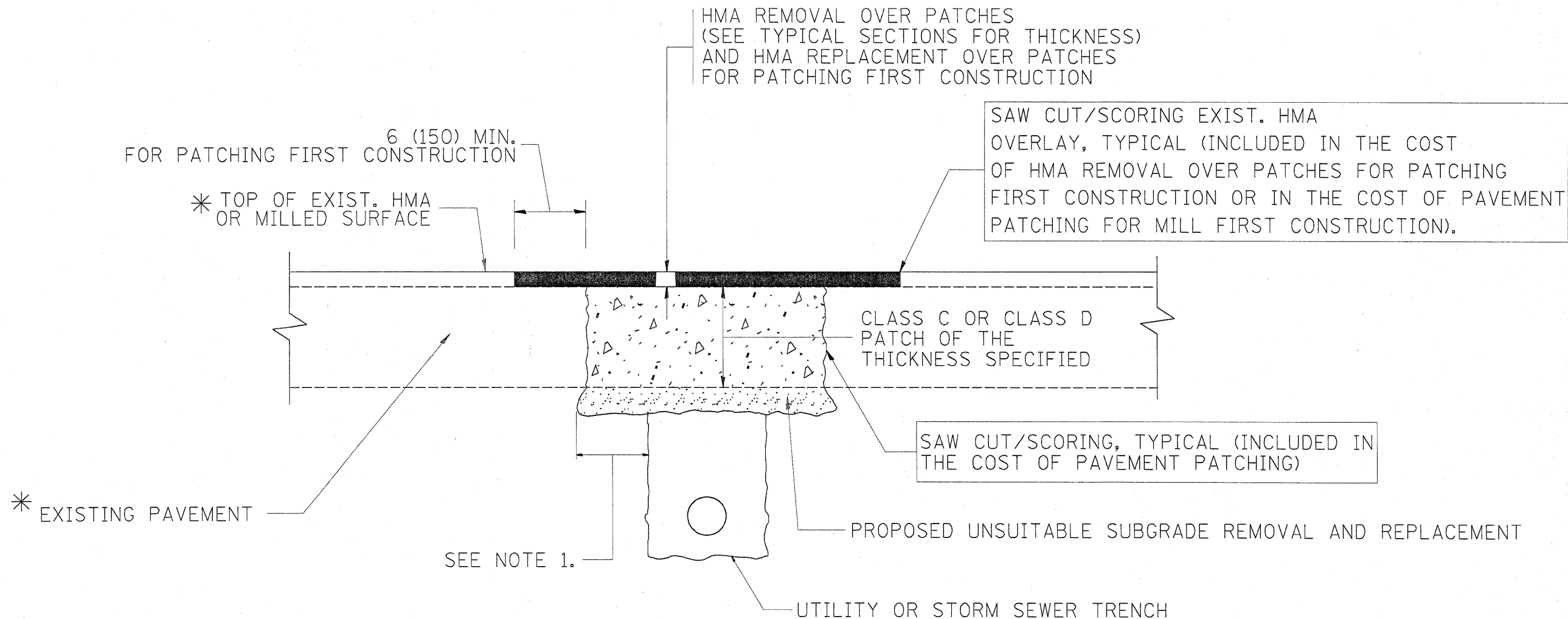
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = mossse	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct\pw_work\PW100T\MOSSSE\d0177698\Dist15.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		866	3 RS-3	LAKE	35	25			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - R. WIEDEMAN 05-14-04		BD600-03 (BD-8)			CONTRACT NO. 60J94				
PLOT DATE = 2/3/2010		DATE - 10-25-94	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = mcsse	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\PW\DOT\MOSSE\d0177698\DistS	ad.dgn	DRAWN -	REVISED - R. BORO 01-01-07		866	3 RS-3	LAKE	35	26			
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07	REVISED - K. ENG 10-27-08		BD400-04 (BD-22)			CONTRACT NO. 60J94				
PLOT DATE = 2/3/2010	DATE - 10-25-94				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

18" (450) MAX.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

T/2 *

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

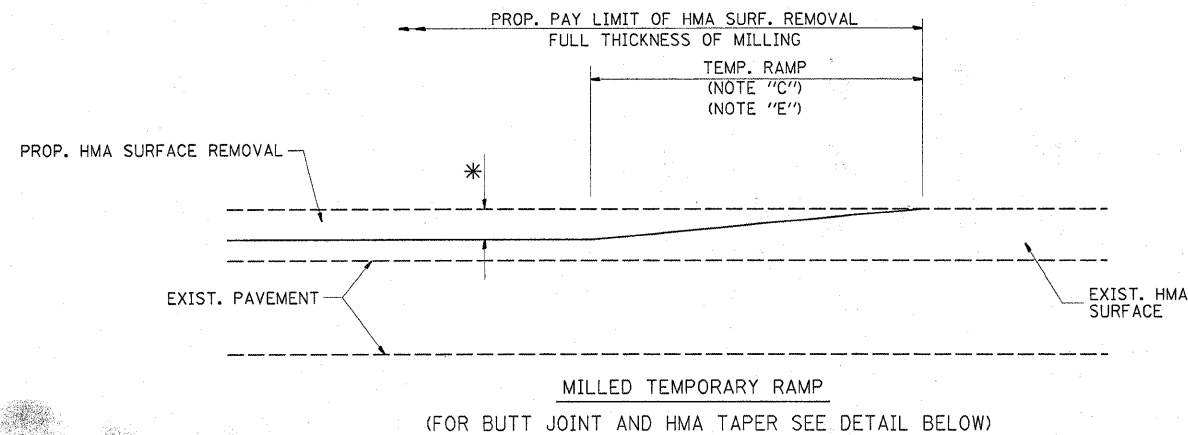
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

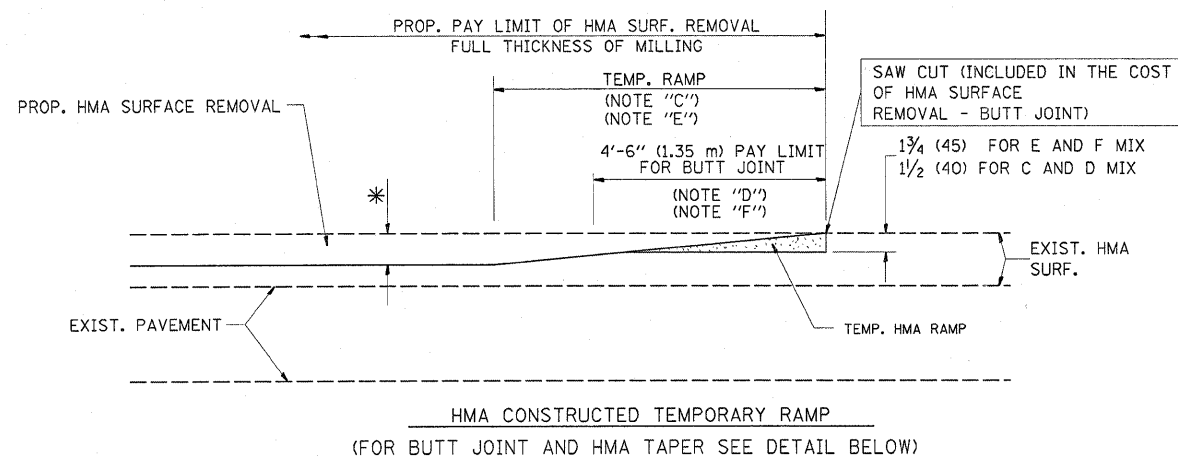
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = mcsse	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw_work\PWIDOT\MCSSE\d0177698\1.dwg	DRAWN -	REVISED - A. ABBAS 03-21-97	866			3 RS-3	LAKE	35	27	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01	BD600-06 (BD-24)			CONTRACT NO. 60J94				
PLOT DATE = 2/3/2010	DATE - 03-11-94	REVISED - R. BORO 12-15-09	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	

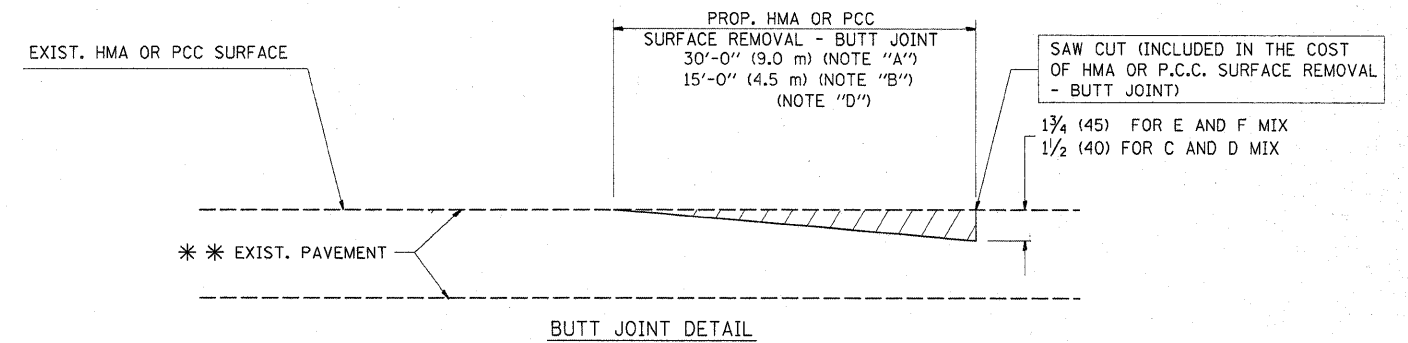


OPTION 1

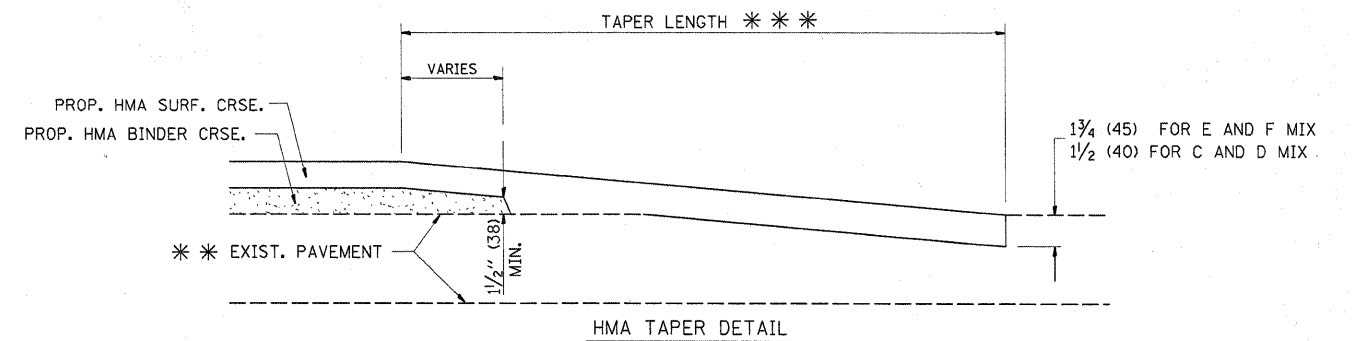


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

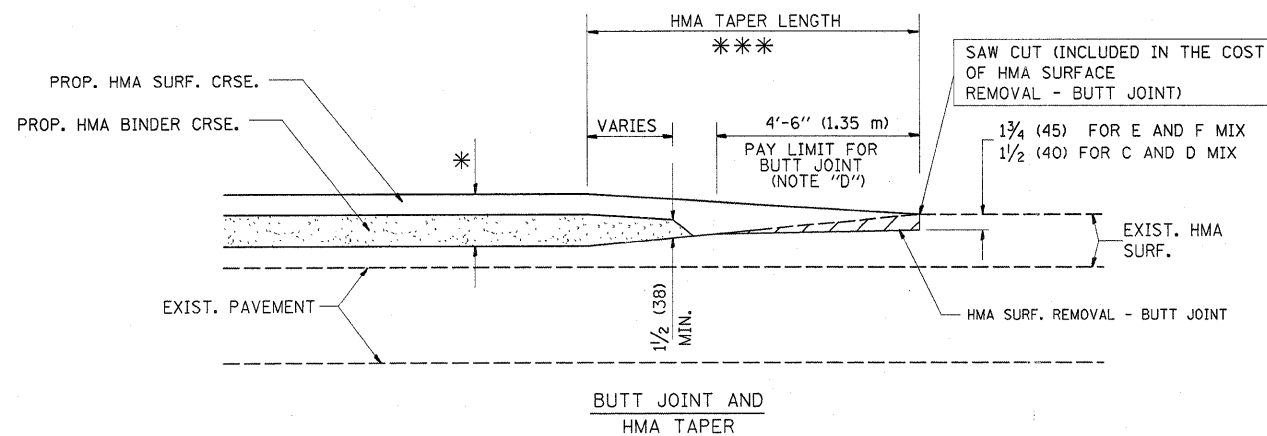
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



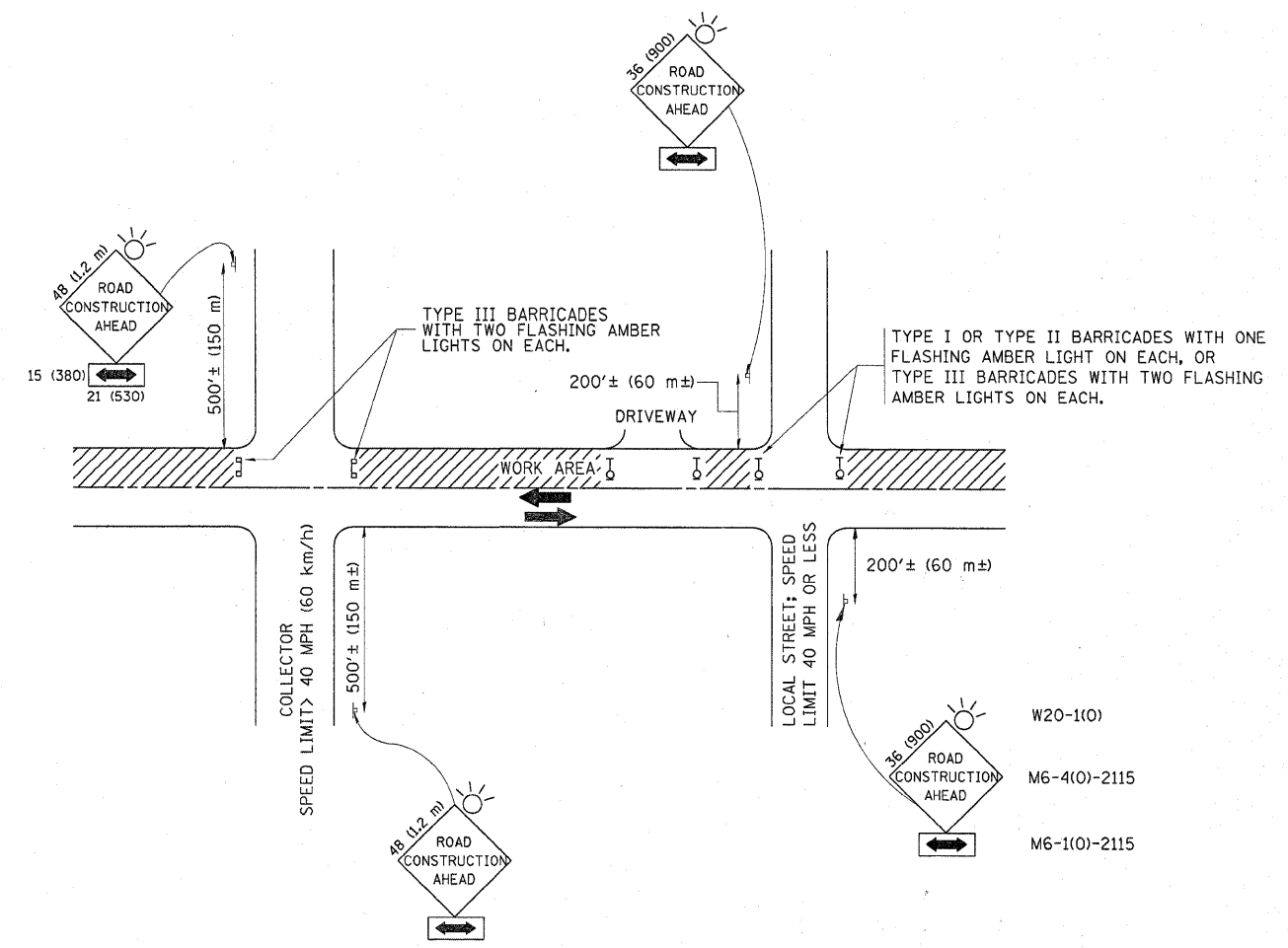
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = m08888	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
ca\pw_work\PW\DOT\M08888\0177698\01st.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 2/3/2010	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.P. RTE. 866	SECTION 3 RS-3	COUNTY LAKE	TOTAL SHEETS 35	SHEET NO. 28
			BD400-05 BD32		CONTRACT NO. 60J94		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

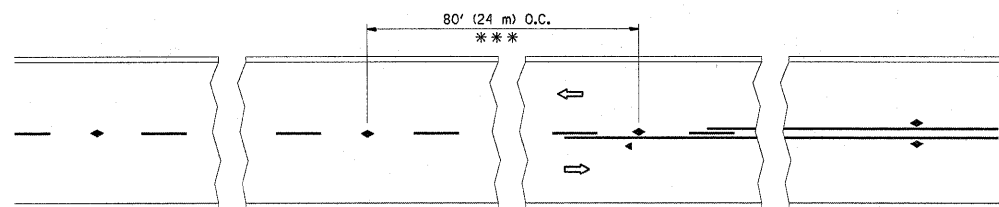
NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

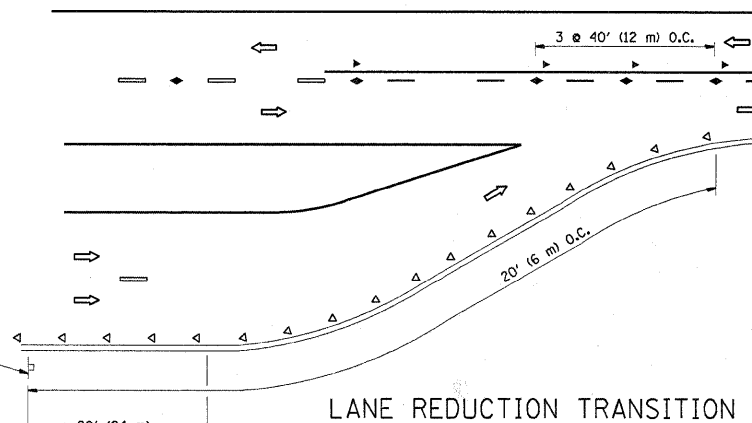
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = mossse	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\FWIDOT\MOSSSE\j0177698\Dist	dd.dgn	DRAWN -	REVISED - A. HOUSEH 03-06-96		866	3 RS-3	LAKE	35	29			
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96		SCALE: NONE			TC-10		CONTRACT NO. 60J94		
	PLOT DATE = 2/3/2010	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00		SHEET NO. 1 OF 1 SHEETS			STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

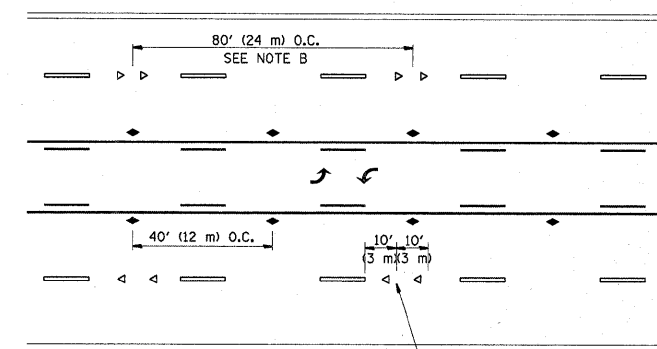


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

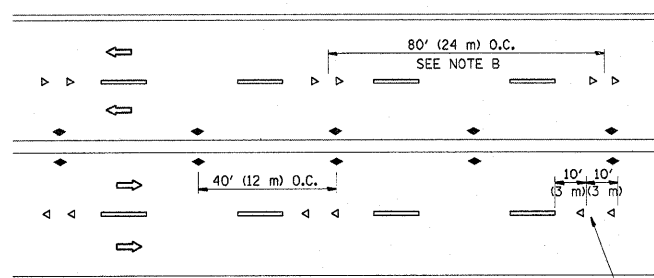
TWO-LANE/TWO-WAY



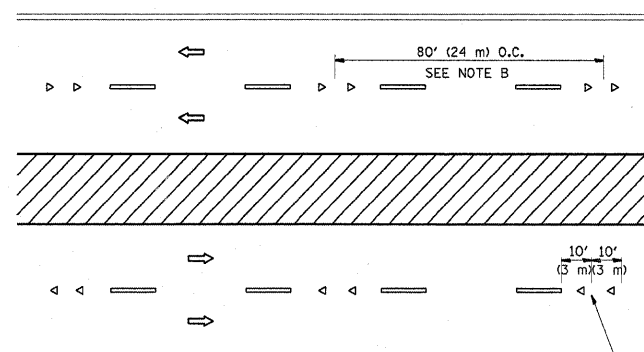
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

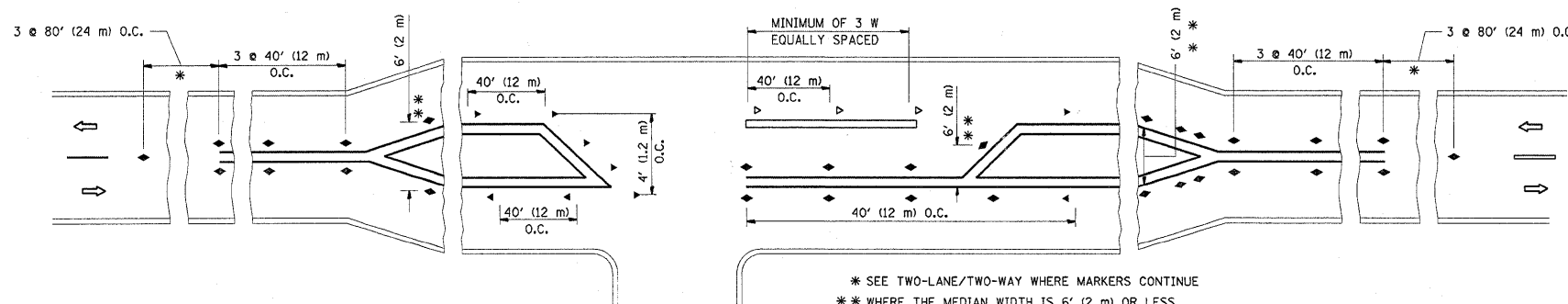
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

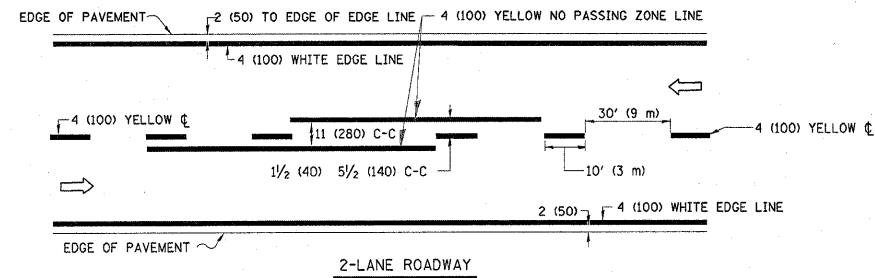
FILE NAME =	USER NAME = mossse	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
ca\pw_work\PW100T\MOSSSE\d0177698\DistS\d.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 2/3/2010	DATE -	REVISED - C. JUICIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

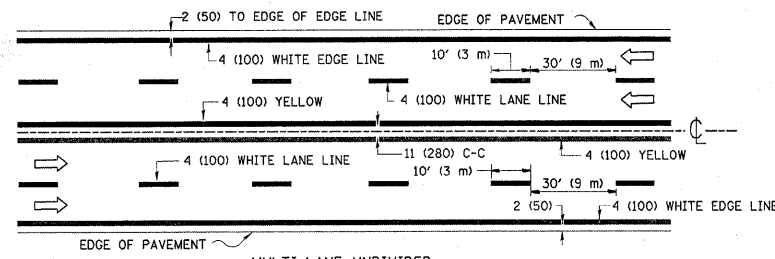
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

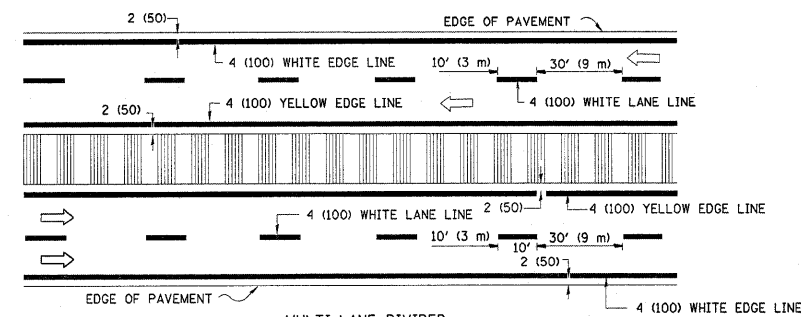
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
866	3 RS-3	LAKE	35	30
TC-11			CONTRACT NO. 60J94	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY



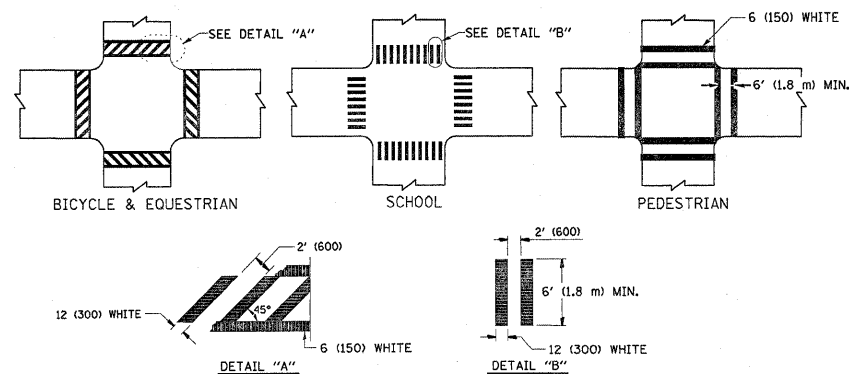
MULTI-LANE UNDIVIDED



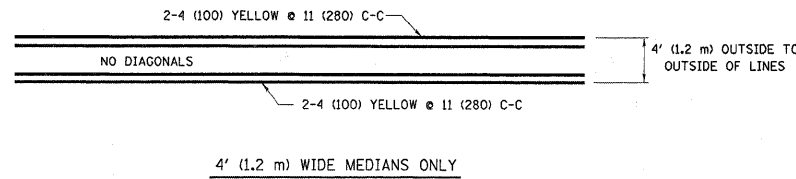
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

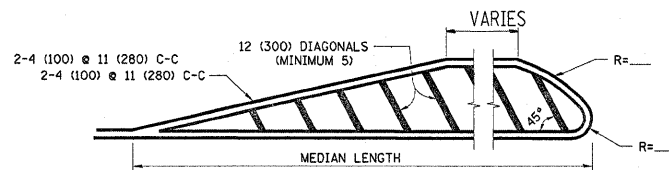
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

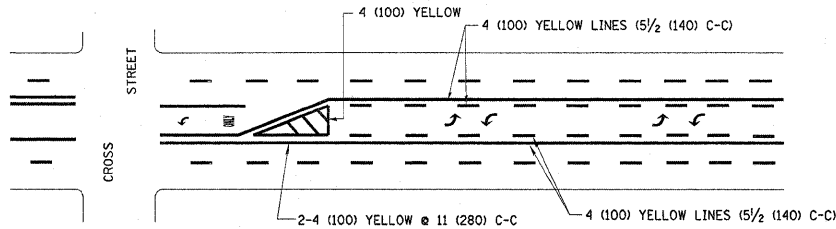


4' (1.2 m) WIDE MEDIANS ONLY

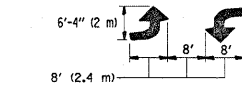


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

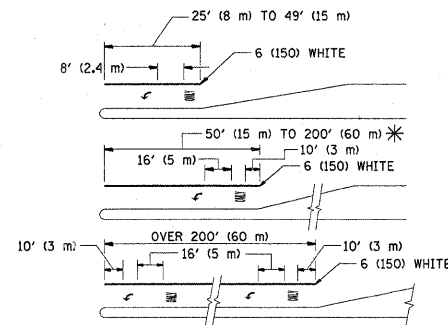


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

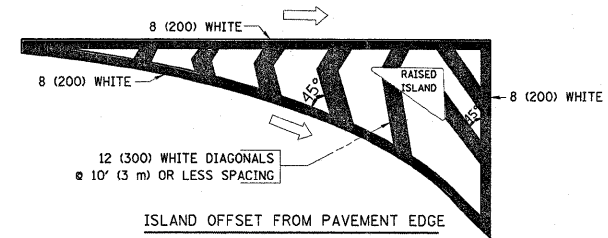
TYPICAL PAINTED MEDIAN MARKING



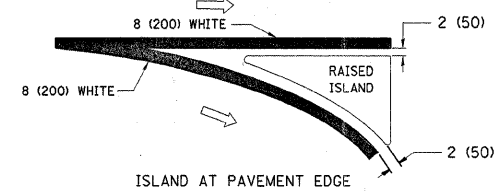
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



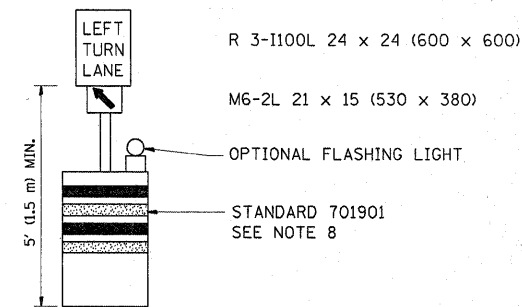
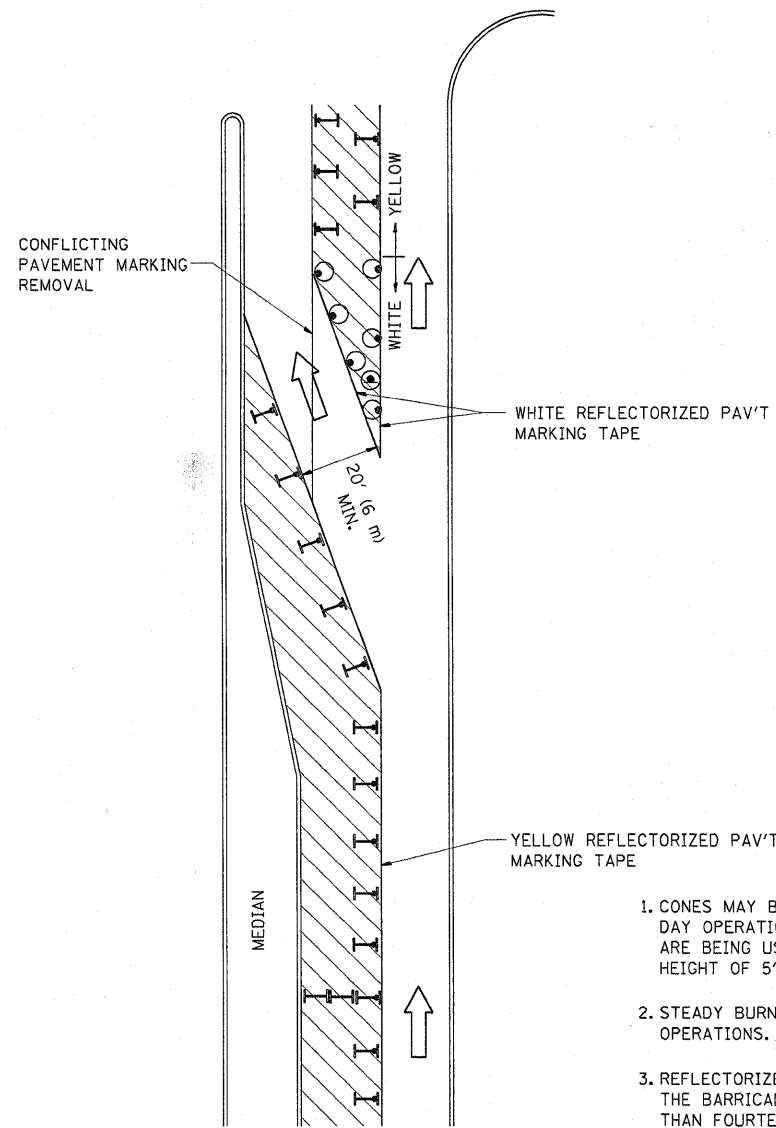
ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

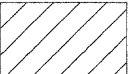
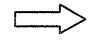






GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

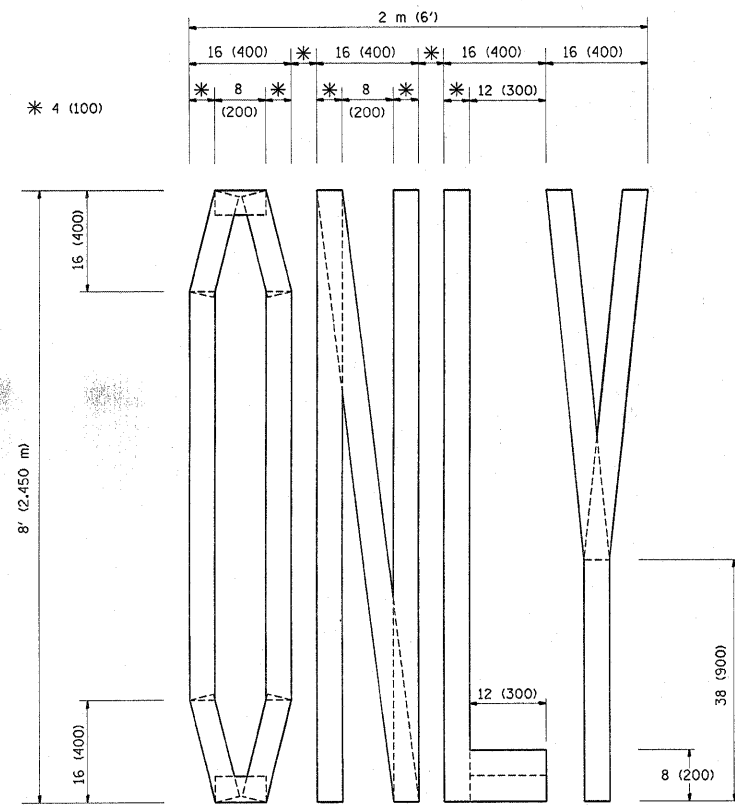
-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = mosse	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
c:\pwork\PW100T\MOSSE\d0177698\Dist	ed.dgn	REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 50.0000' / IN.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 2/3/2010	REVISED - T. RAMMACHER 01-06-00	REVISED -

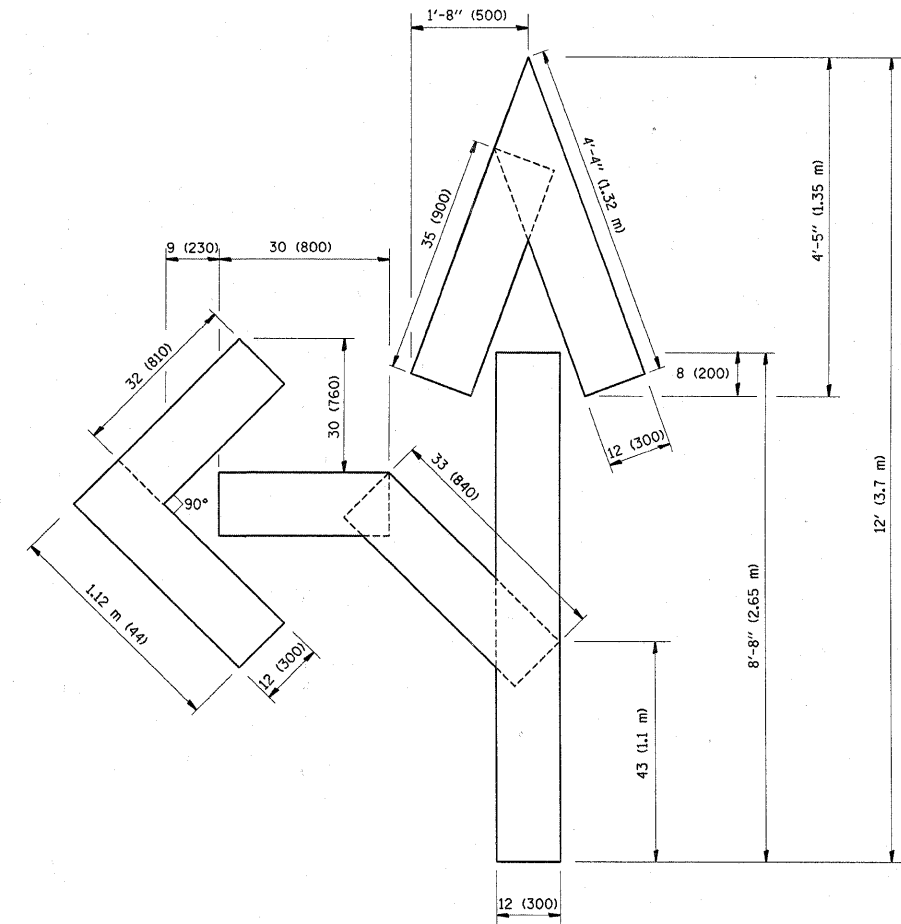
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

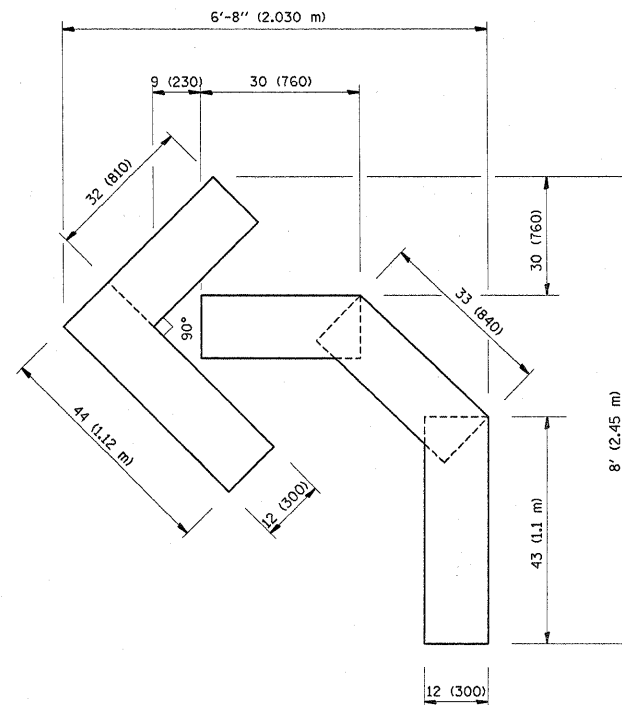
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
866	3 RS-3	LAKE	35	32
TC-14			CONTRACT NO. 60J94	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

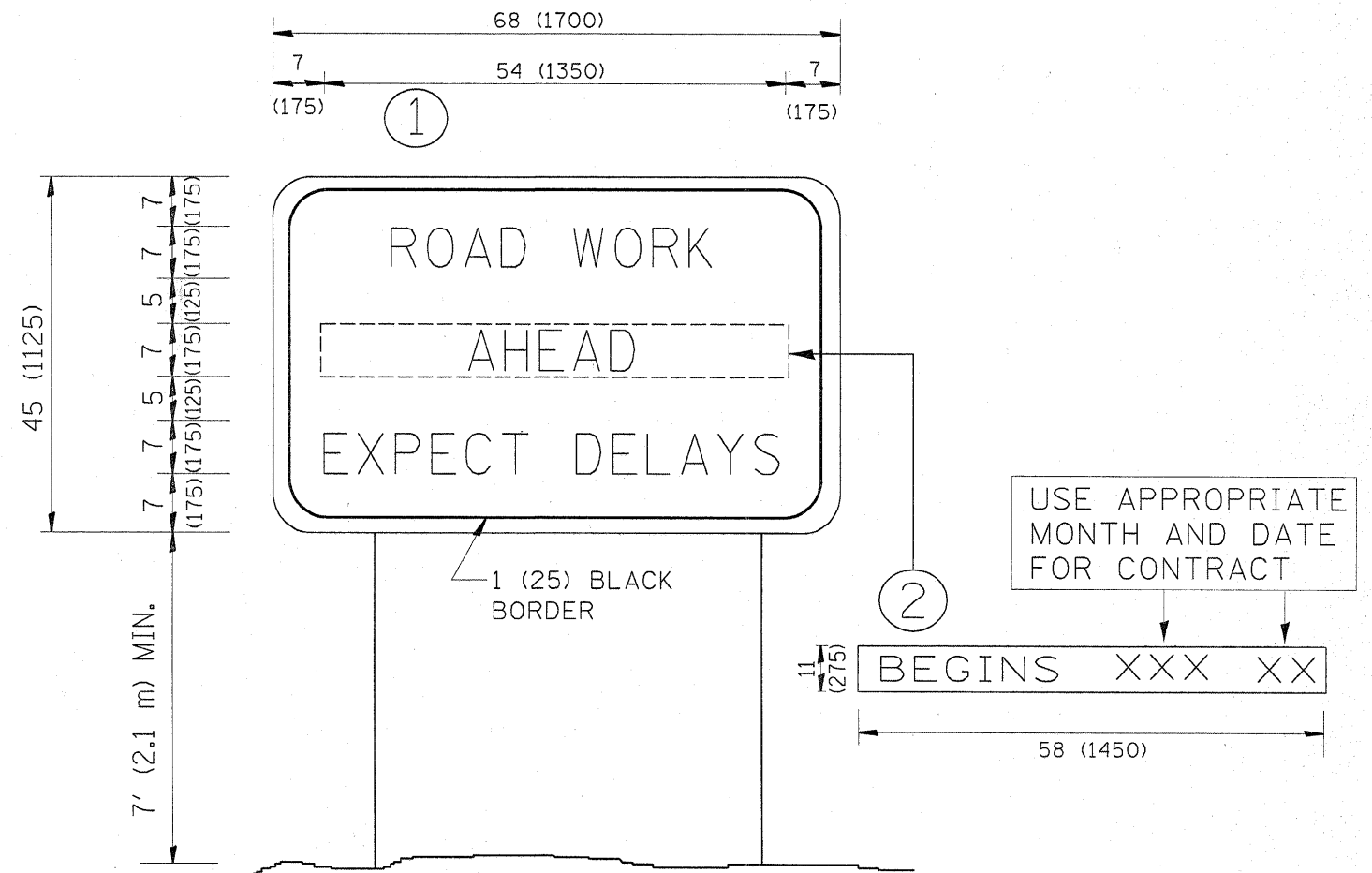
FILE NAME =	USER NAME = mossse	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
c:\pw_work\PW\DOT\MOSSE\0177698\Dist3\d.dgn		DRAWN -	REVISED -T. RAMMACHER 11-04-97
		CHECKED -	REVISED -T. RAMMACHER 03-02-98
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
866	3 RS-3	LAKE	35	33
TC-16			CONTRACT NO. 60J94	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = mossse	DESIGNED -	REVISED - R. MIRS 09-15-97
ct:\pw\work\PWIDOT\MOSSSE\d0177698\Dist\d.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 50.0000 ' / IN.		CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 2/3/2010		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

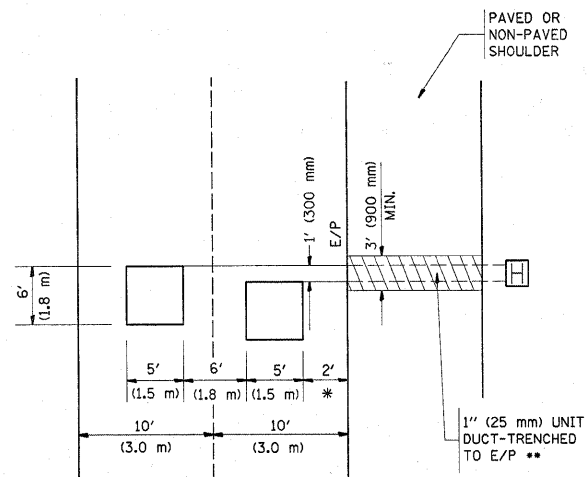
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
866	3 RS-3	LAKE	35	34
TC-22		CONTRACT NO. 60J94		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

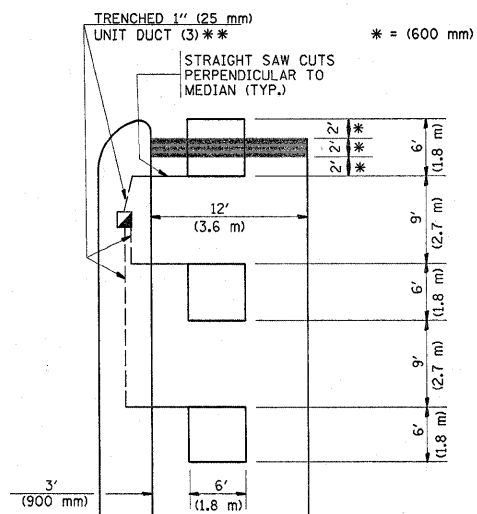


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

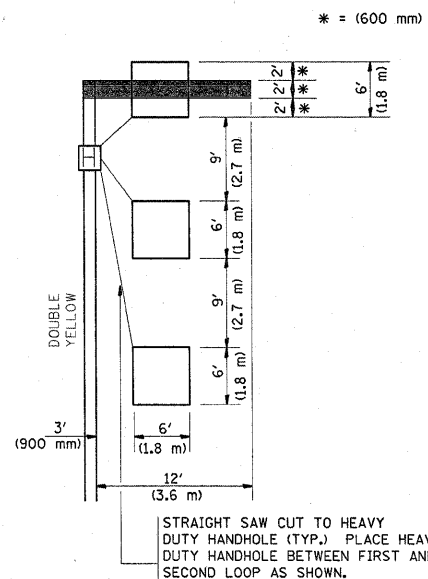


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



* = (600 mm)

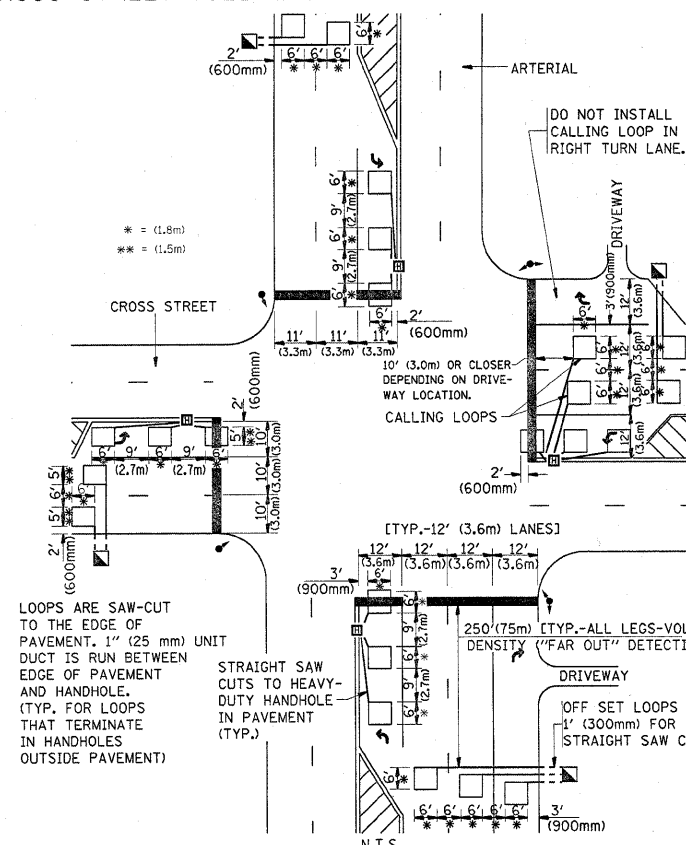
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

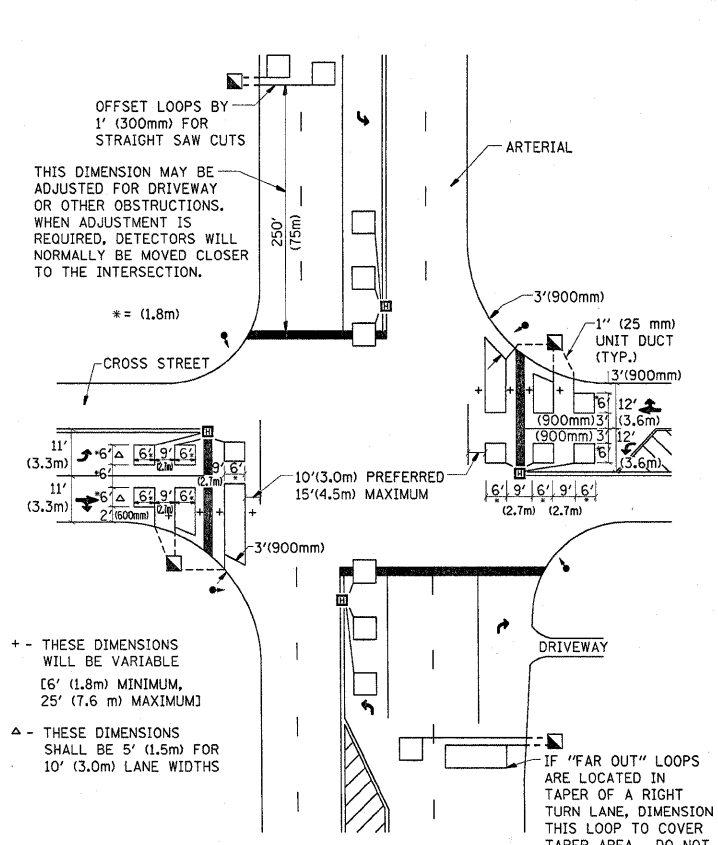
- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE: ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = mossse	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 500.0000' / IN.		CHECKED - R.K.F.	REVISED -			TS-07					
PLOT DATE = 2/3/2010		DATE -	REVISED -			CONTRACT NO. 60J94					
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.				
				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							