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HIGHWAY STANDARDS

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

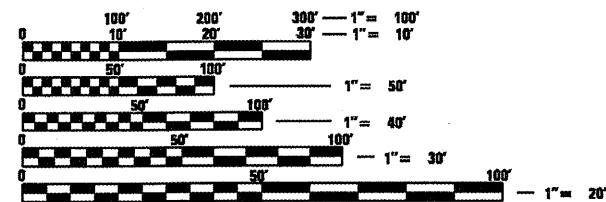
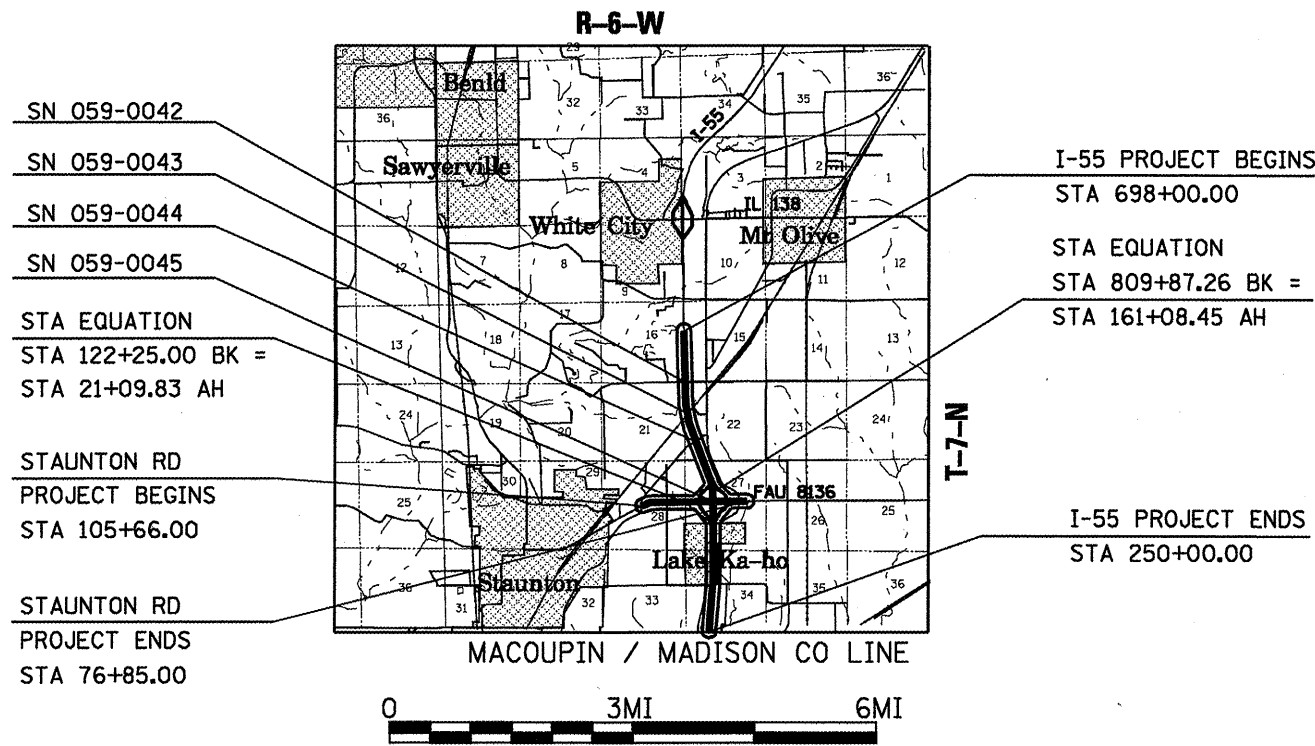
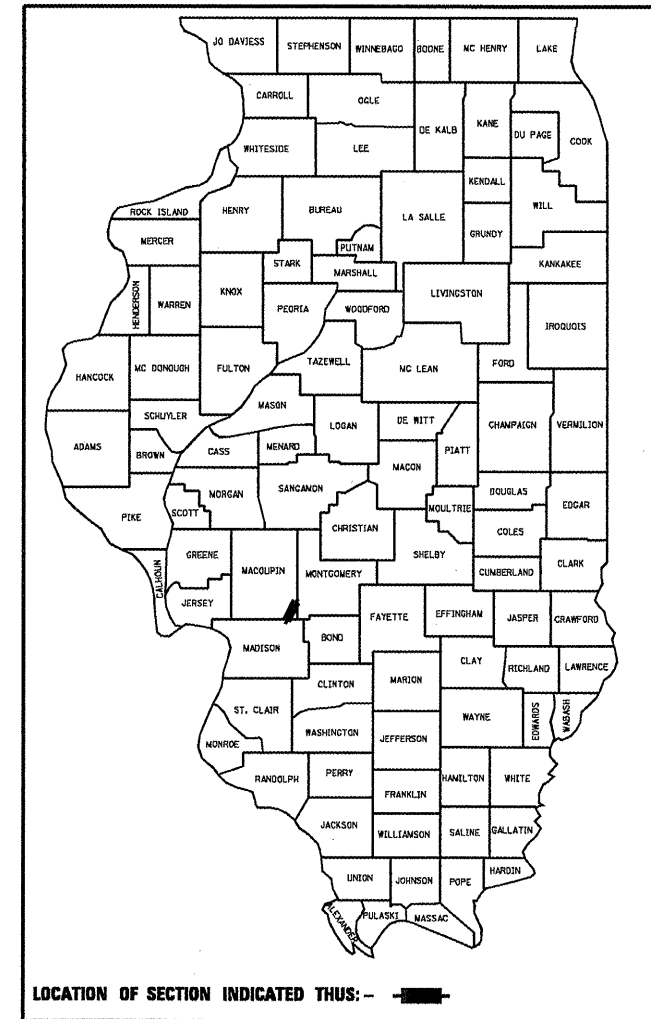
**PROPOSED
HIGHWAY PLANS**

**FAI ROUTE 55 (I-55)
SECTION 59RS-2, BR**

**MACOUPIN COUNTY
C-96-113-09**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	1
		ILLINOIS	CONTRACT NO. 72A60	

D-96-532-06



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: SAL MADONIA (217)782-4761
TEAM ENGINEER: JEFF MYERS (217)524-7940

GROSS LENGTH = 27,312.98 FT. = 5.17 MILE
NET LENGTH = 27,312.98 FT. = 5.17 MILE

CONTRACT NO. 72A60

ADT'S

I-55: 25,900 TO 27,200 (2007)
I-55 TRUCKS: 32.4% TO 33.6% (2007)
STAUNTON RD: 2,500 TO 3,600 (2005)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 25 2010
Regan Z. Dambler
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

April 9 2010
Scott E. Still, P.E./bc
REGIONAL ENGINEER OF DESIGN AND ENVIRONMENT

April 9 2010
Christine M. Reed/bc
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

GENERAL NOTES

1. ALL OF THE DISTURBED AREAS WITHIN THE RIGHT OF WAY NOT COVERED BY SURFACING MATERIAL SHALL BE SEEDED.
2. CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE AND EROSION PROTECTION FOR THE DURATION OF THIS PROJECT.
3. SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED SHALL BE DETERMINED BY THE ENGINEER.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE JULIE NUMBER IS 800-892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED. ANY DAMAGE TO THE UNDERGROUND FACILITIES, CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE, INCLUDING TEMPORARY REPAIRS WHICH MAY BE REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS.
5. THE LOCATIONS OF THOSE BURIED AND ABOVEGROUND UTILITIES SHOWN ARE APPROXIMATE, ARE SHOWN FOR CONTRACTOR INFORMATIONAL USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTIONS PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR, OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVEGROUND UTILITY LOCATION, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. REROUTING, DISCONNECTION, PROTECTION, ETC. OF ANY UTILITIES MUST BE COORDINATED BETWEEN THE CONTRACTOR, UTILITY COMPANY, AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDS ASSOCIATED WITH BURIED AND ABOVEGROUND UTILITIES, REMAINS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
6. THE THICKNESS OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT MIXTURE IS PLACED.
7. BEFORE ORDERING PIPE CULVERTS OR PIPE DRAINS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR EXACT LENGTHS.
8. AGGREGATE (PRIME COAT): FA 20 MAY BE USED IN ADDITION TO THE GRADATIONS LISTED IN THE 2ND PARAGRAPH OF ARTICLE 1003.03(c).
9. FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SANDBAGS PER BARRICADE.
10. WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS UNTIL AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS.
11. THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

GRANULAR MATERIALS	2.05	TONS/CU YD
RIPRAP	1.75	TON/CU YD
BITUMINOUS MATERIAL (PRIME COAT)	0.00038	TON/SQ YD
AGGREGATE (PRIME COAT)	0.002	TON/SQ YD
HOT-MIX ASPHALT SURFACE COURSE	112	LBS/SQ YD/IN
HOT-MIX ASPHALT BINDER COURSE	112	LBS/SQ YD/IN
12. ALL ELEVATIONS REFER TO U. S. G. S. MEAN SEA LEVEL DATUM.

COMMITMENTS:

THERE ARE NO COMMITMENTS ON THIS PROJECT.

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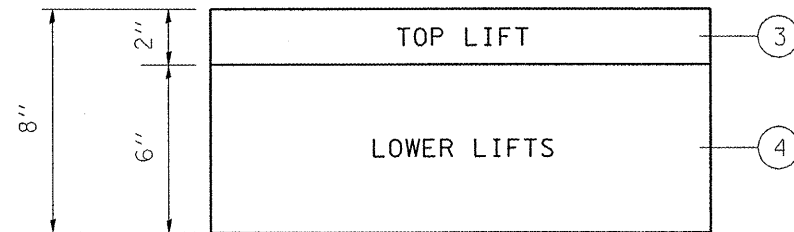
DISTRICT SIX	
EXAMINED <i>Oil Walker</i>	20 <u>3/5/10</u>
OPERATIONS ENGINEER	
EXAMINED <i>MARCH 4</i>	20 <u>10</u>
PROGRAM IMPLEMENTATION ENGINEER	
EXAMINED <i>March 24</i>	20 <u>10</u>
PROGRAM DEVELOPMENT ENGINEER	

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES, INDEX OF SHEETS	F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\PWIDOT\COXTE\dms25312\067246	gennote.dgn	DRAWN -	REVISED -			55	59RS-2, BR	MACOUPIN	100	2
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						SCALE:	SHEET NO. OF	SHEETS	STA.	TO STA.
						ILLINOIS FED. AID PROJECT				
						CONTRACT NO. 72A60				

The following mixture requirements are applicable for this project:

Mixture Number	1	2	3	4
Mixture Use(s)	Interstate & Ramps Polymer HMA Binder	Interstate & Ramps Polymer HMA Surface	Staunton Mainline, Bridge decks; Interstate Shoulders; HMA Shoulders; 8" Shoulders (top lift 2" min.); & Incidental HMA Surfacing	HMA Shoulders 8" (Lower Lifts), HMA Base Course, Patching
AC/PG:	SBS PG70-22	SBS PG76-22	PG 64-22	PG 64-22
Design Air Voids:	4.0% @ N105	4.0% @ N105	4.0% @ N50	4.0% @ N50
Mixture Composition: (Gradation Mixture)	IL 19.0	IL 9.5 or 12.5	IL 9.5 or 12.5 Mix C	IL 19.0
Friction Aggregate:	N/A	Mix E		N/A

ASPHALT PAVING LIFT DIAGRAM



HOT-MIX ASPHALT SHOULDERS, 8"

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES				HMA ROADWAY 100% STATE	SN 059-0042 100% STATE	SN 059-0044 100% STATE	SN 059-0045 100% STATE
				<i>100% STATE</i>			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE			
				1000-2A	SFTY-2A	SFTY-2A	SFTY-2A
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	22	22			
20200500	EARTH EXCAVATION (WIDENING)	CU YD	110	64		46	
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	57	57			
20400800	FURNISHED EXCAVATION	CU YD	55	55			
21400100	GRADING AND SHAPING DITCHES	FOOT	400	400			
25000200	SEEDING, CLASS 2	ACRE	0.25	0.25			
25100115	MULCH, METHOD 2	ACRE	0.25	0.25			
25100630	EROSION CONTROL BLANKET	SQ YD	75	75			
28000500	INLET AND PIPE PROTECTION	EACH	3	3			
28100807	STONE DUMPED RIPRAP, CLASS A4	TON	30	30			
31100300	SUB-BASE GRANULAR MATERIAL, TYPE A 4"	SQ YD	498	498			
31101900	SUB-BASE GRANULAR MATERIAL, TYPE C	TON	51	51			
35300300	PORTLAND CEMENT CONCRETE BASE COURSE 8"	SQ YD	37	37			
35501320	HOT-MIX ASPHALT BASE COURSE, 9"	SQ YD	341	0		182	159
35501328	HOT-MIX ASPHALT BASE COURSE, 11"	SQ YD	9,692	426			9,266
35800100	PREPARATION OF BASE	SQ YD	117	117			
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	31	31			
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	27	27			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	90	90			
40600300	AGGREGATE (PRIME COAT)	TON	474	474			
40600895	CONSTRUCTING TEST STRIP	EACH	3	3			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	1,907	1,907			

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES				HMA ROADWAY 100% STATE	SN 059-0042 100% STATE	SN 059-0044 100% STATE	SN 059-0045 100% STATE
				<i>100% STATE</i>			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE			
				I000-2A	SFTY-2A	SFTY-2A	SFTY-2A
40600990	TEMPORARY RAMP	SQ YD	800	800			
40603245	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105	TON	19,314	19,314			
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	11,787	11,544		115	128
40603575	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N105	TON	10,529	10,529			
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	179	179			
44000100	PAVEMENT REMOVAL	SQ YD	262	262			
44000152	HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"	SQ YD	6,276	6,276			
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	68,017	68,017			
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	36,160	36,160			
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	13,062	13,062			
44000162	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"	SQ YD	55,987	55,987			
44000169	HOT-MIX ASPHALT SURFACE REMOVAL, 5"	SQ YD	56,096	56,096			
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	30	30			
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	694	318			376
44000915	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	2,471			1,170	1,301
44002020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	973	614			359
44003100	MEDIAN REMOVAL	SQ FT	50	50			
44004250	PAVED SHOULDER REMOVAL	SQ YD	9,584	318			9,266
44200132	PAVEMENT PATCHING, TYPE II, 11 INCH	SQ YD	234	234			
44200577	CLASS A PATCHES, TYPE II, 12 INCH	SQ YD	45	45			
44200620	CLASS A PATCHES, TYPE II, 14 INCH	SQ YD	99	99			
44200624	CLASS A PATCHES, TYPE III, 14 INCH	SQ YD	16	16			

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		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	5
CONTRACT NO. 72A60				
ILLINOIS FED. AID PROJECT				

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES				HMA ROADWAY 100% STATE	SN 059-0042 100% STATE	SN 059-0044 100% STATE	SN 059-0045 100% STATE
				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	I000-2A	SFTY-2A	SFTY-2A	SFTY-2A
44200966	CLASS B PATCHES, TYPE I, 10 INCH	SQ YD	47	47			
44200970	CLASS B PATCHES, TYPE II, 10 INCH	SQ YD	138	138			
44213000	PATCHING REINFORCEMENT	SQ YD	160	160			
44213200	SAW CUTS	FOOT	2,192	2,192			
48101200	AGGREGATE SHOULDERS, TYPE B	TON	4,069	4,069			
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	2,842	2,842			
50102400	CONCRETE REMOVAL	CU YD	23			9.5	13.5
50105220	PIPE CULVERT REMOVAL	FOOT	29	29			
50157300	PROTECTIVE SHIELD	SQ YD	820			380	440
50300255	CONCRETE SUPERSTRUCTURE	CU YD	25.9			10.8	15.1
50300300	PROTECTIVE COAT	SQ YD	76			31	45
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	5,790			2,320	3,470
50500715	JACK AND REMOVE EXISTING BEARINGS	EACH	30			12	18
50606401	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES - NO. 1	L SUM	1			1	
50606402	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES - NO. 2	L SUM	1				1
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	5,310			2,070	3,240
50800515	BAR SPLICERS	EACH	60			28	32
52000110	PREFORMED JOINT STRIP SEAL	FOOT	225			91	134
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	30			12	18
52100520	ANCHOR BOLTS, 1"	EACH	120			48	72
54200223	PIPE CULVERTS, CLASS D, TYPE 1 18"	FOOT	13	13			
54200229	PIPE CULVERTS, CLASS D, TYPE 1 24"	FOOT	16	16			

100% STATE

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	6
CONTRACT NO. 72A60				
ILLINOIS FED. AID PROJECT				

Rev.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES				HMA ROADWAY 100% STATE	SN 059-0042 100% STATE	SN 059-0044 100% STATE	SN 059-0045 100% STATE
				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	1000-2A	SFTY-2A	SFTY-2A	SFTY-2A
54215553	METAL END SECTIONS 18"	EACH	1	1			
54215559	METAL END SECTIONS 24"	EACH	1	1			
58100200	WATERPROOFING MEMBRANE SYSTEM	SQ YD	2,480			1,174	1,306
58700300	CONCRETE SEALER	SQ FT	4,168			2,260	1,908
59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	10	10			
60260100	INLETS TO BE ADJUSTED	EACH	2				2
60265505	MEDIAN INLET (604101) TO BE RECONSTRUCTED	EACH	4	4			
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	560	560			
60610400	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24	FOOT	47	47			
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	554	554			
60622400	CONCRETE MEDIAN, TYPE SM-6.06	SQ FT	160	160			
63400105	GUARD POSTS	EACH	14	14			
63500105	DELINEATORS	EACH	265	265			
64200105	SHOULDER RUMBLE STRIP	FOOT	77,014	77,014			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12			
67100100	MOBILIZATION	L SUM	1	1			
70100200	TRAFFIC CONTROL AND PROTECTION, STANDARD 701331	EACH	1				1
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1			1	
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	4	4			
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1		1		
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1			
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1			0.1	0.9

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES				HMA ROADWAY 100% STATE	SN 059-0042 100% STATE	SN 059-0044 100% STATE	SN 059-0045 100% STATE
				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	I000-2A	SFTY-2A	SFTY-2A	SFTY-2A
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1			
70100820	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	L SUM	1	1			
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	1			
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	15	15			
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1			1	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	16	16			
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	15,479	15,479			
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	54,860	10,060		6,400	38,400
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1,107	1,107			
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	2,650	2,650			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	941	941			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	149	149			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	2,460	2,460			
70400100	TEMPORARY CONCRETE BARRIER	FOOT	462.5			462.5	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	462.5			462.5	
* 72000100	SIGN PANEL - TYPE 1	SQ FT	15	15			
* 72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	6	6			
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	30	30			
* 78001120	PAINT PAVEMENT MARKING - LINE 5"	FOOT	3,200			3,200	
* 78004200	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS AND SYMBOLS	SQ FT	125	125			
* 78004220	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	FOOT	10,060	10,060			
* 78004230	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 6"	FOOT	1,107	1,107			

*Specialty Items

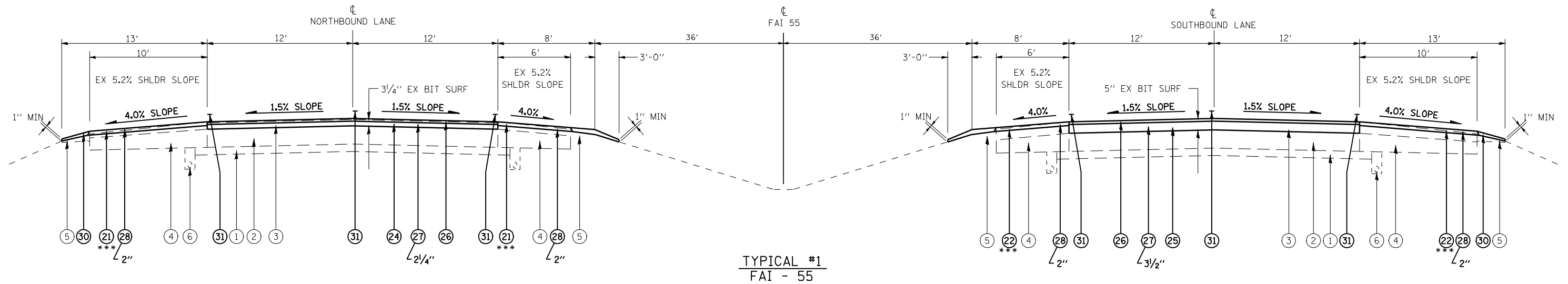
FILE NAME :	USER NAME : loughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 100.0000 "/ IN.	CHECKED -	REVISED -					CONTRACT NO. 72A60							
PLOT DATE = Mon-26-2010 10:13:17AM	DATE -	REVISED -					ILLINOIS FED. AID PROJECT							

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES				HMA ROADWAY 100% STATE	SN 059-0042 100% STATE	SN 059-0044 100% STATE	SN 059-0045 100% STATE
				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	1000-2A	SFTY-2A	SFTY-2A	SFTY-2A
* 78004240	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	FOOT	2,650	2,650			
* 78004250	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	FOOT	188	188			
* 78004280	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	FOOT	149	149			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1,242	1,242			
* 78200300	PRISMATIC CURB REFLECTOR	EACH	273	273			
78300100	PAVEMENT MARKING REMOVAL	SQ FT	27,000			3,000	24,000
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	706	706			
X0320157	CLEANING UNDERDRAIN OUTLETS	EACH	171	171			
X0321583	PIPE ELBOW 18"	EACH	1	1			
X0322279	OUTLET MARKER	EACH	171	171			
X0322729	MATERIAL TRANSFER DEVICE	TON	29,843	29,843			
X0322932	SILICONE JOINT SEALER, 1.5"	FOOT	60		60		
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	172			93	79
X0325702	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1			
X0976500	END SECTIONS TO BE REMOVED	EACH	1	1			
X2503000	MAINTENANCE MOWING	ACRE	118	118			
* X7030072	GROOVING FOR RECESSED PAVEMENT MARKING 6"	FOOT	76,893	76,893			
X7200201	WIDTH RESTRICTION SIGNING	L SUM	1				1
* X7800620	URETHANE PAVEMENT MARKING - LINE 5"	FOOT	121,491	121,491			
* X7800640	URETHANE PAVEMENT MARKING - LINE 8"	FOOT	539	539			
* X7800650	URETHANE PAVEMENT MARKING - LINE 12"	FOOT	753	753			
Z0010615	CLEANING EXISTING INLETS	EACH	3	3			

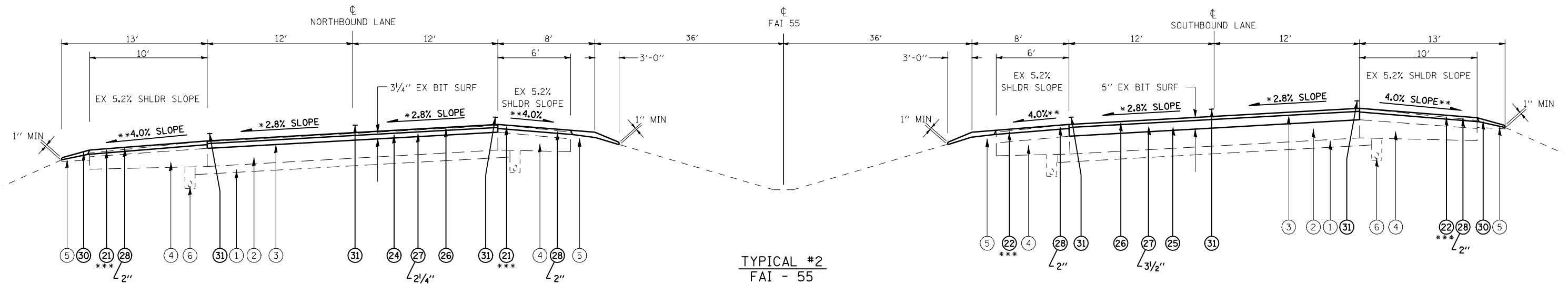
*Specialty Items

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		CHECKED -	REVISED -						CONTRACT NO. 72A60						
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT										



TYPICAL #1
FAI - 55

STA 698+00.00 TO STA 744+52.54
 STA 766+33.65 TO STA 788+61.68
 STA EQ 809+87.27 BK = STA 161+08.45 AH
 STA 161+08.45 TO STA 250+00.00



TYPICAL #2
FAI - 55

STA 744+52.54 TO STA 766+33.65
 STA 788+61.98 TO STA 809+87.26
 STA EQ 809+87.27 BK = STA 161+08.45 AH

* MATCH EXISTING CROSS SLOPES AT TRANSITIONS
AND SUPER ELEVATED SECTIONS

** WHEN THE SUPERELEVATION RATE OF PAVEMENT IS
BETWEEN 0.0% AND 4.0%, THE SHOULDER SLOPE SHALL
BE SLOPED AT 4.0%. WHEN THE SUPER ELEVATION RATE
OF PAVEMENT EXCEEDS 4.0%, THE SHOULDER SHALL BE
SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN
THE PAVEMENT AND SHOULDER IS 8.0% FOR THE HIGH
SIDE OF THE SE, AND MATCH THE SE FOR THE LOW SIDE
OF THE SE.

*** HMA SURFACE REMOVAL SHALL BE THE DEPTH SPECIFIED AT
THE EOP, AND MAY VARY DUE TO EXISTING SHOULDER SLOPE

NOTE:

SEE " EMERGENCY MEDIAN CROSS OVER
DETAIL" SHEET FOR DETAILS OF THE
WIDENING OF THE HMA SHOULDERS

EXISTING LEGEND

- ① EX STABILIZED SUB-BASE 4"
- ② EX CRPCC PAVEMENT 9"
- ③ EX HOT-MIX ASPHALT SURFACE COURSE (THICKNESS VARIES)
- ④ EX HMA SHOULDERS (THICKNESS VARIES)
- ⑤ EX AGGREGATE SHOULDERS
- ⑥ EX PIPE UNDERDRAINS 6"
- ⑦ EX PCC PAVEMENT 8"
- ⑧ EX COMBINATION CONCRETE CURB & GUTTER M-6.06
- ⑨ EX PCC MEDIAN SURFACE 4"

PROPOSED LEGEND

- ⑳ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- ㉑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- ㉒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- ㉓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ㉔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- ㉕ PR HOT-MIX ASPHALT SURFACE REMOVAL, 5"
- ㉖ PR POLYMERIZED HMA SURFACE COURSE, MIX "E", N105 1 1/2"
- ㉗ PR POLYMERIZED HMA BINDER COURSE, IL-19.0, N105
- ㉘ PR POLYMERIZED HMA SURFACE COURSE, MIX "C", N50
- ㉙ PR HMA SHOULDER 8"
- ㉚ PR AGGREGATE SHOULDERS, TYPE B (WEDGE)
- ㉛ PR PAVEMENT MARKING

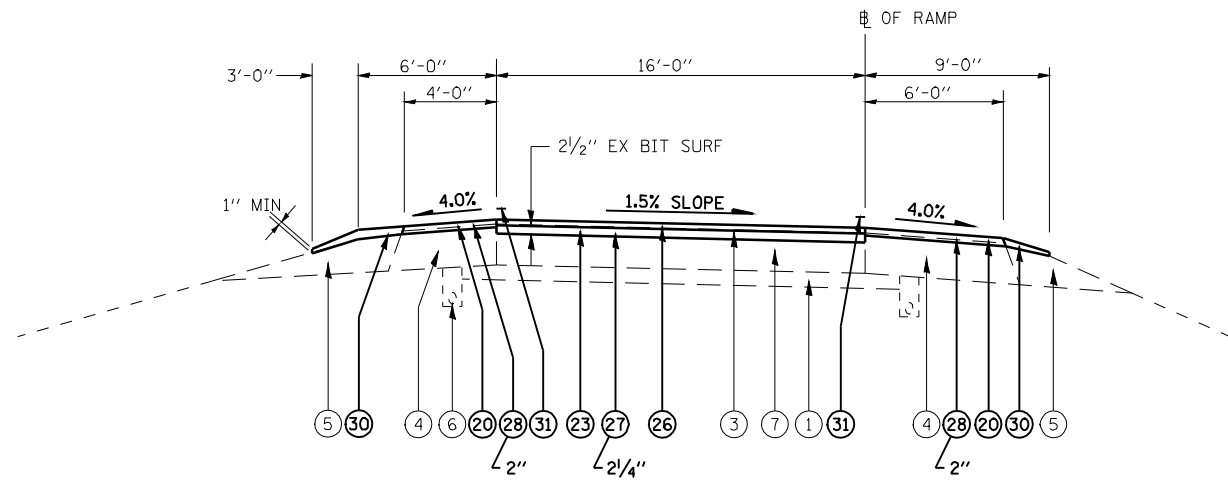
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

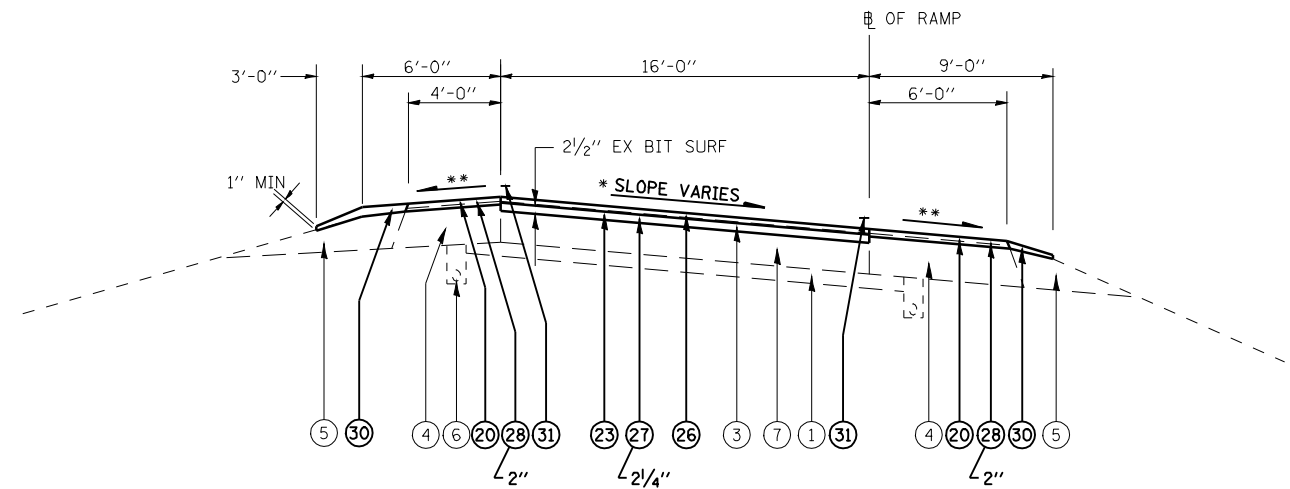
TYPICAL SECTIONS
MAINLINE I-55

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

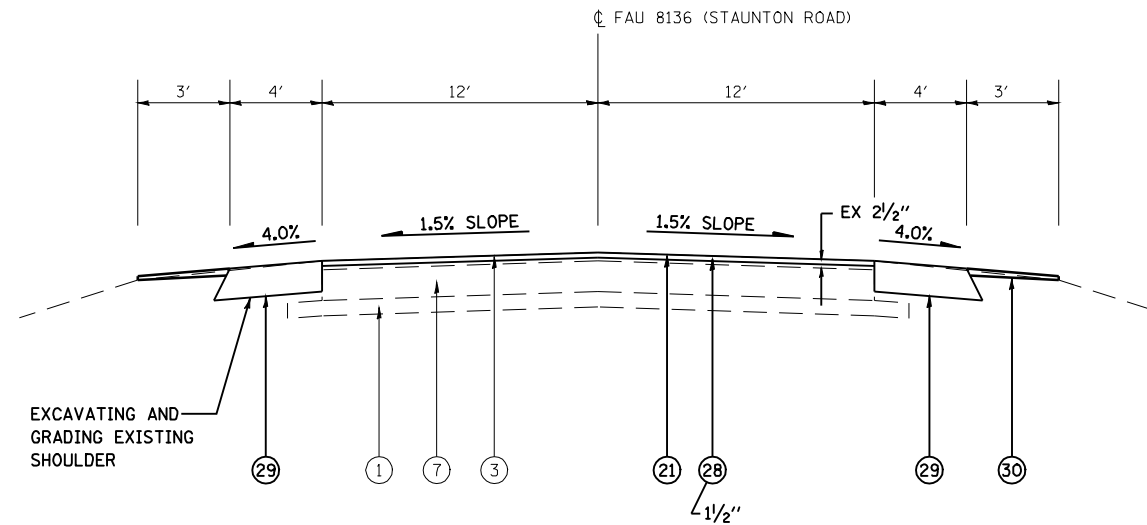
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	11
CONTRACT NO. 72A60				
ILLINOIS FED. AID PROJECT				



TYPICAL #3
RAMP (TANGENT)
FAU 8136 (STAUNTON ROAD) INTERCHANGE



TYPICAL #4
RAMP (SUPERELEVATED)
FAU 8136 (STAUNTON ROAD) INTERCHANGE



TYPICAL #5
FAU 8136 (STAUNTON ROAD)
STA 105+66.00 TO STA 106+57.83
STA 119+04.40 TO STA 122+25.00
STA EQ 122+25.00 BK = STA 21+09.83 AH
STA 21+09.83 TO STA 22+12.78

* MATCH EXISTING CROSS SLOPES AT TRANSITIONS AND SUPER ELEVATED SECTIONS

** WHEN THE SUPERELEVATION RATE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%, THE SHOULDER SLOPE SHALL BE SLOPED AT 4.0%. WHEN THE SUPER ELEVATION RATE OF PAVEMENT EXCEEDS 4.0%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDER IS 8.0% FOR THE HIGH SIDE OF THE SE, AND MATCH THE SE FOR THE LOW SIDE OF THE SE.

*** HMA SURFACE REMOVAL SHALL BE THE DEPTH SPECIFIED AT THE EOP, AND MAY VARY DUE TO EXISTING SHOULDER SLOPE

EXISTING LEGEND

- ① EX STABILIZED SUB-BASE 4"
- ② EX CRPCC PAVEMENT 9"
- ③ EX HOT-MIX ASPHALT SURFACE COURSE (THICKNESS VARIES)
- ④ EX HMA SHOULDERS (THICKNESS VARIES)
- ⑤ EX AGGREGATE SHOULDERS
- ⑥ EX PIPE UNDERDRAINS 6"
- ⑦ EX PCC PAVEMENT 8"
- ⑧ EX COMBINATION CONCRETE CURB & GUTTER M-6.06
- ⑨ EX PCC MEDIAN SURFACE 4"

PROPOSED LEGEND

- ⑳ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- ㉑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ㉒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- ㉓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ㉔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- ㉕ PR HOT-MIX ASPHALT SURFACE REMOVAL, 5"
- ㉖ PR POLYMERIZED HMA SURFACE COURSE, MIX "E", N105 1 1/2"
- ㉗ PR POLYMERIZED HMA BINDER COURSE, IL-19.0, N105
- ㉘ PR POLYMERIZED HMA SURFACE COURSE, MIX "C", N50
- ㉙ PR HMA SHOULDER 8"
- ㉚ PR AGGREGATE SHOULDERS, TYPE B (WEDGE)
- ㉛ PR PAVEMENT MARKING

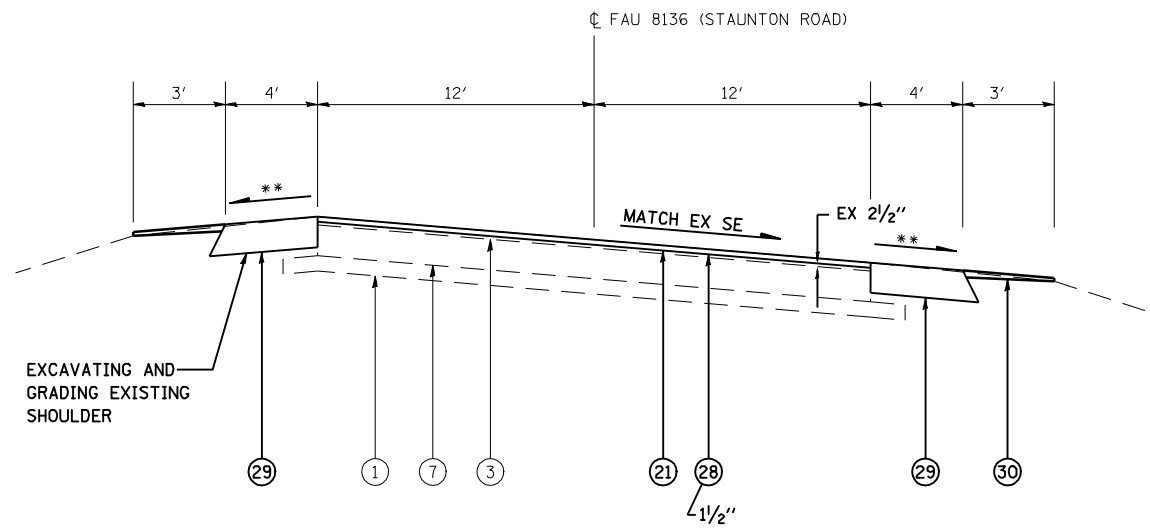
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
FAU 8136 (STAUNTON RD)

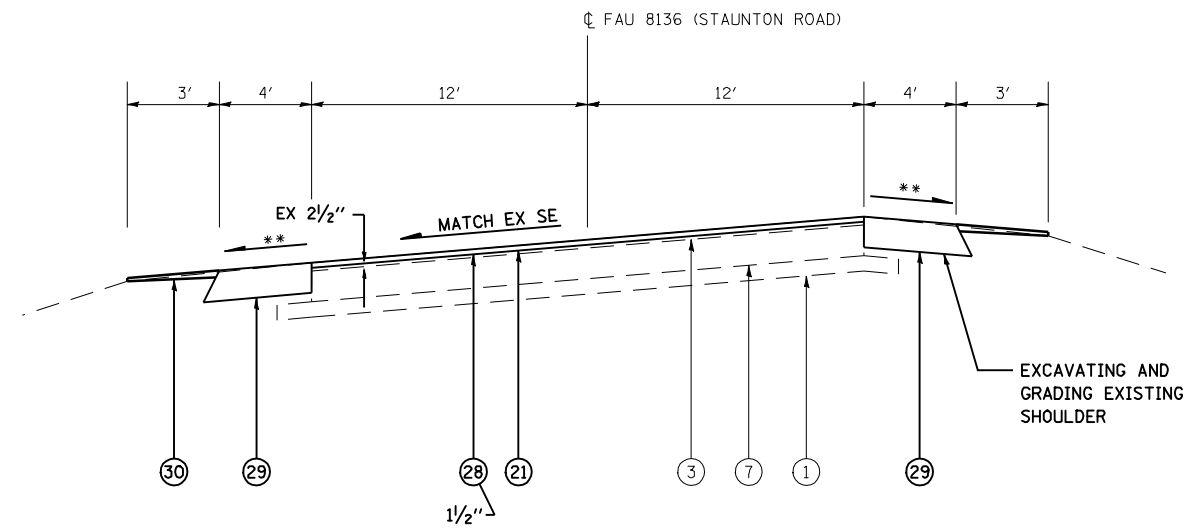
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	



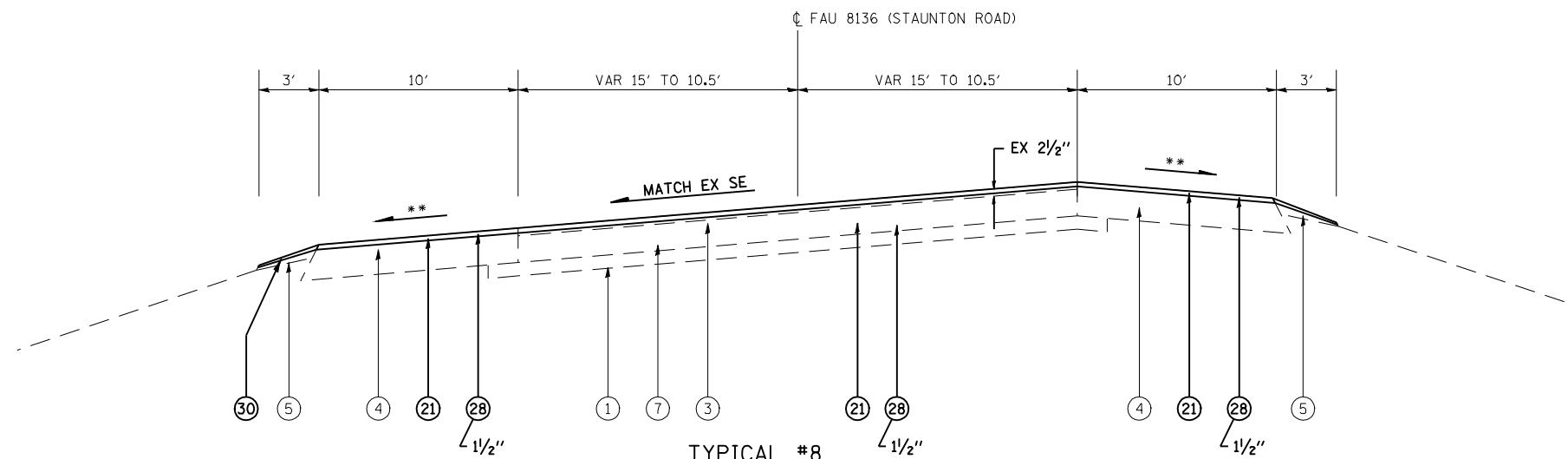
TYPICAL #6

FAU 8136 (STAUNTON ROAD)
STA 106+57.83 TO STA 119+04.40



TYPICAL #7

FAU 8136 (STAUNTON ROAD)
STA 22+12.78 TO STA 24+59.23



TYPICAL #8

FAU 8136 (STAUNTON ROAD)
STA 24+59.23 TO STA 26+15.68

* MATCH EXISTING CROSS SLOPES AT TRANSITIONS AND SUPER ELEVATED SECTIONS

** WHEN THE SUPERELEVATION RATE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%, THE SHOULDER SLOPE SHALL BE SLOPED AT 4.0%. WHEN THE SUPER ELEVATION RATE OF PAVEMENT EXCEEDS 4.0%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDER IS 8.0% FOR THE HIGH SIDE OF THE SE, AND MATCH THE SE FOR THE LOW SIDE OF THE SE.

*** HMA SURFACE REMOVAL SHALL BE THE DEPTH SPECIFIED AT THE EOP, AND MAY VARY DUE TO EXISTING SHOULDER SLOPE

EXISTING LEGEND

- ① EX STABILIZED SUB-BASE 4"
- ② EX CRPCC PAVEMENT 9"
- ③ EX HOT-MIX ASPHALT SURFACE COURSE (THICKNESS VARIES)
- ④ EX HMA SHOULDERS (THICKNESS VARIES)
- ⑤ EX AGGREGATE SHOULDERS
- ⑥ EX PIPE UNDERDRAINS 6"
- ⑦ EX PCC PAVEMENT 8"
- ⑧ EX COMBINATION CONCRETE CURB & GUTTER M-6.06
- ⑨ EX PCC MEDIAN SURFACE 4"

PROPOSED LEGEND

- ⑳ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- ㉑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ㉒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- ㉓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ㉔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- ㉕ PR HOT-MIX ASPHALT SURFACE REMOVAL, 5"
- ㉖ PR POLYMERIZED HMA SURFACE COURSE, MIX "E", N105 1 1/2"
- ㉗ PR POLYMERIZED HMA BINDER COURSE, IL-19.0, N105
- ㉘ PR POLYMERIZED HMA SURFACE COURSE, MIX "C", N50
- ㉙ PR HMA SHOULDER 8"
- ㉚ PR AGGREGATE SHOULDERS, TYPE B (WEDGE)
- ㉛ PR PAVEMENT MARKING

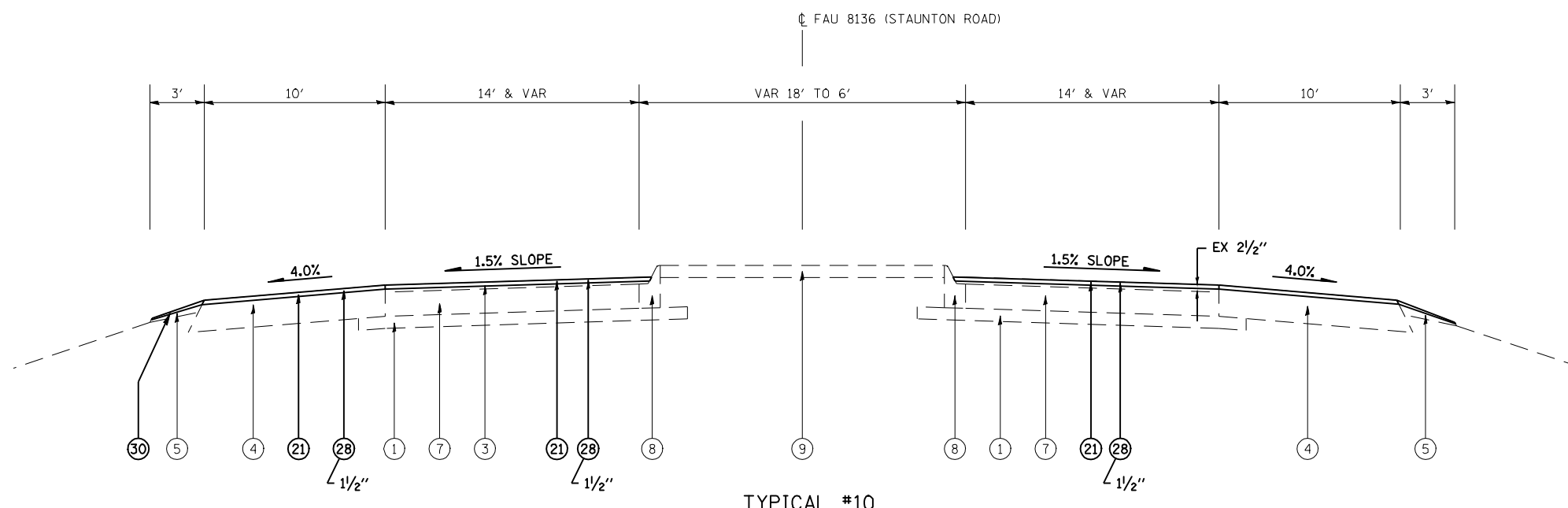
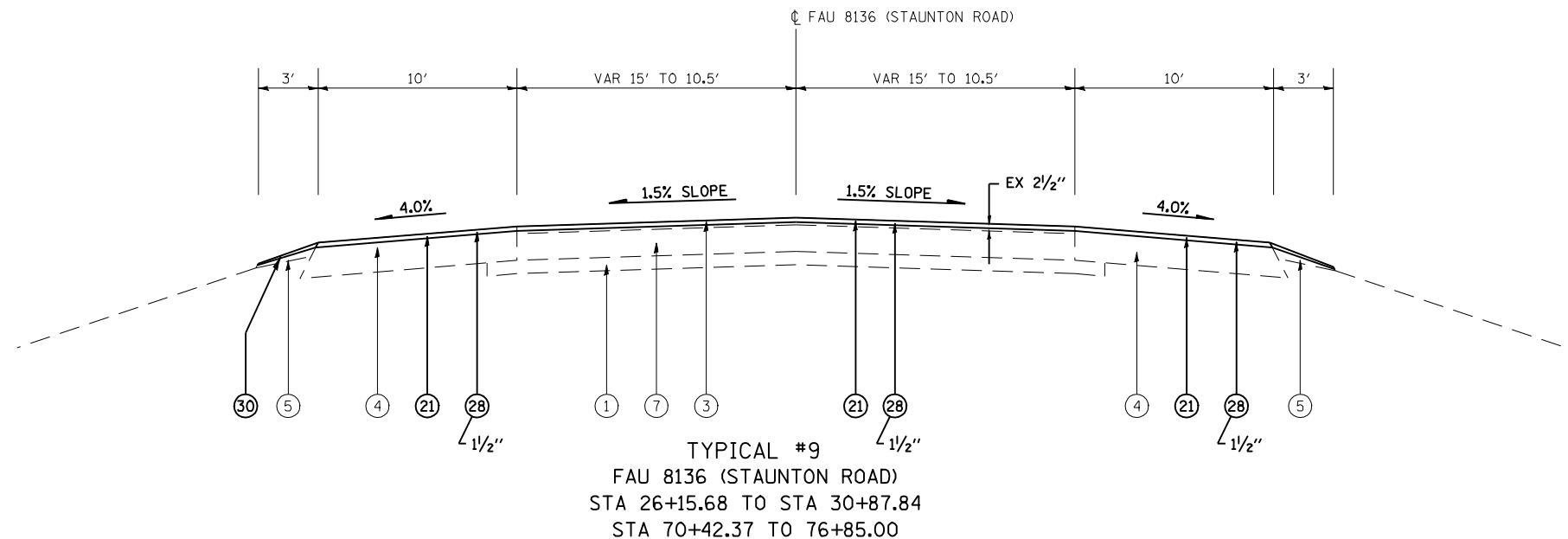
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
FAU 8136 (STAUNTON RD)

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	13
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	



* MATCH EXISTING CROSS SLOPES AT TRANSITIONS AND SUPER ELEVATED SECTIONS

** WHEN THE SUPERELEVATION RATE OF PAVEMENT IS BETWEEN 0.0% AND 4.0%, THE SHOULDER SLOPE SHALL BE SLOPED AT 4.0%. WHEN THE SUPER ELEVATION RATE OF PAVEMENT EXCEEDS 4.0%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDER IS 8.0% FOR THE HIGH SIDE OF THE SE, AND MATCH THE SE FOR THE LOW SIDE OF THE SE.

- EXISTING LEGEND
- ① EX STABILIZED SUB-BASE 4"
 - ② EX CRPCC PAVEMENT 9"
 - ③ EX HOT-MIX ASPHALT SURFACE COURSE (THICKNESS VARIES)
 - ④ EX HMA SHOULDERS (THICKNESS VARIES)
 - ⑤ EX AGGREGATE SHOULDERS
 - ⑥ EX PIPE UNDERDRAINS 6"
 - ⑦ EX PCC PAVEMENT 8"
 - ⑧ EX COMBINATION CONCRETE CURB & GUTTER M-6.06
 - ⑨ EX PCC MEDIAN SURFACE 4"

- PROPOSED LEGEND
- ⑳ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
 - ㉑ PR HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
 - ㉒ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 - ㉓ PR HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 - ㉔ PR HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
 - ㉕ PR HOT-MIX ASPHALT SURFACE REMOVAL, 5"
 - ㉖ PR POLYMERIZED HMA SURFACE COURSE, MIX "E", N105 1 1/2"
 - ㉗ PR POLYMERIZED HMA BINDER COURSE, IL-19.0, N105
 - ㉘ PR POLYMERIZED HMA SURFACE COURSE, MIX "C", N50
 - ㉙ PR HMA SHOULDER 8"
 - ㉚ PR AGGREGATE SHOULDERS, TYPE B (WEDGE)
 - ㉛ PR PAVEMENT MARKING

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS FAU 8136 (STAUNTON RD)			F.A.I. RTE. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =
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HOT-MIX ASPHALT SURFACE REMOVAL

LOCATION	PAVEMENT WIDTH (FOOT)	LT / MED SHOULDER WIDTH (FOOT)	RT/OUT SHOULDER WIDTH (FOOT)	HMA SURF REM 3/4"	HMA SURF REM 1 1/2"	HMA SURF REM 2"	HMA SURF REM 2 1/2"	HMA SURF REM 3 1/4"	HMA SURF REM 5"
	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)
I-55 MAINLINE									
SOUTHBOUND									
STA 698+00.00 TO STA 793+73.53	24	6	10			17,019.61			25,529.41
STA 793+73.53 TO STA 799+12.93	24	6				359.60			1,438.40
STA 799+12.93 TO STA 809+87.26 BK	24	6	10			1,909.92			2,864.88
STA 161+08.45 AH TO STA 177+53.43	24	6	10			2,924.41			4,386.61
STA 177+53.43 TO STA 188+86.78	24	6				755.57			3,022.27
STA 188+86.58 TO STA 250+00.00	24	6	10			10,868.30			16,302.45
NORTHBOUND									
STA 697+80.00 TO STA 790+45.83	24	6	10		16,472.59			24,708.88	
STA 790+45.83 TO STA 801+84.07	24	6			758.83			3,035.31	
STA 801+84.07 TO STA 809+87.26 BK	24	6	10		1,427.89			2,141.84	
STA 161+08.45 AH TO STA 179+65.63	24	6	10		3,301.65			4,952.48	
STA 179+65.63 TO STA 184+95.77	24	6			353.43			1,413.71	
STA 184+95.77 TO STA 250+00.00	24	6	10		11,563.08			17,344.61	
MEDIAN CROSSOVER									
STA 705+75	30 & VAR					274.79			
STA 245+30	30 & VAR					274.79			
FAU 8136 (STANTON INTERCHANGE RAMP)									
RAMP A									
STA 0+00.00 TO STA 3+06.89	1 TO 16		6			204.59			289.84
STA 3+06.89 TO STA 5+33.13	16		6			150.83			402.20
STA 5+33.13 TO STA 6+17.19	16	4	6			93.40			149.44
STA 6+17.19 TO STA 20+37.63	16	4	6	1,578.27			2,525.23		
STA 20+37.63 TO STA 21+58.64	VARY	VARY	VARY				719.79		
GORE AREA	VARY								248.22
RAMP B									
STA 0+23.00 TO STA 1+75.00	VARY	VARY	VARY				837.61		
STA 1+75.00 TO STA 15+13.14	16	4	6	1,486.82			2,378.92		
STA 15+13.14 TO STA 15+38.14	16	4	6			27.78			44.44
STA 15+38.14 TO STA 16+45.64	16 TO 14	16 TO 8	6			215.00			179.17
STA 16+45.64 TO STA 17+45.64	14	8 TO 6	6			144.44			155.56
STA 17+45.64 TO STA 19+45.64	14		6			133.33			311.11
STA 19+45.64 TO STA 26+68.61	16 TO 1		10			803.30			682.81
GORE AREA	VARY								88.89
RAMP C									
STA 0+00.00 TO STA 3+02.71	1 TO 16		6		201.81			285.89	
STA 3+02.71 TO STA 5+89.89	16		6		191.45			510.54	
STA 5+89.89 TO STA 21+39.70	16	4	6	1,722.01			2,755.22		
STA 21+39.70 TO STA 22+58.16	VARY	VARY	VARY				664.87		
GORE AREA	VARY							248.22	
RAMP D									
STA 0+24.00 TO STA 1+75.00	VARY	VARY	VARY				798.37		
STA 1+75.00 TO STA 15+15.12	16	4	6	1,489.02			2,382.44		
STA 15+15.12 TO STA 16+03.97	16 TO 14	16 TO 8	6			177.70		148.08	
STA 16+03.97 TO STA 16+91.00	14	8 TO 6	6			125.77		135.44	
STA 16+91.00 TO STA 18+24.16	14		6			88.77		207.14	
STA 18+24.16 TO STA 26+66.50	16 TO 1		6			561.56		795.54	
GORE AREA	VARY							59.18	

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PLOT DATE = Mar-23-2010 03:21:17PM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT										

HOT MIX ASPHALT SURFACE REMOVAL - CONTINUED

LOCATION	PAVEMENT WIDTH (FOOT)	LT / MED SHOULDER WIDTH (FOOT)	RT/OUT SHOULDER WIDTH (FOOT)	HMA SURF REM 3/4" (SQ YD)	HMA SURF REM 1 1/2" (SQ YD)	HMA SURF REM 2" (SQ YD)	HMA SURF REM 2 1/2" (SQ YD)	HMA SURF REM 3 1/4" (SQ YD)	HMA SURF REM 5" (SQ YD)
FAU 8136 (STAUNTON ROAD)									
STA 105+66.00 TO STA 122+25.00 BK	24				4,424.00				
STA 21+09.83 AH TO STA 24+59.23	24				931.73				
STA 24+59.23 TO STA 25+12.42	24	4 TO 8	4 TO 10		218.67				
STA 25+12.42 TO STA 25+52.01	24	8 TO 10	10		189.15				
STA 25+52.01 TO STA 29+37.84	24 TO 29	10	10		1,993.46				
STA 29+37.84 TO STA 34+50.00	29	10	10		2,788.43				
STA 34+50.00 TO STA 34+78.15	29 TO 46	10			148.57				
STA 34+78.15 TO STA 35+41.43	46	10			393.74				
STA 35+41.43 TO STA 35+80.78	46 TO 29	10			207.68				
STA 35+80.78 TO STA 40+83.10	29	10	10		2,734.85				
STA 40+83.10 TO STA 41+27.12	29		10		190.75				
STA 41+27.12 TO STA 41+59.80	29				105.30				
STA 41+59.80 TO STA 42+08.23	29 TO 40				185.65				
STA 42+08.23 TO STA 42+35.78	46				140.81				
STA 42+35.78 TO STA 42+38.51	46	10			16.99				
STA 42+38.51 TO STA 42+98.20	46	10	10		437.73				
STA 42+98.20 TO STA 43+54.27	40 TO 41	10	10		376.92				
STA 43+54.27 TO STA 45+65.00	41	10	10		1,428.28				
STA 45+65.00 TO STA 48+67.00	41 TO 29	10	10		1,845.56				
STA 48+67.00 TO STA 48+74.00	29	10	10		38.11				
STA 51+26.00 TO STA 51+36.36	29	10	10		56.40				
STA 51+36.36 TO STA 54+35.00	29 TO 41	10	10		1,825.02				
STA 54+35.00 TO STA 56+54.40	41	10	10		1,487.04				
STA 56+54.40 TO STA 57+06.11	41 TO 40	10	10		347.61				
STA 57+06.11 TO STA 57+60.31	46	10	10		397.47				
STA 57+60.31 TO STA 57+72.69	46		10		77.03				
STA 57+72.69 TO STA 57+96.92	46				123.84				
STA 57+96.92 TO STA 58+44.68	40 TO 29				183.08				
STA 58+44.68 TO STA 58+81.00	29				117.03				
STA 58+81.00 TO STA 59+47.15	29	10			286.65				
STA 59+47.15 TO STA 64+56.75	29	10	10		2,774.49				
STA 64+56.75 TO STA 65+31.75	29		10		325.00				
STA 65+31.75 TO STA 65+39.24	29				24.13				
STA 65+39.24 TO STA 65+66.86	29 TO 43				110.48				
STA 65+66.86 TO STA 66+31.62	46				331.00				
STA 66+31.62 TO STA 66+59.24	43 TO 29				110.48				
STA 66+59.24 TO STA 66+71.02	29				37.96				
STA 66+71.02 TO STA 67+14.71	29		10		189.32				
STA 67+14.71 TO STA 69+55.51	29 TO 32	10	10		1,351.16				
STA 69+55.51 TO STA 71+90.37	29	10	10		1,278.68				
STA 71+90.37 TO STA 75+98.18	31 TO 24	10	10		2,152.33				
STA 75+98.18 TO STA 76+00.00	24	10	10		8.90				
STA 76+00.00 TO STA 76+85.00	24 TO 21	10	10		401.39				
TOTAL				6,276	68,017	36,160	13,062	55,987	56,096

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ea:\pwwork\pwwork\COXTE\dms25312\0672400-schedule.dgn	DRAWN -	REVISED -	55						59RS-2, BR	MACOUPIN	100	16	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 72A60						
PLOT DATE = Mar-23-2010 03:21:19PM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT										

PAVING

LOCATION	PAVEMENT WIDTH (FOOT)	LEFT SHOULDER WIDTH (FOOT)	RIGHT SHOULDER WIDTH (FOOT)	PAVT AREA	SHLD AREA	BIT MATL PR CT (TON)	AGG PR CT (TON)	HMA BIND CSE (TON)	HMA SURF CSE MIX E N105 (TON)	HMA SURF CSE MIX C N50 (TON)
I-55 MAINLINE										
SOUTHBOUND										
STA 698+00.00 TO STA 793+73.53	24	10	6	25,529.41	17,019.61	16.17	85.10	5,003.76	2,144.47	1,906.20
STA 793+73.53 TO STA 799+12.93	24		6	1,438.40	359.60	0.68	3.60	281.93	120.83	40.28
STA 799+12.93 TO STA 809+87.26 BK	24	10	6	2,864.88	1,909.92	1.81	9.55	561.52	240.65	213.91
STA 161+08.45 AH TO STA 177+53.43	24	10	6	4,386.61	2,924.41	2.78	14.62	859.78	368.48	327.53
STA 177+53.43 TO STA 188+86.58	24		6	3,021.73	755.43	1.44	7.55	592.26	253.83	84.61
STA 188+86.58 TO STA 250+00.00	24	10	6	16,302.45	10,868.30	10.32	54.34	3,195.28	1,369.41	1,217.25
NORTHBOUND										
STA 697+80.00 TO STA 790+45.83	24	10	6	24,708.88	16,472.59	15.65	82.36	3,113.32	2,075.55	1,844.93
STA 790+45.83 TO STA 801+84.07	24		6	3,035.31	758.83	1.44	7.59	382.45	254.97	84.99
STA 801+84.07 TO STA 809+87.26 BK	24	10	6	2,141.84	1,427.89	1.36	7.14	269.87	179.91	159.92
STA 161+08.45 AH TO STA 179+65.63	24	10	6	4,952.48	3,301.65	3.14	16.51	624.01	416.01	369.78
STA 179+65.63 TO STA 184+95.77	24		6	1,413.71	353.43	0.67	3.53	178.13	118.75	39.58
STA 184+95.77 TO STA 250+00.00	24	10	6	17,344.61	11,563.08	10.98	57.82	2,185.42	1,456.95	1,295.06
MEDIAN CROSSOVER										
STA 705+75	30 & VAR				274.79	0.10	0.55			30.78
STA 245+30	30 & VAR				274.79	0.10	0.55			30.78
FAU 8136 (STALUNTON INTERCHANGE RAMP)										
RAMP A										
STA 0+00.00 TO STA 3+06.89	1 TO 16		6	289.84	204.59	0.19	0.99	56.81	24.35	22.91
STA 3+06.89 TO STA 5+33.13	16		6	402.20	150.83	0.21	1.11	78.83	33.78	16.89
STA 5+33.13 TO STA 6+17.19	16	4	6	149.44	93.40	0.09	0.49	29.29	12.55	10.46
STA 6+17.19 TO STA 20+25.00	16	4	6	2,502.77	1,564.23	1.55	8.13	315.35	210.23	175.19
STA 20+25.00 TO STA 21+82.44	VARY	VARY	VARY	462.87	220.79	0.26	1.37		64.80	24.73
GORE AREA	VARY			248.22		0.09	0.50	48.65	20.85	
RAMP B										
STA 0+22.20 TO STA 1+75.00	VARY	VARY	VARY	528.82	228.83	0.29	1.52		74.03	25.63
STA 1+75.00 TO STA 15+13.14	16	4	6	2,378.92	1,486.82	1.47	7.73	299.74	199.83	166.52
STA 15+13.14 TO STA 15+38.14	16	4	6	44.44	27.78	0.03	0.14	8.71	3.73	3.11
STA 15+38.14 TO STA 16+45.64	16 TO 14	16 TO 8	6	179.17	215.00	0.15	0.79	35.12	15.05	24.08
STA 16+45.64 TO STA 17+45.64	14	8 TO 6	6	155.56	144.44	0.11	0.60	30.49	13.07	16.18
STA 17+45.64 TO STA 19+45.64	14		6	311.11	133.33	0.17	0.89	60.98	26.13	14.93
STA 19+45.64 TO STA 26+68.61	16 TO 1		10	682.81	803.30	0.56	2.97	133.83	57.36	89.97
GORE AREA	VARY			88.89		0.03	0.18	17.42	7.47	
RAMP C										
STA 0+00.00 TO STA 3+02.71	1 TO 16		6	285.89	201.81	0.19	0.98	36.02	24.01	22.60
STA 3+02.71 TO STA 5+89.89	16		6	510.54	191.45	0.27	1.40	64.33	42.89	21.44
STA 5+89.89 TO STA 21+25.00	16	4	6	2,729.08	1,705.68	1.69	8.87	343.86	229.24	191.04
STA 21+25.00 TO STA 22+60.21	VARY	VARY	VARY	471.30	213.14	0.26	1.37		65.98	23.87
GORE AREA	VARY			248.22		0.09	0.50	31.28	20.85	
RAMP D										
STA 0+24.00 TO STA 1+50.00	VARY	VARY	VARY	475.42	203.89	0.26	1.36		66.56	22.84
STA 1+50.00 TO STA 15+15.12	16	4	6	2,426.88	1,516.80	1.50	7.89	305.79	203.86	169.88
STA 15+15.12 TO STA 16+03.97	16 TO 14	16 TO 8	6	148.08	177.70	0.12	0.65	18.66	12.44	19.90
STA 16+03.97 TO STA 16+91.00	14	8 TO 6	6	135.44	125.77	0.10	0.52	17.07	11.38	14.09
STA 16+91.00 TO STA 18+24.16	14		6	207.14	88.77	0.11	0.59	26.10	17.40	9.94
STA 18+24.16 TO STA 26+66.50	16 TO 1		6	795.54	561.56	0.52	2.71	100.24	66.83	62.89
GORE AREA	VARY			59.18		0.02	0.12	7.46	4.97	

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FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwork\PWID01\COXTE\dms25312\0672A00-schedule.dgn		DRAWN -	REVISED -		55	59RS-2, BR	MACOUPIN	100	17				
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 72A60								
PLOT DATE = Mar-23-2010 03:21:21PM		DATE -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			

PAVING - CONTINUED

LOCATION	PAVEMENT WIDTH (FOOT)	LEFT SHOULDER WIDTH (FOOT)	RIGHT SHOULDER WIDTH (FOOT)	PAVT AREA	SHLD AREA	BIT MATL PR CT (TON)	AGG PR CT (TON)	HMA BIND CSE (TON)	HMA SURF CSE MIX E N105 (TON)	HMA SURF CSE MIX C N50 (TON)
FAU 8136 (STAUNTON ROAD)										
STA 105+66.00 TO STA 122+25.00 BK	24			4,424.00		1.68	8.85			371.62
STA 21+09.83 AH TO STA 24+59.23	24			931.73		0.35	1.86			78.27
STA 24+59.23 TO STA 25+12.42	24	4 TO 8	4 TO 10	141.84	76.83	0.08	0.44			18.37
STA 25+12.42 TO STA 25+52.01	24	8 TO 10	10	105.57	83.58	0.07	0.38			15.89
STA 25+52.01 TO STA 29+37.84	24 TO 29	10	10	1,136.06	857.40	0.76	3.99			167.45
STA 29+37.84 TO STA 34+50.00	29	10	10	1,650.29	1,138.13	1.06	5.58			234.23
STA 34+50.00 TO STA 34+78.15	29 TO 46	10		117.29	31.28	0.06	0.30			12.48
STA 34+78.15 TO STA 35+41.43	46	10		323.43		0.12	0.65			27.17
STA 35+41.43 TO STA 35+80.78	46 TO 29	10		163.96	43.72	0.08	0.42			17.45
STA 35+80.78 TO STA 40+83.10	29	10	10	1,618.59	1,116.27	1.04	5.47			229.73
STA 40+83.10 TO STA 41+27.12	29		10	141.84	48.91	0.07	0.38			16.02
STA 41+27.12 TO STA 41+59.80	29			105.30		0.04	0.21			8.85
STA 41+59.80 TO STA 42+08.23	29 TO 40			185.65		0.07	0.37			15.59
STA 42+08.23 TO STA 42+35.78	46			140.81		0.05	0.28			11.83
STA 42+35.78 TO STA 42+38.51	46	10		13.95	3.03	0.01	0.03			1.43
STA 42+38.51 TO STA 42+98.20	46	10	10	305.08	132.64	0.17	0.88			36.77
STA 42+98.20 TO STA 43+54.27	40 TO 41	10	10	252.32	124.60	0.14	0.75			31.66
STA 43+54.27 TO STA 45+65.00	41	10	10	959.99	468.29	0.54	2.86			119.98
STA 45+65.00 TO STA 48+67.00	41 TO 29	10	10	1,174.44	671.11	0.70	3.69			155.03
STA 48+67.00 TO STA 48+74.00	29	10	10	22.56	15.56	0.01	0.08			3.20
STA 51+26.00 TO STA 51+36.36	29	10	10	33.38	23.02	0.02	0.11			4.74
STA 51+36.36 TO STA 54+35.00	29 TO 41	10	10	1,161.38	663.64	0.69	3.65			153.30
STA 54+35.00 TO STA 56+54.40	41	10	10	999.49	487.56	0.57	2.97			124.91
STA 56+54.40 TO STA 57+06.11	41 TO 40	10	10	232.70	114.91	0.13	0.70			29.20
STA 57+06.11 TO STA 57+60.31	46	10	10	277.02	120.44	0.15	0.79			33.39
STA 57+60.31 TO STA 57+72.69	46		10	63.28	13.76	0.03	0.15			6.47
STA 57+72.69 TO STA 57+96.92	46			123.84		0.05	0.25			10.40
STA 57+96.92 TO STA 58+44.68	40 TO 29			183.08		0.07	0.37			15.38
STA 58+44.68 TO STA 58+81.00	29			117.03		0.04	0.23			9.83
STA 58+81.00 TO STA 59+47.15	29	10		213.15	73.50	0.11	0.57			24.08
STA 59+47.15 TO STA 64+56.75	29	10	10	1,642.04	1,132.44	1.05	5.55			233.06
STA 64+56.75 TO STA 65+31.75	29		10	241.67	83.33	0.12	0.65			27.30
STA 65+31.75 TO STA 65+39.24	29			24.13		0.01	0.05			2.03
STA 65+39.24 TO STA 65+66.86	29 TO 43			110.48		0.04	0.22			9.28
STA 65+66.86 TO STA 66+31.62	46			331.00		0.13	0.66			27.80
STA 66+31.62 TO STA 66+59.24	43 TO 29			110.48		0.04	0.22			9.28
STA 66+59.24 TO STA 66+71.02	29			37.96		0.01	0.08			3.19
STA 66+71.02 TO STA 67+14.71	29		10	140.78	48.54	0.07	0.38			15.90
STA 67+14.71 TO STA 69+55.51	29 TO 32	10	10	816.04	535.11	0.51	2.70			113.50
STA 69+55.51 TO STA 71+90.37	29	10	10	756.77	521.91	0.49	2.56			107.41
STA 71+90.37 TO STA 75+98.18	31 TO 24	10	10	1,246.09	906.24	0.82	4.30			180.80
STA 75+98.18 TO STA 76+00.00	24	10	10	4.85	4.04	0.00	0.02			0.75
STA 76+00.00 TO STA 76+85.00	24 TO 21	10	10	212.50	188.89	0.15	0.80			33.72
ENTRANCES, SIDE ROADS						0.70	3.50			
TOTAL						90.1	474	19,314*	10,529*	11,543

HOT-MIX ASPHALT SHOULDERS 8"

LOCATION	LEFT SHOULDER WIDTH (FT)	RIGHT SHOULDER WIDTH (FT)	HMA SHLDS 8" (SQ YD)	EXCAV & GR EX SHLDS (UNIT)
I-55 MAINLINE				
SOUTHBOUND				
STA 701+32.00 TO STA 705+61.00	4.00		190.67	4.30
STA 240+85.00 TO STA 245+14.00	4.00		190.67	4.30
NORTHBOUND				
STA 705+91.00 TO STA 710+20.00		4.00	190.67	4.30
STA 245+44.00 TO STA 249+73.00		4.00	190.67	4.30
FAU 8136 (STAUNTON ROAD)				
STA 105+66.00 TO STA 122+25.00 BK	4.00	4.00	1,474.67	33.20
STA 21+09.83 AH TO STA 24+59.03	4.00	4.00	310.40	7.00
RAMP INTERSECTION IMPROVEMENTS			294.27	
TOTAL			2,842	57

* - TO BE PLACED WITH THE MATERIAL TRANSFER DEVICE

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwork\PIWID01\COXTE\dms25312\0672A00-schedule.dgn		DRAWN -	REVISED -		55	59RS-2, BR	MACOUPIN	100	18				
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 72A60								
PLOT DATE = Mar-23-2010 03:21:24PM		DATE -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			

AGGREGATE SHOULDERS, TYPE B

LOCATION	MEDIAN SHOULDER WIDTH (FT)	OUTSIDE SHOULDER WIDTH (FT)	AVERAGE THICKNESS (IN)	AREA (SQ YD)	WEIGHT (TONS)
I-55 MAINLINE					
SOUTHBOUND					
STA 698+00.00 TO STA 793+73.53	5	3	1.5	8,509.80	726.88
STA 793+73.53 TO STA 799+12.93	5		1.5	299.67	25.60
STA 799+12.93 TO STA 809+87.26 BK	5	3	1.5	954.96	81.57
STA 161+08.45 AT0 STA 177+53.43	5	3	1.5	1,462.20	124.90
STA 177+53.43 TO STA 188+86.58	5		1.5	629.53	53.77
STA 188+86.58 TO STA 250+00.00	5	3	1.5	5,434.15	464.17
NORTHBOUND					
STA 697+80.00 TO STA 790+45.83	5	3	1.5	8,236.29	703.52
STA 790+45.83 TO STA 801+84.07	5		1.5	632.36	54.01
STA 801+84.07 TO STA 809+87.26 BK	5	3	1.5	713.95	60.98
STA 161+08.45 AT0 STA 179+65.63	5	3	1.5	1,650.83	141.01
STA 179+65.63 TO STA 184+95.77	5		1.5	294.52	25.16
STA 184+95.77 TO STA 250+00.00	5	3	1.5	5,781.54	493.84
FAU 8136 (STALANTON INTERCHANGE RAMP)					
RAMP A					
STA 0+00.00 TO STA 5+94.63		LEFT	RIGHT		
STA 5+94.63 TO STA 20+87.48	5	3	2.0	1,326.98	151.13
RAMP B					
STA 0+88.10 TO STA 15+38.14	5	3	2.0	1,288.92	146.79
STA 15+38.14 TO STA 26+68.61		3	2.0	376.82	42.92
RAMP C					
STA 0+00.00 TO STA 5+48.83		3	2.0	182.94	20.83
STA 5+48.83 TO STA 22+20.02	5	3	2.0	1,485.50	169.18
RAMP D					
STA 0+90.21 TO STA 15+15.12	5	3	2.0	1,266.59	144.25
STA 15+15.12 TO STA 26+66.50		3	2.0	383.79	43.71
FAU 8136 (STALANTON ROAD)					
STA 105+66.00 TO STA 122+25.00 BK	3	3	1.5	1,106.00	94.47
STA 21+09.83 A TO STA 34+66.77	3	3	1.5	904.63	77.27
STA 35+55.32 TO STA 41+12.46	3	3	1.5	371.43	31.73
STA 41+12.46 TO STA 41+38.13		3	1.5	8.56	0.73
STA 42+31.22 TO STA 42+33.98	3		1.5	0.92	0.08
STA 42+33.98 TO STA 48+74.00	3	3	1.5	426.68	36.45
STA 51+26.00 TO STA 57+64.32	3	3	1.5	425.55	36.35
STA 57+64.32 TO STA 57+77.31		3	1.5	4.33	0.37
STA 58+60.78 TO STA 59+01.20	3		1.5	13.47	1.15
STA 59+01.20 TO STA 64+90.38	3	3	1.5	392.79	33.55
STA 64+90.38 TO STA 65+53.50		3	1.5	21.04	1.80
STA 66+45.63 TO STA 66+81.86		3	1.5	12.08	1.03
STA 66+81.86 TO STA 76+85.00	3	3	1.5	668.76	57.12
TOTAL					4,069

TEMPORARY RAMPS

LOCATION	LENGTH	WIDTH	AREA (SQ YD)
I-55 MAINLINE			
SOUTHBOUND			
STA 698+00.00 TO STA 698+16.67	16.67	40.0	74.09
STA 249+83.33 TO STA 250+00.00	16.67	40.0	74.09
NORTHBOUND			
STA 697+80.00 TO STA 697+92.50	12.50	40.0	55.56
STA 249+87.50 TO STA 250+00.00	12.50	40.0	55.56
FAU 8136 (STALANTON ROAD)			
STA 105+66.00 TO STA 105+71.00	5.00	32.0	17.78
STA 48+69.00 TO STA 48+74.00	5.00	48.0	26.67
STA 51+26.00 TO STA 51+31.00	5.00	48.0	26.67
STA 76+80.00 TO STA 76+85.00	5.00	48.0	26.67
AT RAMP A	8.33	100.0	92.56
AT RAMP B	8.33	100.0	92.56
AT RAMP C	8.33	100.0	92.56
AT RAMP D	8.33	100.0	92.56
WEST FRONTAGE RD (RT)	5.00	42.0	23.33
EAST FRONTAGE RD (LT)	5.00	44.0	24.44
EAST FRONTAGE RD (RT)	5.00	44.0	24.44
TOTAL			800

MAINTENANCE MOWING

LOCATION	LENGTH (FOOT)	LEFT WIDTH (FOOT)	MEDIAN WIDTH (FOOT)	RIGHT WIDTH (FOOT)	TOTAL WIDTH (FOOT)	AREA (ACRES)	NUMBER OF MOWINGS	TOTAL AREA (ACRES)
I-55 MAINLINE								
STA 697+80 TO STA 809+87 BK	11,207	20	76	20	116	29.84	2	59.68
STA 161+08 AH TO STA 250+00	8,892	20	76	20	116	23.68	2	47.36
FAU 8136 (STALANTON INTERCHANGE RAMP)								
RAMP A								
STA 0+00 TO STA 6+62	662			15	15	0.23	2	0.46
STA 6+62 TO STA 21+59	1,497	15		15	30	1.03	2	2.06
RAMP B								
STA 0+23.00 TO STA 15+38	1,515	15		15	30	1.04	2	2.08
STA 15+38.00 TO STA 26+69	1,131			15	15	0.39	2	0.78
RAMP C								
STA 0+00 TO STA 6+62	662			15	15	0.23	2	0.46
STA 6+62 TO STA 22+58	1,596	15		15	30	1.10	2	2.20
RAMP D								
STA 0+23 TO STA 15+15	1,492	15		15	30	1.03	2	2.06
STA 15+15 TO STA 26+67	1,152			15	15	0.40	2	0.80
TOTAL								118

DELINEATORS

LOCATION	RADIUS (FOOT)	SPACING (FOOT)	IN CURVE (EACH)	TOTAL (EACH)
I-55 MAINLINE				
NORTHBOUND				
STA 705+75 IN ADVANCE OF MED X-OVER	N/A		2	2
STA 744+53 TO STA 790+46	5,730	400	12	12
STA 801+84 TO STA 809+87	5,730	400	3	3
STA 245+30 IN ADVANCE OF MED X-OVER	N/A		2	2
SOUTHBOUND				
STA 705+75 IN ADVANCE OF MED X-OVER	N/A		2	2
STA 744+53 TO STA 793+74	5,730	400	13	13
STA 799+96 TO STA 809+87	5,730	400	3	3
STA 245+30 IN ADVANCE OF MED X-OVER	N/A		2	2
FAU 8136 (STALANTON ROAD) INTERCHANGE				
RAMP A				
STA 0+00 TO STA 6+62	N/A	100	7	7
STA 6+62 TO STA 20+96	VAR (477 - 764)	65	23	46
RAMP B				
STA 0+93 TO STA 16+46	VAR (477 - 764)	65	24	48
STA 16+46 TO STA 26+69	N/A	100	11	11
RAMP C				
STA 0+00 TO STA 6+62	N/A	100	7	7
STA 6+62 TO STA 22+17	VAR (477 - 764)	65	24	48
RAMP D				
STA 0+88 TO STA 16+02	VAR (477 - 764)	65	24	48
STA 16+02 TO STA 26+66	N/A	100	11	11
TOTAL				265

SHOULDER RUMBLE STRIP

LOCATION	LENGTH (FOOT)
I-55 MAINLINE	
SOUTHBOUND OUTSIDE SHOULDER	
STA 698+00.00 TO STA 793+73.53	9,573.53
STA 799+12.93 TO STA 809+87.26 BK	1,074.33
STA 161+08.45 AH TO STA 177+53.43	1,644.98
STA 188+86.58 TO STA 250+00.00	6,113.42
SOUTHBOUND INSIDE SHOULDER	
STA 698+00.00 TO STA 809+87.26 BK	11,187.26
STA 161+08.45 AH TO STA 250+00.00	8,891.55
NORTHBOUND OUTSIDE SHOULDER	
STA 697+80.00 TO STA 790+45.83	9,265.83
STA 801+84.07 TO STA 809+87.26 BK	803.19
STA 161+08.45 AH TO STA 179+65.63	1,857.18
STA 184+95.77 TO STA 250+00.00	6,504.23
NORTHBOUND INSIDE SHOULDER	
STA 697+80.00 TO STA 809+87.26 BK	11,207.26
STA 161+08.45 AH TO STA 250+00.00	8,891.55
TOTAL	77,014

RAMP INTERSECTION IMPROVEMENTS - REMOVAL ITEMS

LOCATION	PAVED SHOULDER REMOVAL (SQ YD)	PAVEMENT REMOVAL (SQ YD)	MEDIAN REMOVAL (SQ FT)	CONCRETE MEDIAN SURFACE REMOVAL (SQ FT)	COMB CURB & GUTTER REMOVAL (FOOT)
FAU 8136 (STAUNTON RAMPS)					
RAMP A					
STA 20+69 TO STA 21+50	36.61				
STA 21+40		24.43			
RAMP B					
STA 0+23 TO STA 1+73	72.53				
RAMP C					
STA 21+63 TO STA 22+60	110.02				
STA 22+02 TO STA 22+60	24.76				
STA 22+45		12.83			
RAMP D					
STA 0+23 TO STA 1+75	73.83				
FAU 8136 (STAUNTON ROAD)					
STA 41+12 TO STA 42+08		134.15			
STA 42+64 TO STA 42+72			31.88		
STA 42+72 TO STA 43+54				313.53	
STA 42+72 TO STA 43+54					165.6
STA 56+54 TO STA 57+31					152.8
STA 56+54 TO STA 57+31				300.89	
STA 57+31 TO STA 57+35			18.06		
STA 57+96 TO STA 58+71		90.83			
TOTAL	318	262	50	614	318

RAMP INTERSECTION IMPROVEMENTS - PLACEMENT ITEMS

LOCATION	PCC BASE CSE 8" (SQ YD)	HMA BASE CSE 11" (SQ YD)	SUBBASE GRAN MATL TY A 4" (SQ YD)	SUBBASE GRAN MATL TY C (TON)	HMA SHLD 8" (SQ YD)	CONCRETE MEDIAN SURF 4" (SQ FT)	CONCRETE MEDIAN SM-6.06 (SQ FT)	COMB CC&G M-6.06 (FOOT)	COMB CC&G M-6.24 (FOOT)
FAU 8136 (STAUNTON RAMPS)									
RAMP A									
STA 20+69 TO STA 21+50		96.24	105.79		32.02				
STA 21+40				12.71		138.49		29.9	24.4
RAMP B									
STA 0+23 TO STA 1+73		83.06	100.68		78.55				
RAMP C									
STA 21+63 TO STA 22+60		123.84	144.18		87.23				
STA 22+02 TO STA 22+60		36.38	43.31		22.65				
STA 22+45				9.23		100.63		24.4	22.5
RAMP D									
STA 0+23 TO STA 1+75		86.60	104.25		73.82				
FAU 8136 (STAUNTON ROAD)									
STA 41+12 TO STA 42+02								179.8	
STA 42+02 TO STA 42+08							47.18		
STA 42+64 TO STA 43+04	20.90								
STA 43+04 TO STA 43+11							32.96		
STA 43+11 TO STA 43+54					15.17	165.32		99.2	
STA 56+54 TO STA 56+98					13.74	149.75		90.4	
STA 56+98 TO STA 57+04							33.10		
STA 57+04 TO STA 57+35	16.32								
STA 57+97 TO STA 58+04							46.95		
STA 58+04 TO STA 58+71								136.7	
TOTAL	37	426	498	51	294	554	160	560	47

GRADING AND SHAPING, RIPRAP, AND OTHER MISC WORK SCHEDULE

LOCATION	GRADING & SHAPING DITCHES (FOOT)	SEEDING CLASS 2 (ACRE)	MULCH METH 2 (ACRE)	EROSION CONTROL BLANKET (SQ YD)	FURN EXCAV (CU YD)	CONTR LOW STR MATL (CU YD)	STONE DUMPED RR, CL A4 (TON)	TREE REM 6 - 15 UNIT DIAMETER (UNITS)
I-55 MAINLINE								
SB STA 748+65								7
SB STA 748+65							30.0	15
NB STA 749+50		0.01	0.01		5	2		
NB STA 769+00 TO STA 770+50	150.0	0.05	0.05					
NB STA 772+50 TO STA 774+00	150.0	0.05	0.05					
MED STA 773+0 TO STA 774+00	100.0	0.03	0.03					
MED STA 205+78		0.01		37.3		3		
NB STA 205+78		0.01		18.7		1.5		
SB STA 205+78		0.02		18.7	5	3.5		
FAU 8136 (STAUNTON ROAD) INTERCHANGE								
RAMP A								
STA 19+00		0.01	0.01		2			
FAU 8136 (STAUNTON ROAD)								
MED STA 41+12 TO STA 42+02		0.03	0.03		24			
MED STA 58+03 TO STA 58+71		0.02	0.02		17			
TOTAL	400	0.2	0.2	75	53	10	30	22

DRAINAGE SCHEDULE - PIPE CULVERTS

LOCATION	PIPE CULV CL D, TY 1, 18" (FOOT)	PIPE CULV CL D, TY 1, 24" (FOOT)	METAL END SECTIONS 18" (EACH)	METAL END SECTIONS 24" (EACH)	PIPE ELBOW 18" (EACH)	PIPE CULVERT REMOVAL (FOOT)	END SECTION TO BE REMOVED (EACH)	MED INLET 604101 TO BE RECONSTR (EACH)
I-55 MAINLINE								
SB STA 746+50		8.0		1.0		8.0	1.0	
SB STA 748+65		8.0				8.0		
MED STA 205+77								2.0
NB STA 205+77								1.0
SB STA 205+78	13.0		1.0		1.0	13.0	1	1.0
TOTAL	13	16	1	1	1	29	1	4

DRAINAGE SCHEDULE - CLEANING ITEMS

LOCATION	CULVERT TO BE CLEANED (FOOT)	CLEAN EXIST INLETS (EACH)
I-55 MAINLINE		
MED STA 706+00		1
NB STA 706+00 24" RCCP CULVERT	98.0	
NB STA 773+00 24" RCCP CULVERT	98.0	
MED STA 192+50		1
MED STA 245+00		1
TOTAL	196	3

DITCH CHECKS, INLET & PIPE PROTECTION

LOCATION	INLET & PIPE PROTECT (EACH)
I-55 MAINLINE	
NB STA 769+00 TO STA 770+50	1
NB STA 772+50 TO STA 774+00	1
MED STA 773+0 TO STA 774+00	1
TOTAL	3

GUARD POSTS

LOCATION	QUANTITY (EACH)
I-55 MAINLINE	
MED STA 729+50	4
MED STA 742+00	2
MED STA 179+50	2
MED STA 221+00	4
MED STA 233+00	2
TOTAL	14

CLEANING UNDERDRAIN OUTLETS

LOCATION	OUTSIDE	MEDIAN
SOUTHBOUND I-55		
STA 700+00	1	1
STA 705+40	1	1
STA 710+00	1	1
STA 719+00	1	1
STA 724+00	1	1
STA 729+00	1	1
STA 734+00	1	1
STA 739+00	1	1
STA 744+00	1	1
STA 749+00	1	1
STA 756+60	1	1
STA 760+00	1	1
STA 765+00	1	1
STA 770+00	1	1
STA 775+00	1	1
STA 780+00	1	1
STA 785+00	1	1
STA 795+00		1
STA 800+00		1
STA 805+00	1	1
STA 161+25	1	1
STA 166+00	1	1
STA 171+00	1	1
STA 175+00		1
STA 177+00	1	
STA 180+00		1
STA 185+00		1
STA 190+00		1
STA 195+00	1	1
STA 200+00	1	1
STA 201+00	1	1
STA 204+00	1	1
STA 209+00	1	1
STA 220+00	1	1
STA 225+00	1	1
STA 230+00	1	1
STA 235+00	1	1
STA 240+00	1	1
STA 244+75	1	1
NORTHBOUND I-55		
STA 700+00	1	1
STA 705+40	1	1
STA 710+00	1	1
STA 719+00	1	1
STA 724+00	1	1
STA 729+00	1	1
STA 734+00	1	1
STA 739+00	1	1
STA 744+00	1	1
STA 749+00	1	1
STA 756+60	1	1
STA 760+00	1	1
STA 765+00	1	1
STA 770+00	1	1
STA 775+00	1	1
STA 780+00	1	1
STA 785+00	1	1
STA 795+00		1
STA 800+00		1
STA 805+00	1	1
STA 161+25	1	1
STA 166+00	1	1
STA 171+00	1	1
STA 175+00		1
STA 177+50	1	

CONTINUED

CLEANING UNDERDRAIN OUTLETS

LOCATION	OUTSIDE	MEDIAN
NORTHBOUND I-55		
STA 180+00		1
STA 185+00		1
STA 190+00	1	1
STA 195+00	1	1
STA 200+00	1	1
STA 201+00	1	1
STA 204+00	1	1
STA 209+00	1	1
STA 220+00	1	1
STA 225+00	1	1
STA 230+00	1	1
STA 235+00	1	1
STA 240+00	1	1
STA 244+75	1	1
FAU 8136 (STAUNTON ROAD) INTERCHANGE		
RAMP A		
STA 0+00	1	
STA 5+00	1	
STA 11+50	1	1
STA 16+15	1	1
RAMP B		
STA 4+90	1	1
STA 9+80	1	1
STA 14+90	1	1
STA 21+80	1	1
STA 26+95	1	1
RAMP C		
STA 0+00	1	
STA 5+00	1	
STA 7+15		1
STA 10+00	1	
STA 12+00		1
STA 16+00	1	
STA 17+00		1
RAMP D		
STA 4+90	1	1
STA 9+90	1	1
STA 14+90	1	1
STA 17+10	1	1
STA 17+90	1	1
STA 26+65	1	1
TOTAL	86	85

GRAND TOTAL 171

OUTLET MARKERS

LOCATION	OUTSIDE	MEDIAN
SOUTHBOUND I-55		
STA 700+00	1	1
STA 705+40	1	1
STA 710+00	1	1
STA 719+00	1	1
STA 724+00	1	1
STA 729+00	1	1
STA 734+00	1	1
STA 739+00	1	1
STA 744+00	1	1
STA 749+00	1	1
STA 756+60	1	1
STA 760+00	1	1
STA 765+00	1	1
STA 770+00	1	1
STA 775+00	1	1
STA 780+00	1	1
STA 785+00	1	1
STA 795+00		1
STA 800+00		1
STA 805+00	1	1
STA 161+25	1	1
STA 166+00	1	1
STA 171+00	1	1
STA 175+00		1
STA 177+00	1	
STA 180+00		1
STA 185+00		1
STA 190+00		1
STA 195+00	1	1
STA 200+00	1	1
STA 201+00	1	1
STA 204+00	1	1
STA 209+00	1	1
STA 220+00	1	1
STA 225+00	1	1
STA 230+00	1	1
STA 235+00	1	1
STA 240+00	1	1
STA 244+75	1	1
NORTHBOUND I-55		
STA 700+00	1	1
STA 705+40	1	1
STA 710+00	1	1
STA 719+00	1	1
STA 724+00	1	1
STA 729+00	1	1
STA 734+00	1	1
STA 739+00	1	1
STA 744+00	1	1
STA 749+00	1	1
STA 756+60	1	1
STA 760+00	1	1
STA 765+00	1	1
STA 770+00	1	1
STA 775+00	1	1
STA 780+00	1	1
STA 785+00	1	1
STA 795+00		1
STA 800+00		1
STA 805+00	1	1
STA 161+25	1	1
STA 166+00	1	1
STA 171+00	1	1
STA 175+00		1
STA 177+50	1	

CONTINUED

OUTLET MARKERS

LOCATION	OUTSIDE	MEDIAN
NORTHBOUND I-55		
STA 180+00		1
STA 185+00		1
STA 190+00	1	1
STA 195+00	1	1
STA 200+00	1	1
STA 201+00	1	1
STA 204+00	1	1
STA 209+00	1	1
STA 220+00	1	1
STA 225+00	1	1
STA 230+00	1	1
STA 235+00	1	1
STA 240+00	1	1
STA 244+75	1	1
FAU 8136 (STAUNTON ROAD) INTERCHANGE		
RAMP A		
STA 0+00	1	
STA 5+00	1	
STA 11+50	1	1
STA 16+15	1	1
RAMP B		
STA 4+90	1	1
STA 9+80	1	1
STA 14+90	1	1
STA 21+80	1	1
STA 26+95	1	1
RAMP C		
STA 0+00	1	
STA 5+00	1	
STA 7+15		1
STA 10+00	1	
STA 12+00		1
STA 16+00	1	
STA 17+00		1
RAMP D		
STA 4+90	1	1
STA 9+90	1	1
STA 14+90	1	1
STA 17+10	1	1
STA 17+90	1	1
STA 26+65	1	1
TOTAL	86	85

GRAND TOTAL 171

CLASS A PATCHING SCHEDULE (I-55)

LOCATION	LENGTH DL (FOOT)	LENGTH PL (FOOT)	WIDTH (FOOT)	CL A 12" TY II (SQ YD)	CL A 14" TY II (SQ YD)	CL A 14" TY III (SQ YD)	SAW CUTS (FOOT)	PATCH REINF (SQ YD)
SOUTHBOUND I-55								
STA 714+65	12.0		12.0			16.00	72.0	16.00
STA 723+90	6.0		12.0		8.00		60.0	8.00
STA 723+90		6.0	12.0		8.00		60.0	8.00
STA 781+70	6.0		12.0		8.00		60.0	8.00
STA 781+70		6.0	12.0		8.00		60.0	8.00
STA 807+00	10.0		12.0		13.33		68.0	13.33
STA 808+25	8.0		12.0		10.67		64.0	10.67
STA 169+65	6.0		12.0		8.00		60.0	8.00
STA 178+50	6.0		12.0		8.00		60.0	8.00
STA 186+50	8.0		12.0		10.67		64.0	10.67
STA 215+90	6.0		12.0		8.00		60.0	8.00
STA 234+60	6.0		12.0		8.00		60.0	8.00
NORTHBOUND I-55								
STA 727+50	10.0		12.0	13.33			68.0	13.33
STA 729+30	6.0		12.0	8.00			60.0	8.00
STA 741+10	6.0		12.0	8.00			60.0	8.00
STA 746+30	6.0		12.0	8.00			60.0	8.00
STA 185+60	6.0		12.0	8.00			60.0	8.00
TOTALS				45.33	98.67	16.00	1056.0	160.00

PAVEMENT PATCHING SCHEDULE - FAU 8136 (STAUNTON ROAD)

LOCATION	LENGTH DL (FOOT)	WIDTH (FOOT)	PAVT PATCH 11" TY II (SQ YD)
WESTBOUND FAU 8136 (STAUNTON ROAD)			
STA 106+00	6.0	12.0	8.00
STA 110+50 TURN LANE	6.0	12.0	8.00
STA 111+50 TURN LANE	6.0	12.0	8.00
STA 115+50	6.0	12.0	8.00
STA 119+20	6.0	12.0	8.00
STA 120+50	6.0	12.0	8.00
STA 121+44	6.0	12.0	8.00
STA 23+25	6.0	12.0	8.00
STA 25+20	6.0	12.0	8.00
STA 28+30	6.0	12.0	8.00
STA 29+50	6.0	12.0	8.00
STA 45+10	6.0	12.0	8.00
STA 46+00	6.0	12.0	8.00
STA 47+00	6.0	15.0	10.00
EASTBOUND FAU 8136 (STAUNTON ROAD)			
STA 106+50	6.0	12.0	8.00
STA 108+98	6.0	12.0	8.00
STA 109+50	6.0	12.0	8.00
STA 110+50	6.0	12.0	8.00
STA 111+50	6.0	12.0	8.00
STA 112+50	6.0	12.0	8.00
STA 115+50	6.0	12.0	8.00
STA 116+50	6.0	12.0	8.00
STA 117+50	6.0	12.0	8.00
STA 119+50	6.0	12.0	8.00
STA 120+50	6.0	12.0	8.00
STA 121+50	6.0	12.0	8.00
STA 23+25	6.0	12.0	8.00
STA 32+10	6.0	12.0	8.00
STA 38+89	6.0	12.0	8.00
TOTALS			234.00

CLASS B PATCHING SCHEDULE (EXIT 41- FAU 8136 STAUNTON ROAD)

LOCATION	LENGTH (FOOT)	WIDTH (FOOT)	CL B 10" TY I (SQ YD)	CL B 10" TY II (SQ YD)	SAW CUTS (FOOT)	DOWEL BARS (EACH)	TIE BARS (EACH)
FAU 8136 (STAUNTON ROAD) INTERCHANGE							
RAMP A							
STA 13+00	6.0	9.0		6.00	39.0	16	3
STA 13+00	6.0	7.0	4.67		33.0	12	
STA 21+00	6.0	9.0		6.00	39.0	16	3
STA 21+00	6.0	7.0	4.67		33.0	12	
STA 21+25	6.0	9.0		6.00	39.0	16	3
STA 21+25	6.0	7.0	4.67		33.0	12	
STA 21+25	6.0	9.0		6.00	39.0	16	3
STA 21+25	6.0	7.0	4.67		33.0	12	
RAMP B							
STA 0+50	10.0	9.0		10.00	47.0	16	5
STA 0+50	10.0	7.0		7.78	41.0	12	
STA 7+60	6.0	9.0		6.00	39.0	16	3
STA 7+60	6.0	7.0	4.67		33.0	12	
STA 8+60	6.0	9.0		6.00	39.0	16	3
STA 8+60	6.0	7.0	4.67		33.0	12	
STA 9+75	6.0	9.0		6.00	39.0	16	3
STA 9+75	6.0	7.0	4.67		33.0	12	
STA 13+50	6.0	9.0		6.00	39.0	16	3
STA 13+50	6.0	7.0	4.67		33.0	12	
STA 16+40	10.0	9.0		10.00	47.0	16	5
STA 16+40	10.0	7.0		7.78	41.0	12	
RAMP C							
STA 6+50	8.0	9.0		8.00	43.0	12	4
STA 6+50	8.0	7.0		6.22	37.0	16	
STA 8+25	8.0	9.0		8.00	43.0	12	4
STA 8+25	8.0	7.0		6.22	37.0	16	
STA 12+00	8.0	9.0		8.00	43.0	12	4
STA 12+00	8.0	7.0		6.22	37.0	16	
RAMP D							
STA 14+00	6.0	9.0		6.00	39.0	12	3
STA 14+00	6.0	7.0	4.67		33.0	16	
STA 16+00	6.0	9.0		6.00	39.0	12	3
STA 16+00	6.0	7.0	4.67		33.0	16	
TOTALS			46.70	138.22	1136.0	420	52

PATCHING SUMMARY

LOCATION	CL A 12" TY II (SQ YD)	CL A 14" TY II (SQ YD)	CL A 14" TY III (SQ YD)	CL B 10" TY I (SQ YD)	CL B 10" TY II (SQ YD)	PAVT PATCH 11" TY II (SQ YD)	PATCH REINF (SQ YD)	SAW CUTS (FOOT)	DOWEL BARS (EACH)	TIE BARS (EACH)
I-55 MAINLINE	45.33	98.67	16.00				160.00	1,056.0		0
FAU 8136 INTERCHANGE RAMP				46.70	138.22			1,136.0	420	52
FAU 8136 (STAUNTON ROAD)						234.00				
GRAND TOTAL	45	99	16	47	138	234	160	2,192	420	52

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
es:\pwork\pwork\COXTE\dms25312\0672480-schedule.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = Mar-23-2010 03:21:34PM	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	22
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72A60	

URETHANE PAVEMENT MARKING LINE 5'

LOCATION		COLOR	LENGTH (FOOT)
I-55 NORTHBOUND			
STA 698+00 TO STA 790+46	OUTSIDE	WHITE	9,246
STA 801+84 TO STA 809+87	OUTSIDE	WHITE	803
STA 161+08 TO STA 179+47	OUTSIDE	WHITE	1,839
STA 184+96 TO STA 250+00	OUTSIDE	WHITE	6,504
STA 698+00 TO STA 809+87	MEDIAN	YELLOW	11,187
STA 161+08 TO STA 250+00	MEDIAN	YELLOW	8,892
I-55 SOUTHBOUND			
STA 698+00 TO STA 793+74	OUTSIDE	WHITE	9,574
STA 799+76 TO STA 809+87	OUTSIDE	WHITE	1,011
STA 161+08 TO STA 177+53	OUTSIDE	WHITE	1,645
STA 188+87 TO STA 250+00	OUTSIDE	WHITE	6,113
STA 698+00 TO STA 809+87	MEDIAN	YELLOW	11,187
STA 161+08 TO STA 250+00	MEDIAN	YELLOW	8,892
FAU 8136 (STAUNTON ROAD) RAMP A			
STA 0+00 TO STA 20+05	RT	WHITE	2,005
STA 20+05 TO STA 21+82	RT	WHITE	218
STA 5+95 TO STA 20+69	LT	YELLOW	1,474
STA 20+69 TO STA 21+82	LT	YELLOW	88
FAU 8136 (STAUNTON ROAD) RAMP B			
STA 0+23 TO STA 1+29	RT	WHITE	133
STA 1+29 TO STA 26+69	RT	WHITE	2,540
STA 0+23 TO STA 1+73	LT	YELLOW	160
STA 1+73 TO STA 15+38	LT	YELLOW	1,365
FAU 8136 (STAUNTON ROAD) RAMP C			
STA 0+00 TO STA 21+63	RT	WHITE	2,163
STA 21+63 TO STA 22+58	RT	WHITE	185
STA 5+49 TO STA 22+02	LT	YELLOW	1,653
STA 22+02 TO STA 22+58	LT	YELLOW	64
FAU 8136 (STAUNTON ROAD) RAMP D			
STA 0+23 TO STA 1+24	RT	WHITE	137
STA 1+24 TO STA 26+66	RT	WHITE	2,542
STA 0+23 TO STA 1+75	LT	YELLOW	161
STA 1+75 TO STA 15+15	LT	YELLOW	1,340
FAU 8136 (STAUNTON ROAD) EASTBOUND			
STA 105+66 TO STA 122+25	RT	WHITE	1,659
STA 21+10 TO STA 21+55	RT	WHITE	45
STA 22+27 TO STA 34+50	RT	WHITE	1,223
STA 34+50 TO STA 35+81	SIDE RD	WHITE	188
STA 35+81 TO STA 41+27	RT	WHITE	546
STA 42+38 TO STA 57+73	RT	WHITE	1,535
STA 59+47 TO STA 65+27	RT	WHITE	580
STA 65+27 TO STA 66+71	SIDE RD	WHITE	199
STA 66+71 TO STA 76+85	RT	WHITE	1,014
FAU 8136 (STAUNTON ROAD) WESTBOUND			
STA 105+66 TO STA 122+25	LT	WHITE	1,659
STA 21+10 TO STA 21+21	LT	WHITE	11
STA 22+40 TO STA 40+83	LT	WHITE	1,843
STA 42+36 TO STA 57+60	LT	WHITE	1,524
STA 58+81 TO STA 64+57	LT	WHITE	576
STA 64+57 TO STA 67+15	SIDE RD	WHITE	413
STA 67+15 TO STA 76+85	LT	WHITE	970
FAU 8136 (STAUNTON ROAD) CENTERLINE			
STA 105+66 TO STA 115+08	CL	YELLOW	1,178
STA 115+08 TO STA 122+25	CL	YELLOW	1,434
STA 21+10 TO STA 27+60	CL	YELLOW	1,300
STA 27+60 TO STA 30+87	CL	YELLOW	1,308
STA 30+87 TO STA 31+73	CL	YELLOW	258
STA 31+73 TO STA 34+80	CL	YELLOW	614
STA 35+40 TO STA 42+08	CL	YELLOW	1,336
STA 42+98 TO STA 57+06	CL	YELLOW	2,816
STA 57+97 TO STA 65+69	CL	YELLOW	1,544
STA 66+30 TO STA 68+88	CL	YELLOW	516
STA 68+88 TO STA 70+42	CL	YELLOW	462
STA 70+42 TO STA 73+04	CL	YELLOW	1,048
STA 73+04 TO STA 76+85	CL	YELLOW	572
TOTAL			121,491

PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5'

LOCATION	LENGTH (FEET)
I-55 MAINLINE	
NORTHBOUND	
STA 698+00 TO STA 809+87	2,800
STA 161+08 TO STA 250+00	2,230
SOUTHBOUND	
STA 698+00 TO STA 809+87	2,800
STA 161+08 TO STA 250+00	2,230
TOTAL	10,060

PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24'

LOCATION	LENGTH (FEET)
FAU 8136 (STAUNTON ROAD) RAMP A	
STA 21+35 LT STOP BAR	18
STA 21+50 RT STOP BAR	18
RAMP C	
STA 22+40 LT STOP BAR	18
STA 22+45 RT STOP BAR	18
WEST FRONTAGE ROAD	
STA 150+36 LT STOP BAR	22
EAST FRONTAGE ROAD	
STA 172+90 RT STOP BAR	18
STA 172+95 RT STOP BAR	13
STA 157+45 LT STOP BAR	24
TOTAL	149

PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS AND SYMBOLS

LOCATION	QUANTITY (SQ FT)
FAU 8136 (STAUNTON ROAD)	
EASTBOUND	
STA 54+37 LEFT TURN ARROW	15.6
STA 55+17 LEFT TURN ARROW	15.6
STA 55+92 LEFT TURN ARROW	15.6
STA 56+77 LEFT TURN ARROW	15.6
WESTBOUND	
STA 43+33 LEFT TURN ARROW	15.6
STA 44+08 LEFT TURN ARROW	15.6
STA 44+93 LEFT TURN ARROW	15.6
STA 45+80 LEFT TURN ARROW	15.6
TOTAL	124.8

URETHANE PAVEMENT MARKING LINE 8''

LOCATION	LENGTH (FOOT)
FAU 8136 (STAUNTON ROAD) RAMP A	
STA 21+40 ISLAND	181
RAMP C	
STA 22+40 ISLAND	185
FAU 8136 (STAUNTON ROAD)	
STA 65+75 ISLAND (AT E FRONTAGE RD)	173
TOTAL	539

PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8''

LOCATION	LENGTH (FEET)
FAU 8136 (STAUNTON ROAD) RAMP A	
STA 3+07 TO STA 5+95 (RAMP STATIONING) GORE AREA	288
STA 796+85 TO STA 799+76 (I-55 STATIONING) GORE AREA	291
STA 19+55 (WRONG WAY ARROW)	36.5
STA 20+52 (WRONG WAY ARROW)	36.5
RAMP B	
STA 15+38 TO STA 19+46 (RAMP STATIONING) GORE AREA	408
STA 177+53 TO STA 181+63 (I-55 STATIONING) GORE AREA	410
RAMP C	
STA 3+03 TO STA 5+49 (RAMP STATIONING) GORE AREA	246
STA 179+47 TO STA 181+93 (I-55 STATIONING) GORE AREA	246
STA 20+63 (WRONG WAY ARROW)	36.5
STA 21+83 (WRONG WAY ARROW)	36.5
RAMP D	
STA 15+15 TO STA 18+24 (RAMP STATIONING) GORE AREA	309
STA 798+78 TO STA 801+84 (I-55 STATIONING) GORE AREA	306
TOTAL	2,650

PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 6''

LOCATION	LENGTH (FOOT)
FAU 8136 (STAUNTON ROAD)	
EASTBOUND	
STA 54+35 TO STA 57+06 LEFT TURN LANE	271
WESTBOUND	
STA 42+98 TO STA 45+91 LEFT TURN LANE	293
RAMP A	
STA 793+73 TO STA 796+85 I-55 STATIONING	78
RAMP B	
STA 181+63 TO STA 188+87 I-55 STATIONING	181
RAMP C	
STA 181+93 TO STA 184+96 I-55 STATIONING	76
RAMP D	
STA 790+46 TO STA 798+78 I-55 STATIONING	208
TOTAL	1,107

PREFORMED PAVEMENT MARKING LINE 12''

LOCATION	LENGTH (FOOT)
FAU 8136 (STAUNTON ROAD) RAMP A	
STA 796+85 TO STA 799+76 (I-55 STATIONING) GORE AREA	83
RAMP C	
STA 179+47 TO STA 181+93 (I-55 STATIONING) GORE AREA	105
TOTAL	188

URETHANE PAVEMENT MARKING LINE 12''

LOCATION	LENGTH (FOOT)
FAU 8136 (STAUNTON ROAD) RAMP A	
STA 21+40 ISLAND	212
RAMP C	
STA 22+40 ISLAND	225
FAU 8136 (STAUNTON ROAD)	
EASTBOUND	
STA 27+60 TO STA 31+73 MEDIAN	131
WESTBOUND	
STA 65+75 (E FRONTAGE RD) ISLAND	73
STA 68+88 TO STA 73+04 MEDIAN	112
TOTAL	753

SHORT TERM PAVEMENT MARKING

LOCATION	CENTERLINE WHITE (FOOT)	SHOULDER YELLOW (FOOT)	SHOULDER WHITE (FOOT)	TOTAL (FOOT)
I-55 MAINLINE				
NORTHBOUND				
STA 698+00 TO STA 790+46	924.6	739.6	739.6	2,403.8
STA 790+46 TO STA 801+84	113.8	91.0		204.8
STA 801+84 TO STA 809+87	80.3	64.2	64.2	208.7
STA 161+08 TO STA 179+47	183.9	147.2	147.2	478.3
STA 179+47 TO STA 184+96	54.9	44.0		98.9
STA 184+96 TO STA 250+00	650.4	520.4	520.4	1,691.2
SOUTHBOUND				
STA 698+00 TO STA 793+74	957.4	766.0	766.0	2,489.4
STA 793+74 TO STA 799+76	60.2	48.2		108.4
STA 799+76 TO STA 809+87	101.1	80.8	80.8	262.7
STA 161+08 TO STA 177+53	164.5	131.6	131.6	427.7
STA 177+53 TO STA 188+87	113.4	90.8		204.2
STA 188+87 TO STA 250+00	611.3	489.0	489.0	1,589.3
FAU 8136 (STALANTON ROAD) RAMPS				
RAMP A				
STA 0+00 TO STA 3+07			24.6	24.6
STA 3+07 TO STA 21+82		150.0	150.0	300.0
RAMP B				
STA 0+23 TO STA 19+46		153.8	153.8	307.6
STA 19+46 TO STA 26+69			57.8	57.8
RAMP C				
STA 0+00 TO STA 3+03			24.2	24.2
STA 3+03 TO STA 22+58		156.4	156.4	312.8
RAMP D				
STA 0+23 TO STA 18+24		144.0	144.0	288.0
STA 18+24 TO STA 26+66			67.4	67.4
FAU 8136 (STALANTON ROAD)				
STA 105+66 TO STA 122+25	663.6		265.6	929.2
STA 21+10 TO STA 24+59	139.6		56.0	195.6
STA 24+59 TO STA 40+87	651.2		260.4	911.6
STA 40+87 TO STA 42+36	59.6			59.6
STA 42+36 TO STA 57+73	614.8		246.0	860.8
STA 57+73 TO STA 58+81	43.2			43.2
STA 58+81 TO STA 65+27	258.4		103.2	361.6
STA 65+27 TO STA 66+71	57.6			57.6
STA 66+71 TO STA 76+85	405.6		162.4	568.0
TOTALS	6,909.4	3,817.0	4,810.6	15,479

WORK ZONE PAVEMENT MARKING REMOVAL

LOCATION	CENTERLINE (SQ FT)	SHOULDER YELLOW (SQ FT)	SHOULDER WHITE (SQ FT)	TOTAL (SQ FT)
I-55 MAINLINE				
NORTHBOUND				
STA 698+00 TO STA 790+46		154.2	154.2	308.4
STA 790+46 TO STA 801+84		19.0	19.0	38.0
STA 801+84 TO STA 809+87		13.4	13.4	26.8
STA 161+08 TO STA 179+47		30.7	30.7	61.4
STA 179+47 TO STA 184+96		9.2	9.2	18.4
STA 184+96 TO STA 250+00		108.5	108.5	217.0
SOUTHBOUND				
STA 698+00 TO STA 793+74		159.7	159.7	319.4
STA 793+74 TO STA 799+76		10.0	10.0	20.0
STA 799+76 TO STA 809+87		16.9	16.9	33.8
STA 161+08 TO STA 177+53		27.4	27.4	54.8
STA 177+53 TO STA 188+87		18.9	18.9	37.8
STA 188+87 TO STA 250+00		102.0	102.0	204.0
FAU 8136 (STALANTON ROAD) RAMPS				
RAMP A				
STA 0+00 TO STA 3+07			5.1	5.1
STA 3+07 TO STA 21+82		31.3	31.3	62.6
RAMP B				
STA 0+23 TO STA 19+46		32.1	32.1	64.2
STA 19+46 TO STA 26+69			12.1	12.1
RAMP C				
STA 0+00 TO STA 3+03			5.1	5.1
STA 3+03 TO STA 22+58		32.6	32.6	65.2
RAMP D				
STA 0+23 TO STA 18+24		30.0	30.0	60.0
STA 18+24 TO STA 26+66			14.0	14.0
FAU 8136 (STALANTON ROAD)				
STA 105+66 TO STA 122+25	138.4		55.4	193.8
STA 21+10 TO STA 24+59	29.2		11.6	40.8
STA 24+59 TO STA 40+87	135.8		54.4	190.2
STA 40+87 TO STA 42+36	12.4			12.4
STA 42+36 TO STA 57+73	128.2		51.2	179.4
STA 57+73 TO STA 58+81	9.0			9.0
STA 58+81 TO STA 65+27	53.8		21.6	75.4
STA 65+27 TO STA 66+71	12.0			12.0
STA 66+71 TO STA 76+85	84.6		33.8	118.4
TOTALS	603.4	795.9	1060.2	2460

GROOVING FOR RECESSED PAVEMENT MARKING 6''

LOCATION	LENGTH (FOOT)
I-55 MAINLINE	
NORTHBOUND	
STA 698+00 TO STA 790+46	OUTSIDE 9,246
STA 801+84 TO STA 809+87	OUTSIDE 803
STA 161+08 TO STA 179+47	OUTSIDE 1,839
STA 184+96 TO STA 250+00	OUTSIDE 6,504
STA 698+00 TO STA 809+87	MEDIAN 11,187
STA 161+08 TO STA 250+00	MEDIAN 8,892
SOUTHBOUND	
STA 698+00 TO STA 793+74	OUTSIDE 9,574
STA 799+76 TO STA 809+87	OUTSIDE 1,011
STA 161+08 TO STA 177+53	OUTSIDE 1,645
STA 188+87 TO STA 250+00	OUTSIDE 6,113
STA 698+00 TO STA 809+87	MEDIAN 11,187
STA 161+08 TO STA 250+00	MEDIAN 8,892
TOTAL	76,893

EARTHWORK SCHEDULE

LOCATION	EARTH EXCAVATION (WIDENING) CU YDS	EXCAVATION ADJUSTED FOR SHRINKAGE (25%) CU YDS	EMBANKMENT CU YDS	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (-) = FURNISHED EX CU YDS
RAMP A				
STA 20+69 TO STA 21+50	16	12	6	6
RAMP B				
STA 0+23 TO STA 1+73	20	15	2	13
RAMP C				
STA 21+63 TO STA 22+59	17	13	3	10
RAMP D				
STA 0+25 TO STA 0+90	10	8	3	5
TOTALS	63	47	14	33

RAISED REFLECTIVE PAVEMENT MARKER

LOCATION	LANE LOCATION	COLOR	# OF REFL PER SPACING	TOTAL (EACH)
I-55 MAINLINE				
NORTHBOUND				
STA 698+00 TO STA 809+87	CENTERLINE	CRYSTAL	2 PER 80	280
STA 161+08 TO STA 250+00	CENTERLINE	CRYSTAL	2 PER 80	224
SOUTHBOUND				
STA 698+00 TO STA 809+87	CENTERLINE	CRYSTAL	2 PER 80	280
STA 161+08 TO STA 250+00	CENTERLINE	CRYSTAL	2 PER 80	224
FAU 8136 (STAUNTON ROAD) RAMPS				
RAMP A				
STA 0+00 TO STA 6+62	OUTSIDE EOP	CRYSTAL	1 PER 20	34
STA 6+62 TO STA 14+21	INSIDE EOP	AMBER	1 PER 40	19
STA 3+07 TO STA 5+95	GORE (RAMP)	CRYSTAL	1 PER 40	8
STA 796+85 TO STA 799+76	GORE (I-55)	CRYSTAL	1 PER 40	8
RAMP C				
STA 0+00 TO STA 6+62	OUTSIDE EOP	CRYSTAL	1 PER 20	34
STA 6+62 TO STA 13+44	INSIDE EOP	AMBER	1 PER 40	18
STA 3+03 TO STA 5+49	GORE (RAMP)	CRYSTAL	1 PER 40	7
STA 179+47 TO STA 181+93	GORE (I-55)	CRYSTAL	1 PER 40	7
FAU 8136 (STAUNTON ROAD)				
STA 105+66 TO STA 122+25	CENTERLINE	AMBER	1 PER 80	21
STA 21+10 TO STA 27+60	CENTERLINE	AMBER	1 PER 80	9
STA 27+60 TO STA 30+87	CL GORE	AMBER	1 PER 40	18
STA 27+60 TO STA 30+87	CL GORE	AMBER	1 PER 40	18
STA 70+42 TO STA 73+04	CL GORE	AMBER	1 PER 40	14
STA 70+42 TO STA 73+04	CL GORE	AMBER	1 PER 40	14
STA 73+04 TO STA 76+85	CENTERLINE	AMBER	1 PER 80	5

TOTAL

1,242

PRISMATIC CURB REFLECTOR

LOCATION	LANE LOCATION	COLOR	APPROX SPACING	# OF LINES	TOTAL (EACH)
FAU 8136 (STAUNTON ROAD) RAMPS					
RAMP A					
STA 21+40	ISLAND	CRYSTAL	10' / 2'		15
RAMP C					
STA 22+45	ISLAND	CRYSTAL	10' / 2'		15
FAU 8136 (STAUNTON ROAD)					
STA 30+88	MEDIAN	AMBER	+/- 2' SPACING	1	1
STA 30+88 TO STA 34+78	MEDIAN	AMBER	40'	2	20
STA 34+78	MEDIAN	AMBER	+/- 2' SPACING	1	1
STA 35+41	MEDIAN	AMBER	+/- 2' SPACING	1	1
STA 35+41 TO STA 42+08	MEDIAN	AMBER	40'	2	34
STA 42+08	MEDIAN	AMBER	+/- 2' SPACING	1	3
STA 42+98	MEDIAN	AMBER	+/- 2' SPACING	1	1
EB STA 42+98 TO STA 51+36	MEDIAN	AMBER	40'	1	21
WB STA 42+98 TO STA 48+67	MEDIAN	AMBER	20'	1	29
WB STA 48+67 TO STA 57+02	MEDIAN	AMBER	40'	1	21
EB STA 48+67 TO STA 57+02	MEDIAN	AMBER	20'	1	42
STA 57+02	MEDIAN	AMBER	+/- 2' SPACING	1	1
STA 58+01	MEDIAN	AMBER	+/- 2' SPACING	1	3
STA 58+01 TO STA 65+68	MEDIAN	AMBER	40'	2	40
STA 65+68	MEDIAN	AMBER	+/- 2' SPACING	1	1
STA 66+31	MEDIAN	AMBER	+/- 2' SPACING	1	1
STA 66+31 TO STA 70+40	MEDIAN	AMBER	40'	2	22
STA 70+40	MEDIAN	AMBER	+/- 2' SPACING	1	1

TOTAL

273

RAISED REFLECTIVE PAVEMENT MARKER REMOVAL

LOCATION	LANE LOCATION	COLOR	# OF REFL PER SPACING	TOTAL (EACH)
I-55 MAINLINE				
NORTHBOUND				
STA 698+00 TO STA 809+87	CENTERLINE	CRYSTAL	1 PER 80	140
STA 161+08 TO STA 250+00	CENTERLINE	CRYSTAL	1 PER 80	112
SOUTHBOUND				
STA 698+00 TO STA 809+87	CENTERLINE	CRYSTAL	1 PER 80	140
STA 161+08 TO STA 250+00	CENTERLINE	CRYSTAL	1 PER 80	112
FAU 8136 (STAUNTON ROAD) RAMPS				
RAMP A				
STA 0+00 TO STA 6+62	OUTSIDE EOP	CRYSTAL	1 PER 20	34
STA 6+62 TO STA 14+21	INSIDE EOP	AMBER	1 PER 40	19
STA 3+07 TO STA 5+95	GORE (RAMP)	CRYSTAL	1 PER 40	8
STA 796+85 TO STA 799+76	GORE (I-55)	CRYSTAL	1 PER 40	8
RAMP C				
STA 0+00 TO STA 6+62	OUTSIDE EOP	CRYSTAL	1 PER 20	34
STA 6+62 TO STA 13+44	INSIDE EOP	AMBER	1 PER 40	18
STA 3+03 TO STA 5+49	GORE (RAMP)	CRYSTAL	1 PER 40	7
STA 179+47 TO STA 181+93	GORE (I-55)	CRYSTAL	1 PER 40	7
FAU 8136 (STAUNTON ROAD)				
STA 105+66 TO STA 122+25	CENTERLINE	AMBER	1 PER 80	21
STA 20+80 TO STA 27+60	CENTERLINE	AMBER	1 PER 80	9
STA 27+60 TO STA 30+87	CL GORE	AMBER	1 PER 40	18
STA 70+42 TO STA 73+04	CL GORE	AMBER	1 PER 40	14
STA 73+04 TO STA 76+85	CENTERLINE	AMBER	1 PER 80	5

TOTAL

706

SIGNS

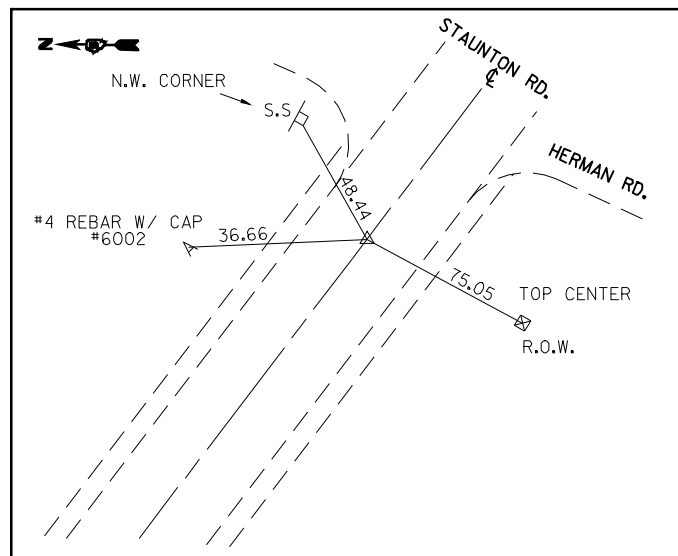
LOCATION	TYPE	RELOCATE SIGN PANEL ASSEMBLY - TYPE A EACH	SIGN PANEL - TYPE 1 SQ FT	TELESCOPING STEEL SIGN SUPPORT FOOT
FAU 8136 (STAUNTON INTERCHANGE RAMPS)				
RAMP A				
LT STA 21+22	STOP 36"x36"		7.46	15
STA 21+38	STOP	1		
RT STA 21+33	STOP	1		
LT STA 20+70	DO NOT ENTER	1		
RAMP C				
LT STA 22+37	STOP 36"x36"		7.46	15
STA 22+44	STOP	1		
RT STA 22+30	STOP	1		
RT STA 20+85	DO NOT ENTER	1		

TOTAL

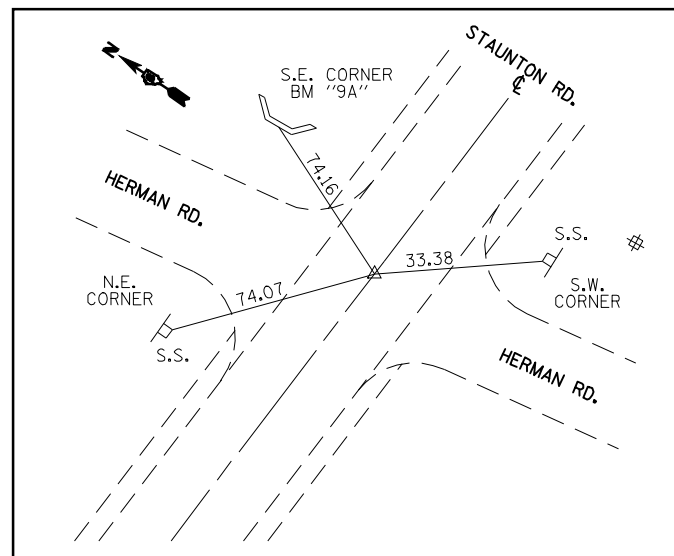
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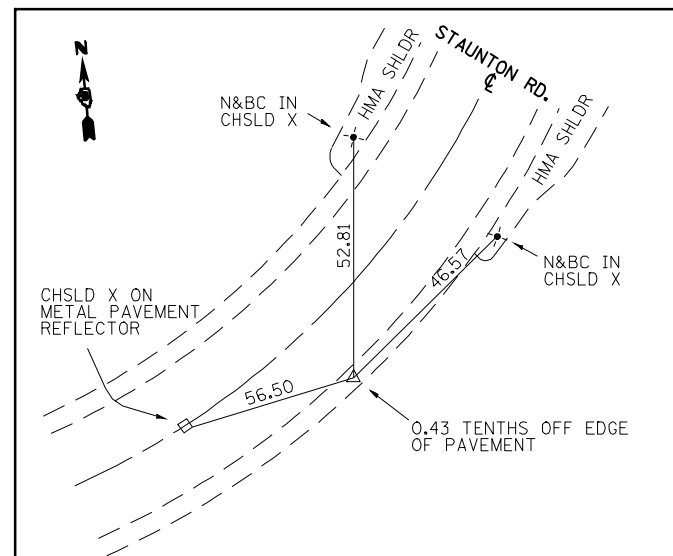
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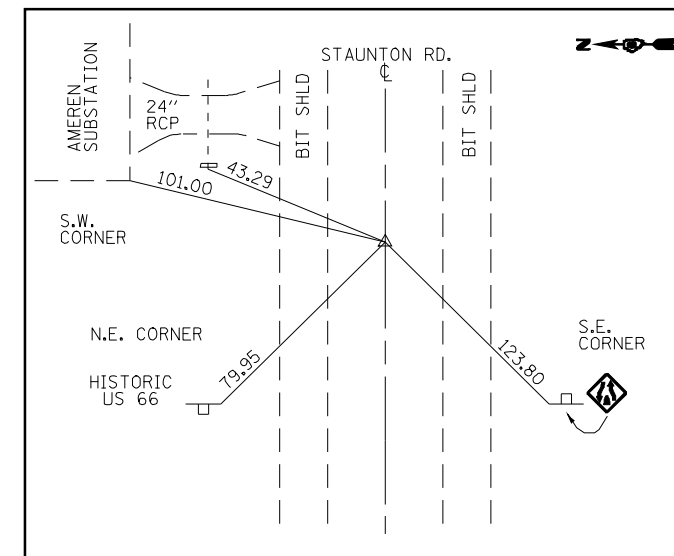
STA EQ 122+25 A109 BK = 21+09.83 A111 AH
(PK NAIL IN CHSLD "X")



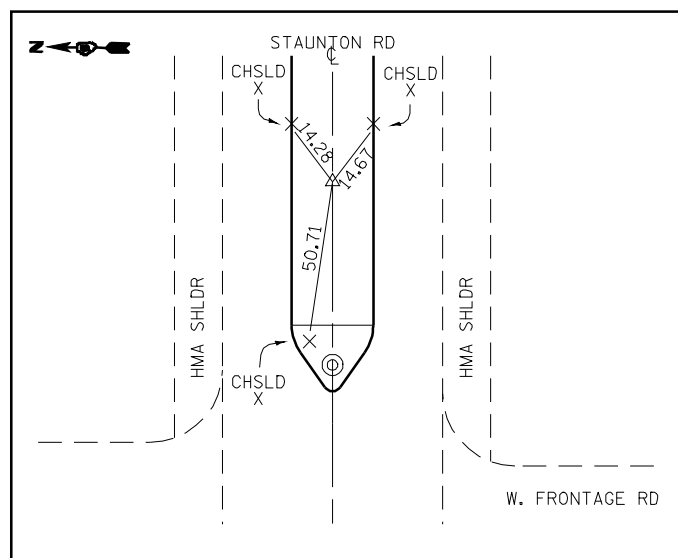
PC STA. 22+12.791
(PK NAIL IN CHSLD "X")



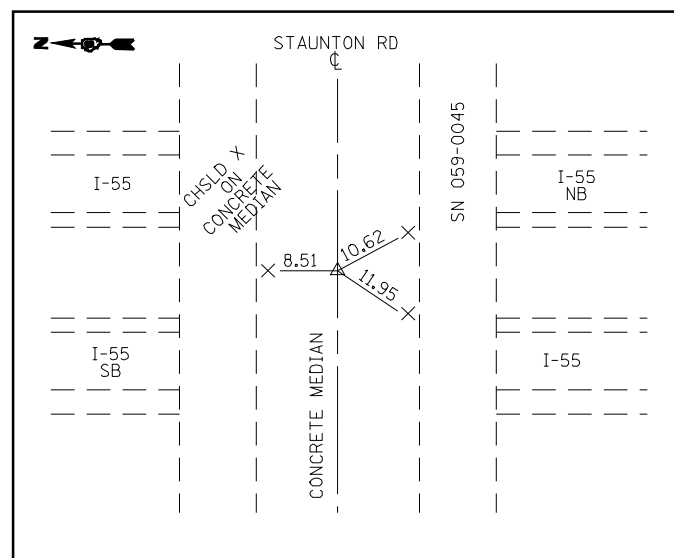
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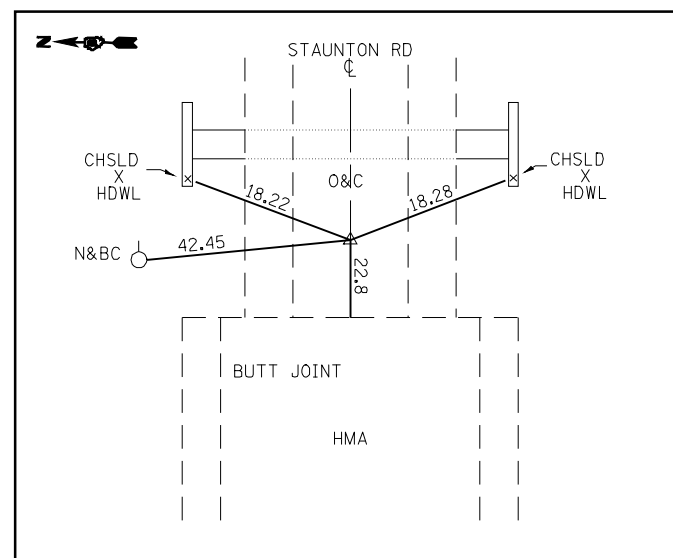
PT STA. 26+15.683
(PK NAIL IN CHSLD "X")



POT STA. 36+07.43
(60 D NAIL FLUSH)



50+00 = 164+89.336
(STAR DRILL)



POT 76+70.034
(REBAR BURIED 10")
(60D SURFACE MARK)

- BM *10 : SET CHSLD "□" ON S CURB OF CONC MEDIAN @ ROUND INLET OPPOSITE GATEWAY CYLINDER
TECH ± 400 W OF W FRONTAGE RD
STA. 30+97.6; 2.6 LT
NAVD '88 = 640.241
- BM *11 : SET CHSLD "□" ON S CURB OF CONC MEDIAN 22 FT W OF END MEDIAN @ SBL I-55 ON & OFF RAMP
STA. 41+77.6; 7.6 RT
NAVD '88 = 651.834
- BM *11A : SET CHSLD "□" TOP CONC H.R. @ NW BRIDGE OVER I-55
SN 059-0045
STA. 48+76.1; 34.6 LT
NAVD '88 = 665.100
- BM *12 : RR SPIKE @ CONC MEDIAN @ EAST BRIDGE DECK, 1.3 W OF CONC / GRASS LINE (ALSO SEE X-TIE FOR #6004)
STA. 51+23.5; 1.1 LT
NAVD '88 = 662.681
- BM *12A : SET CHSLD "□" ON N SIDE L.P. FOUNDATION SE QUAD NBL ON & OFF RAMP
STA. 57+49.3; 49.2 RT
NAVD '88 = 651.110
- BM *13 : SET CHSLD "□" ON S CURB OF CONC MEDIAN, UNDER POWERLINE ± 370 FT E OF I-55 NBL EXIT RAMP
STA. 61+63.7; 6.41 RT
NAVD '88 = 645.347

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

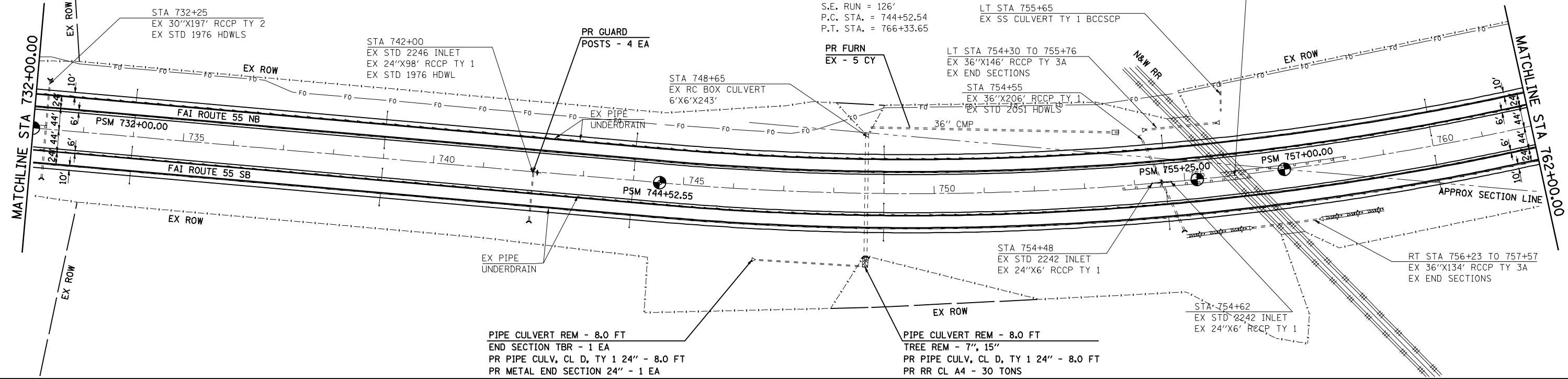
CROSS TIES AND BENCHMARKS	
SCALE:	SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	26
CONTRACT NO. 72A60				
ILLINOIS FED. AID PROJECT				

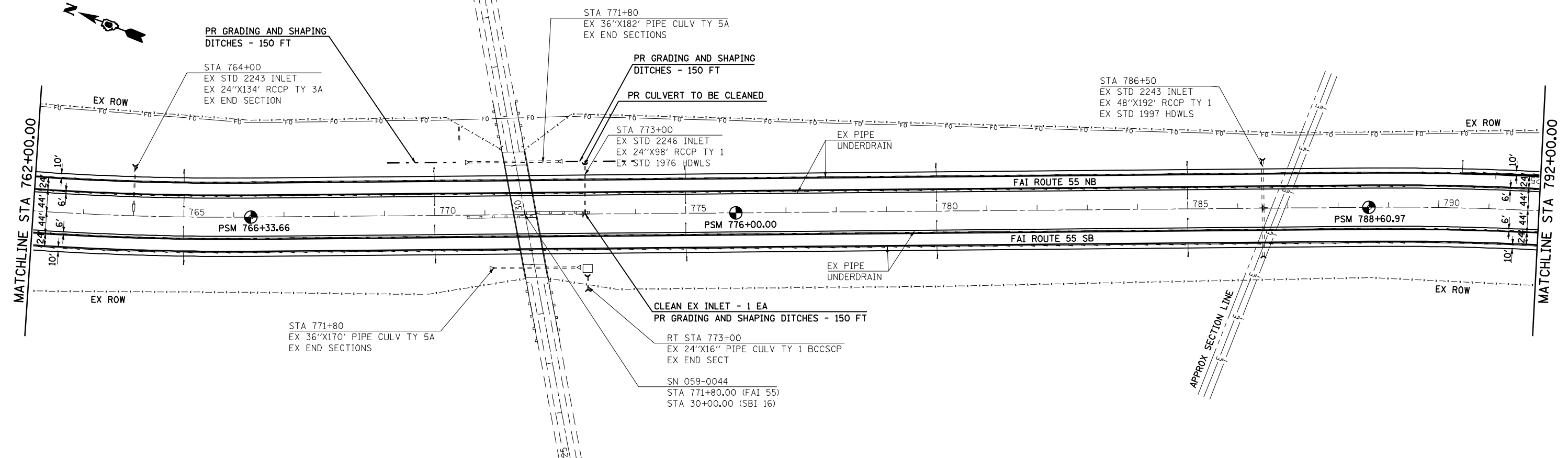
SEC 22, T7N, R6W, 3RD PM

EXIST. CURVE I55N-5
 PI STA. = 755+56.46
 $\Delta = 21^\circ 48' 40''$ (LT)
 $D = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 1,103.92'$
 $L = 2,181.11'$
 $E = 105.38'$
 $e = 2.8\%$
 $T.R. = 46'$
 $S.E. RUN = 126'$
 $P.C. STA. = 744+52.54$
 $P.T. STA. = 766+33.65$

SN 059-0043
 STA 755+89.61 (FAI 55)
 STA 50+00.00 (N&W RR)



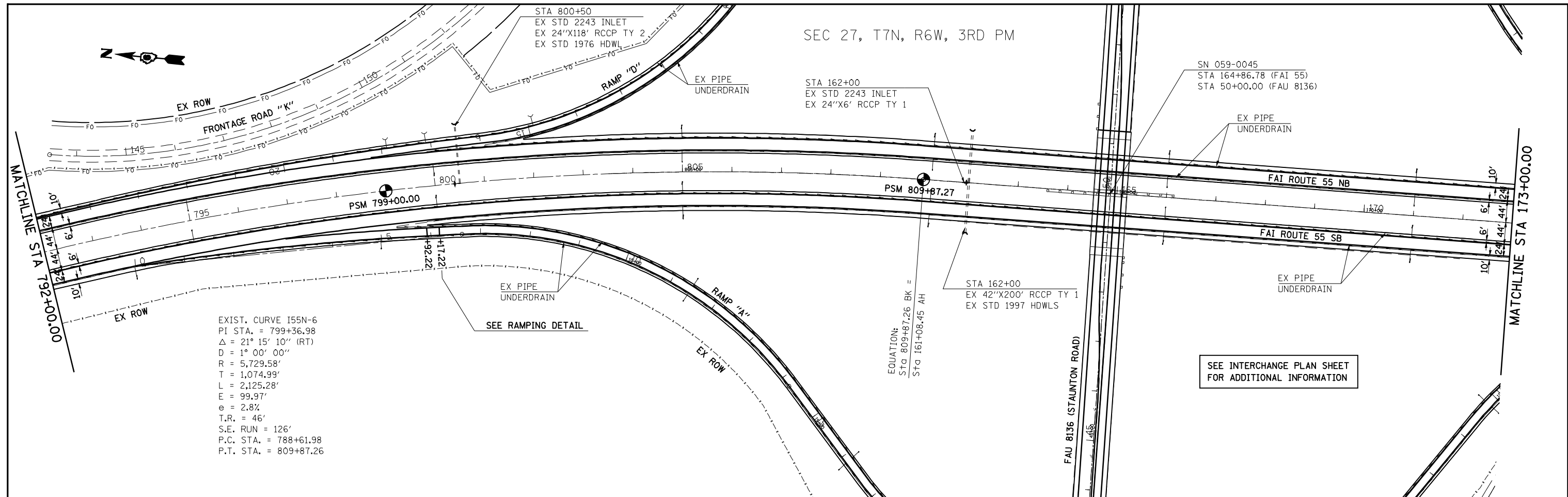
SEC 22, T7N, R6W, 3RD PM



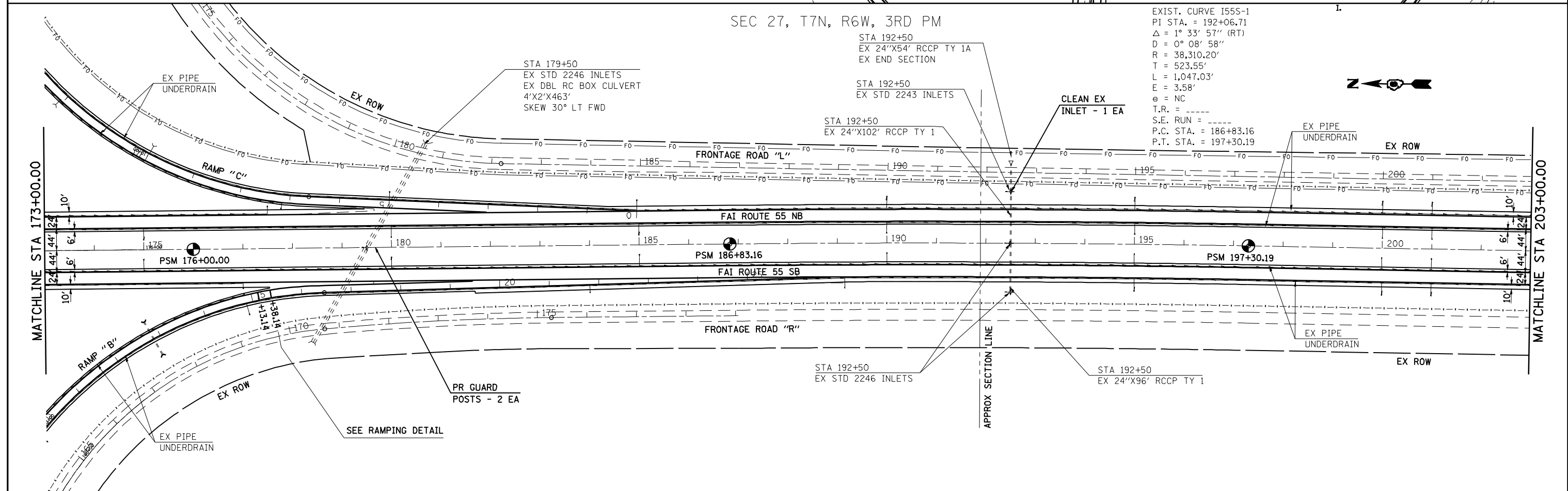
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEETS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwwork\pwwid00\LAUGHLINRL\dms25312\0672460-sht-plan100.dgn		DRAWN -	REVISED -		55	59RS-2, BR	MACOUPIN	100	28			
PLOT SCALE = 200.0000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 72A60							
PLOT DATE = Mar-26-2010 11:08:53AM		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

SCALE: 1" = 100' SHEET NO. OF SHEETS STA. 732+00.00 TO STA. 792+00.00

SEC 27, T7N, R6W, 3RD PM



SEC 27, T7N, R6W, 3RD PM



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwwork\p\WD00\LAUGHLINRL\dms25312\0672460-ah-1-plan100.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PLAN SHEETS			
SCALE: 1" = 100'	SHEET NO.	OF SHEETS	STA. 792+00.00 TO STA. 203+00.00

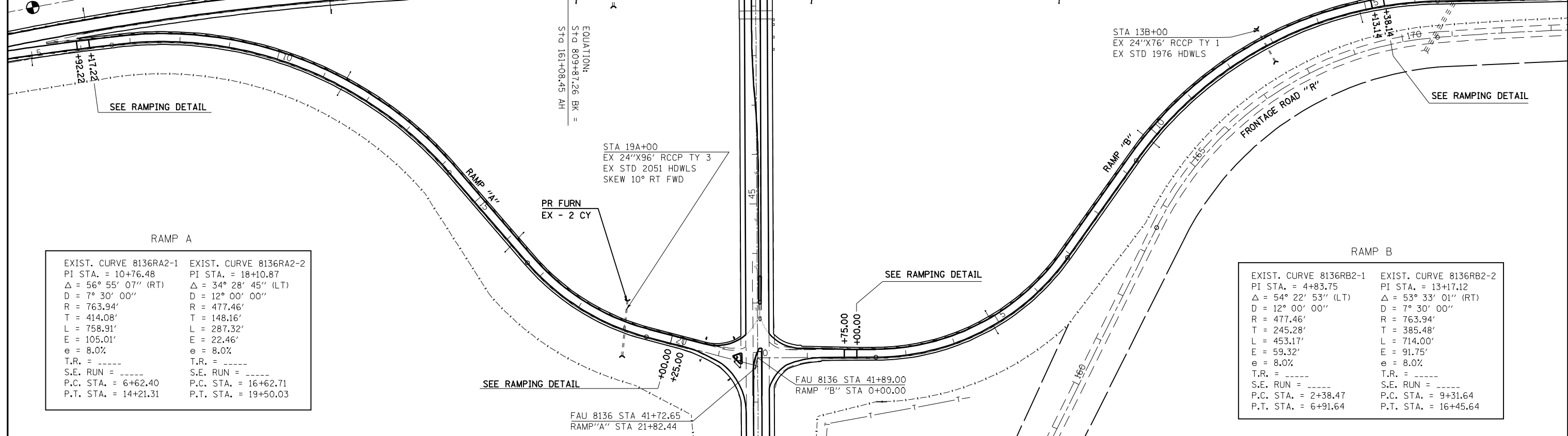
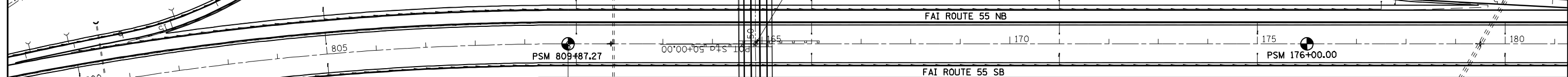
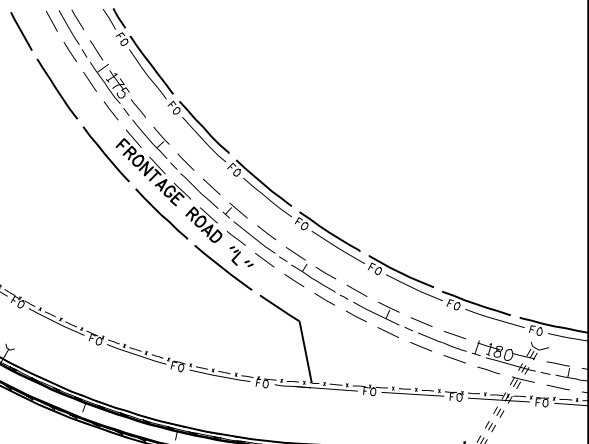
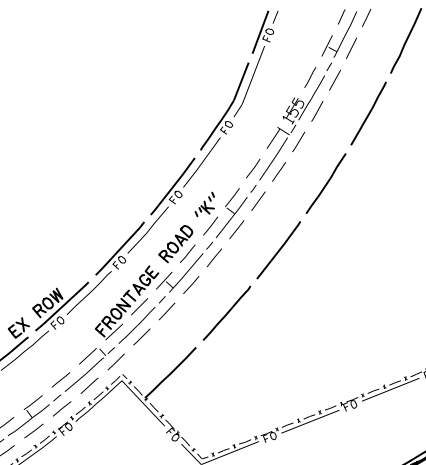
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	29
CONTRACT NO. 72A60				
ILLINOIS FED. AID PROJECT				

RAMP D

EXIST. CURVE 8136RD-1 PI STA. = 5+03.88 Δ = 58° 17' 58" (LT) D = 12° 00' 00" R = 477.46' T = 266.29' L = 485.82' E = 69.24' e = 8.0% T.R. = ---- S.E. RUN = ---- P.C. STA. = 2+37.59 P.T. STA. = 7+23.41	EXIST. CURVE 8136RD-2 PI STA. = 13+05.44 Δ = 47° 44' 27" (RT) D = 7° 30' 00" R = 763.94' T = 338.06' L = 636.54' E = 71.46' e = 8.0% T.R. = ---- S.E. RUN = ---- P.C. STA. = 9+67.38 P.T. STA. = 16+03.93	EXIST. CURVE 8136RD-3 PI STA. = 21+36.30 Δ = 8° 57' 00" (LT) D = 0° 50' 32" R = 6,802.38' T = 532.37' L = 1,062.57' E = 20.80' e = ---- T.R. = ---- S.E. RUN = ---- P.C. STA. = 16+03.93 P.T. STA. = 26+66.50
--	---	---

RAMP C

EXIST. CURVE C1 PI STA. = 10+28.02 Δ = 51° 09' 04" (RT) D = 7° 30' 00" R = 763.94' T = 365.62' L = 682.01' E = 82.98' e = 8.0% T.R. = ---- S.E. RUN = ---- P.C. STA. = 6+62.40 P.T. STA. = 13+44.41	EXIST. CURVE C2 PI STA. = 18+30.28 Δ = 54° 29' 30" (LT) D = 12° 00' 00" R = 477.46' T = 245.86' L = 454.09' E = 59.58' e = 8.0% T.R. = ---- S.E. RUN = ---- P.C. STA. = 15+84.41 P.T. STA. = 20+38.50
---	---



RAMP A

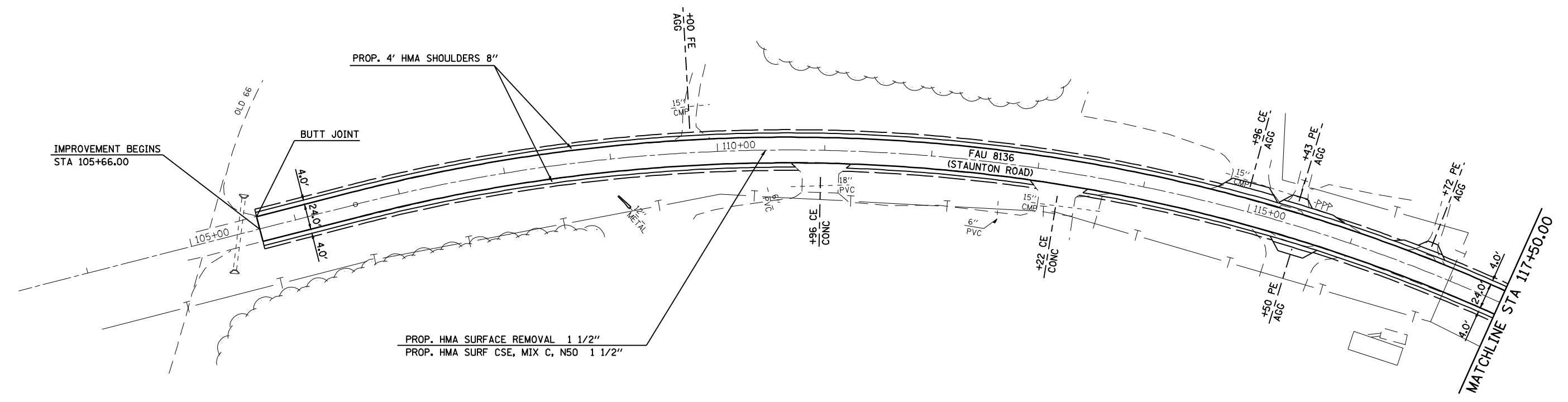
EXIST. CURVE 8136RA2-1 PI STA. = 10+76.48 Δ = 56° 55' 07" (RT) D = 7° 30' 00" R = 763.94' T = 414.08' L = 758.91' E = 105.01' e = 8.0% T.R. = ---- S.E. RUN = ---- P.C. STA. = 6+62.40 P.T. STA. = 14+21.31	EXIST. CURVE 8136RA2-2 PI STA. = 18+10.87 Δ = 34° 28' 45" (LT) D = 12° 00' 00" R = 477.46' T = 148.16' L = 287.32' E = 22.46' e = 8.0% T.R. = ---- S.E. RUN = ---- P.C. STA. = 16+62.71 P.T. STA. = 19+50.03
---	--

RAMP B

EXIST. CURVE 8136RB2-1 PI STA. = 4+83.75 Δ = 54° 22' 53" (LT) D = 12° 00' 00" R = 477.46' T = 245.28' L = 453.17' E = 59.32' e = 8.0% T.R. = ---- S.E. RUN = ---- P.C. STA. = 2+38.47 P.T. STA. = 6+91.64	EXIST. CURVE 8136RB2-2 PI STA. = 13+17.12 Δ = 53° 33' 01" (RT) D = 7° 30' 00" R = 763.94' T = 385.48' L = 714.00' E = 91.75' e = 8.0% T.R. = ---- S.E. RUN = ---- P.C. STA. = 9+31.64 P.T. STA. = 16+45.64
---	--

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN SHEETS			F.A.I. RTE. = 55	SECTION = 59RS-2, BR	COUNTY = MACOUPIN	TOTAL SHEETS = 100	SHEET NO. = 31
es:\pwwork\PIWIDOT\LAUGHLINRL\dms25312\0672460-sht-plan100.dgn	PLOT SCALE = 200.0000' / IN.	DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 72A60		
PLOT DATE = Mar-26-2010 11:04:41AM	DATE -	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

EXIST. CURVE C998
 PI STA. = 113+13.64
 $\Delta = 43^\circ 59' 05''$ (RT)
 $D = 3^\circ 31' 43''$
 $R = 1,623.81'$
 $T = 655.81'$
 $L = 1,246.56'$
 $E = 127.43'$
 $e = 7.0\%$
 $T.R. = 38'$
 $S.E. RUN = 179'$
 $P.C. STA. = 106+57.83$
 $P.T. STA. = 119+04.40$

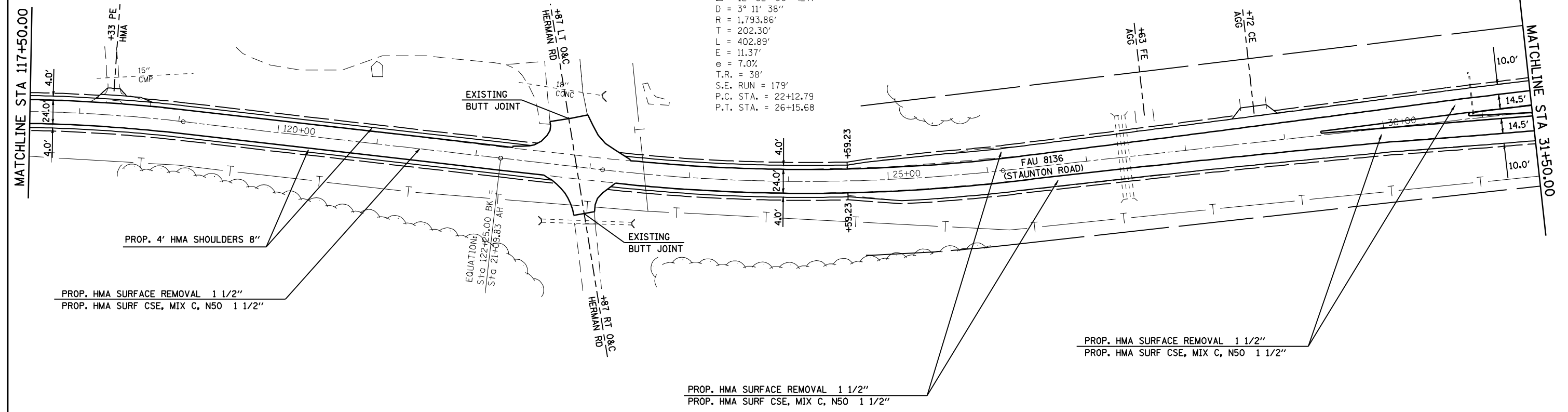


SEE ENTRANCE DETAILS
FOR MORE INFORMATION

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 8136 (STAUNTON ROAD) PLAN SHEETS			F.A.I. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ea:\pwwork\PWIDOT\LAUGHLINRL\dms25312\0672460-sht-plan-TR481.dgn		DRAWN -	REVISED -		55	59RS-2, BR	MACOUPIN	100	32			
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT				
PLOT DATE = Mar-26-2010 11:17:12AM		DATE -	REVISED -		SCALE: 1" = 50'	SHEET NO. OF SHEETS	STA. TO STA. 117+50.00					



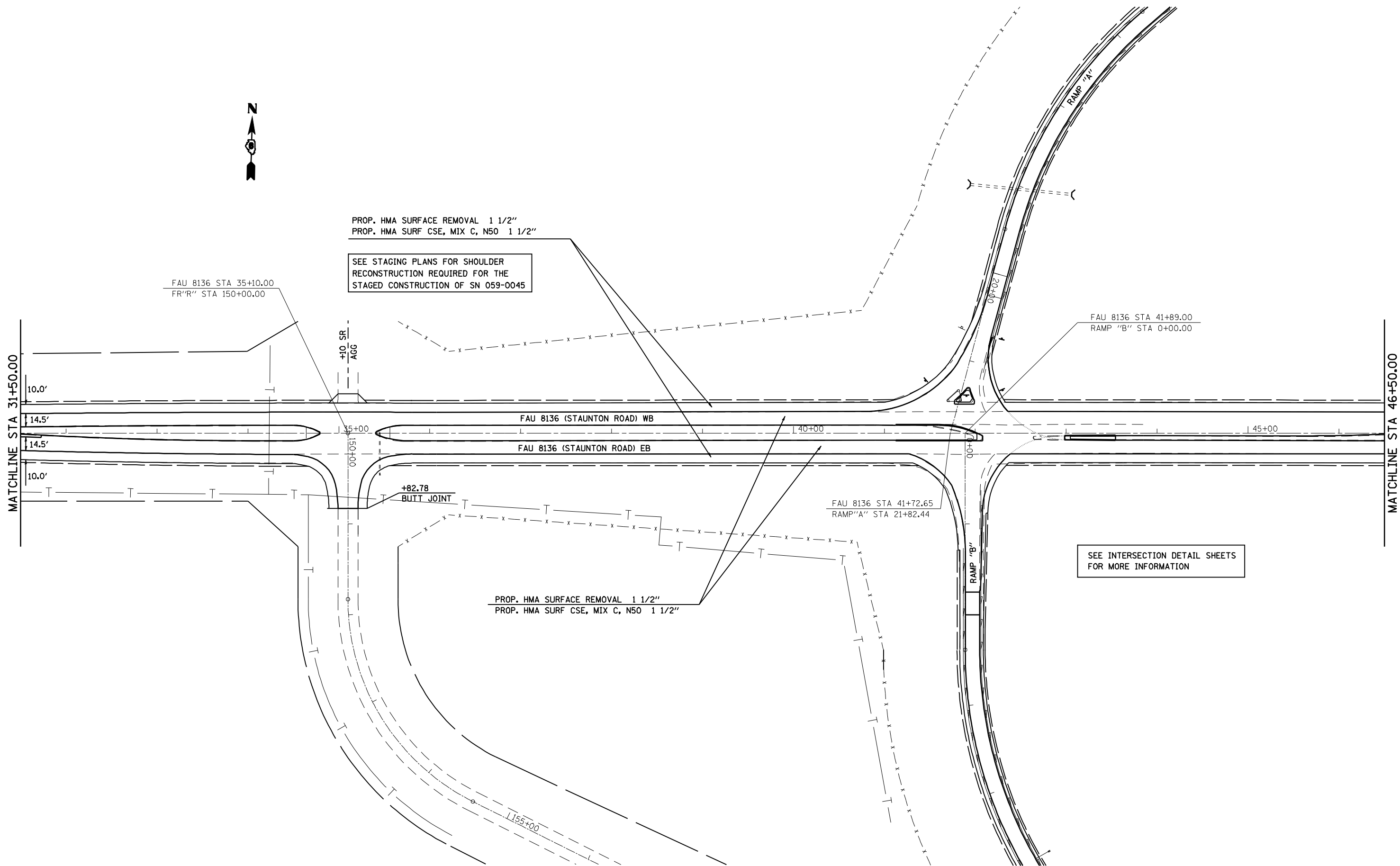
EXIST. CURVE DPICALC1
 PI STA. = 24+15.09
 $\Delta = 12^\circ 52' 06''$ (LT)
 $D = 3^\circ 11' 38''$
 $R = 1,793.86'$
 $T = 202.30'$
 $L = 402.89'$
 $E = 11.37'$
 $e = 7.0\%$
 $T.R. = 38'$
 $S.E. RUN = 179'$
 $P.C. STA. = 22+12.79$
 $P.T. STA. = 26+15.68$



SEE STAGING PLANS FOR SHOULDER
 RECONSTRUCTION REQUIRED FOR THE
 STAGED CONSTRUCTION OF SN 059-0045

SEE ENTRANCE DETAILS
 FOR MORE INFORMATION

FILE NAME = e:\pwwork\pwwid\LAUGHLINRL\dms25312\0672460-aht-plan-TR481.dgn	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 8136 (STAUNTON ROAD) PLAN SHEETS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -					55	59RS-2, BR	MACOUPIN	100	33
PLOT DATE = Mar-26-2010 11:17:15AM	DATE -	REVISED -	REVISED -	SCALE: 1" = 50'			SHEET NO.	OF	SHEETS	STA. 117+50.00 TO STA. 31+50.00	CONTRACT NO. 72A60	
ILLINOIS FED. AID PROJECT												



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwork\pwidot\LAUGHLINRL\dms25312\0672460-sht-plan-TR481.dgn		DRAWN -	REVISED -
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	PLOT DATE = Mar-26-2010 11:17:17AM	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAU 8136 (STAUNTON ROAD)
PLAN SHEETS

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. 31+50.00 TO STA. 46+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	34
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	

EQUATION:
Sta 809+87.26 BK =
Sta 161+08.45 AH



SN 059-0045
STA 164+86.78 (FAI 55)
STA 50+00.00 (FAU 8136)

PROP. HMA SURFACE REMOVAL 1 1/2"
PROP. HMA SURF CSE, MIX C, N50 1 1/2"

SEE STAGING PLANS FOR SHOULDER
RECONSTRUCTION REQUIRED FOR THE
STAGED CONSTRUCTION OF SN 059-0045

FAU 8136 STA 58+11.00
RAMP "C" STA 22+81.16
RAMP "D" STA 0+00.00

4"
CMP

FAU 8136 (STAUNTON ROAD) WB
60+00

FAU 8136 (STAUNTON ROAD) EB

PROP. HMA SURFACE REMOVAL 1 1/2"
PROP. HMA SURF CSE, MIX C, N50 1 1/2"

SEE INTERSECTION DETAIL SHEETS
FOR MORE INFORMATION

MATCHLINE STA 46+50.00

MATCHLINE STA 61+50.00

I-55 SB

I-55 NB

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwork\pwidot\LAUGHLINRL\dms25312\0672460-aht-plan-TR481.dgn		DRAWN -	REVISED -
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	PLOT DATE = Mar-26-2010 11:17:19AM	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAU 8136 (STAUNTON ROAD)
PLAN SHEETS**

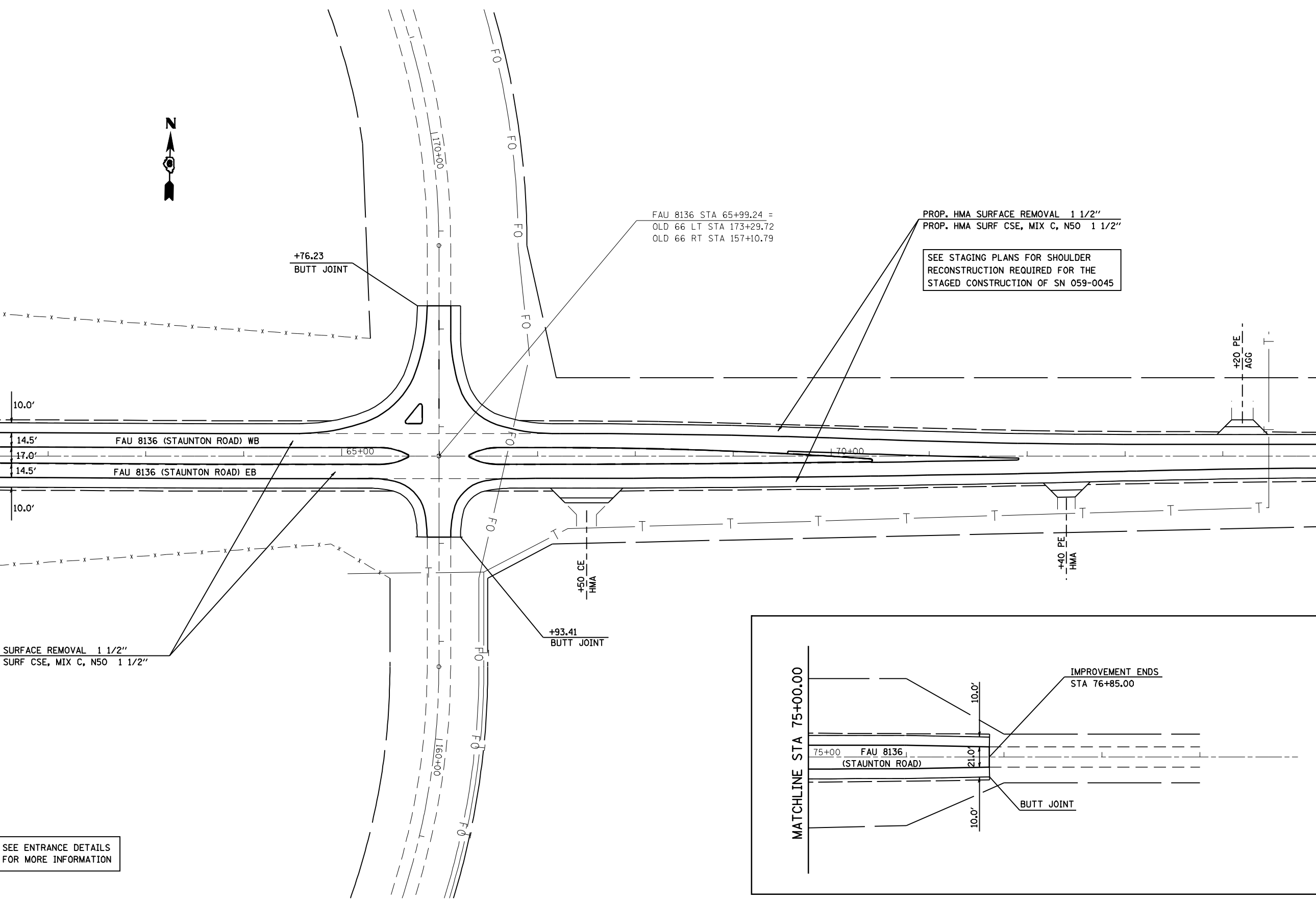
SCALE: 1" = 50' SHEET NO. OF SHEETS STA. 46+50.00 TO STA. 61+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	35
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72A60	



MATCHLINE STA 61+50.00

MATCHLINE STA 75+00.00

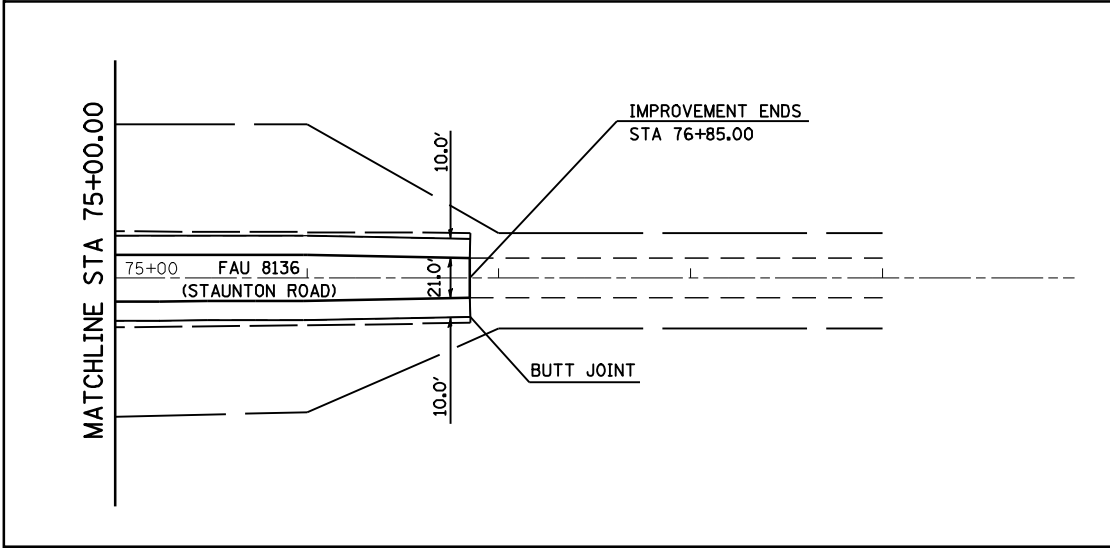


PROP. HMA SURFACE REMOVAL 1 1/2"
 PROP. HMA SURF CSE, MIX C, N50 1 1/2"
 SEE STAGING PLANS FOR SHOULDER RECONSTRUCTION REQUIRED FOR THE STAGED CONSTRUCTION OF SN 059-0045

FAU 8136 STA 65+99.24 =
 OLD 66 LT STA 173+29.72
 OLD 66 RT STA 157+10.79

PROP. HMA SURFACE REMOVAL 1 1/2"
 PROP. HMA SURF CSE, MIX C, N50 1 1/2"

SEE ENTRANCE DETAILS FOR MORE INFORMATION



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

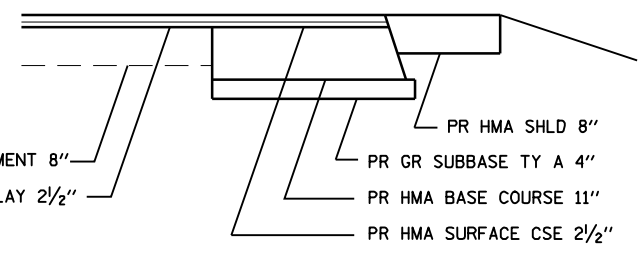
**FAU 8136 (STAUNTON ROAD)
 PLAN SHEETS**

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. 61+50.00 TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	36
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	



SEE INTERSECTION DETAIL SHEETS FOR ADDITIONAL INFORMATION ON WIDENING AND ISLAND DETAILS



SECTION A-A

PR PAVEMENT REMOVAL
 PR CONCRETE MEDIAN SURFACE 4"
 PR SUB-BASE GRANULAR MAT, TYPE C
 PR CCC&G M-6.06
 PR CCC&G M-6.24

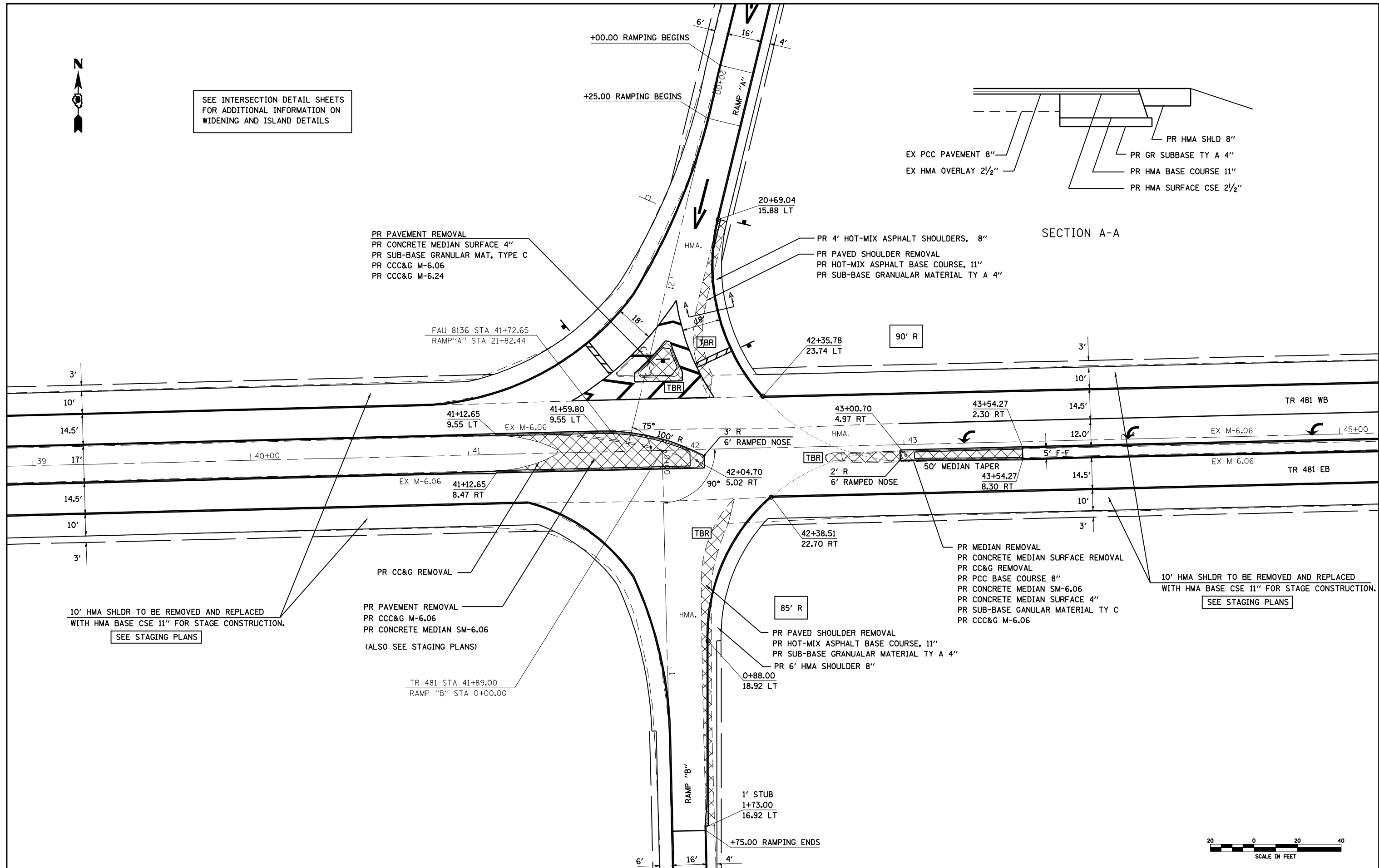
PR 4' HOT-MIX ASPHALT SHOULDERS, 8"
 PR PAVED SHOULDER REMOVAL
 PR HOT-MIX ASPHALT BASE COURSE, 11"
 PR SUB-BASE GRANULAR MATERIAL TY A 4"

PR MEDIAN REMOVAL
 PR CONCRETE MEDIAN SURFACE REMOVAL
 PR CC&G REMOVAL
 PR PCC BASE COURSE 8"
 PR CONCRETE MEDIAN SM-6.06
 PR CONCRETE MEDIAN SURFACE 4"
 PR SUB-BASE GRANULAR MATERIAL TY C
 PR CCC&G M-6.06

10' HMA SHLDR TO BE REMOVED AND REPLACED WITH HMA BASE CSE 11" FOR STAGE CONSTRUCTION.
 SEE STAGING PLANS

10' HMA SHLDR TO BE REMOVED AND REPLACED WITH HMA BASE CSE 11" FOR STAGE CONSTRUCTION.
 SEE STAGING PLANS

PR PAVEMENT REMOVAL
 PR CCC&G M-6.06
 PR CONCRETE MEDIAN SM-6.06
 (ALSO SEE STAGING PLANS)



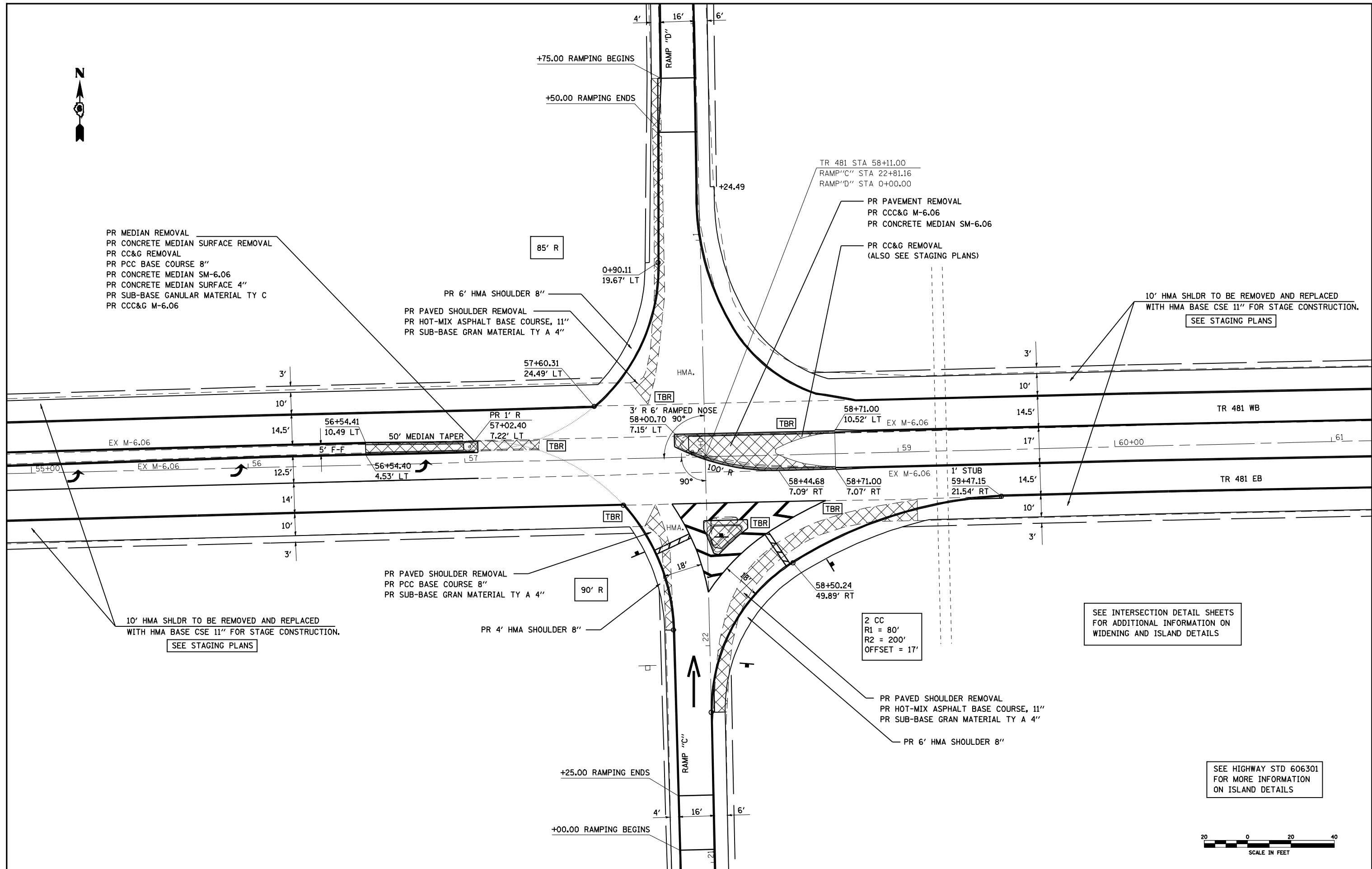
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PLOT DATE = Mar-23-2010 03:22:13PM		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**INTERSECTION PLAN SHEET
 RAMPS A,B & FAU 8136**

SCALE: 1" = 20' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	37
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	



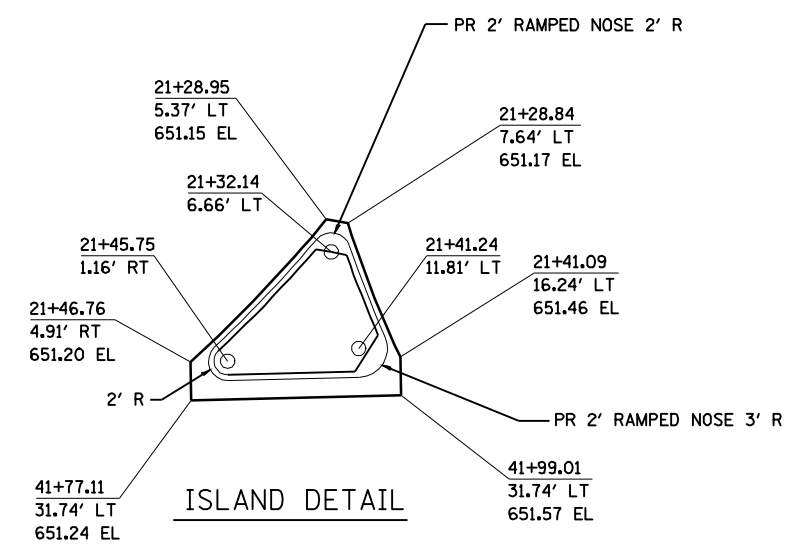
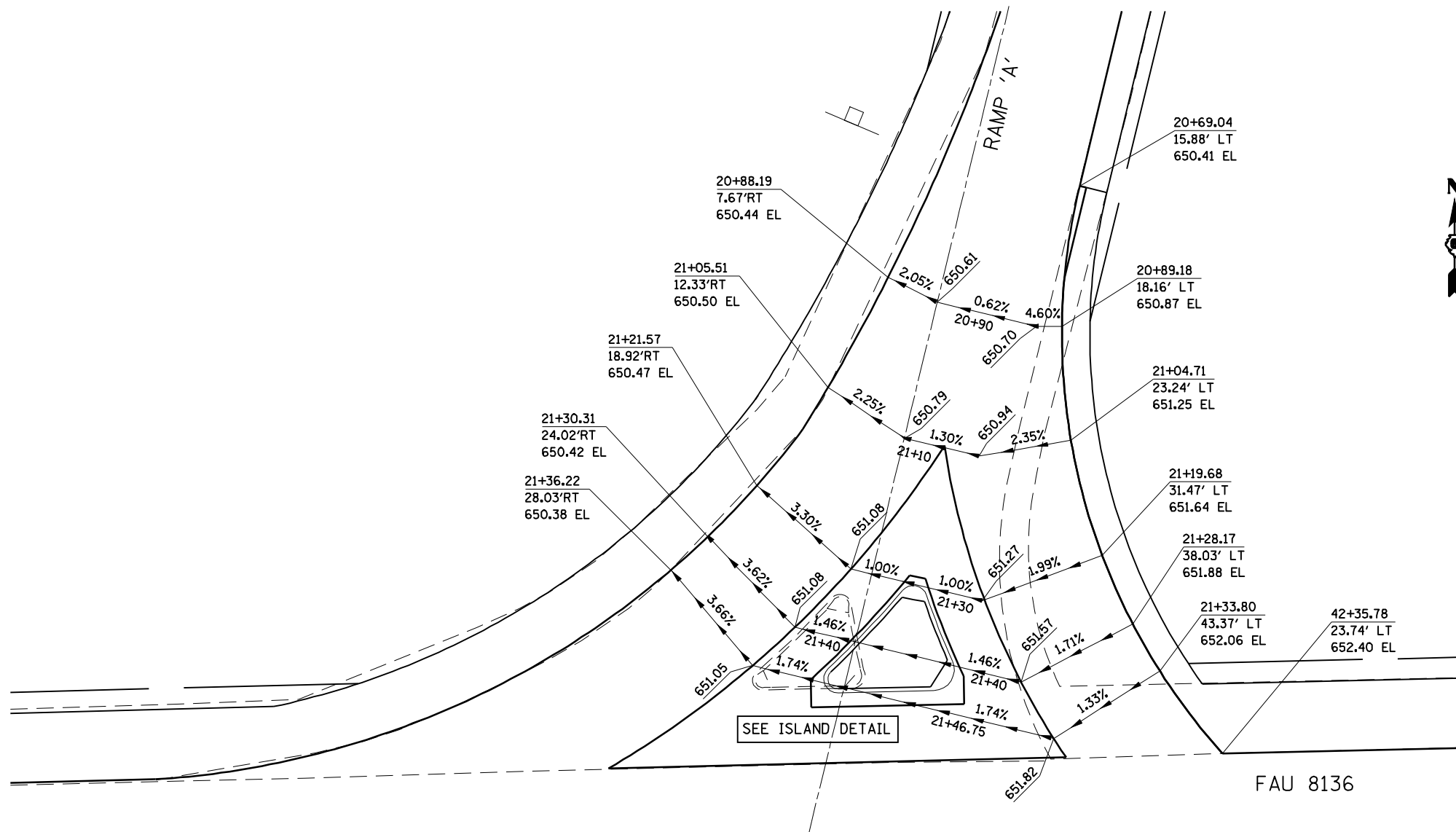
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PLOT SCALE = 40.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = Mar-23-2010 03:22:15PM		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

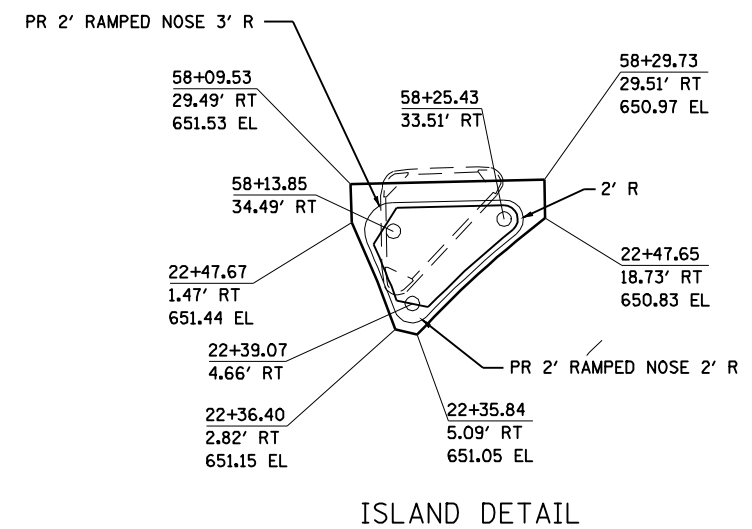
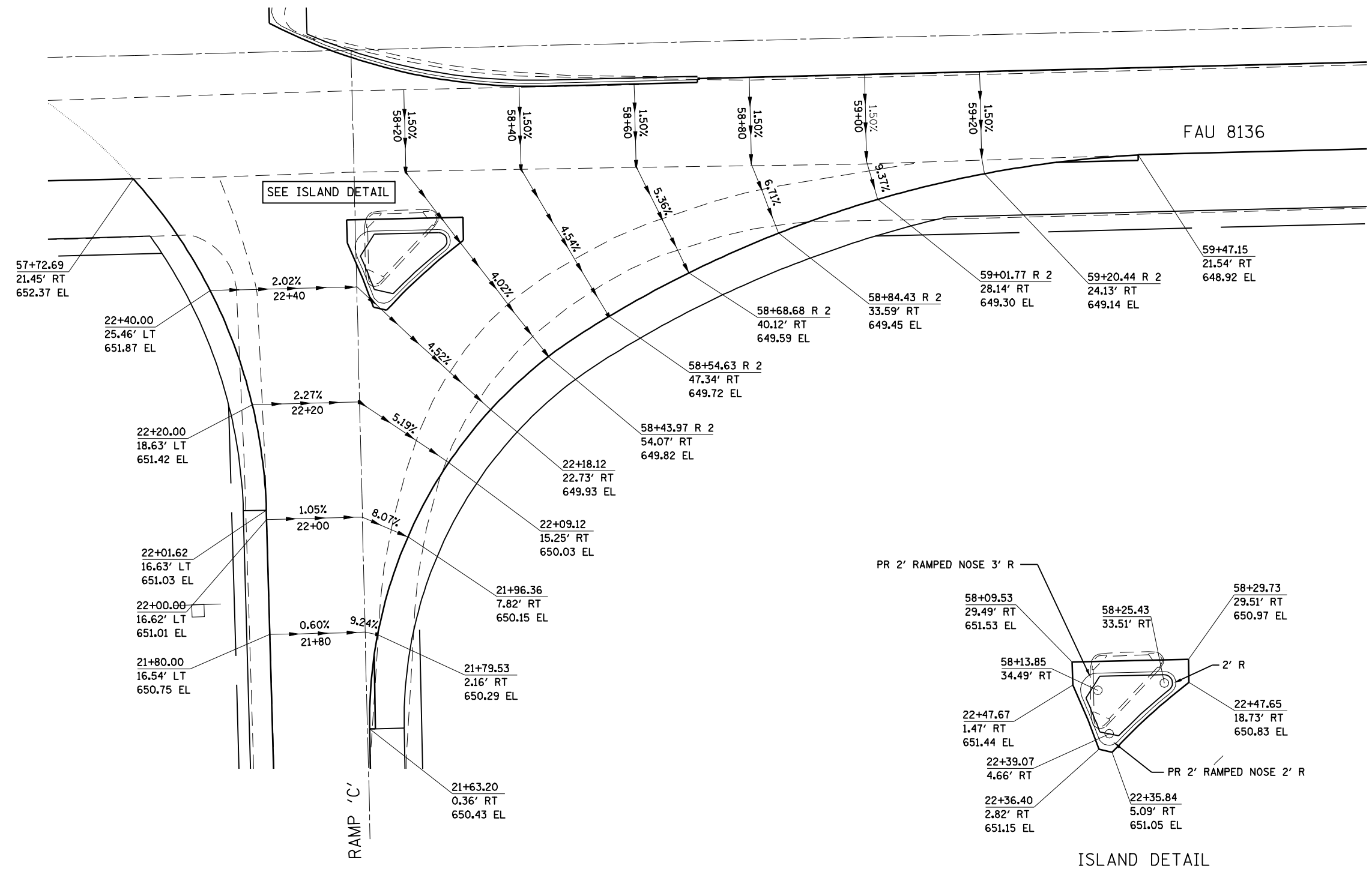
**INTERSECTION PLAN SHEET
RAMPS C,D & FAU 8136**

SCALE: 1" = 20' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	38
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	



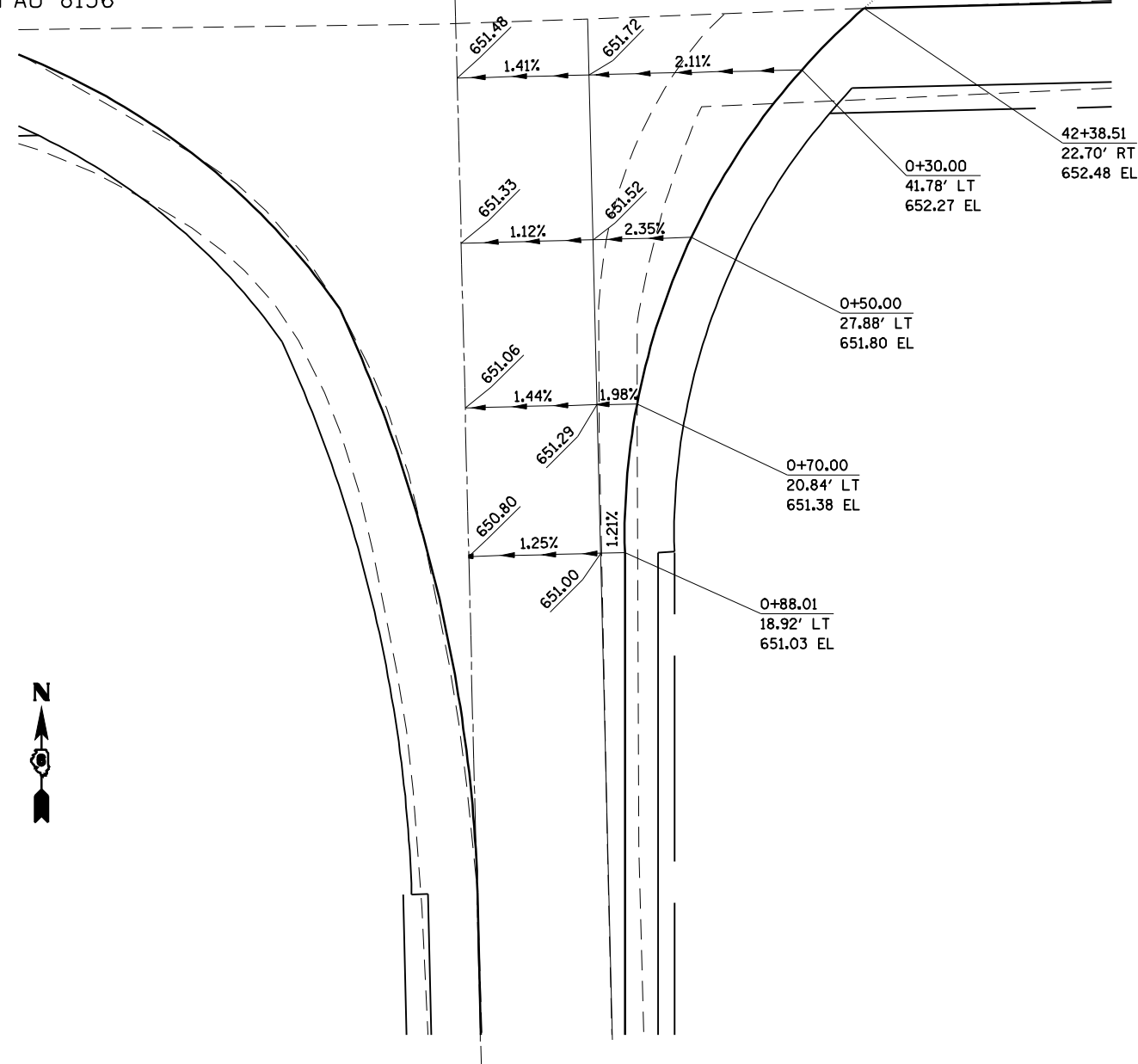
FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INTERSECTION DETAIL SHEET RAMP A & FAU 8136			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
e:\pwwork\pwwid01\COXTE\dna25312\072460	intersect-det.dgn	DRAWN -	REVISED -		55	59RS-2, BR	MACOUPIN	100	39			
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 72A60							
	PLOT DATE = Mar-23-2010 03:22:17PM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE: 1" = 10'	SHEET NO.	OF SHEETS	STA.	TO STA.				



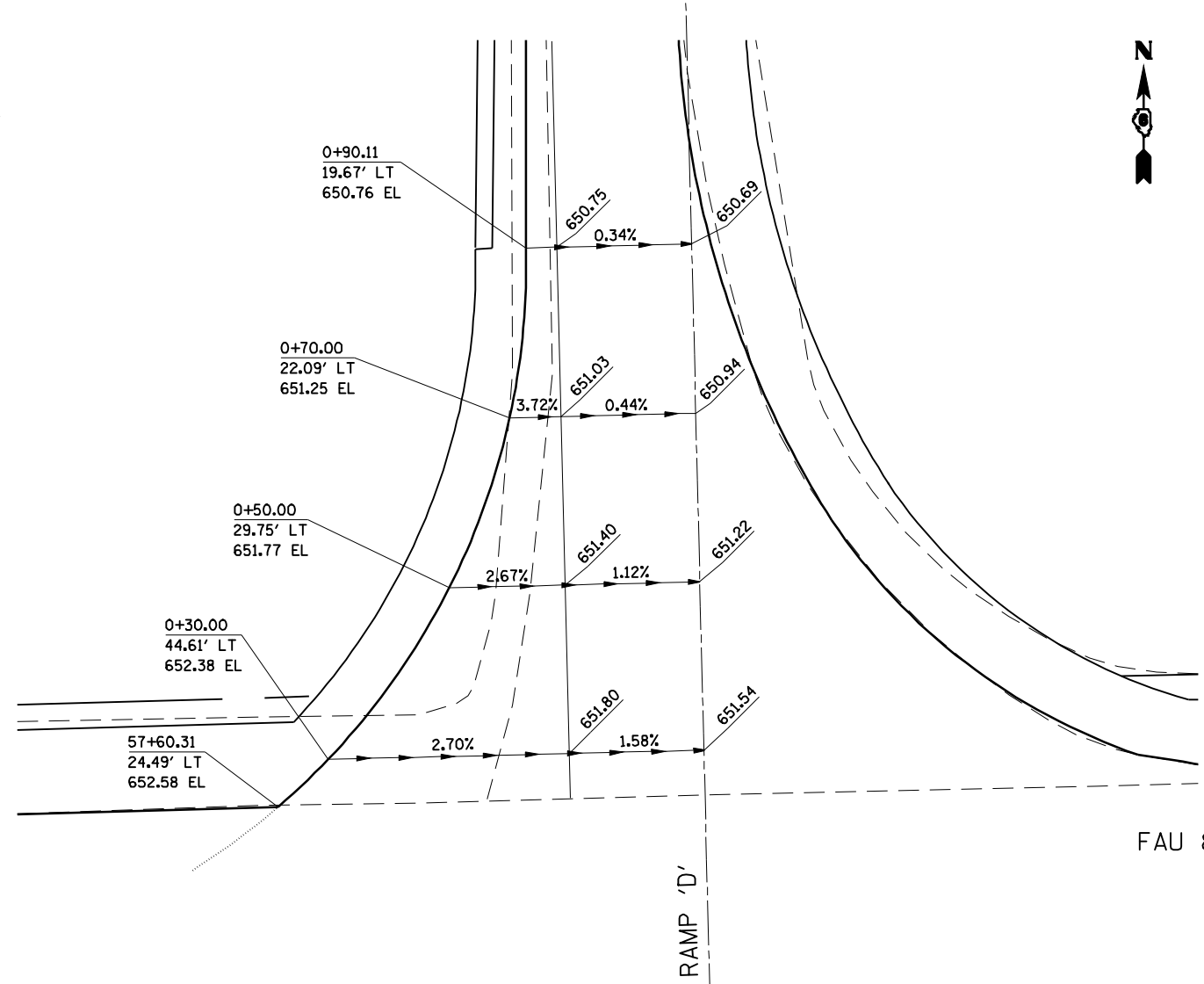
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er:\pwork\PWIDOT\COXTE\dna25312\072460	intersect.dwg	DRAWN -	REVISED -		55	59RS-2, BR	MACOUPIN	100	40			
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -		CONTRACT NO. 72A60							
	PLOT DATE = Mar-23-2010 03:22:19PM	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
					SCALE: 1" = 10'	SHEET NO.	OF SHEETS	STA.	TO STA.			

FAU 8136

RAMP 'B'



RAMP 'B'



RAMP 'D'

FAU 8136

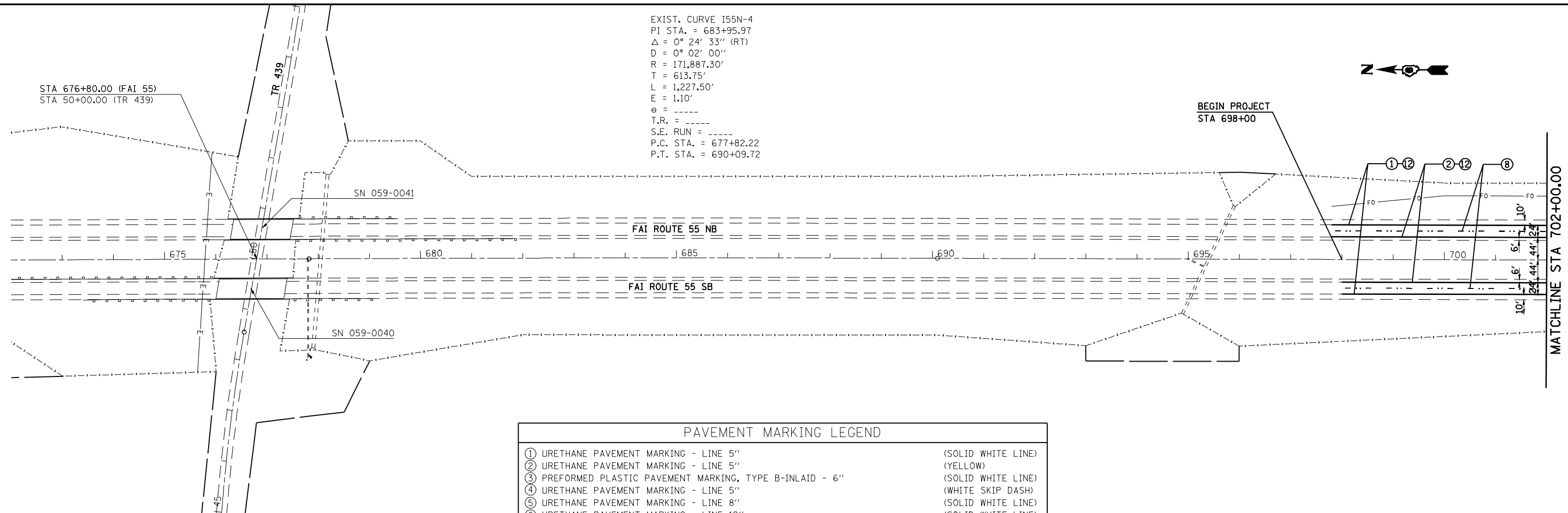
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	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = Mar-23-2010 03:22:22PM	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

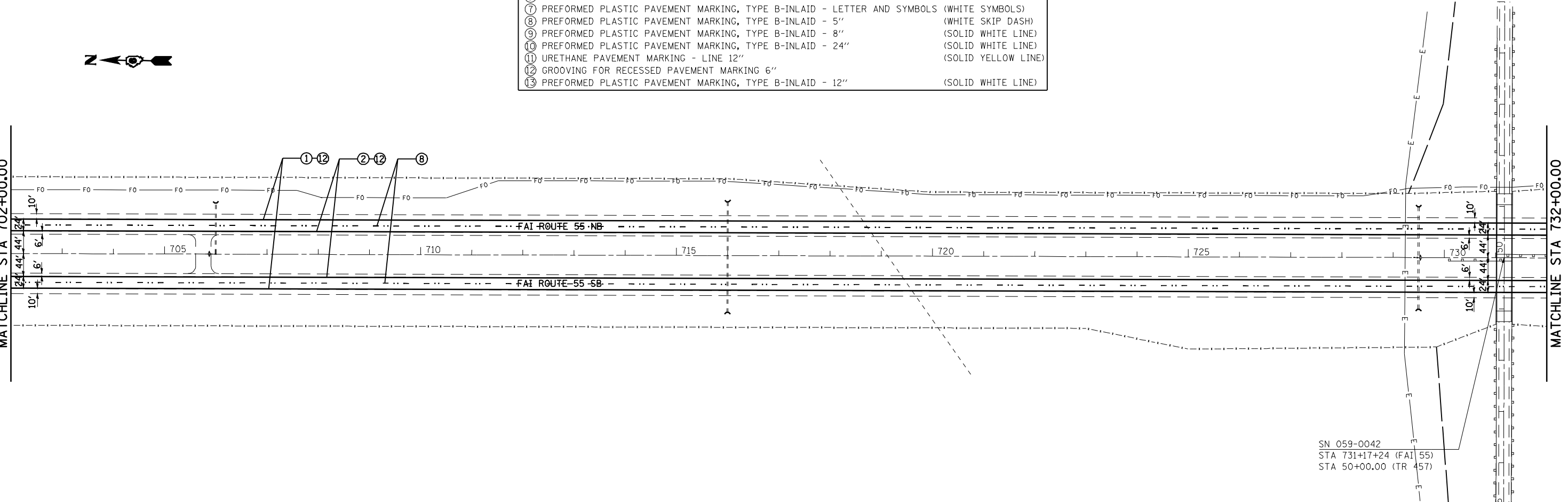
INTERSECTION DETAIL SHEET RAMPS B,D & FAU 8136			
SCALE: 1" = 10'	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	41
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	

EXIST. CURVE I55N-4
 PI STA. = 683+95.97
 $\Delta = 0^\circ 24' 33''$ (RT)
 $D = 0^\circ 02' 00''$
 $R = 171,887.30'$
 $T = 613.75'$
 $L = 1,227.50'$
 $E = 1.10'$
 $e = \text{-----}$
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 677+82.22
 P.T. STA. = 690+09.72



PAVEMENT MARKING LEGEND		
①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(YELLOW)
③	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP DASH)
⑤	URETHANE PAVEMENT MARKING - LINE 8"	(SOLID WHITE LINE)
⑥	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LETTER AND SYMBOLS	(WHITE SYMBOLS)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 5"	(WHITE SKIP DASH)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 8"	(SOLID WHITE LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 24"	(SOLID WHITE LINE)
⑪	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID YELLOW LINE)
⑫	GROOVING FOR RECESSED PAVEMENT MARKING 6"	
⑬	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 12"	(SOLID WHITE LINE)

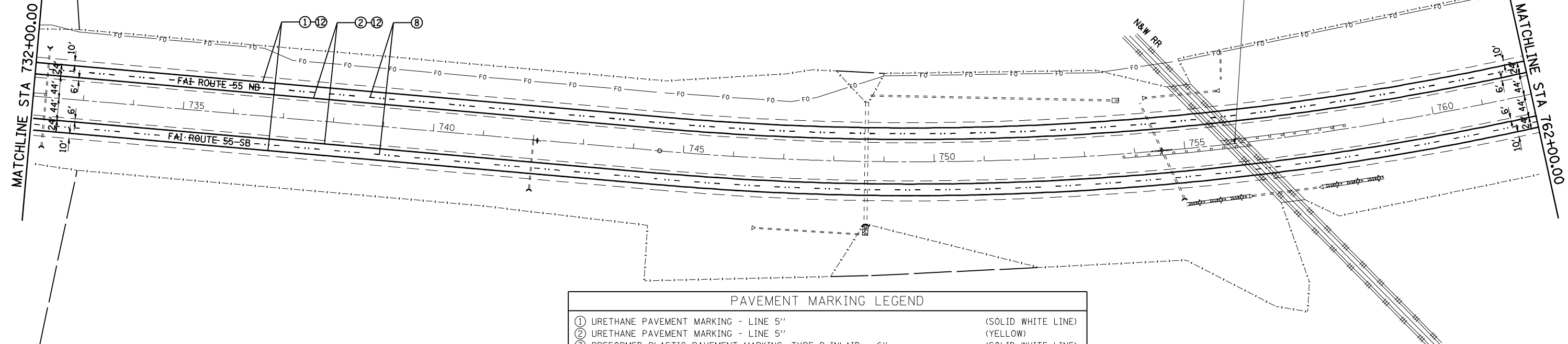


FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLAN SHEETS			F.A.I. RTE. 55	SECTION 59RS-2, BR	COUNTY MACOUPIN	TOTAL SHEETS 100	SHEET NO. 42
es:\pwwork\pwwid01\COXTE\dms25312\0672400-0-shr-pmk.dgn		DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET NO.	OF SHEETS	STA.	TO STA. 732+00.00	ILLINOIS FED. AID PROJECT		
		CHECKED -	REVISED -							CONTRACT NO. 72A60		
		DATE -	REVISED -							CONTRACT NO. 72A60		



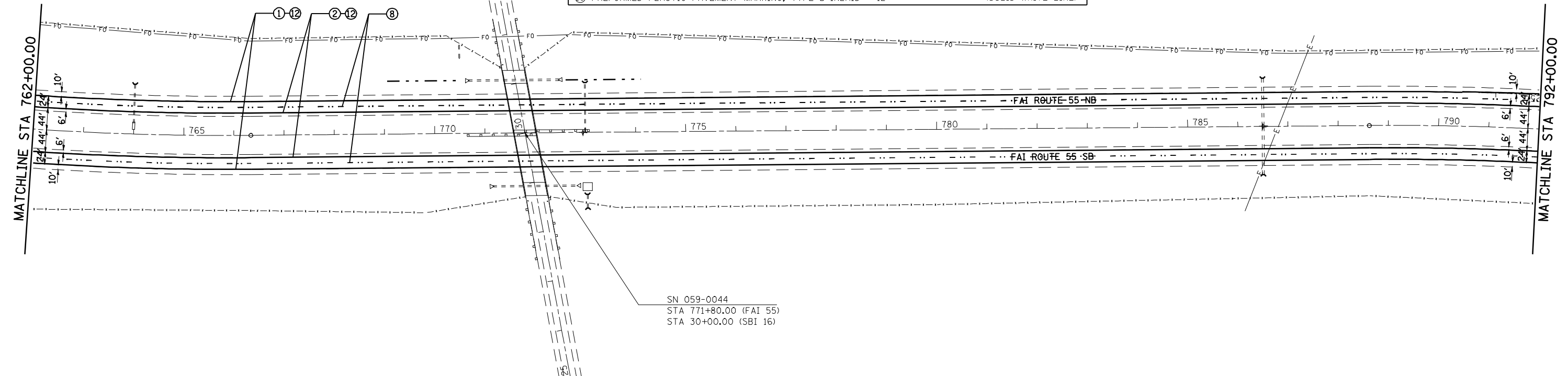
EXIST. CURVE I55N-5
 PI STA. = 755+56.46
 $\Delta = 21^\circ 48' 40''$ (LT)
 $D = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 1,103.92'$
 $L = 2,181.11'$
 $E = 105.38'$
 $e = 8.0\%$
 $T.R. = 46'$
 $S.E. RUN = 126'$
 $P.C. STA. = 744+52.54$
 $P.T. STA. = 766+33.65$

SN 059-0043
 STA 755+89.61 (FAI 55)
 STA 50+00.00 (N&W RR)



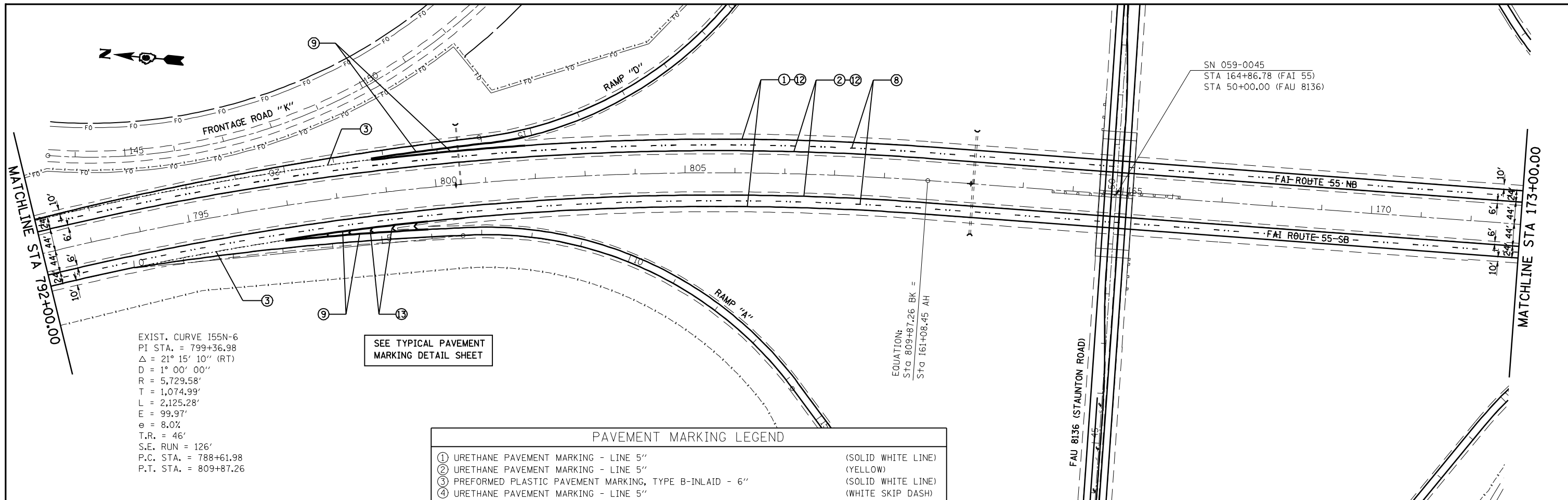
PAVEMENT MARKING LEGEND

- | | |
|--|---------------------|
| ① URETHANE PAVEMENT MARKING - LINE 5" | (SOLID WHITE LINE) |
| ② URETHANE PAVEMENT MARKING - LINE 5" | (YELLOW) |
| ③ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 6" | (SOLID WHITE LINE) |
| ④ URETHANE PAVEMENT MARKING - LINE 5" | (WHITE SKIP DASH) |
| ⑤ URETHANE PAVEMENT MARKING - LINE 8" | (SOLID WHITE LINE) |
| ⑥ URETHANE PAVEMENT MARKING - LINE 12" | (SOLID WHITE LINE) |
| ⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LETTER AND SYMBOLS | (WHITE SYMBOLS) |
| ⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 5" | (WHITE SKIP DASH) |
| ⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 8" | (SOLID WHITE LINE) |
| ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 24" | (SOLID WHITE LINE) |
| ⑪ URETHANE PAVEMENT MARKING - LINE 12" | (SOLID YELLOW LINE) |
| ⑫ GROOVING FOR RECESSED PAVEMENT MARKING 6" | |
| ⑬ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 12" | (SOLID WHITE LINE) |



SN 059-0044
 STA 771+80.00 (FAI 55)
 STA 30+00.00 (SBI 16)

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLAN SHEETS			F.A.I. RTE. 55	SECTION 59RS-2, BR	COUNTY MACOUPIN	TOTAL SHEETS 100	SHEET NO. 43
es:\pwwork\p\WID01\COXTE\dma25312\0672400-sh1-pmk.dgn		DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET NO.	OF SHEETS	STA. 732+00.00 TO STA. 792+00.00	CONTRACT NO. 72A60			
PLOT SCALE = 200.0000' / IN.		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
PLOT DATE = Mar-23-2010 03:22:27PM		DATE -	REVISED -									



EXIST. CURVE I55N-6
 PI STA. = 799+36.98
 $\Delta = 21^\circ 15' 10''$ (RT)
 $D = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 1,074.99'$
 $L = 2,125.28'$
 $E = 99.97'$
 $e = 8.0\%$
 $T.R. = 46'$
 $S.E. RUN = 126'$
 $P.C. STA. = 788+61.98$
 $P.T. STA. = 809+87.26$

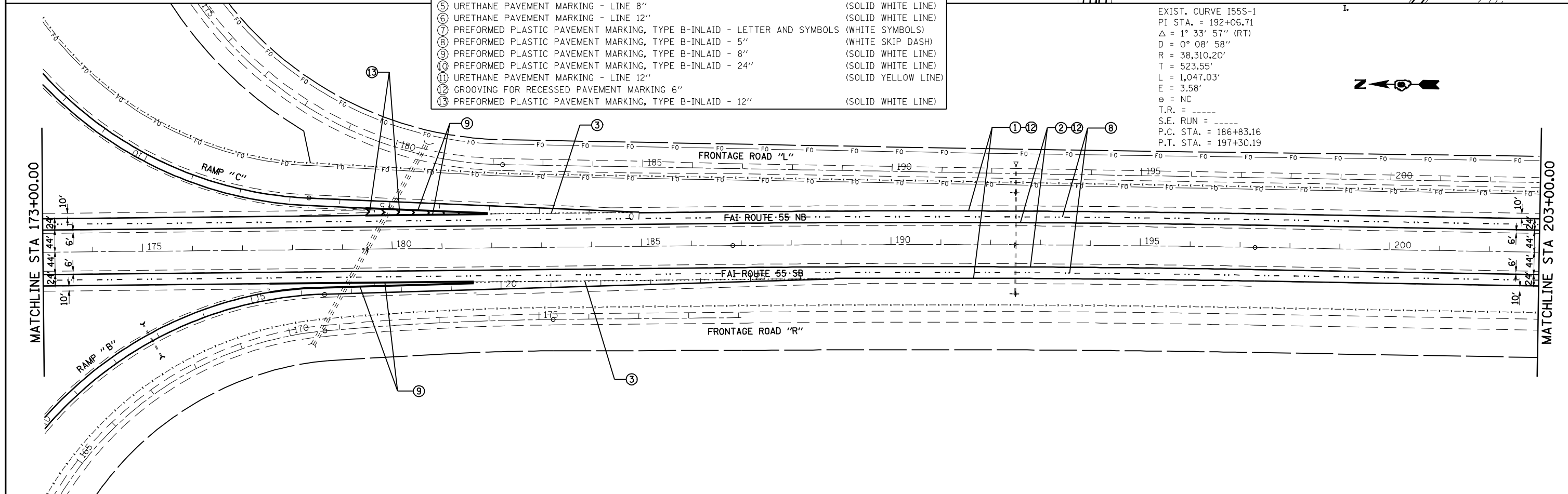
SEE TYPICAL PAVEMENT MARKING DETAIL SHEET

EQUATION:
 STA 809+87.26 BK =
 STA 161+08.45 AH

SN 059-0045
 STA 164+86.78 (FAI 55)
 STA 50+00.00 (FAU 8136)

PAVEMENT MARKING LEGEND		
①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(YELLOW)
③	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP DASH)
⑤	URETHANE PAVEMENT MARKING - LINE 8"	(SOLID WHITE LINE)
⑥	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LETTER AND SYMBOLS	(WHITE SYMBOLS)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 5"	(WHITE SKIP DASH)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 8"	(SOLID WHITE LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 24"	(SOLID WHITE LINE)
⑪	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID YELLOW LINE)
⑫	GROOVING FOR RECESSED PAVEMENT MARKING 6"	
⑬	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 12"	(SOLID WHITE LINE)

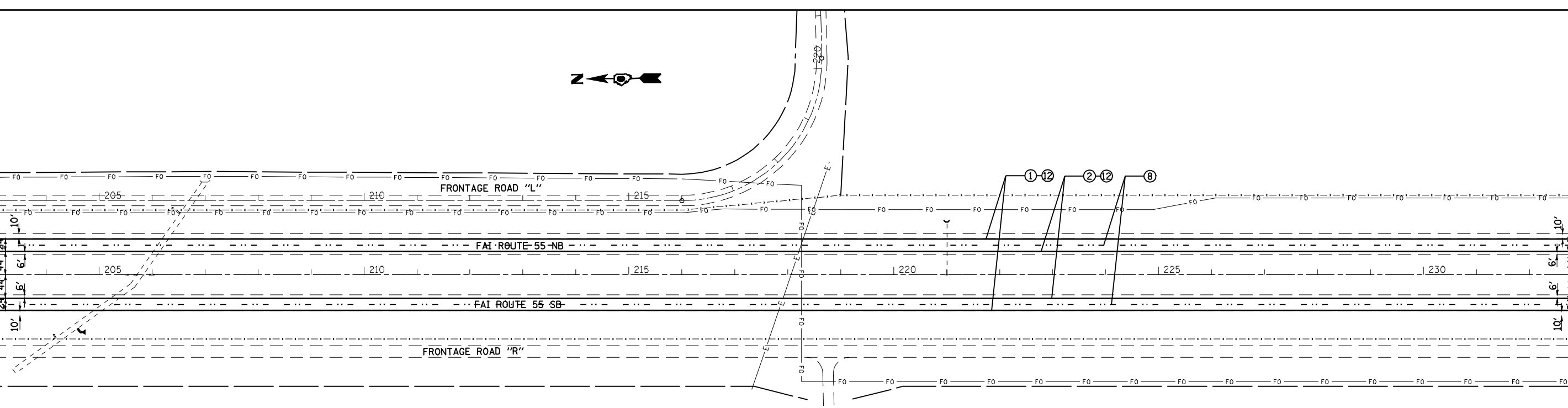
EXIST. CURVE I55S-1
 PI STA. = 192+06.71
 $\Delta = 1^\circ 33' 57''$ (RT)
 $D = 0^\circ 08' 58''$
 $R = 38,310.20'$
 $T = 523.55'$
 $L = 1,047.03'$
 $E = 3.58'$
 $e = NC$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 186+83.16$
 $P.T. STA. = 197+30.19$



FILE NAME = e:\pwork\PWID01\COXTE\dms25312\0672A00-shk-pmk.dgn	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLAN SHEETS			F.A.I. RTE. 55	SECTION 59RS-2, BR	COUNTY MACOUPIN	TOTAL SHEETS 100	SHEET NO. 44
PLOT SCALE = 200.0000' / IN.	PLOT DATE = Mar-23-2010 03:22:29PM	DRAWN -	REVISED -		SCALE: 1" = 100'	SHEET NO.	OF	SHEETS	STA. 792+00.00	TO STA. 203+00.00	CONTRACT NO. 72A60	
		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

MATCHLINE STA 203+00.00

MATCHLINE STA 233+00.00



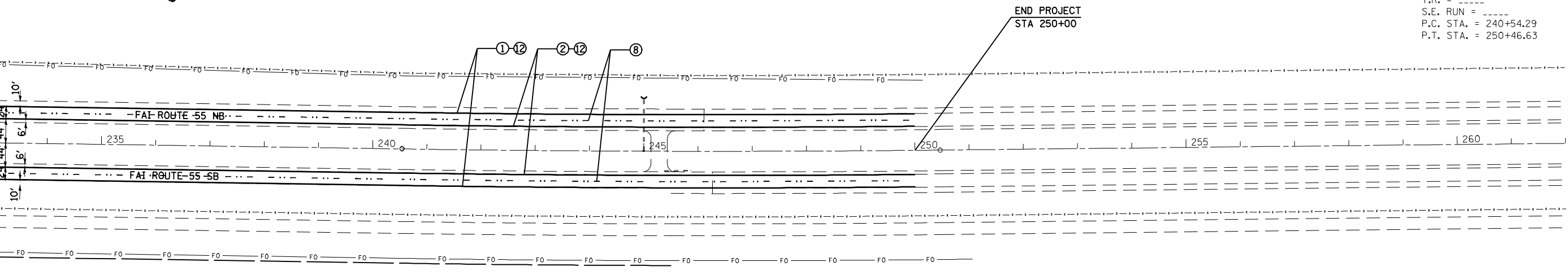
PAVEMENT MARKING LEGEND

①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(YELLOW)
③	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP DASH)
⑤	URETHANE PAVEMENT MARKING - LINE 8"	(SOLID WHITE LINE)
⑥	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LETTER AND SYMBOLS	(WHITE SYMBOLS)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 5"	(WHITE SKIP DASH)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 8"	(SOLID WHITE LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 24"	(SOLID WHITE LINE)
⑪	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID YELLOW LINE)
⑫	GROOVING FOR RECESSED PAVEMENT MARKING 6"	
⑬	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 12"	(SOLID WHITE LINE)

EXIST. CURVE I55S-2
 PI STA. = 245+50.49
 $\Delta = 1^\circ 29' 35''$ (LT)
 D = $0^\circ 09' 02''$
 R = 38,084.20'
 T = 496.20'
 L = 992.34'
 E = 3.23'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 240+54.29
 P.T. STA. = 250+46.63

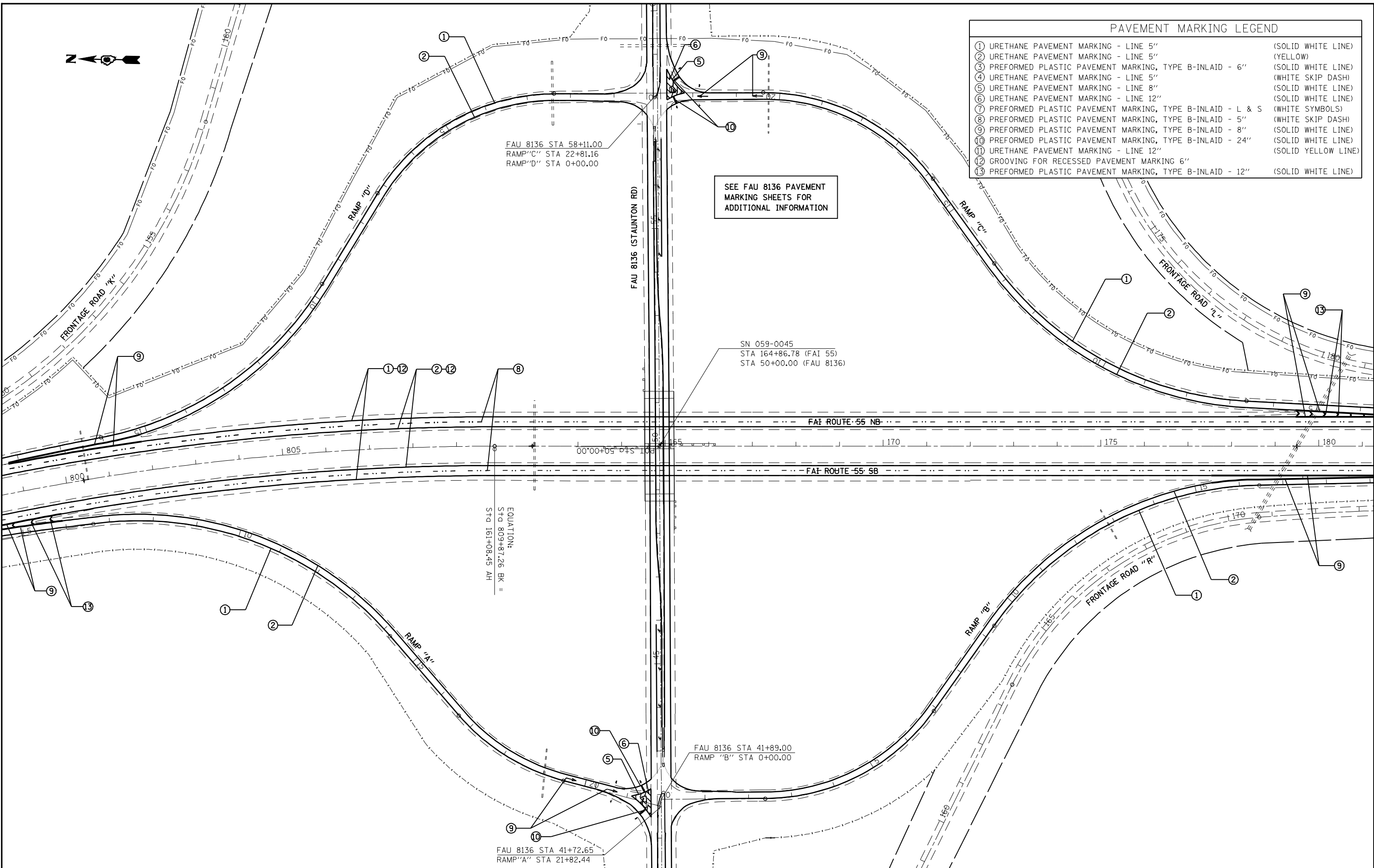
MATCHLINE STA 233+00.00

END PROJECT
 STA 250+00





PAVEMENT MARKING LEGEND		
①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(YELLOW)
③	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP DASH)
⑤	URETHANE PAVEMENT MARKING - LINE 8"	(SOLID WHITE LINE)
⑥	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID WHITE LINE)
⑦	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - L & S	(WHITE SYMBOLS)
⑧	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 5"	(WHITE SKIP DASH)
⑨	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 8"	(SOLID WHITE LINE)
⑩	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 24"	(SOLID WHITE LINE)
⑪	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID YELLOW LINE)
⑫	GROOVING FOR RECESSED PAVEMENT MARKING 6"	
⑬	PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 12"	(SOLID WHITE LINE)



FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
es:\pwwork\pwidot\COXTE\dna25312\0672400-shk-pmk.dgn		DRAWN -	REVISED -
PLOT SCALE = 200.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = Mar-23-2010 03:22:35PM		DATE -	REVISED -

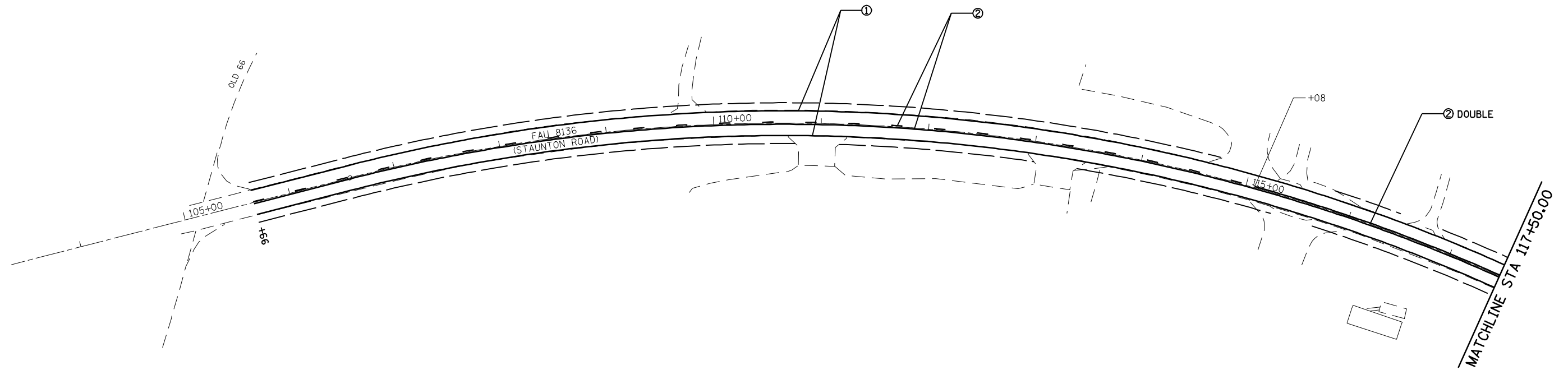
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN SHEETS			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.
1" = 100'			

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	46
CONTRACT NO. 72A60				
ILLINOIS FED. AID PROJECT				



EXIST. CURVE C998
 PI STA. = 113+13.64
 $\Delta = 43^\circ 59' 05''$ (RT)
 D = 3° 31' 43"
 R = 1,623.81'
 T = 655.81'
 L = 1,246.56'
 E = 127.43'
 e = 7.0%
 T.R. = 38'
 S.E. RUN = 179'
 P.C. STA. = 106+57.83
 P.T. STA. = 119+04.40



PAVEMENT MARKING LEGEND		
①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(YELLOW)
③	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP DASH)
⑤	URETHANE PAVEMENT MARKING - LINE 8"	(SOLID WHITE LINE)
⑥	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LETTER AND SYMBOLS	(WHITE SYMBOLS)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 5"	(WHITE SKIP DASH)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 8"	(SOLID WHITE LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 24"	(SOLID WHITE LINE)
⑪	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID YELLOW LINE)
⑫	GROOVING FOR RECESSED PAVEMENT MARKING 6"	
⑬	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 12"	(SOLID WHITE LINE)

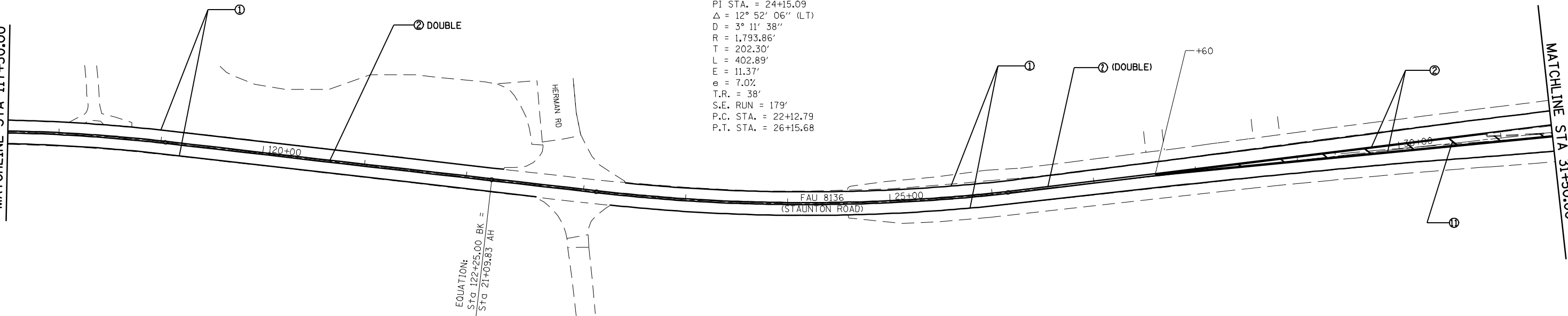
FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAU 8136 (STAUNTON ROAD) PAVEMENT MARKING PLAN SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
er:\pwork\pwork\WIDOT\COXTE\dma25312\0672400-ahst-plan-TR481PM.dgn	DRAWN -	REVISED -	55			59RS-2, BR	MACOUPIN	100	47	
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 72A60							
PLOT DATE = Mar-23-2010 03:22:38PM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
						SCALE: 1" = 50'	SHEET NO. OF SHEETS	STA.	TO STA. 117+50.00	



MATCHLINE STA 117+50.00

MATCHLINE STA 31+50.00

EXIST. CURVE DPICALC1
 PI STA. = 24+15.09
 $\Delta = 12^\circ 52' 06''$ (LT)
 D = $3^\circ 11' 38''$
 R = 1,793.86'
 T = 202.30'
 L = 402.89'
 E = 11.37'
 e = 7.0%
 T.R. = 38'
 S.E. RUN = 179'
 P.C. STA. = 22+12.79
 P.T. STA. = 26+15.68



EQUATION:
 Sta 122+25.00 BK =
 Sta 21+09.83 AH

PAVEMENT MARKING LEGEND

①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(YELLOW)
③	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP DASH)
⑤	URETHANE PAVEMENT MARKING - LINE 8"	(SOLID WHITE LINE)
⑥	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LETTER AND SYMBOLS	(WHITE SYMBOLS)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 5"	(WHITE SKIP DASH)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 8"	(SOLID WHITE LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 24"	(SOLID WHITE LINE)
⑪	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID YELLOW LINE)
⑫	GROOVING FOR RECESSED PAVEMENT MARKING 6"	
⑬	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 12"	(SOLID WHITE LINE)

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
er\pwwork\PWIDOT\COXTE\dma25312\0672480-shr-plan-TR481PM.dgn		DRAWN -	REVISED -
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = Mar-23-2010 03:22:40PM		DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

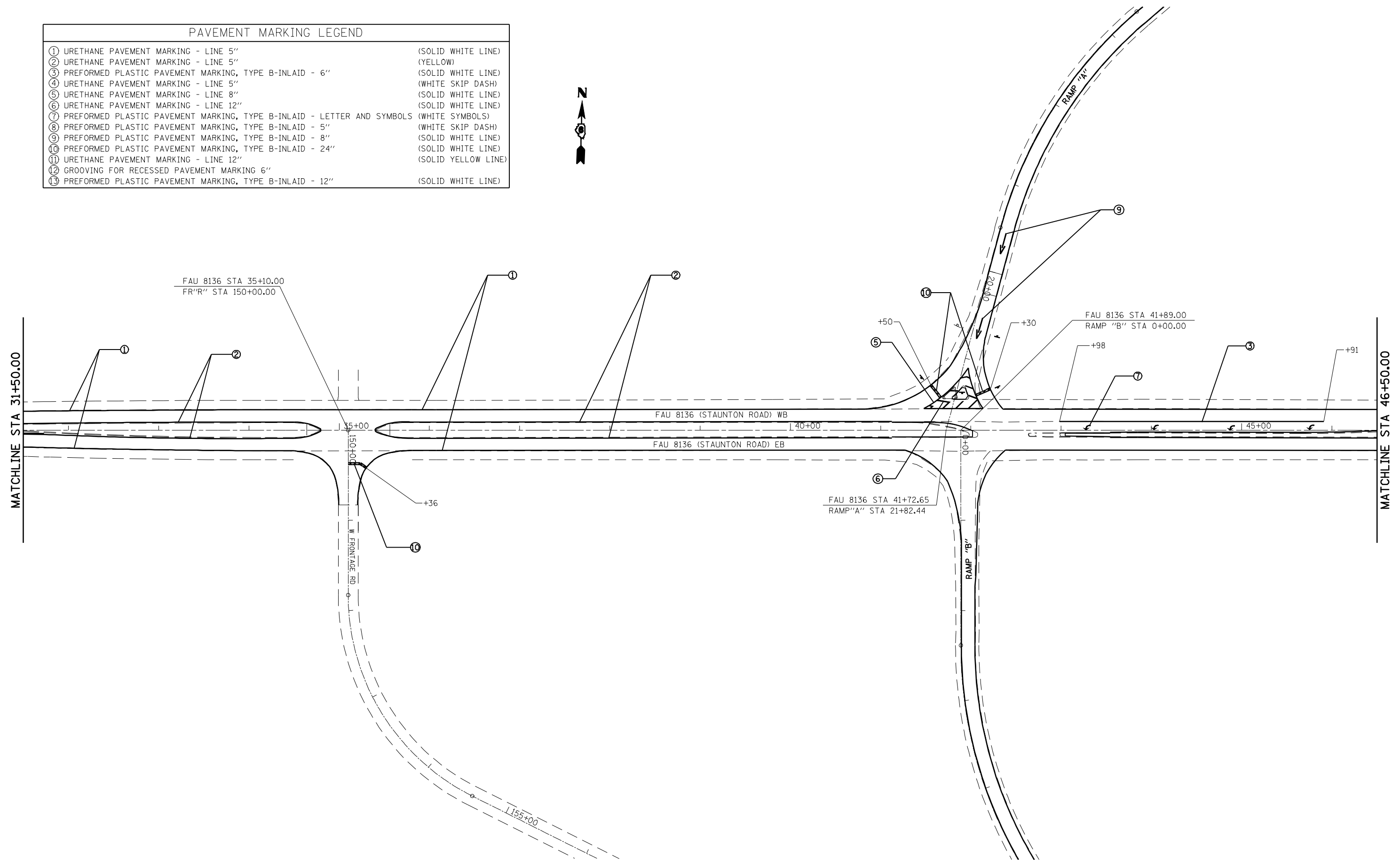
FAU 8136 (STAUNTON ROAD)
 PAVEMENT MARKING PLAN SHEETS

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. 117+50.00 TO STA. 31+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	48
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	

PAVEMENT MARKING LEGEND

① URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
② URETHANE PAVEMENT MARKING - LINE 5"	(YELLOW)
③ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 6"	(SOLID WHITE LINE)
④ URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP DASH)
⑤ URETHANE PAVEMENT MARKING - LINE 8"	(SOLID WHITE LINE)
⑥ URETHANE PAVEMENT MARKING - LINE 12"	(SOLID WHITE LINE)
⑦ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LETTER AND SYMBOLS	(WHITE SYMBOLS)
⑧ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 5"	(WHITE SKIP DASH)
⑨ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 8"	(SOLID WHITE LINE)
⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 24"	(SOLID WHITE LINE)
⑪ URETHANE PAVEMENT MARKING - LINE 12"	(SOLID YELLOW LINE)
⑫ GROOVING FOR RECESSED PAVEMENT MARKING 6"	
⑬ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 12"	(SOLID WHITE LINE)



FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
e:\pwork\PWIDOT\COXTE\dms25312\0672400-shs-plan-TR481PM.dgn		DRAWN -	REVISED -
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = Mar-23-2010 03:22:42PM		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAU 8136 (STAUNTON ROAD)
PAVEMENT MARKING PLAN SHEETS

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. 31+50.00 TO STA. 46+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	49
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	

EQUATION:
Sta 809+87.26 BK =
Sta 161+08.45 AH

I-55

SN 059-0045
STA 164+86.78 (FAI 55)
STA 50+00.00 (FAU 8136)

FAU 8136 STA 58+11.00
RAMP "C" STA 22+81.16
RAMP "D" STA 0+00.00

MATCHLINE STA 46+50.00

MATCHLINE STA 61+50.00

FAU 8136 (STAUNTON ROAD) WB

FAU 8136 (STAUNTON ROAD) EB

50+00

55+00

60+00

+35

③

⑦

+06

+40

⑩

+45

⑨

20+00

PAVEMENT MARKING LEGEND

①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(YELLOW)
③	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP DASH)
⑤	URETHANE PAVEMENT MARKING - LINE 8"	(SOLID WHITE LINE)
⑥	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LETTER AND SYMBOLS	(WHITE SYMBOLS)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 5"	(WHITE SKIP DASH)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 8"	(SOLID WHITE LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 24"	(SOLID WHITE LINE)
⑪	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID YELLOW LINE)
⑫	GROOVING FOR RECESSED PAVEMENT MARKING 6"	
⑬	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 12"	(SOLID WHITE LINE)

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
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PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = Mar-23-2010 03:22:45PM		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

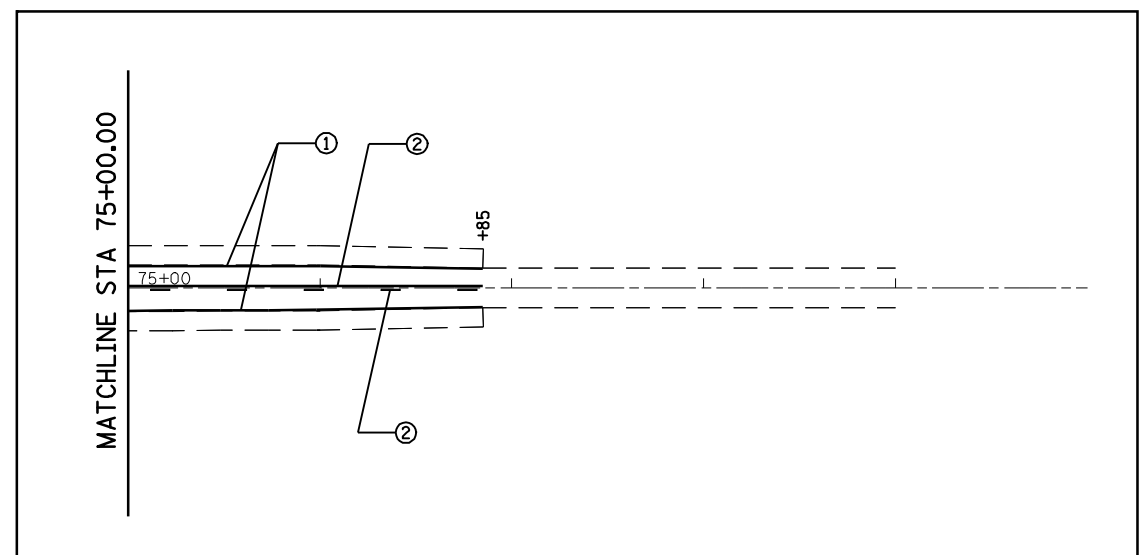
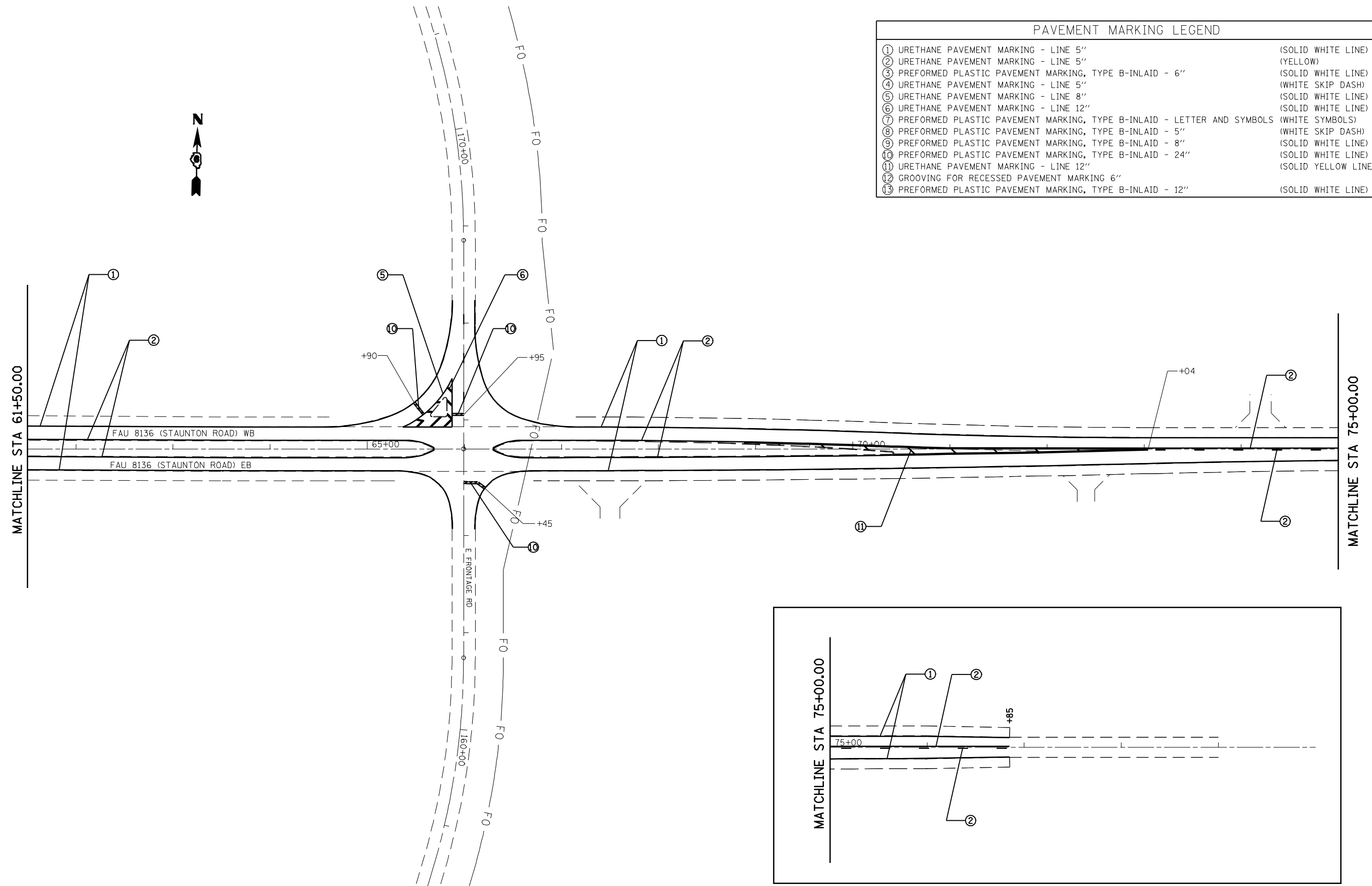
FAU 8136 (STAUNTON ROAD)
PAVEMENT MARKING PLAN SHEETS

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. 46+50.00 TO STA. 61+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	50
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72A60	



PAVEMENT MARKING LEGEND		
①	URETHANE PAVEMENT MARKING - LINE 5"	(SOLID WHITE LINE)
②	URETHANE PAVEMENT MARKING - LINE 5"	(YELLOW)
③	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 6"	(SOLID WHITE LINE)
④	URETHANE PAVEMENT MARKING - LINE 5"	(WHITE SKIP DASH)
⑤	URETHANE PAVEMENT MARKING - LINE 8"	(SOLID WHITE LINE)
⑥	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID WHITE LINE)
⑦	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LETTER AND SYMBOLS	(WHITE SYMBOLS)
⑧	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 5"	(WHITE SKIP DASH)
⑨	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 8"	(SOLID WHITE LINE)
⑩	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 24"	(SOLID WHITE LINE)
⑪	URETHANE PAVEMENT MARKING - LINE 12"	(SOLID YELLOW LINE)
⑫	GROOVING FOR RECESSED PAVEMENT MARKING 6"	
⑬	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - 12"	(SOLID WHITE LINE)



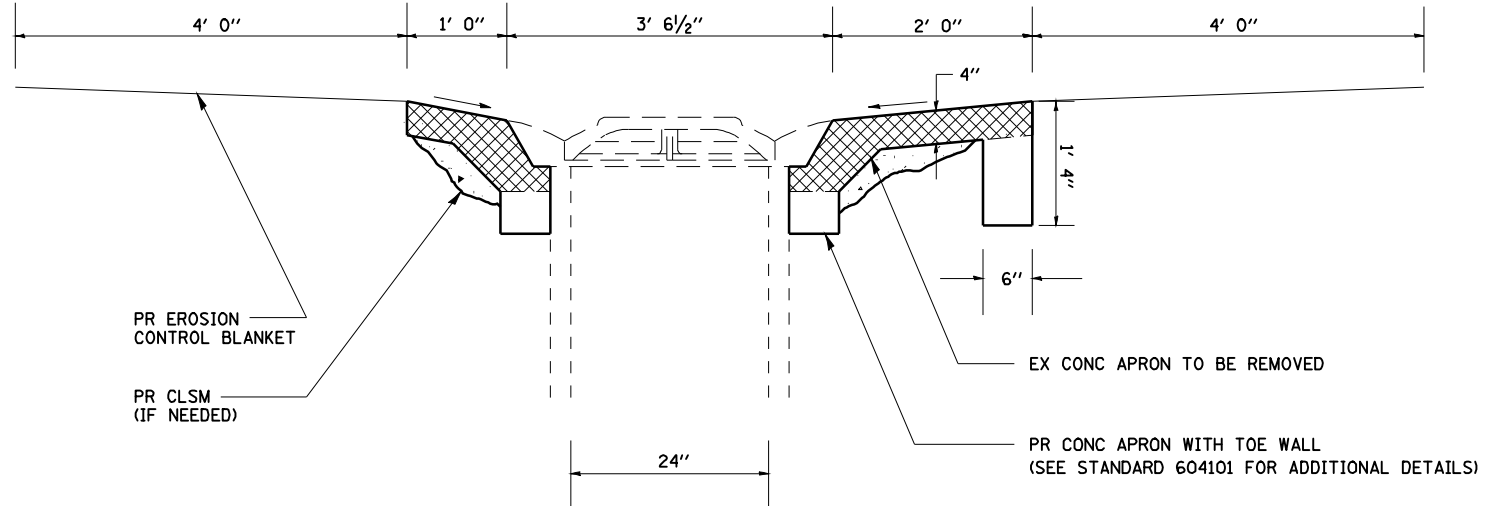
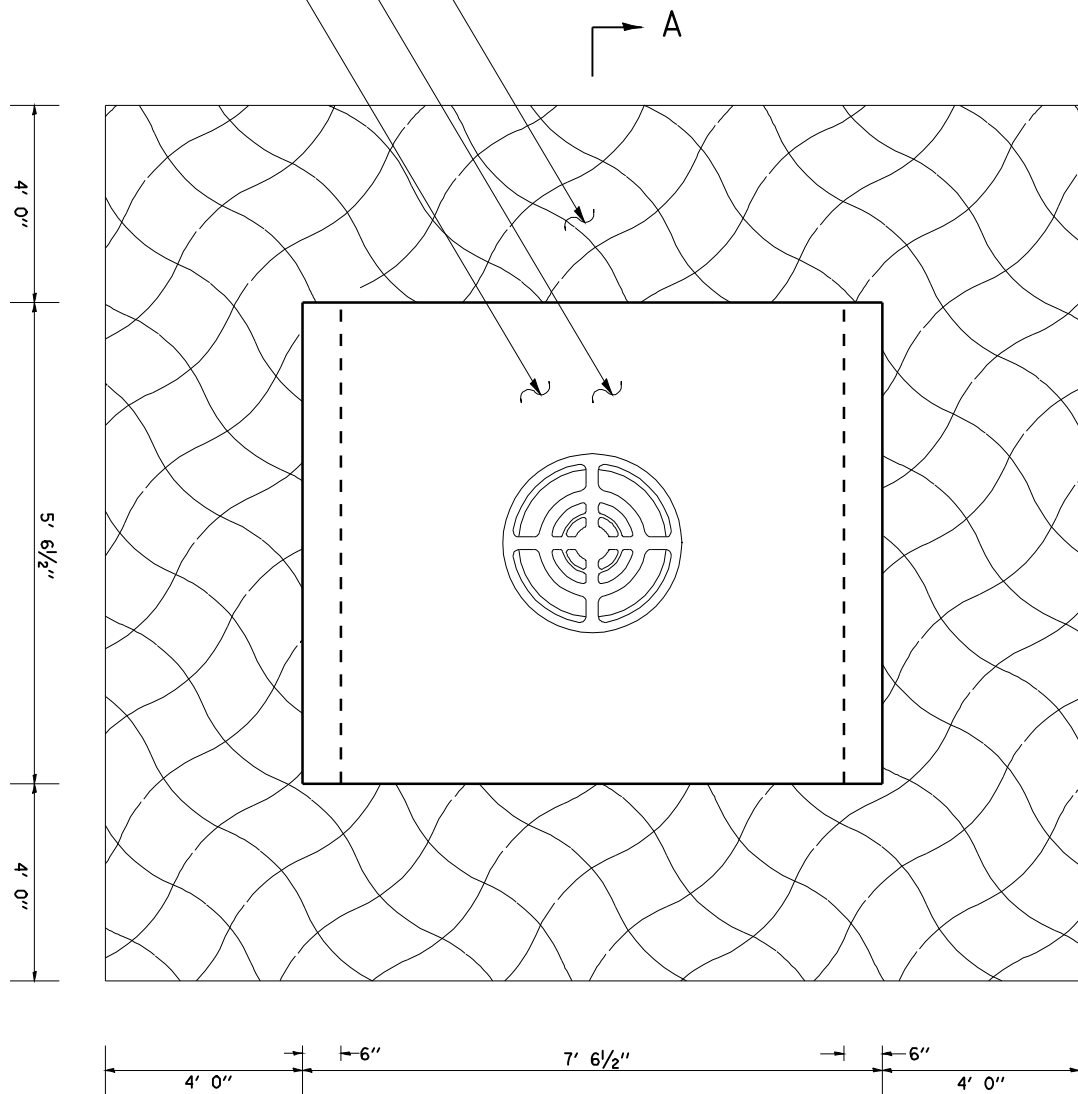
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er\pwork\PWIDOT\COXTE\dms25312\0672480-shs-plan-TR481PM.dgn		DRAWN -	REVISED -
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = Mar-23-2010 03:22:54PM		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FAU 8136 (STAUNTON ROAD) PAVEMENT MARKING PLAN SHEETS			
SCALE: 1" = 50'	SHEET NO.	OF SHEETS	STA. 61+50.00 TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	51
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	

PR EROSION CONTROL BLANKET
 PR CONC APRON
 EX CONC APRON



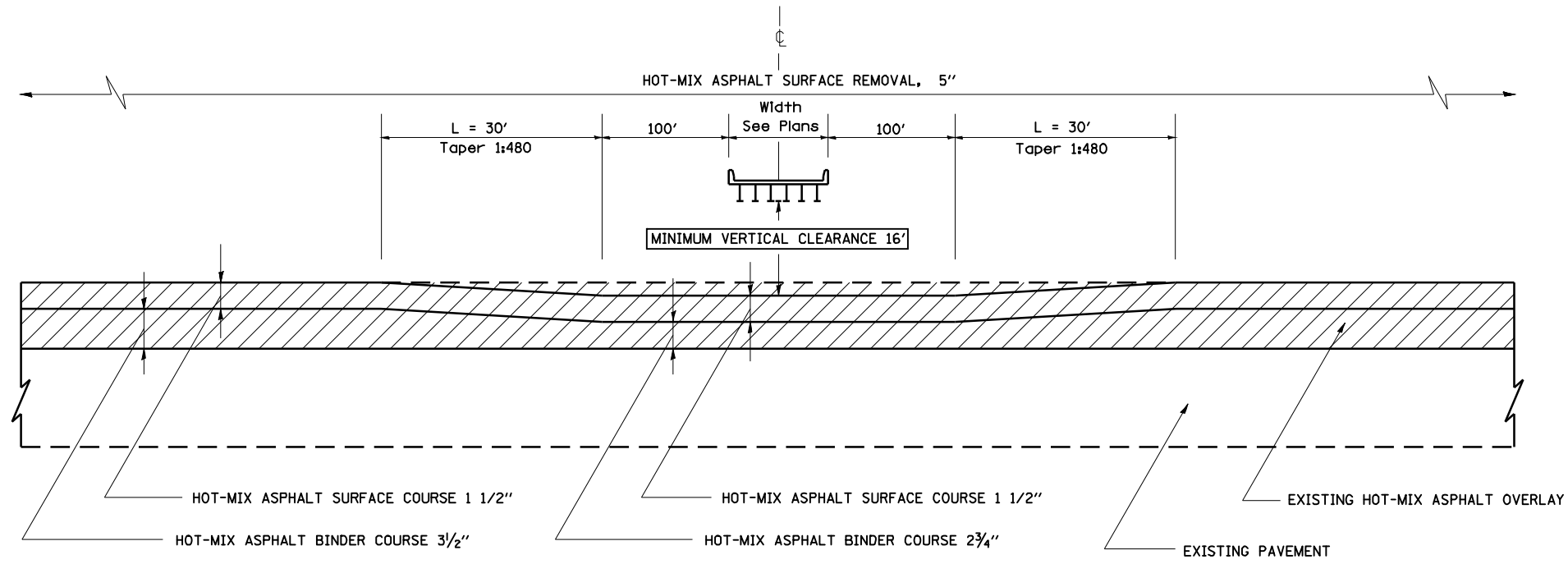
SECTION A-A

REPAIRS TO INLET STANDARD 2250

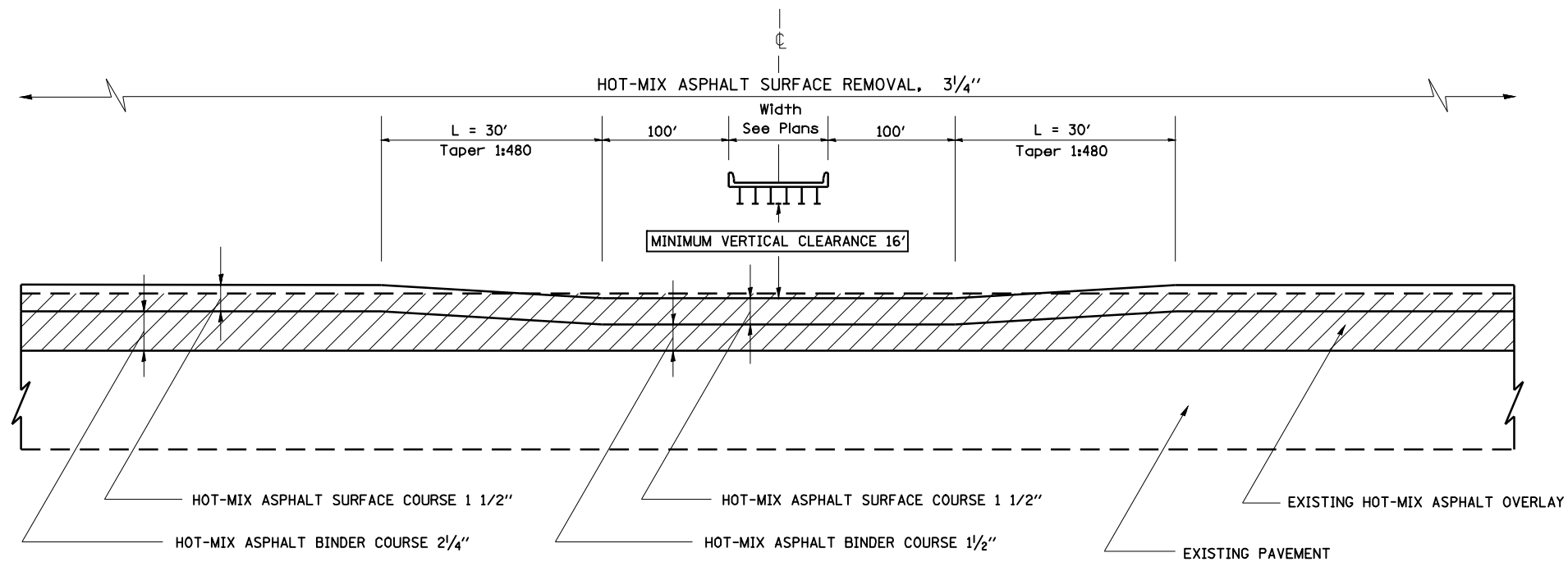
LOCATION	MED INLET (604101) TO BE RECONSTRUCTED (EACH)	EROSION CONTROL BLANKET (SQ YD)	CONTROLLED LOW STRENGTH MATERIAL (CU YD)
MED STA 205+78	2	37.3	3.0
NB STA 205+78	1	18.7	1.5
SB STA 205+78	1	18.7	1.5
TOTAL	4	74.7	6.0

NOTES:

- SEE SEEDING SCHEDULE FOR SEEDING QUANTITIES.
- SEE STANDARD 604101 FOR ADDITIONAL DETAILS NOT SHOWN. THE TOE WALL AT THE LONG END OF THE APRON IS IN ADDITION TO WHAT IS SHOWN IN STANDARD 604101.
- CONTROLLED LOW STRENGTH MATERIAL TO BE USED IF DEEMED NECESSARY BY THE ENGINEER, THE QUANTITY IN THE ABOVE SCHEDULE IS ESTIMATED.



OVERHEAD STRUCTURE RAMPING DETAIL
SN 059-0043 SB ONLY



OVERHEAD STRUCTURE RAMPING DETAIL
SN 059-0042 NB ONLY

NOTES:

HOT-MIX ASPHALT RESURFACING OF THE EXISTING PAVEMENT AND SHOULDERS SHALL BE DONE IN A MANNER THAT MAINTAINS A MINIMUM VERTICAL CLEARANCE OF 16 FEET AT ALL OVERHEAD STRUCTURES. THE MINIMUM VERTICAL CLEARANCE SHALL BE MAINTAINED FROM OUTSIDE TO OUTSIDE OF THE PAVED SHOULDERS.

PRIOR TO THE START OF RESURFACING OPERATIONS, THE CONTRACTOR, IN THE PRESENCE OF THE ENGINEER, SHALL MEASURE AND DOCUMENT THE EXISTING VERTICAL CLEARANCE AT ALL OVERHEAD STRUCTURES. MEASUREMENTS SHALL BE TAKEN AT THE OUTSIDE EDGES OF THE PAVED SHOULDERS, AT THE EDGE OF EACH LANE, AND AT ANY SPLICE PLATES OVER THE SHOULDERS OR PAVEMENT. IF NECESSARY, THE ENGINEER SHALL MAKE ADJUSTMENTS TO THE RESURFACING THICKNESS SHOWN IN THE PLANS TO MAINTAIN THE REQUIRED MINIMUM VERTICAL CLEARANCE.

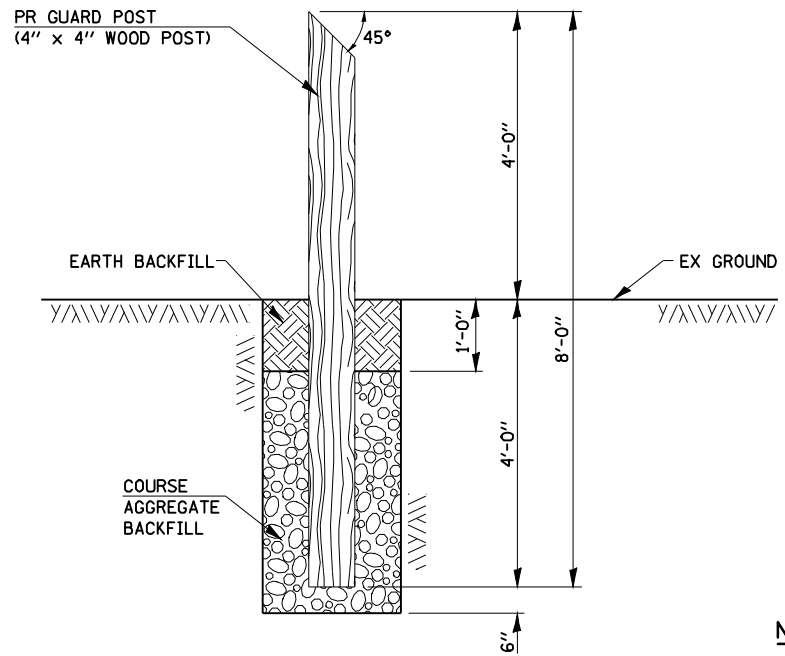
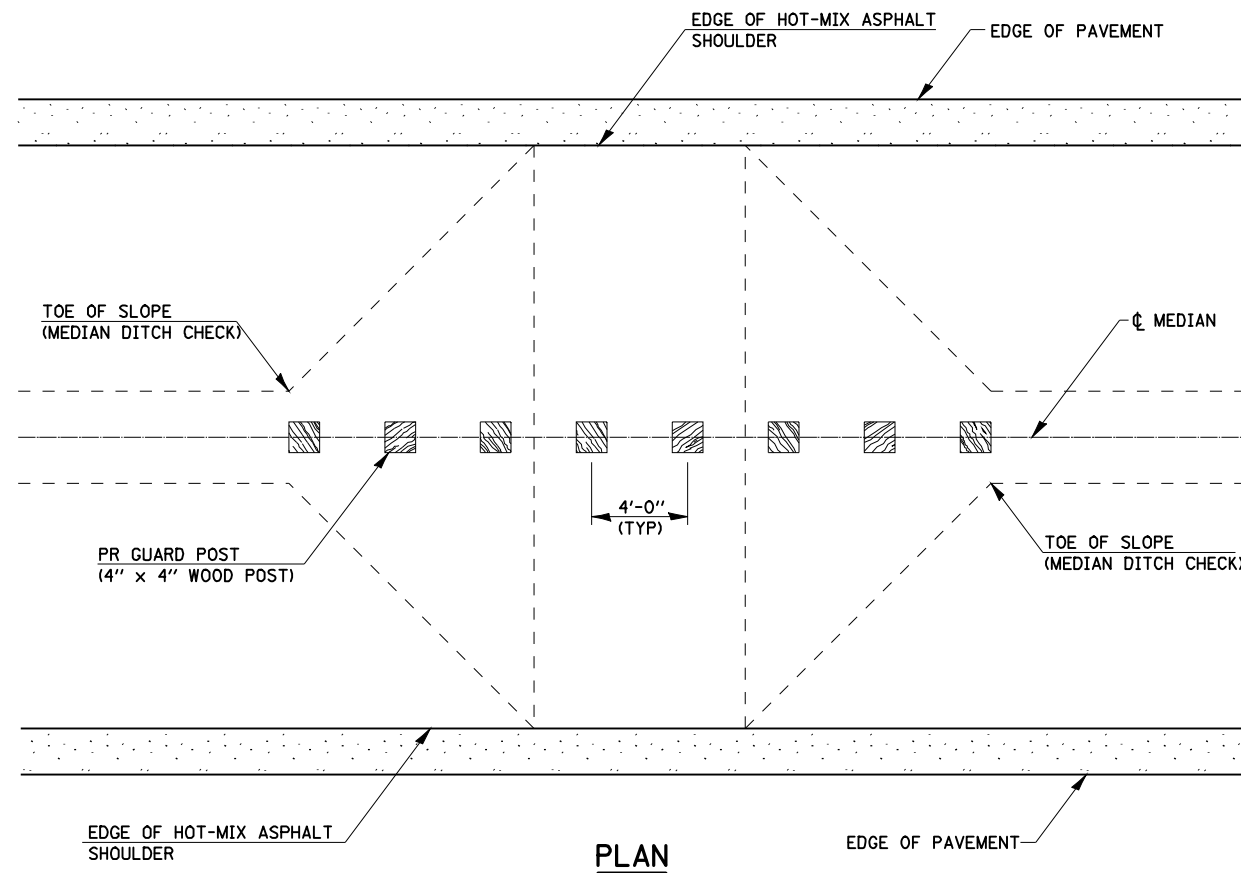
FOLLOWING PLACEMENT OF THE SURFACE COURSE AND HMA SHOULDERS, THE CONTRACTOR, IN THE PRESENCE OF THE ENGINEER, SHALL MEASURE AND DOCUMENT THE VERTICAL CLEARANCE AS DESCRIBED ABOVE. IF THE MINIMUM VERTICAL CLEARANCE IS LESS THAN 16 FEET, THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL REMOVE AND REPLACE THE HMA SURFACE COURSE AS DIRECTED BY THE ENGINEER.

THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS HOT-MIX ASPHALT ITEMS.

HOT-MIX ASPHALT SHOULDERS SHALL BE RAMPED IN THE SAME MANNER AS THE HMA BINDER AND SURFACE COURSES.

 - HOT-MIX ASPHALT SURFACE REMOVAL

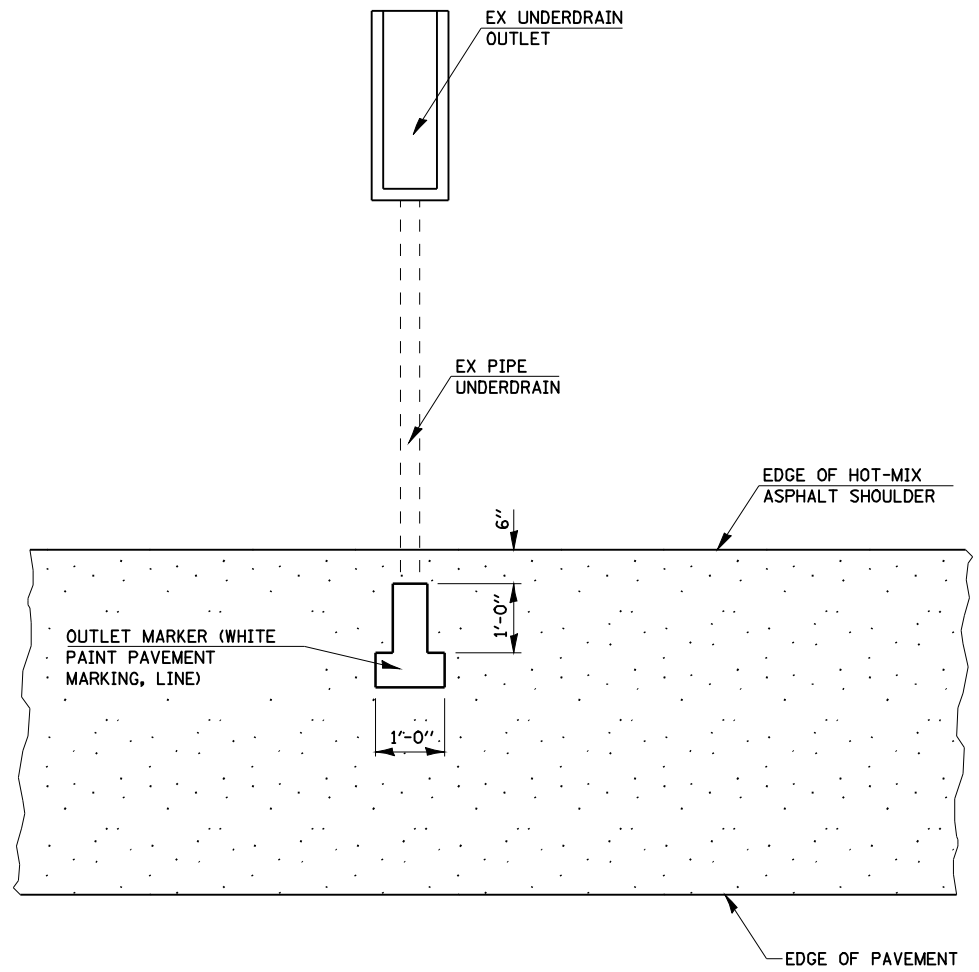
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er:\pwork\PWIDOT\COXTE\dms25312\0672480-Detail.s.dgn		DRAWN -	REVISED -		55	59RS-2, BR	MACOUPIN	100	54			
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 72A60							
PLOT DATE = Mar-23-2010 03:23:02PM		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				



SECTION THRU POST

GUARD POST DETAIL

- NOTES:**
- 1.) PROPOSED GUARDPOSTS USED TO REPLACE MISSING POSTS AT UNAUTHORIZED MEDIAN CROSSOVERS.
 - 2.) ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY FOR INSTALLATION OF GUARD POSTS AS DETAILED HEREIN SHALL BE INCLUDED IN THE COST FOR "GUARD POSTS".



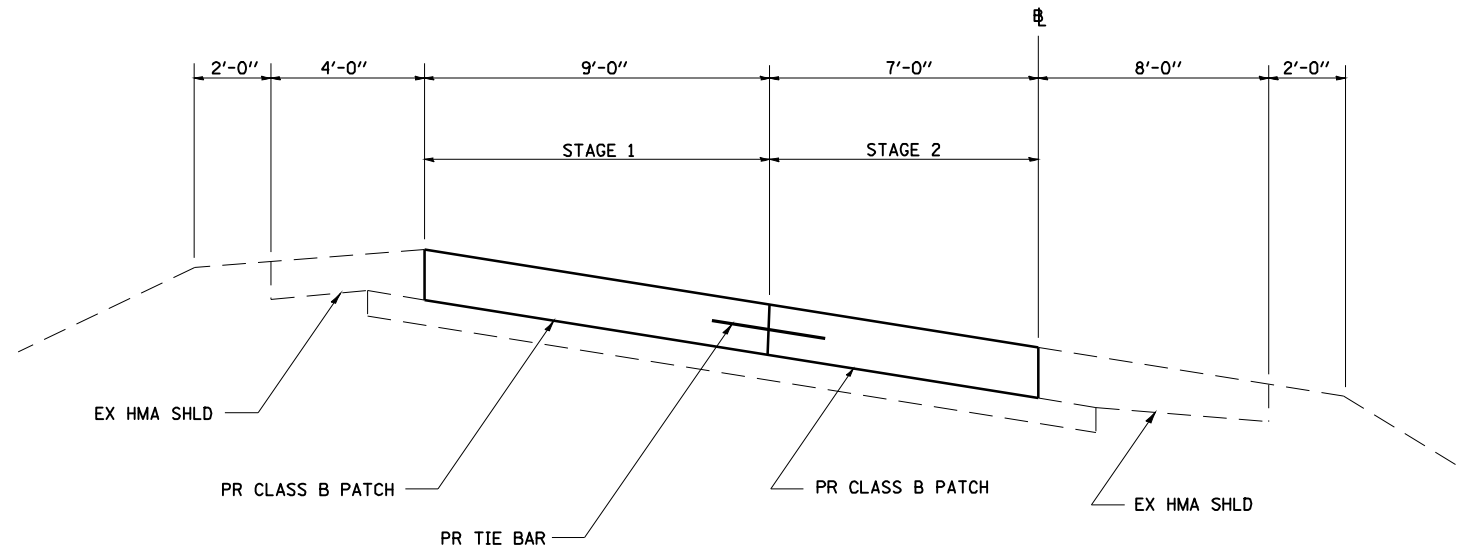
PLAN FOR UNDERDRAIN OUTLET MARKER

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -
er\pwork\PWIDOT\COXTE\dma25312\0672000-Detail.dgn		DRAWN -	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MEDIAN GUARD POST DETAIL AND UNDERDRAIN OUTLET MARKERS			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

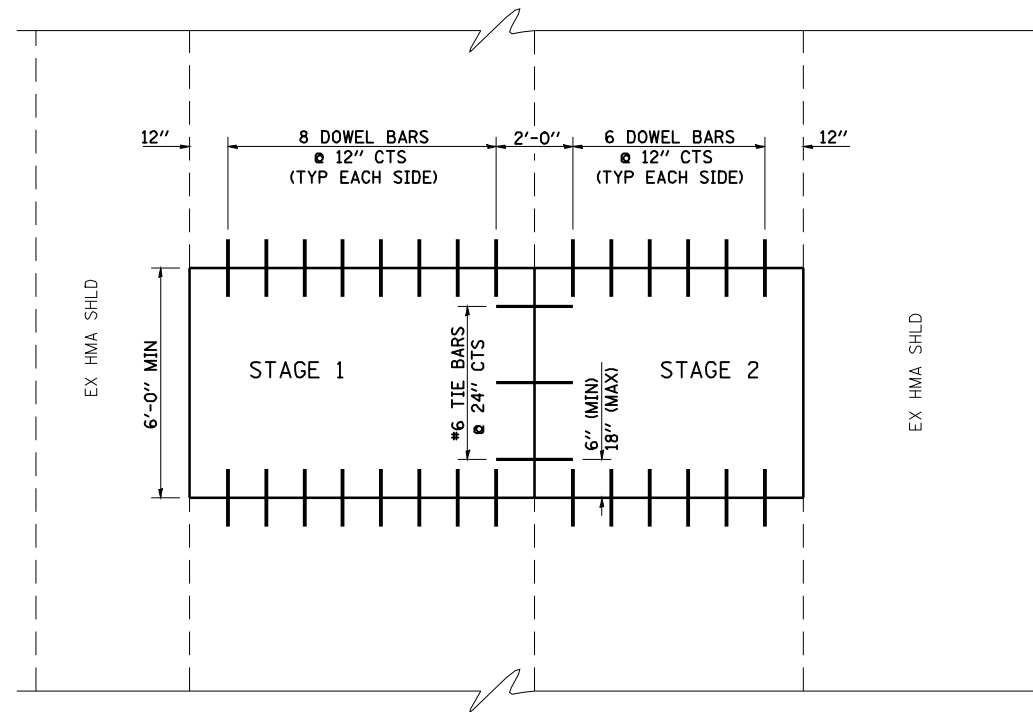
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	55
CONTRACT NO. 72A60				
ILLINOIS FED. AID PROJECT				



TYPICAL SECTION - CLASS B PATCH ON RAMPS

NOTE:

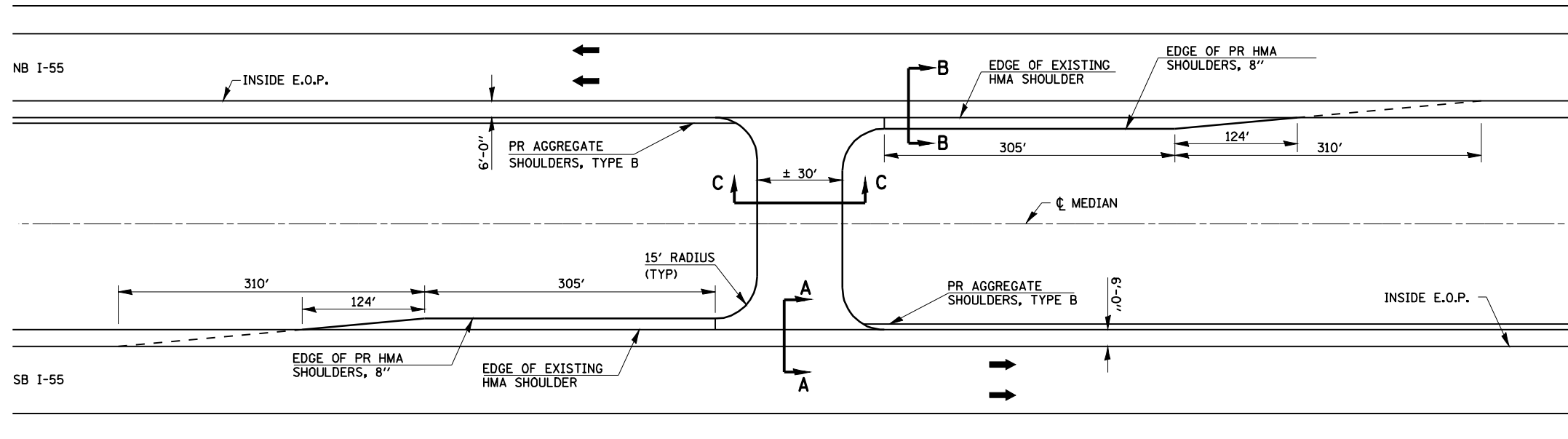
1. THIS DETAIL IS TO BE USED IN CONJUNCTION WITH STANDARD 442101.
2. TIE BARS SHALL BE NO. 6 DEFORMED BARS AT 24" CENTERS WHICH SHALL BE DRILLED AND GROUTED ACCORDING TO ARTICLE 420.05(b) OF THE STANDARD SPECIFICATIONS.



PLAN - CLASS B PATCH ON RAMPS

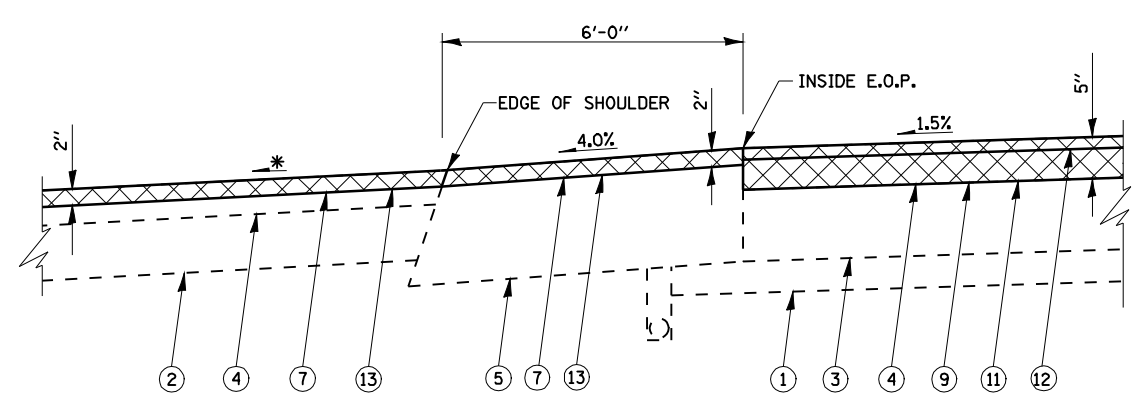
FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CLASS B PATCHING RAMP DETAIL				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
er:\pwork\pwidot\COXTE\dma25312\0672400-Detail.dgn		DRAWN -	REVISED -		55	59RS-2, BR	MACOUPIN	100	56				
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PLOT DATE = Mar-23-2010 03:23:07PM		DATE -	REVISED -		ILLINOIS FED. AID PROJECT								
				SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.			

EMERGENCY MEDIAN CROSSOVERS



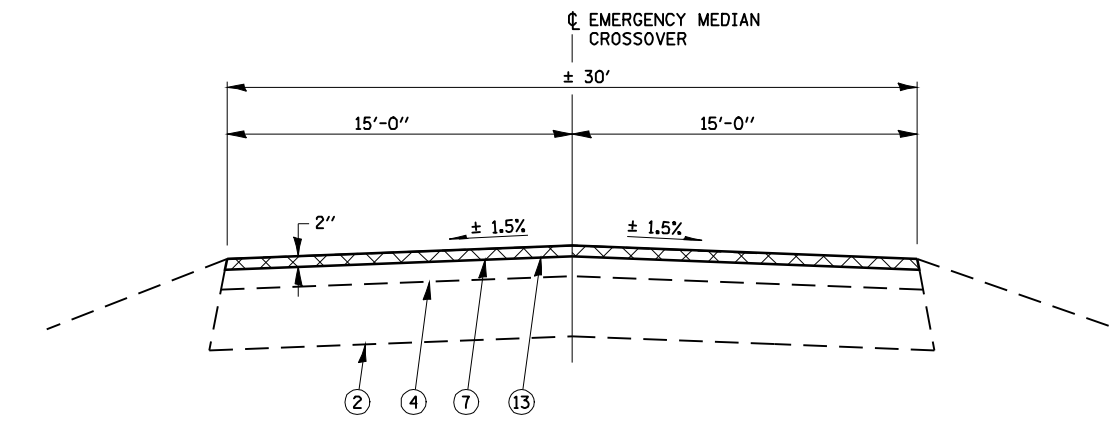
NOTE:
TWO DELINEATORS SHALL BE PLACED IN ADVANCE OF THE MEDIAN CROSSOVERS. ONE SHALL BE AT THE CROSSOVER, THE OTHER 800 FT IN ADVANCE OF THE FIRST. THE DELINEATOR AT THE CROSSOVER SHALL HAVE TWO REFLECTORS, AND THE DELINEATOR 800 FT IN ADVANCE SHALL HAVE THREE REFLECTORS

PLAN VIEW



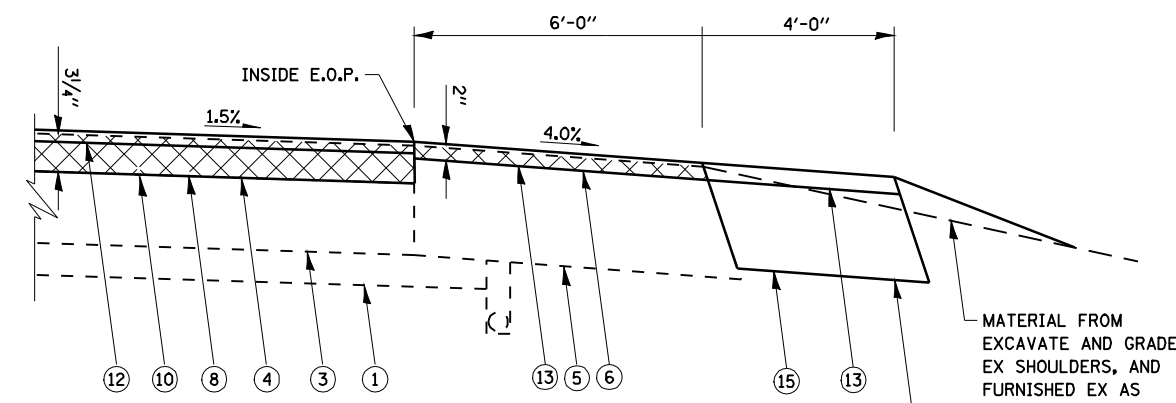
* - MAINTAIN SLOPES AWAY FROM MAINLINE PAVEMENT TO ENSURE PROPER DRAINAGE INTO MEDIAN DITCHES.

SECTION A - A



SECTION C - C

LEGEND	
①	EX AGGREGATE SUB-BASE, 4"
②	EX AGGREGATE BASE COURSE
③	EX P.C.C. PAVEMENT
④	EX HOT-MIX ASPHALT OVERLAY
⑤	EX HOT-MIX ASPHALT SHOULDER
⑥	PR HOT-MIX ASPHALT SURFACE REMOVAL 1/2"
⑦	PR HOT-MIX ASPHALT SURFACE REMOVAL 2"
⑧	PR HOT-MIX ASPHALT SURFACE REMOVAL 3/4"
⑨	PR HOT-MIX ASPHALT SURFACE REMOVAL 5"
⑩	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 2 1/4"
⑪	PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE 3 1/2"
⑫	PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
⑬	PR HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 2"
⑭	PR AGGREGATE SHOULDERS, TYPE B
⑮	PR HOT-MIX ASPHALT SHOULDERS, 8"

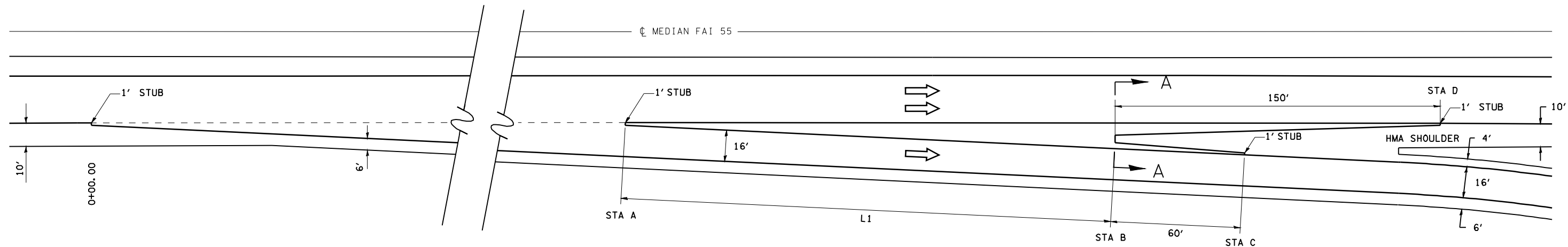


SECTION B - B

* - MAINTAIN SLOPES AWAY FROM MAINLINE PAVEMENT TO ENSURE PROPER DRAINAGE INTO MEDIAN DITCHES.

EMERGENCY CROSSOVER IMPROVEMENT

LOCATION	HMA SURF	EXCAV & GR	HMA	HMA SURF CSE
	REM 2"	EX SHLDS	SHOULDERS 8"	MIX C N50
	SQ YD	UNIT	SQ YD	TON
I-55 STA 705+75	274.79	8.6	381.34	30.78
I-55 STA 245+30	274.79	8.6	381.34	30.78
TOTAL	549.38	17.2	762.68	61.56



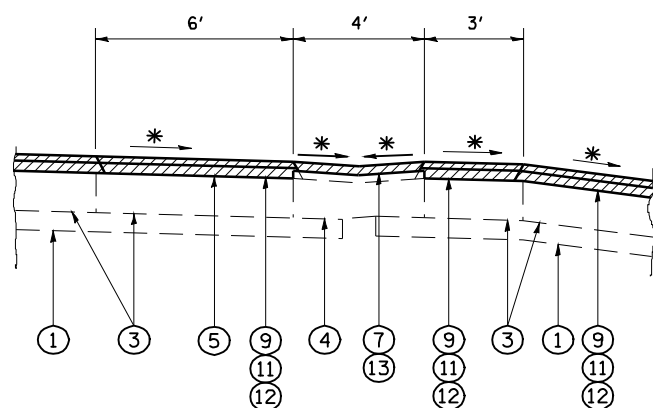
PLAN - EXIT TERMINAL

INTERCHANGE	RAMP	ADJACENT MAINLINE	STA A		STA B		STA C	STA D	L1
			RAMP	MAINLINE	RAMP	MAINLINE	RAMP	MAINLINE	(FEET)
STAUNTON RD	A	SB I-55	3+07	796+85	5+33	799+14	5+93	800+64	226
STAUNTON RD	C	NB I-55	3+03	181+93	5+30	179+66	5+90	178+16	227

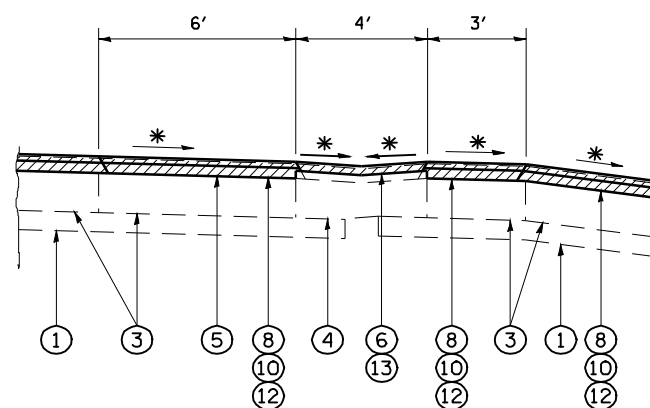
LEGEND

- 1 EX STABILIZED SUB-BASE
- 2 EX PCC BASE COURSE
- 3 EX PCC PAVEMENT
- 4 EX HOT-MIX ASPHALT SHOULDER
- 5 EX HOT-MIX ASPHALT
- 6 PR HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2"
- 7 PR HOT-MIX ASPHALT SURFACE REMOVAL 2"
- 8 PR HOT-MIX ASPHALT SURFACE REMOVAL 3 1/4"
- 9 PR HOT-MIX ASPHALT SURFACE REMOVAL 5"
- 10 PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105 2 1/4"
- 11 PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105 3 1/2"
- 12 PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N105 1 1/2"
- 13 PR HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50 2"

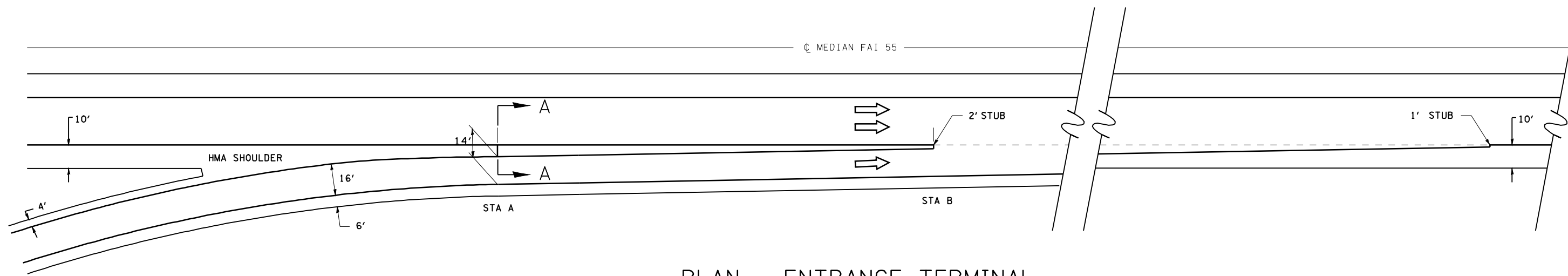
* MATCH EXIST SLOPE



SECTION A-A
(RAMP A)



SECTION A-A
(RAMP C)



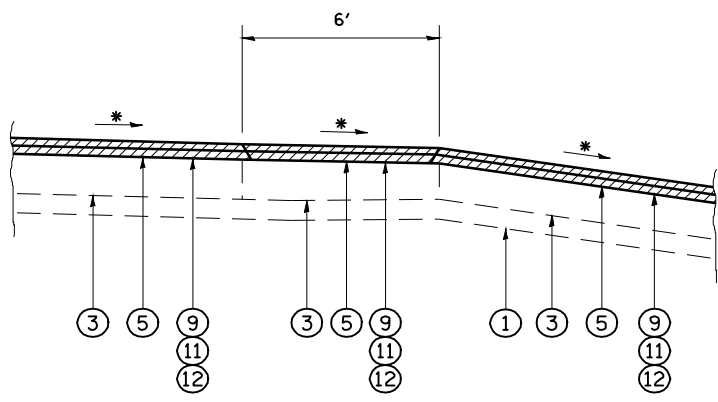
PLAN - ENTRANCE TERMINAL

INTERCHANGE	RAMP	ADJACENT MAINLINE	STA A		STA B	
			RAMP	MAINLINE	RAMP	MAINLINE
STAUNTON RD	B	SB I-55	17+46	179+63	19+46	181+63
STAUNTON RD	D	NB I-55	16+91	800+09	18+24	798+78

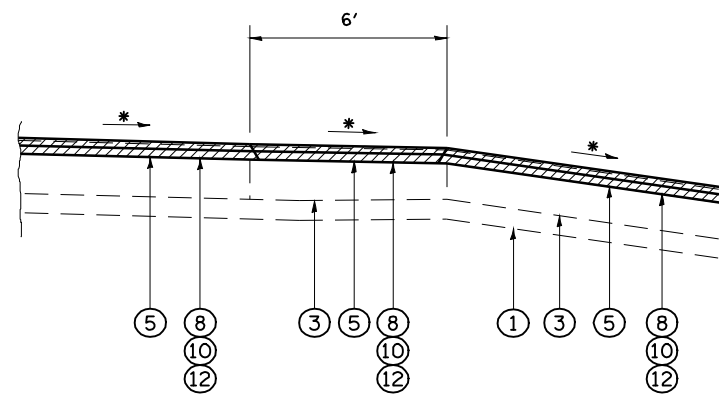
LEGEND

- 1 EX STABILIZED SUB-BASE
- 2 EX PCC BASE COURSE
- 3 EX PCC PAVEMENT
- 4 EX HOT-MIX ASPHALT SHOULDER
- 5 EX HOT-MIX ASPHALT
- 6 PR HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2"
- 7 PR HOT-MIX ASPHALT SURFACE REMOVAL 2"
- 8 PR HOT-MIX ASPHALT SURFACE REMOVAL 3 1/4"
- 9 PR HOT-MIX ASPHALT SURFACE REMOVAL 5"
- 10 PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105 2 1/4"
- 11 PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105 3 1/2"
- 12 PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N105 1 1/2"
- 13 PR HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50 2"

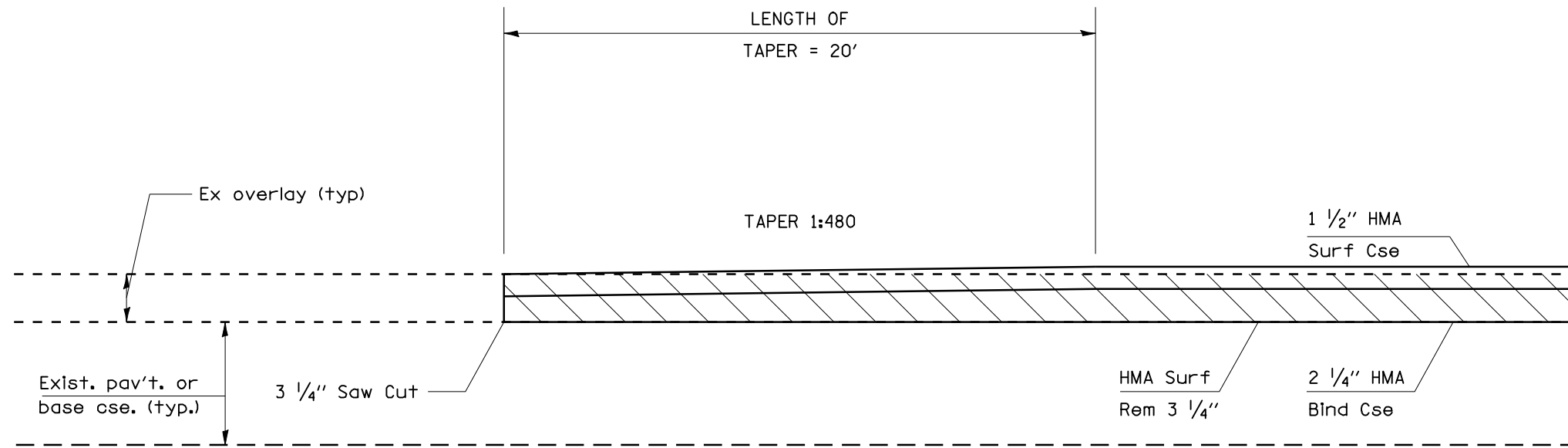
* MATCH EXIST SLOPE



SECTION A-A
(RAMP B)

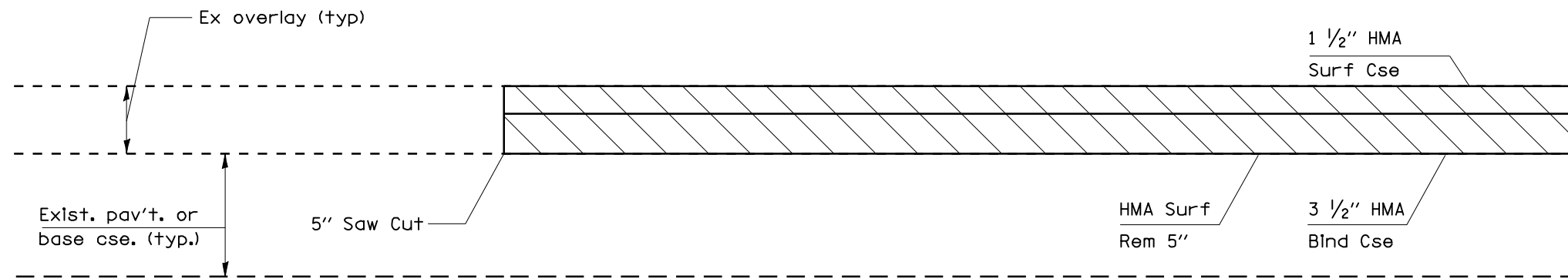


SECTION A-A
(RAMP D)



Detail A

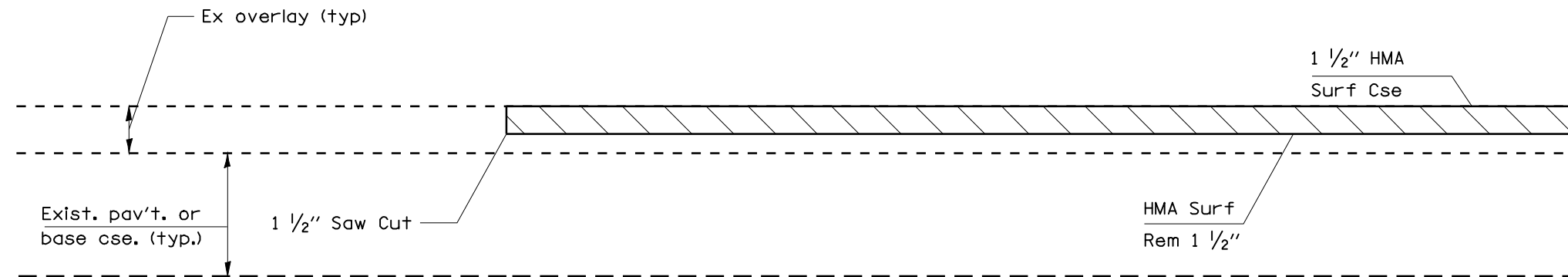
I-55 NB STA 697+80 TO 698+00
 I-55 NB STA 245+80 TO 250+00



Detail B

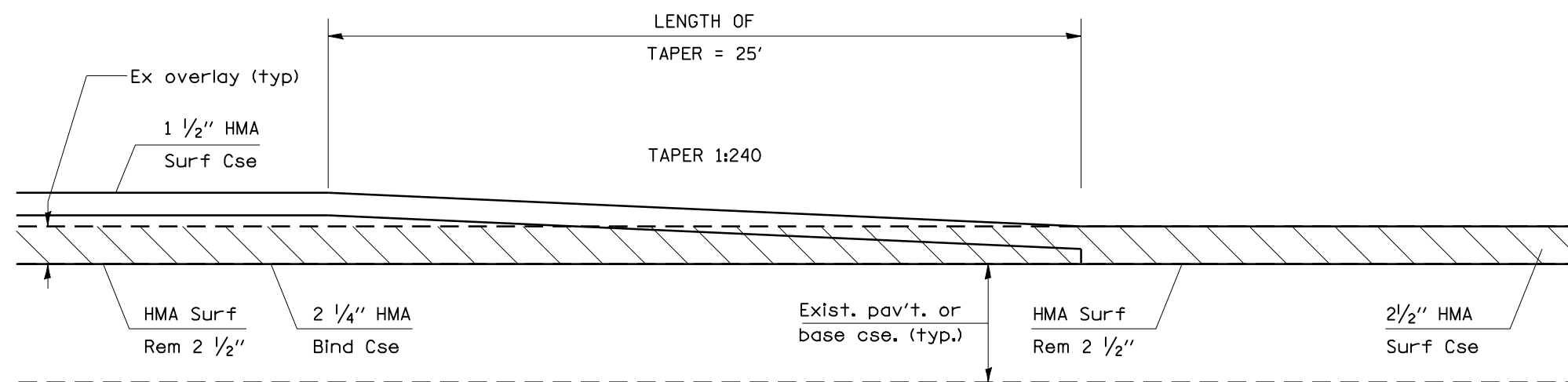
I-55 SB STA 698+00
 I-55 SB STA 250+00

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND TRANSITION DETAILS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = Mar-23-2010 03:23:17PM		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
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Detail C

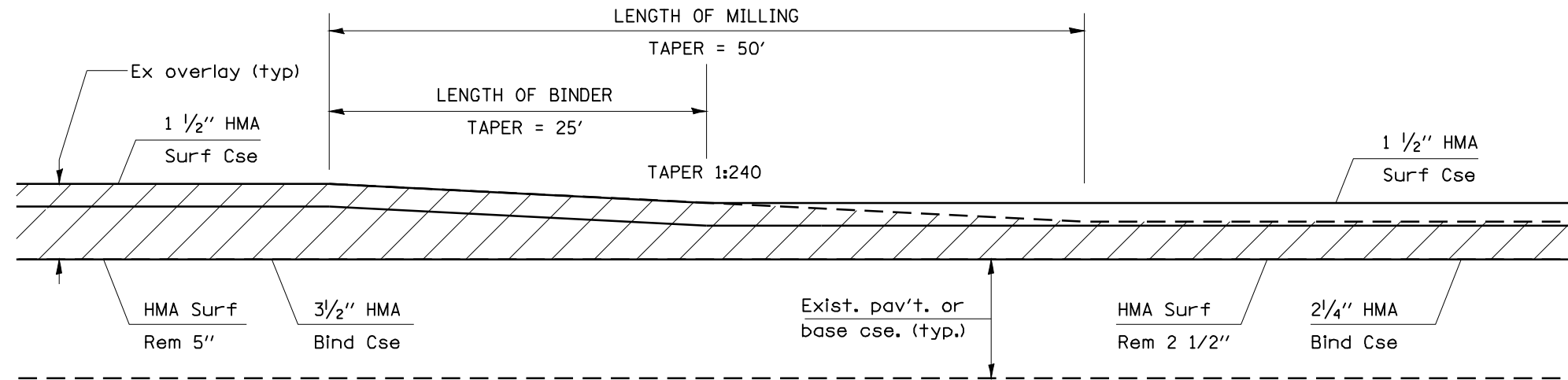
STAUNTON ROAD STA 105+66
 STAUNTON ROAD STA 48+74
 STAUNTON ROAD STA 51+26
 STAUNTON ROAD STA 76+85



Detail D

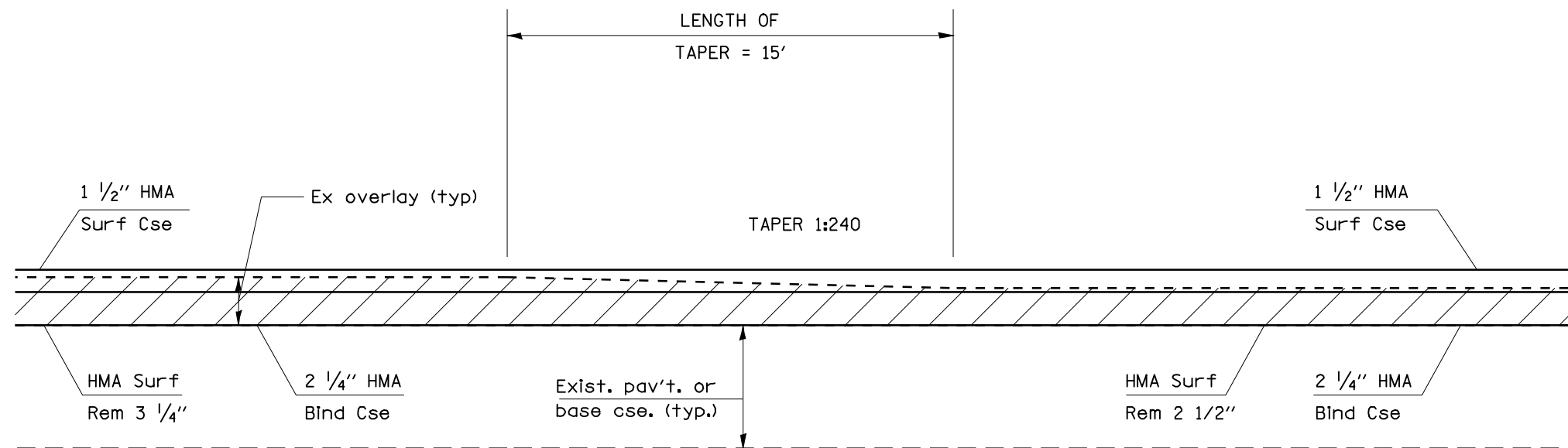
STAUNTON INTERCHANGE RAMP A STA 20+00.00 TO 20+25.00
 STAUNTON INTERCHANGE RAMP B STA 1+75.00 TO 2+00.00
 STAUNTON INTERCHANGE RAMP C STA 21+00.00 TO 21+25.00
 STAUNTON INTERCHANGE RAMP D STA 1+50.00 TO 1+75.00

FILE NAME =	USER NAME = coxte	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND TRANSITION DETAILS				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Detail E

STAUNTON INTERCHANGE RAMP A STA 5+92.22 TO STA 6+17.22 *
 STAUNTON INTERCHANGE RAMP B STA 15+13.14 TO STA 15+38.14 *



Detail F

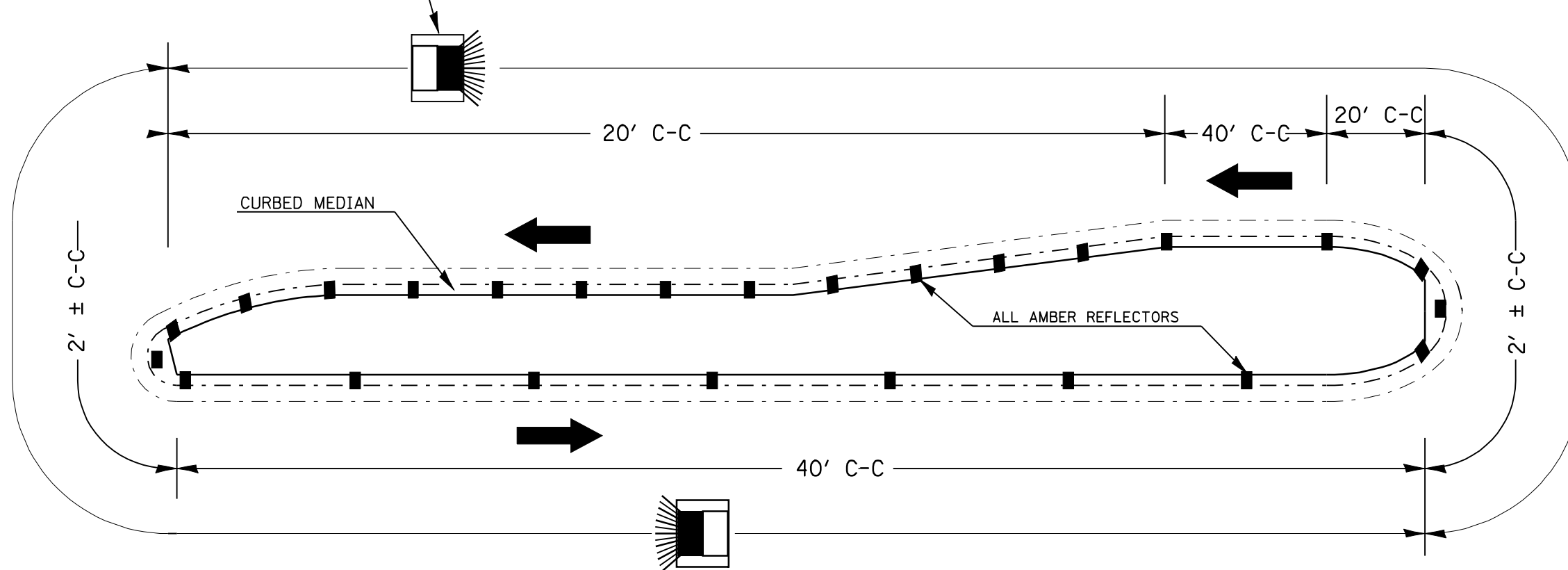
STAUNTON INTERCHANGE RAMP C STA 5+35 TO STA 5+50 *
 STAUNTON INTERCHANGE RAMP D STA 15+87 TO STA 16+02 *

* TRANSITION STATIONS ARE BASED UPON THE RESURFACING TRANSITION FROM THE MOST RECENT RESURFACING, ACTUAL CONDITIONS MAY VARY. THE INTENT IS FOR ALL EX HMA ON THE PAVEMENT TO BE MILLED OFF.

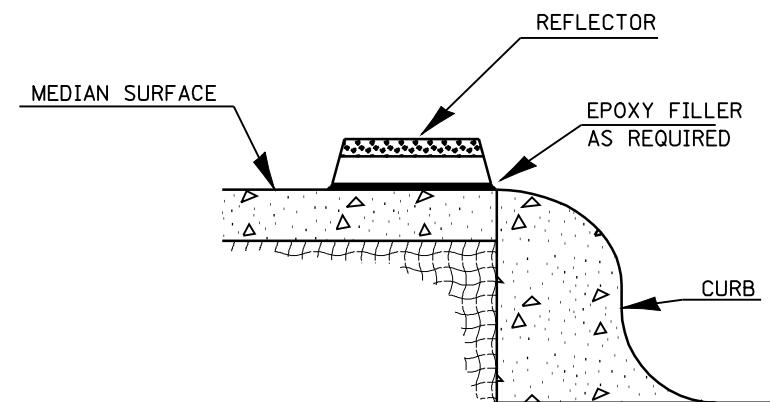
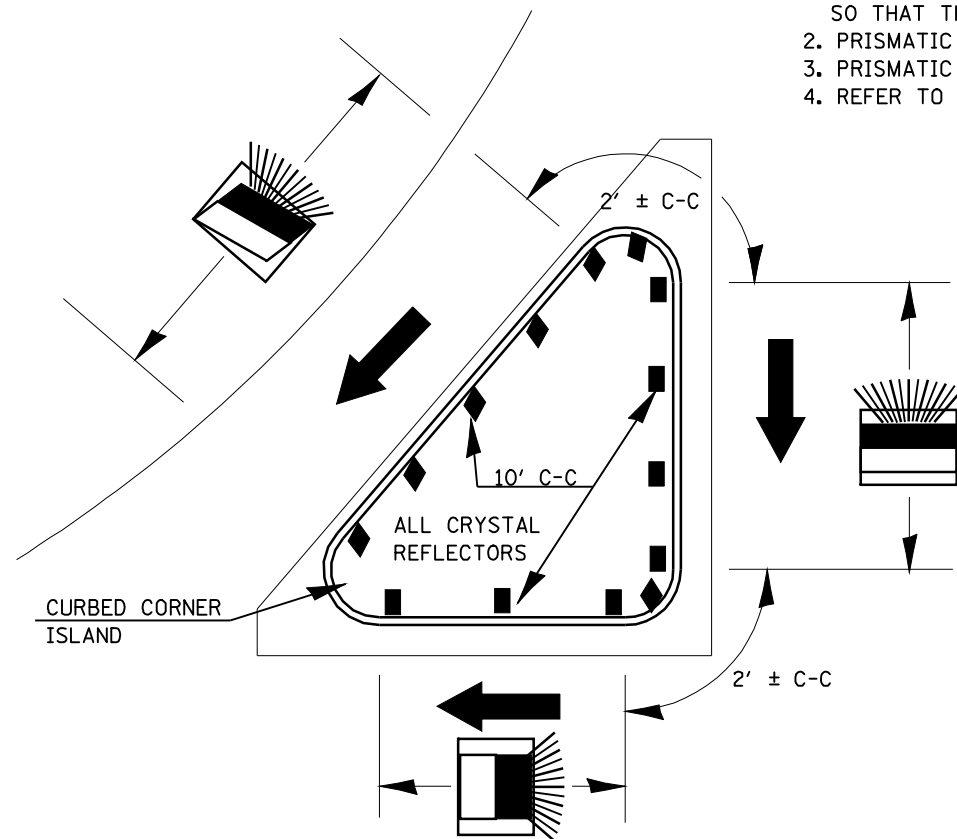
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er\pwwork\PWIDOT\COXTE\dma25312\0672000-Details.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	55	59RS-2, BR	MACOUPIN	100 62
PLOT SCALE = 100.0000' / IN.		CHECKED -	REVISED -						CONTRACT NO. 72A60					
PLOT DATE = Mar-23-2010 03:23:22PM		DATE -	REVISED -						ILLINOIS FED. AID PROJECT					

TYPICAL PLACEMENT OF PRISMATIC REFLECTORS ON CURBS

MARKER ORIENTATION WITHIN LIMITS SHOWN

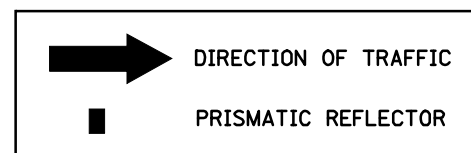


1. PRISMATIC REFLECTORS SHALL BE MONO-DIRECTIONAL AND POSITIONED SO THAT THE REFLECTIVE FACE IS FACING THE APPROACHING TRAFFIC.
2. PRISMATIC REFLECTORS SHALL BE SECURED IN PLACE WITH AN EPOXY ADHESIVE.
3. PRISMATIC REFLECTORS SHALL BE EITHER AMBER OR CRYSTAL IN COLOR.
4. REFER TO SCHEDULES FOR PRISMATIC REFLECTOR QUANTITIES.



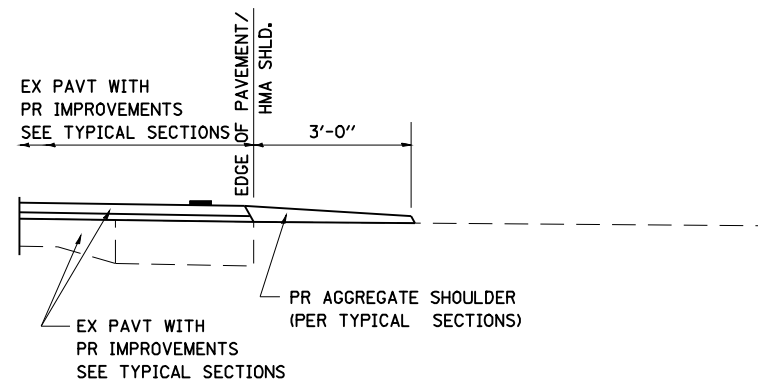
SECTION VIEW

LEGEND

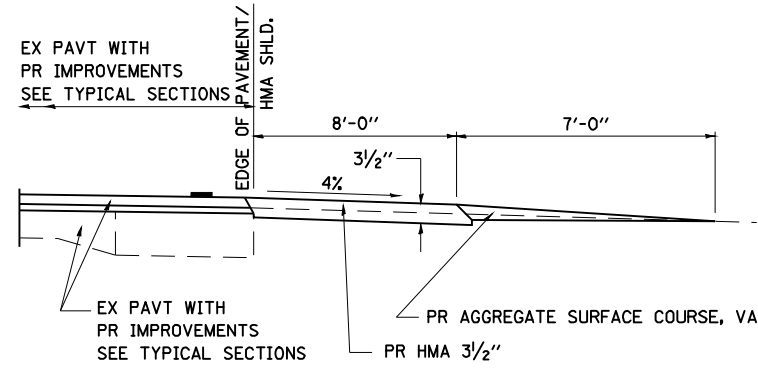


NOT TO SCALE

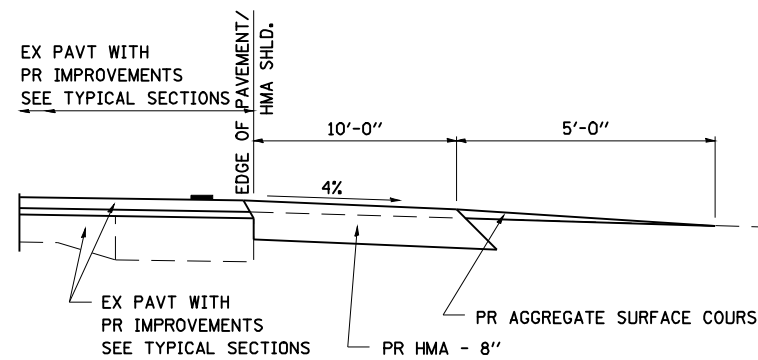
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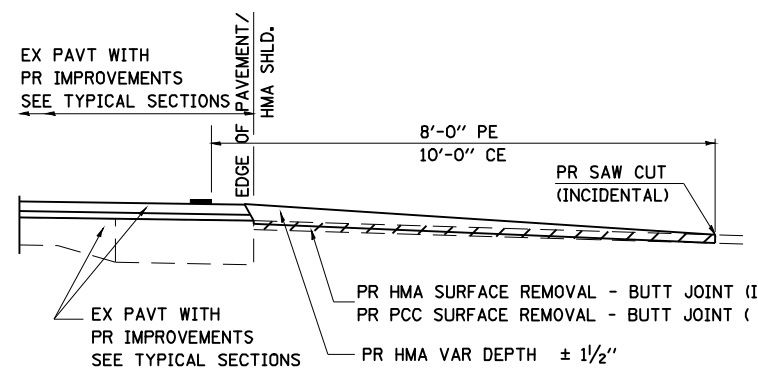
SECTION A-A FOR EX EARTH/ AGGREGATE FE



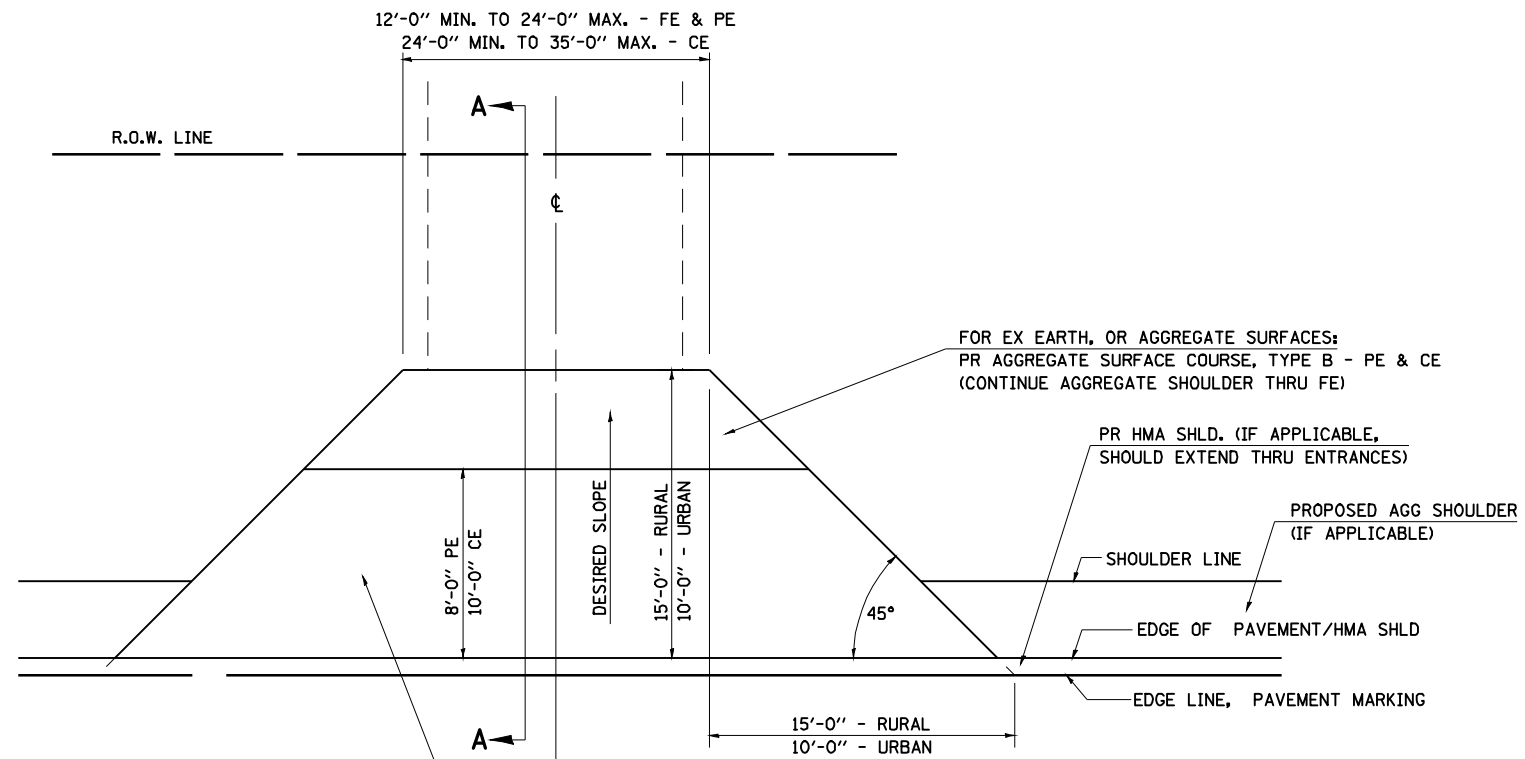
SECTION A-A FOR EX EARTH/AGGREGATE PE



SECTION A-A FOR EX EARTH/AGGREGATE CE & SIDE ROAD



SECTION A-A FOR EX HMA/ PC CONCRETE PE, CE & SIDE ROAD



FOR EX EARTH OR AGGREGATE SURFACES:
 PR HMA SURFACE REMOVAL (IF APPLICABLE)
 PR AGGREGATE SHOULDER THRU - FE
 PR HMA CONCRETE 3 1/2" - PE
 PR HMA CONCRETE 8" - CE

FOR EX HMA CONCRETE SURFACES:
 PR HMA SURFACE REMOVAL-BUTT JOINT

FOR EX PCC SURFACES:
 PR PCC SURFACE REMOVAL-BUTT JOINT

GENERAL NOTES:

THE RESIDENT ENGINEER WILL DETERMINE THE EXACT TYPE OF IMPROVEMENT TO BE COMPLETED FOR ALL ENTRANCES, SIDEROADS AND MAILBOX TURNOUTS ON THIS PROJECT.

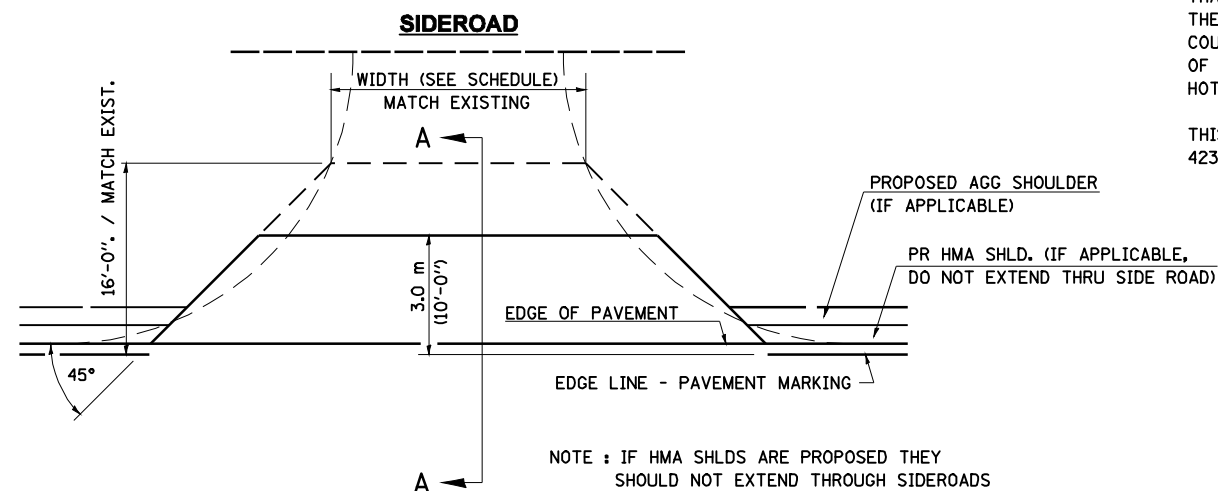
THE PLAN DETAILS AND SCHEDULES SHOULD BE USED AS A GUIDE FOR THE ENGINEER TO IMPLEMENT THE FINAL DESIGN. THE ENGINEER MAY DECIDE TO SALVAGE PORTIONS OF THE EXISTING ENTRANCE PAVEMENT STRUCTURE; THEREFORE, REDUCING PAY ITEM QUANTITIES. NO ADDITIONAL PAYMENT WILL BE ALLOWED FOR THIS REDUCTION IN QUANTITIES.

ANY WORK THE ENGINEER REQUIRES WHICH IS NOT COVERED BY A PAY ITEM CONTAINED IN THE PLANS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

HOT-MIX ASPHALT REQUIRED TO CONSTRUCT THE ENTRANCES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 406 AND 408 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.

WHEN THE HOT-MIX ASPHALT PROPOSED FOR THE IMPROVEMENT IS THICKER THAN 75 mm (3 INCHES) AND REQUIRES PLACEMENT IN MORE THAN ONE LIFT. THE BOTTOM LIFT(S) SHALL MEET THE REQUIREMENTS OF HOT-MIX ASPHALT BASE COURSE IN SECTION 406 OF THE STANDARD SPECIFICATIONS AND THE TOP LIFT OF 50 mm (2 INCHES) SHALL MEET THE REQUIREMENTS OF INCIDENTAL HOT-MIX ASPHALT SURFACING.

THIS WORK WILL BE PAID FOR IN ACCORDANCE WITH SECTIONS 351, 358, 408, 423 AND 440 OF THE STANDARD SPECIFICATIONS.



NOTE : IF HMA SHLDS ARE PROPOSED THEY SHOULD NOT EXTEND THROUGH SIDEROADS

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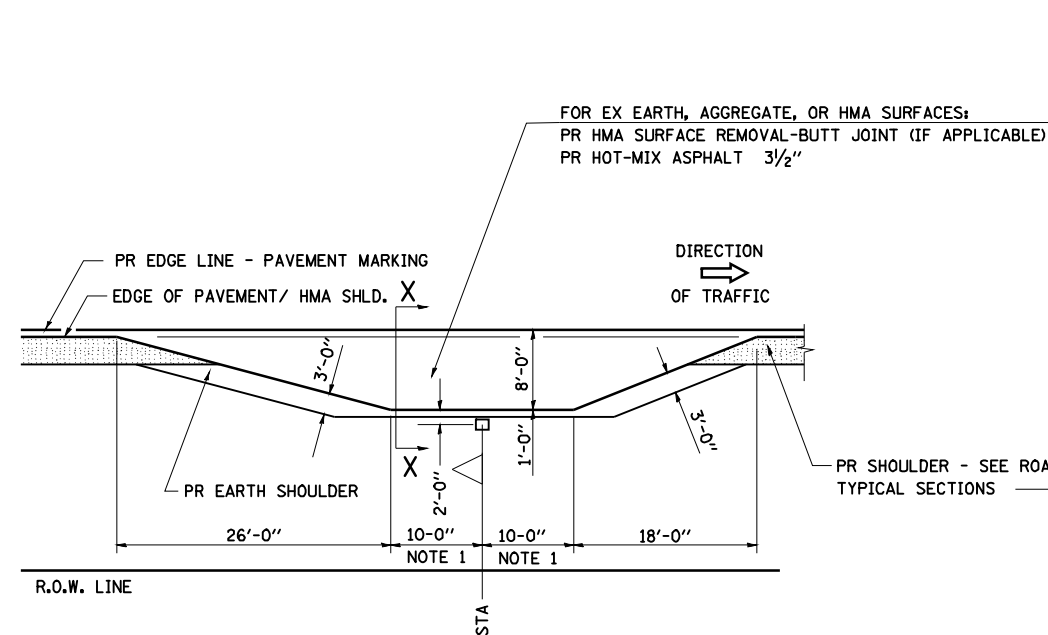
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DIST. 6 DETAILS FOR RURAL/URBAN ENT., MAILBOX
 TURNOUT & SIDEROADS W/O CONC. GUTTER (3P-PROJ.)**

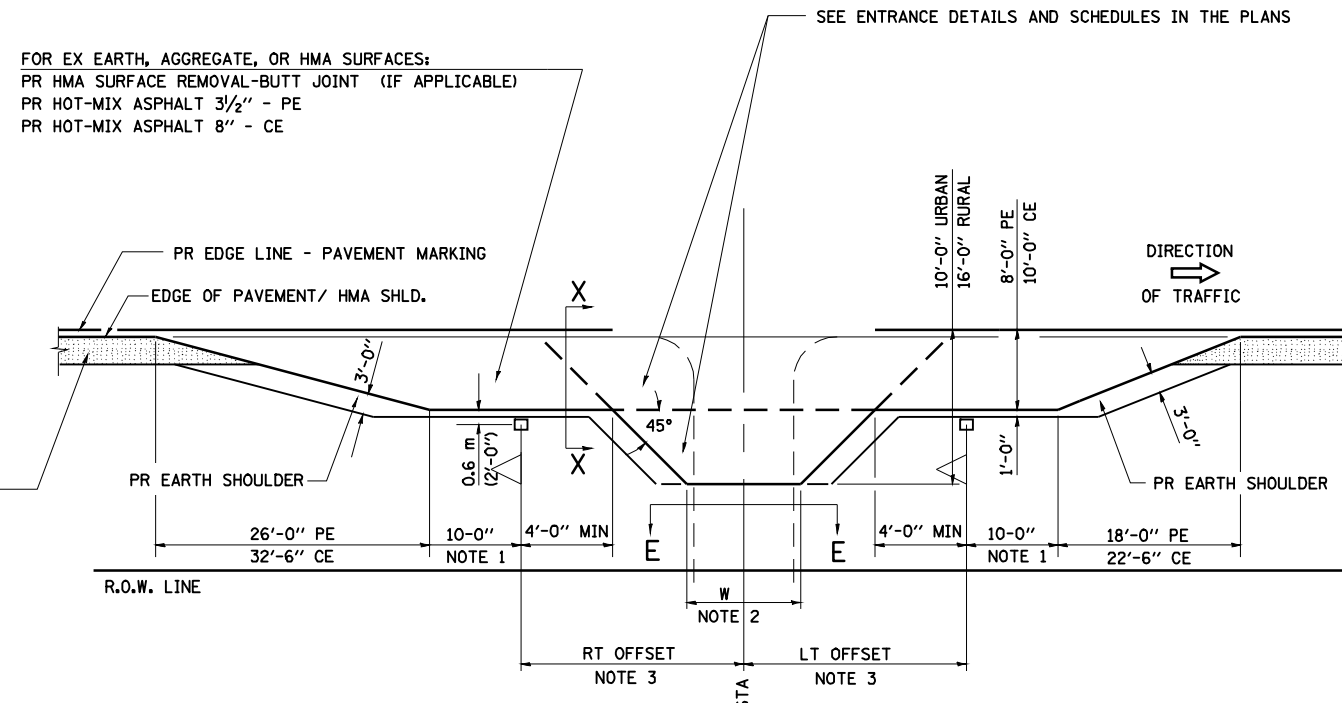
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	64
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	

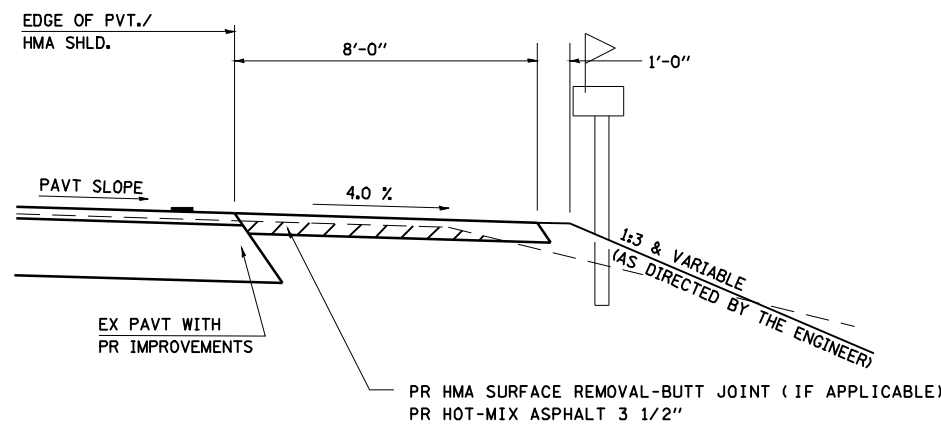
DETAILS OF MAILBOX TURNOUTS



PLAN - MAILBOX TURNOUTS

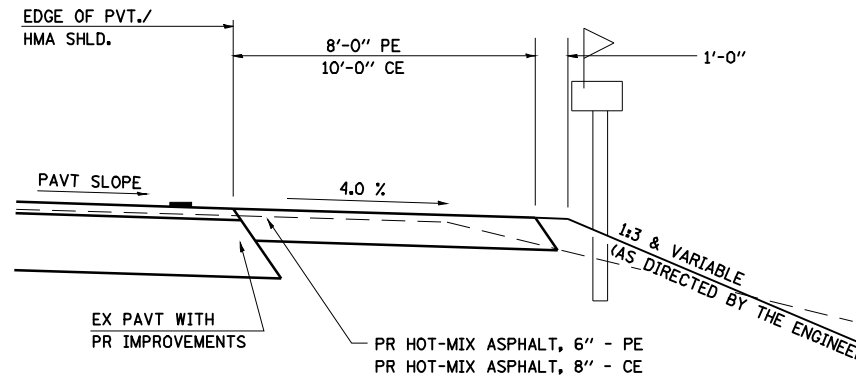


PLAN - COMBINED MAILBOX TURNOUT WITH TRAILING OR LEADING ENTRANCE



**SECTION X-X THRU MAILBOX TURNOUT
ALSO APPLIES TO MAILBOX TURNOUTS COMBINED WITH
EX EARTH, AGGREGATE, OR HMA PE & FE**

(DETAIL APPLIES WHEN M.B. TURNOUT DOES NOT EXIST.
IF EXISTING, TREAT SAME AS ENTRANCE.)



**SECTION X-X THRU MAILBOX TURNOUT
COMBINED WITH EX HOT-MIX ASPHALT & PC CONC PE & CE**

(DETAIL APPLIES WHEN M.B. TURNOUT DOES NOT EXIST.
IF EXISTING, TREAT SAME AS ENTRANCE.)

- NOTE 1 IF MORE THAN ONE MAILBOX IS PRESENT, DIMENSION FROM CENTER OF END MAILBOX.

NOTE 2 FOR ENTRANCE LAYOUT DIMENSIONS AND SECTIONS A-A & E-E REFER TO THE SCHEDULES IN THE PLANS.

NOTE 3 BOTH LT OR RT OFFSETS FOR MAILBOX SHOWN USE OFFSET DIMENSION PER SCHEDULE AND REFER TO LAYOUT SHOWN ON THE PLAN.

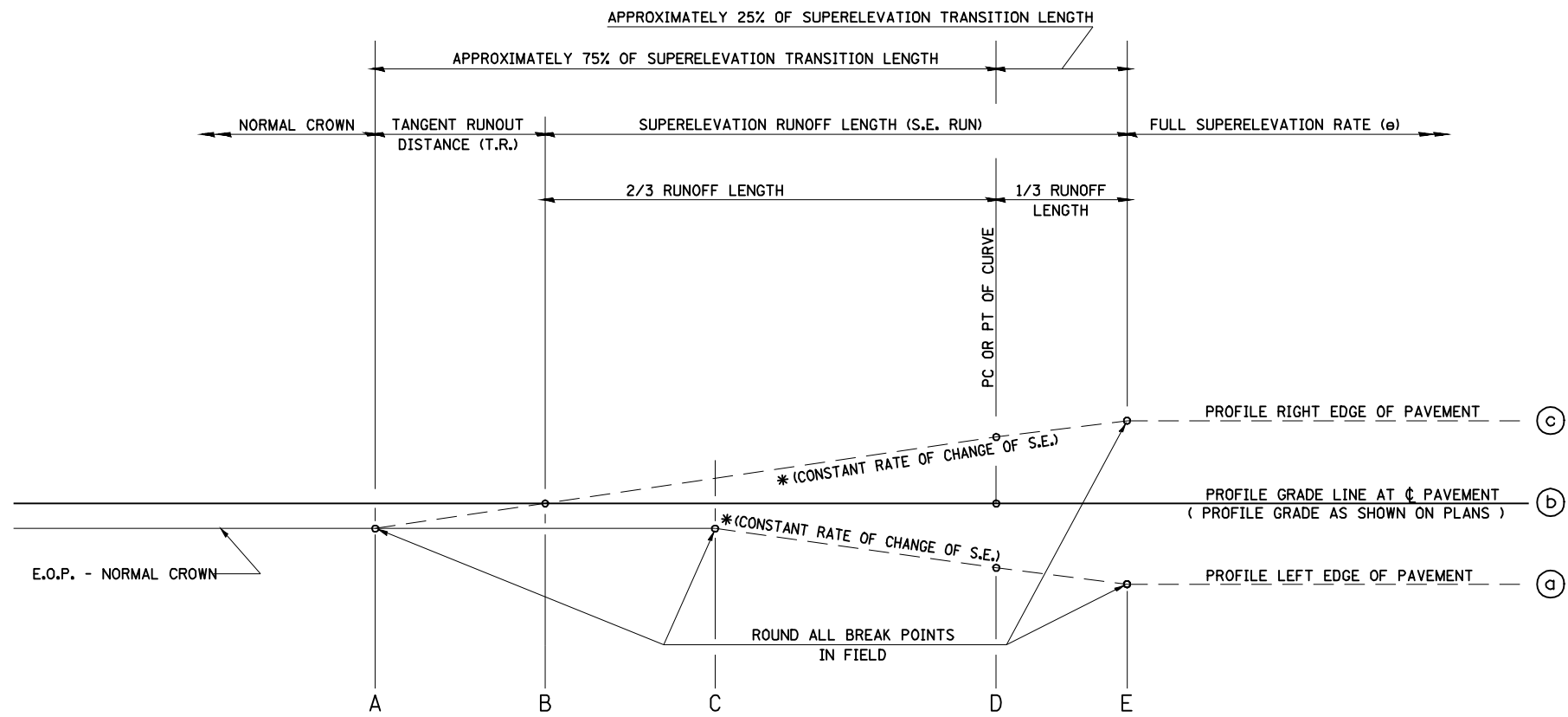
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DIST. 6 DETAILS FOR RURAL/URBAN ENT., MAILBOX TURNOUT & SIDEROADS W/O CONC. GUTTER (3P-PROJ.)			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	65
CONTRACT NO. 72A60				
ILLINOIS FED. AID PROJECT				

ENTRANCE IMPROVEMENT SCHEDULE FOR RURAL / URBAN "PPP" PROJECTS																	
LOCATION	TYPE OF ENTRANCE	EX MATERIAL TYPE	WIDTH 1	WIDTH 2	WIDTH 3	WIDTH 4	LENGTH (FROM EDGE OF PVT/HMA SHLD TO LIMITS OF HMA APRON)	LENGTH (FROM EDGE OF PVT/HMA SHLD TO LIMITS OF IMPROVEMENT)	PR HMA CONC. THICKNESS	HMA SURF. REMOVAL - BUTT JOINT	PREP OF BASE	DRIVEWAY PAVT REM	AGG FOR TEMP ACCESS	AGGREGATE SURFACE COURSE TY - B	BIT (P. C.)	AGG (P. C.)	INCIDENTAL HMA SURF.
(LT / RT) (STA) (+)	(FE / PE / CE / MB) - (RURAL / URBAN)	(EARTH / AGG. / HMA / P. C. C.)	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	INCH	SQ. YD.	SQ. YD.	SQ YD	TON	TON	TON	TON	TON
LT STA 114+96	CE	AGG	35.0	46.1	57.3	0.0	5.5	5.5	8.0	0.0	31.6	0.0	14.4	2.8	0.00	0.0	14.2
LT STA 115+43	PE	AGG	14.3	28.4	64.7	20.0	4.0	7.0	3.5	0.0	25.1	8.4	5.0	1.9	0.00	0.0	4.9
RT STA 115+50	PE	AGG	24.0	37.9	45.8	0.0	4.0	7.0	3.5	0.0	18.6	12.6	3.7	2.7	0.00	0.0	3.6
LT STA 116+72	PE	AGG	10.3	24.4	51.3	14.0	4.0	7.0	3.5	0.0	19.9	4.6	4.0	1.5	0.00	0.0	3.9
LT STA 118+33	PE	HMA	12.4	26.5	57.3	14.0	4.0	7.0	3.5	0.0	21.7	4.6	4.3	1.7	0.00	0.0	4.3
LT STA 21+87	HERMAN RD	O & C	CADD	CADD	CADD	CADD	0.0	0.0	1.5	254.0	0.0	0.0	0.0	0.0	0.10	0.5	21.3
RT STA 21+87	HERMAN RD	O & C	CADD	CADD	CADD	CADD	0.0	0.0	1.5	140.4	0.0	0.0	0.0	0.0	0.05	0.3	11.8
LT STA 28+72	CE	AGG	0.0	26.0	46.0	0.0	10.0	0.0	0.0	0.0	0.0	0.0	0.0	4.6	0.00	0.0	0.0
LT STA 35+10	FR "R"	AGG	0.0	22.0	42.0	0.0	10.0	0.0	0.0	0.0	0.0	0.0	0.0	4.0	0.00	0.0	0.0
RT STA 35+10	FR "R"	HMA	CADD	CADD	CADD	CADD	0.0	0.0	1.5	261.3	0.0	0.0	0.0	0.0	0.10	0.52	21.9
LT STA 65+99	OLD 66	HMA	CADD	CADD	CADD	CADD	0.0	0.0	1.5	832.0	0.0	0.0	0.0	0.0	0.32	1.66	69.9
RT STA 65+99	OLD 66	HMA	CADD	CADD	CADD	CADD	0.0	0.0	1.5	273.8	0.0	0.0	0.0	0.0	0.10	0.55	23.0
RT STA 67+50	CE	HMA	44.0	54.0	74.0	0.0	10.0	5.0	1.5	71.4	0.0	0.0	0.0	3.1	0.03	0.0	0.0
RT STA 72+40	PE	HMA	18.0	32.0	48.0	0.0	8.0	7.0	1.5	35.6	0.0	0.0	0.0	2.2	0.01	0.0	0.0
LT STA 74+20	PE	AGG	22.0	36.0	52.0	0.0	8.0	7.0	1.5	39.1	0.0	0.0	0.0	2.6	0.01	0.0	0.0
TOTAL =										1907.6	117.0	30.2	31.4	27.2	0.72	3.5	178.9



TYPICAL PROFILE - S.E. TRANSITION

FAU 8136

EXIST. CURVE DPICALC1
 PI STA. = 24+15.09
 $\Delta = 12^\circ 52' 06''$ (LT)
 $D = 3^\circ 11' 38''$
 $R = 1,793.86'$
 $T = 202.30'$
 $L = 402.89'$
 $E = 11.37'$
 $e = 7.0\%$
 $T.R. = 38'$
 $S.E. RUN = 179'$
 $P.C. STA. = 22+12.79$
 $P.T. STA. = 26+15.68$

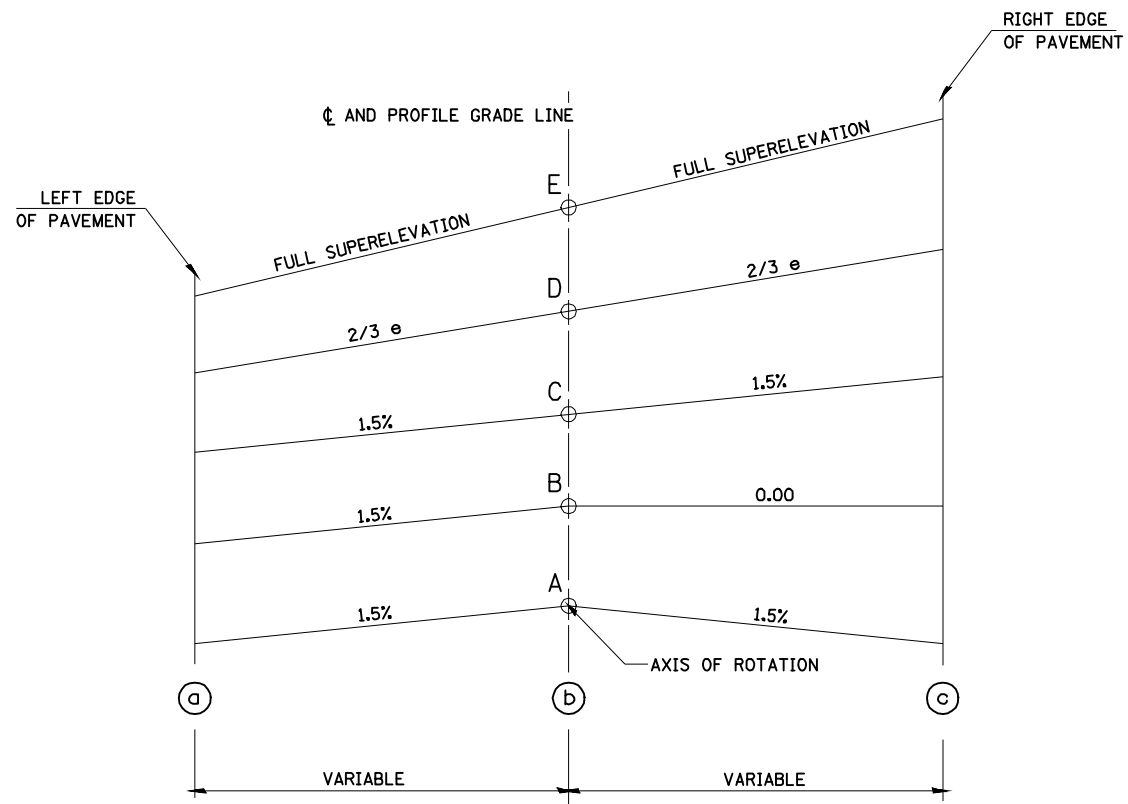
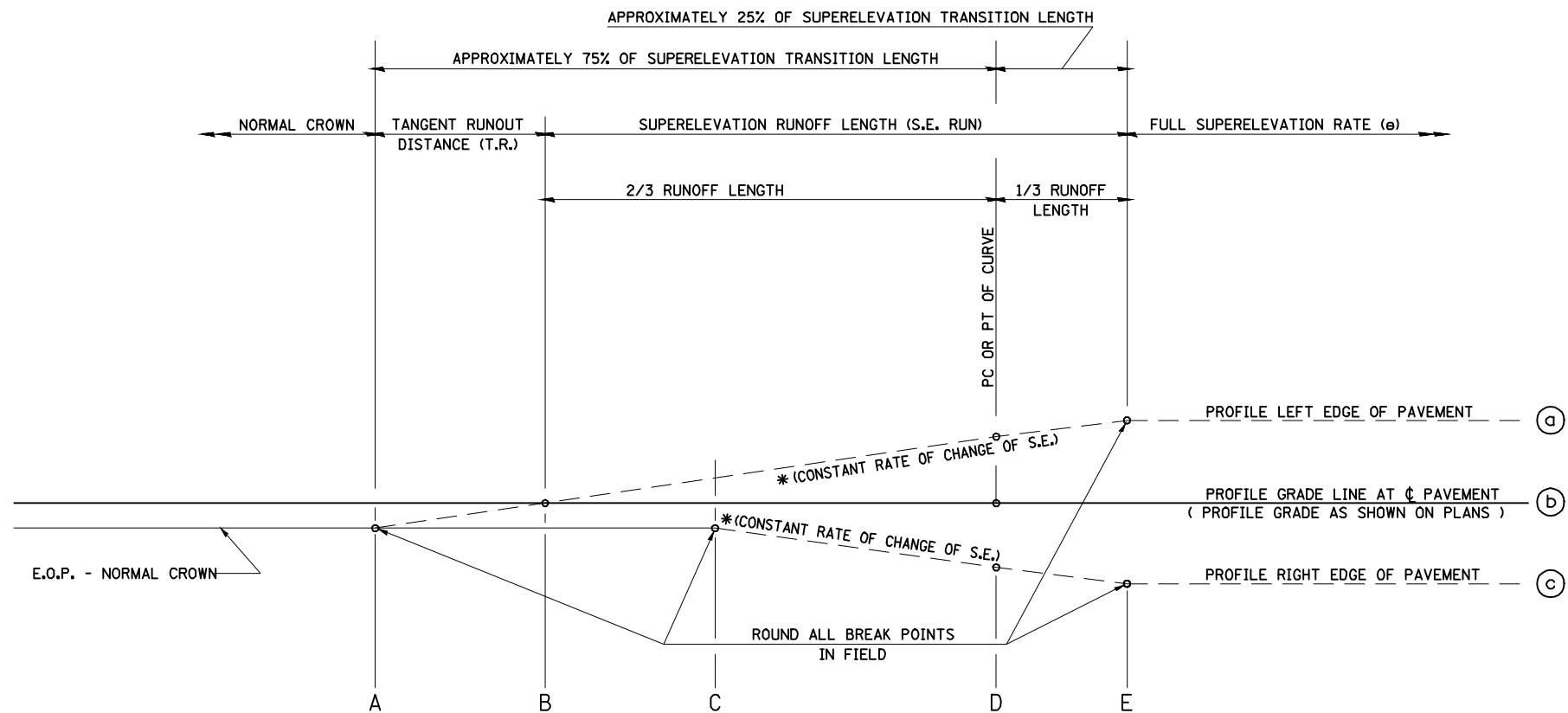


TABLE OF SUPERELEVATION BREAK POINT LOCATIONS

CURVE NO.	e	A	B	C	D	E	TRANSITION
DPICALC1	7.0%	20+55 27+73	20+93 27+35	21+31 26+97	22+12.79 26+15.68	22+72 25+56	TRANS. IN TRANS. OUT



TYPICAL PROFILE - S.E. TRANSITION

FAU 8136

EXIST. CURVE C998
 PI STA. = 113+13.64
 $\Delta = 43^\circ 59' 05''$ (RT)
 $D = 3^\circ 31' 43''$
 $R = 1,623.81'$
 $T = 655.81'$
 $L = 1,246.56'$
 $E = 127.43'$
 $e = 7.0\%$
 $T.R. = 38'$
 $S.E. RUN = 179'$
 $P.C. STA. = 106+57.83$
 $P.T. STA. = 119+04.40$

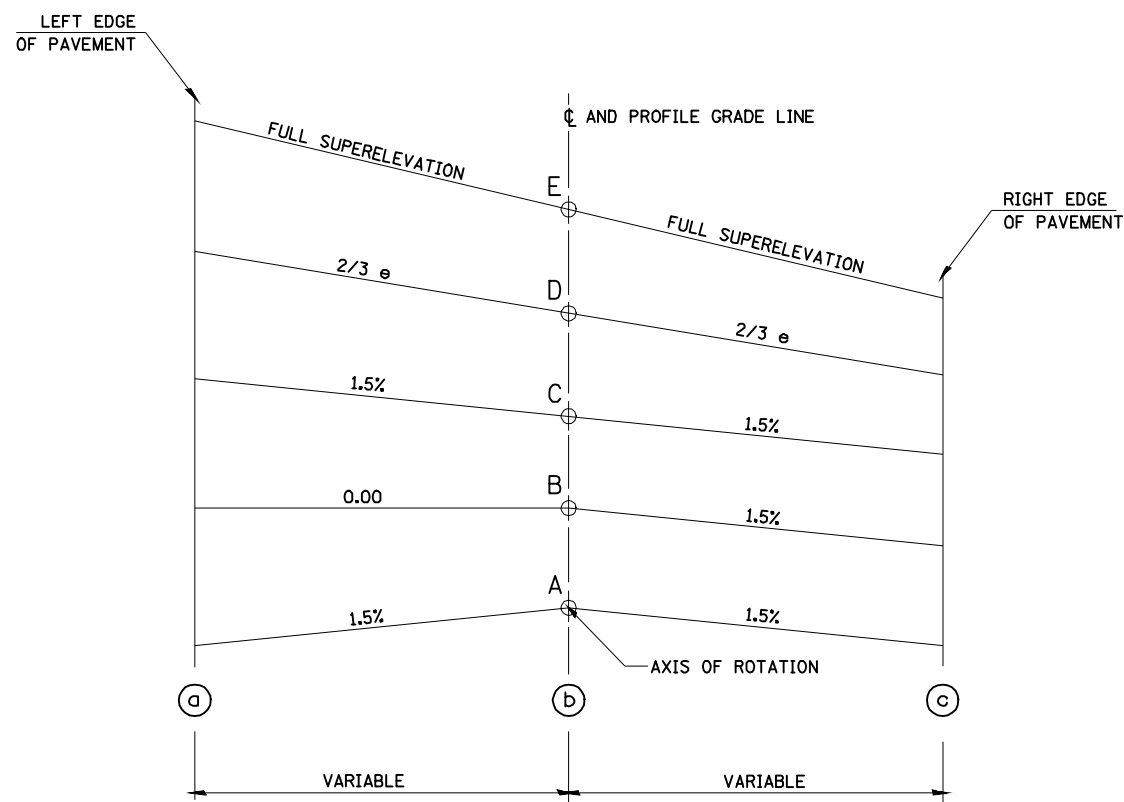
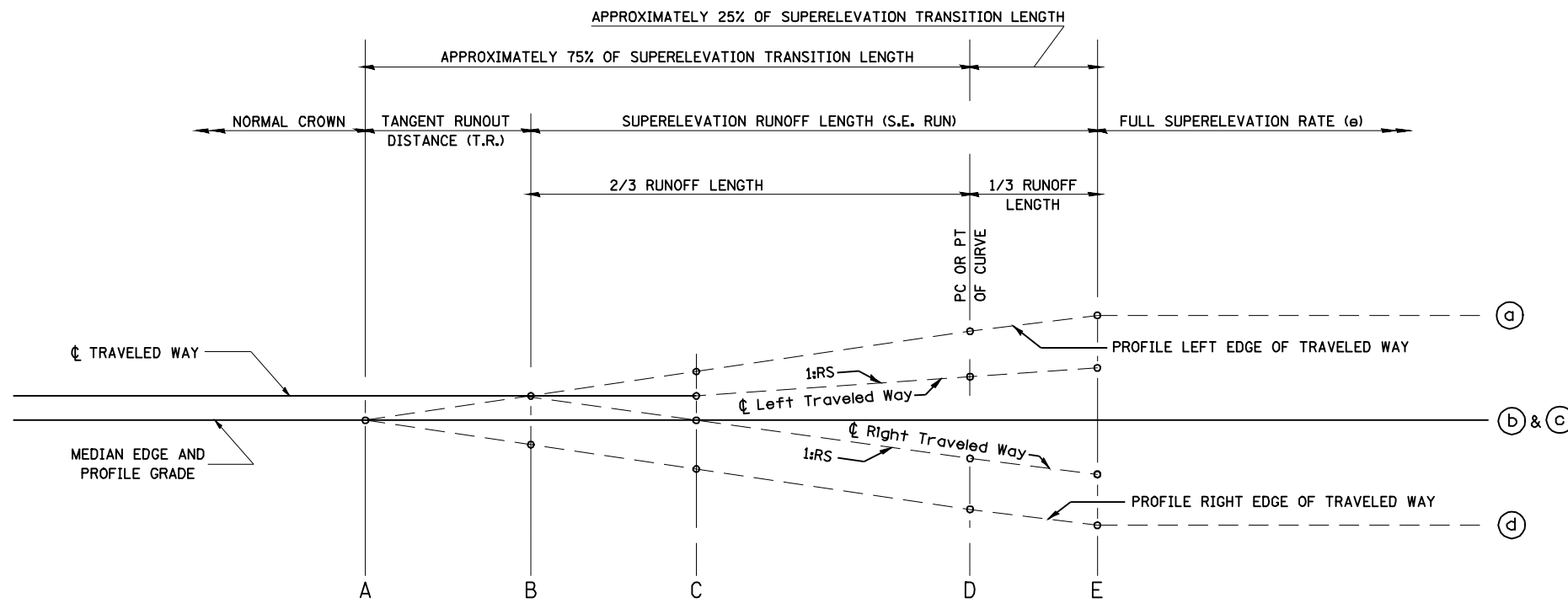


TABLE OF SUPERELEVATION BREAK POINT LOCATIONS

CURVE NO.	e	A	B	C	D	E	TRANSITION
C998	7.0%	105+00	105+38	105+76	106+57.83	107+17	TRANS. IN
		120+62	120+24	119+86	119+04.04	118+45	TRANS. OUT



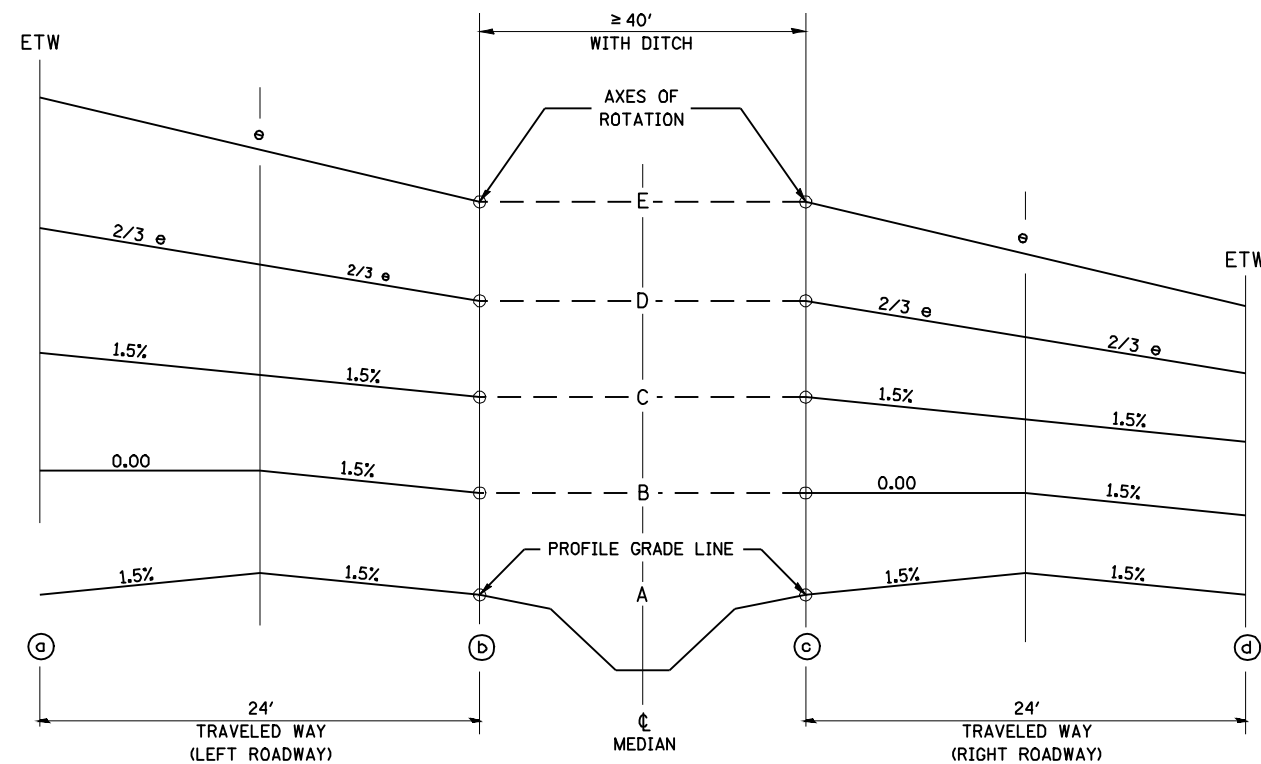
TYPICAL PROFILE - S.E. TRANSITION

I-55
 EXIST. CURVE I55N-5
 PI STA. = 755+56.46
 $\Delta = 21^\circ 48' 40''$ (LT)
 $D = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 1,103.92'$
 $L = 2,181.11'$
 $E = 105.38'$
 $e = 2.8\%$
 $T.R. = 46'$
 $S.E. RUN = 126'$
 P.C. STA. = 744+52.54
 P.T. STA. = 766+33.65

I-55
 EXIST. CURVE I55N-6
 PI STA. = 799+36.98
 $\Delta = 21^\circ 15' 10''$ (RT)
 $D = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 1,074.99'$
 $L = 2,125.28'$
 $E = 99.97'$
 $e = 2.8\%$
 $T.R. = 46'$
 $S.E. RUN = 126'$
 P.C. STA. = 788+61.98
 P.T. STA. = 809+87.26

TABLE OF SUPERELEVATION BREAK POINT LOCATIONS

CURVE NO.	e	A	B	C	D	E	TRANSITION
I55N-5	2.8%	743+23	743+69	744+15	744+53	744+95	TRANS. IN
		767+64	767+18	766+72	766+34	765+92	TRANS. OUT
I55N-6	2.8%	788+24	787+78	788+24	788+62	796+92	TRANS. IN
		811+17	810+71	810+25	809+87	809+45	TRANS. OUT



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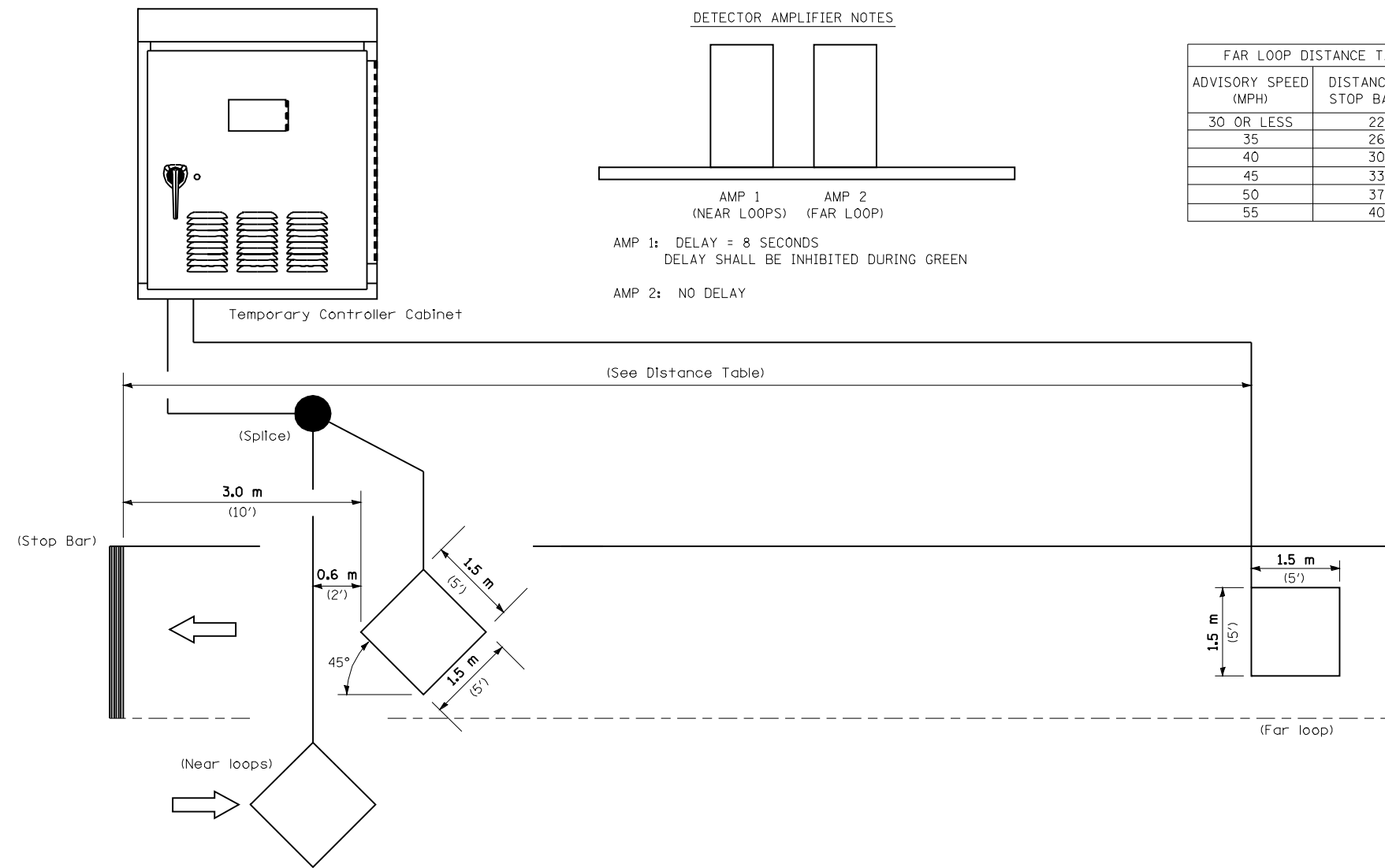
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUPERELEVATION TRANSITION
 DETAIL FOR TWO LANE HIGHWAY

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	69
CONTRACT NO. 72A60				
ILLINOIS FED. AID PROJECT				

SUPER.DGN



INDUCTION LOOP DETECTOR

BRIDGE TEMP SIGNAL.DGN

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY BRIDGE TRAFFIC SIGNAL
LOOP PLACEMENT DETAIL SHEET**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	70
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	

STAGE I NOTES

- CONSTRUCT HMA BASE COURSE FOR STAGE I AND STAGE II TRAFFIC USING TRAFFIC CONTROL STANDARD 701201.
- ERECT SIGNS, TRAFFIC SIGNALS, TEMPORARY BARRIERS, ETC. ACCORDING TO TRAFFIC CONTROL STANDARD 701321.
- PLACE TEMPORARY PAVEMENT MARKING LINE TO ALLOW FOR A 12'-0" TRAFFIC LANE.
- CONSTRUCT THE STAGE I PORTION OF THE BRIDGE REPAIR.

EARTH EXCAVATION (WIDENING)

STATION TO	STATION	OFFSET	CU YD
27+60	28+66	LT	12
27+60	28+59	RT	11
31+42	32+40	LT	11
31+34	32+40	RT	12
TOTAL			46

HOT-MIX ASPHALT BASE COURSE 9"

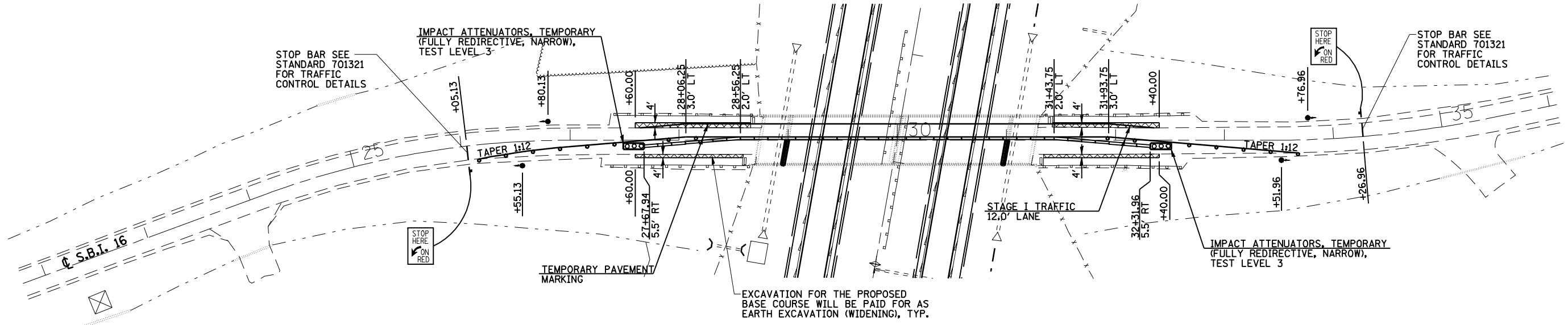
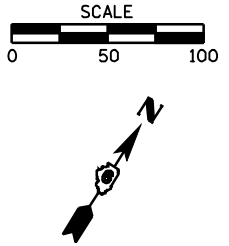
STATION TO	STATION	OFFSET	SQ YD
27+60	28+66	LT	47
27+60	28+59	RT	44
31+42	32+40	LT	44
31+34	32+40	RT	47
TOTAL			182

IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3

STATION	OFFSET	EACH
27+65	RT	1
32+32	RT	1
TOTAL		2

TEMPORARY CONCRETE BARRIER

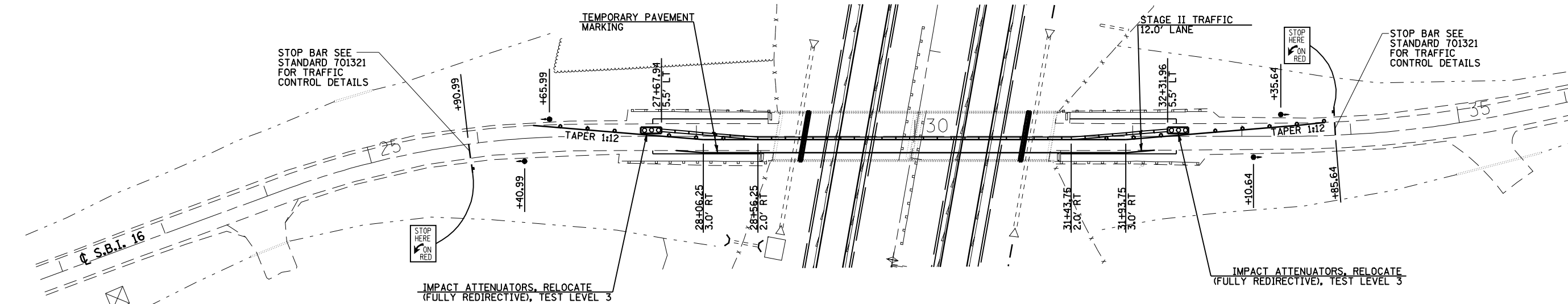
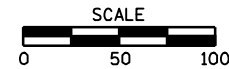
STATION TO	STATION	FOOT
27+67.94	32+31.96	462.5



DECATUR, ILLINOIS

ENGINEERS - CONSULTANTS

BLANK, WESSELINK, COOK & ASSOCIATES



- GENERAL NOTES FOR STAGE CONSTRUCTION:**
- ALL SIGNAGE, SPACING, TRAFFIC CONTROL EQUIPMENT AND TEMPORARY PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF HIGHWAY STANDARD 701321 AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE EACH FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701321.
 - ALL TEMPORARY BRIDGE TRAFFIC SIGNALS FOR CONSTRUCTION WILL BE MEASURED AS 1 (ONE) UNIT.
 - BARRIER OFFSETS ARE FROM THE CENTER OF THE BARRIER.
 - SEE BRIDGE PLANS FOR ADDITIONAL DETAILS.
 - STOP BARS TO BE LOCATED PER STD. 701321.
 - PRIOR TO ROUTING TRAFFIC ONTO THE SHOULDERS AS SHOWN IN THE STAGING PLANS, THE CONTRACTOR SHALL SECURE THE GRATINGS ON SHOULDER INLETS AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK WILL BE INCLUDED IN THE TRAFFIC CONTROL STANDARD 701321 PAY ITEM.

STAGE II NOTES

- RELOCATE SIGNS, TEMPORARY BARRIERS, ETC IN ACCORDANCE WITH TRAFFIC CONTROL STANDARD 701321 AND AS SHOWN ON THIS PLAN.
- PLACE TEMPORARY PAVEMENT MARKING LINE TO ALLOW FOR A 12'-0" DRIVING LANE.
- CONSTRUCT THE STAGE II PORTION OF THE BRIDGE REPAIR.
- REMOVE TEMPORARY BARRIERS, SIGNALS, AND SIGNS ASSOCIATED WITH TRAFFIC CONTROL STANDARD 701321.

IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE) TEST LEVEL 3

STATION	OFFSET	EACH
27+65	LT	1
32+32	LT	1
TOTAL		2

RELOCATE TEMPORARY CONCRETE BARRIER

STATION TO	STATION	FOOT
27+67.94	32+31.96	462.5

LEGEND

- TEMPORARY SIGNAL LIGHT
- TEMPORARY CONCRETE BARRIER
- TO BE REMOVED
- EXISTING
- PROPOSED
- PROPOSED HMA BASE COURSE

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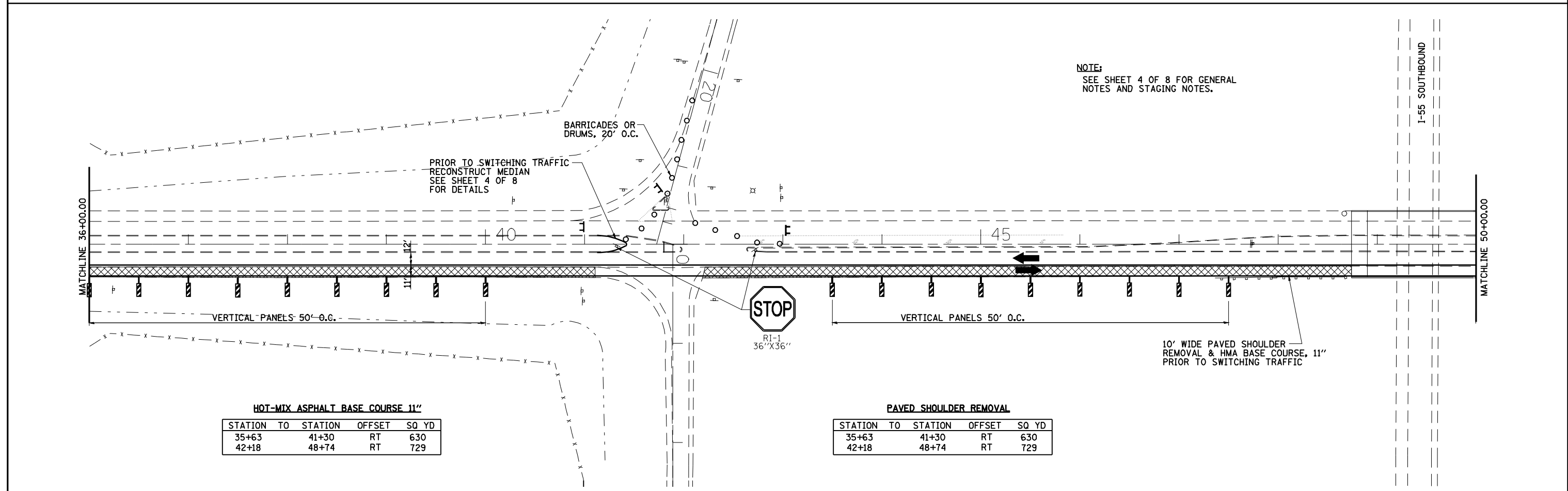
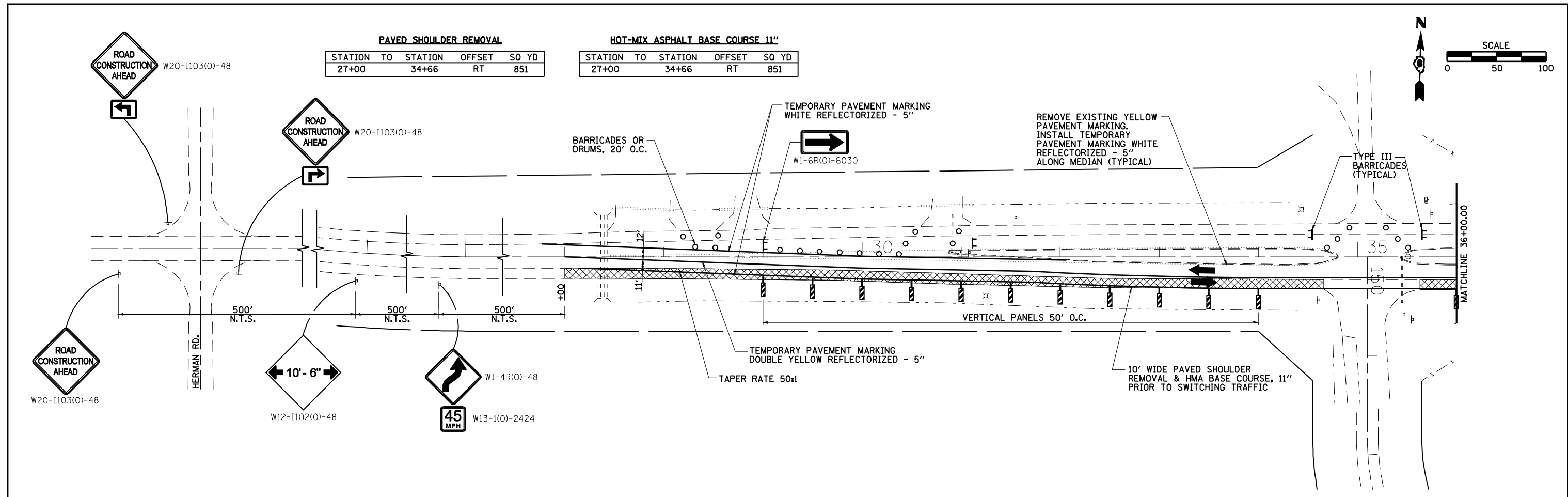
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE I & II CONSTRUCTION (SN 059-0044)

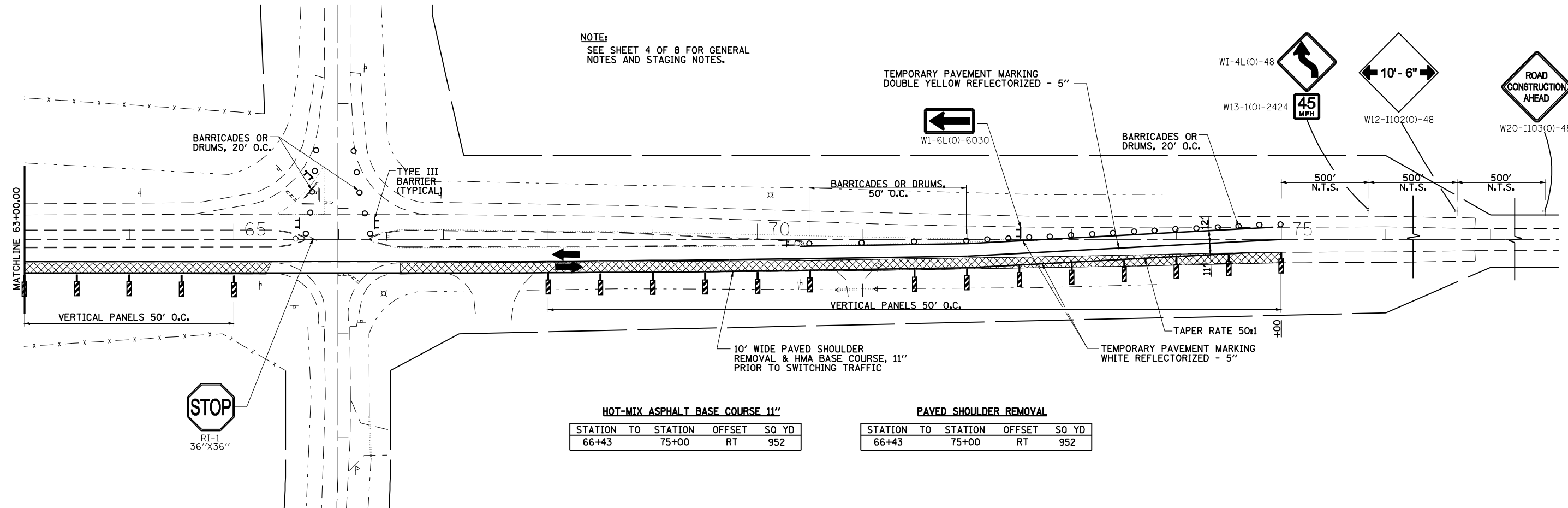
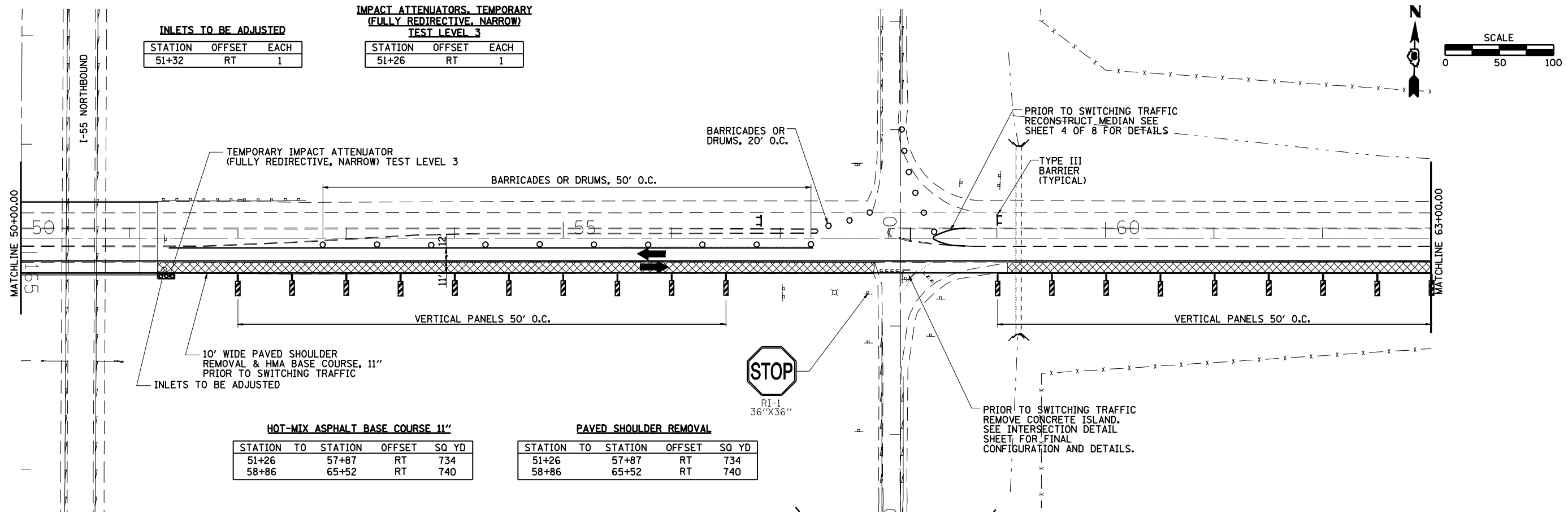
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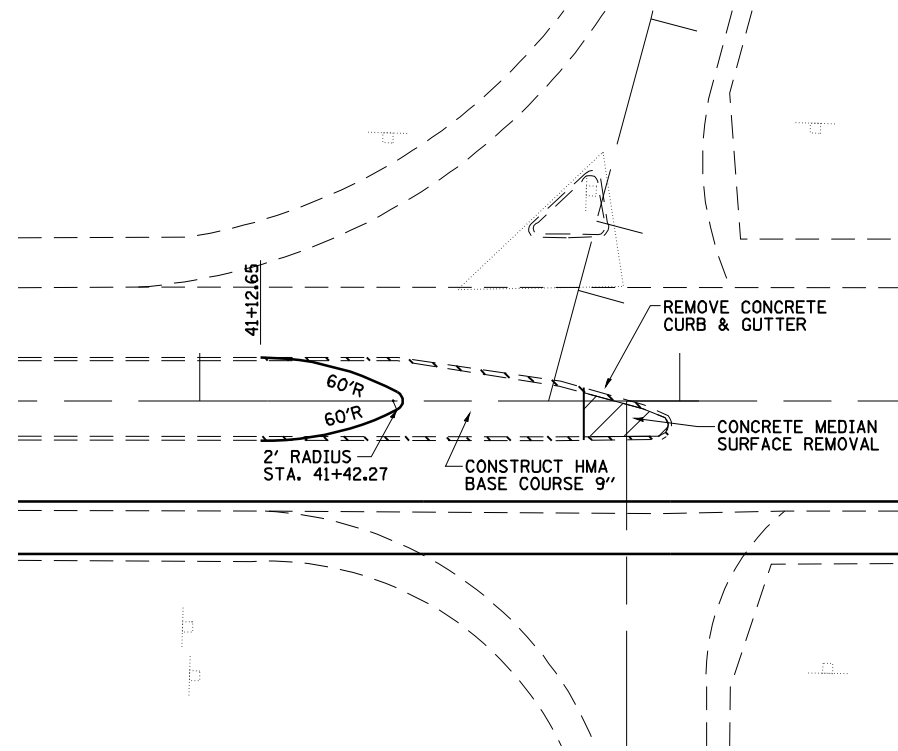
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	71
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 72A60

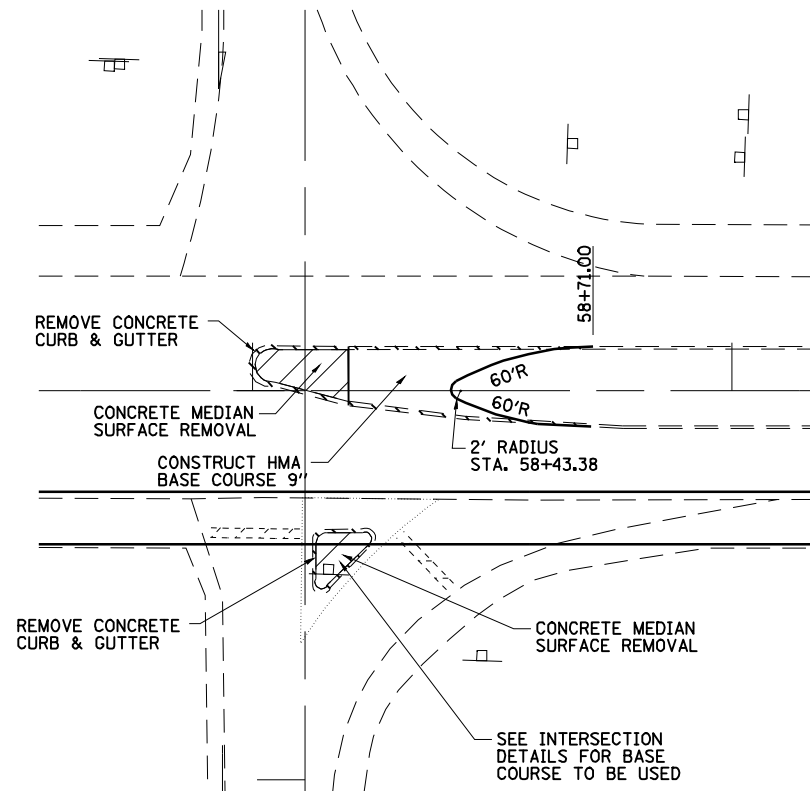


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MEDIAN DETAIL AT SOUTHBOUND INTERSECTION



MEDIAN DETAIL AT NORTHBOUND INTERSECTION

GENERAL NOTES FOR STAGE CONSTRUCTION

1. ALL SIGNAGE, SPACING, TRAFFIC CONTROL EQUIPMENT AND TEMPORARY MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF HIGHWAY STANDARD 701331, AND SHALL BE INCLUDED IN THE CONTRACT UNIT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701331.
2. REMOVAL OF EXISTING PAVEMENT MARKING THAT CONFLICTS WITH THE TEMPORARY PAVEMENT MARKINGS FOR TRAFFIC CONTROL WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PAVEMENT MARKING REMOVAL.
3. REMOVAL OF TEMPORARY PAVEMENT MARKING FOR TRAFFIC CONTROL & PROTECTION STAGING WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR WORK ZONE PAVEMENT MARKING REMOVAL.
4. PRIOR TO ROUTING TRAFFIC ONTO THE SHOULDERS AS SHOWN IN THE STAGING PLANS, THE CONTRACTOR SHALL SECURE THE GRATINGS ON SHOULDER INLETS AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK WILL BE INCLUDED IN THE TRAFFIC CONTROL STANDARD 701331 PAY ITEM.

STAGE I NOTES/ANTICIPATED SEQUENCE

1. CONSTRUCT TEMPORARY MEDIAN CROSSOVER AT THE I-55 RAMPS/STAUNTON ROAD INTERSECTIONS UTILIZING HIGHWAY STANDARD 701201.
2. CONSTRUCT HMA BASE COURSE, 11", FOR STAGE I TRAFFIC, UTILIZING TRAFFIC CONTROL STD. 701326.
3. ERECT SIGNS, TEMPORARY BARRIERS, TEMPORARY PAVEMENT MARKINGS ACCORDING TO THESE PLANS AND TRAFFIC PROTECTION AND CONTROL 701331.
4. REMOVE THE STAGE I PORTION OF THE EXISTING STRUCTURE AND PAVEMENT.
5. CONSTRUCT THE STAGE I PORTION OF THE BRIDGE REPAIR AND PAVEMENT RESURFACING.
6. CONSTRUCT HMA BASE COURSE, 11", FOR STAGE II TRAFFIC.

COMBINATION CURB & GUTTER REMOVAL

STATION TO	STATION	OFFSET	FEET	
41+12.34	41+92.44	LT	81	
41+12.34	41+92.44	RT	101	
58+07.57	58+71.00	LT	84	
58+07.57	58+71.00	RT	65	
58+12.00	58+25.00	RT	45	
			TOTAL	376

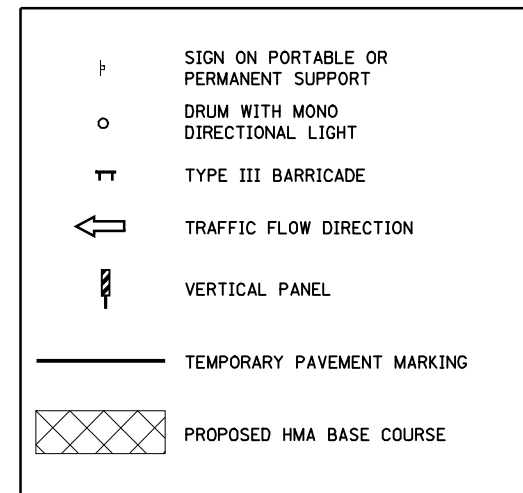
HOT-MIX ASPHALT BASE COURSE 9"

STATION TO	STATION	OFFSET	SQ.YD.	
41+12.34	41+98.00	LT&RT	92	
57+99.00	58+71.00	LT&RT	67	
			TOTAL	159

CONCRETE MEDIAN SURFACE REMOVAL

STATION TO	STATION	SQ.FT.	
41+80	42+00	116	
57+99	58+20	158	
58+12	58+25	85	
		TOTAL	359

LEGEND



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	PLOT DATE = Mar-23-2010 03:23:53PM	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE I CONSTRUCTION (SN 059-0045)

SCALE: SHEET NO. 4 OF 8 SHEETS STA. TO STA.

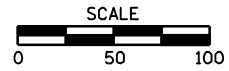
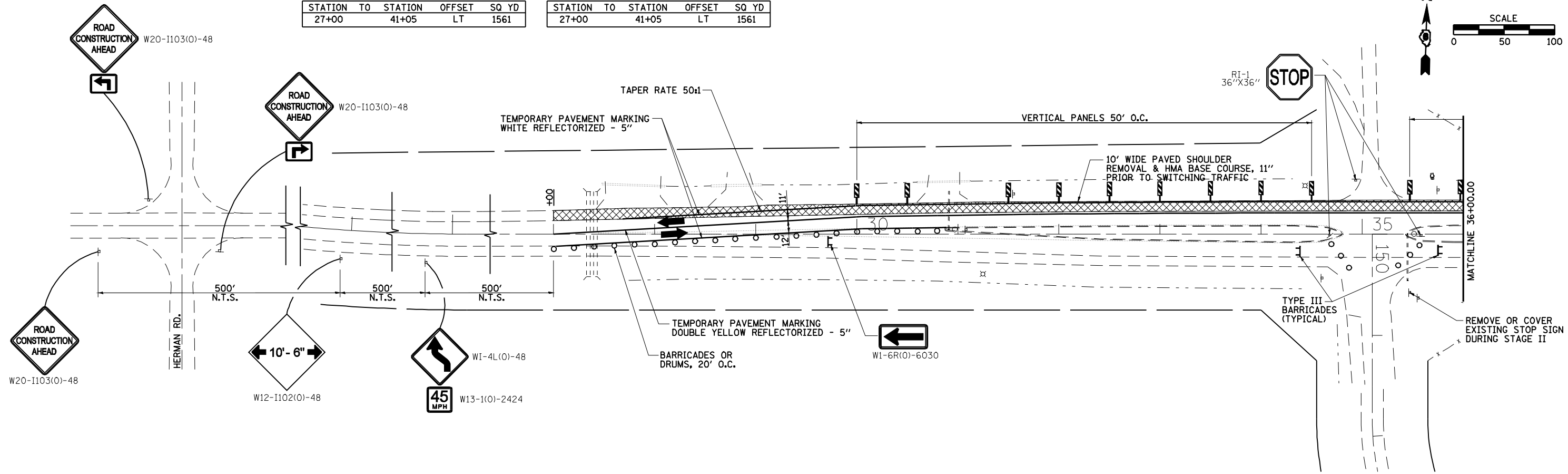
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	74
			CONTRACT NO. 72A60	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PAVED SHOULDER REMOVAL

STATION TO	STATION	OFFSET	SQ YD
27+00	41+05	LT	1561

HOT-MIX ASPHALT BASE COURSE 11"

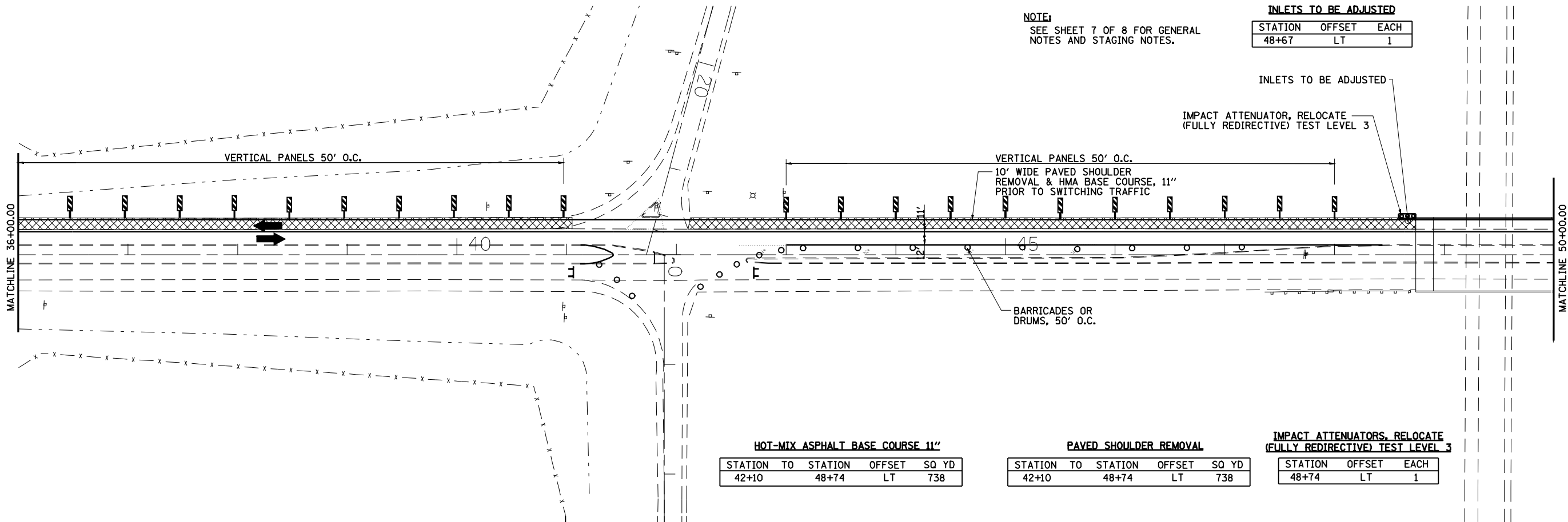
STATION TO	STATION	OFFSET	SQ YD
27+00	41+05	LT	1561



NOTE:
SEE SHEET 7 OF 8 FOR GENERAL NOTES AND STAGING NOTES.

INLETS TO BE ADJUSTED

STATION	OFFSET	EACH
48+67	LT	1



HOT-MIX ASPHALT BASE COURSE 11"

STATION TO	STATION	OFFSET	SQ YD
42+10	48+74	LT	738

PAVED SHOULDER REMOVAL

STATION TO	STATION	OFFSET	SQ YD
42+10	48+74	LT	738

IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE) TEST LEVEL 3

STATION	OFFSET	EACH
48+74	LT	1

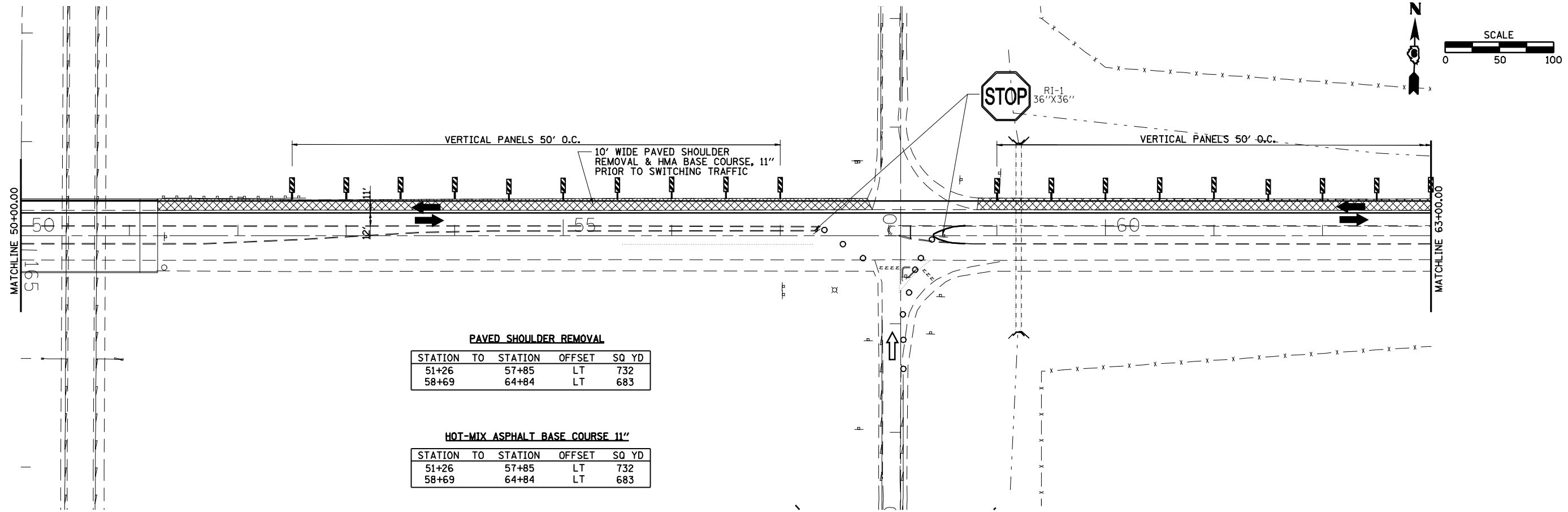
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		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE II CONSTRUCTION (SN 059-0045)

SCALE: SHEET NO. 5 OF 8 SHEETS STA. -- TO STA. --

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	75
CONTRACT NO. 72A60				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

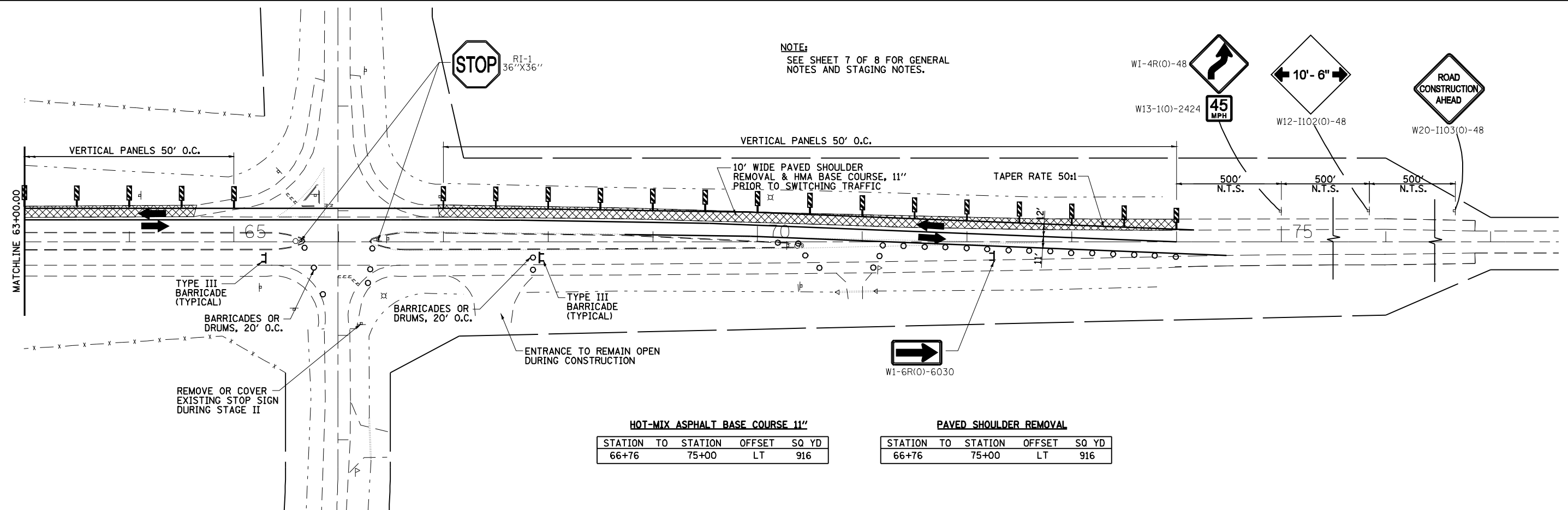


PAVED SHOULDER REMOVAL

STATION TO	STATION	OFFSET	SQ YD
51+26	57+85	LT	732
58+69	64+84	LT	683

HOT-MIX ASPHALT BASE COURSE 11"

STATION TO	STATION	OFFSET	SQ YD
51+26	57+85	LT	732
58+69	64+84	LT	683



NOTE:
SEE SHEET 7 OF 8 FOR GENERAL NOTES AND STAGING NOTES.

HOT-MIX ASPHALT BASE COURSE 11"

STATION TO	STATION	OFFSET	SQ YD
66+76	75+00	LT	916

PAVED SHOULDER REMOVAL

STATION TO	STATION	OFFSET	SQ YD
66+76	75+00	LT	916

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE II CONSTRUCTION (SN 059-0045)

SCALE: SHEET NO. 6 OF 8 SHEETS STA. -- TO STA. --

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2, BR	MACOUPIN	100	76
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 72A60	

GENERAL NOTES FOR STAGE CONSTRUCTION

1. ALL SIGNAGE, SPACING, TRAFFIC CONTROL EQUIPMENT AND TEMPORARY MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF HIGHWAY STANDARD 701331, AND SHALL BE INCLUDED IN THE CONTRACT UNIT LUMP SUM PRICE FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701331.
2. REMOVAL OF EXISTING PAVEMENT MARKING THAT CONFLICTS WITH THE TEMPORARY PAVEMENT MARKINGS FOR TRAFFIC CONTROL WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PAVEMENT MARKING REMOVAL.
3. REMOVAL OF TEMPORARY PAVEMENT MARKING FOR TRAFFIC CONTROL & PROTECTION STAGING WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR WORK ZONE PAVEMENT MARKING REMOVAL.
4. PRIOR TO ROUTING TRAFFIC ONTO THE SHOULDERS AS SHOWN IN THE STAGING PLANS, THE CONTRACTOR SHALL SECURE THE GRATINGS ON SHOULDER INLETS AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK WILL BE INCLUDED IN THE TRAFFIC CONTROL STANDARD 701331 PAY ITEM.

STAGE II NOTES/ANTICIPATED SEQUENCE

1. RELOCATE SIGNS, TEMPORARY BARRIERS, TEMPORARY PAVEMENT MARKINGS ACCORDING TO THESE PLANS AND TRAFFIC PROTECTION AND CONTROL 701331.
2. REMOVE THE STAGE II PORTION OF THE EXISTING STRUCTURE AND PAVEMENT.
3. CONSTRUCT THE STAGE II PORTION OF THE BRIDGE REPAIR AND PAVEMENT RESURFACING.
4. RECONSTRUCT THE MEDIAN NOSE AT THE RAMP/STAUNTON ROAD INTERSECTION. UTILIZING HIGHWAY STANDARD 701201.

LEGEND

	SIGN ON PORTABLE OR PERMANENT SUPPORT
	DRUM WITH MONO DIRECTIONAL LIGHT
	TYPE III BARRICADE
	TRAFFIC FLOW DIRECTION
	VERTICAL PANEL
	TEMPORARY PAVEMENT MARKING
	PROPOSED HMA BASE COURSE

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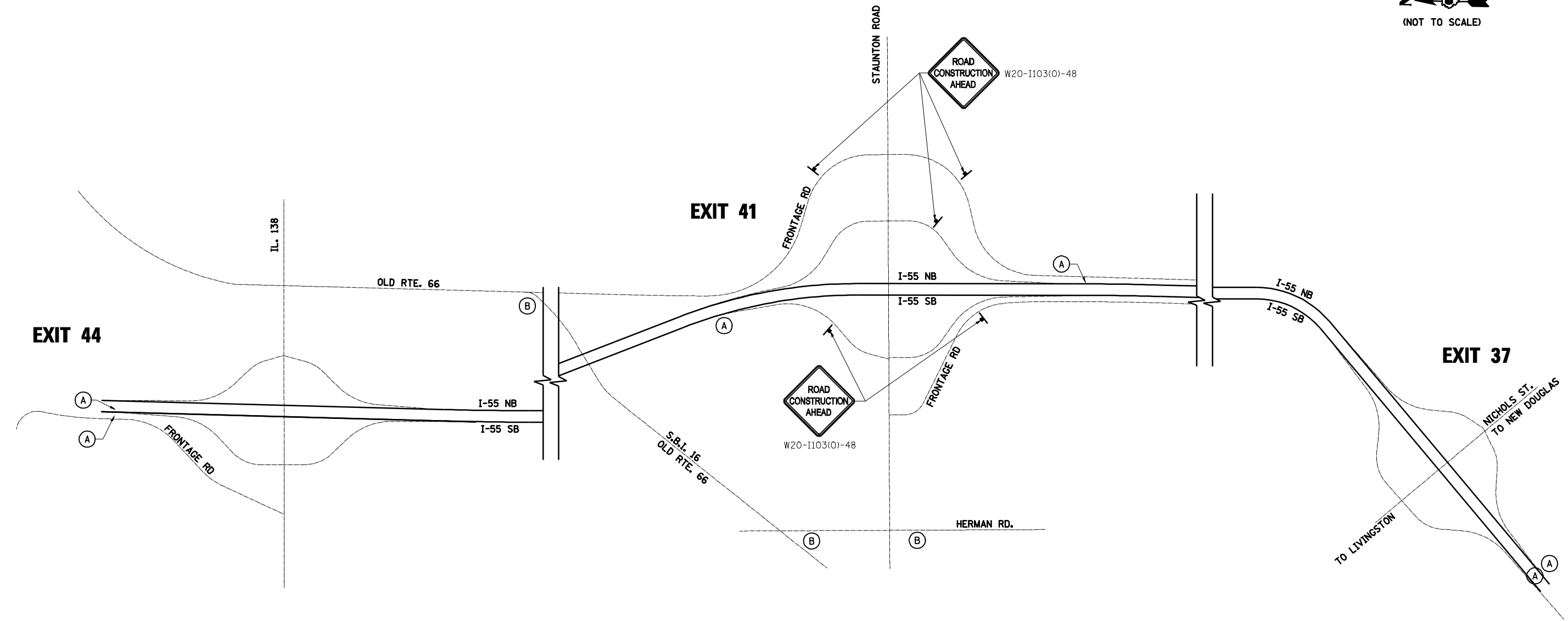
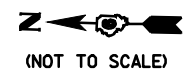
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE II CONSTRUCTION (SN 059-0045)

SCALE: SHEET NO. 7 OF 8 SHEETS STA. TO STA.

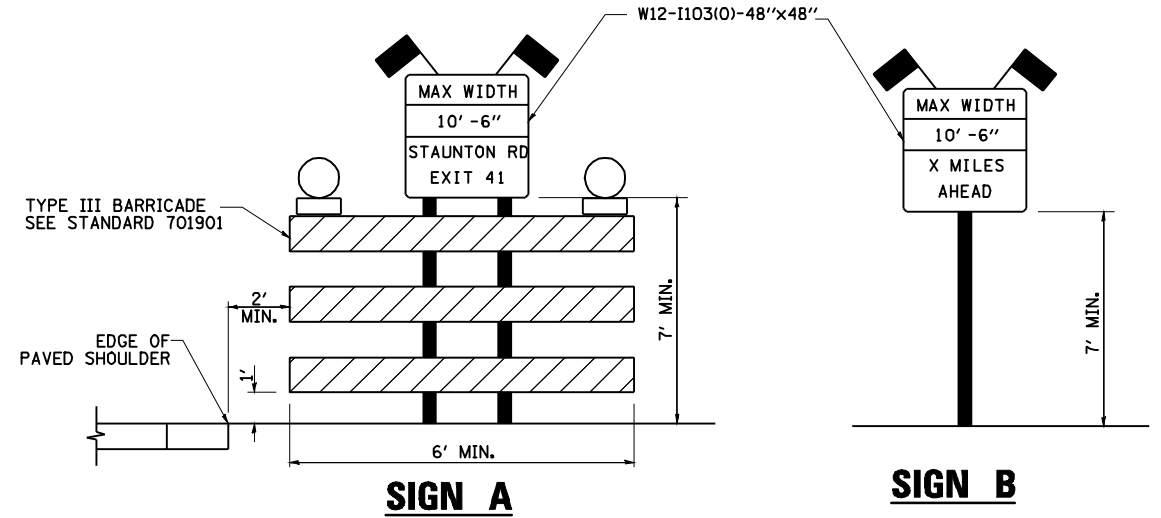
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2,BR	MACOUPIN	100	77
CONTRACT NO. 72A60				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

BLANK, WESSELINK, COOK & ASSOCIATES ENGINEERS - CONSULTANTS DECATUR, ILLINOIS



GENERAL NOTES

1. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED AND MAINTAINED BY THE CONTRACTOR.
2. LOCATIONS OF TRAFFIC CONTROL DEVICES MAY BE ADJUSTED BY THE ENGINEER.
3. ALL TRAFFIC CONTROL SHOWN ON THIS SHEET SHALL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE FOR WIDTH RESTRICTION SIGNING.
4. ALL SIGNS SHALL BE POST MOUNTED UNLESS OTHERWISE DIRECTED.
5. ALL SIGNS SHOWN ORANGE (O) SHALL BE FLUORESCENT ORANGE.



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MAXIMUM WIDTH SIGN LOCATION

SCALE: SHEET NO. 8 OF 8 SHEETS STA. TO STA.

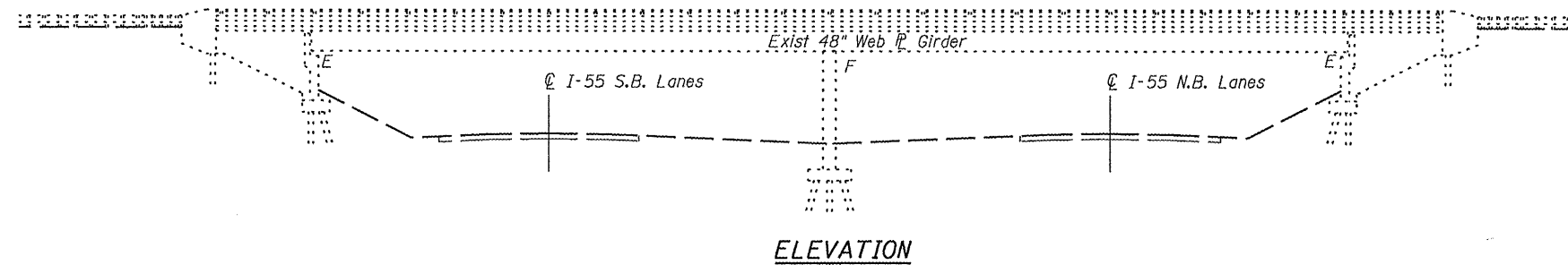
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	59RS-2,BR	MACOUPIN	100	78
CONTRACT NO. 72A60			ILLINOIS FED. AID PROJECT	

Existing Structure: S.N. 059-0042 Built as F.A.I. Rte. 55 - Sec 59-IHB-2 In 1973. The super structure is a R.C. Slab, two-span, steel continuous structure with vaulted sand filled approach spans. The structure length is 249'-8" back-to-back of approach bents and the width is 32' out-to-out. The two steel span lengths are both 102'-10" and the concrete approach slabs are both 21'-6".

Traffic Control & Protection Standard 701201 will be used to replace the expansion joint seal.

No Salvage.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

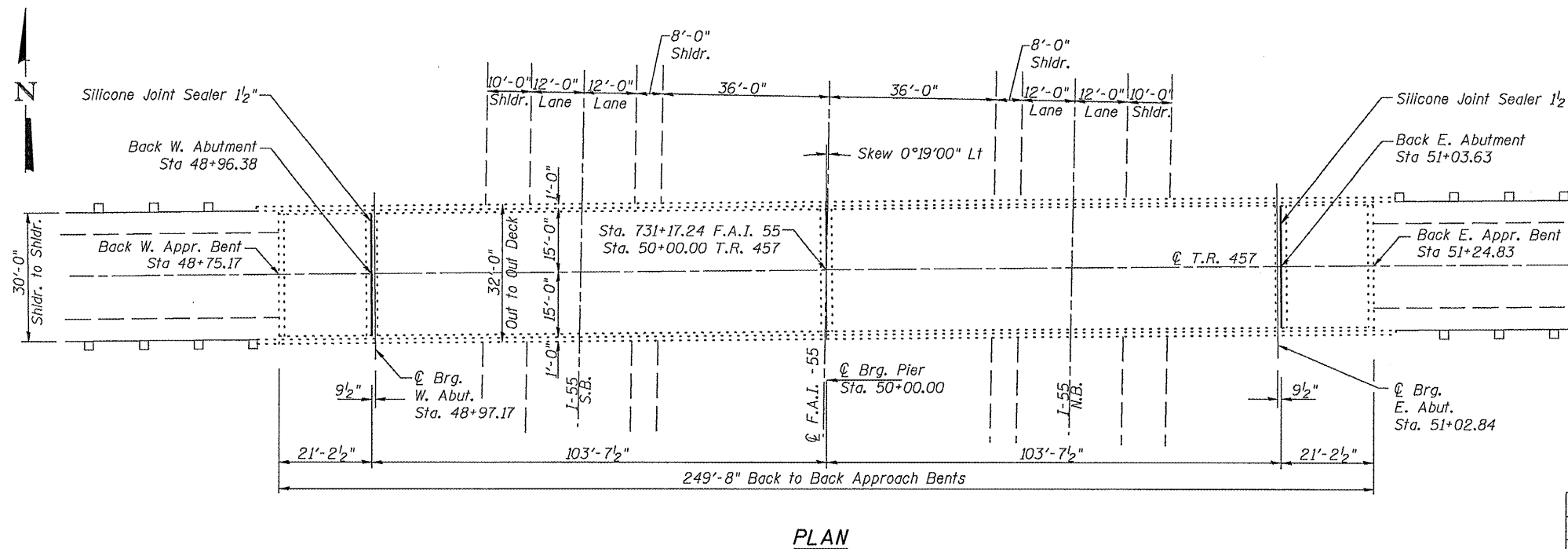


GENERAL NOTES

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

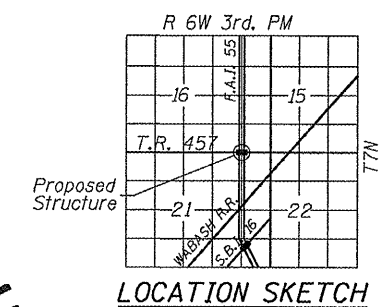
INDEX OF SHEETS

- 1 General Plan
- 2 Expansion Joint Details

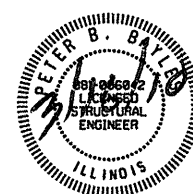


TOTAL BILL OF MATERIAL

Item	Unit	Quantity
Silicone Joint Sealer, 1 1/2"	Foot	60

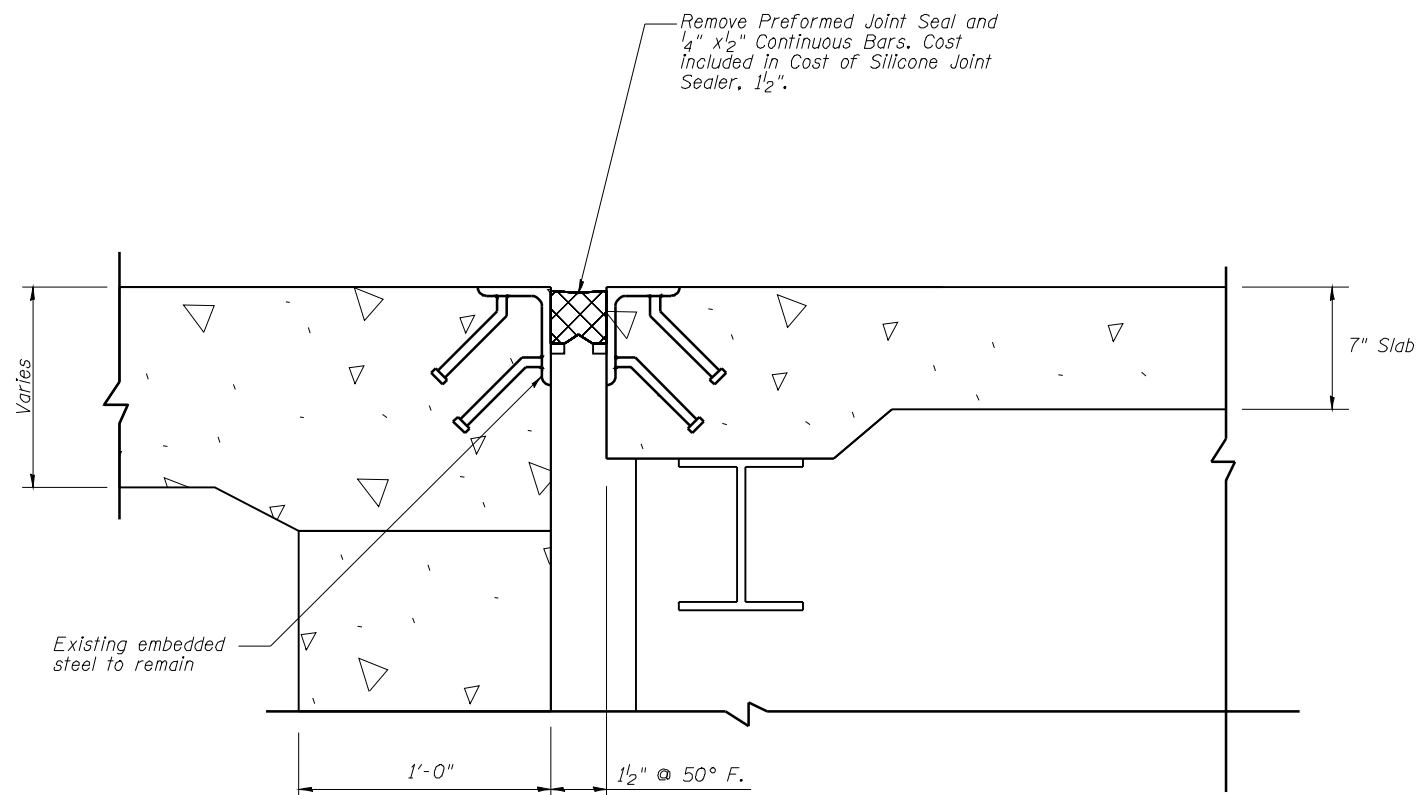


GENERAL PLAN
T.R. 457 OVER I-55
F.A.I. RTE. 55
SECTION 59RS-2, BR
MACOUPIN COUNTY
STATION 731+17.24
STRUCTURE NO. 059-0042

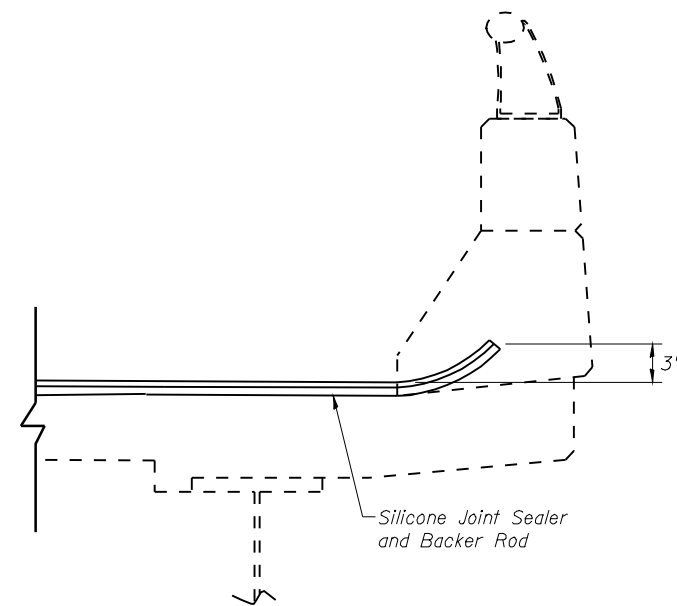


Peter B. Boyles
Peter B. Boyles, P.E., S.E.
Structural Engineer License No. 081-006042
Expiration Date: 11/30/2010

SHEET NO. 1	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2 SHEETS	55	59RS-2, BR	Macoupin	100	79
			CONTRACT NO. 72A60		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			



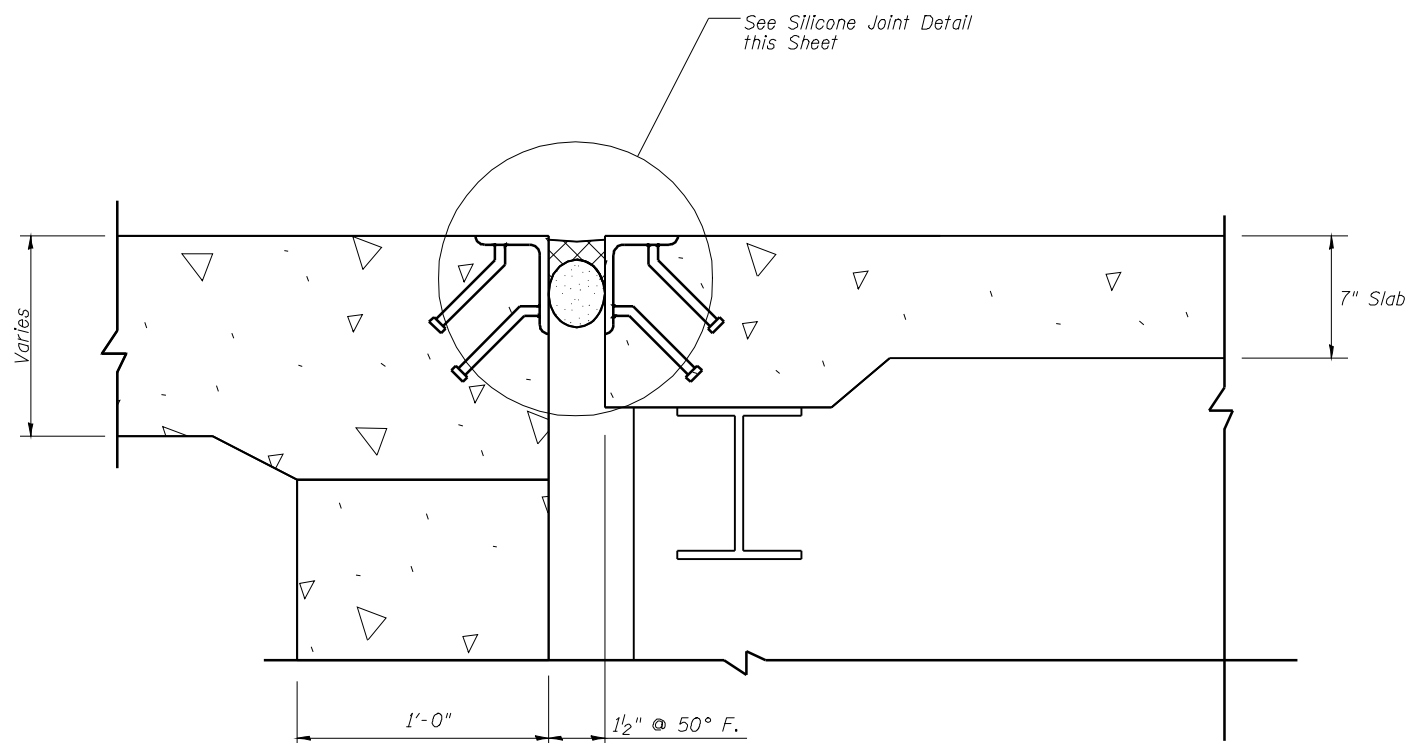
EXISTING SECTION AT ABUTMENT



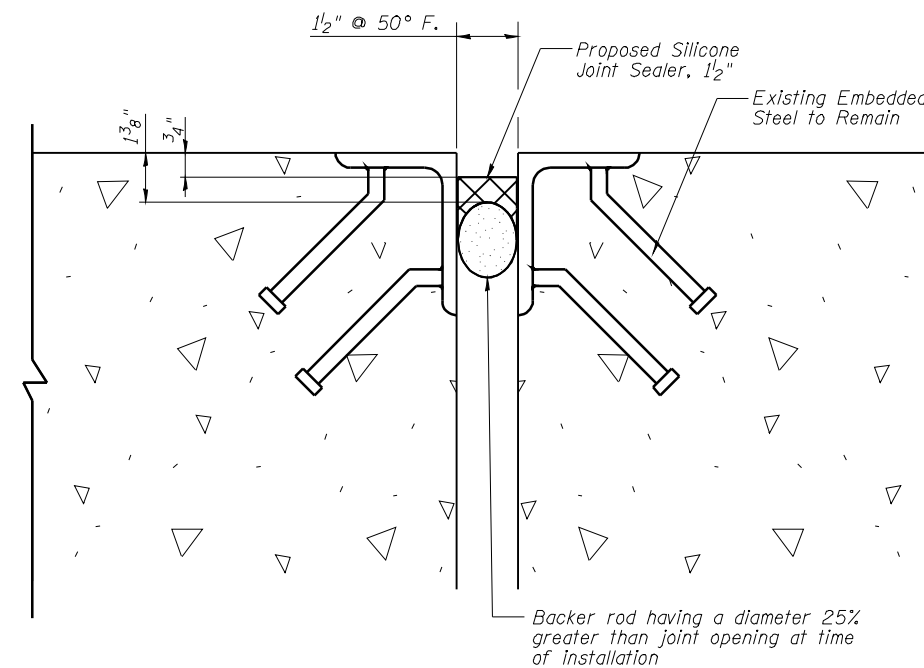
TYPICAL END OF SEAL TREATMENT

NOTES:

1. See Sheet 1 of 2 for Locations of Expansion Joint Replacement.



PROPOSED SECTION AT ABUTMENT



SILICONE JOINT DETAIL

BILL OF MATERIAL

Item	Unit	Quantity
Silicone Joint Sealer, 1/2"	Foot	60

**EXPANSION JOINT DETAILS
STRUCTURE NO. 059-0042**

SHEET NO. 2 2 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	59RS-2, BR	Macoupin	100	80
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT		
CONTRACT NO. 72A60					

S.N. 059-0044 was originally built in 1973. It is a two-span, steel continuous structure with vaulted PPC I-beam concrete approaches. The structure length is 254'-0" back-to-back approach bents and the width is 46'-0" out-to-out. The two steel span lengths are both 100'-5" and the PPC I-beam spans are both 26'-7" with a 10 deg skew.

Structure to be repaired using staged construction. Bridge deck joints, abutment bearings and wearing surface to be replaced.

No Salvage.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

No field welding is permitted except as specified in the contract documents.

Reinforcement bars shall conform to the requirements of ASTM A706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that can not be removed by grinding 1/4 in. deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Protective coat shall not be applied to surfaces to which Waterproofing Membrane System is applied.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

All structural steel shall be shop painted with inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Furnishing and Erecting Structural Steel.

If the analysis submitted to the Contractor for the jacking/temporary support system to be used shows temporary stiffeners are required to prevent web crippling or buckling, the stiffeners shall be steel and bolted to the web. If stiffeners are not required, hardwood timbers shall be installed tightly between the top and bottom flange to prevent flange rotation.

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
HMA Surface Course, Mix "C", N50	Ton	115.0
HMA Surface Removal (Deck)	Sq. Yd.	1170
Concrete Removal	Cu. Yd.	9.5
Concrete Superstructure	Cu. Yd.	10.8
Protective Coat	Sq. Yd.	31
Jack and Remove Existing Bearings	Each	12
Reinforcement Bars, Epoxy Coated	Pound	2070
Bar Splicers	Each	28
Preformed Joint Strip Seal	Foot	91
Elastomeric Bearing Assembly, Type I	Each	12
Waterproofing Membrane System	Sq. Yd.	1174
Concrete Sealer	Sq. Ft.	2260
Structural Repair of Concrete		
Depth equal to or less than 5 inches	Sq. Ft.	93
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	10.5
Deck Slab Repair (Partial)	Sq. Yd.	54.2
Protective Shield	Sq. Yd.	380
Anchor Bolts, 1"	Each	48
Furnishing & Erecting Structural Steel	Pound	2320

INDEX OF SHEETS

- 1 General Plan
- 2 Staging Typical
- 3&4 Deck Joint Repairs
- 5 Preformed Joint Strip Seal
- 6 Bearing Details
- 7 Deck Patching Plan
- 8 Abutment Patching Plan
- 9 Bar Splicer Assembly and Mechanical Splicer Details
- 10 Temporary Concrete Barrier

LOADING HS20-44

Allow 25#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS

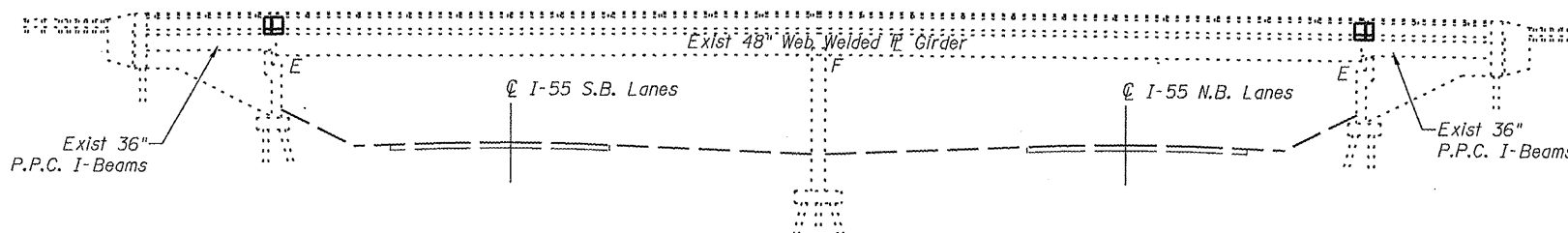
2002 AASHTO

DESIGN STRESSES

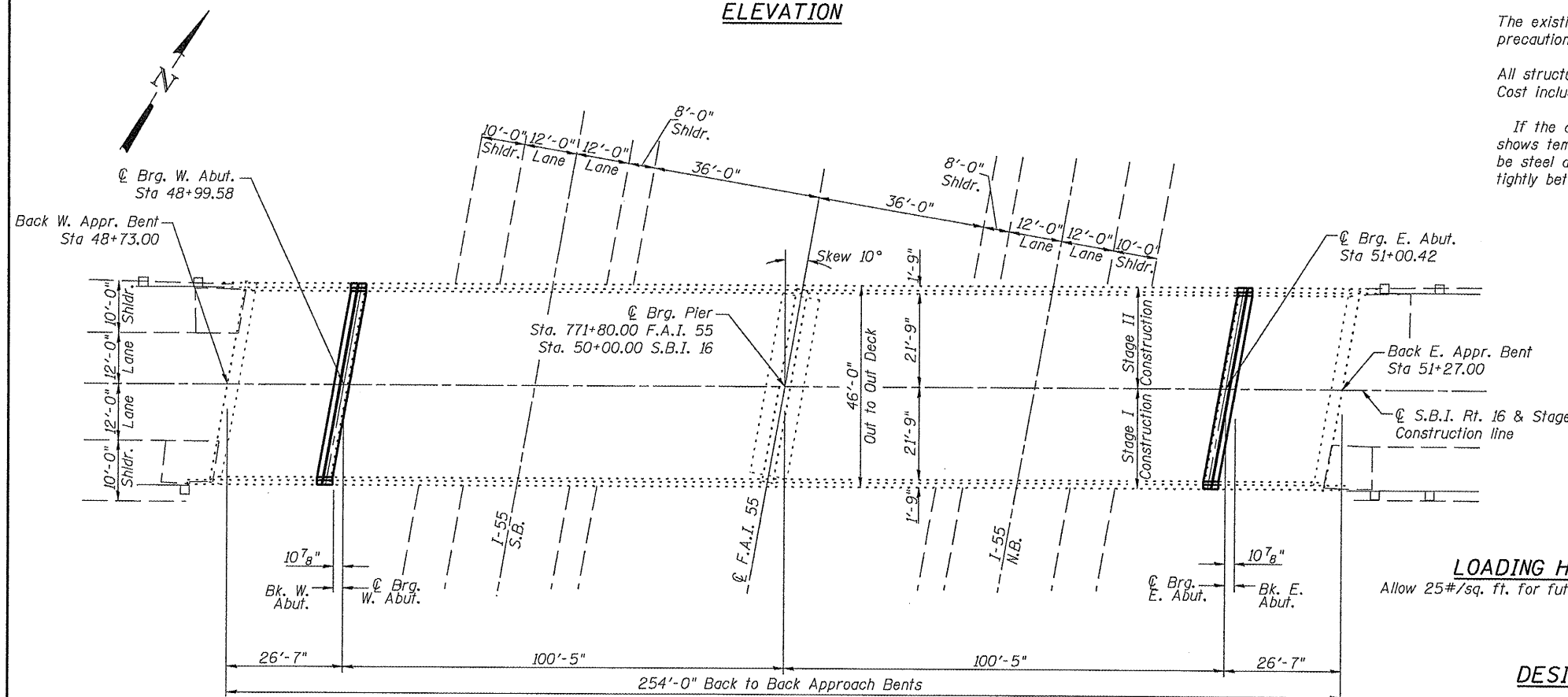
FIELD UNITS
 $f'_c = 3,500$ psi
 $f_y = 60,000$ psi (reinforcement)
 $f_y = 36,000$ psi (structural steel)

GENERAL PLAN

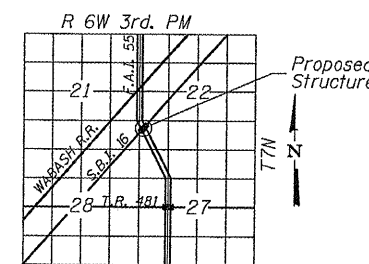
WILLIAMSON RD OVER I-55
 S.B.I. RTE. 16 OVER F.A.I. 55
 SECTION 59RS-2, BR
 MACOUPIN COUNTY
 STATION 30+00.00
 STRUCTURE NO. 059-0044



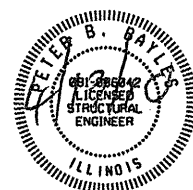
ELEVATION



PLAN



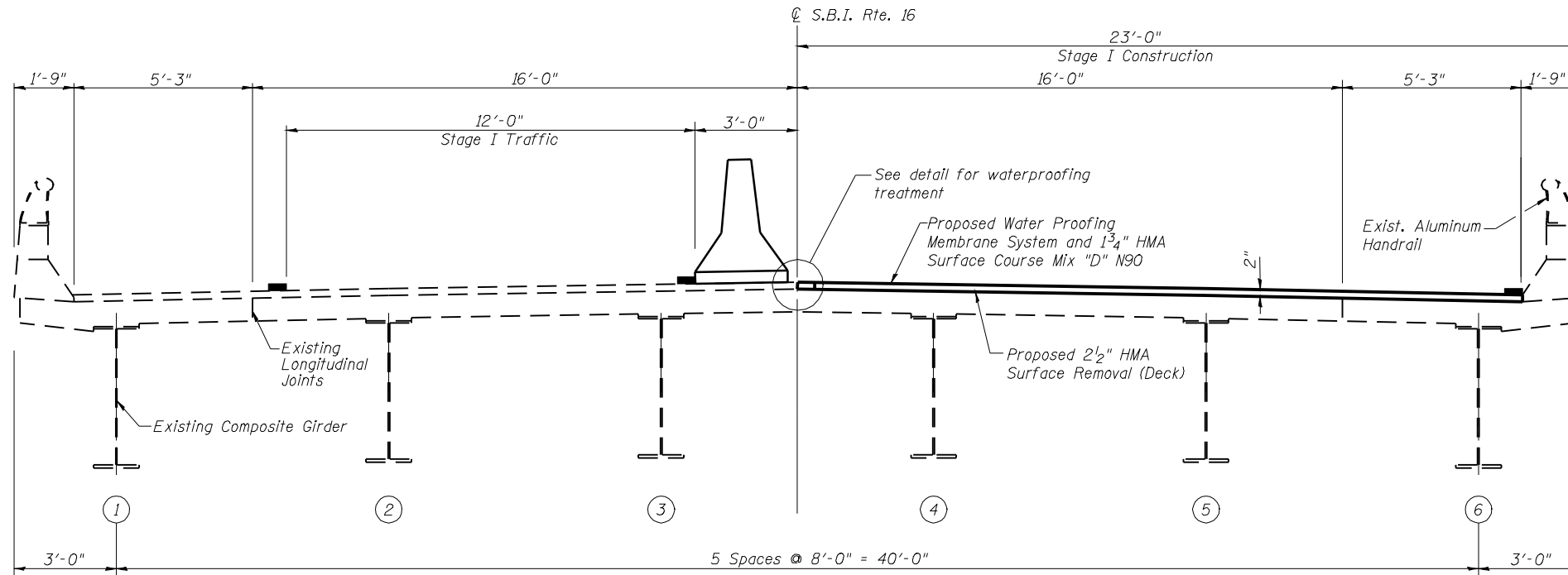
LOCATION SKETCH



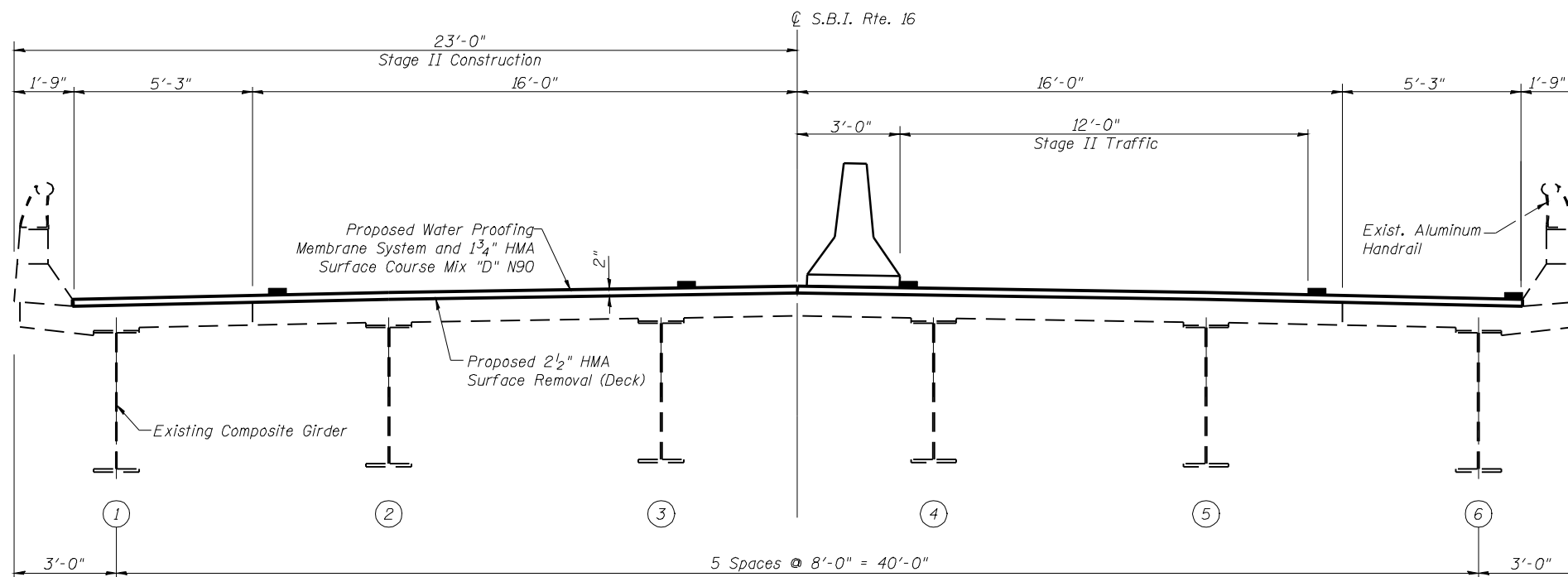
Peter B. Boyles, P.E., S.E.
 Structural Engineer License No. 081-006042
 Expiration Date: 11/30/2010

SHEET NO. 1 10 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT		

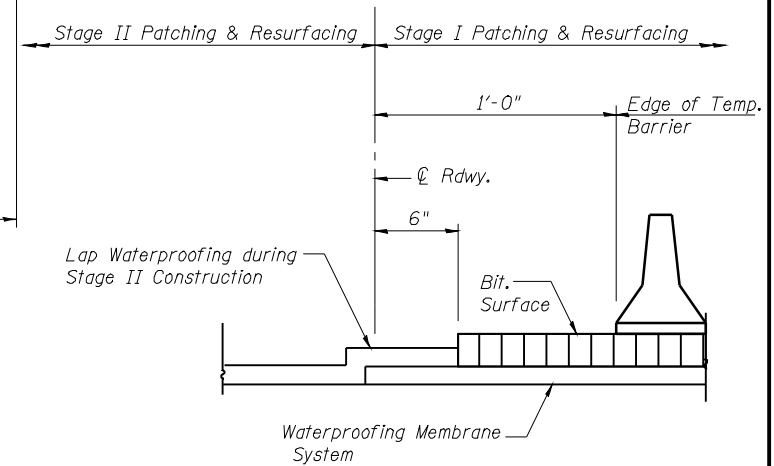
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



STAGE I TYPICAL BRIDGE SECTION
(Looking East)



STAGE II TYPICAL BRIDGE SECTION
(Looking East)



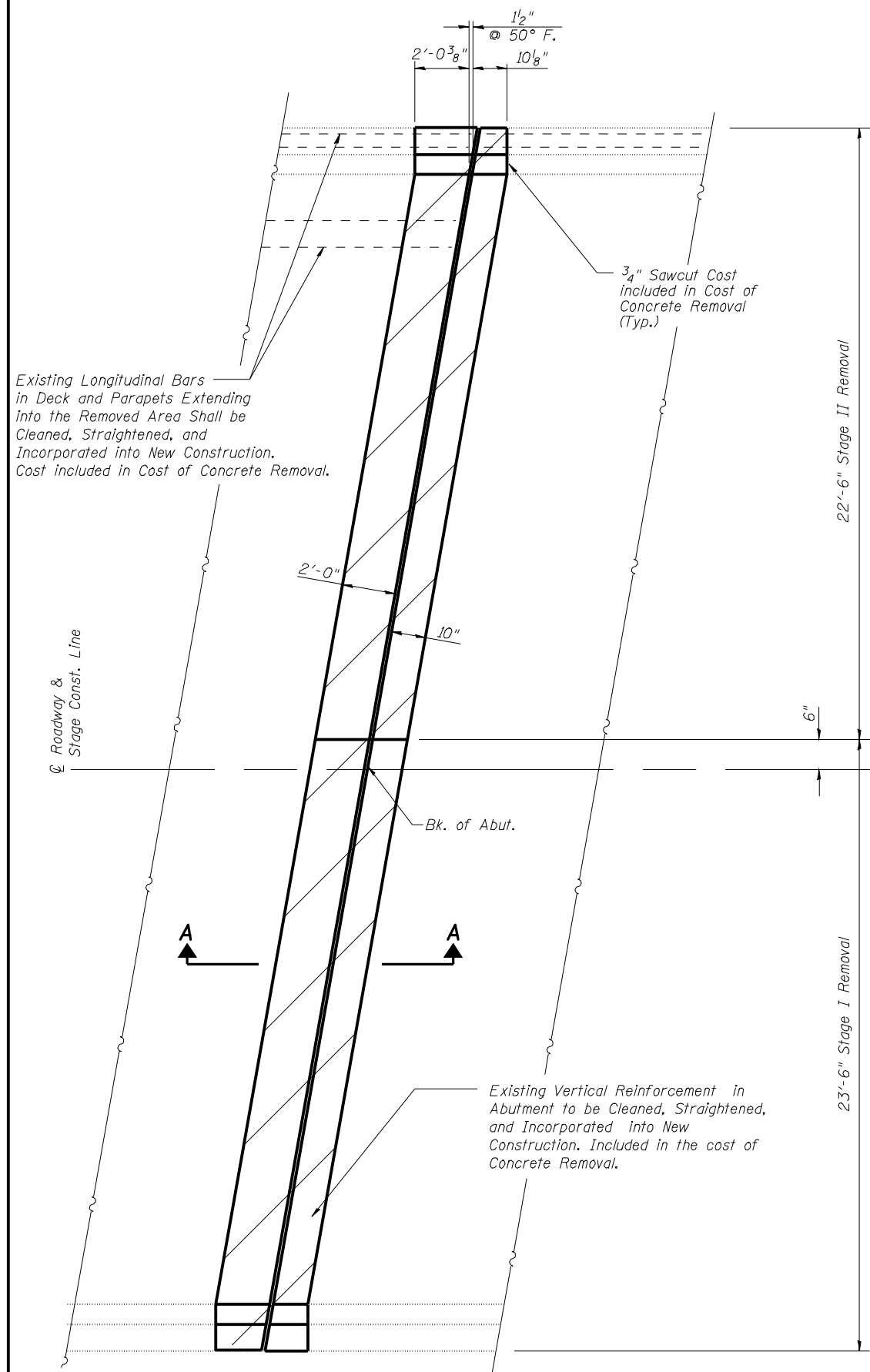
**WATERPROOFING TREATMENT
AT STAGE CONSTRUCTION**

Note:
See Sheet 7 of 10 for deck slab
repair details. Main span shown.
Staging similar for approach spans.

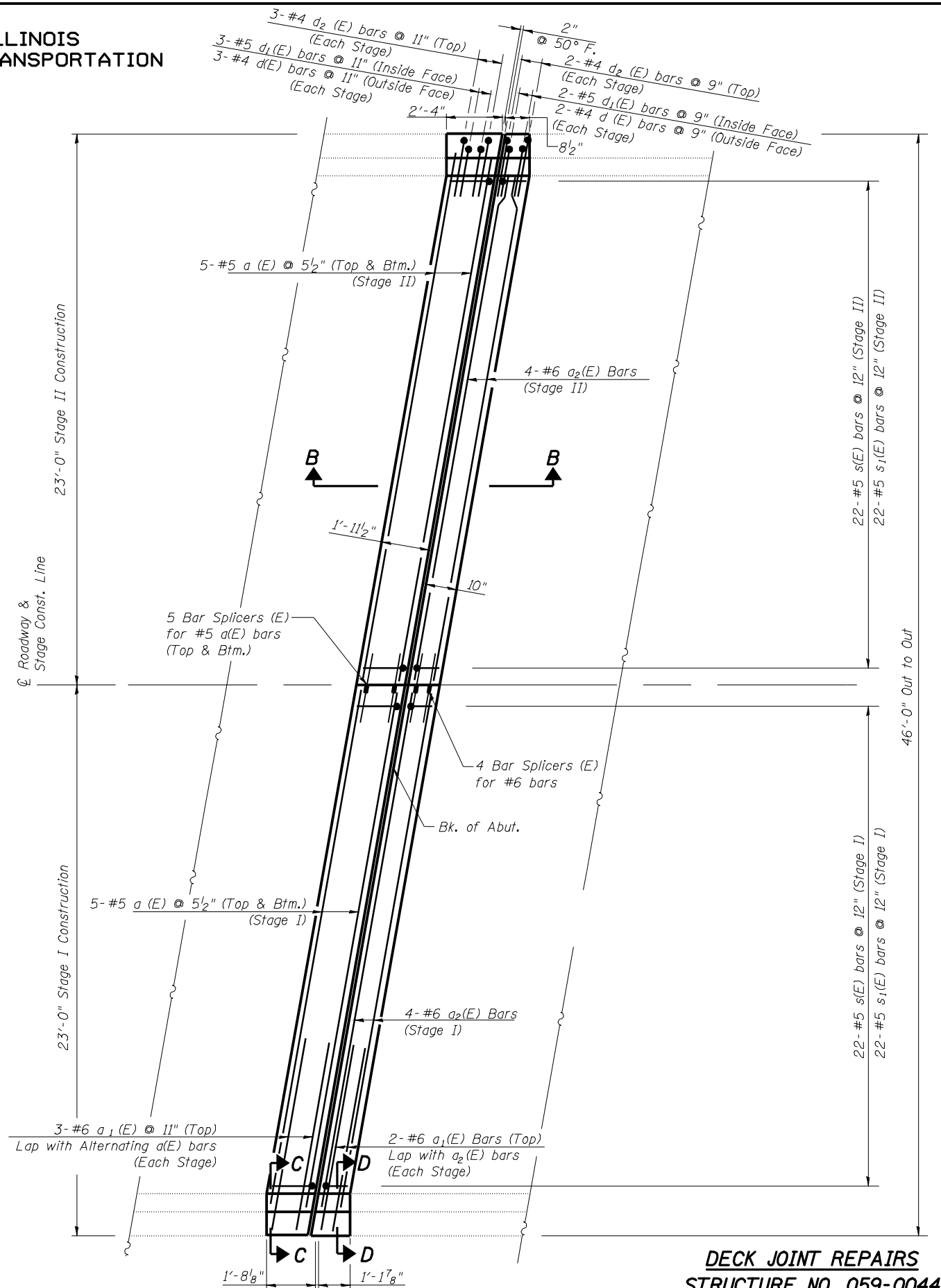
**STAGING TYPICAL
STRUCTURE NO. 059-0044**

SHEET NO. 2 10 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 72A60		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



CONCRETE REMOVAL PLAN
(E. Abutment Shown)
(W. Abutment Sim.)

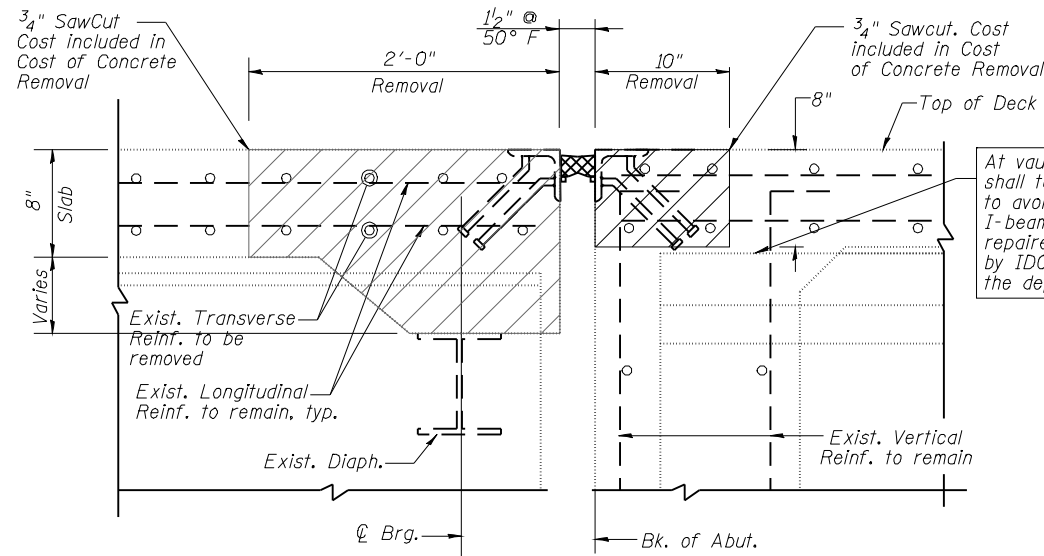


CONCRETE REPLACEMENT PLAN
(E. Abutment Shown)
(W. Abutment Sim.)

DECK JOINT REPAIRS
STRUCTURE NO. 059-0044

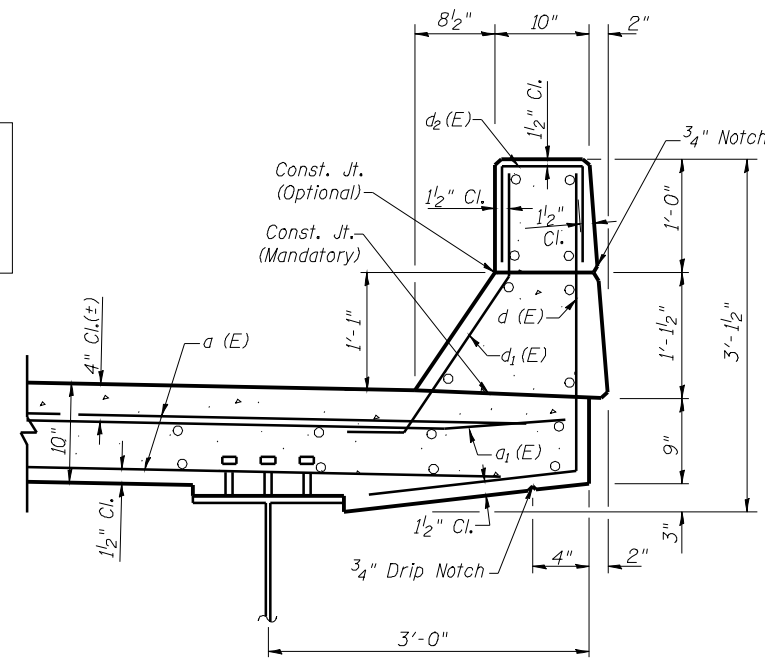
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FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

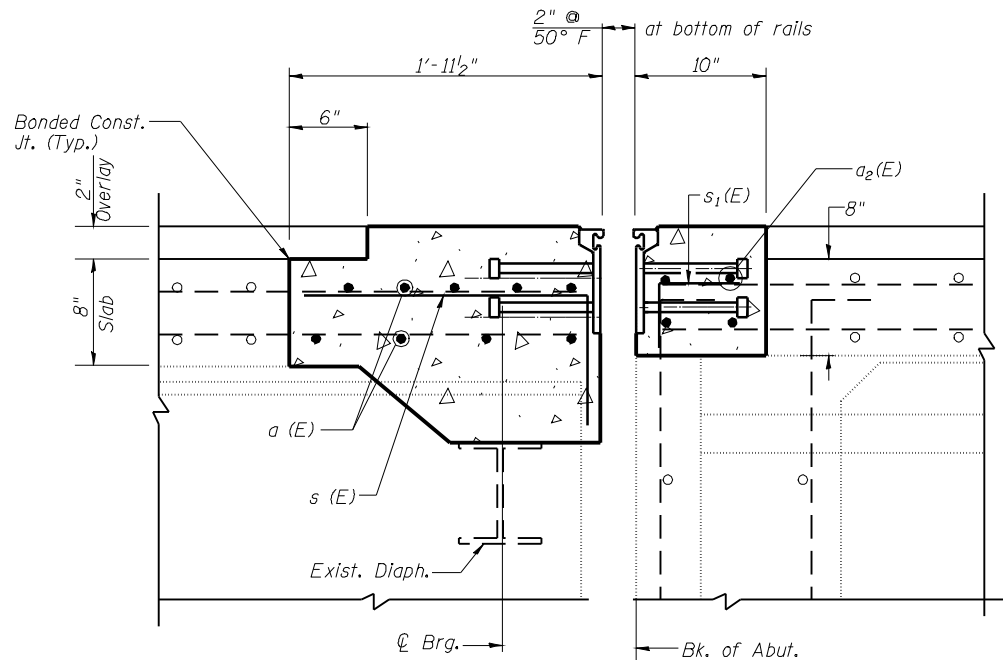
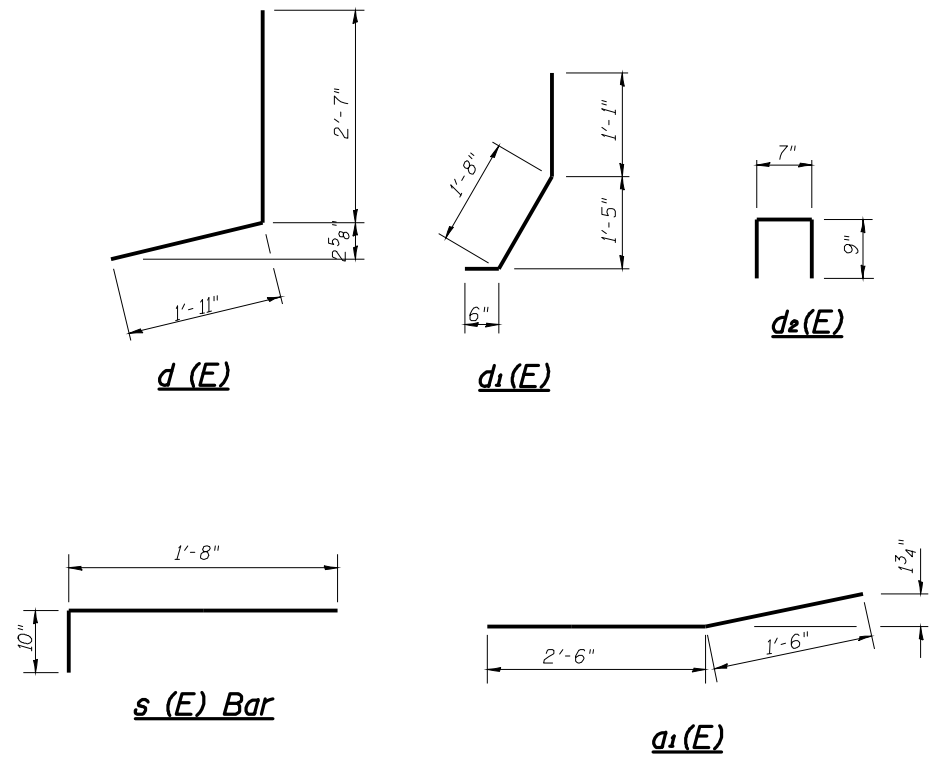


SECTION A-A
(Showing Deck Removal)
(dim at right L's)

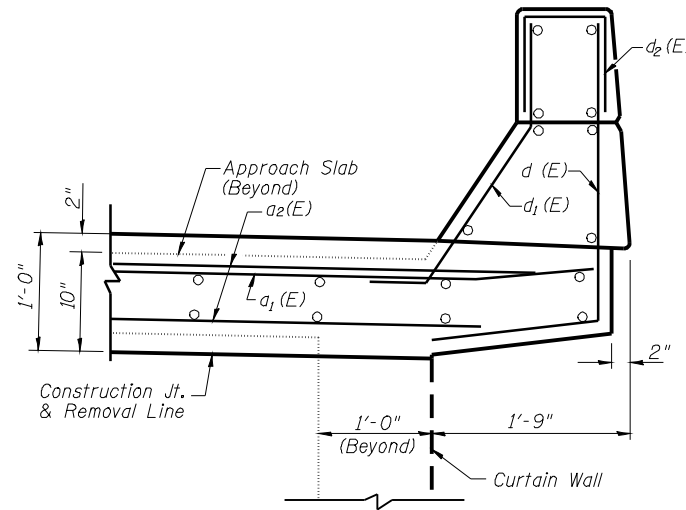
At vaulted abutments Contractor shall take necessary precautions to avoid damaging the P.P.C. I-beams. Any damage shall be repaired as approved and directed by IDOT at no additional cost to the department.



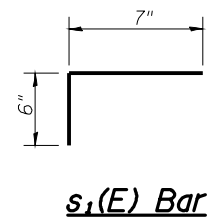
SECTION C-C
(Showing Proposed Parapet at Deck)



SECTION B-B
(Showing Proposed Deck)
(dim at right L's)



SECTION D-D
(Showing Proposed Parapet at Approach)



NOTES:

- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with the cost of Concrete Removal.
- Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when ambient temperature is other than 50° F.
- Removal of all existing expansion joints shall be included in the cost of Concrete Removal.

BILL OF MATERIAL
(E. & W. Abut.)

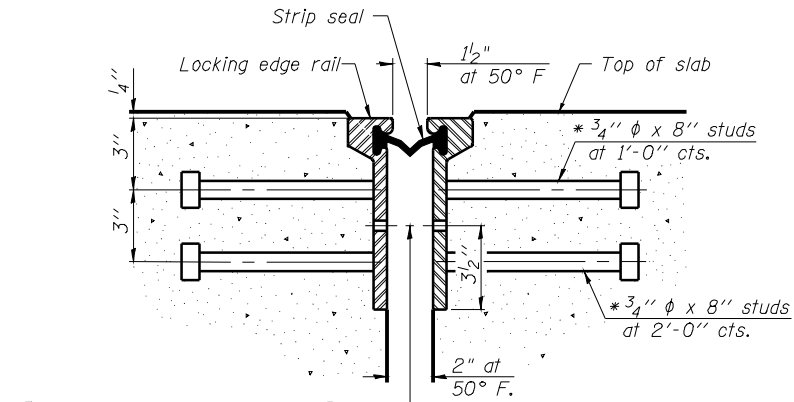
Bar	No.	Size	Length	Shape
a (E)	40	#5	22'-3"	—
a1 (E)	20	#6	4'-0"	—
a2 (E)	16	#6	22'-3"	—
d (E)	20	#4	4'-6"	J
d1 (E)	20	#5	3'-3"	J
d2 (E)	20	#4	2'-1"	□
s (E)	88	#5	2'-6"	—
s1 (E)	88	#5	1'-1"	—
Concrete Removal			Cu. Yd.	9.5
Concrete Superstructure			Cu. Yd.	10.8
Reinforcement Bars, Epoxy Coated			Pound	2070
Bar Splicers			Each	28
Protective Coat			Sq. Yd.	31

DECK JOINT REPAIR DETAILS
STRUCTURE NO. 059-0044

SHEET NO. 4 10 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	59RS-2, BR	Macoupin	100	84
FED. ROAD DIST. NO.			ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 72A60					

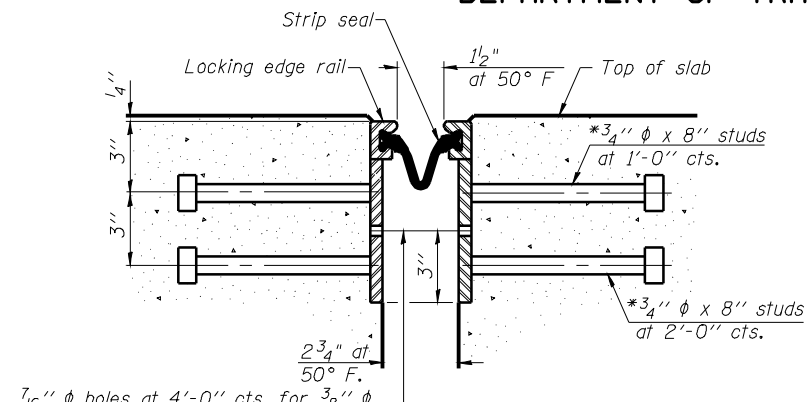
* Granular or solid Flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



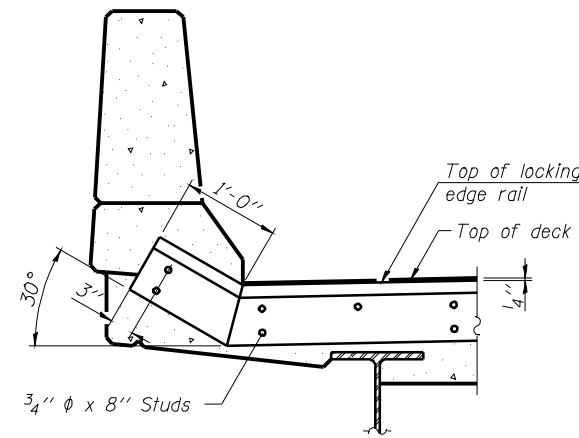
7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

**SECTION THRU
ROLLED RAIL JOINT**

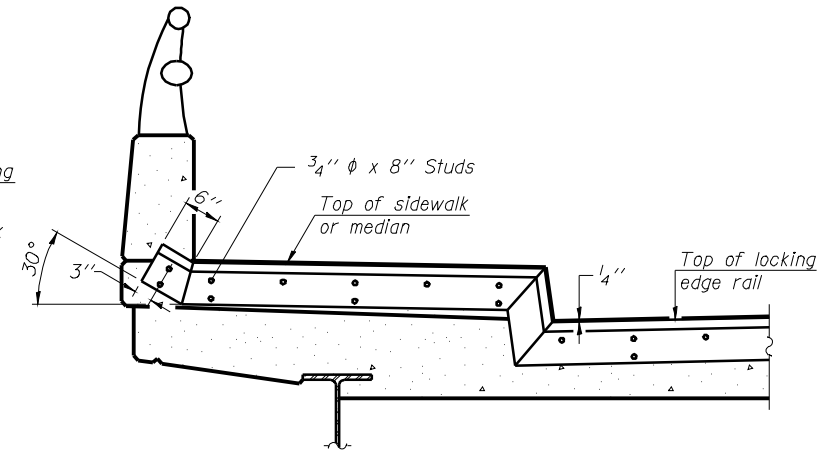


7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

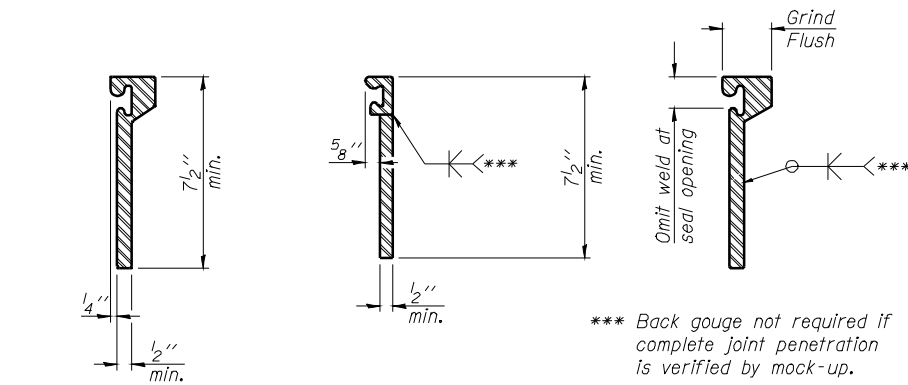
**SECTION THRU
WELDED RAIL JOINT**



AT PARAPET
See Section A-A for end treatment of skews > 30°.

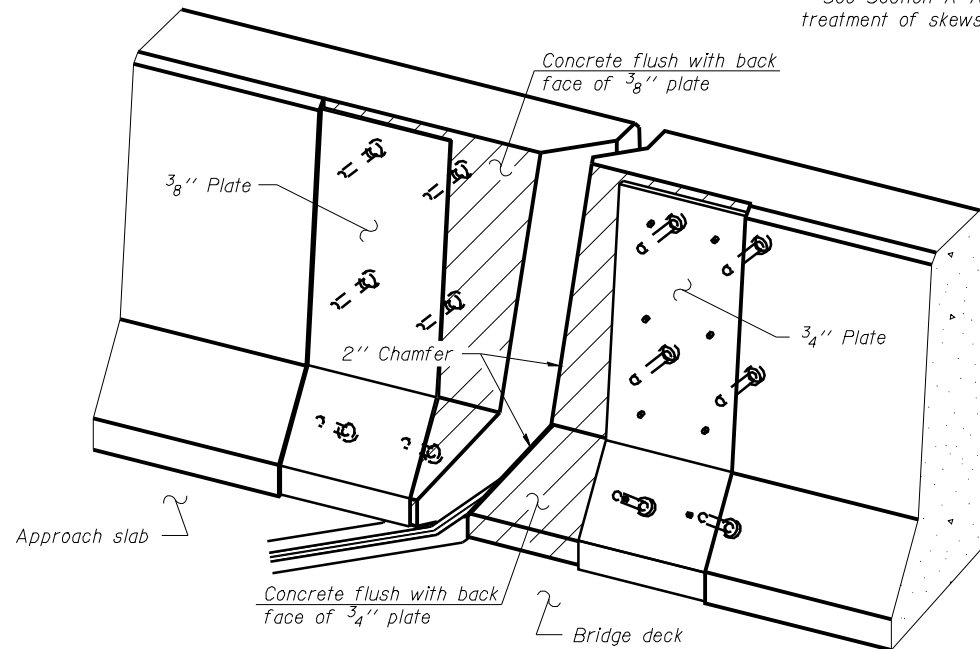


AT SIDEWALK OR MEDIAN
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



**ROLLED
EXTRUDED RAIL**

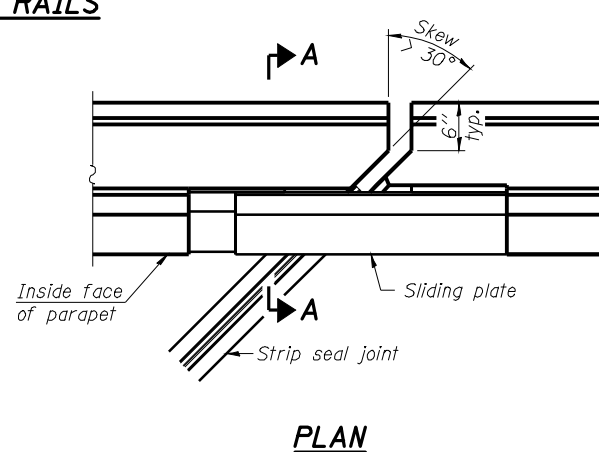
**LOCKING EDGE
RAIL SPLICE**
The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.



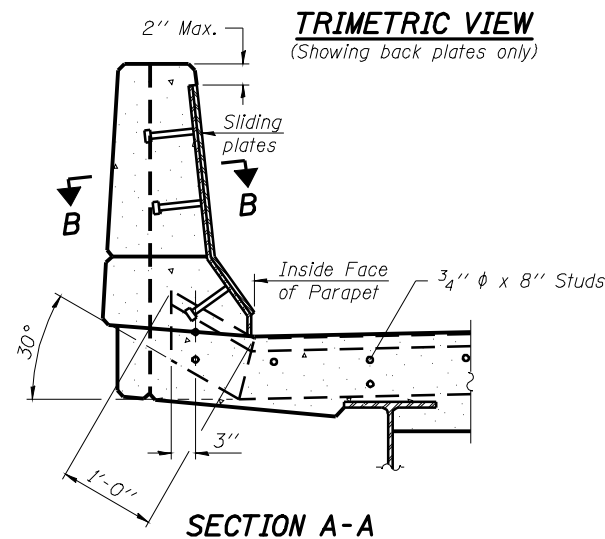
TYPICAL END TREATMENTS

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
The manufacturer's recommended installation methods shall be followed.
The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

LOCKING EDGE RAILS

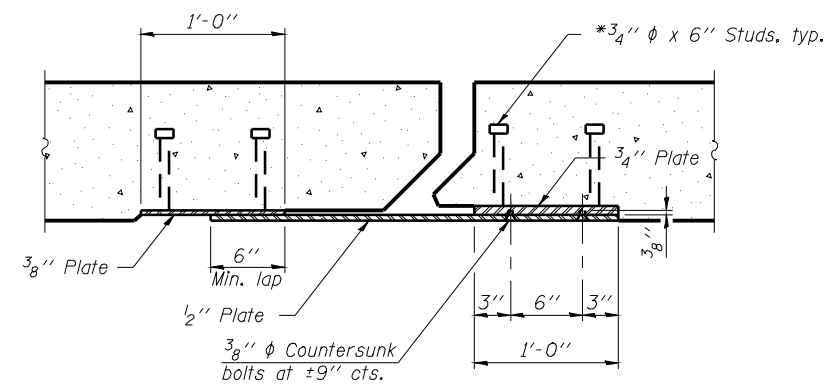


PLAN



SECTION A-A

POINT BLOCK DETAILS
(for skews > 30°)



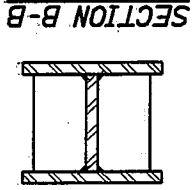
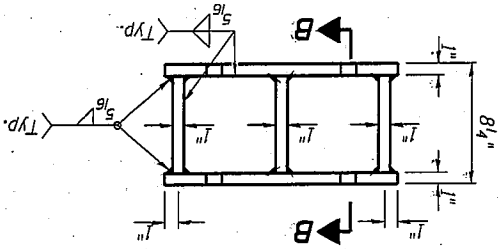
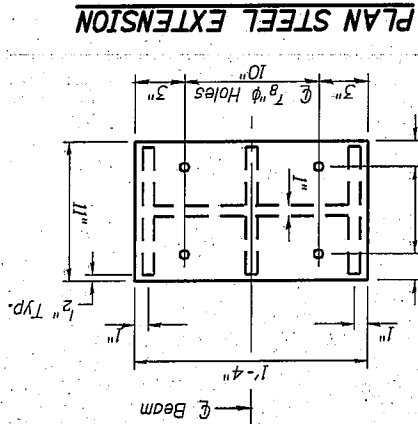
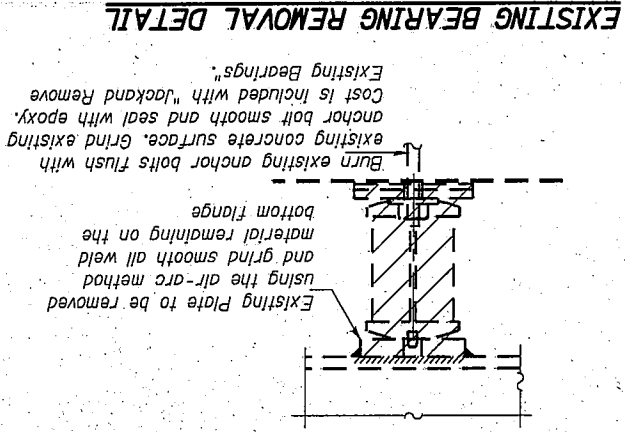
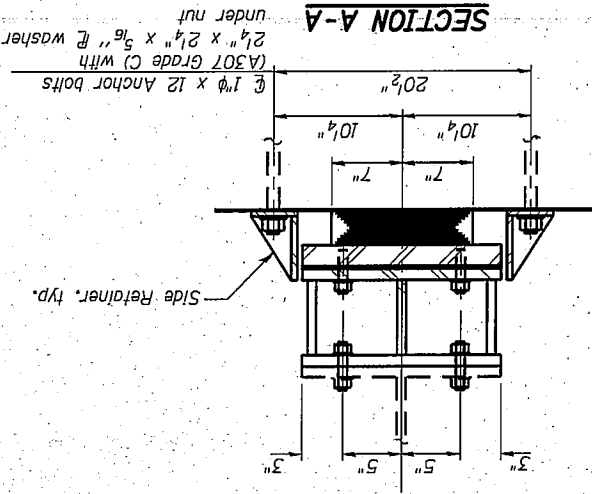
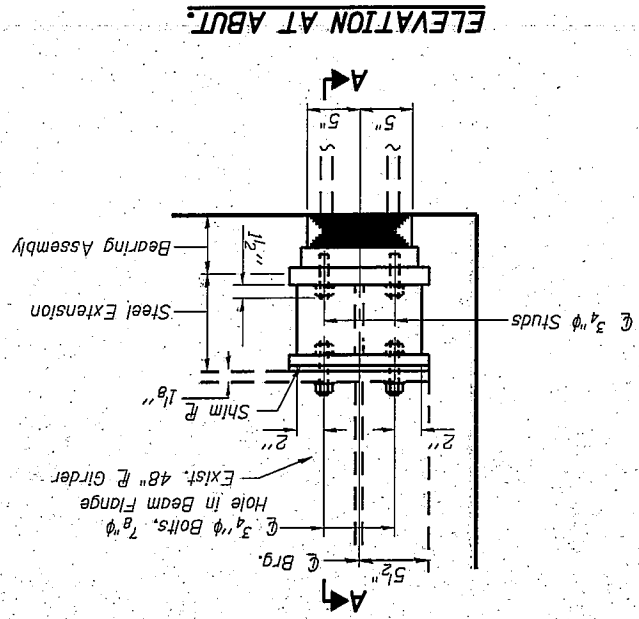
SECTION B-B

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	91

**PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 059-0044**

SHEET NO. 5 10 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	59RS-2, BR	Macoupin	100	85
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT		
CONTRACT NO. 72A60					



BILL OF MATERIAL

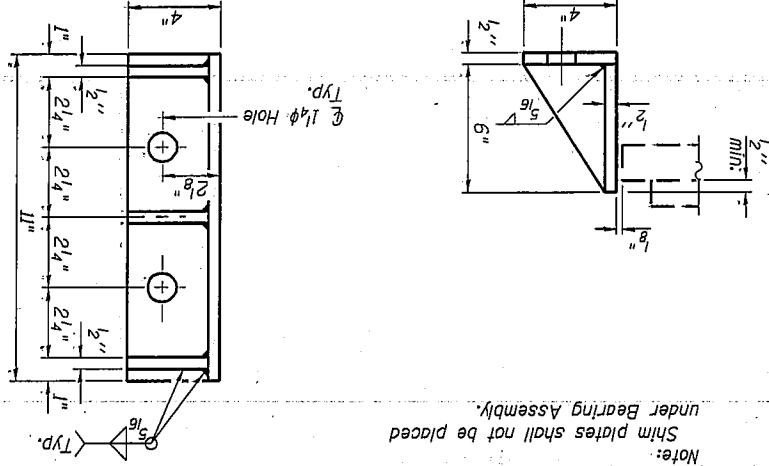
Item	Unit	Total
Elastomeric Bearing Assembly Type I	Each	12
Anchor Bolts, 1"	Each	48
Jack & Remove	Each	12
Exist. Bearings	Each	12
Furnishing & Erecting Structural Steel	Pound	2320

Notes:
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified in lieu of ASTM F1554. Drilled and set anchor bolts shall be installed according to Article 52.06 of the Standard Specifications. Side retainers and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I. Steel Extensions, Shims and Bolts shall be included in the cost of Furnishing and Erecting Structural Steel. Painting of steel shall be according to Article 506.05 of the Standard Specifications. Provide two 8" adjusting shims for each bearing location. Adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details. Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts. Bolts 3/4" in 1/8" holes unless otherwise noted. Prior to ordering any material for shims or extensions, the contractor shall verify in the field all bearing height and shim thickness dimensions.

BEAM REACTIONS

R/D (K)	49.3
R/L (K)	46.5
Imp. (K)	10.2
R (Total)(K)	106.0

Min. Jack capacity = 60 Tons

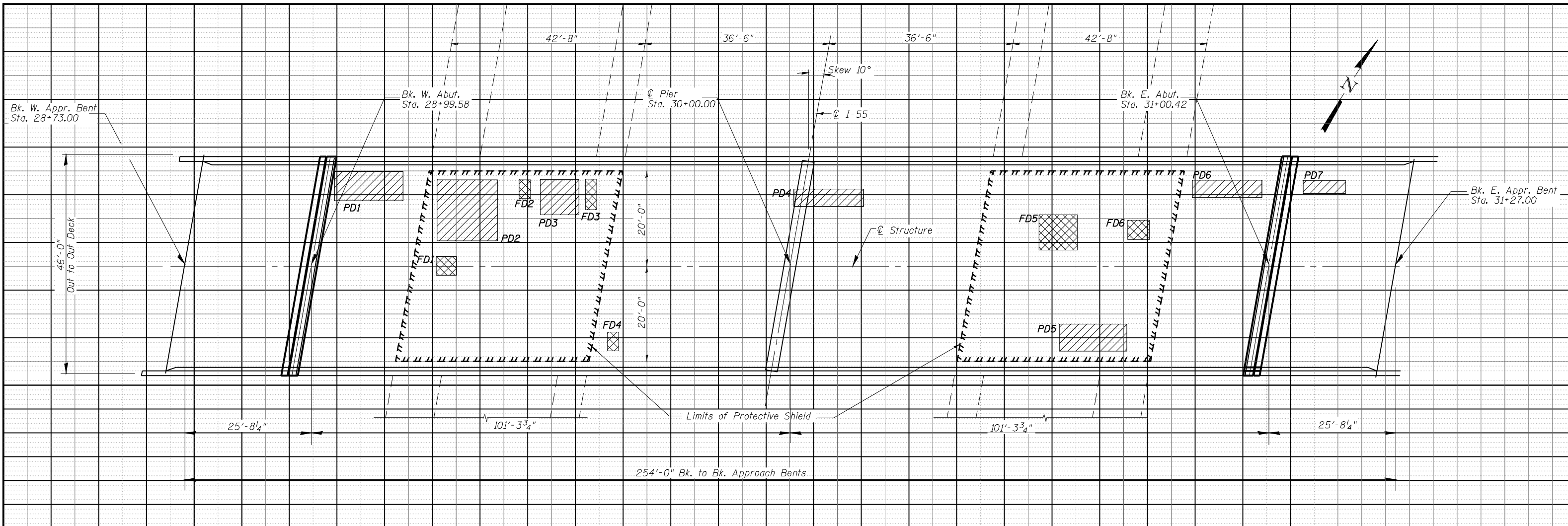


Notes:
Shim plates shall not be placed under Bearing Assembly.

SIDE RETAINER
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

BEARING DETAILS
STRUCTURE NO. 059-0044

SHEET NO. 6		SECTION		COUNTY		TOTAL SHEETS	
55	59RS-2, BR	Macoupin	100	86	CONTRACT NO. 72A60		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		10 SHEETS			



Patch No.	Size	Deck Slab Repair (Part Depth)	Deck Slab Repair (FD TY II)
PD1	6x12	8.0	-
PD2	10x10	11.1	-
FD1	4x4	-	1.8
FD2	4x2	-	0.9
PD3	6x6	4.0	-
FD3	5x2	-	1.1
FD4	4x2	-	0.9
PD4	4x20	8.9	-
FD5	6x6	-	4.0
PD5	6x15	10.0	-
FD6	4x4	-	1.8
PD6	4x20	8.9	-
PD7	3x10	3.3	-

Patch No.	Size	Deck Slab Repair (Part Depth)	Deck Slab Repair (FD TY II)

Patch No.	Size	Deck Slab Repair (Part Depth)	Deck Slab Repair (FD TY II)

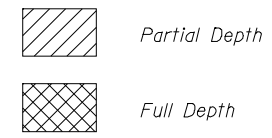
Note:
Following removal of HMA Surface, the Resident Engineer shall inspect & sound existing deck.

Patching Areas will be marked and documented by Resident Engineer.

Quantities and repair areas shown are estimated. Actual quantities to be determined by the Resident Engineer.

Protective Shield shall be installed as directed by resident engineer to protect traffic below. Protection shall be installed to limits shown above if full depth patching is required in that span.

PATCHING LEGEND



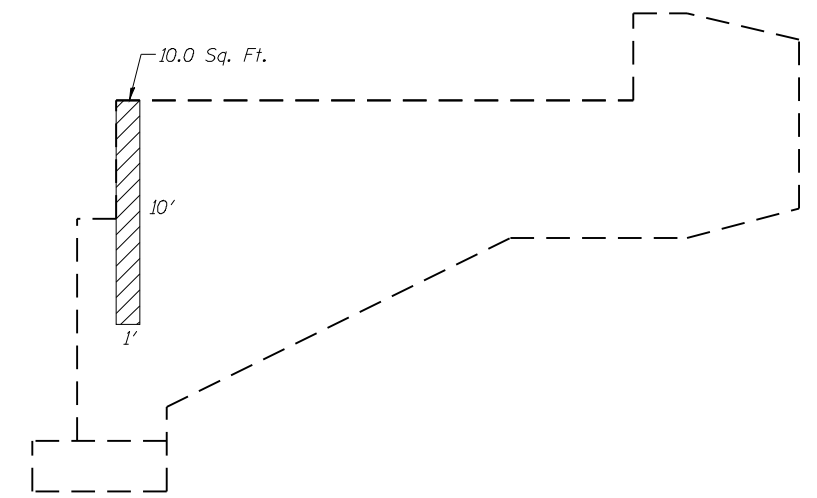
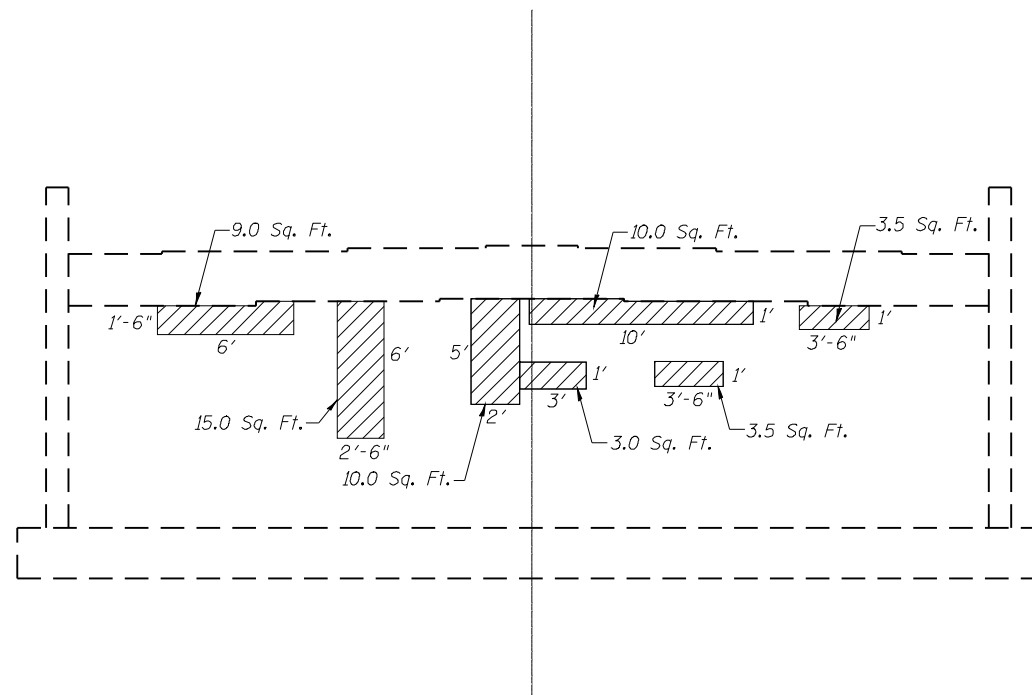
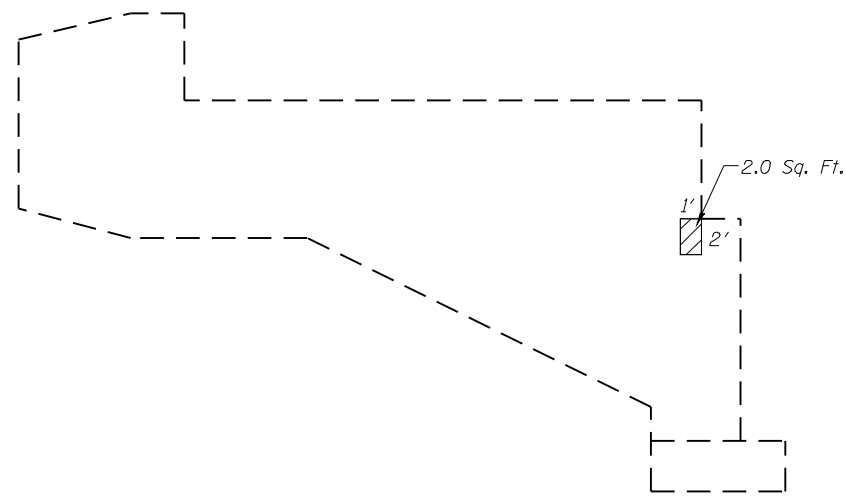
BILL OF MATERIAL

Item	Unit	Total
Deck Slab Repair (Partial)	Sq. Yd.	54.2
Deck Slab Repair (Full Depth Type II)	Sq. Yd.	10.5
Protective Shield	Sq. Yd.	380

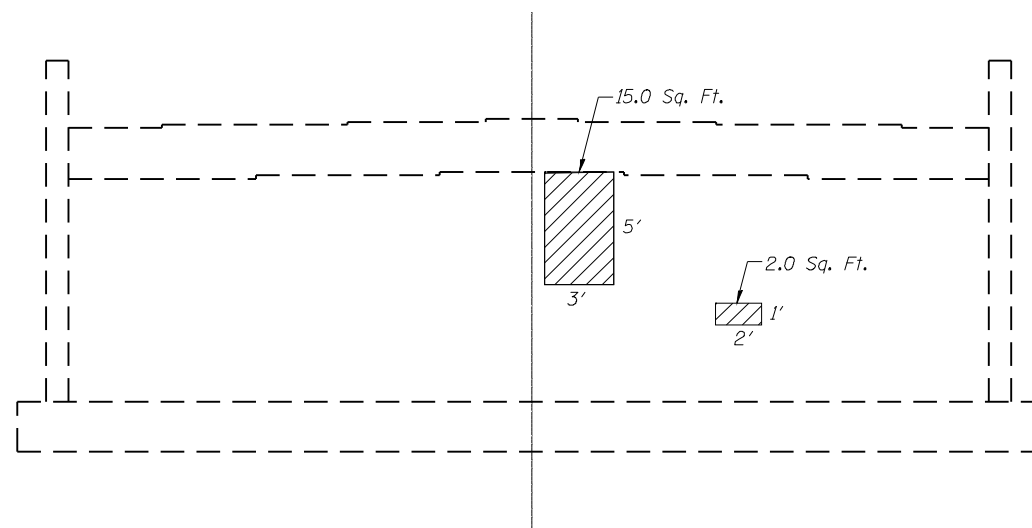
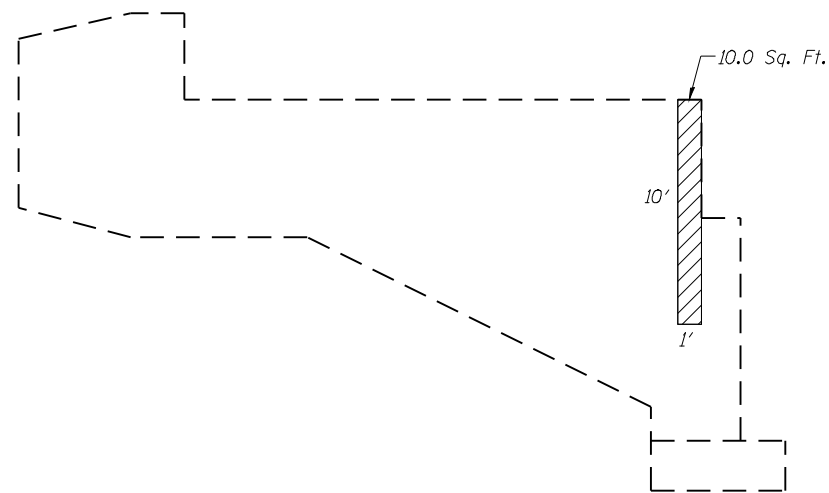
**DECK PATCHING PLAN
STRUCTURE NO. 059-0044**

SHEET NO. 7 10 SHEETS	F.A.I. RTE. 55	SECTION 59RS-2, BR	COUNTY Macoupin	TOTAL SHEETS 100	SHEET NO. 87
	CONTRACT NO. 72A60				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
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EAST ABUTMENT



WEST ABUTMENT

Notes:
Quantities and repair areas shown are estimated, actual quantities to be determined by the Resident Engineer.
Concrete Sealer to be applied to exposed vertical faces and beam seats of the abutments.
The concrete sealer shall be a pigmented epoxy sealer from the department's approved product list for concrete sealers. It shall be applied in two coats. The color shall be per Federal Standard 595B Grey #36622."

Structural Repair of Concrete \leq 5"

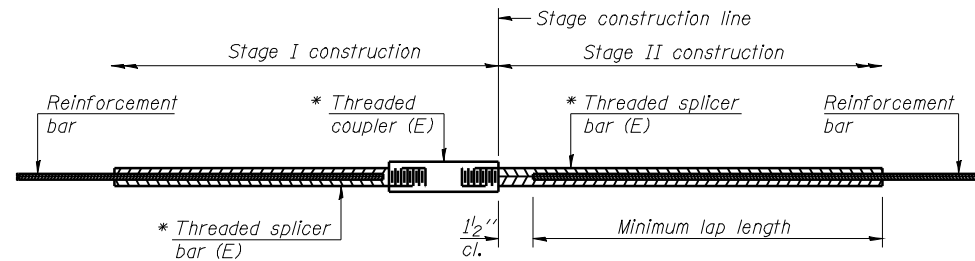
BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete Depth equal to or less than 5 inches	Sq. Ft.	93
Concrete Sealer	Sq. Ft.	2260

**ABUTMENT PATCHING PLAN
STRUCTURE NO. 059-0044**

SHEET NO. 8 10 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	59RS-2, BR	Macoupin	100	88
CONTRACT NO. 72A60					
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

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STANDARD BAR SPLICER ASSEMBLY

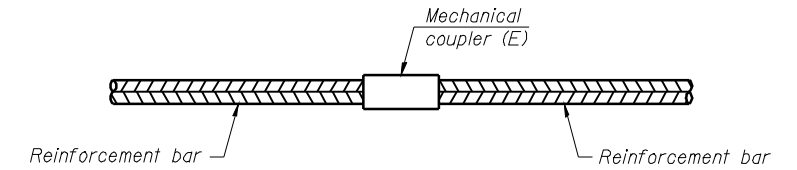
Minimum Lap Lengths				
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4
3, 4	1'-5"	1'-11"	2'-1"	2'-4"
5	1'-9"	2'-5"	2'-7"	2'-11"
6	2'-1"	2'-11"	3'-1"	3'-6"
7	2'-9"	3'-10"	4'-2"	4'-8"
8	3'-8"	5'-1"	5'-5"	6'-2"
9	4'-7"	6'-5"	6'-10"	7'-9"

Table 1: Black bar, 0.8 Class C
Table 2: Black bar, Top bar lap, 0.8 Class C
Table 3: Epoxy bar, 0.8 Class C
Table 4: Epoxy bar, Top bar lap, 0.8 Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

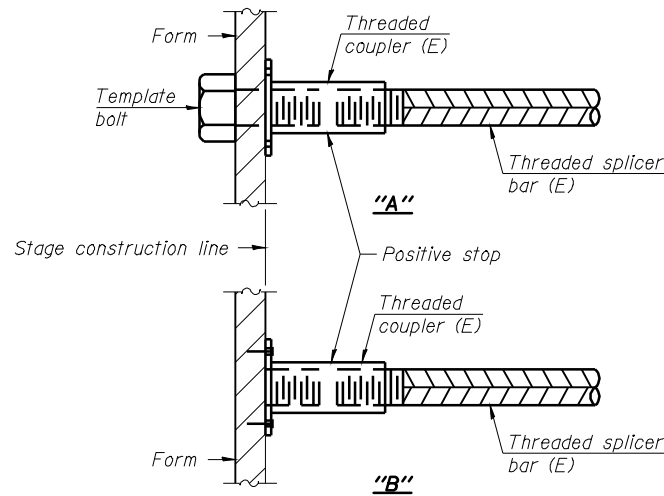
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
W. Approach	#6	4	4
W. Deck	#5	10	3
E. Deck	#5	10	3
E. Approach	#6	4	4



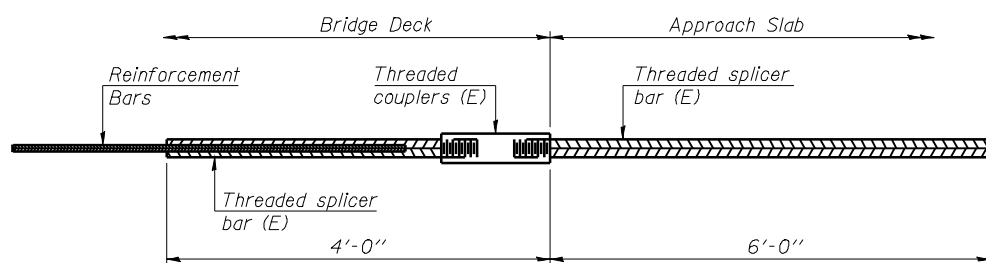
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



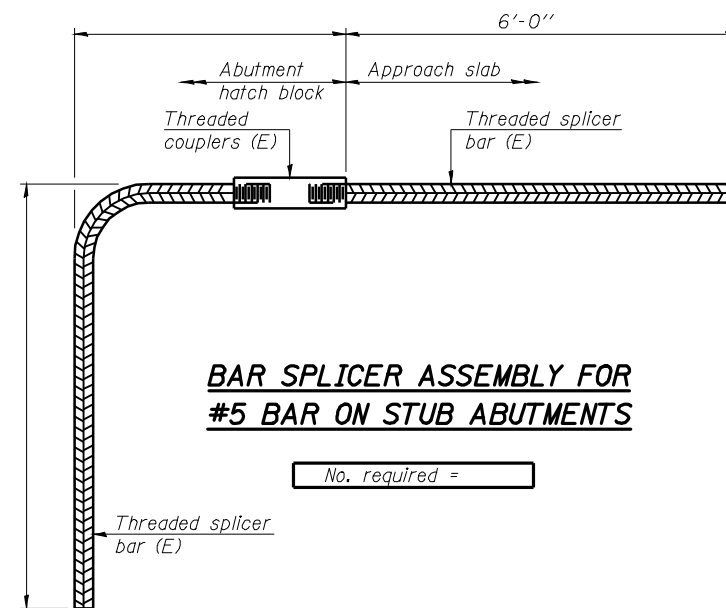
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

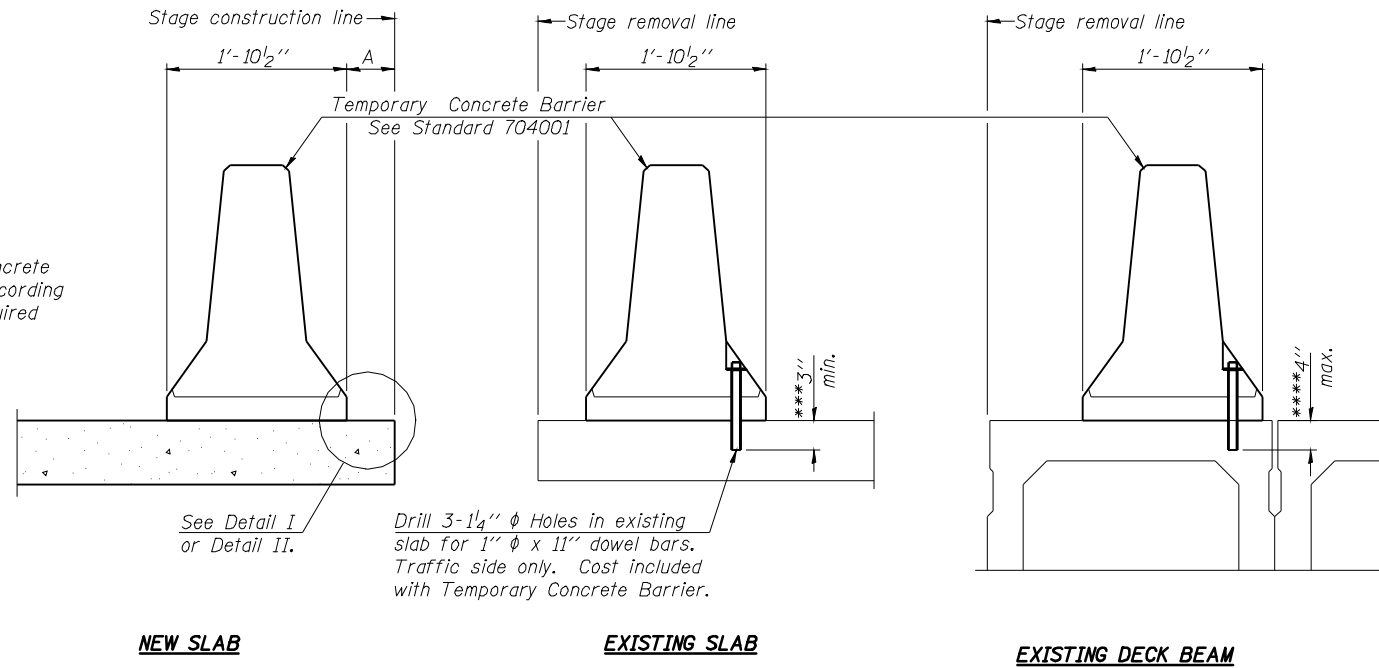
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See special provision for Mechanical Splicers.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 059-0044**

SHEET NO. 9 10 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	59RS-2, BR	Macoupin	100	89
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT		
CONTRACT NO. 72A60					

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When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



Drill 3-1/4" ϕ Holes in existing slab for 1" ϕ x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

NOTES

Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{P} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate ϕ of each barrier panel.

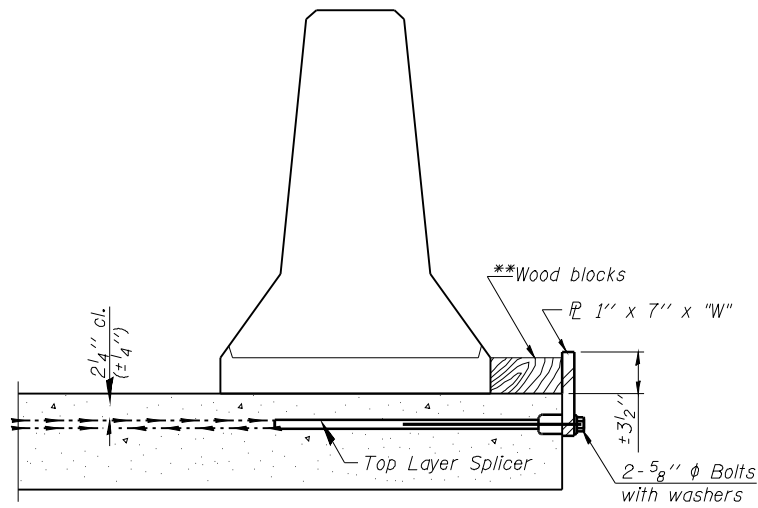
Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel \bar{P} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate ϕ of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

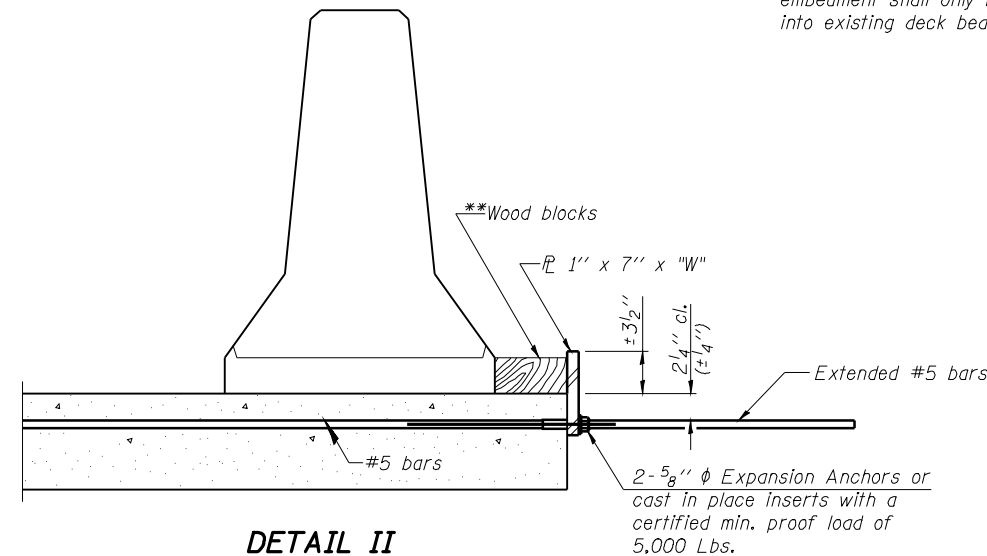
SECTIONS THRU SLAB OR DECK BEAM

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



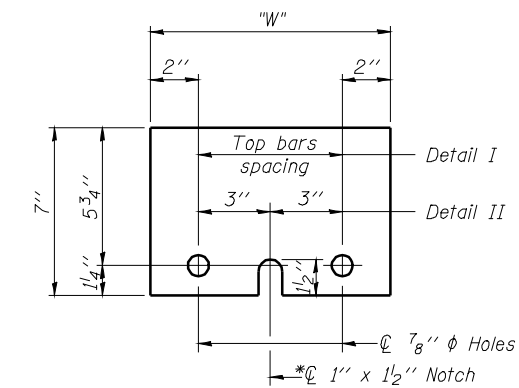
DETAIL I



DETAIL II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"



STEEL RETAINER \bar{P} 1" x 7" x 10"

* Required only with Detail II

**TEMPORARY CONCRETE BARRIER
FOR STAGE CONSTRUCTION
STRUCTURE NO. 059-0044**

SHEET NO. 10	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
10 SHEETS	55	59RS-2, BR	Macoupin	100	90
			CONTRACT NO. 72A60		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

S.N. 059-0045 was originally built in 1975. It is a two-span, steel continuous structure with vaulted concrete approach slabs. The structure length is 252'-0" back-to-back approach bents and the width is 68'-0" out-to-out. The two steel span lengths are both 109'-0" and the approach spans are both 16'-2".

Structure to be repaired using staged construction. Bridge deck joints, abutment bearings and wearing surface to be replaced.

No Salvage.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

No field welding is permitted except as specified in the contract documents.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that can not be removed by grinding 1/4 in. deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Protective coat shall not be applied to surfaces to which Waterproofing Membrane System is applied.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

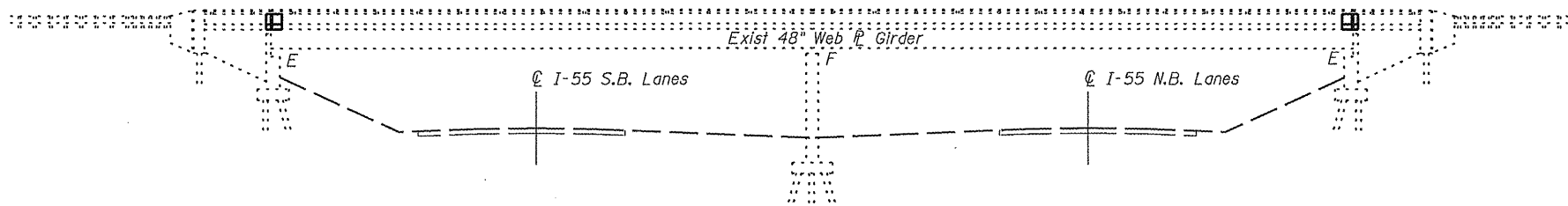
All structural steel shall be shop painted with inorganic zinc rich primer per AASHTO M300, Type I. Cost included with Furnishing and Erecting Structural Steel.

If the analysis submitted to the Contractor for the jacking/temporary support system to be used shows temporary stiffeners are required to prevent web crippling or buckling, the stiffeners shall be steel and bolted to the web. If stiffeners are not required, hardwood timbers shall be installed tightly between the top and bottom flange to prevent flange rotation.

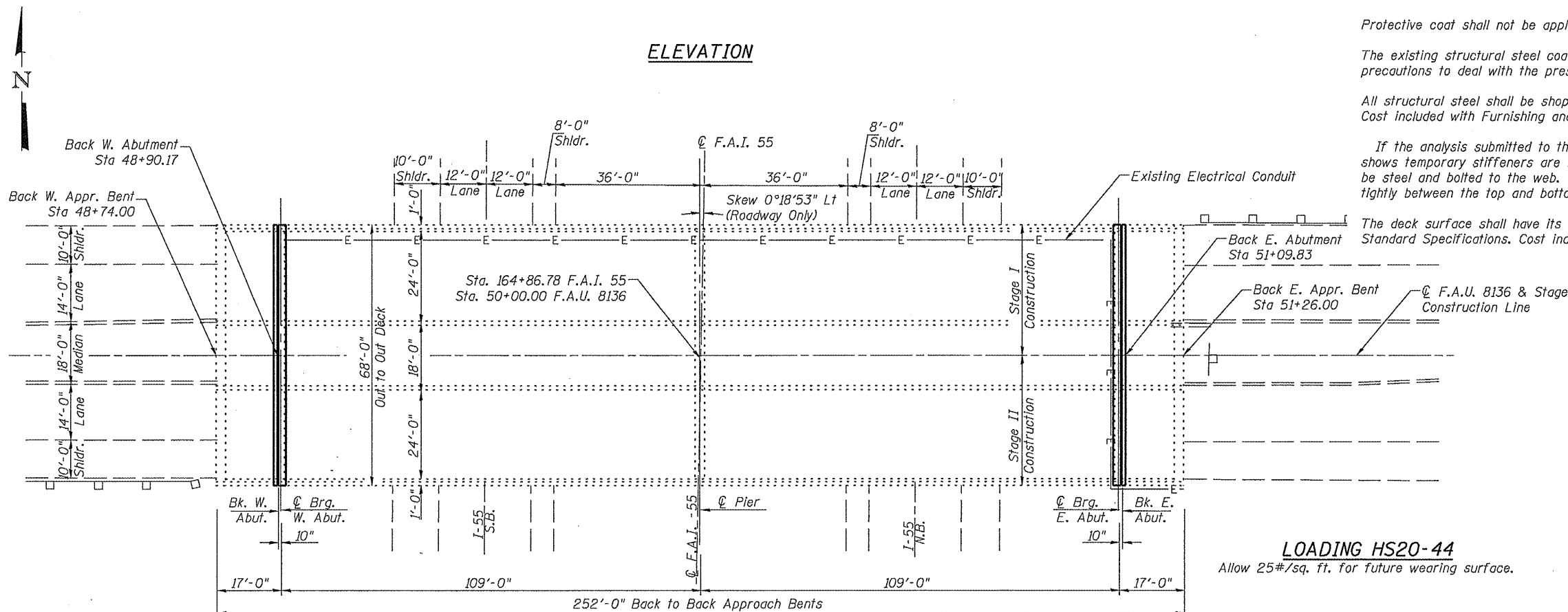
The deck surface shall have its final finish tined according to Article 420.09 (e)(1) of the Standard Specifications. Cost included with Concrete Superstructures.

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
HMA Surface Course, Mix "C", N50	Ton	127.8
HMA Surface Removal (Deck)	Sq. Yd.	1301
Concrete Removal	Cu. Yd.	13.5
Concrete Superstructure	Cu. Yd.	15.1
Protective Coat	Sq. Yd.	45
Jack and Remove Existing Bearings	Each	18
Reinforcement Bars, Epoxy Coated	Pound	3240
Bar Splacers	Each	32
Preformed Joint Strip Seal	Foot	134
Elastomeric Bearing Assembly, Type I	Each	18
Waterproofing Membrane System	Sq. Yd.	1306
Concrete Sealer	Sq. Ft.	1908
Structural Repair of Concrete	Sq. Ft.	79
Depth equal to or less than 5 inches	Sq. Yd.	43.9
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	60.3
Deck Slab Repair (Partial)	Sq. Yd.	440
Protective Shield	Pound	3470
Furnishing & Erecting Structural Steel	Each	72
Anchor Bolts, 1"		



ELEVATION



PLAN

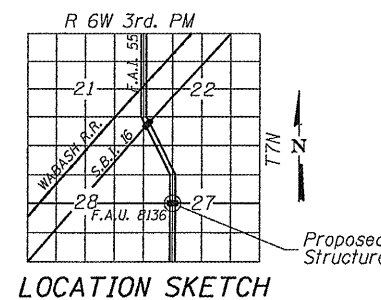
LOADING HS20-44

Allow 25# / sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS
2002 AASHTO

INDEX OF SHEETS

- 1 General Plan
- 2 Staging Typical
- 3&4 Deck Joint Repairs
- 5 Preformed Joint Strip Seal
- 6 Bearing Details
- 7 Deck Patching Plan
- 8 Abutment Patching Plan
- 9 Bar Splicer Assembly and Mechanical Splicer Details
- 10 Temporary Concrete Barrier



LOCATION SKETCH

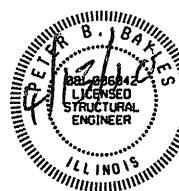
DESIGN STRESSES

FIELD UNITS

$f'_c = 3,500$ psi
 $f_y = 60,000$ psi (reinforcement)
 $f_y = 36,000$ psi (structural steel)

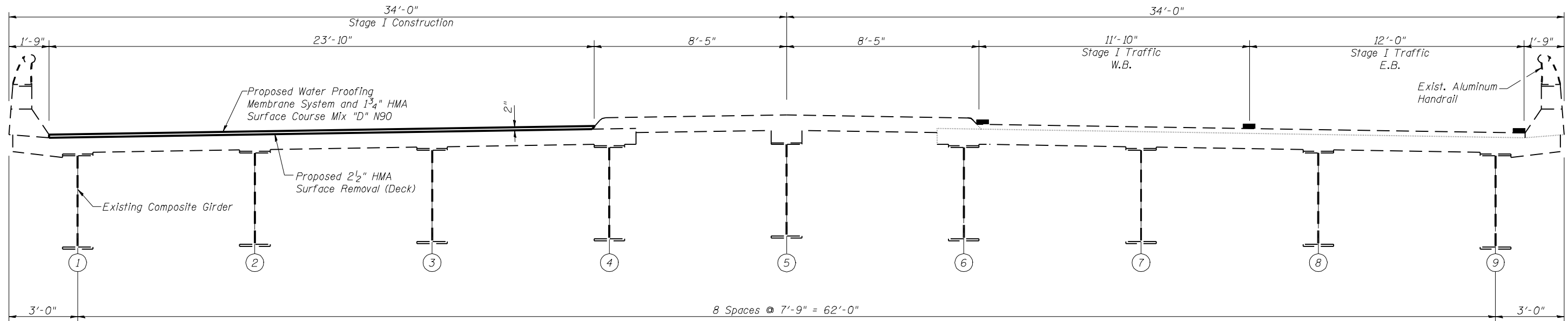
GENERAL PLAN
F.A.U. 8136 OVER I-55
F.A.S. RTE. 2728 OVER F.A.I. 55
SECTION 59RS-2, BR
MACOUPIN COUNTY
STATION 50+00.00
STRUCTURE NO. 059-0045

SHEET NO. 1	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
10 SHEETS	55	59RS-2, BR	Macoupin	100	91
			CONTRACT NO. 72A60		
		FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		

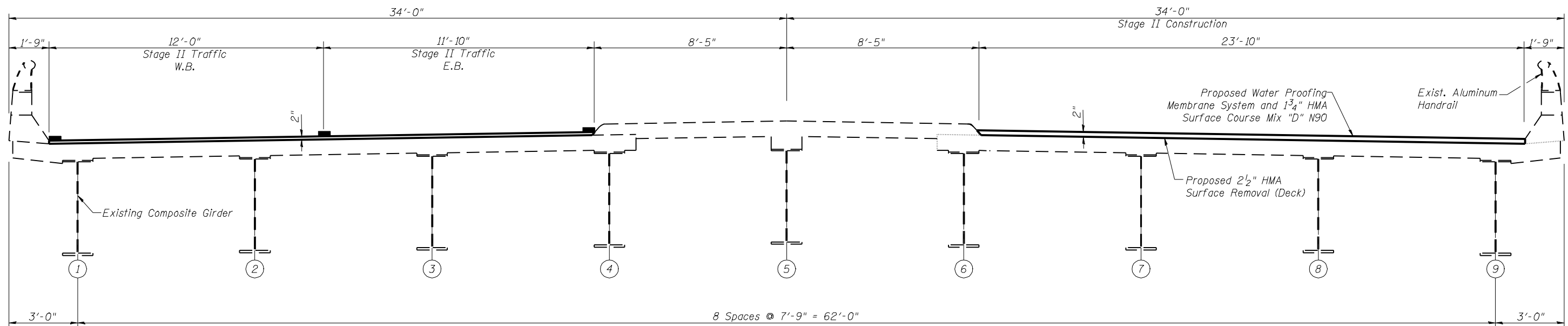


Peter B. Boyles, P.E., S.E.
Structural Engineer License No. 081-006042
Expiration Date: 11/30/2010

STATE OF ILLINOIS
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STAGE I TYPICAL BRIDGE SECTION
(Looking East)



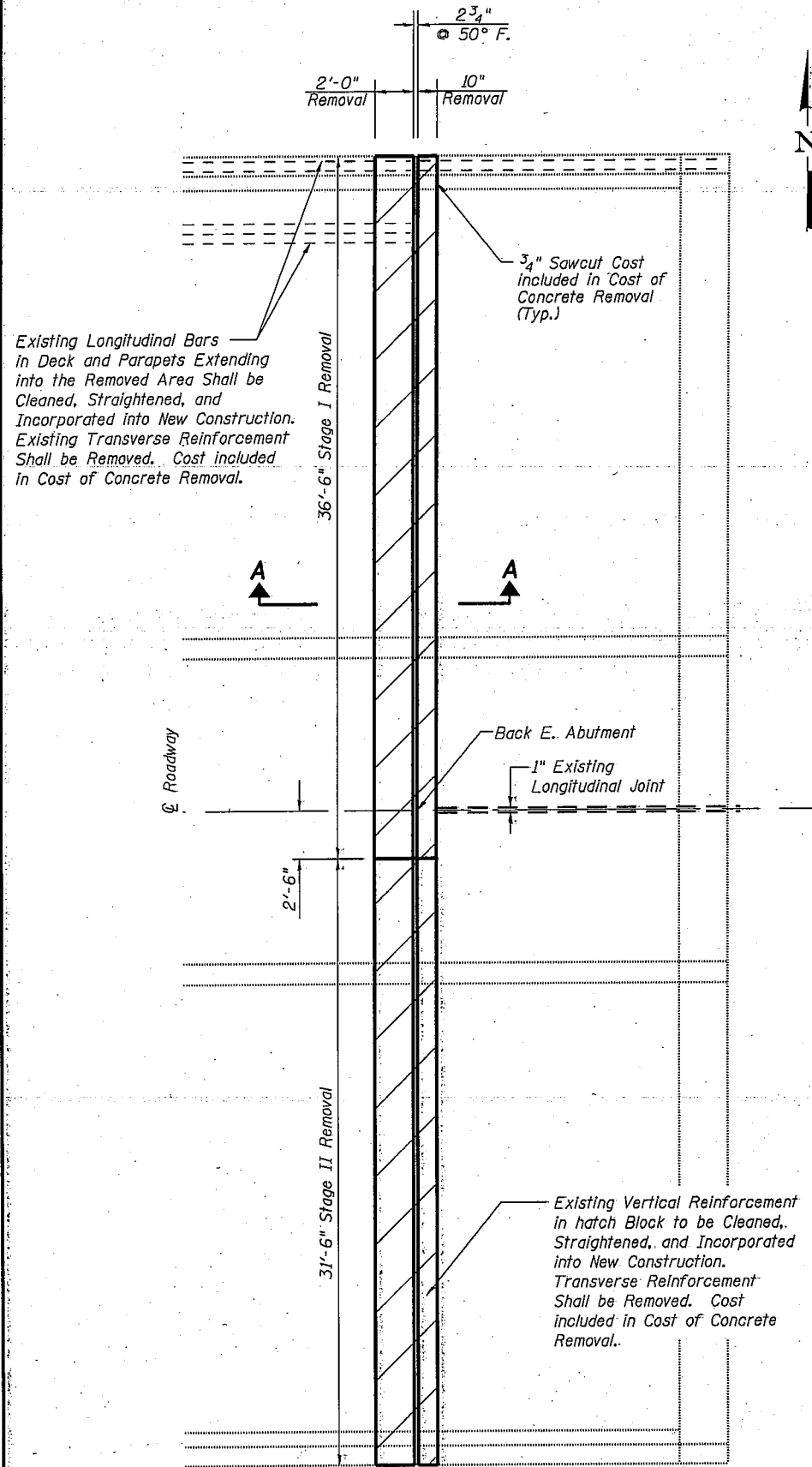
STAGE II TYPICAL BRIDGE SECTION
(Looking East)

**STAGING TYPICAL
STRUCTURE NO. 059-0045**

Note:
See Sheet 7 of 10 for deck slab
repair details (main span shown).
Staging Similar For approach spans.

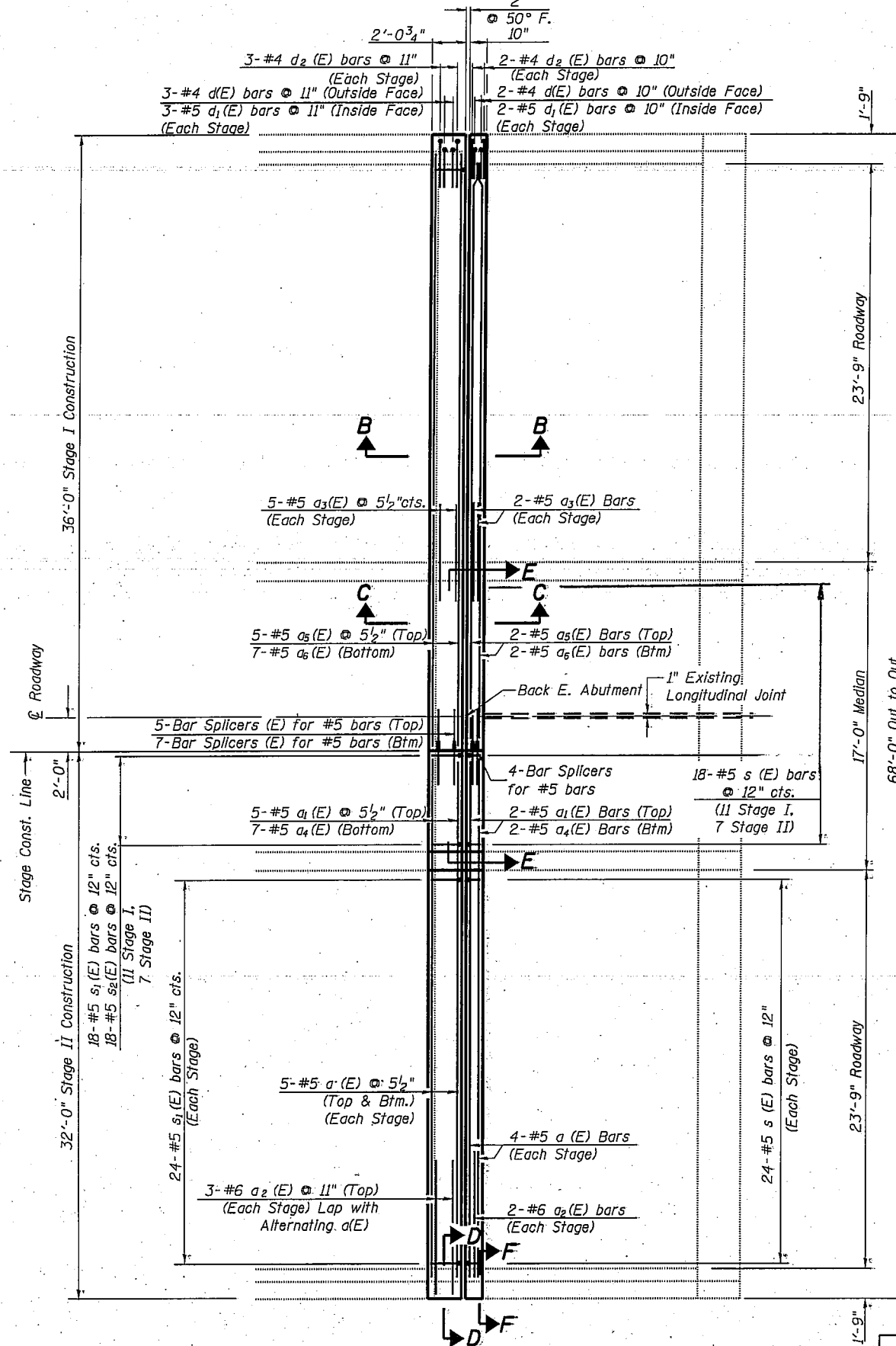
SHEET NO. 2 10 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	59RS-2, BR	Macoupin	100	92
			CONTRACT NO. 72A60		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



CONCRETE REMOVAL PLAN:

(E. Abutment Shown)
(W. Abutment Sim.)



CONCRETE REPLACEMENT PLAN:

(E. Abutment Shown)
(W. Abutment Sim.)

NOTES:

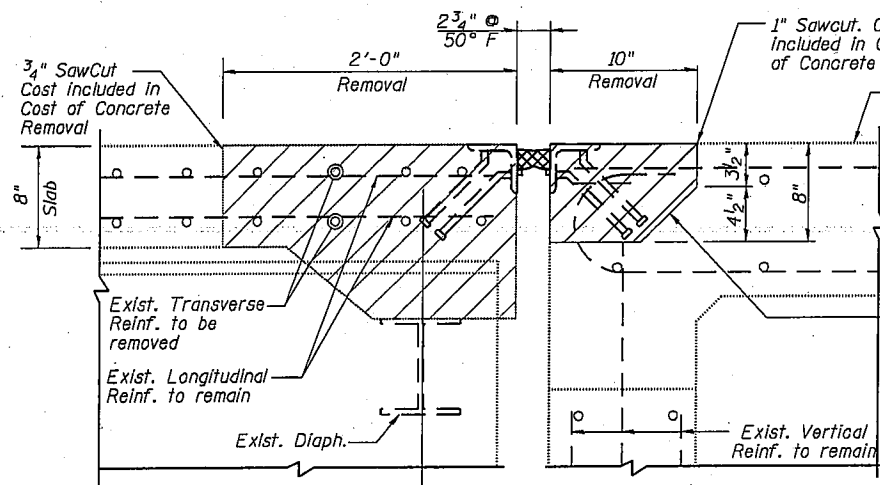
1. Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".

**DECK REPAIRS:
STRUCTURE NO. 059-0045**

SHEET NO. 3:	F.A.I. RTE.:	SECTION:	COUNTY:	TOTAL SHEETS:	SHEET NO.:
	55:	59RS-2, BR:	Macoupin:	100:	93:
10. SHEETS:	CONTRACT NO. 72A60:				
FED. ROAD DIST. NO.:		ILLINOIS FED. AID. PROJECT:			

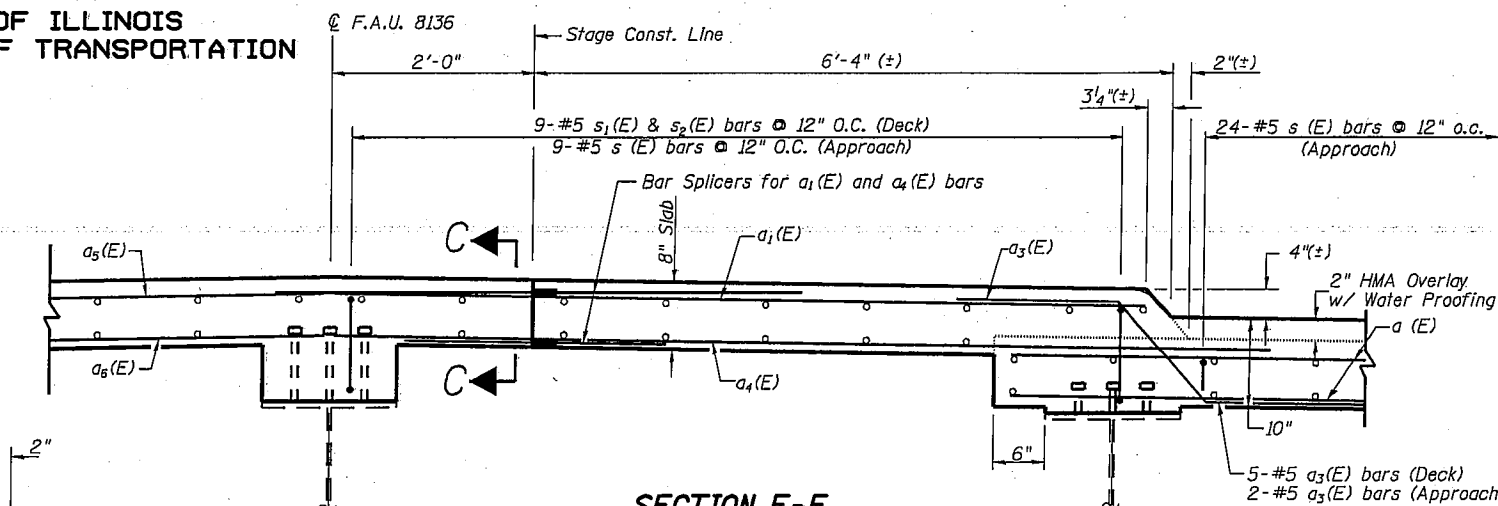
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

© F.A.U. 8136

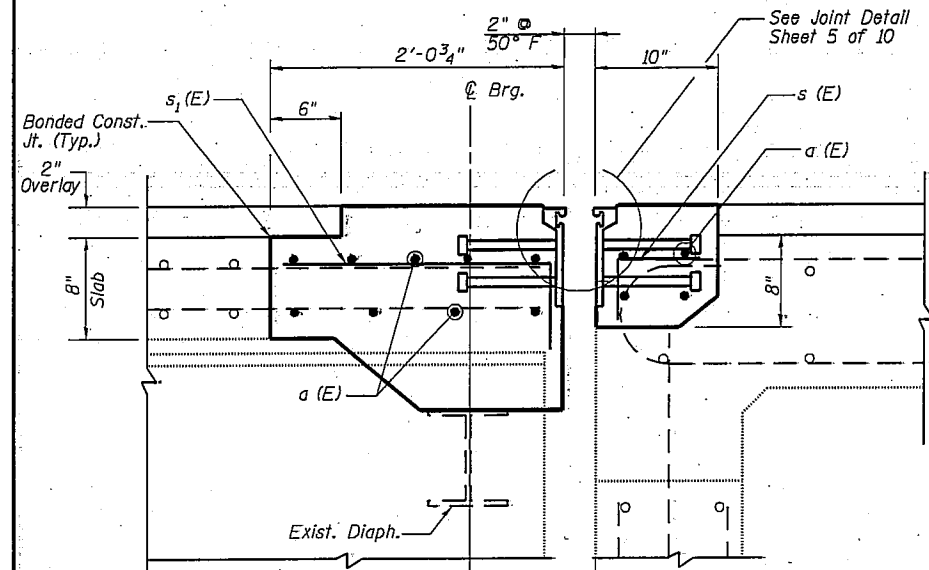


SECTION A-A
(Showing Removal at deck)
(Removal at Median Similar)

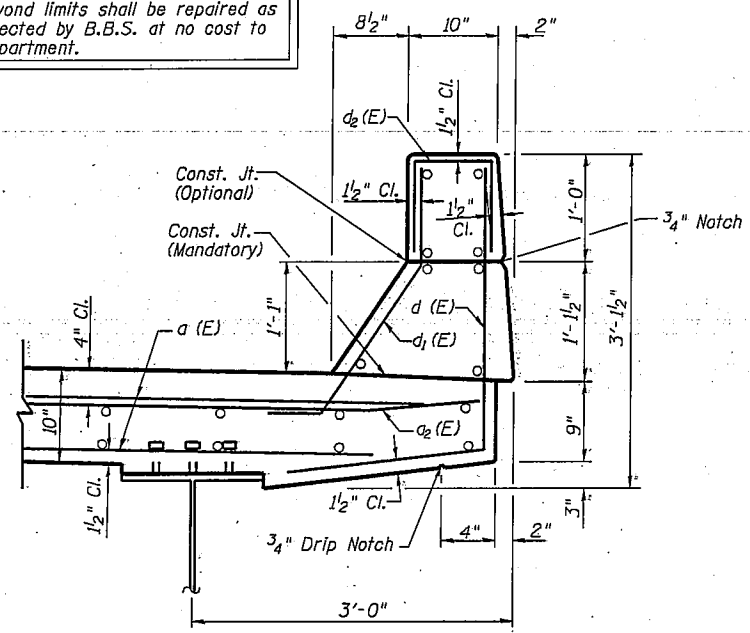
Remove concrete to line indicated as required to remove expansion joint anchorage ONLY. Contact Resident Engineer/B.B.S. if unsound concrete is discovered beyond removal lines indicated. Removal of sound concrete beyond limits shall be repaired as directed by B.B.S. at no cost to Department.



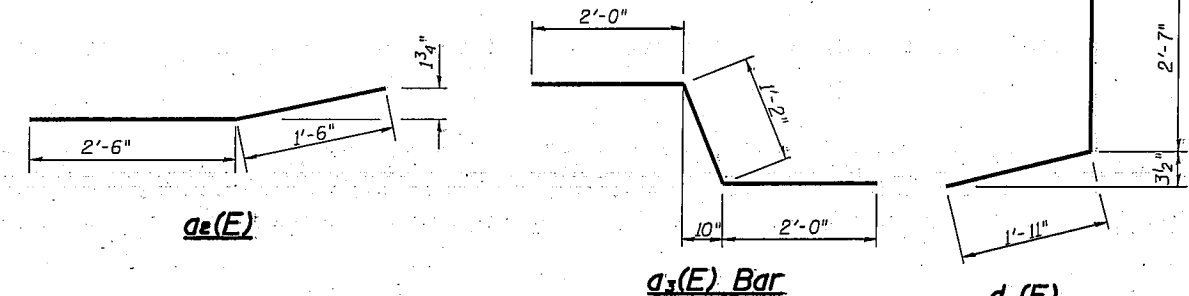
SECTION E-E
(Showing Proposed Median at Deck,
Median at approach Similar)



SECTION B-B
(Showing Proposed Section at Roadway)

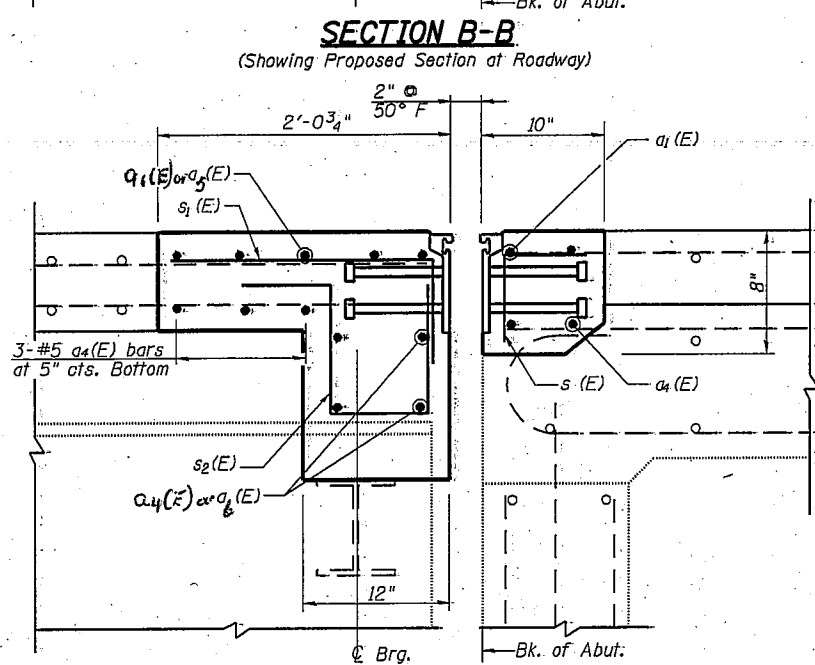


SECTION D-D
(Showing Proposed Parapet)

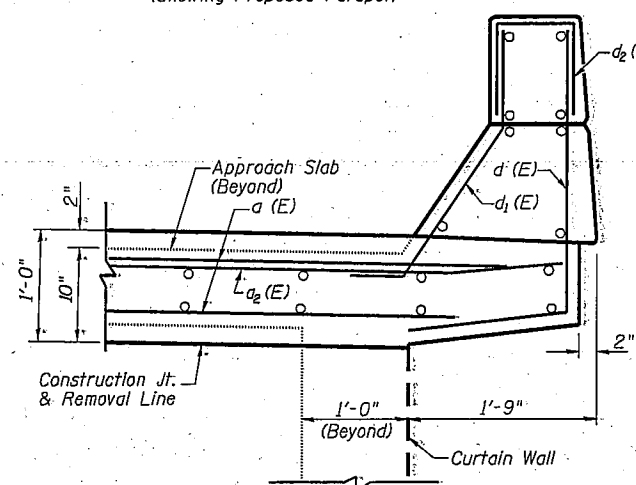


BILL OF MATERIAL
(East & West Abutments)

Bar	No.	Size	Length	Shape
a (E)	56	#5	26'-4"	—
a1(E)	14	#5	6'-3"	—
a2(E)	20	#6	4'-0"	—
a3(E)	28	#5	5'-2"	—
a4(E)	18	#5	8'-0"	—
a5(E)	14	#5	10'-1"	—
a6(E)	18	#5	12'-0"	—
d (E)	20	#4	4'-6"	—
d1(E)	20	#5	3'-3"	—
d2(E)	20	#4	2'-1"	—
s (E)	132	#5	1'-2"	—
s1(E)	132	#5	2'-9"	—
s2(E)	36	#5	3'-3"	—
Concrete Removal			Cu. Yd.	13.5
Concrete Superstructure			Cu. Yd.	15.1
Reinforcement Bars, Epoxy Coated			Pound	3240
Bar Splicers			Each	32
Protective Coat			Sq. Yd.	45



SECTION C-C
(Showing Proposed Section at Median)



SECTION F-F

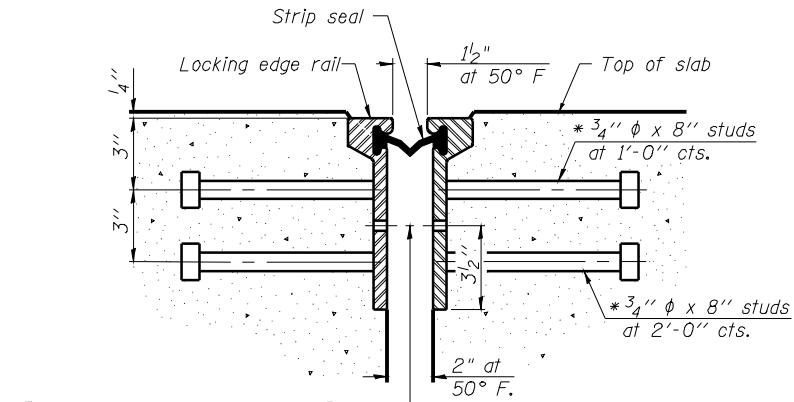
NOTES:

- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included in the cost of Concrete Removal.
- Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when ambient temperature is other than 50° F.
- Removal of all existing expansion joints shall be included in the cost of Concrete Removal.

SHEET NO. 4	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	59RS-2, BR	Macoupin	100	94
CONTRACT NO. 72A60					
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

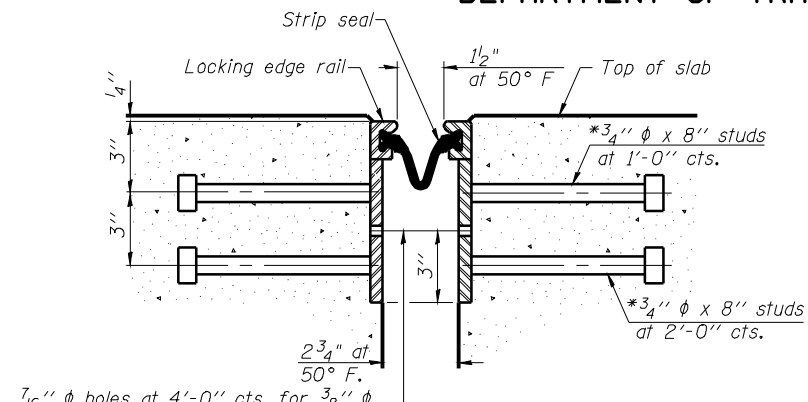
* Granular or solid Flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



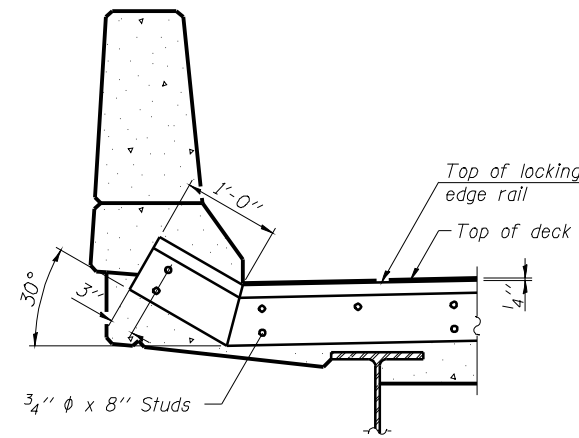
7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU ROLLED RAIL JOINT

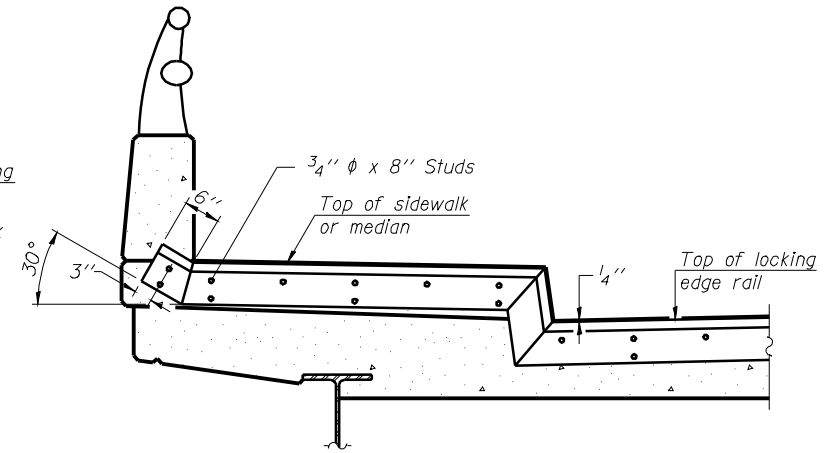


7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

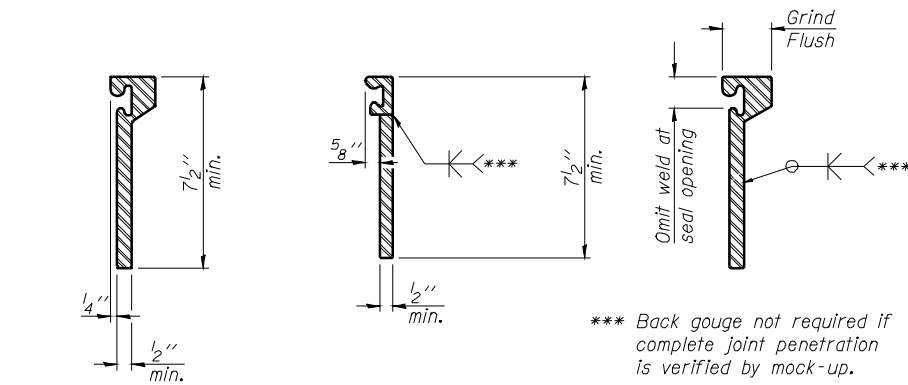
SECTION THRU WELDED RAIL JOINT



AT PARAPET
See Section A-A for end treatment of skews > 30°.



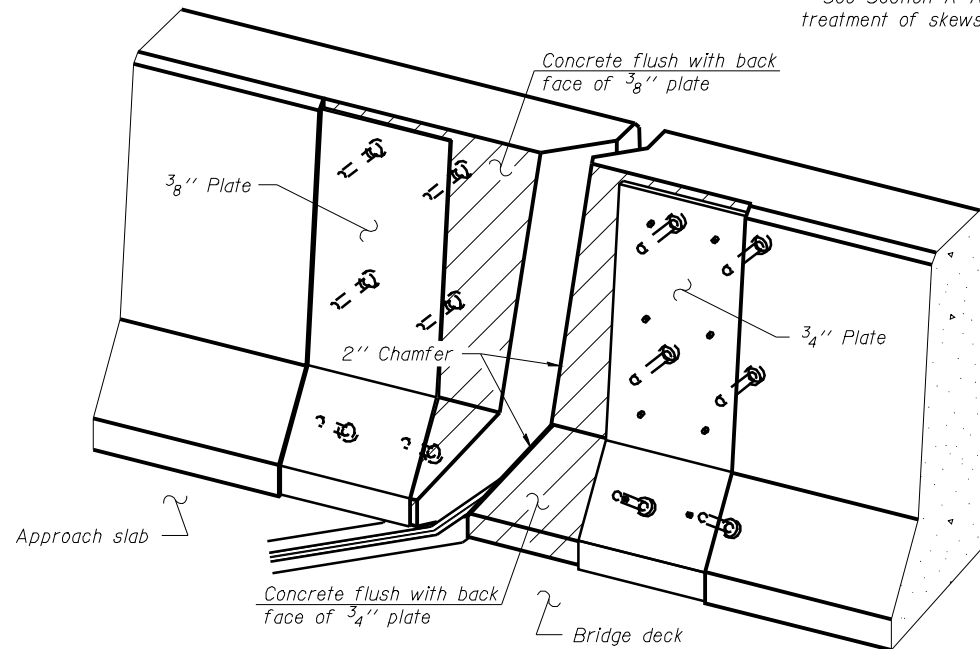
AT SIDEWALK OR MEDIAN
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



ROLLLED EXTRUDED RAIL
WELDED RAIL

LOCKING EDGE RAIL SPLICE
The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.

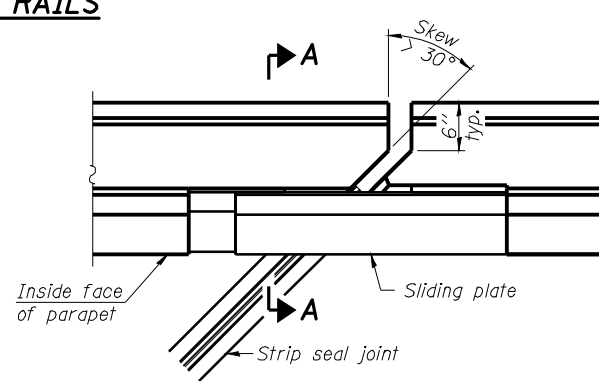
*** Back gouge not required if complete joint penetration is verified by mock-up.



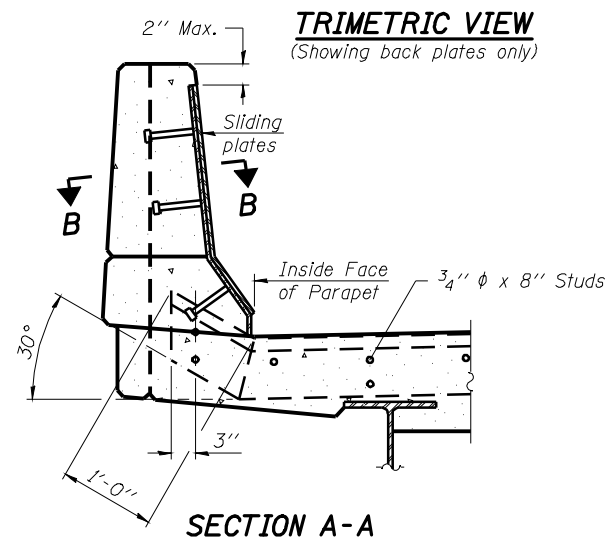
TYPICAL END TREATMENTS

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
The manufacturer's recommended installation methods shall be followed.
The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

LOCKING EDGE RAILS

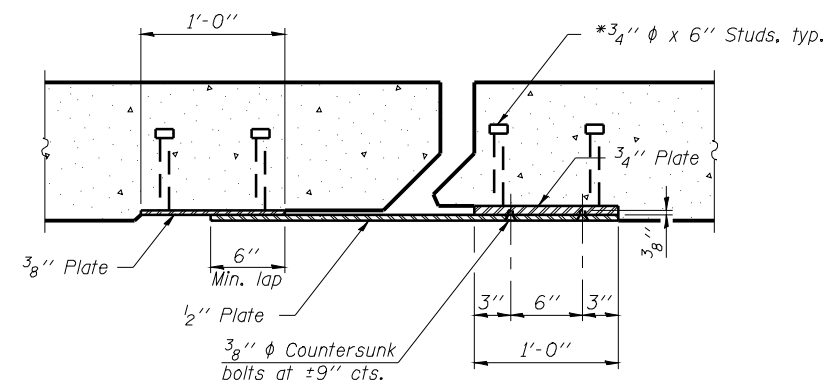


PLAN



SECTION A-A

POINT BLOCK DETAILS
(for skews > 30°)



SECTION B-B

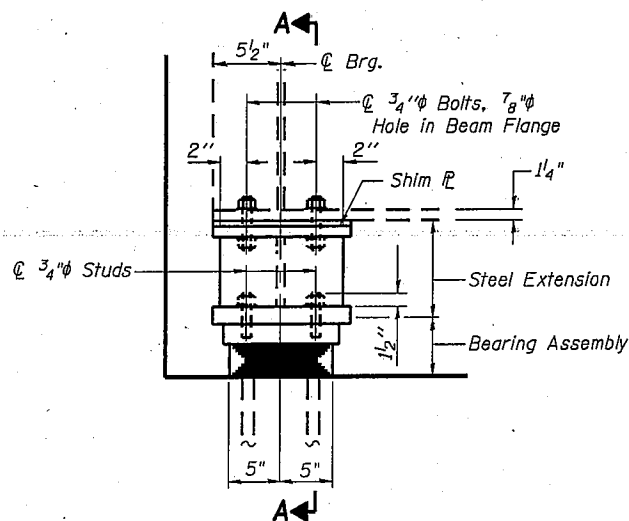
BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	134.0

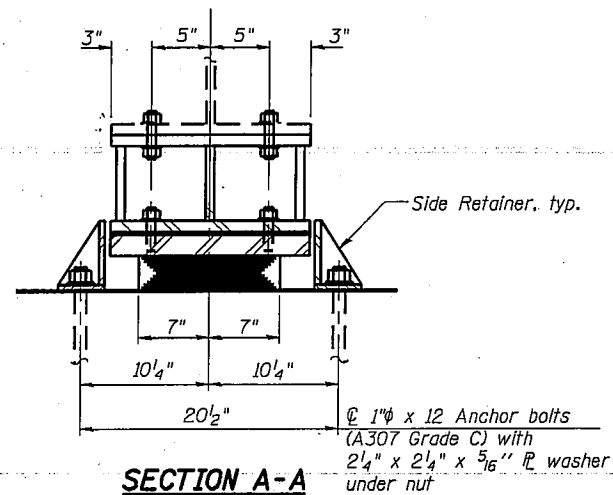
PREFORMED JOINT STRIP SEAL STRUCTURE NO. 059-0045

SHEET NO. 5	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
10 SHEETS	55	59RS-2, BR	Macoupin	100	95
			CONTRACT NO. 72A60		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

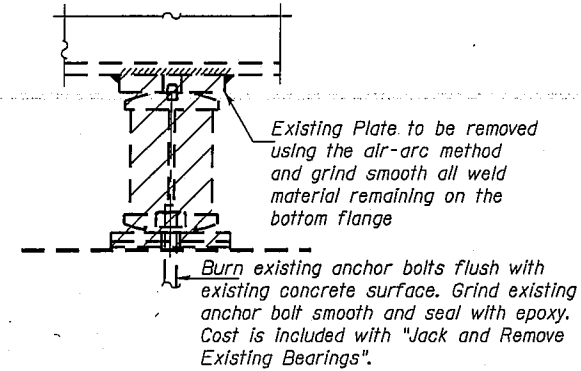
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



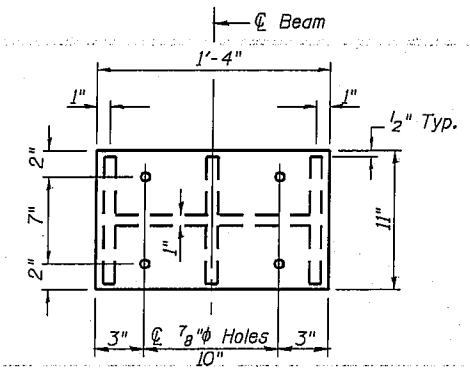
ELEVATION AT ABUT.



SECTION A-A

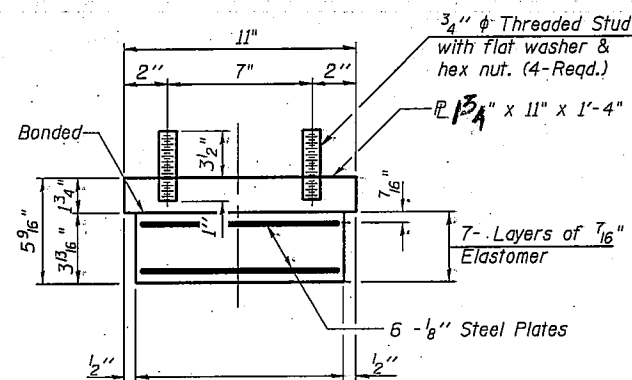


EXISTING BEARING REMOVAL DETAIL



PLAN STEEL EXTENSION

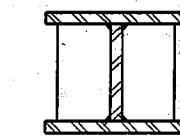
TYPE I ELASTOMERIC EXP. BRG.



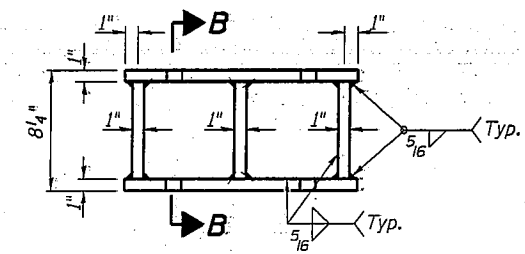
BEARING ASSEMBLY

Note:
Shim plates shall not be placed under Bearing Assembly.

Notes:
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
Side retainers and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.
Steel Extensions, Shims and Bolts shall be included in the cost of Furnishing and Erecting Structural Steel.
Prior to ordering any material for shims or extensions, the contractor shall verify in the field all bearing height and shim thickness dimensions.
Painting of steel shall be according to Article 506.05 of the Standard Specifications.
Provide two 1/8" shims for each bearing location. Adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown in bearing details.
Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts. Bolts 3/4" in 1 3/16" phi holes, unless otherwise noted.



SECTION B-B



ELEVATION STEEL EXTENSION

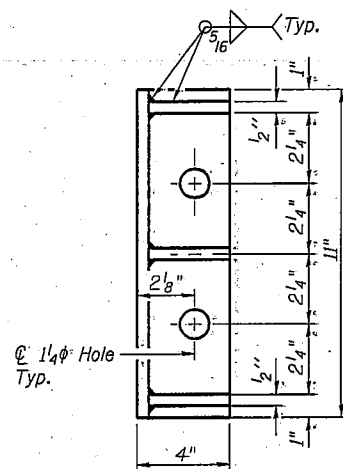
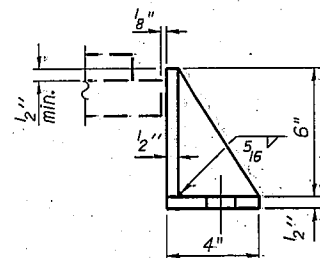
BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly Type I	Each	18
Anchor Bolts, 1"	Each	72
Jack & Remove Exist. Bearings	Each	18
Furnishing & Erecting Structural Steel	Pound	3470

BEAM REACTIONS

R _D (k)	47.1
R _L (k)	45.4
Imp. (k)	9.9
R (Total)(k)	102.4

Min. jack capacity = 60 Tons



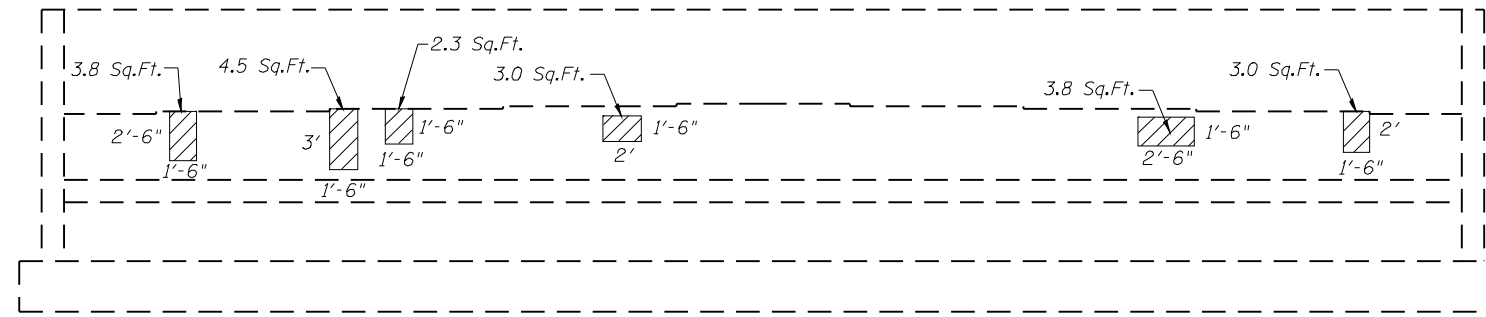
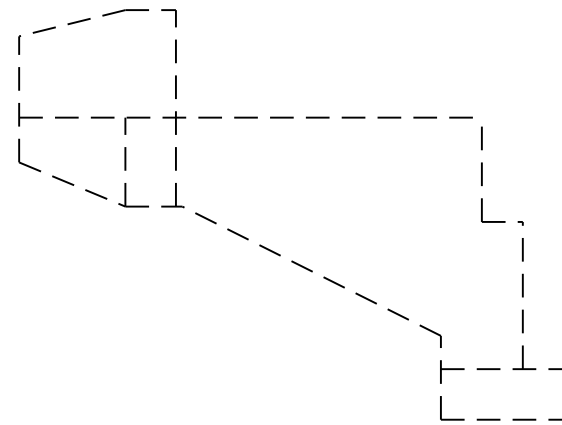
SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

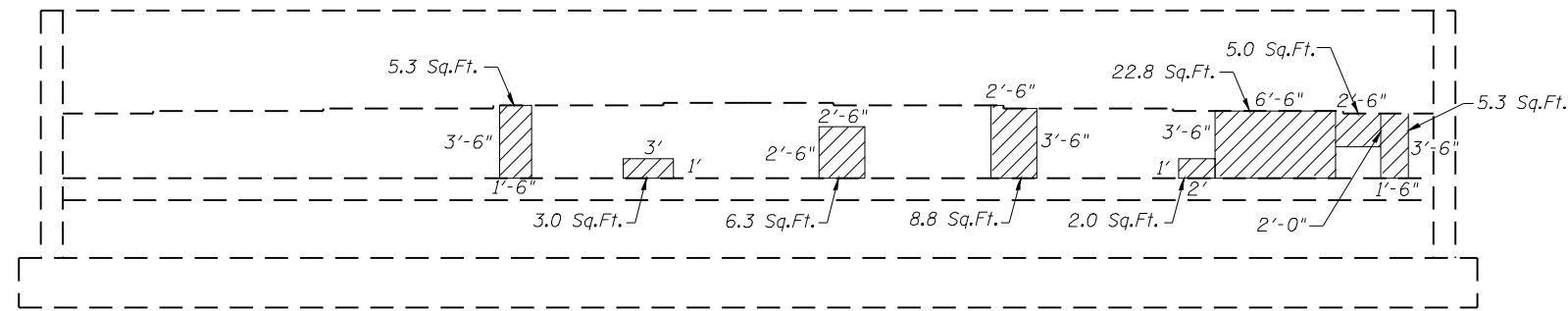
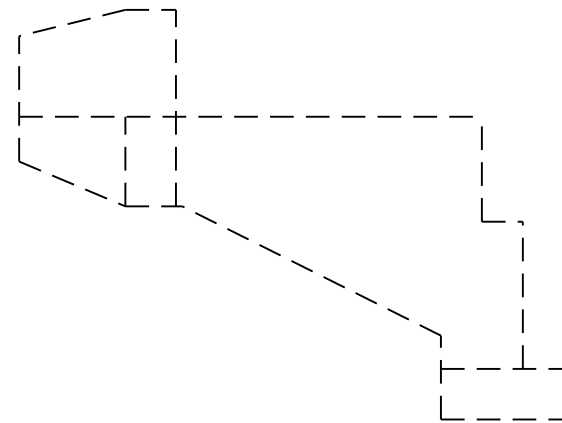
BEARING DETAILS
STRUCTURE NO. 059-0045

SHEET NO. 6	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
10 SHEETS	55	59RS-2, BR	Macoupin	100	96
FED. ROAD. DIST. NO.			ILLINOIS FED. AID. PROJECT		
			CONTRACT NO. 72A60		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



EAST ABUTMENT



WEST ABUTMENT

Notes:

Quantities and repair areas shown are estimated, actual quantities to be determined by the Resident Engineer.

Concrete Sealer to be applied to exposed vertical faces and beam seats of the abutments.

The concrete sealer shall be a pigmented epoxy sealer from the department's approved product list for concrete sealers. It shall be applied in two coats. The color shall be per Federal Standard 595B Grey #36622."

 Structural Repair of Concrete \leq 5"

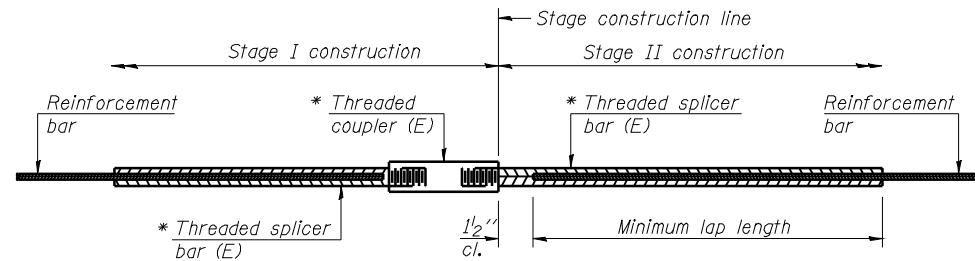
BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete Depth equal to or less than 5 inches	Sq. Ft.	79
Concrete Sealer	Sq. Ft.	1908

**ABUTMENT PATCHING PLAN
STRUCTURE NO. 059-0045**

SHEET NO. 8 10 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	59RS-2, BR	Macoupin	100	98
			CONTRACT NO. 72A60		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



STANDARD BAR SPLICER ASSEMBLY

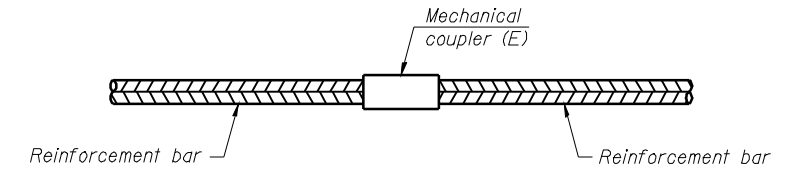
Minimum Lap Lengths				
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4
3, 4	1'-5"	1'-11"	2'-1"	2'-4"
5	1'-9"	2'-5"	2'-7"	2'-11"
6	2'-1"	2'-11"	3'-1"	3'-6"
7	2'-9"	3'-10"	4'-2"	4'-8"
8	3'-8"	5'-1"	5'-5"	6'-2"
9	4'-7"	6'-5"	6'-10"	7'-9"

Table 1: Black bar, 0.8 Class C
Table 2: Black bar, Top bar lap, 0.8 Class C
Table 3: Epoxy bar, 0.8 Class C
Table 4: Epoxy bar, Top bar lap, 0.8 Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

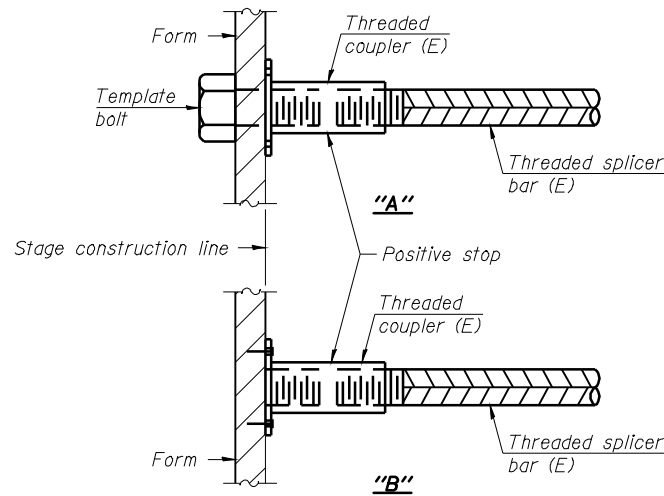
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
W. Approach	#5	4	4
W. Deck	#5	12	3
E. Deck	#5	12	3
E. Approach	#5	4	4



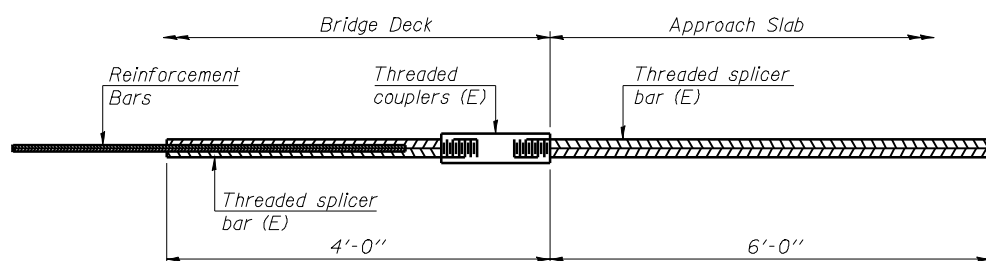
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



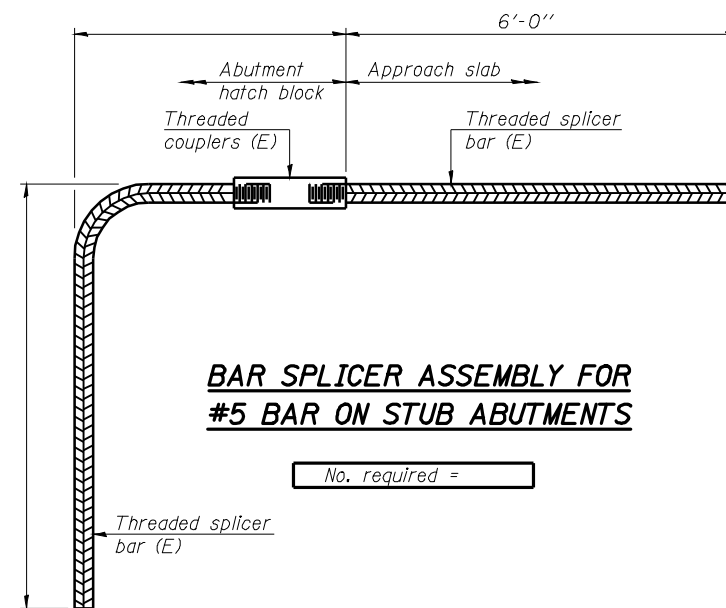
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

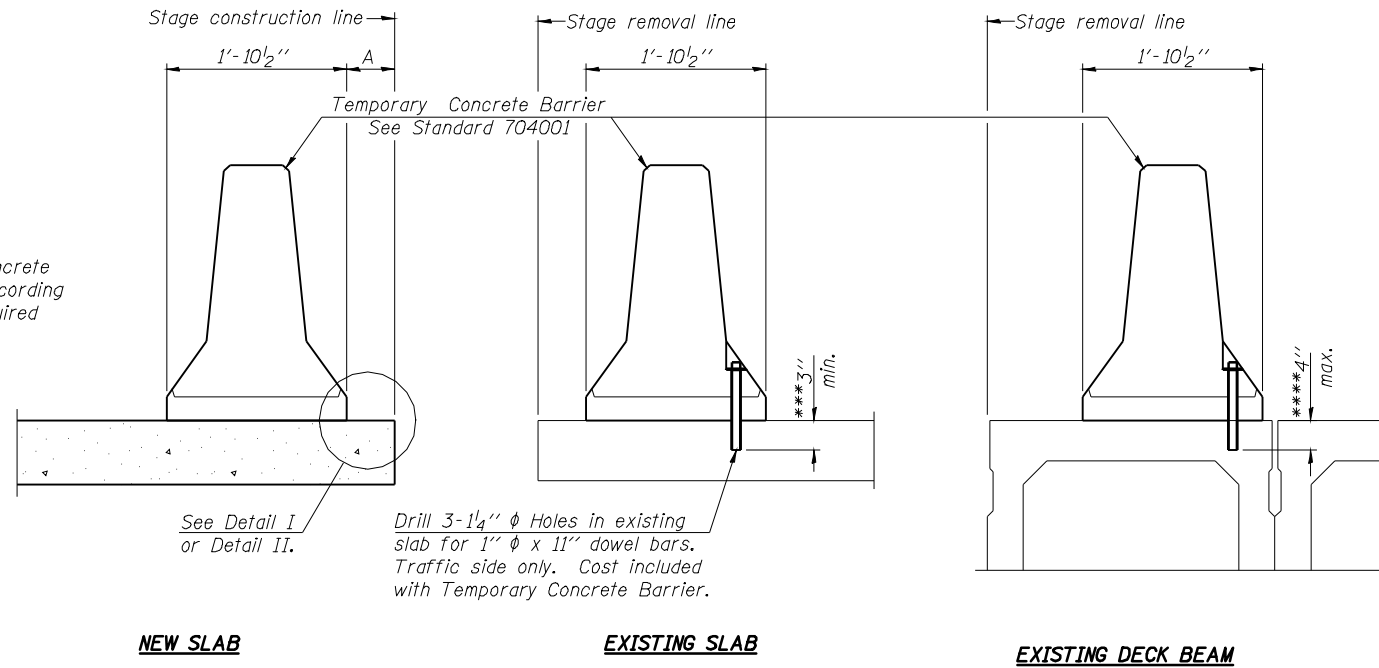
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See special provision for Mechanical Splicers.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 059-0045**

SHEET NO. 9 10 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	59RS-2, BR	MACOUPIN	100	99
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT		
CONTRACT NO. 72A60					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



Drill 3-1/4" ϕ Holes in existing slab for 1" ϕ x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

NOTES

Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{P} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

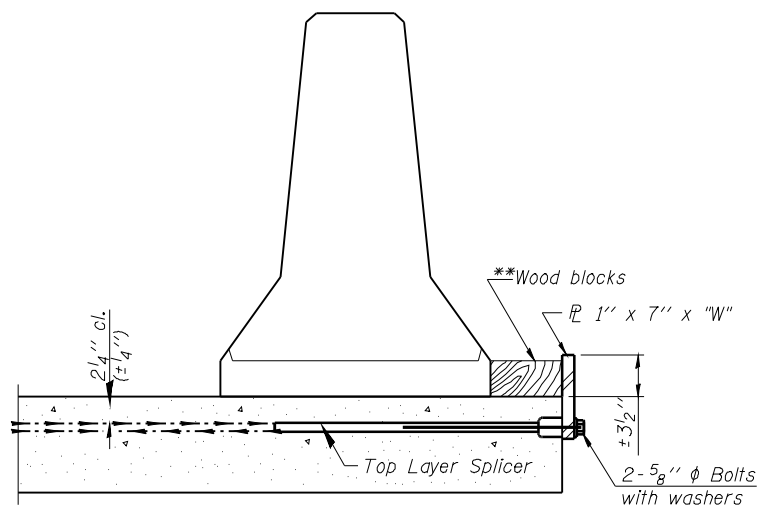
Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x 10" steel \bar{P} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

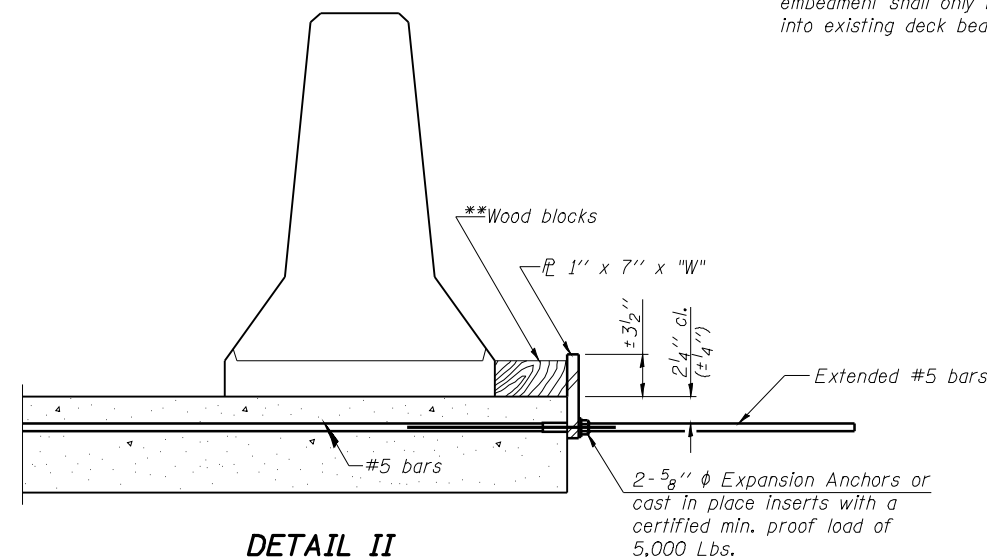
SECTIONS THRU SLAB OR DECK BEAM

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

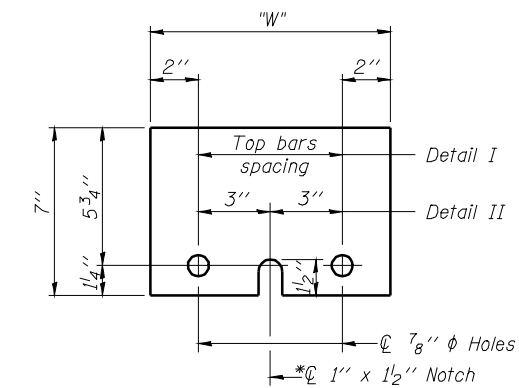
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER \bar{P} 1" x 7" x 10"

* Required only with Detail II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"

**TEMPORARY CONCRETE BARRIER
FOR STAGE CONSTRUCTION
STRUCTURE NO. 059-0045**

SHEET NO. 10 10 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	59RS-2, BR	Macoupin	100	100
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT		
CONTRACT NO. 72A60					