

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	1212 RS - 3	COOK & LAKE	23	1
FED. ROAD DIST. NO.		ILLINOIS CONTRACT NO. 60F37		

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

**PROPOSED**  
**HIGHWAY PLANS**  
**FAP ROUTE 338: ILL 59 (HOUGH ST)**  
**MAIN ST TO BARRINGTON RD**  
**SECTION 1212 RS-3**  
**RESURFACING (MAINTENANCE)**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

**THIS PROJECT IS LOCATED  
 IN THE VILLAGE OF BARRINGTON**

**TRAFFIC DATA**

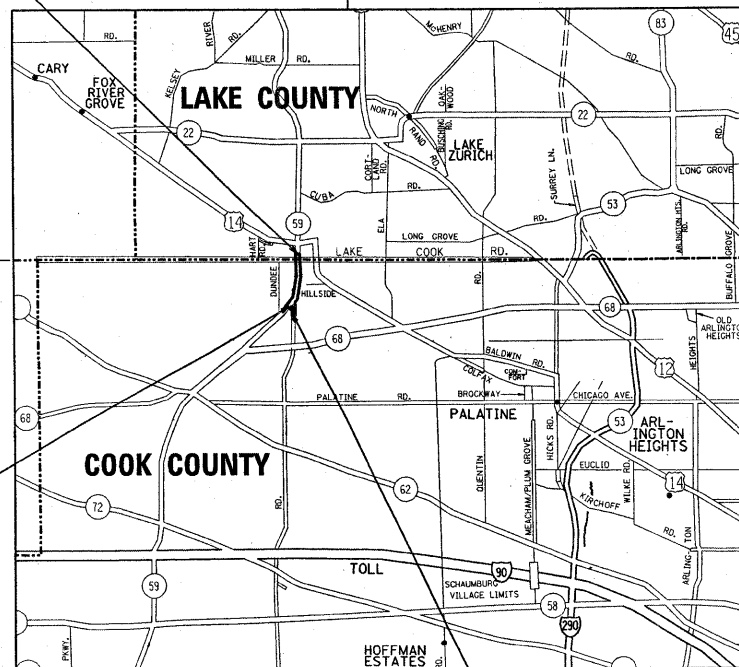
2007 ADT = 19,200  
 POSTED SPEED LIMIT = 25 - 35 MPH

**COOK & LAKE COUNTIES**  
**C-91-068-09**

PROJECT ENDS  
 STA 183+33

R 9 E | R 10 E

T 42 N | T 43 N



CUBA TWP  
 BARRINGTON TWP



PROJECT BEGINS  
 STA 141+66.2 ILL 59

PROJECT BEGINS  
 STA 71+17.7 BARRINGTON RD

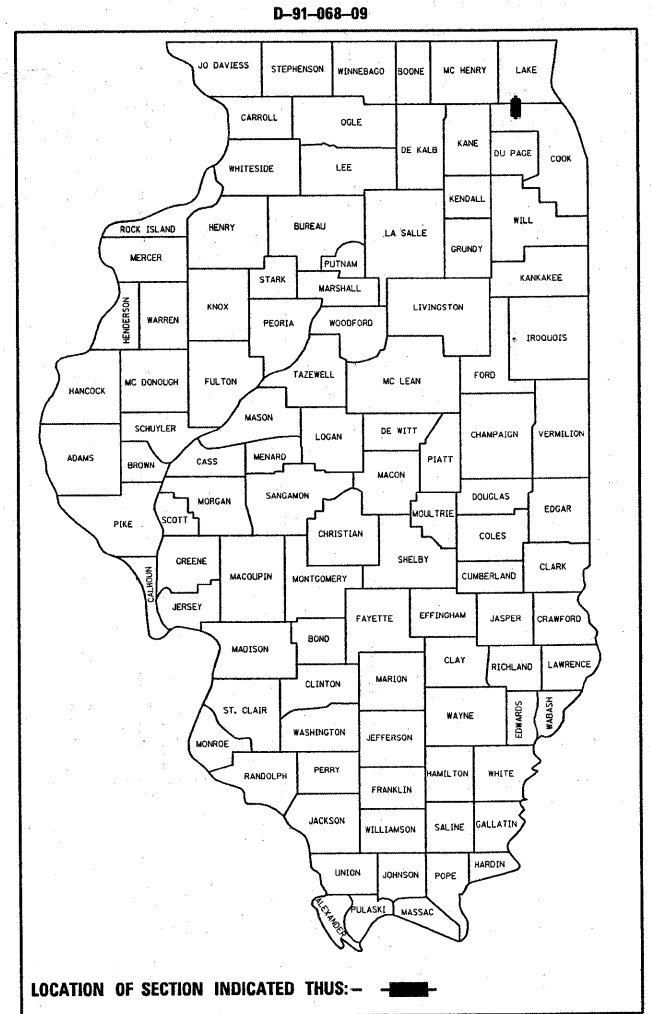
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

PROJECT ENGINEER ROBERT BORO (847) 705-4178  
 PROJECT MANAGER KEN ENG

CONTRACT NO. 60F37

GROSS AND NET LENGTH OF PROJECT = 4488 FEET = 0.85 MILES



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

SUBMITTED MARCH 19, 20 09

*Devin M. O'Keefe*  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 1, 20 09  
*Charles J. Ingersoll*  
 ENGINEER OF DESIGN AND ENVIRONMENT

May 1, 20 09  
*Christine M. Reed*  
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY  
 OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES:

SHEET NO.	DESCRIPTION	STANDARD NO	DESCRIPTION
1	TITLE SHEET	442201-03	CLASS C AND D PATCHES
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES	602001-01	CATCH BASIN TYPE A
3	SUMMARY OF QUANTITIES	602016-01	CATCH BASIN TYPE D
4-7	TYPICAL SECTIONS	604001-03	FRAME AND LIDS, TYPE 1
8-9	ROADWAY & PAVEMENT MARKING PLANS	604051-03	FRAME AND GRATE, TYPE 11
10-12	DETECTOR LOOP REPLACEMENT	606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB & GUTTER
13	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)	701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
14	PAVEMENT PATCHING FOR HOT-MIX ASPHALT SURFACED PAVEMENT(BD-22)	701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEED > 45 MPH
15	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
16	BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS (BD-32)	701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
17	TRAFFIC CONTROL AND PROTECTION FOR SIDES ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)	701901-01	TRAFFIC CONTROL DEVICES
18	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)	780001-02	TYPICAL PAVEMENT MARKINGS
19	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
20	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)		
21	PAVEMENT MARKING LETTER AND SYMBOLS FOR TRAFFIC STAGING (TC-16)		
22	ARTERIAL ROAD INFORMATION SIGNING (TC-22)		
23	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)		

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF BARRINGTON.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BEFORE BEGINING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND REVISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING, EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

THE ENGINEER SHALL CONTACT WALLY CZARNY, TRAFFIC FIELD ENGINEER (OR TECHNICIAN), AT (773) 685-8386 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

SIDEWALK REMOVAL AND SIDEWALK (OF THE TYPE SPECIFIED) LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABBUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

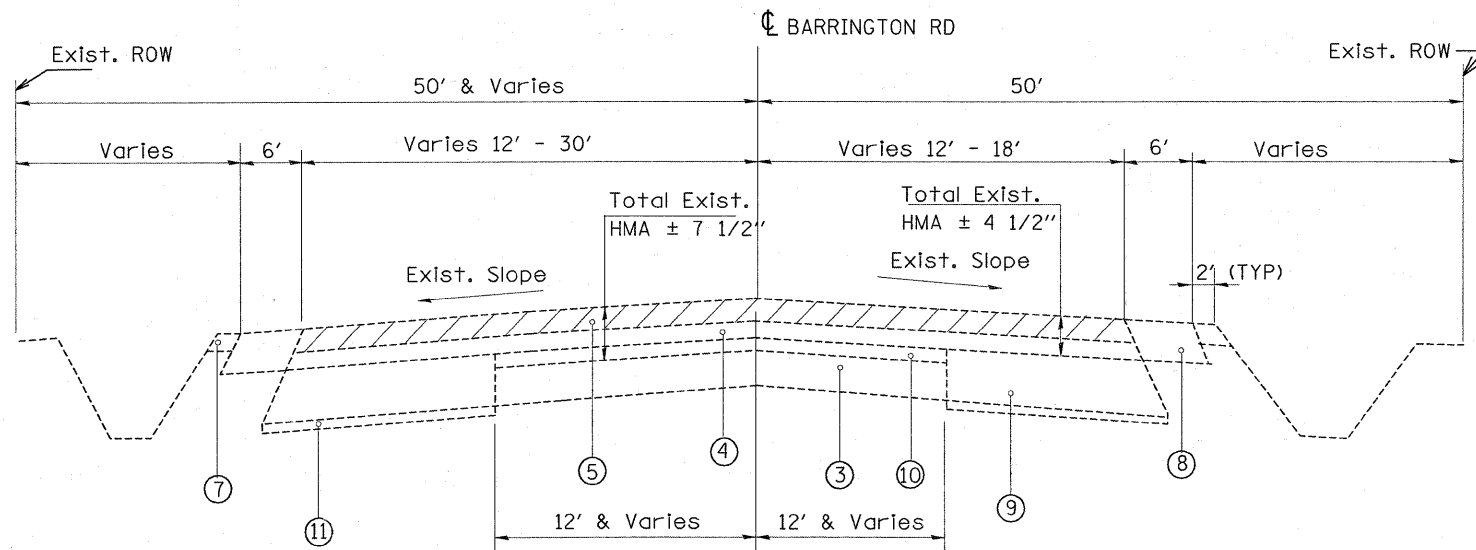
ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

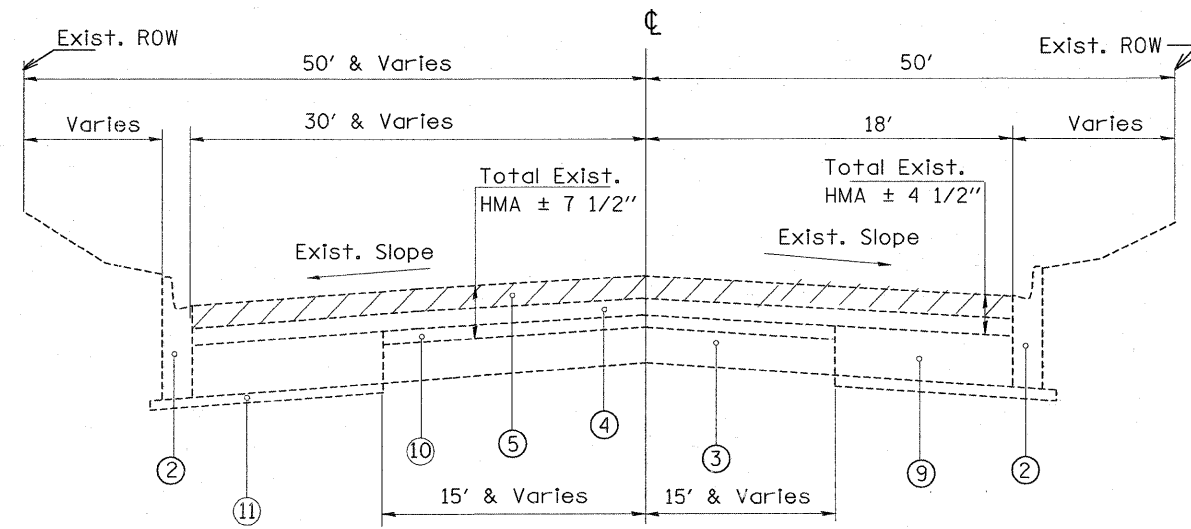
FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL 59 (HOUGH STREET). MAIN STREET TO BARRINGTON ROAD</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\pwidot\GORENGAUTAB\dms89240	sh.r.dwg.dgn	DRAWN -	REVISED -		<b>INDEX OF SHEETS STATE STANDARDS AND GENERAL NOTES</b>		338	1212 RS - 3	COOK & LAKE	23	2
	PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED -		SCALE: 1" = 50'	SHEET NO. OF	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT
	PLOT DATE = 3/18/2009	DATE -	REVISED -		<b>CONTRACT NO. 60F37</b>						

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE I 000-2A						SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE I 000-2A									
CODE NO	ITEM	UNIT	URBAN 100% STATE		I 000						CODE NO	ITEM	UNIT	URBAN 100% STATE		I 000					
			TOTAL QUANTITIES	COOK COUNTY	LAKE COUNTY	TOTAL QUANTITIES								COOK COUNTY	LAKE COUNTY	TOTAL QUANTITIES	COOK COUNTY				
21400100	GRADING AND SHAPING DITCHES	FOOT	30	30						60404800	FRAMES AND GRATES, TYPE 11	EACH	3	3							
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	0.5	0.5						60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	14	14							
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	0.25	0.25						67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	2.5	0.5						
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	0.15	0.15						67100100	MOBILIZATION	L SUM	1	0.9	0.1						
25200110	SODDING, SALT TOLERANT	SQ YD	22	22						70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	0.9	0.1						
25200200	SUPPLEMENTAL WATERING	UNIT	0.2	0.2						70300100	SHORT-TERM PAVEMENT MARKING	FOOT	2967	2703	264						
28100705	STONE DUMPED RIPRAP, CLASS A3	SQ YD	3	3						70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	543.6	363	180.6						
28200200	FILTER FABRIC	SQ YD	3	3						70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	11206	10809	397						
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	8.2	7.6	0.6					70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1915	1186	729						
40600300	AGGREGATE (PRIME COAT)	TON	41	38	3					70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1347	1061	286						
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	6.2	5.7	0.5					70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	291	199	92						
40600895	CONSTRUCTING TEST STRIP	EACH	1	1						* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	543.6	363	180.6						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	225	159	66					* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	11206	10809	397						
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	148.5	132	16.5					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1915	1186	729						
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1706	1585	121					* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1347	1061	286						
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	18	18						* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	291	199	92						
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	20315	18875	1440					* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	267	243	24						
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	18	18						78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	218	200	18						
44000600	SIDEWALK REMOVAL	SQ FT	15	15						* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1792	1155	637						
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	236	236						X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	25.7	25.7						
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	661	588	73					X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	800	743	57						
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	4	4						XX002873	BRICK SIDEWALK REMOVAL	SQ FT	87	77	10						
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	246	220	26					XX004688	BRICK PAVER SIDEWALK	SQ FT	87	77	10						
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	104	93	11					Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	30	30							
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	228	203	25					44201790	CLASS D PATCHES, TYPE II, 12 1/4 INCH	SQ YD	132	132							
55039700	STORM SEWERS TO BE CLEANED	FOOT	5139	4657	482					44201795	CLASS D PATCHES, TYPE III, 12 1/4 INCH	SQ YD	55	55							
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	4	4						44201797	CLASS D PATCHES, TYPE IV, 12 1/4 INCH	SQ YD	123	123							
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	13	13																	
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	24	24																	

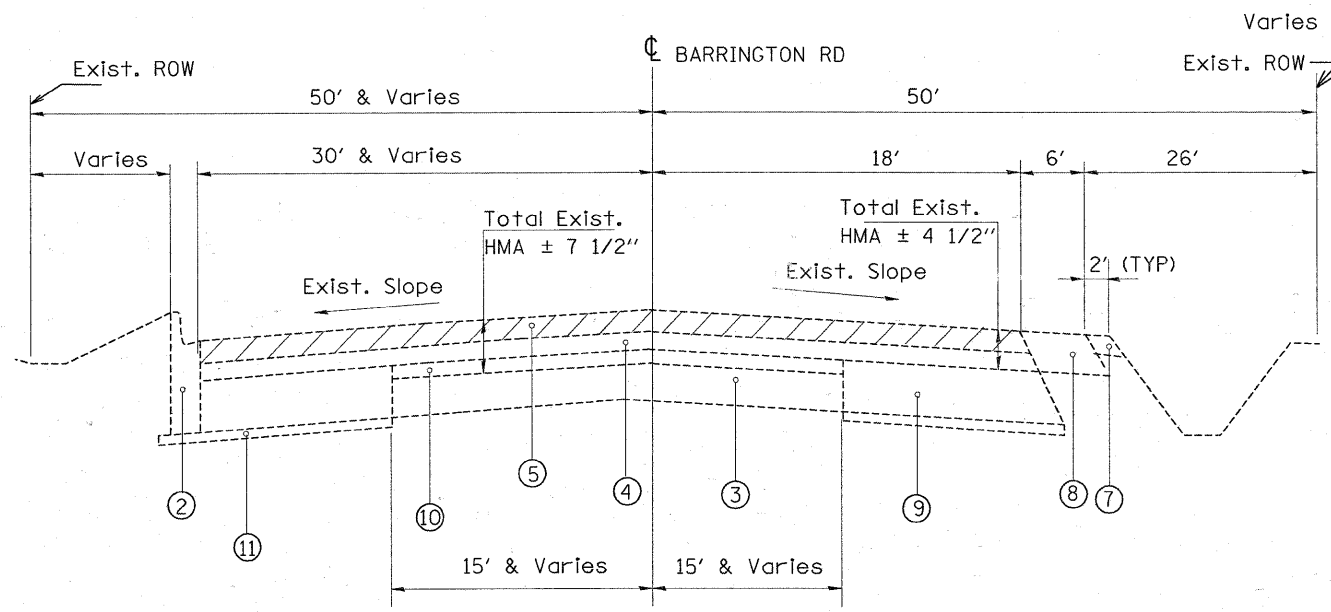
\*SPECIALTY ITEMS



EXISTING TYPICAL SECTION  
STA 71+17.7 TO STA 75+41.38



EXISTING TYPICAL SECTION  
STA 78+58.6 TO STA 80+00 (BARRINGTON RD)  
STA 141+66.2 TO STA 150+81 IL 59 (HOUGH RD)



EXISTING TYPICAL SECTION  
STA 75+41.38 TO STA 78+58.6

**LEGEND**

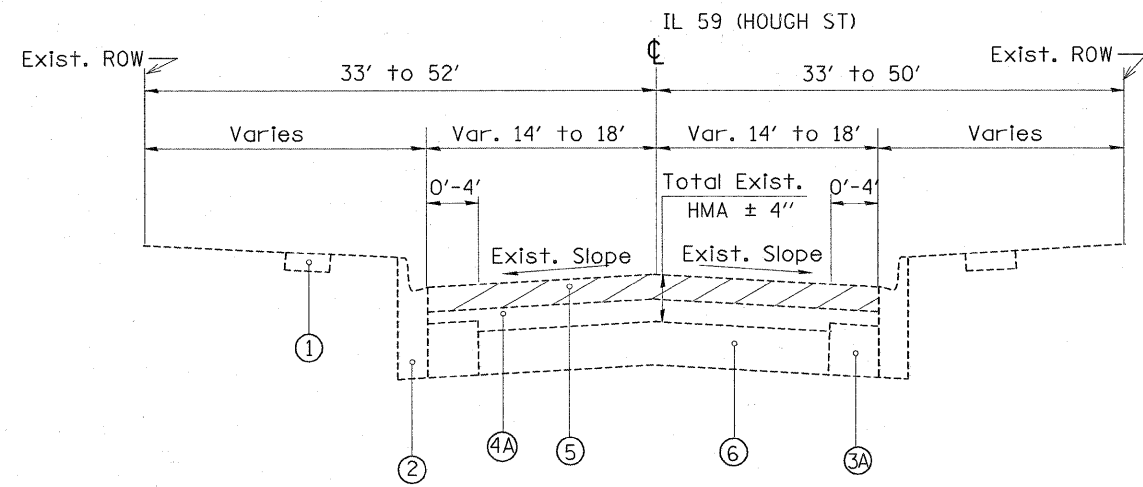
- ① EXISTING PC CONCRETE SIDEWALK
- ② EXISTING COMB. CONCRETE CURB AND GUTTER
- ③ EXISTING PC CONCRETE BASE COURSE ± 7"
- ③A EXISTING PC CONCRETE BASE COURSE ± 8"
- ④ EXISTING HMA, AFTER MILLING SURFACE, ± 2 1/4"
- ④A EXISTING HMA, AFTER MILLING SURFACE 1 3/4"
- ⑤ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑥ EXISTING PAVEMENT ± 8"
- ⑦ EXISTING HMA SHOULDER
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ EXISTING PC CONCRETE BASE COURSE, 10"
- ⑩ EXISTING BINDER, 3"
- ⑪ EXISTING GRANULAR MATERIAL, TYPE B, 4"

FILE NAME =	USER NAME = gorenautab	DESIGNED -	REVISED -
est\pwork\pwidot\GORENGAUTAB\dms09240	sh_rdw.dgn	DRAWN -	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 3/18/2009	DATE -	REVISED -

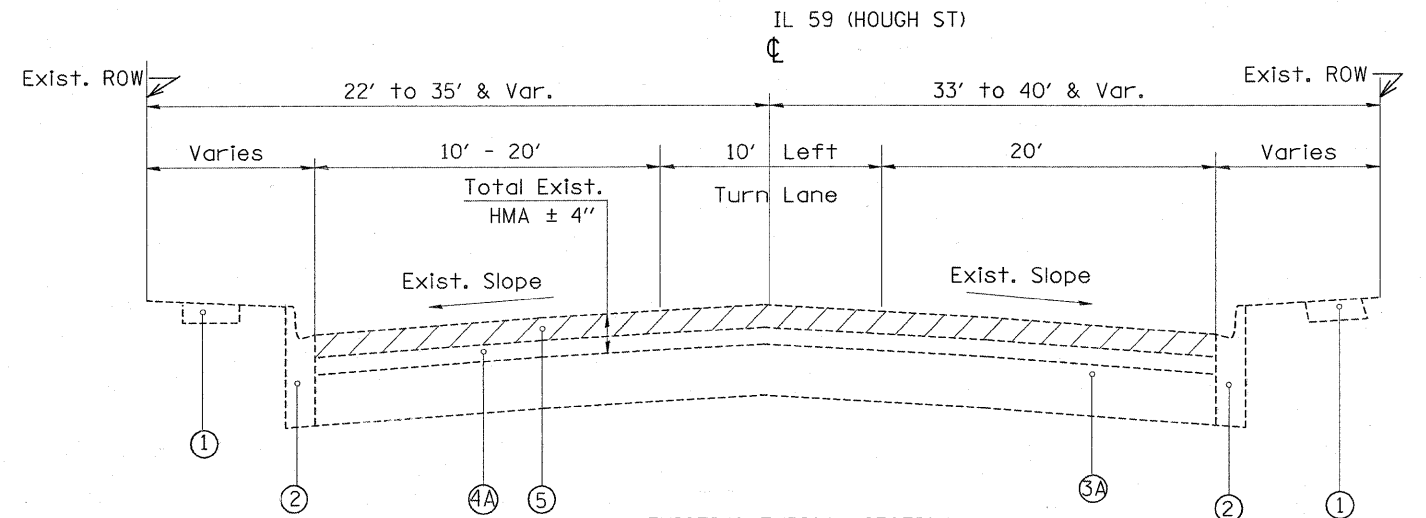
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL 59 (HOUGH STREET). MAIN STREET TO BARRINGTON ROAD  
EXISTING TYPICAL SECTIONS**

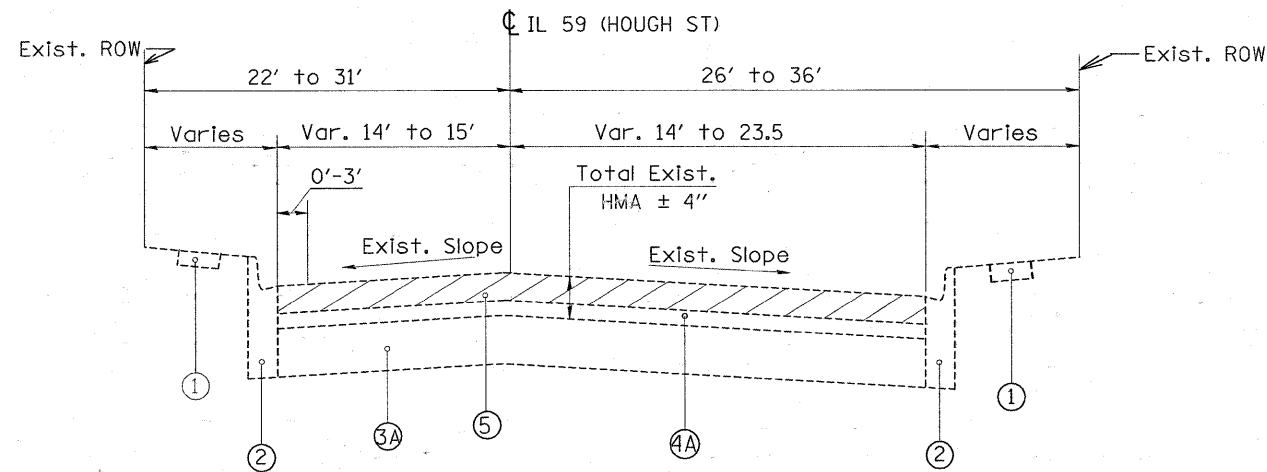
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	1212 RS - 3	COOK & LAKE	23	4
SCALE:			SHEET NO. OF SHEETS STA. 71+17.7 TO STA. 150+81	
[ILLINOIS] FED. AID PROJECT				



EXISTING TYPICAL SECTION  
STA 150+81 TO STA 174+43.1



EXISTING TYPICAL SECTION  
STA 176+05 TO STA 183+33

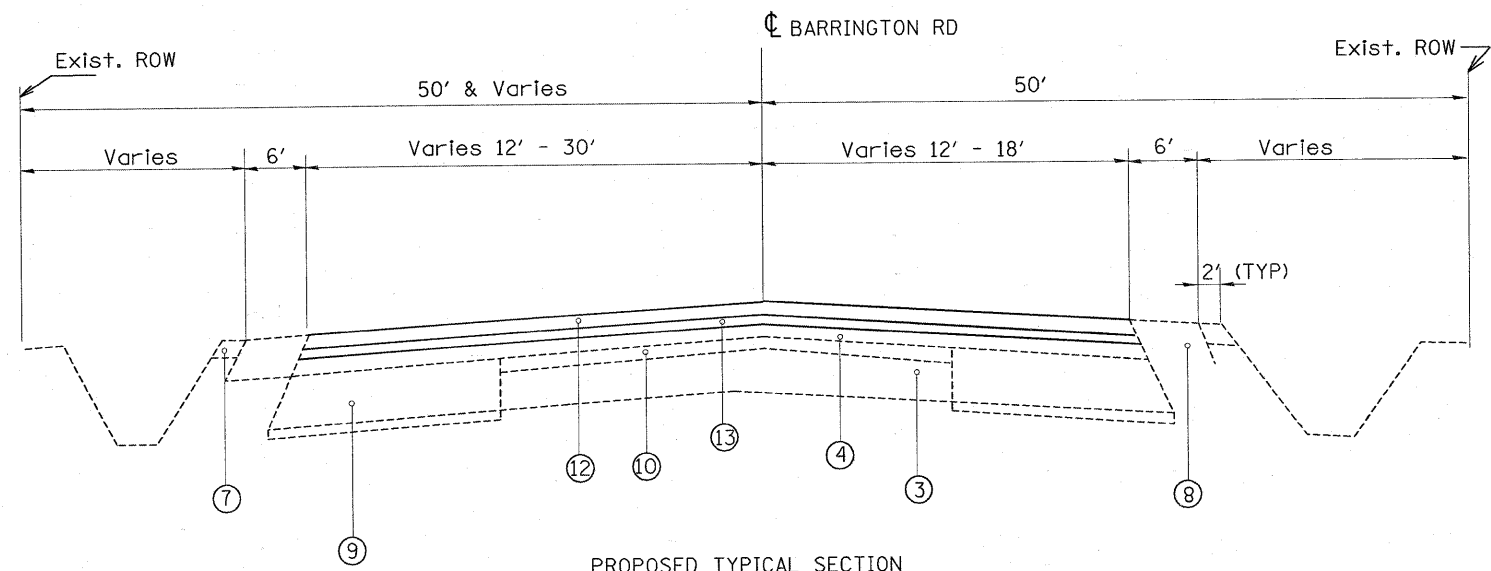


EXISTING TYPICAL SECTION  
STA 174+43.1 TO STA 176+05

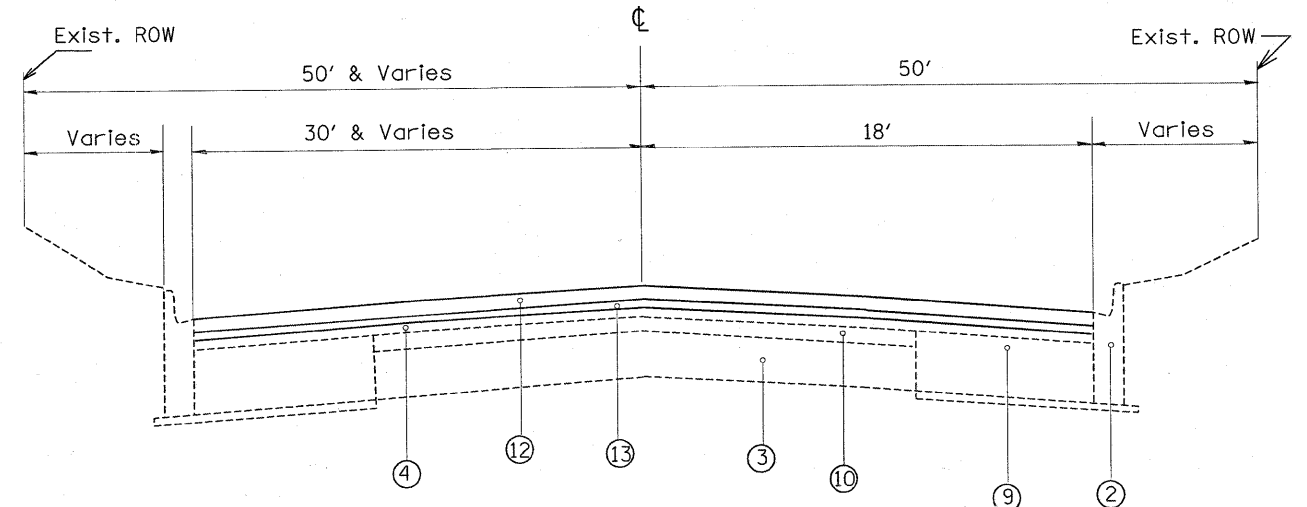
**LEGEND**

- ① EXISTING PC CONCRETE SIDEWALK
- ② EXISTING COMB. CONCRETE CURB AND GUTTER
- ③ EXISTING PC CONCRETE BASE COURSE ± 7"
- ③A EXISTING PC CONCRETE BASE COURSE ± 8"
- ④ EXISTING HMA, AFTER MILLING SURFACE, ± 2 1/4"
- ④A EXISTING HMA, AFTER MILLING SURFACE 1 3/4"
- ⑤ EXISTING PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑥ EXISTING PAVEMENT ± 8"
- ⑦ EXISTING HMA SHOULDER
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ EXISTING P.C. CONCRETE BASE COURSE 10"
- ⑩ EXISTING BINDER, 3"
- ⑪ EXISTING GRANULAR MATERIAL, TYPE B, 4"

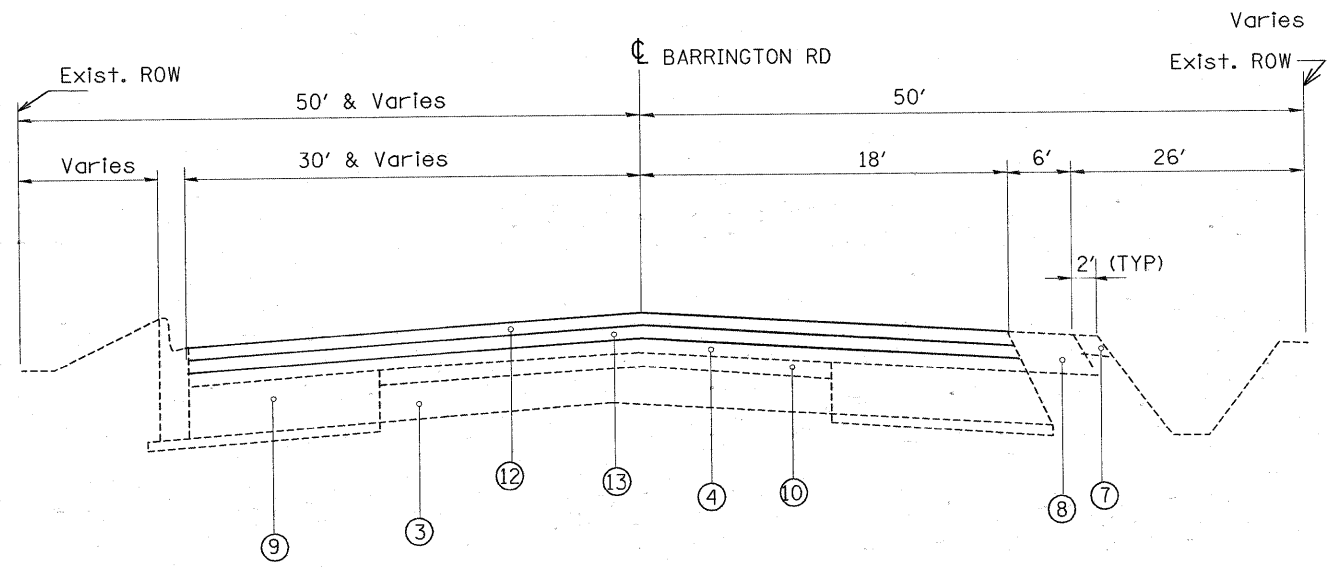
FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL 59 (HOUGH STREET). MAIN STREET TO BARRINGTON ROAD EXISTING TYPICAL SECTIONS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pw\work\PIWIDOT\GORENGAUTAB\dms09240	sh_rdw.dgn	DRAWN -	REVISED -			338	1212 RS - 3	COOK & LAKE	23	5	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	REVISED -			CONTRACT NO. 60F37					
PLOT DATE = 3/18/2009	DATE -	REVISED -	REVISED -			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT					



PROPOSED TYPICAL SECTION  
STA 71+17.7 TO STA 75+41.38



PROPOSED TYPICAL SECTION  
STA 78+58.6 TO STA 80+00 (BARRINGTON RD)  
STA 141+66.2 TO STA 150+81 IL 59 (HOUGH ST)



PROPOSED TYPICAL SECTION  
STA 75+41.38 TO STA 78+58.6

THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

HMA MIXTURE REQUIREMENTS		
MIXTURE TYPE	AC TYPE	AIR VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N 70, (IL 9.5 mm)	PG 64 -22	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (M M), N50, (IL 4.75 mm)	SBS/SBR PG 76-28/-22	4% @ 50 GYR
CLASS D PATCHES, (HMA BINDER IL-19 mm)	PG 64 -22**	4% @ 70 GYR
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES, (HMA BINDER IL-19 mm)	PG 64 -22	4% @ 70 GYR

**NOTE:** THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ.YD./IN.  
\*\*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

**LEGEND:**

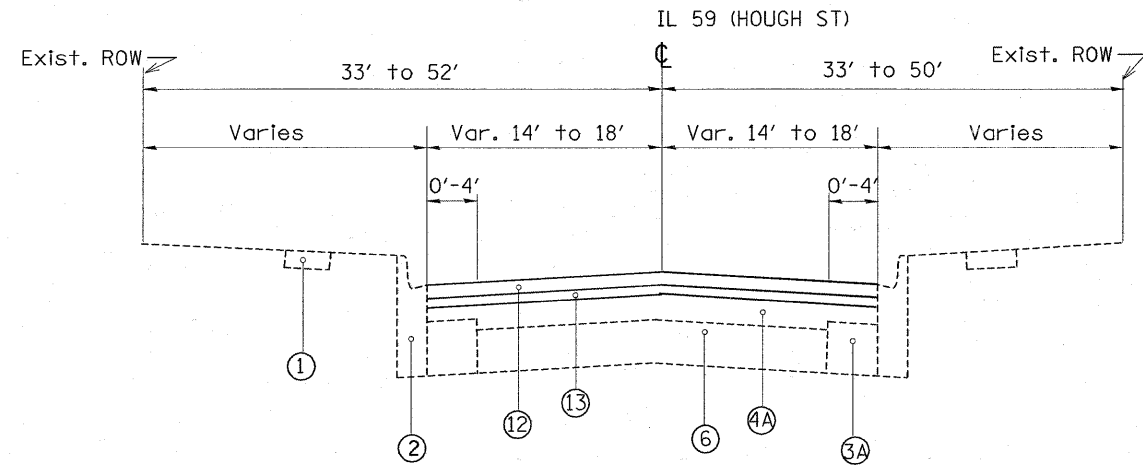
- ① EXISTING PC CONCRETE SIDEWALK
- ② EXISTING COMB. CONCRETE CURB AND GUTTER
- ③ EXISTING PC CONCRETE BASE COURSE ± 7"
- ④A EXISTING PC CONCRETE BASE COURSE ± 8"
- ④ EXISTING HMA, AFTER MILLING SURFACE, ± 2 1/4"
- ④A EXISTING HMA, AFTER MILLING SURFACE 1 3/4"
- ⑥ EXISTING PAVEMENT ± 8"
- ⑦ EXISTING HMA SHOULDER
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ EXISTING PC CONCRETE BASE COURSE, 10"
- ⑩ EXISTING BINDER 3"
- ⑫ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑬ PROPOSED LEVELING BINDER (MACHINE METHOD), N50, 3/4"

FILE NAME =	USER NAME = gorenautab	DESIGNED -	REVISED -
ct\pwwork\PIWIDOT\GORENGAUTAB\dms89240	sh_rdwj.dgn	DRAWN -	REVISED -
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISIONS -	REVISIONS -
PLOT DATE = 3/18/2009	DATE -	REVISIONS -	REVISIONS -

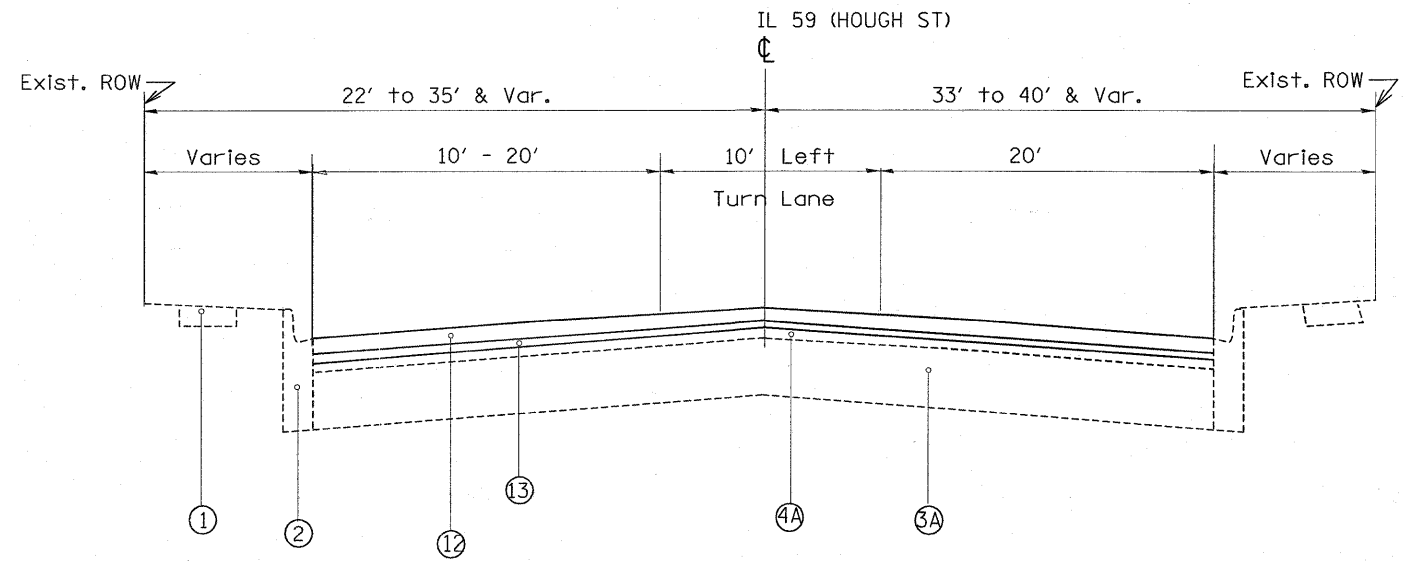
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL 59 (HOUGH STREET). MAIN STREET TO BARRINGTON ROAD			
PROPOSED TYPICAL SECTIONS			
SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	

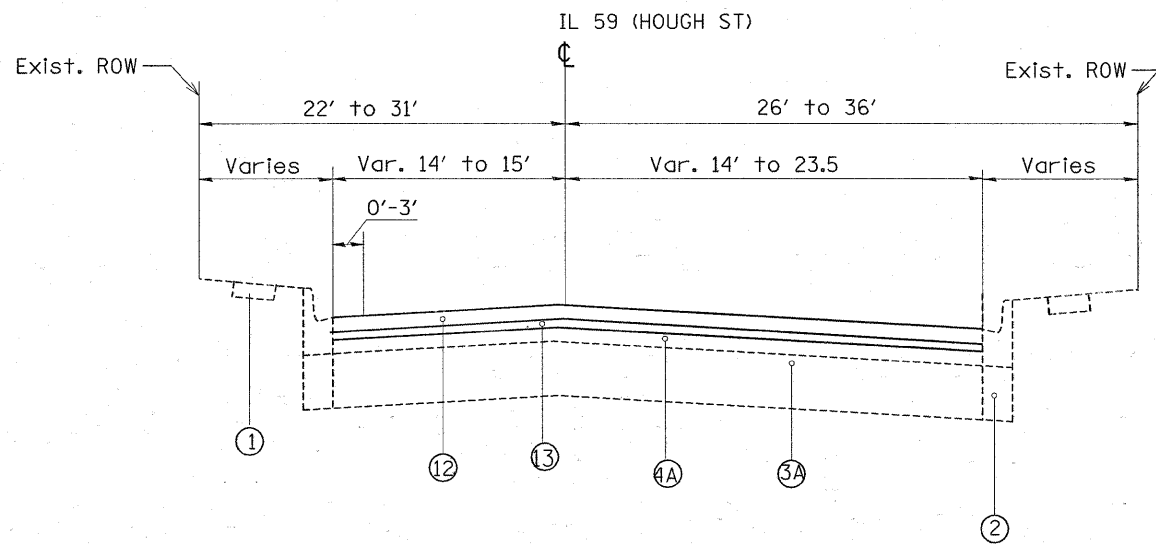
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	1212 RS - 3	COOK & LAKE	23	6
CONTRACT NO. 60F37				
ILLINOIS FED. AID PROJECT				



PROPOSED TYPICAL SECTION  
STA 150+81 TO STA 174+43.1



PROPOSED TYPICAL SECTION  
STA 176+05 TO STA 183+33



PROPOSED TYPICAL SECTION  
STA 174+43.1 TO STA 176+05

**LEGEND:**

- ① EXISTING PC CONCRETE SIDEWALK
- ② EXISTING COMB. CONCRETE CURB AND GUTTER
- ③ EXISTING PC CONCRETE BASE COURSE ± 7"
- ③A EXISTING PC CONCRETE BASE COURSE ± 8"
- ④ EXISTING HMA, AFTER MILLING SURFACE, ± 2 1/4"
- ④A EXISTING HMA, AFTER MILLING SURFACE 1 3/4"
- ⑥ EXISTING PAVEMENT ± 8"
- ⑦ EXISTING HMA SHOULDER
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ EXISTING P.C. CONCRETE BASE COURSE, 10"
- ⑫ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑬ PROPOSED LEVELING BINDER (MACHINE METHOD), N50, 3/4"

THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING

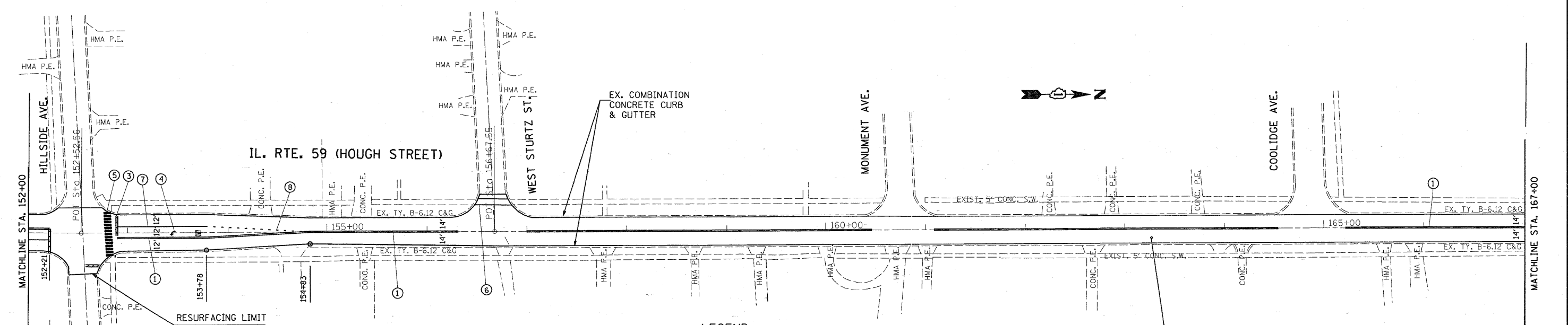
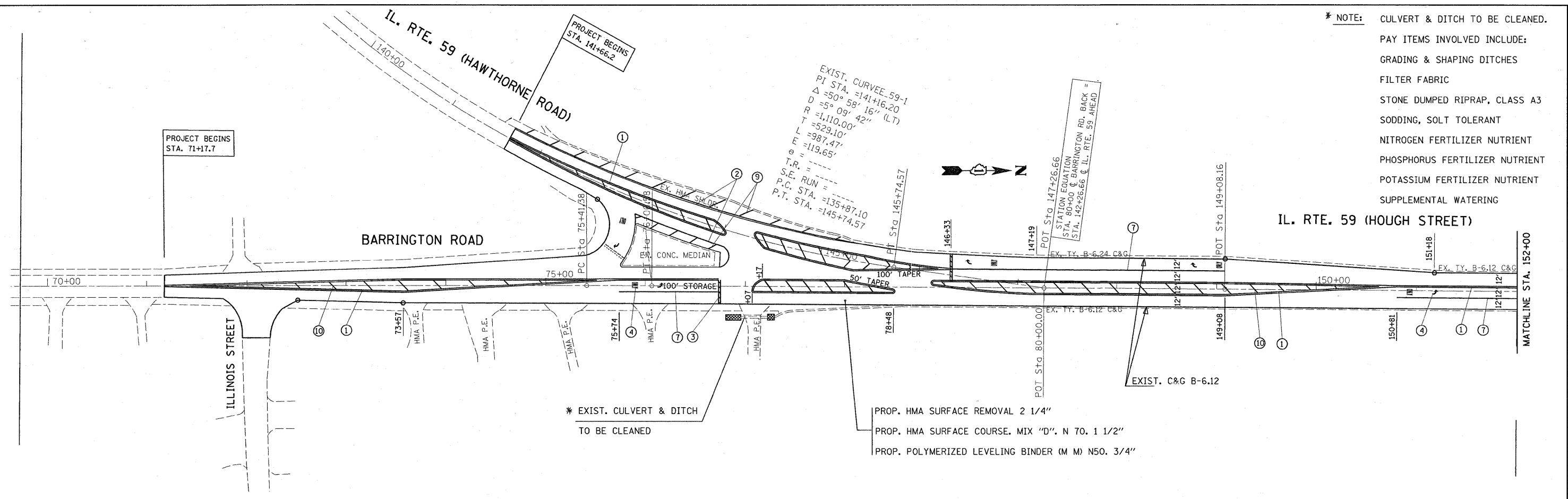
FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL 59 (HOUGH STREET). MAIN STREET TO BARRINGTON ROAD PROPOSED TYPICAL SECTIONS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\p\1007\GORENGAUTAB\dms89240	sh_rdw.dgn	DRAWN -	REVISED -			338	1212 RS - 3	COOK & LAKE	23	7	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 60F37					
	PLOT DATE = 3/18/2009	DATE -	REVISED -			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT				

\* NOTE: CULVERT & DITCH TO BE CLEANED.

- PAY ITEMS INVOLVED INCLUDE:
- GRADING & SHAPING DITCHES
  - FILTER FABRIC
  - STONE DUMPED RIPRAP, CLASS A3
  - SODDING, SOLT TOLERANT
  - NITROGEN FERTILIZER NUTRIENT
  - PHOSPHORUS FERTILIZER NUTRIENT
  - POTASSIUM FERTILIZER NUTRIENT
  - SUPPLEMENTAL WATERING

EXIST. CURVEE-59-1  
 PI STA. =141+16.20  
 $\Delta = 50^\circ 58' 16''$  (LT)  
 $D = 5^\circ 09' 42''$   
 $R = 1,110.00'$   
 $L = 529.10'$   
 $E = 987.47'$   
 $\theta = 119.65'$   
 T.R. =  
 S.E. RUN =  
 P.C. STA. =135+87.10  
 P.T. STA. =145+74.57

STATION EQUATION  
 STA. 80+00 @ BARRINGTON RD, BACK =  
 STA. 142+26.66 @ ILL. RTE. 59 AHEAD

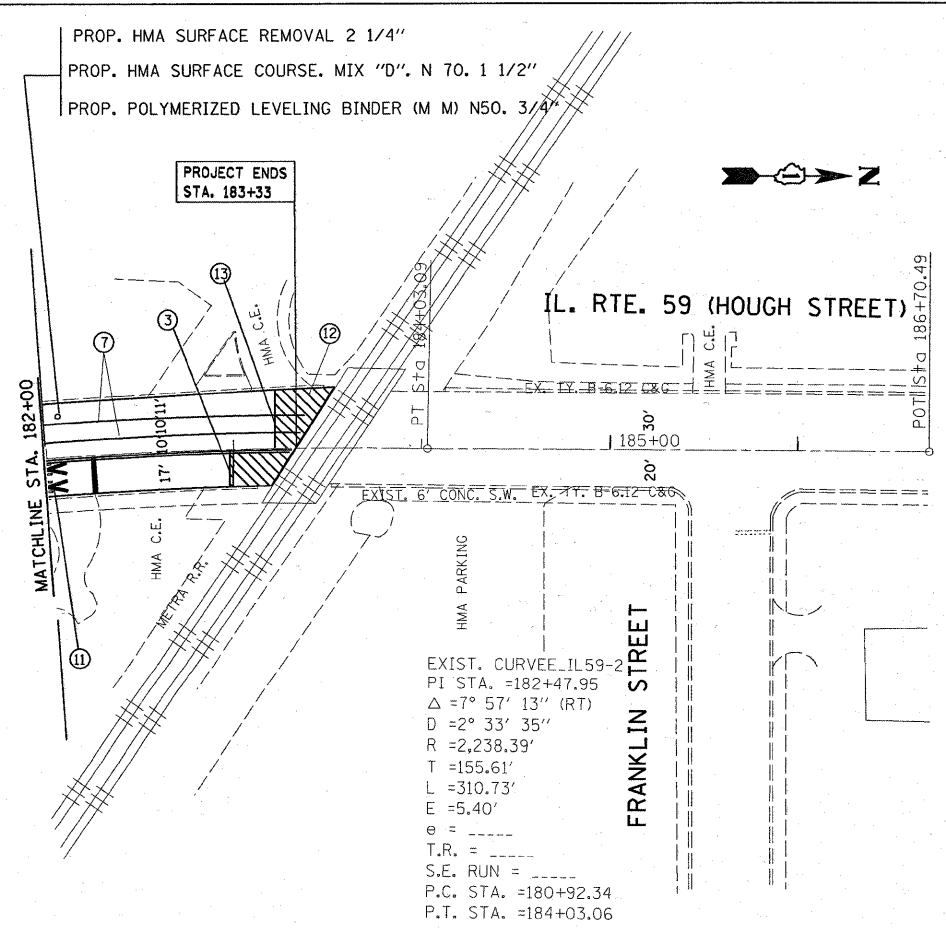
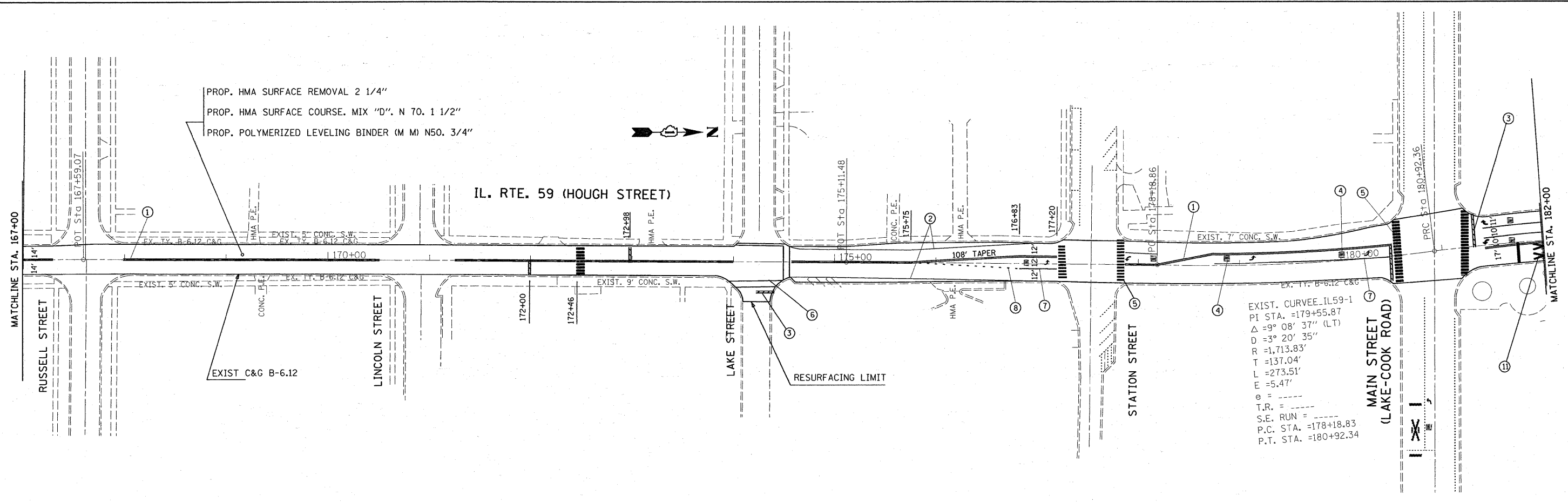


**LEGEND**

- ① THPL. PAVT MARKING 4" SOLID DOUBLE YELLOW CENTERLINE (TYP)
- ② THPL. PAVT MARKING 4" SOLID WHITE EDGE LINE (TYP)
- ③ THPL. PAVT MARKING 24" SOLID WHITE STOP BAR (TYP)
- ④ THPL. PAVT MARKING LETTERS AND SYMBOLS (TYP)
- ⑤ THPL. PAVT MARKING 12" SOLID WHITE SCHOOL CROSSING (TYP)
- ⑥ THPL. PAVT MARKING 6" SOLID WHITE CROSS WALK (TYP)
- ⑦ THPL. PAVT MARKING 6" SOLID WHITE LINE, LEFT TURN LANE (TYP)
- ⑧ THPL. PAVT MARKING 6" SOLID WHITE DOTTED LINE (TYP)
- ⑨ THPL. PAVT MARKING 12" SOLID WHITE DIAGONALS (TYP)
- ⑩ THPL. PAVT MARKING 12" SOLID YELLOW DIAGONALS (TYP)
- ⑪ THPL. PAVT MARKING "RR MARKING" LETTERS & SYMBOLS
- ⑫ THPL. PAVT MARKING 6" SOLID WHITE DIAGONALS (TYP)
- ⑬ THPL. PAVT MARKING 4" SOLID WHITE LINE (TYP)

FILE NAME = c:\pwwork\pwwid\DOT\CORENGAUTAB\dms89240	USER NAME = gorengautab	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 59 (HOUGH ST.)</b>	F.A.P RTE. 338	SECTION 1212 RS-3	COUNTY COOK & LAKE	TOTAL SHEETS 23	SHEET NO. 8	
PLOT SCALE = 50,0000' / IN.	DRAWN -	REVISED -	SCALE:			SHEET NO. OF SHEETS	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 60F37		
PLOT DATE = 3/28/2009	CHECKED -	REVISED -	STA. 141+66.2 TO STA. 167+00								
	DATE -	REVISED -									



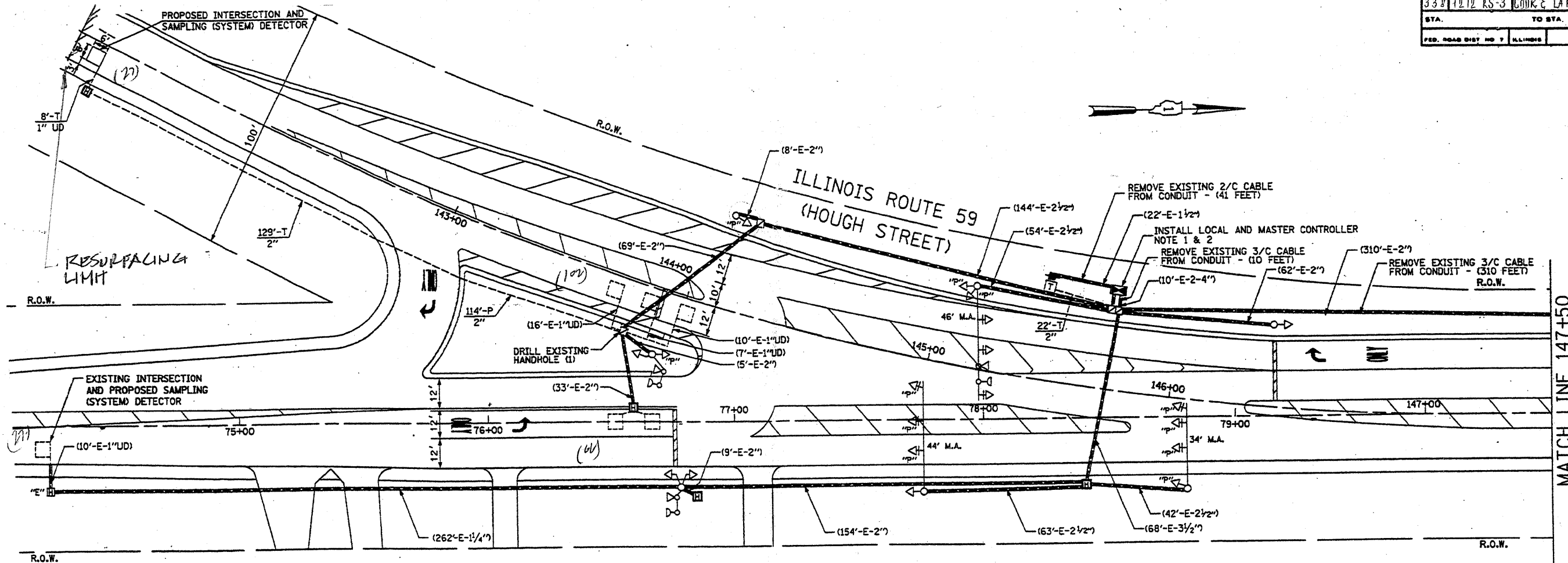


- LEGEND**
- ① THPL. PAVT MARKING 4" SOLID DOUBLE YELLOW CENTERLINE (TYP)
  - ② THPL. PAVT MARKING 4" SOLID WHITE EDGE LINE (TYP)
  - ③ THPL. PAVT MARKING 24" SOLID WHITE STOP BAR (TYP)
  - ④ THPL. PAVT MARKING LETTERS & SYMBOLS (TYP)
  - ⑤ THPL. PAVT MARKING 12" SOLID WHITE SCHOOL CROSSING (TYP)
  - ⑥ THPL. PAVT MARKING 6" SOLID WHITE CROSS WALK (TYP)
  - ⑦ THPL. PAVT MARKING 6" SOLID WHITE LINE, LEFT TURN LANE (TYP)
  - ⑧ THPL. PAVT MARKING 6" SOLID WHITE DOTTED LINE (TYP)
  - ⑨ THPL. PAVT MARKING 12" SOLID WHITE DIAGONALS (TYP)
  - ⑩ THPL. PAVT MARKING 12" SOLID YELLOW DIAGONALS (TYP)
  - ⑪ THPL. PAVT MARKING "RR MARKING" LETTERS & SYMBOLS
  - ⑫ THPL. PAVT MARKING 6" SOLID WHITE DIAGONALS (TYP)
  - ⑬ THPL. PAVT MARKING 4" SOLID WHITE LINE (TYP)

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL ROUTE 59 (HOUGH ST.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwwork\pwwork\gorengautab\dms89240\sh-dwg.dgn		DRAWN -	REVISED -			338	1212 RS-3	COOK & LAKE	23	9	
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60F37					
PLOT DATE = 3/20/2009		DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

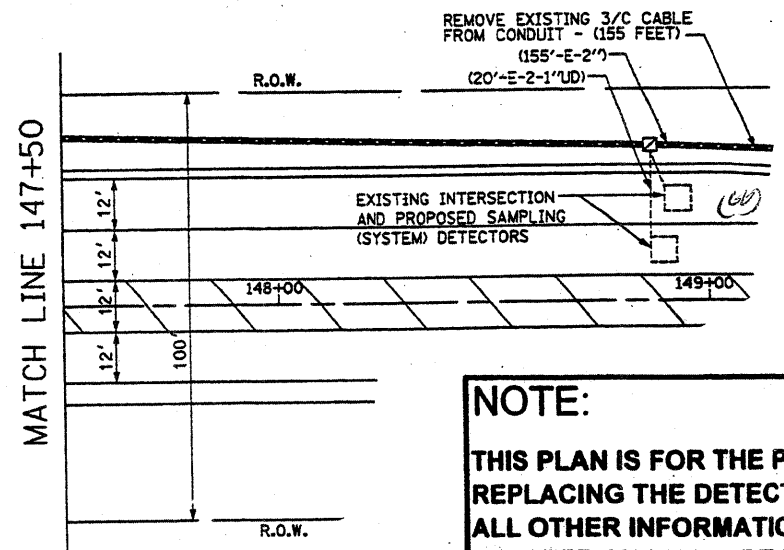
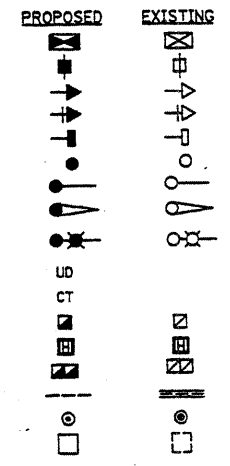
P.A.D. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
33.0	1212 AS-3	COOK & LAKE	23	10
STA.	TO STA.		FED. ROAD DIST. NO. 7 ILLINOIS	
			FED. AID PROJECT	

60F 37



**BARRINGTON ROAD TRAFFIC SIGNAL LEGEND**

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- MAST ARM ASSEMBLY AND POLE, ALUMINUM
- COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE
- UNIT DUCT
- COMMON TRENCH
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSHBUTTON DETECTOR
- DETECTOR LOOP



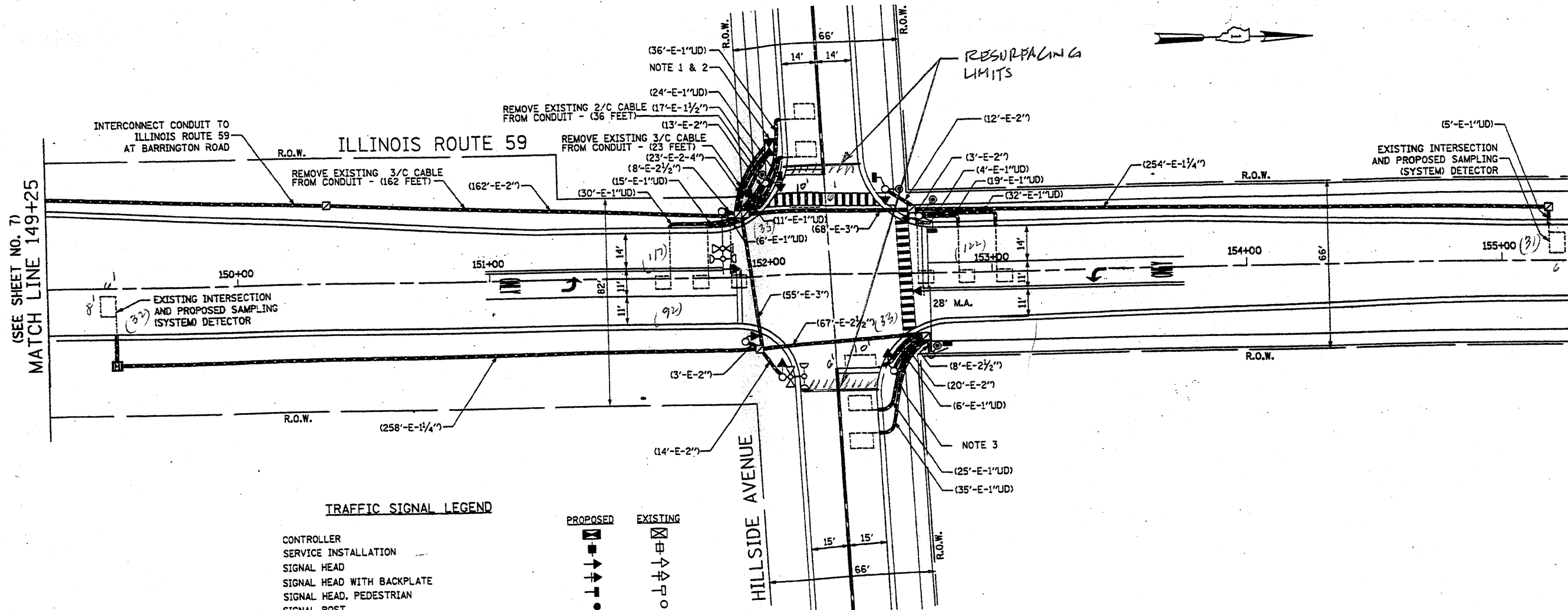
**NOTE:**  
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

**REPLACE ALL DETECTOR LOOPS AS SHOWN**  
 (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
80600600	288	Foot	Detector Loop Replacement

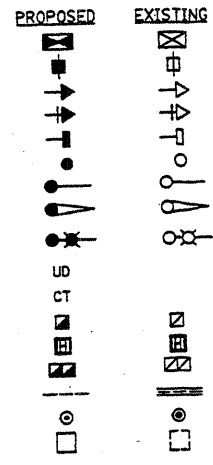
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETECTOR LOOP REPLACEMENT**  
 ILL. ROUTE 59 @ BARRINGTON RD.  
 SCALE: NONE  
 DATE: MAR. 2009  
 DRAWN BY: JHE  
 DESIGNED BY: JHE  
 CHECKED BY: DAD



**TRAFFIC SIGNAL LEGEND**

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- MAST ARM ASSEMBLY AND POLE, ALUMINUM
- COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE
- UNIT DUCT
- COMMON TRENCH
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSHBUTTON DETECTOR
- DETECTOR LOOP



**REPLACE ALL DETECTOR LOOPS AS SHOWN**  
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
80500600	343	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

**NOTE:**  
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

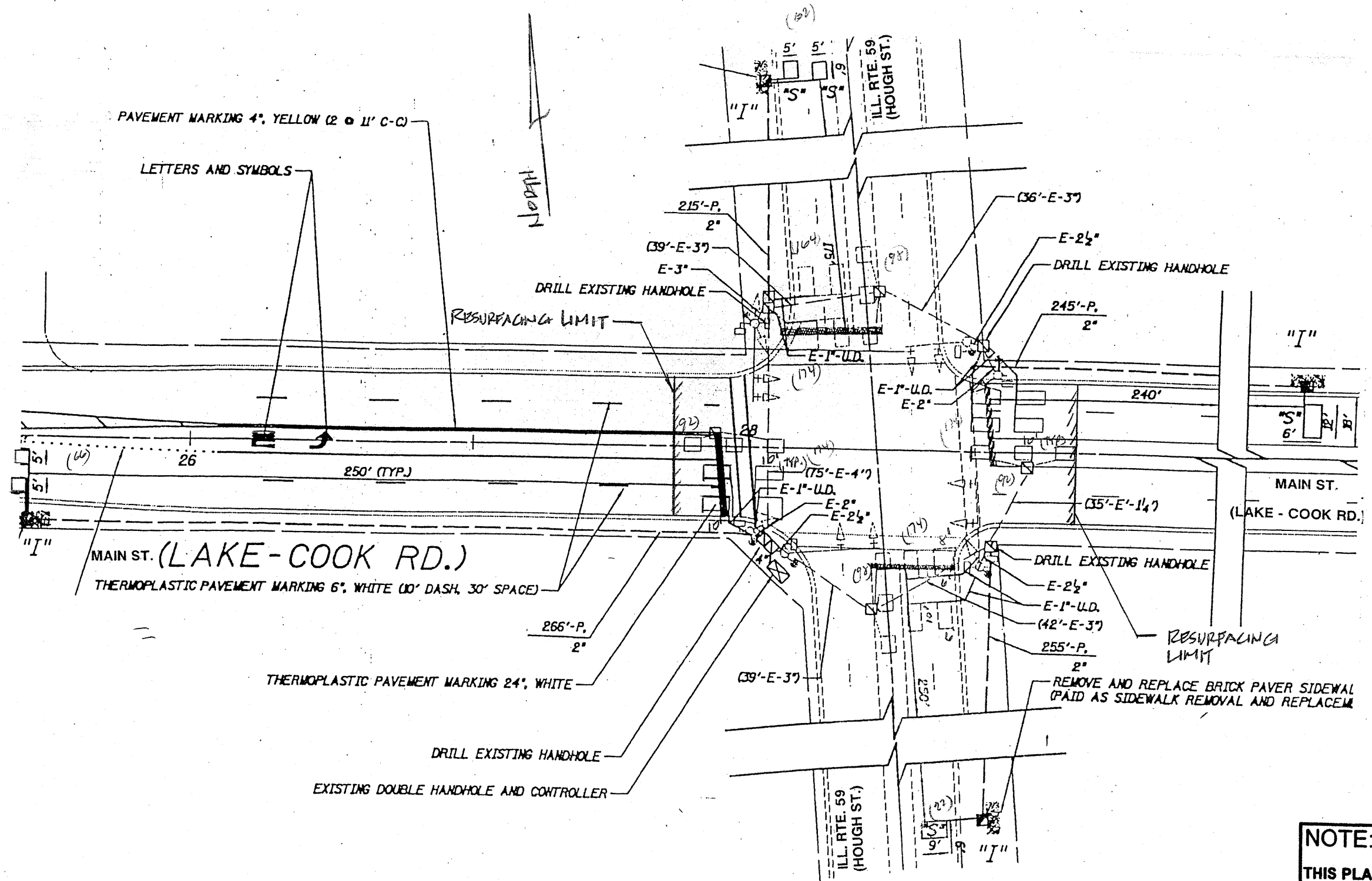
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETECTOR LOOP REPLACEMENT**

ILL. ROUTE 59 @ HILLSIDE AVENUE

SCALE: NONE  
DATE: MAR. 2009

DRAWN BY: JHE  
DESIGNED BY: JHE  
CHECKED BY: DAD

60F37



**REPLACE ALL DETECTOR LOOPS AS SHOWN**

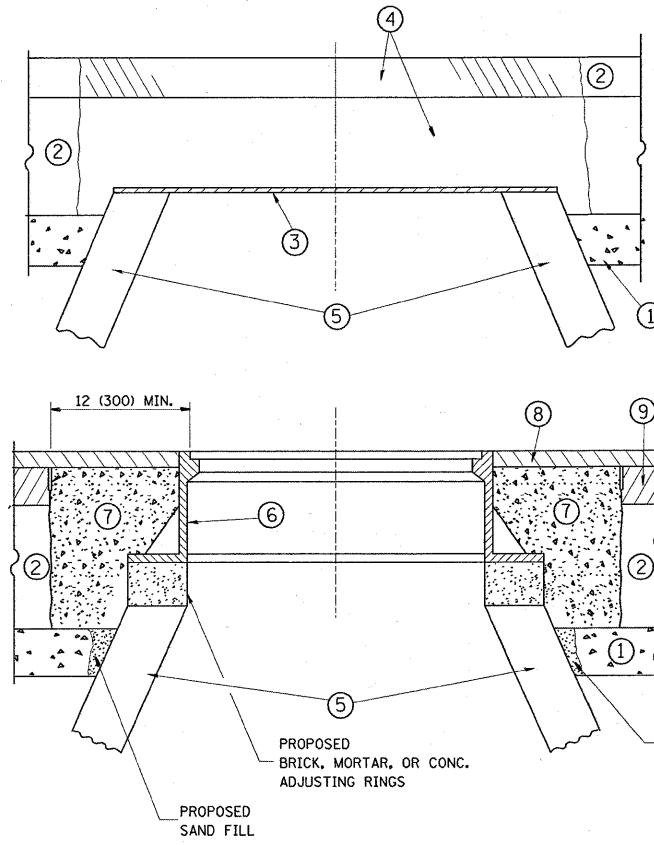
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	1,161	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

**NOTE:**  
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DETECTOR LOOP REPLACEMENT**  
ILL. ROUTE 59 @ LAKE/COOK RD.  
SCALE: NONE  
DATE: MAR. 2009  
DRAWN BY: JHE  
DESIGNED BY: JHE  
CHECKED BY: JHE



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

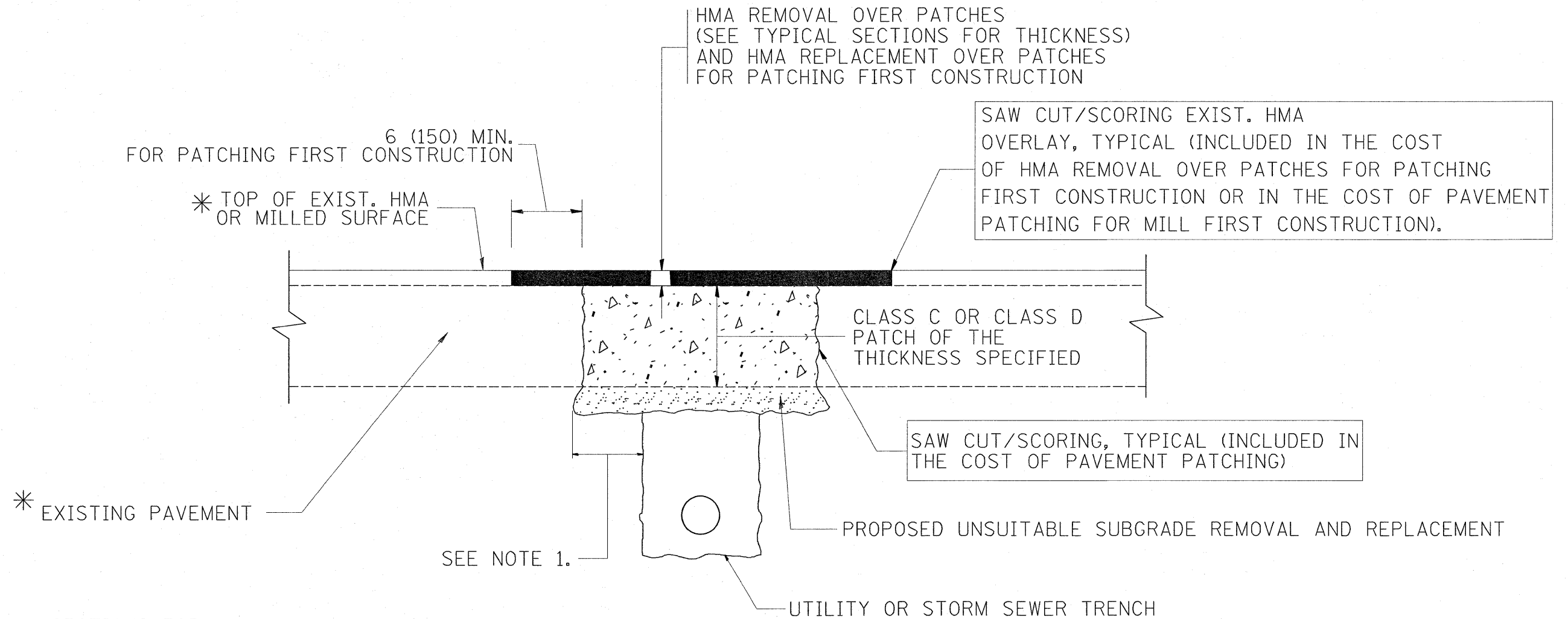
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"  
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = gorengautab	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct\pw_work\NPWIDOT\GORENGAUTAB\dms89240	D:\st5td.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	338	1212 RS - 3	COOK & LAKE	23	13
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04	REVISED - R. BORO 01-01-07					<b>BD600-03 (BD-8)</b>		CONTRACT NO. 60F37		
PLOT DATE = 3/20/2009	DATE - 10-25-94							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
at\pwork\PWIDOT\GORENGAUTAB\dms89240	DistStd.dgn	DRAWN -	REVISED - R. BORO 01-01-07		338	1212M RS - 3	COOK & LAKE	23	14			
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - R. BORO 09-04-07		<b>BD400-04 (BD-22)</b>			CONTRACT NO. 60F37				
	PLOT DATE = 3/28/2009	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

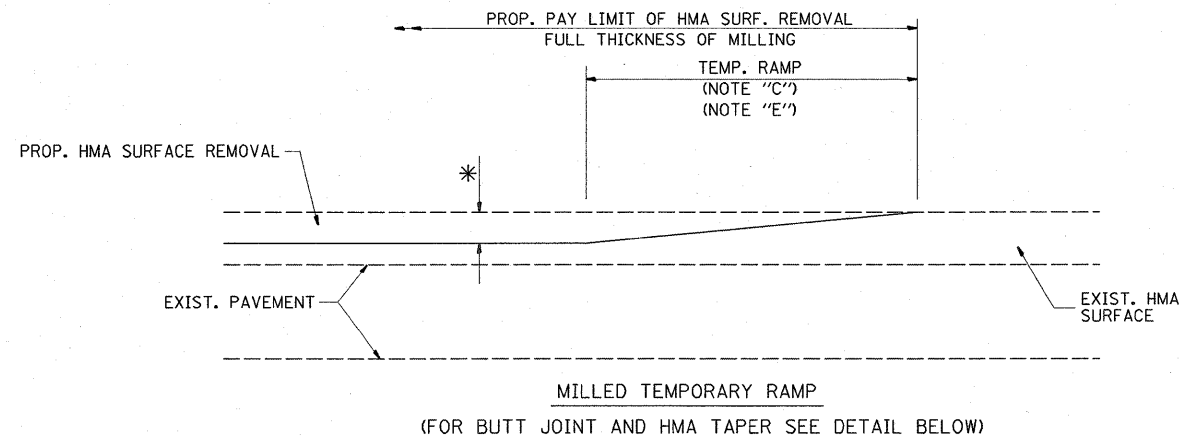
**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

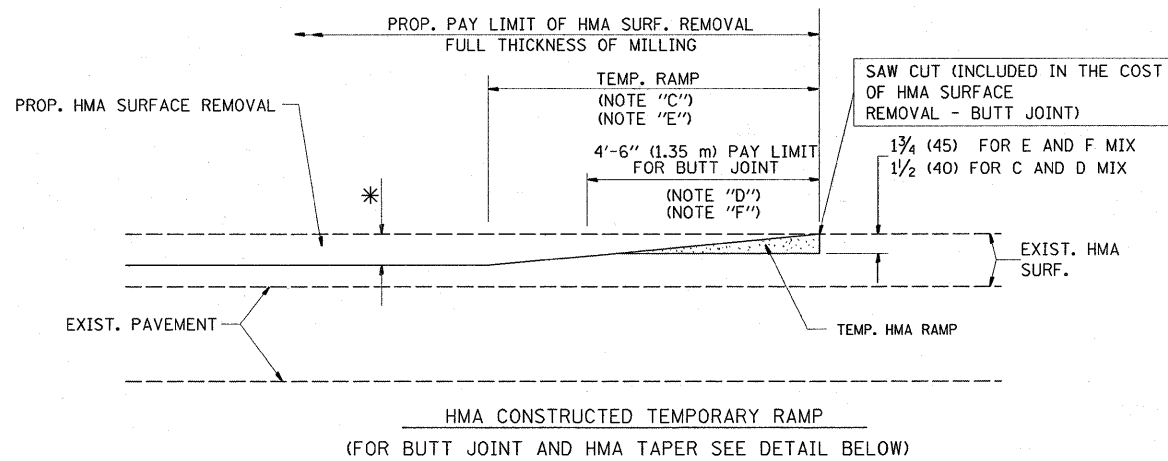
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\PW100T\GORENGAUTAB\dms892401	DistStd.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97		338	1212 RS - 3	COOK & LAKE	23	15		
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01	REVISED - R. BORO 01-01-07		<b>BD600-06 (BD-24)</b>		CONTRACT NO. 60F37				
PLOT DATE = 3/20/2009	DATE - 03-11-94				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

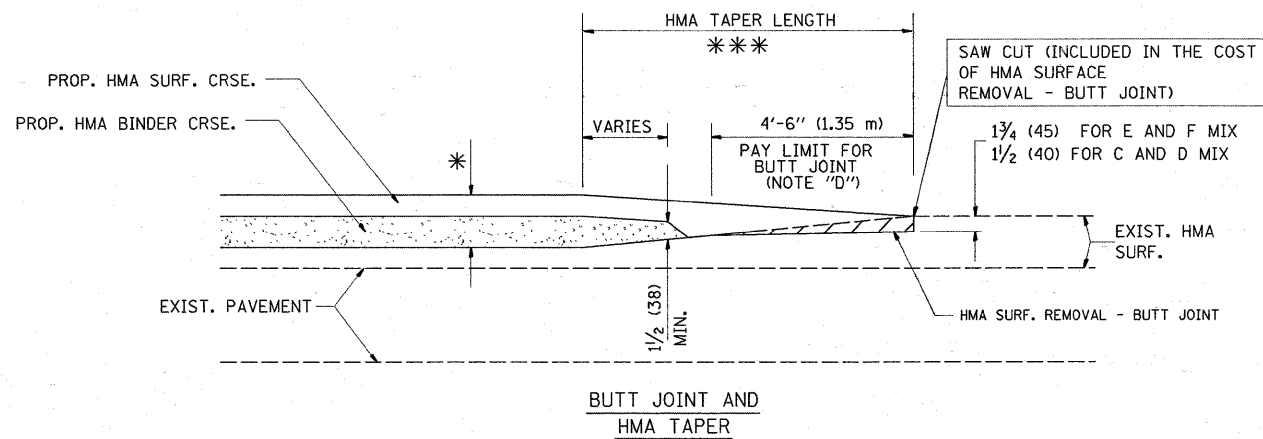


**OPTION 1**

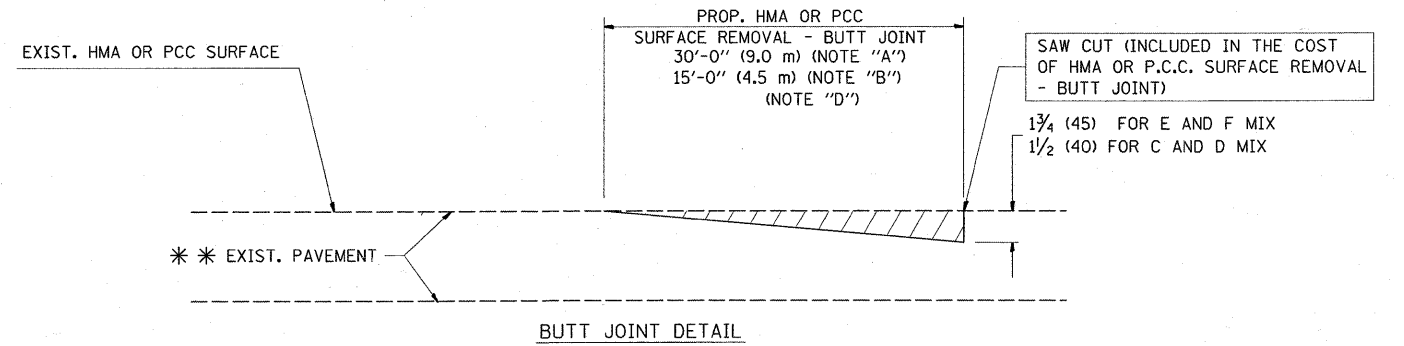


**OPTION 2**

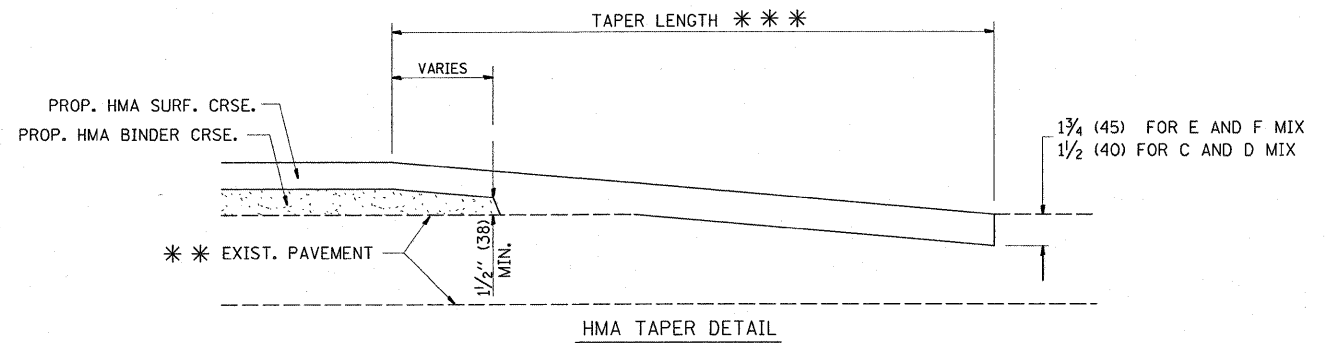
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
c:\pw_work\PW1D0T\GORENGAUTAB\dms89240	DistStd.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 3/20/2009	DATE - 06-13-90	REVISED - R. BORO 01-01-07

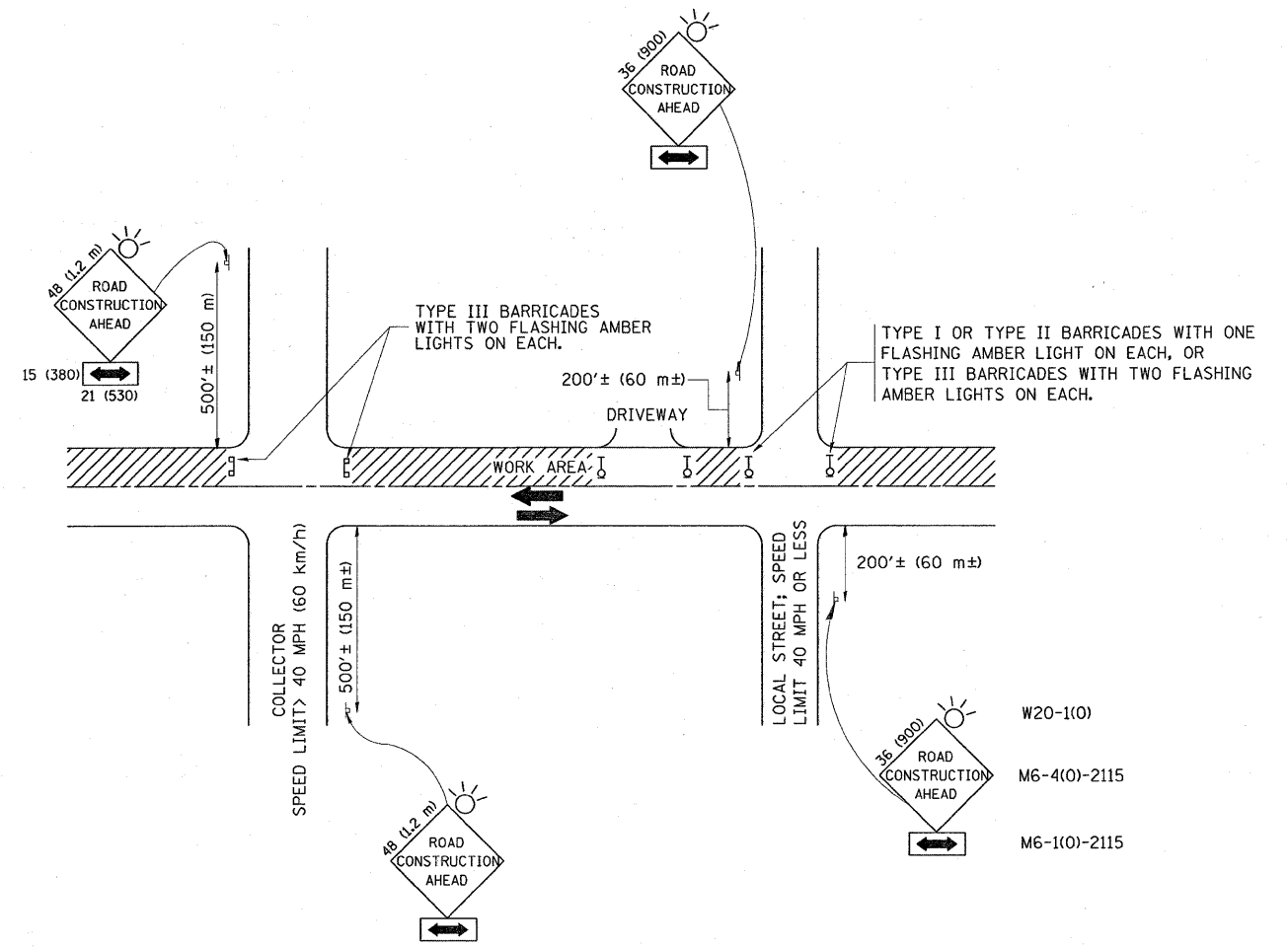
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	1212 RS - 3	COOK & LAKE	22	16
BD400-05 BD32			CONTRACT NO. 60F37	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

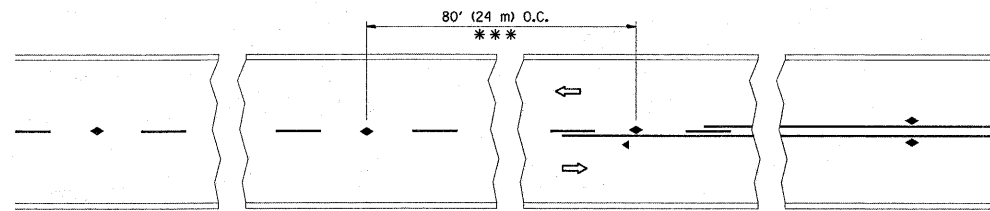
FILE NAME =	USER NAME = gorengautab	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
ct:\pwwork\pwwid01\GORENGAUTAB\dms89248	DistStd.dgn	DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 3/28/2009	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

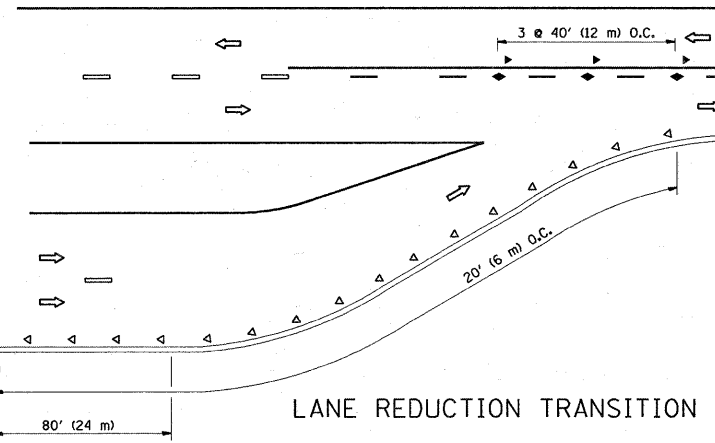
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	1212 RS - 3	COOK & LAKE	23	17
TC-10			CONTRACT NO. 60F37	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

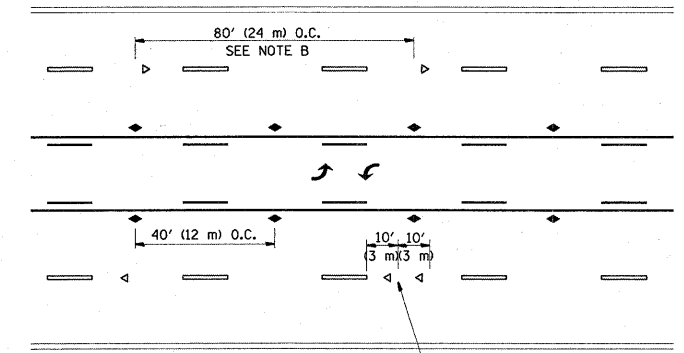


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

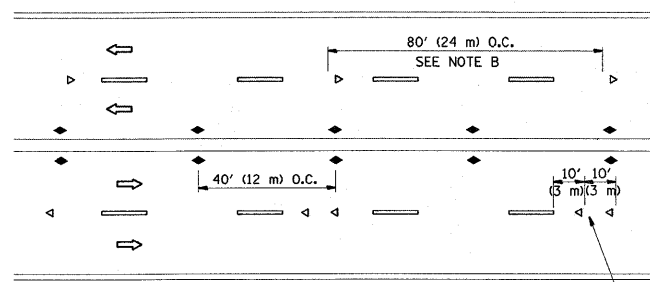
TWO-LANE/TWO-WAY



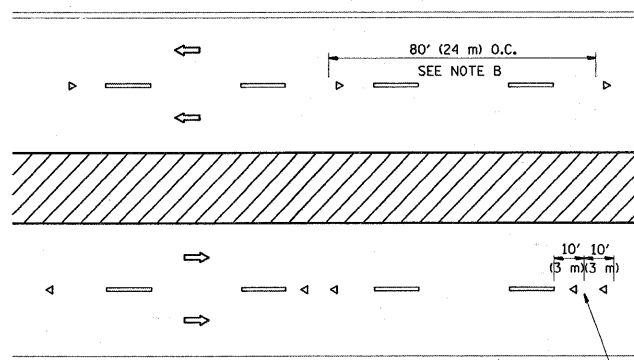
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

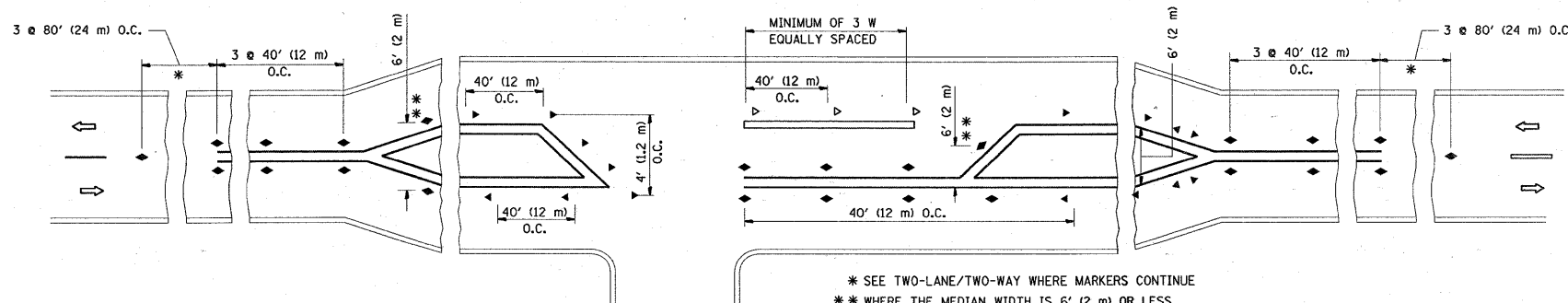
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

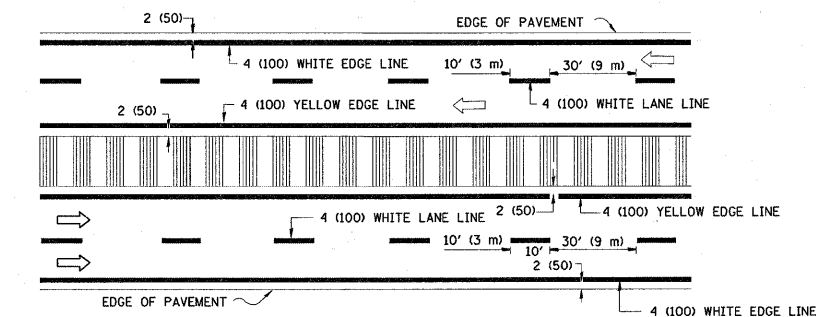
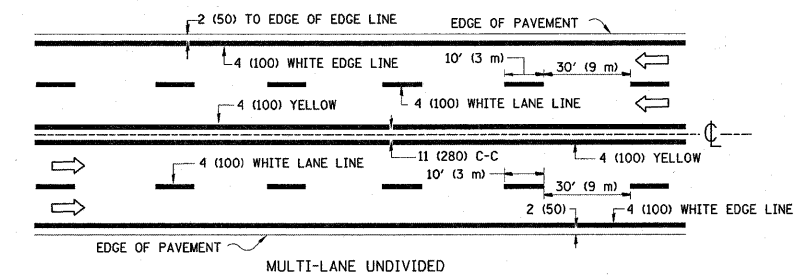
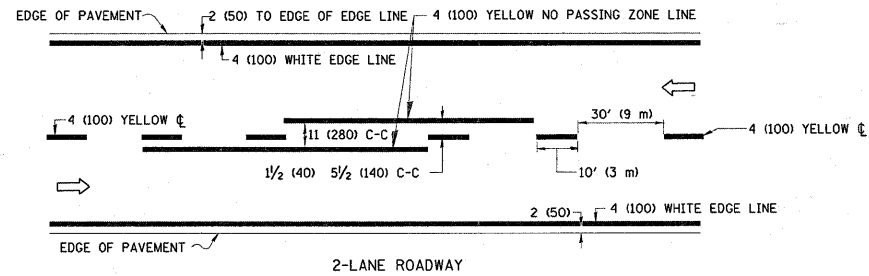
FILE NAME =	USER NAME = gorangaub	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
ci\pw_work\WPWIDOT\GORENGAUB\dms89240	DistStd.dgn	DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 3/28/2009	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

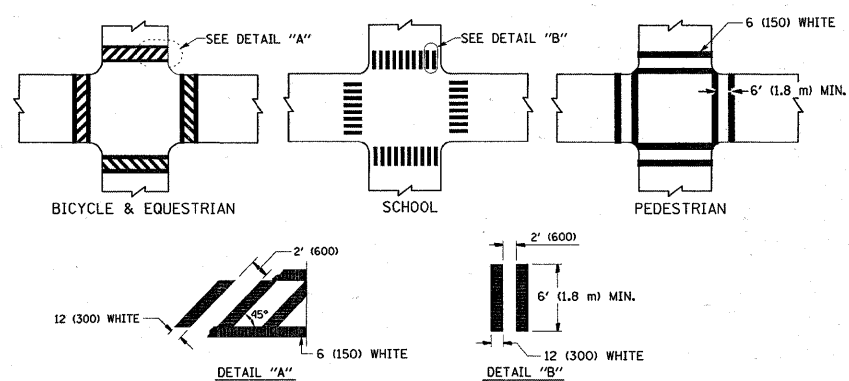
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	1212 RS - 3	COOK & LAKE	23	18
TC-11			CONTRACT NO. 60F37	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

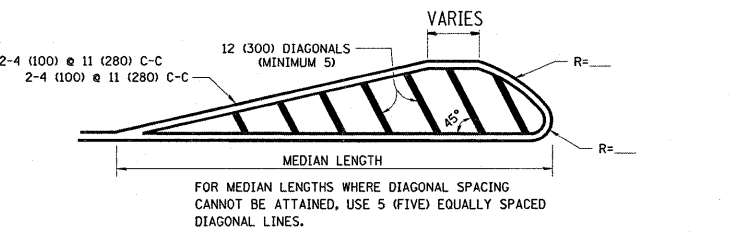
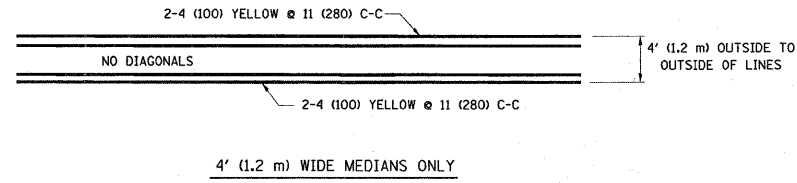


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**

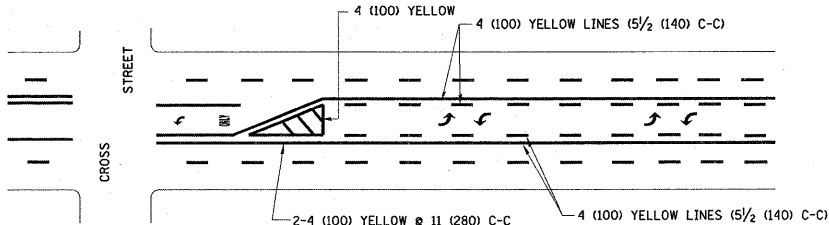


**TYPICAL CROSSWALK MARKING**

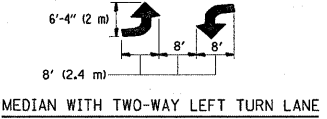


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

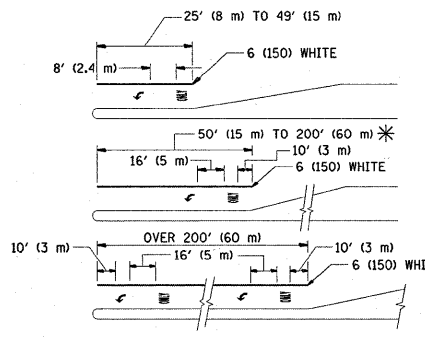
**MEDIANS OVER 4' (1.2 m) WIDE**



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



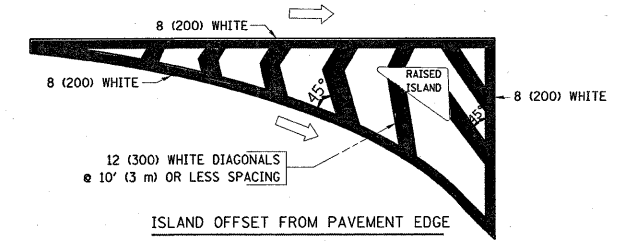
**TYPICAL PAINTED MEDIAN MARKING**



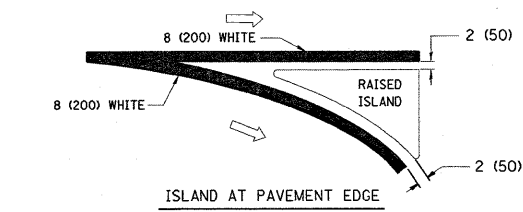
FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**ISLAND OFFSET FROM PAVEMENT EDGE**



**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR" IS 6' (1.8 m) "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

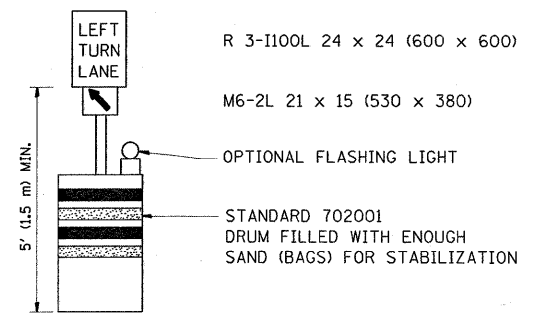
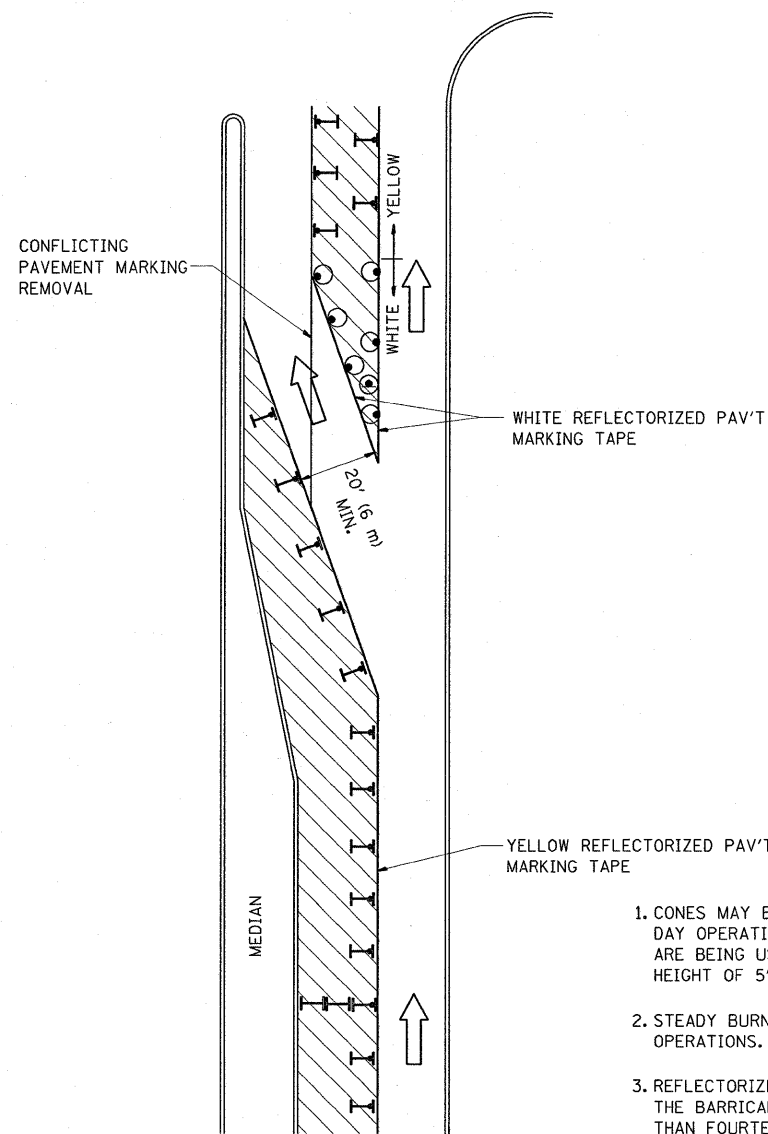
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = gorenautab	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
PLT SCALE = 50.0000' / IN.	DIST STD.DGN	DRAWN -	REVISED - A. HOUSEH 10-09-96
PLT DATE = 3/28/2009		CHECKED -	REVISED - A. HOUSEH 10-17-96
		DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

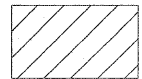





F.A.P. RTE. 338	SECTION 1212 RS - 3	COUNTY COOK & LAKE	TOTAL SHEETS 23	SHEET NO. 19
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				CONTRACT NO. 60F37



**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

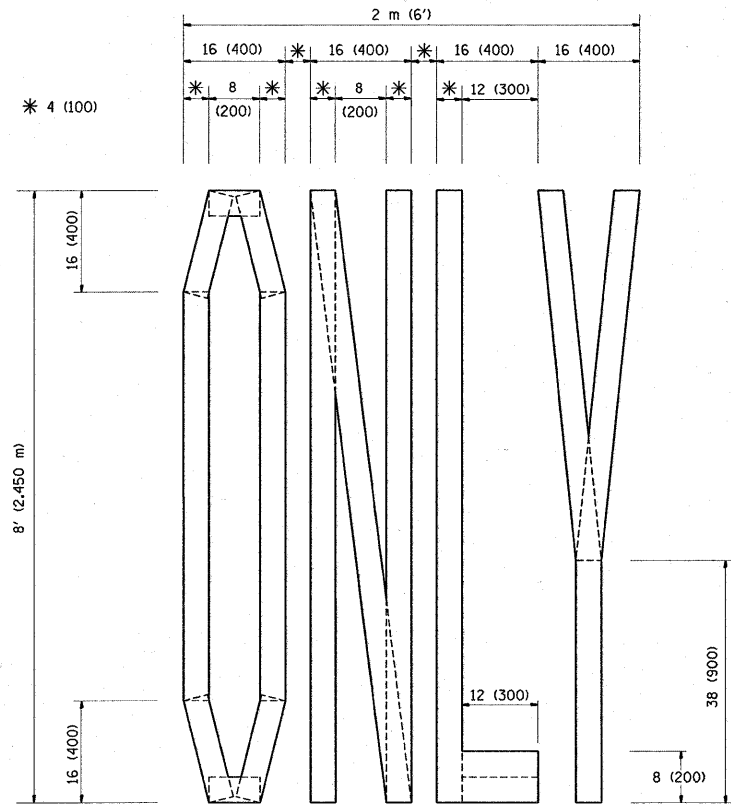
FILE NAME =	USER NAME = gorengoutab	DESIGNED -	REVISED -T. RAMMACHER 09-08-94
ct:\pw_work\PWIDOT\GORENGAUTAB\dms89240	DistStd.dgn	DRAWN -	REVISED - A. HOUSEH 11-07-95
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-12-96
	PLOT DATE = 3/20/2009	DATE -	REVISED -T. RAMMACHER 01-06-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

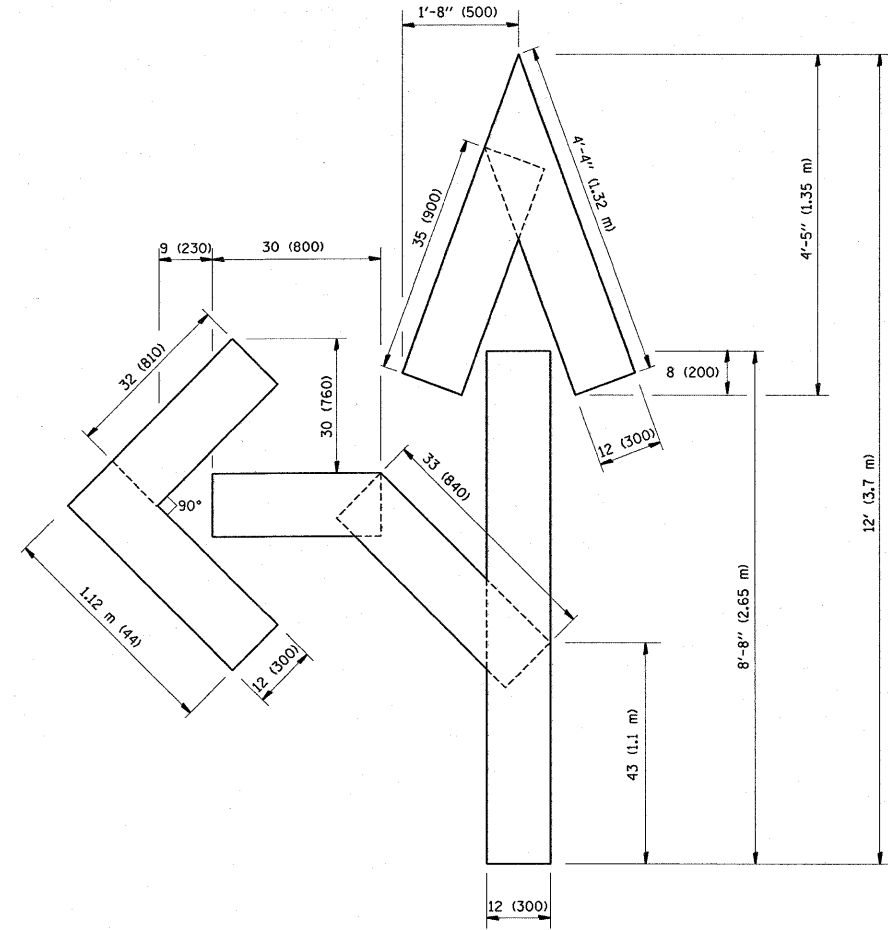
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

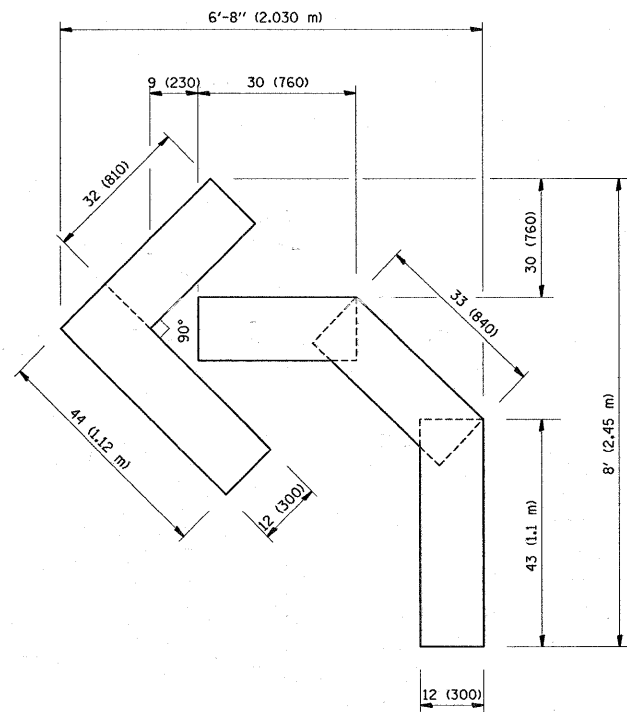
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	1212 RS - 3	COOK & LAKE	23	20
TC-14			CONTRACT NO. 60F37	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

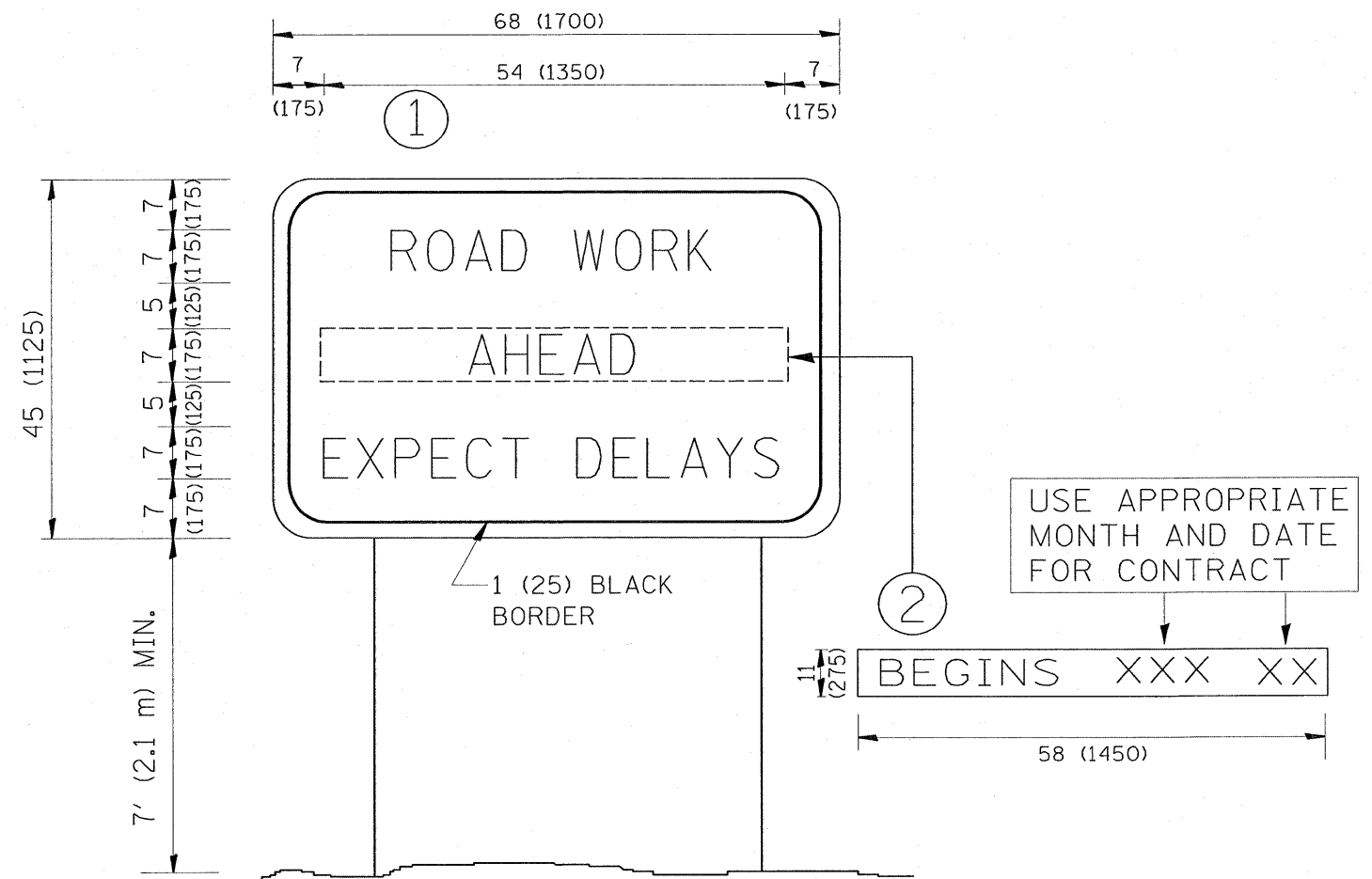
FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
c:\pw_work\PWIDOT\GORENGAUTAB\dms89240	DistStd.dgn	DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000" / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 3/20/2009	DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
338	1212 RS - 3	COOK & LAKE	23	21
TC-16			CONTRACT NO. 60F37	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

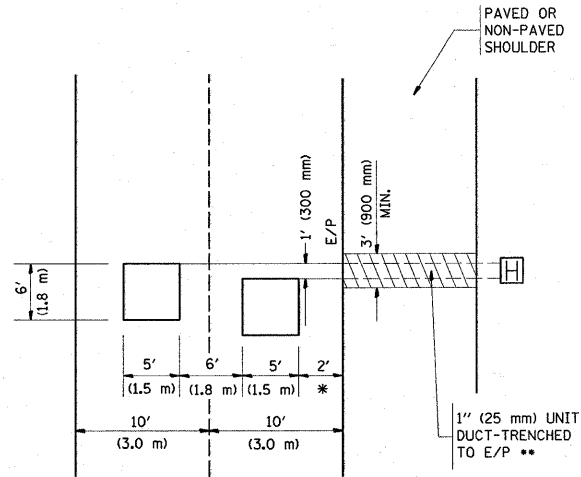
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gorengautab	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\PWIDOT\GORENGAUTAB\dms89240	DistStd.dgn	DRAWN -	REVISED - R. MIRS 12-11-97			338	1212 RS - 3	COOK & LAKE	23	22	
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99			<b>TC-22</b>		CONTRACT NO. 60F37			
	PLOT DATE = 3/20/2009	DATE -	REVISED - C. JUCIUS 01-31-07			FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT					
				SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.		TO STA.	

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

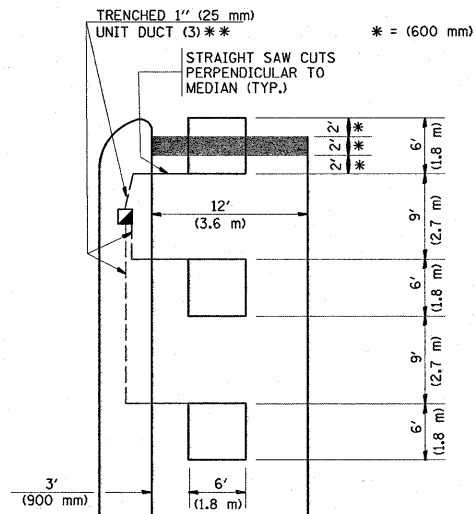


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

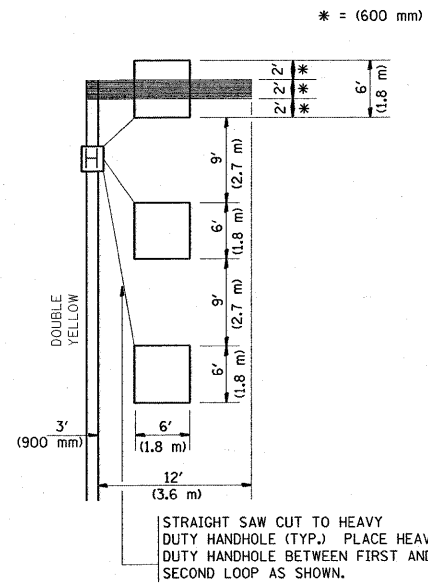


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

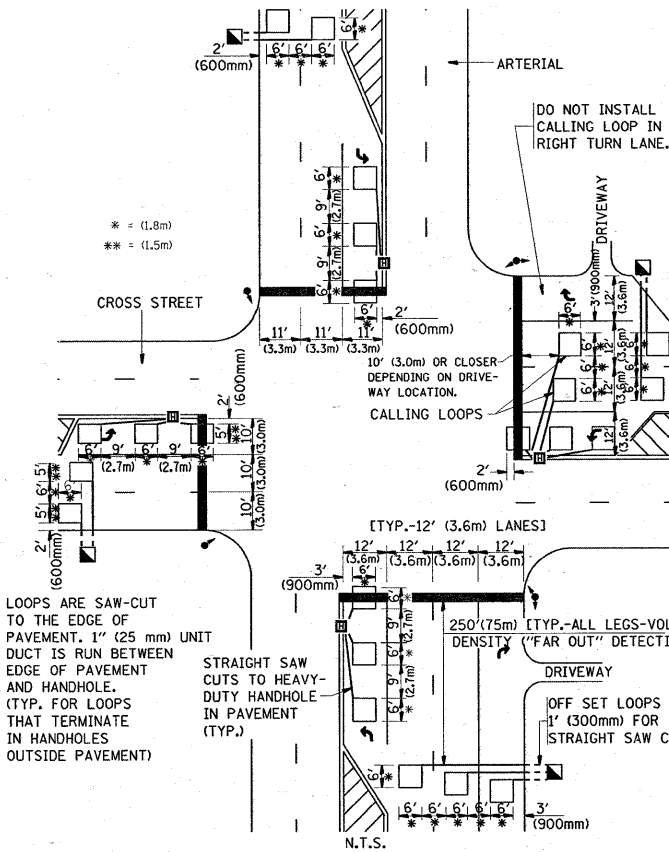
**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**



\* = (600 mm)

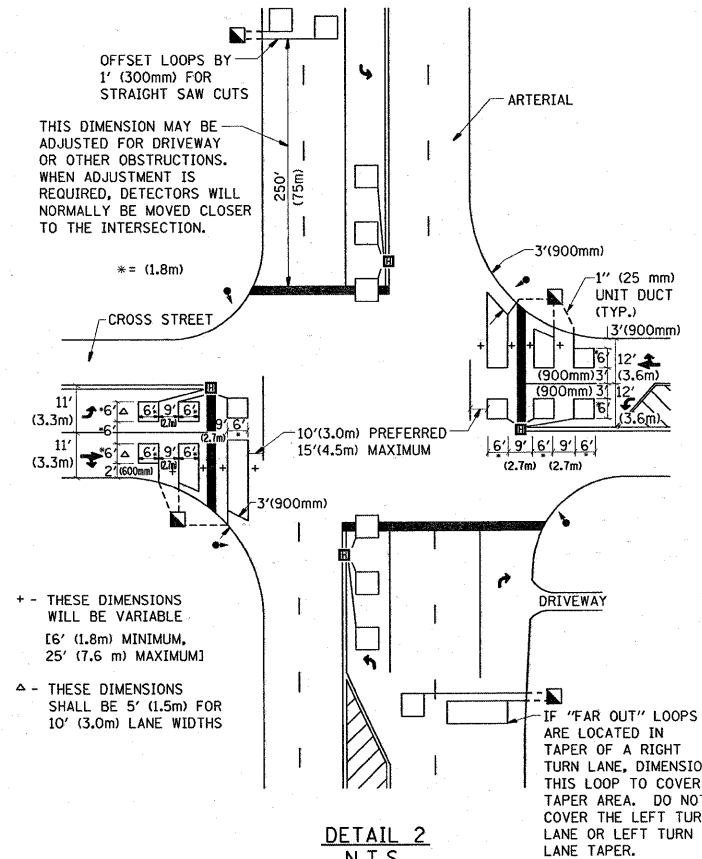
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = gorengeutab	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
02:\pwwork\pwwid\DOT\GORENGAUTAB\dms89240	D:\st\dtd.dgn	DRAWN -	REVISED -			338	1212 RS-3	COOK & LAKE	23	23
PLOT SCALE = 50.0000' / IN.		CHECKED - R.K.F.	REVISED -			<b>TS-07</b>		CONTRACT NO. 60F37		
PLOT DATE = 3/28/2009		DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT