

CITY OF AURORA AURORA, ILLINOIS

CONSTRUCTION PLANS FOR AURORA MUNICIPAL AIRPORT

REHABILITATE AND STRENGTHEN TAXIWAY ALPHA ENDS AND CONNECTING TAXIWAYS - PHASE 2

ILLINOIS PROJECT: ARR-3770
A.I.P. PROJECT: 3-17-0003-B36

FEBRUARY 1, 2008

CITY OF AURORA, ILLINOIS

APPROVED Thomas Weisner MAYOR
THOMAS WEISNER

DATE 2-8-08 20

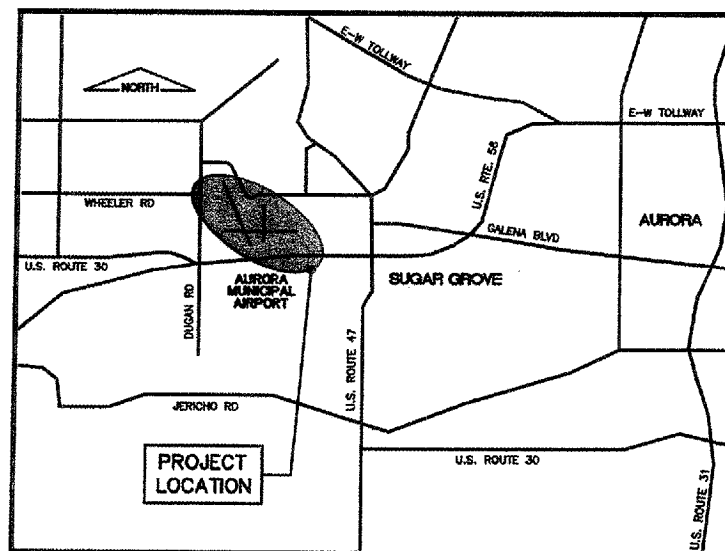
APPROVED [Signature] AIRPORT DIRECTOR
ROBERT A. RIESER, P.E., A.A.E.

DATE 2-7-08 20

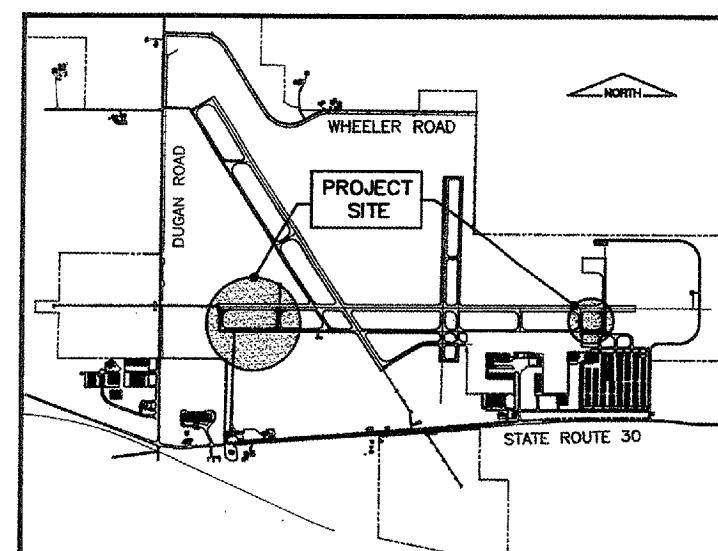
CMT 07285-04
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
DAVID K. PEABODY
062-048437
STATE OF ILLINOIS
EXP. 11-30-09

SUBMITTED BY D. Kyle Peabody, P.E.
D. KYLE PEABODY, P.E.

DATE April 3 2008



LOCATION MAP



SITE PLAN

CALL J.U.L.I.E
BEFORE EXCAVATING
1-800-892-0123

AURORA MUNICIPAL AIRPORT

SECTION: 17 AND 18 COUNTY: KANE
RANGE: 7 EAST U.S. ROUTE 30
TOWNSHIP: 38 NORTH SUGAR GROVE TOWNSHIP

DESIGN INFORMATION

DESIGN AIRCRAFT APPROACH CATEGORY D
DESIGN AIRCRAFT GROUP III

K:\Aurora\0428505 Try A Ends Rehab\PH2\Draw\S
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 LAYOUT: Exhibit1
 UPDATE BY: Marc Katz
 SURVEY BOOK #
 DATE: Monday, April 14, 2008 1:35:14 PM
 XREF DWG: tbcint.dwg
 tb.dwg

SUMMARY OF QUANTITIES

ITEM NUMBER	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR108158	1/C #8 5 KV UG CABLE IN UD	LF	4,395	
AR109210	VAULT MODIFICATIONS	LS	1	
AR110212	2" STEEL DUCT, DIRECT BURY	LF	30	
AR110504	4-WAY CONCRETE ENCASED DUCT	LF	60	
AR110550	SPLIT DUCT	LF	100	
AR125410	MITL - STAKE MOUNTED	EACH	53	
AR125415	MITL - BASE MOUNTED	EACH	6	
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	3	
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	2	
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	2	
AR125470	MODIFY EXISTING SIGN PANEL	EACH	10	
AR125565	SPLICE CAN	EACH	1	
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	38	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	7	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	2	
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	1,715	
AR152455	EMBANKMENT IN PLACE	CY	690	
AR156520	INLET PROTECTION	EACH	6	
AR163520	CONSTRUCTION FENCE	LF	985	
AR201610	BITUMINOUS BASE COURSE	TON	275	
AR208515	POROUS GRANULAR EMBANKMENT	CY	200	
AR209604	CRUSHED AGG. BASE COURSE - 4"	SY	718	
AR401610	BITUMINOUS SURFACE COURSE	TON	135	
AR401655	BUTT JOINT CONSTRUCTION	SY	290	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	5,408	
AR501509	9" PCC PAVEMENT	SY	9,105	
AR501530	PCC TEST BATCH	EACH	1	
AR501900	REMOVE PCC PAVEMENT	SY	100	
AR602510	BITUMINOUS PRIME COAT	GAL	200	
AR603510	BITUMINOUS TACK COAT	GAL	267	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	3,620	
AR620525	PAVEMENT MARKING - BLACK BORDER	SF	3,540	
AR620900	PAVEMENT MARKING REMOVAL	SF	520	
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	LF	1,155	
AR705900	REMOVE UNDERDRAIN	LF	910	
AR705904	REMOVE UNDERDRAIN CLEANOUT	EACH	1	
AR751943	ADJUST MANHOLE	EACH	2	
AR800023	2/C #4 600V, 1/C #8 GND. UG CABLE	LF	710	
AR800081	REMOVE BIT. PVMT. (VAR. DEPTH)	SY	4,000	
AR800160	TEMPORARY GAS LINE CROSSING	EACH	1	
AR901510	SEEDING	ACRE	0.9	
AR904510	SODDING	SY	1,790	
AR905520	TOPSOILING (FROM OFF SITE)	CY	644	
AR908510	MULCHING	ACRE	0.9	

INDEX TO SHEETS

- COVER SHEET
- INDEX TO SHEETS / SUMMARY OF QUANTITIES
- SITE PLAN / PROJECT CONTROL PLAN
- SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS
- SEQUENCE OF CONSTRUCTION PER AC 150/S370-2E (LATEST EDITION)
- TYPICAL SECTIONS
- EXISTING CONDITIONS / PROPOSED REMOVALS - SHEET 1
- EXISTING CONDITIONS / PROPOSED REMOVALS - SHEET 2
- PLAN AND PROFILE - SHEET 1
- PLAN AND PROFILE - SHEET 2
- PAVEMENT JOINTING PLAN - SHEET 1
- PAVEMENT JOINTING PLAN - SHEET 2
- PAVEMENT JOINTING DETAILS
- GRADING AND LANDSCAPING PLAN - SHEET 1
- GRADING AND LANDSCAPING PLAN - SHEET 2
- STORM WATER POLLUTION PREVENTION PLAN NOTES
- STORM WATER POLLUTION PREVENTION PLAN DETAILS
- LIGHTING AND PAVEMENT MARKING PLAN - SHEET 1
- LIGHTING AND PAVEMENT MARKING PLAN - SHEET 2
- AIRFIELD SIGNAGE PLAN
- ELECTRICAL DETAILS - SHEET 1
- ELECTRICAL DETAILS - SHEET 2
- VAULT MODIFICATION DETAIL

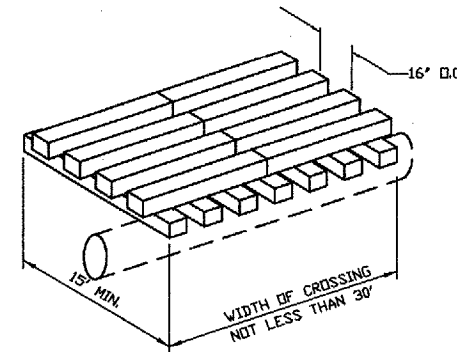
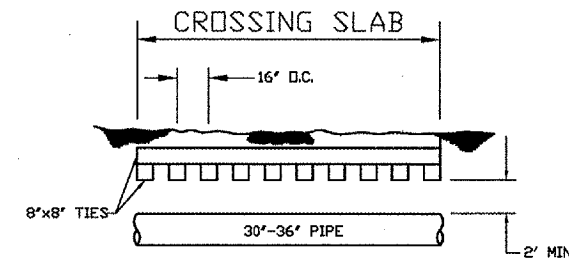
REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2**

**INDEX TO SHEETS/
 SUMMARY OF QUANTITIES**



NOTES:

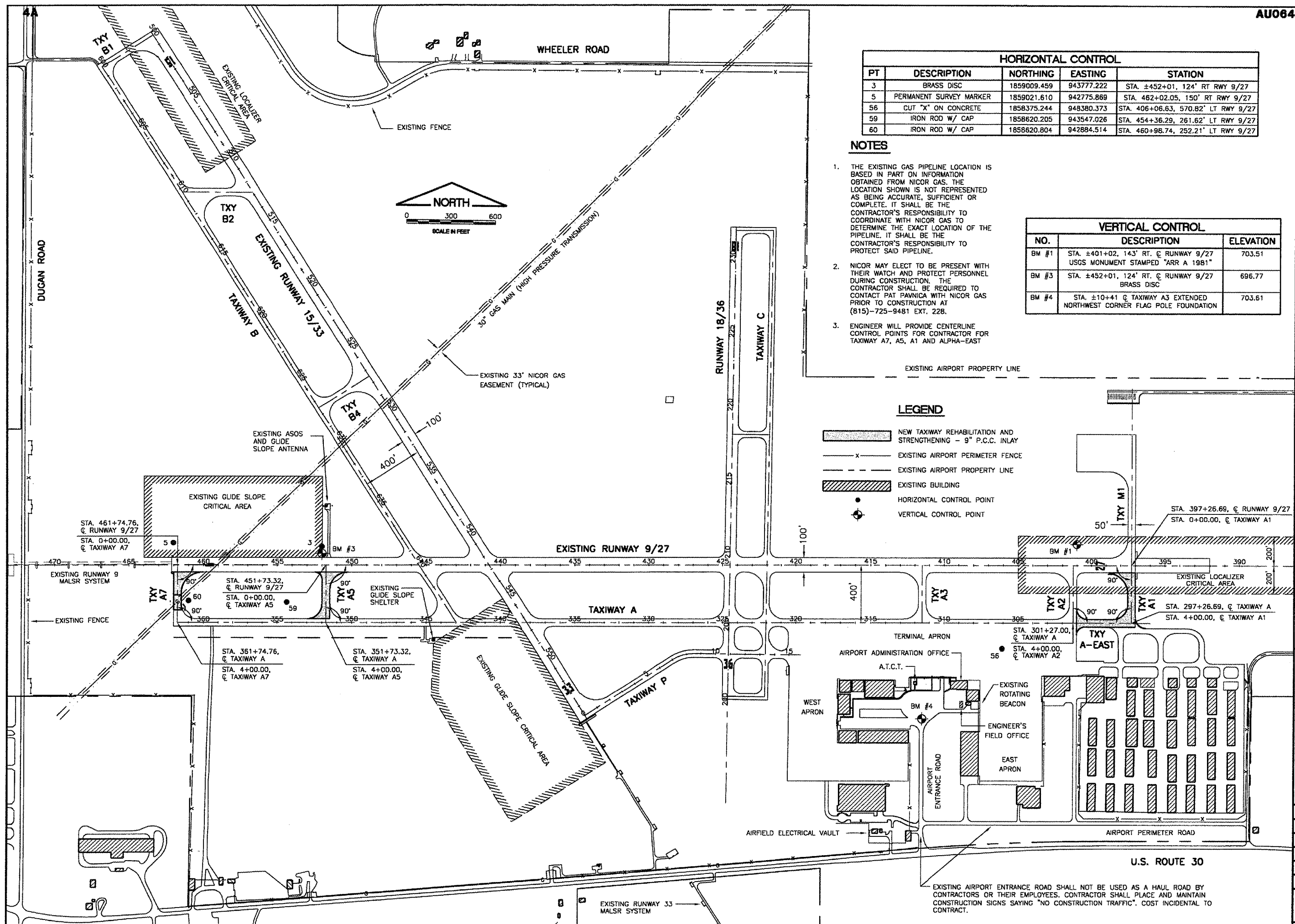
- LAY TIES IN TWO LAYERS. SET LOWER TIES AND BACKFILL TO TOP OF TIES; SET SECOND LAYER AND BACKFILL TO GRADE.
- PROVIDE TEMPORARY FENCE TO DEFINE CROSSING.
- MAINTAIN TEMPORARY FENCING DURING CONSTRUCTION.
- MAX LOADING: WHEEL LOAD: 15,000 LBS, TRACK LOAD: 80,000 LBS.
- ALTERNATIVE METHODS OF TEMPORARY GAS PIPELINE CROSSING MUST BE SUBMITTED FOR APPROVAL BY NICOR GAS.

**DETAIL OF TIE SLAB FOR HEAVY EQUIPMENT
 CROSSING PIPELINES**

NO SCALE
 (DETAIL PROVIDED BY NICOR GAS)

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DESIGN BY:	AAO
DRAWN BY:	AAO
CHECKED BY:	
APPROVED BY:	
DATE:	10/31/07
JOB No:	07285-04
ILLINOIS PROJECT: ARR-3770 A.I.P. PROJECT: 3-17-0003-836	
FINAL SUBMITTAL	
SHEET 2 OF 23 SHEETS	



HORIZONTAL CONTROL

PT	DESCRIPTION	NORTHING	EASTING	STATION
3	BRASS DISC	1859009.459	943777.222	STA. ±452+01, 124' RT RWY 9/27
5	PERMANENT SURVEY MARKER	1859021.610	942775.869	STA. 462+02.05, 150' RT RWY 9/27
56	CUT "X" ON CONCRETE	1858375.244	948380.373	STA. 406+06.63, 570.82' LT RWY 9/27
59	IRON ROD W/ CAP	1858620.205	943547.026	STA. 454+36.29, 261.62' LT RWY 9/27
60	IRON ROD W/ CAP	1858620.804	942884.514	STA. 460+98.74, 252.21' LT RWY 9/27

NOTES

- THE EXISTING GAS PIPELINE LOCATION IS BASED IN PART ON INFORMATION OBTAINED FROM NICOR GAS. THE LOCATION SHOWN IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE WITH NICOR GAS TO DETERMINE THE EXACT LOCATION OF THE PIPELINE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT SAID PIPELINE.
- NICOR MAY ELECT TO BE PRESENT WITH THEIR WATCH AND PROTECT PERSONNEL DURING CONSTRUCTION. THE CONTRACTOR SHALL BE REQUIRED TO CONTACT PAT PAVNICA WITH NICOR GAS PRIOR TO CONSTRUCTION AT (815)-725-9481 EXT. 228.
- ENGINEER WILL PROVIDE CENTERLINE CONTROL POINTS FOR CONTRACTOR FOR TAXIWAY A7, A5, A1 AND ALPHA-EAST

VERTICAL CONTROL

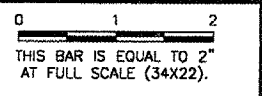
NO.	DESCRIPTION	ELEVATION
BM #1	STA. ±401+02, 143' RT. @ RUNWAY 9/27 USGS MONUMENT STAMPED "ARR A 1981"	703.51
BM #3	STA. ±452+01, 124' RT. @ RUNWAY 9/27 BRASS DISC	696.77
BM #4	STA. ±10+41 @ TAXIWAY A3 EXTENDED NORTHWEST CORNER FLAG POLE FOUNDATION	703.61

LEGEND

- NEW TAXIWAY REHABILITATION AND STRENGTHENING - 9" P.C.C. INLAY
- EXISTING AIRPORT PERIMETER FENCE
- EXISTING AIRPORT PROPERTY LINE
- EXISTING BUILDING
- HORIZONTAL CONTROL POINT
- VERTICAL CONTROL POINT

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NUMBER	BY	DATE



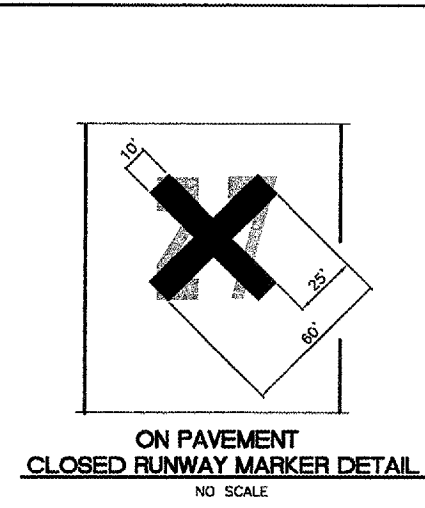
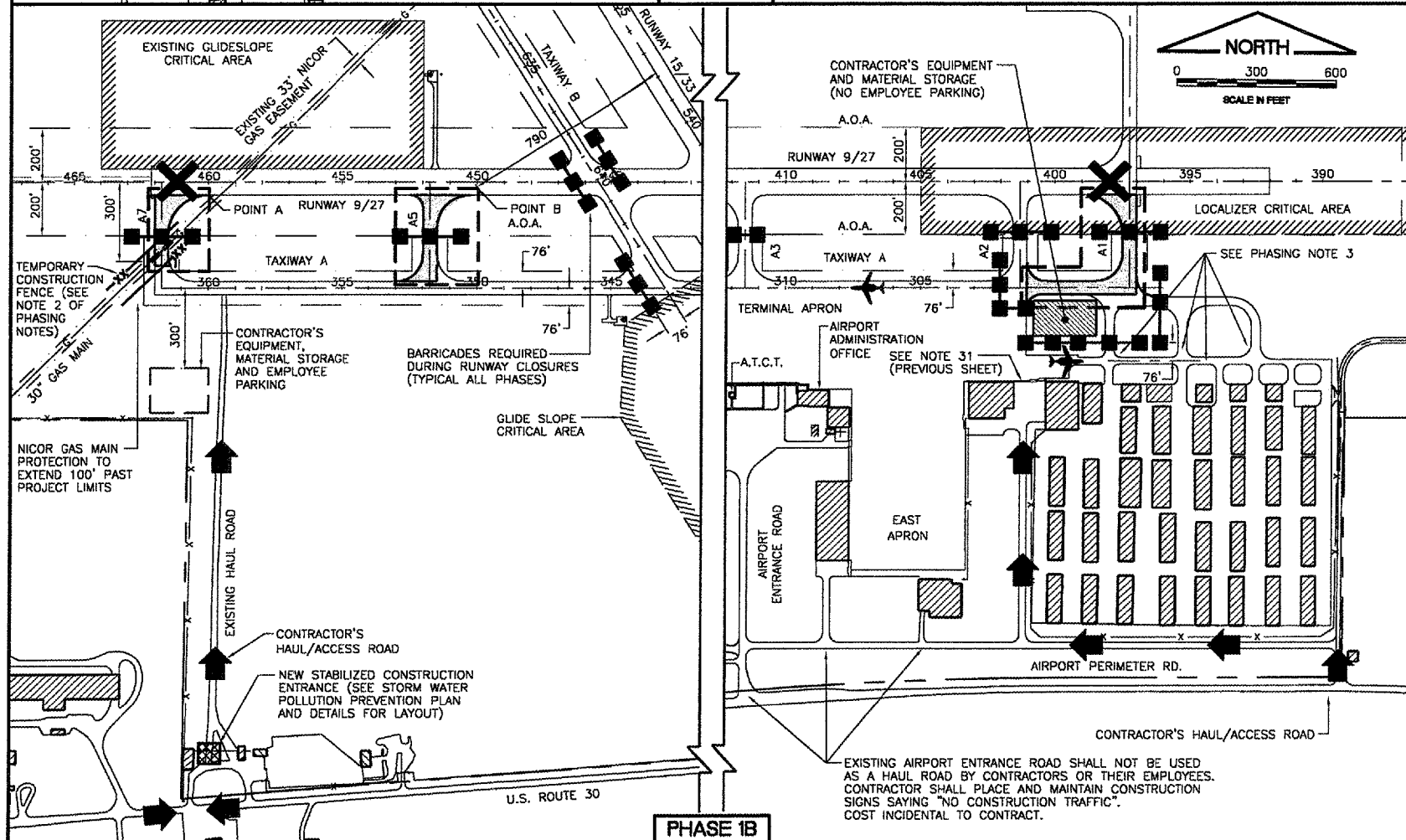
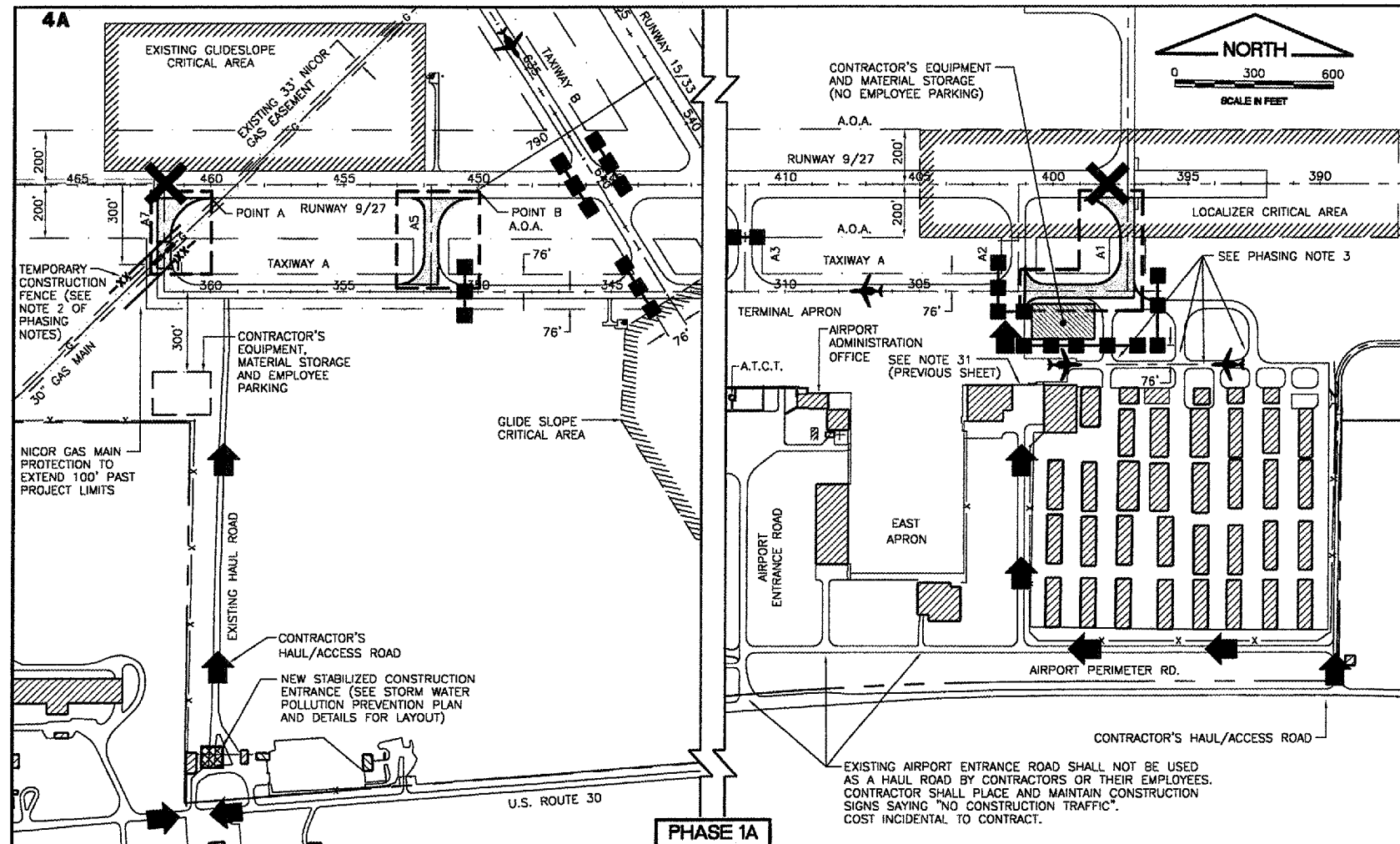
**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2
 SITE PLAN/PROJECT CONTROL PLAN**

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DESIGN BY:	AAO
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	
DATE:	10/31/07
JOB No:	07285-04
ILLINOIS PROJECT:	ARR-3770
A.I.P. PROJECT:	3-17-0003-B36
FINAL SUBMITTAL	
SHEET 3 OF 23 SHEETS	

EXISTING AIRPORT ENTRANCE ROAD SHALL NOT BE USED AS A HAUL ROAD BY CONTRACTORS OR THEIR EMPLOYEES. CONTRACTOR SHALL PLACE AND MAINTAIN CONSTRUCTION SIGNS SAYING "NO CONSTRUCTION TRAFFIC". COST INCIDENTAL TO CONTRACT.



DESIGN AIRCRAFT APPROACH CATEGORY: D
DESIGN AIRPORT GROUP: III

GROUND CONTROL FREQUENCY: 121.70
AIR CONTROL FREQUENCY: 120.60
MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 20'
CLOSEST CONSTRUCTION POINT TO RUNWAY 9/27 - POINT A
LATITUDE: 41°46'12.09" N (NAD 83)
LONGITUDE: 88°29'04.63" W
EXISTING ELEVATION: XXX.XX
CLOSEST CONSTRUCTION POINT TO RUNWAY 15/33 - POINT B
LATITUDE: 41°48'12.49" N (NAD 83)
LONGITUDE: 88°28'51.40" W
EXISTING ELEVATION: XXX.XX

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS AND SUBCONTRACTORS.

WORK AREA	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/ RESTRICTIONS
PHASE 1A	NO RESTRICTIONS	RUNWAY 9/27, TAXIWAY A BETWEEN TAXIWAY A1 AND A2, TAXIWAY A1, A5 AND A7 AND TAXIWAY A BETWEEN A5 AND A7 CLOSED
PHASE 1B	BEGIN WORK AFTER ALL P.C.C. PAVING IS COMPLETE AS WELL AS ALL GRADING WITHIN RUNWAY 9/27 A.O.A.	DAY CLOSURES OF RUNWAY 9/27, WHEN WORKING WITHIN RUNWAY 9/27 A.O.A. CLOSURE OF TAXIWAY A1, A5 AND A7 AS WELL AS TAXIWAY ALPHA BETWEEN TAXIWAY A1 AND A2 AND ALSO BETWEEN TAXIWAY A7 AND TAXIWAY BRAVO FOR DURATION OF WORK

SUGGESTED SEQUENCE OF CONSTRUCTION

- PHASE 1A**
- CLOSE RUNWAY 9/27.
 - PLACE X'S AND BARRICADES AS SHOWN OR AS DIRECTED BY THE ENGINEER. (SEE PHASING NOTE 4)
 - PLACE CONSTRUCTION FENCE AND TIE SLAB (PER DETAIL) FOR GAS MAIN PROTECTION.
 - REMOVE BITUMINOUS PAVEMENT AND UNDERDRAIN.
 - WIDEN FILLETS.
 - CONSTRUCT ALL P.C.C. PAVEMENT
 - CONSTRUCT SHOULDER GRADING (SEE PHASING NOTE 1).
 - CLEAN PAVEMENTS, REMOVE BARRICADES AND OPEN RUNWAY 9/27.

- PHASE 1B**
- COORDINATE DAY CLOSURES OF RUNWAY 9/27 AS NEEDED WITH RESIDENT ENGINEER.
 - PLACE X'S AND BARRICADES AS SHOWN OR AS DIRECTED BY THE ENGINEER. (SEE PHASING NOTE 4)
 - CONSTRUCT UNDERDRAIN, LIGHTING AND LANDSCAPING IMPROVEMENTS.
 - CLEAN PAVEMENTS, REMOVE BARRICADES AND OPEN TAXIWAYS.
(PH 1B WORK WITHIN RUNWAY AOA SHALL BE EXPEDITED)

- CLOSED RUNWAY MARKER DETAIL NOTES**
1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
 2. MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
 3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
 4. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
 5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
 6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

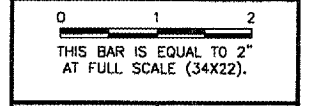
- LEGEND**
- NEW P.C.C. PAVEMENT
 - A.O.A.
 - AIR OPERATIONS AREA (A.O.A.)
ACTIVE RUNWAYS 200' CENTERLINE TO A.O.A.
ACTIVE TAXIWAYS 76' CENTERLINE TO A.O.A.
 - AIRCRAFT MOVEMENT AREAS
 - BARRICADES WITH FLASHING RED LIGHTS AND SIGNS "DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA" (SEE GENERAL NOTE 11 ON PREVIOUS SHEET)
 - TEMPORARY CONSTRUCTION FENCING FOR GAS MAIN PROTECTION
 - CONTRACTOR'S ACCESS/HAUL ROAD
 - LIMITS OF WORK

- PHASING NOTES**
1. PHASE 1A WORK SHALL BE EXPEDITED TO MINIMIZE RUNWAY 9/27 CLOSURE TIME. THE CONTRACTOR SHALL COMPLETE ALL PCC PAVING AS WELL AS SHOULDER GRADING WITHIN RUNWAY 9/27 A.O.A. FIRST SO RUNWAY 9/27 CAN BE OPENED.
 2. CONTRACTOR SHALL INSTALL TEMPORARY CONSTRUCTION FENCE PER PLAN AND PROFILE SHEET 10 AND PER NICOR REQUIREMENTS.
 3. CONTRACTOR SHALL MAINTAIN OPERATIONAL LIGHTING TO SKY HAVEN AREA AT ALL TIMES THROUGHOUT DURATION OF THE WORK. ALL TEMPORARY CABLING AND SPLICING SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
 4. IN ADDITION TO BARRICADES SHOWN, BARRICADES SHALL BE PLACED ACROSS RUNWAY 9/27 AT THE AOA LINE EAST AND WEST OF TAXIWAY CHARLIE ANYTIME RUNWAY 9/27 IS CLOSED.

AU064
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UPDATE BY: Marc Katz
SURVEY BOOK #
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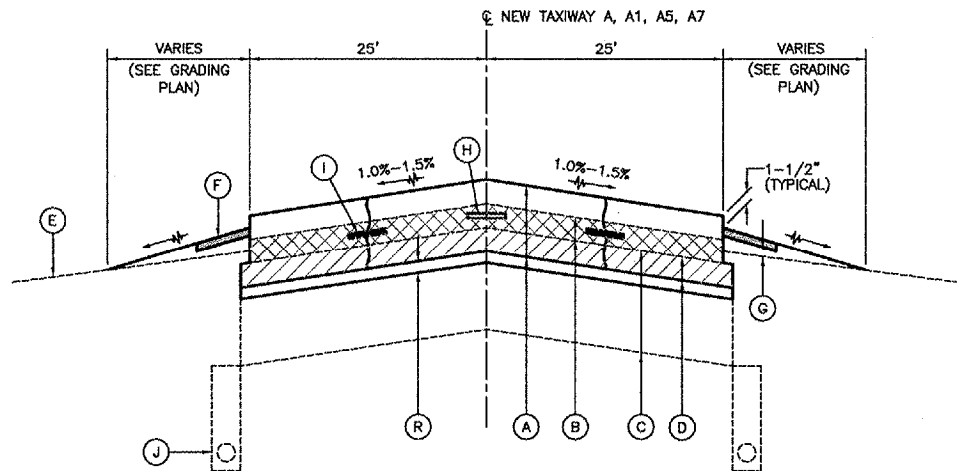
**AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
ENDS AND CONNECTING TAXIWAYS - PHASE 2**

**SEQUENCE OF CONSTRUCTION
PER AC 150/5370-2E (LATEST EDITION)**

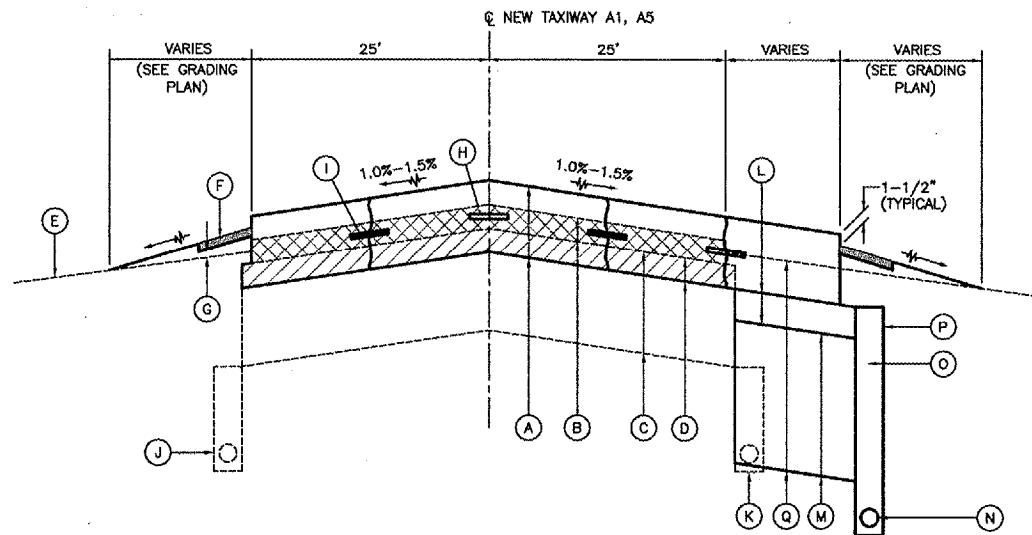
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CHECKED BY:	CAL
APPROVED BY:	
DATE:	10/31/07
JOB No:	07285-04
ILLINOIS PROJECT: ARR-3770 A.I.P. PROJECT: 3-17-0003-B36	
FINAL SUBMITTAL	
SHEET 5 OF 23 SHEETS	

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 UPDATE BY: mkatz
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 DATE: Mon 4/14/08 1:38pm
 XREF DWG: tbcint.dwg
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 tyend-base.dwg



**TAXIWAY A EAST, A1, A5 AND A7
 TYPICAL SECTION A-A**
 NOT TO SCALE

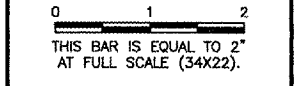


**TAXIWAY A/A1 AND A/A5 FILLET WIDENING
 TYPICAL SECTION B-B**
 NOT TO SCALE

- LEGEND**
- (A) NEW 9" P.C.C. PAVEMENT (501)
 - (B) EXISTING BITUMINOUS PAVEMENT TO BE REMOVED (VARIABLE DEPTH 4"-12")(SEE NOTE 5)
 - (C) EXISTING CRUSHED AGGREGATE BASE COURSE (VARIABLE DEPTH; 13" @ TAXIWAY A1 AND 18" @ TAXIWAY A5 AND A7)
 - (D) EXISTING CRUSHED AGGREGATE BASE COURSE TO BE REMOVED AND PAID FOR AS UNCLASSIFIED EXCAVATION (152) (VARIABLE DEPTH)(SEE NOTE 1)
 - (E) EXISTING GROUNDLINE
 - (F) NEW 6" WIDE SOD (904)(SEE NOTE 2)
 - (G) NEW TOPSOIL PLACEMENT (MINIMUM 4")(905)
 - (H) NEW DOWEL BAR (501)
 - (I) NEW TIE BAR (501)
 - (J) EXISTING UNDERDRAIN TO REMAIN
 - (K) EXISTING UNDERDRAIN TO BE REMOVED (TRENCH SHALL BE BACKFILLED WITH CRUSHED AGGREGATE BACKFILL (208). INCIDENTAL TO UNDERDRAIN REMOVAL)
 - (L) NEW 4" CRUSHED AGGREGATE BASE COURSE (209)
 - (M) NEW 18" CRUSHED AGGREGATE BASE COURSE PLACEMENT (SEE NOTE 3)
 - (N) NEW 6" UNDERDRAIN W/ SOCK (705)
 - (O) NEW POROUS BACKFILL (INCIDENTAL TO UNDERDRAIN)(705)
 - (P) NEW UNDERDRAIN TRENCH FABRIC ENVELOPE (INCIDENTAL TO UNDERDRAIN)(705)
 - (Q) NEW UNCLASSIFIED EXCAVATION (152)
 - (R) LEVELING COURSE IN AREAS OF VARIABLE DEPTH BITUMINOUS PAVEMENT REMOVAL (AR800081) (INCIDENTAL TO AR800081)

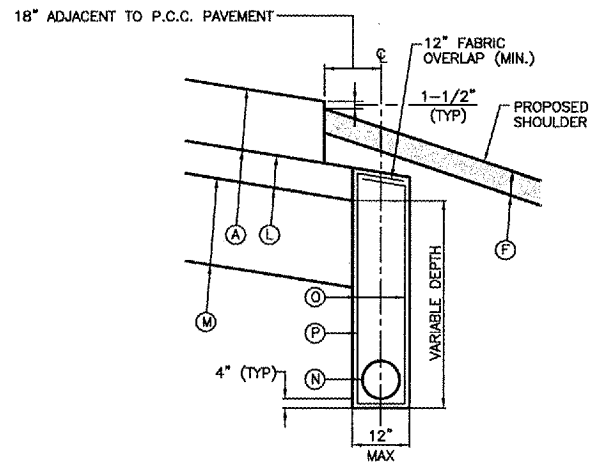
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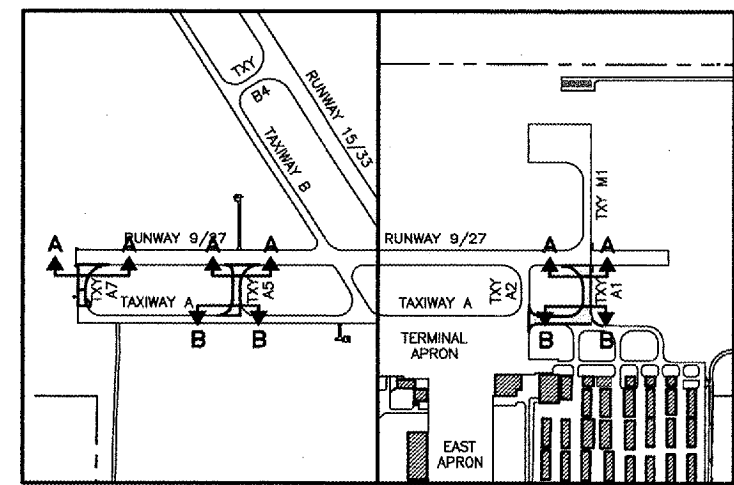
**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2**

TYPICAL SECTIONS



**UNDERDRAIN DETAIL
 EDGE OF FILLET
 WIDENING PAVEMENT AREAS**
 NOT TO SCALE

- NOTES**
- GRADING AND RE-COMPACTING OF EXISTING CRUSHED AGGREGATE BASE COURSE SHALL BE INCIDENTAL TO CRUSHED AGGREGATE BASE COURSE REMOVAL.
 - SHOULDER SHALL BE DISCED OR SCARIFIED PRIOR TO PLACEMENT OF TOPSOIL. MAXIMUM PAY WIDTH FOR SOD SHALL BE 6'-0".
 - CONTRACTOR SHALL REUSE EXISTING CRUSHED AGGREGATE BASE BEING REMOVED. PAYMENT SHALL BE PER 152 EMBANKMENT IN PLACE.
 - ALL PROPOSED UNDERDRAIN AT EDGE OF PAVEMENT SHALL BE INSTALLED AFTER THE 9" P.C.C. PAVEMENT IS CONSTRUCTED.
 - SEE EXISTING CONDITION SHEETS FOR AREAS TO BE PAID FOR AS AR800081 - REMOVE BITUMINOUS PAVEMENT (VAR. DEPTH) OR AR401900 - REMOVE BITUMINOUS PAVEMENT.



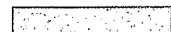
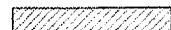

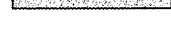


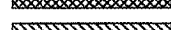
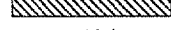
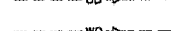

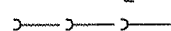
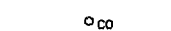







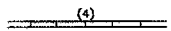
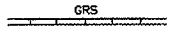
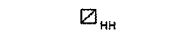


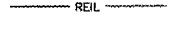
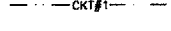
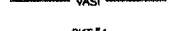

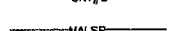



KEY MAP

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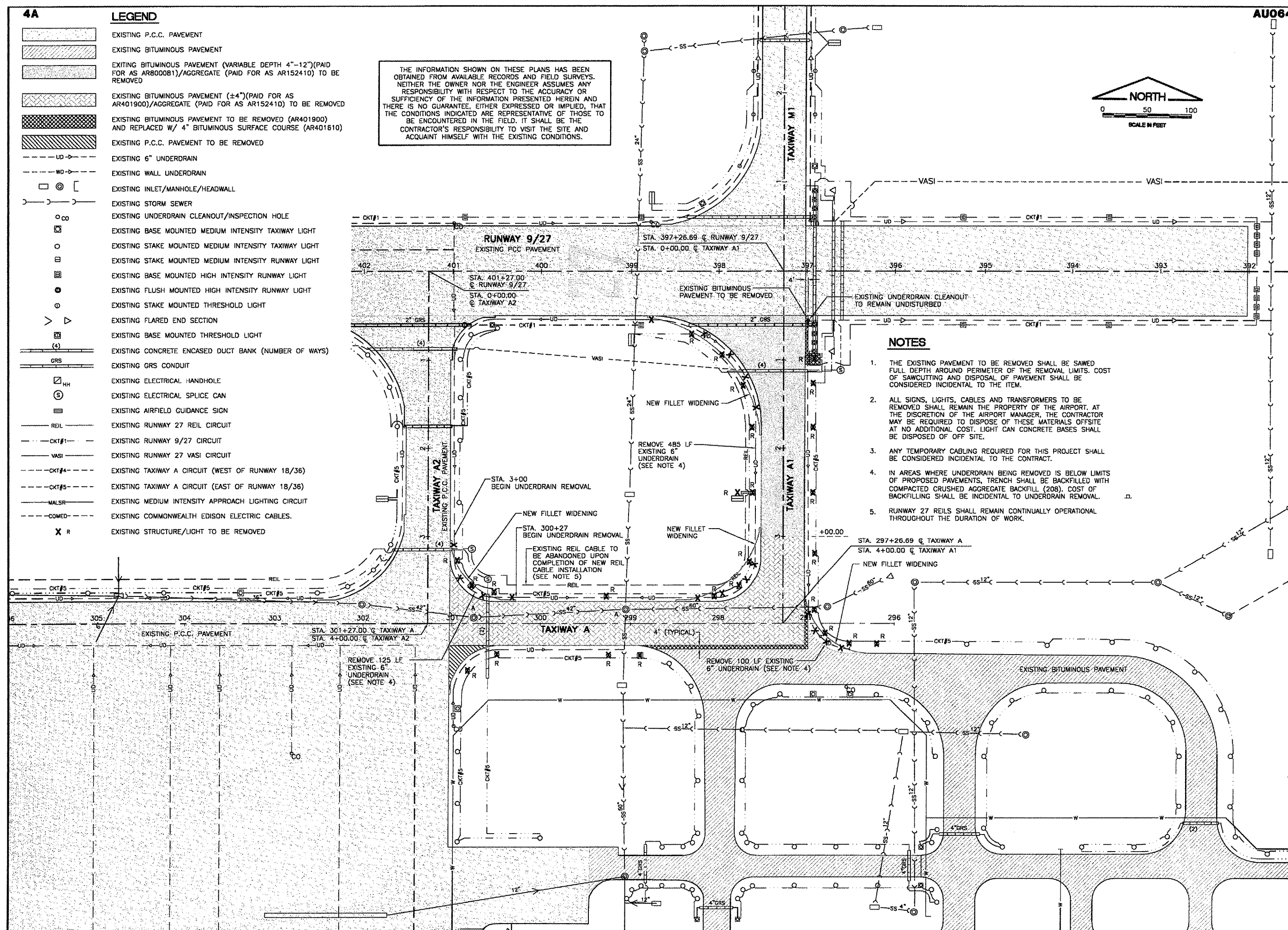
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FINAL SUBMITTAL	
SHEET 6 OF 23 SHEETS	

LEGEND

-  EXISTING P.C.C. PAVEMENT
-  EXISTING BITUMINOUS PAVEMENT
-  EXISTING BITUMINOUS PAVEMENT (VARIABLE DEPTH 4"-12") (PAID FOR AS AR8000B1)/AGGREGATE (PAID FOR AS AR152410) TO BE REMOVED
-  EXISTING BITUMINOUS PAVEMENT (±4") (PAID FOR AS AR401900)/AGGREGATE (PAID FOR AS AR152410) TO BE REMOVED
-  EXISTING BITUMINOUS PAVEMENT TO BE REMOVED (AR401900) AND REPLACED W/ 4" BITUMINOUS SURFACE COURSE (AR401610)
-  EXISTING P.C.C. PAVEMENT TO BE REMOVED
-  EXISTING 6" UNDERDRAIN
-  EXISTING WALL UNDERDRAIN
-  EXISTING INLET/MANHOLE/HEADWALL
-  EXISTING STORM SEWER
-  EXISTING UNDERDRAIN CLEANOUT/INSPECTION HOLE
-  EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
-  EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
-  EXISTING STAKE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT
-  EXISTING BASE MOUNTED HIGH INTENSITY RUNWAY LIGHT
-  EXISTING FLUSH MOUNTED HIGH INTENSITY RUNWAY LIGHT
-  EXISTING STAKE MOUNTED THRESHOLD LIGHT
-  EXISTING FLARED END SECTION
-  EXISTING BASE MOUNTED THRESHOLD LIGHT
-  EXISTING CONCRETE ENCASED DUCT BANK (NUMBER OF WAYS)
-  EXISTING GRS CONDUIT
-  EXISTING ELECTRICAL HANDHOLE
-  EXISTING ELECTRICAL SPLICE CAN
-  EXISTING AIRFIELD GUIDANCE SIGN
-  EXISTING RUNWAY 27 REIL CIRCUIT
-  EXISTING RUNWAY 9/27 CIRCUIT
-  EXISTING RUNWAY 27 VASI CIRCUIT
-  EXISTING TAXIWAY A CIRCUIT (WEST OF RUNWAY 18/36)
-  EXISTING TAXIWAY A CIRCUIT (EAST OF RUNWAY 18/36)
-  EXISTING MEDIUM INTENSITY APPROACH LIGHTING CIRCUIT
-  EXISTING COMMONWEALTH EDISON ELECTRIC CABLES.
-  EXISTING STRUCTURE/LIGHT TO BE REMOVED

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

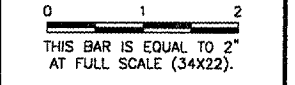


NOTES

1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
2. ALL SIGNS, LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT MANAGER, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE AT NO ADDITIONAL COST. LIGHT CAN CONCRETE BASES SHALL BE DISPOSED OF OFF SITE.
3. ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. IN AREAS WHERE UNDERDRAIN BEING REMOVED IS BELOW LIMITS OF PROPOSED PAVEMENTS, TRENCH SHALL BE BACKFILLED WITH COMPACTED CRUSHED AGGREGATE BACKFILL (208). COST OF BACKFILLING SHALL BE INCIDENTAL TO UNDERDRAIN REMOVAL.
5. RUNWAY 27 REILS SHALL REMAIN CONTINUALLY OPERATIONAL THROUGHOUT THE DURATION OF WORK.

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**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2**

**EXISTING CONDITIONS/PROPOSED
 REMOVALS - SHEET 1**

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A.I.P. PROJECT:	3-17-0003-B36
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SHEET 7 OF 23 SHEETS	

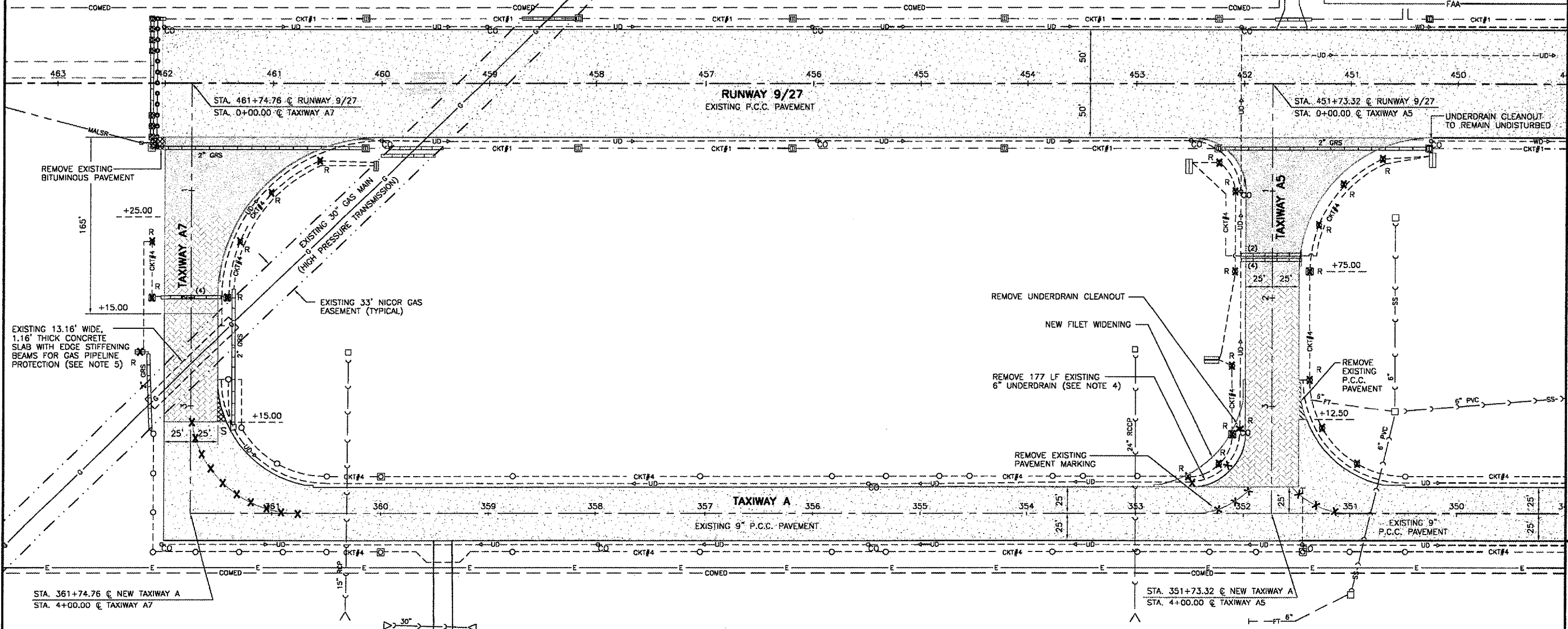
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LEGEND

- | | | | | | |
|--|--|--|--|--|---|
| | EXISTING P.C.C. PAVEMENT | | EXISTING UNDERDRAIN CLEANOUT/INSPECTION HOLE | | EXISTING ELECTRICAL HANDHOLE |
| | EXISTING BITUMINOUS PAVEMENT (VARIABLE DEPTH 4"-12") (PAID FOR AS AR800081)/AGGREGATE (PAID FOR AS AR152410) TO BE REMOVED | | EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT | | EXISTING ELECTRICAL SPLICE CAN |
| | EXISTING BITUMINOUS PAVEMENT (±4") (PAID FOR AS AR401900)/AGGREGATE (PAID FOR AS AR152410) TO BE REMOVED | | EXISTING STAKE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT | | EXISTING AIRFIELD GUIDANCE SIGN |
| | EXISTING BITUMINOUS PAVEMENT TO BE REMOVED (AR401900) AND REPLACED W/ 4" BITUMINOUS SURFACE COURSE (AR401610) | | EXISTING STAKE MOUNTED HIGH INTENSITY RUNWAY LIGHT | | EXISTING RUNWAY 27 REIL CIRCUIT |
| | EXISTING P.C.C. PAVEMENT TO BE REMOVED | | EXISTING FLUSH MOUNTED HIGH INTENSITY RUNWAY LIGHT | | EXISTING RUNWAY 27 VASI CIRCUIT |
| | EXISTING 6" UNDERDRAIN | | EXISTING STAKE MOUNTED THRESHOLD LIGHT | | EXISTING TAXIWAY A CIRCUIT (WEST OF RUNWAY 18/36) |
| | EXISTING WALL UNDERDRAIN | | EXISTING FLARED END SECTION | | EXISTING TAXIWAY A CIRCUIT (EAST OF RUNWAY 18/36) |
| | EXISTING INLET/MANHOLE/HEADWALL | | EXISTING BASE MOUNTED THRESHOLD LIGHT | | EXISTING MEDIUM INTENSITY APPROACH LIGHTING CIRCUIT |
| | EXISTING STORM SEWER | | EXISTING CONCRETE ENCASED DUCT BANK (NUMBER OF WAYS) | | EXISTING COMMONWEALTH EDISON ELECTRIC CABLES |
| | | | EXISTING GRS CONDUIT | | EXISTING STRUCTURE/LIGHT TO BE REMOVED |

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0 1 2
 THIS BAR IS EQUAL TO 2"
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**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2**
**EXISTING CONDITIONS/PROPOSED
 REMOVALS - SHEET 2**

NOTES

1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
2. ALL SIGNS, LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT MANAGER, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE AT NO ADDITIONAL COST. LIGHT CAN CONCRETE BASES SHALL BE DISPOSED OF OFF SITE.
3. ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. IN AREAS WHERE UNDERDRAIN BEING REMOVED IS BELOW LIMITS OF PROPOSED PAVEMENTS, TRENCH SHALL BE BACKFILLED WITH COMPACTED CRUSHED AGGREGATE BACKFILL (208). COST OF BACKFILLING SHALL BE INCIDENTAL TO UNDERDRAIN REMOVAL.
5. THE EXISTING GAS PIPELINE LOCATION IS BASED IN PART ON INFORMATION OBTAINED FROM NICOR GAS. THE LOCATION SHOWN IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE WITH NICOR GAS TO DETERMINE THE EXACT LOCATION OF THE PIPELINE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT SAID PIPELINE.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



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APPROVED BY:	
DATE:	10/31/07
JOB No:	07285-04
ILLINOIS PROJECT:	ARR-3770
A.I.P. PROJECT:	3-17-0003-B36
FINAL SUBMITTAL	
SHEET	8 OF 23 SHEETS

4A

LEGEND

- NEW BITUMINOUS PAVEMENT
- EXISTING P.C.C. PAVEMENT
- EXISTING BITUMINOUS PAVEMENT
- LIMITS OF NEW 9" P.C.C. PAVEMENT
- REMOVE BITUMINOUS PAVEMENT
- NEW BUTT JOINT CONSTRUCTION
- EXISTING 6" UNDERDRAIN
- EXISTING WALL UNDERDRAIN
- EXISTING STORM SEWER
- EXISTING MANHOLE/INLET/SLOPE BOX
- EXISTING MANHOLE TO BE ADJUSTED
- EXISTING UNDERDRAIN CLEANOUT
- NEW 6" C.P.P.U.P. UNDERDRAIN
- NEW TEMPORARY CONSTRUCTION FENCE

NOTES

1. CONTRACTOR SHALL FIELD VERIFY EXISTING UNDERDRAIN INVERTS BEFORE INSTALLING NEW UNDERDRAIN CONNECTIONS.
2. ALL UNDERDRAIN CONNECTIONS, CORING INTO STRUCTURES, TEES, BENDS, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UNDERDRAIN.
3. UNDERDRAIN SLOPES FOLLOW EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
4. INSTALL NEW ELECTRICAL DUCTS/CONDUITS TO BE CLEAR OF UNDERDRAIN, COST INCIDENTAL.
5. UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.
6. A MINIMUM OF 4" OF BITUMINOUS PAVEMENT SHALL BE PLACED ON 18" OF EMBANKMENT IN PLACE (152) AND 4" OF CRUSHED AGGREGATE BASE COURSE (209).

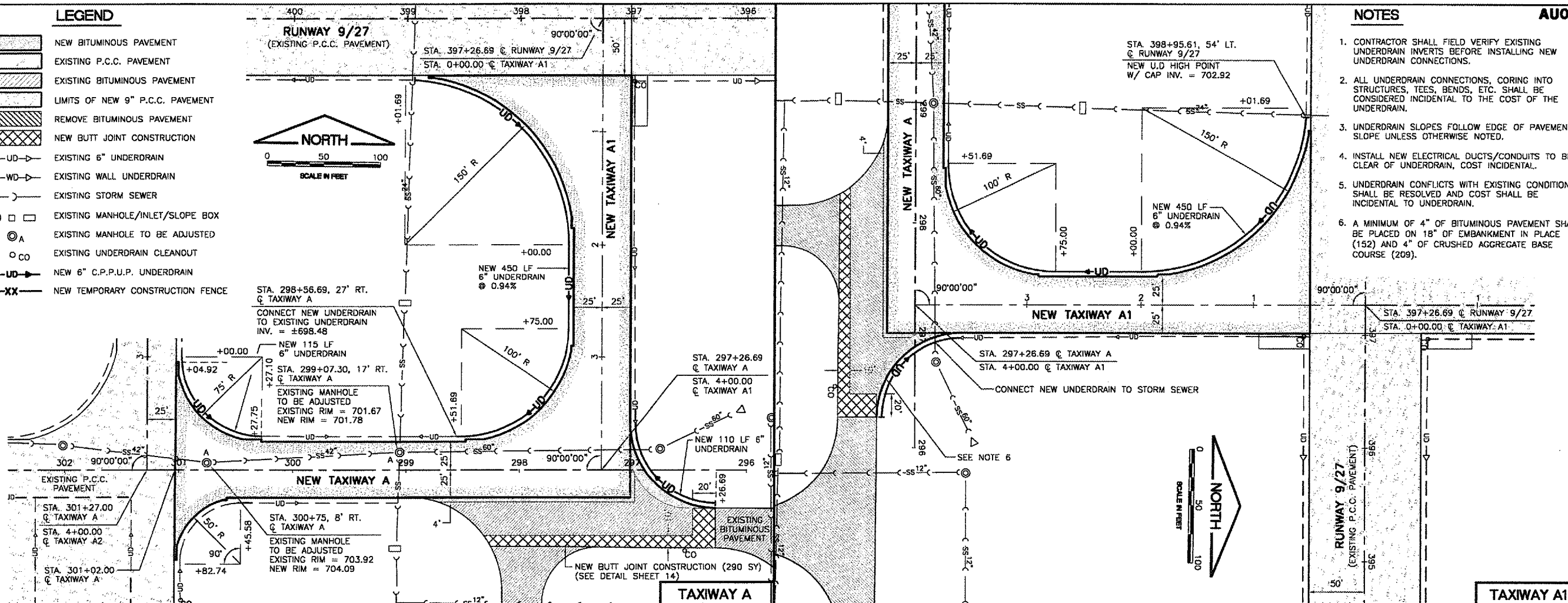
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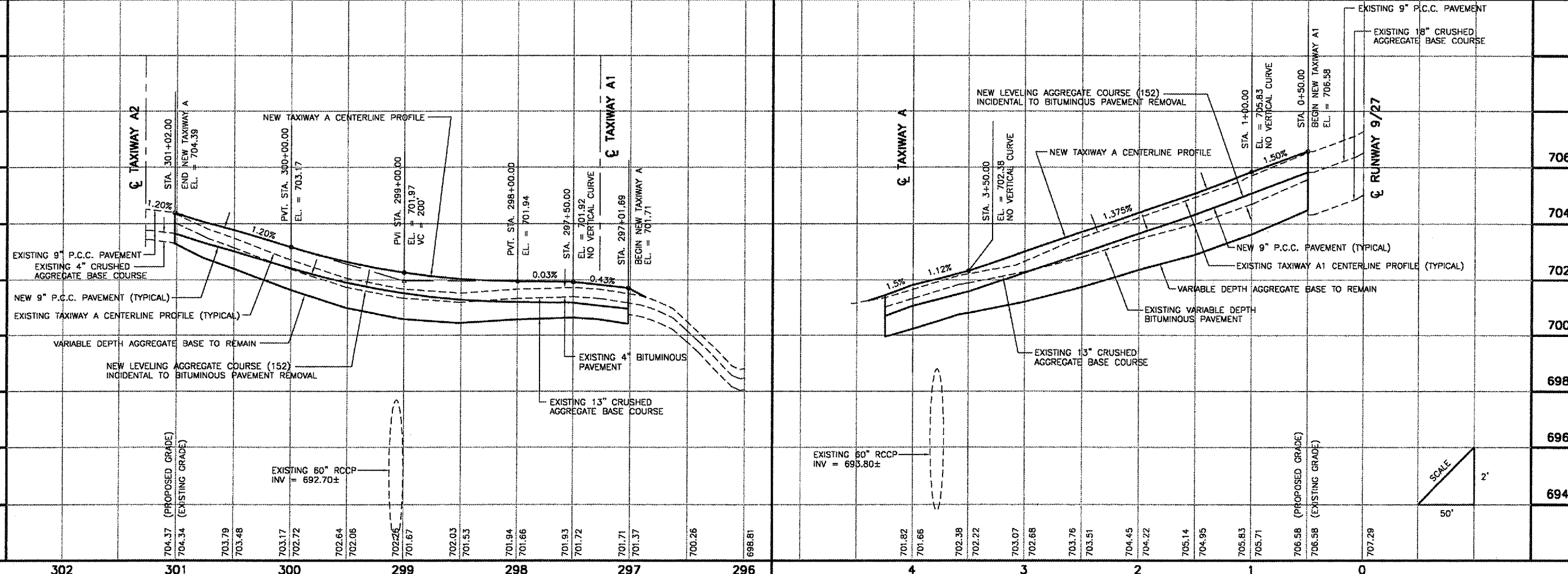
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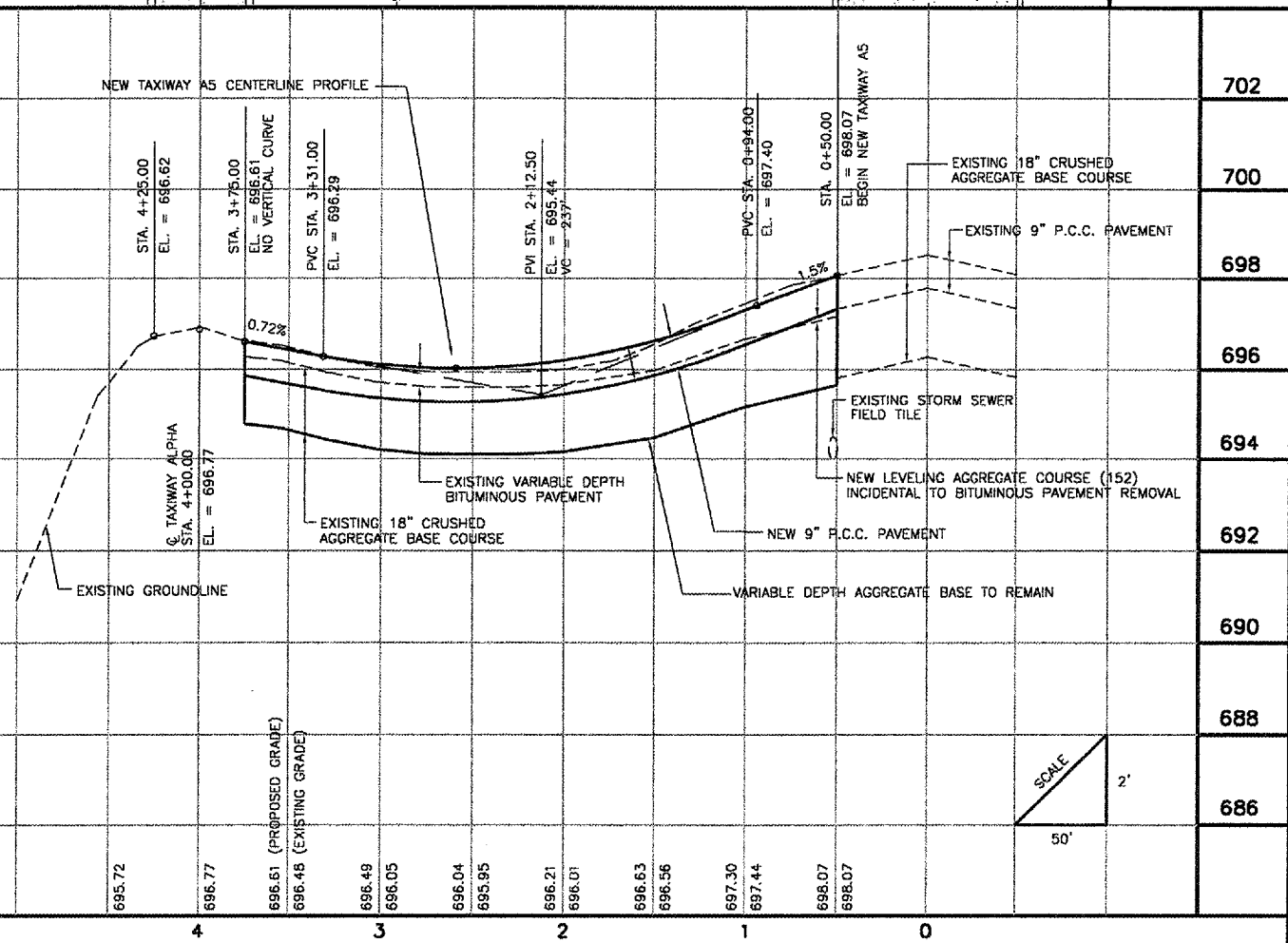
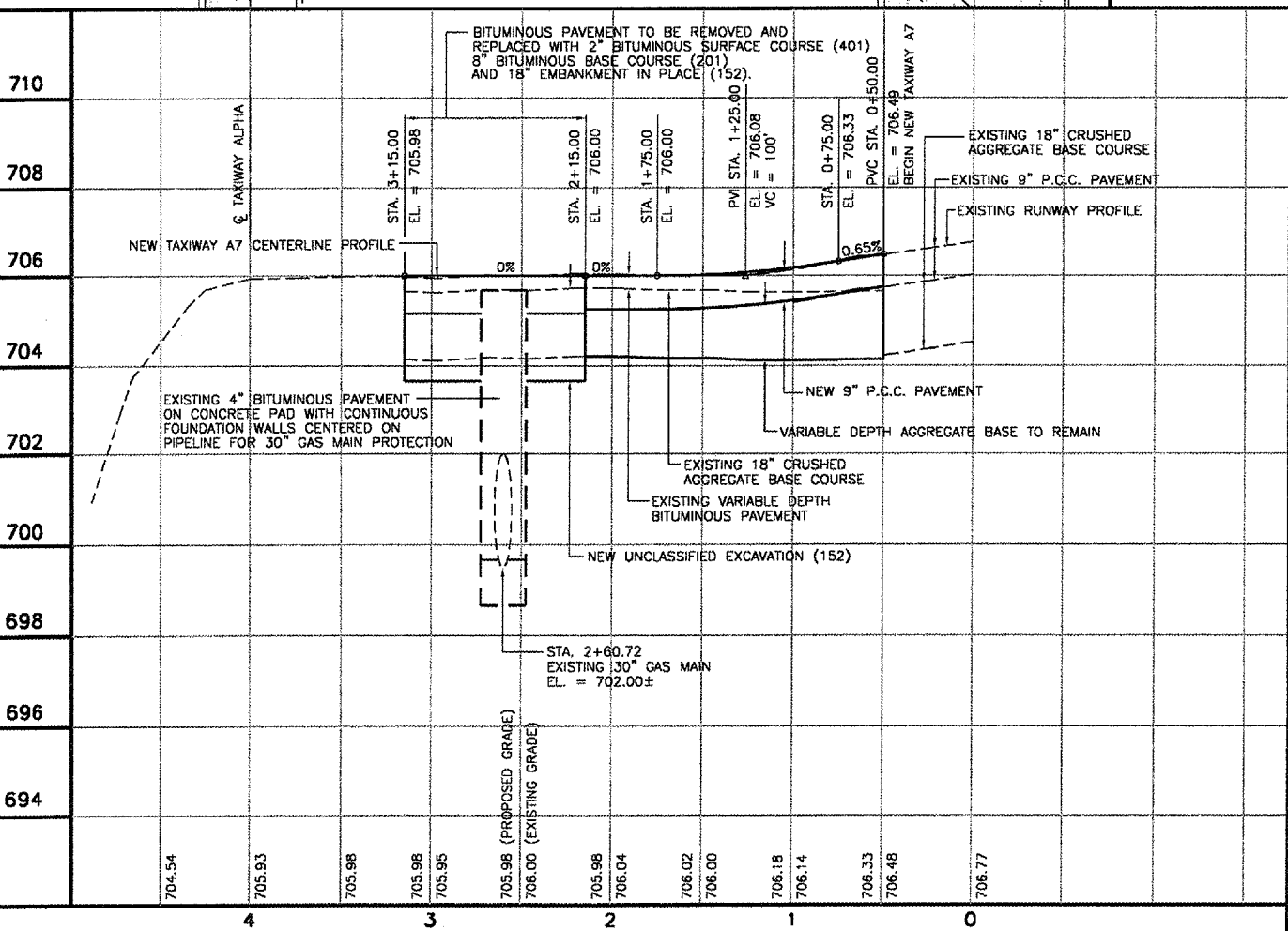
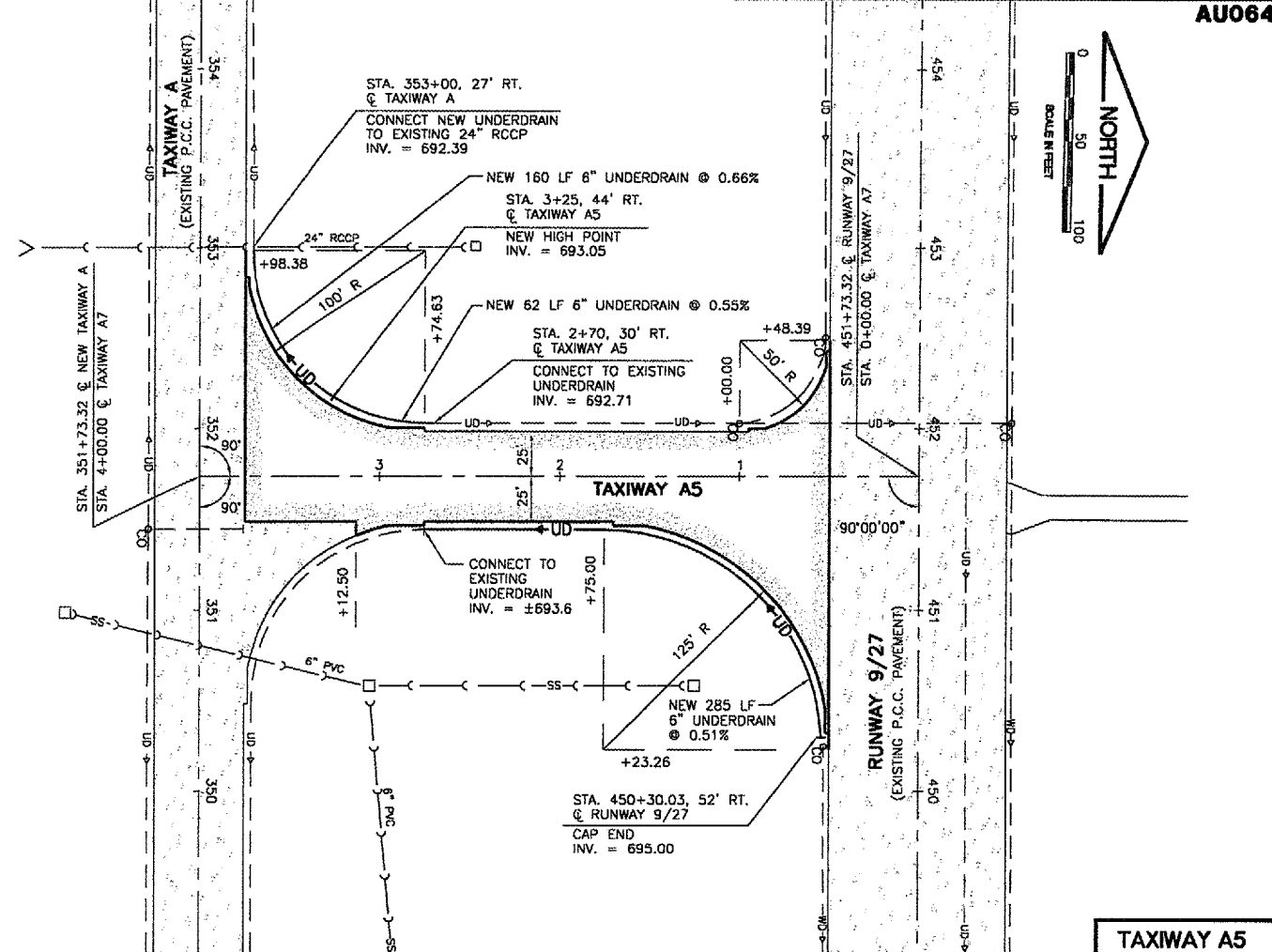
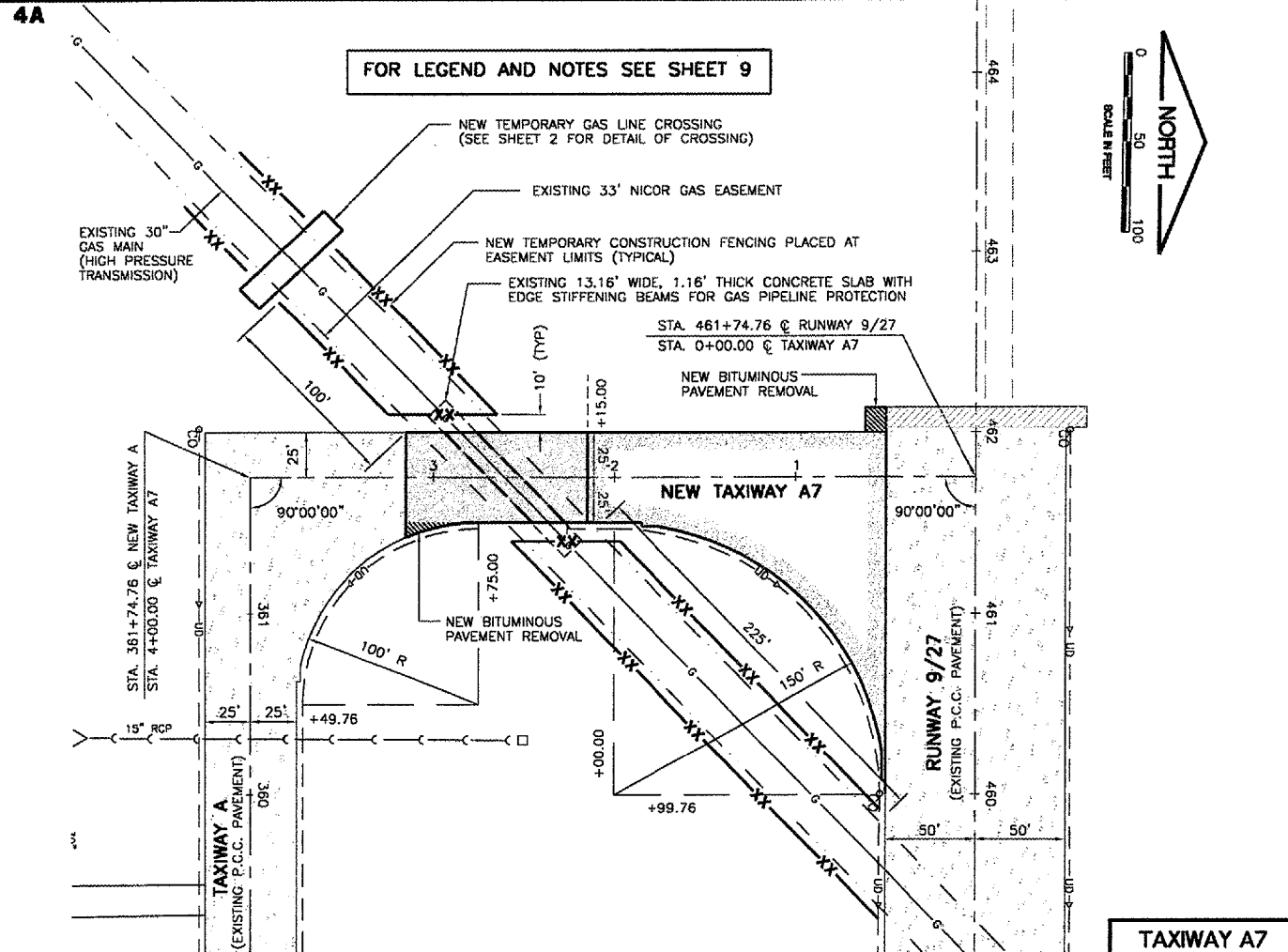


AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2
 PLAN AND PROFILE - SHEET 1



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 A.I.P. PROJECT: 3-17-0003-B36
FINAL SUBMITTAL
 SHEET 9 OF 23 SHEETS



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**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2**

**PLAN AND PROFILE -
 SHEET 2**

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A.I.P. PROJECT: 3-17-0003-836	
FINAL SUBMITTAL	

SHEET 10 OF 23 SHEETS

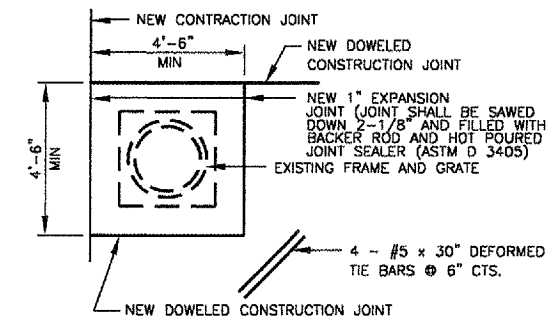
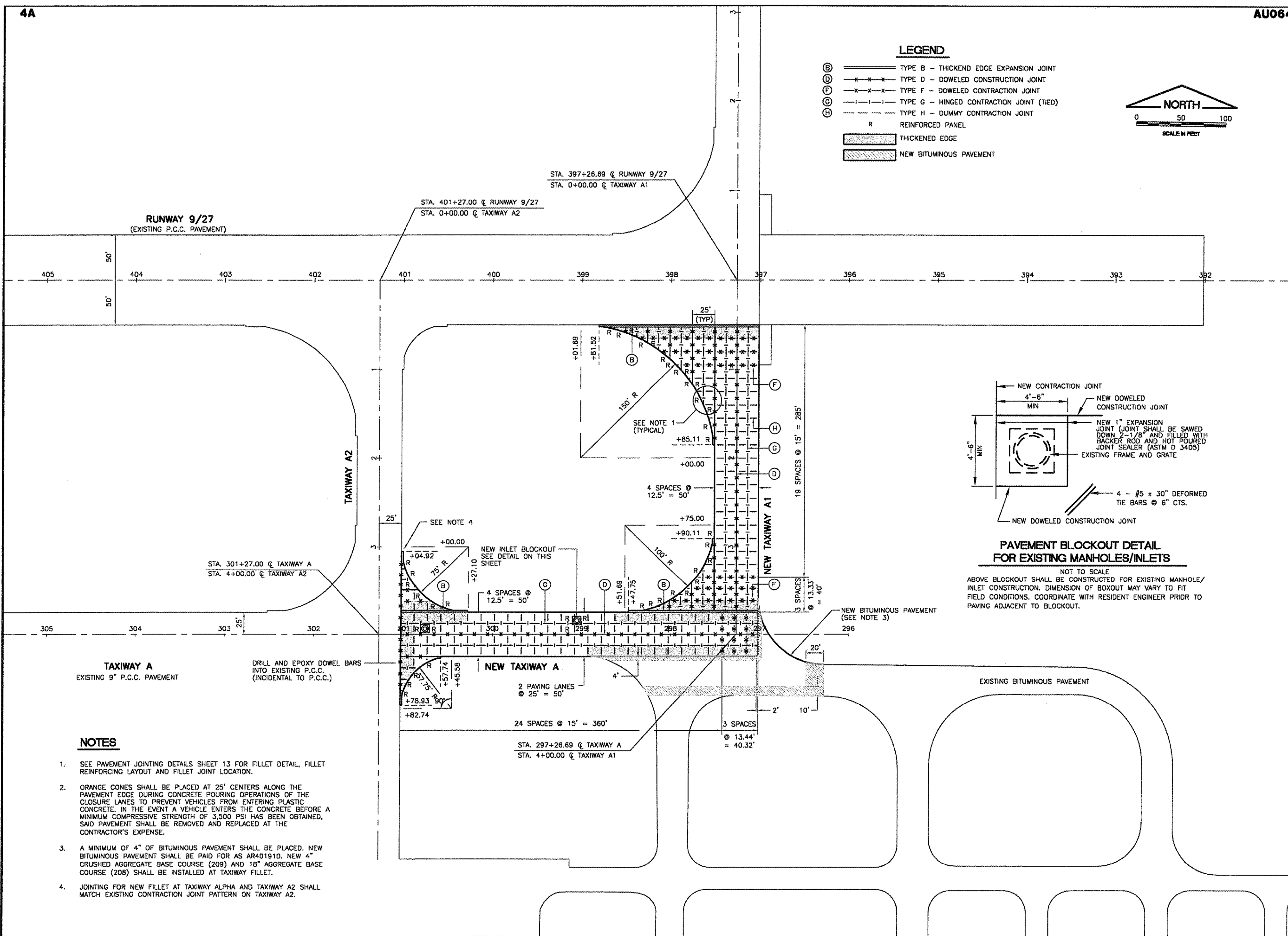
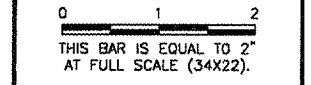
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- LEGEND**
- (B) TYPE B - THICKEND EDGE EXPANSION JOINT
 - (D) TYPE D - DOWELED CONSTRUCTION JOINT
 - (F) TYPE F - DOWELED CONSTRUCTION JOINT
 - (G) TYPE G - HINGED CONTRACTION JOINT (TIED)
 - (H) TYPE H - DUMMY CONTRACTION JOINT
 - R REINFORCED PANEL
 - [Hatched Box] THICKENED EDGE
 - [Dotted Box] NEW BITUMINOUS PAVEMENT



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PAVEMENT BLOCKOUT DETAIL FOR EXISTING MANHOLES/INLETS

NOT TO SCALE.

ABOVE BLOCKOUT SHALL BE CONSTRUCTED FOR EXISTING MANHOLE/INLET CONSTRUCTION. DIMENSION OF BOXOUT MAY VARY TO FIT FIELD CONDITIONS. COORDINATE WITH RESIDENT ENGINEER PRIOR TO PAVING ADJACENT TO BLOCKOUT.

NOTES

- SEE PAVEMENT JOINTING DETAILS SHEET 13 FOR FILLET DETAIL, FILLET REINFORCING LAYOUT AND FILLET JOINT LOCATION.
- ORANGE CONES SHALL BE PLACED AT 25' CENTERS ALONG THE PAVEMENT EDGE DURING CONCRETE POURING OPERATIONS OF THE CLOSURE LANES TO PREVENT VEHICLES FROM ENTERING PLASTIC CONCRETE. IN THE EVENT A VEHICLE ENTERS THE CONCRETE BEFORE A MINIMUM COMPRESSIVE STRENGTH OF 3,500 PSI HAS BEEN OBTAINED, SAID PAVEMENT SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.
- A MINIMUM OF 4" OF BITUMINOUS PAVEMENT SHALL BE PLACED. NEW BITUMINOUS PAVEMENT SHALL BE PAID FOR AS AR401910. NEW 4" CRUSHED AGGREGATE BASE COURSE (209) AND 18" AGGREGATE BASE COURSE (208) SHALL BE INSTALLED AT TAXIWAY FILLET.
- JOINTING FOR NEW FILLET AT TAXIWAY ALPHA AND TAXIWAY A2 SHALL MATCH EXISTING CONTRACTION JOINT PATTERN ON TAXIWAY A2.

**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2**

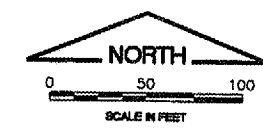
**PAVEMENT JOINTING PLAN -
 SHEET 1**

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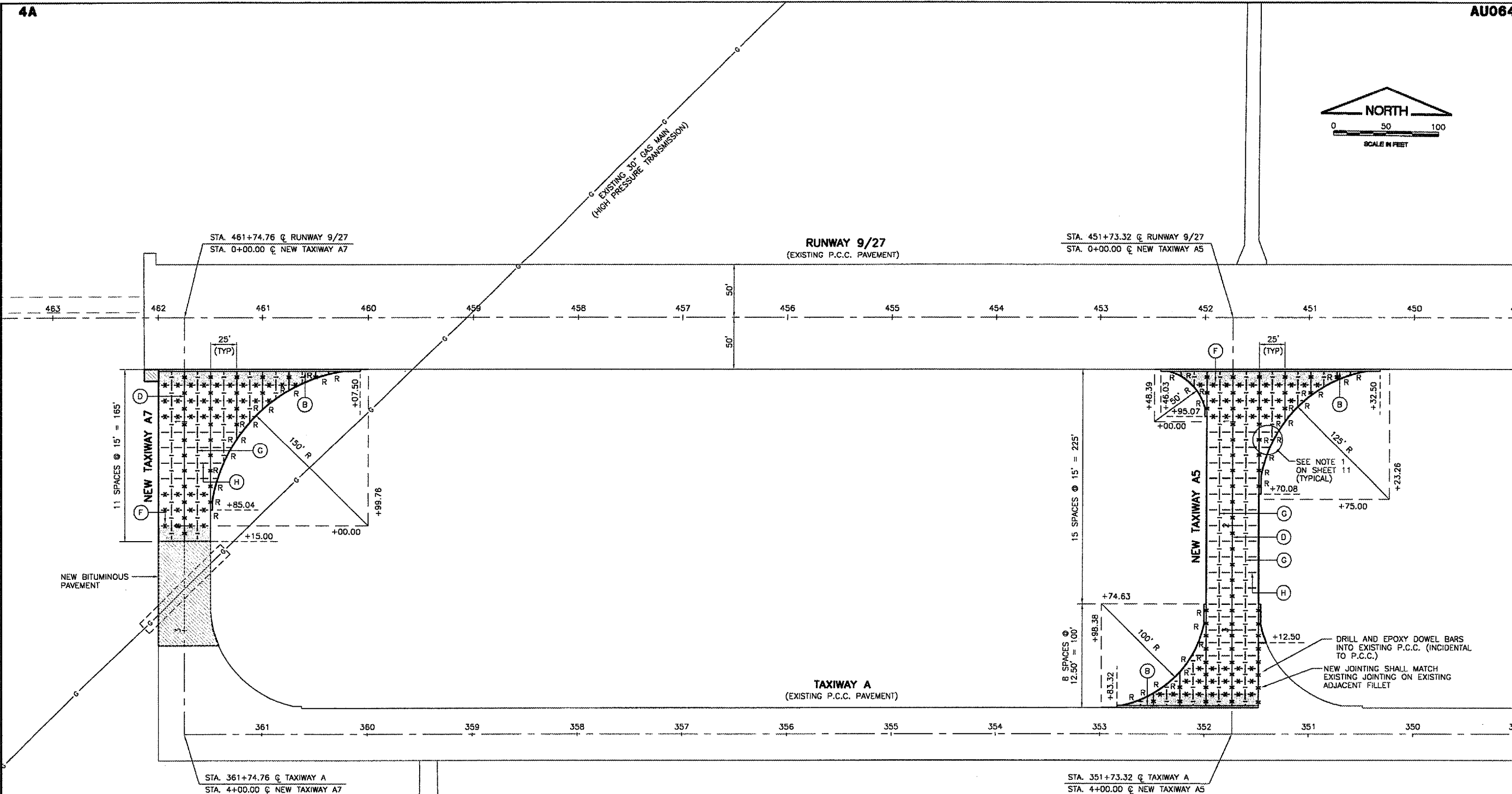
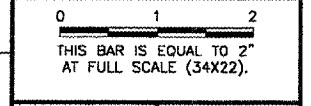
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SHEET 11 OF 23 SHEETS	

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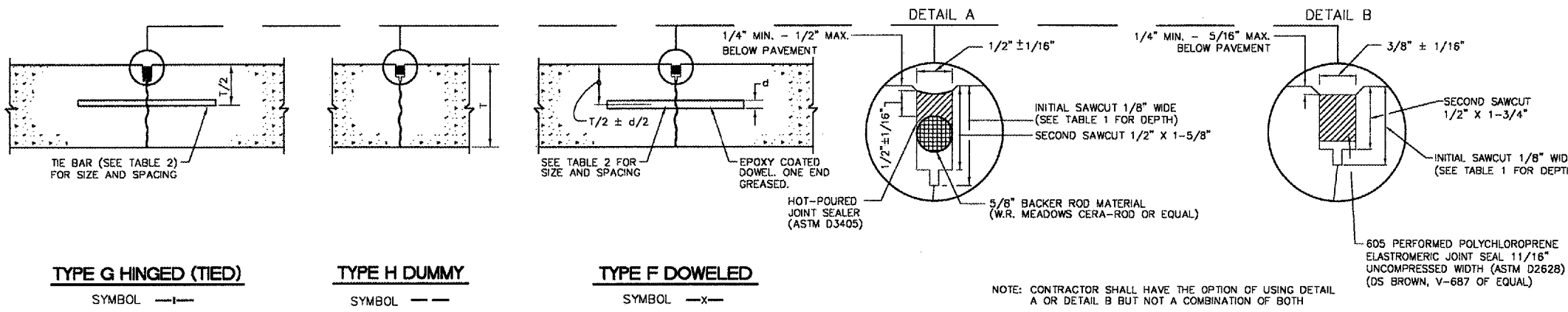
**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2**

**PAVEMENT JOINTING PLAN -
 SHEET 2**

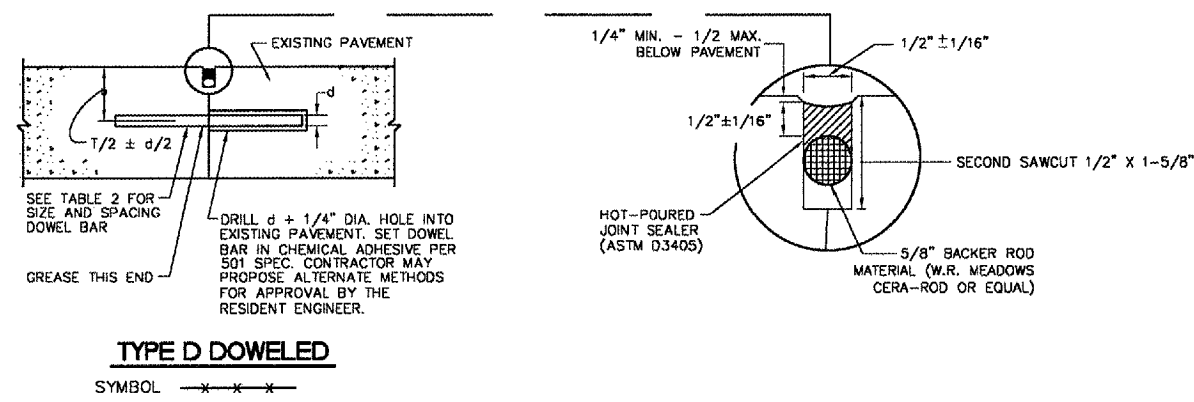
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- LEGEND**
- (B) ——— TYPE B - THICKEND EDGE EXPANSION JOINT
 - (D) * * * TYPE D - DOWELED CONSTRUCTION JOINT
 - (F) - - - TYPE F - DOWELED CONTRACTION JOINT
 - (G) - - - TYPE G - HINGED CONTRACTION JOINT (TIED)
 - (H) - - - TYPE H - DUMMY CONTRACTION JOINT
 - R REINFORCED PANEL
 - THICKENED EDGE
 - NEW BITUMINOUS PAVEMENT

DESIGN BY:	AAQ
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	
DATE:	10/31/07
JOB No:	07285-04
ILLINOIS PROJECT: ARR-3770 A.I.P. PROJECT: 3-17-0003-B36	
FINAL SUBMITTAL	
SHEET 12 OF 23 SHEETS	



CONTRACTION JOINTS



CONSTRUCTION JOINTS

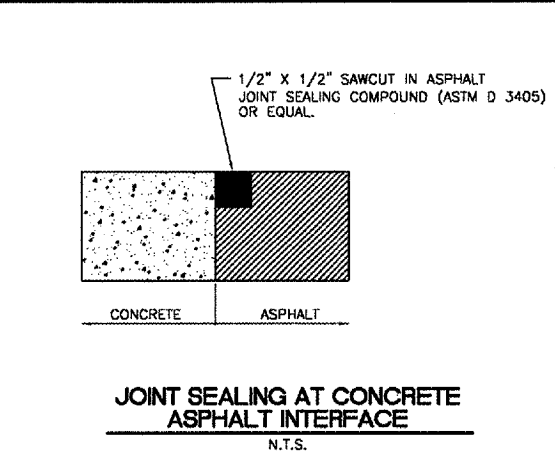
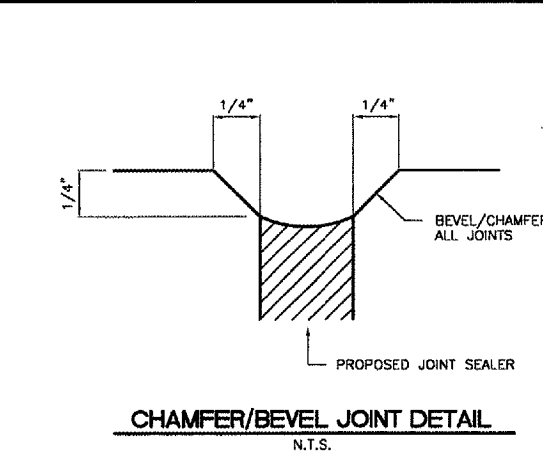


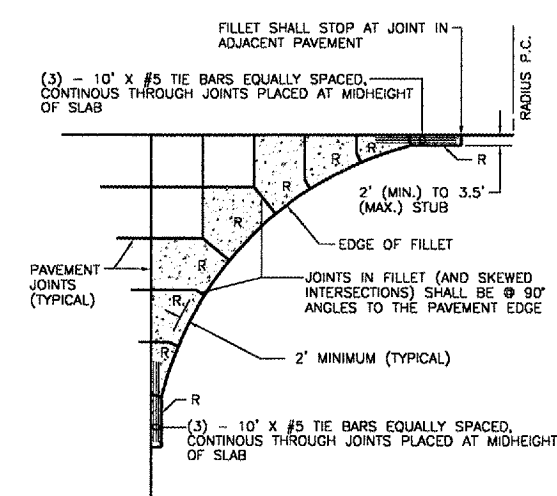
TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES $T = (T/4) \pm 1/4"$
6	1.50"
7	1.75"
8	2.00"
9	2.25"
10	2.50"
11	2.75"
12	3.00"

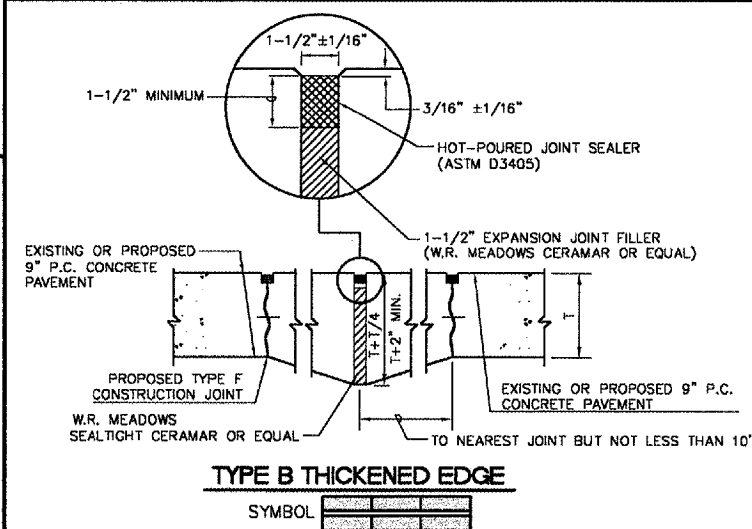
TABLE 2

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"

DIMENSION TABLES

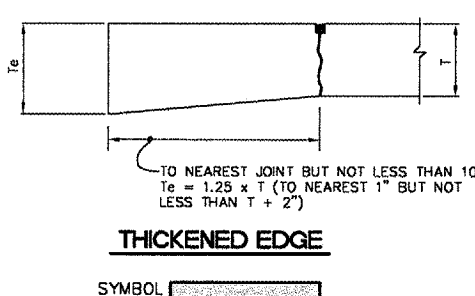
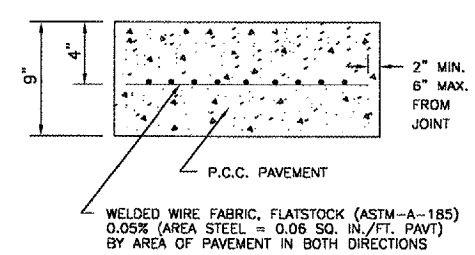


R DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH DEFORMED WIRE FABRIC AS SHOWN ON THIS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)



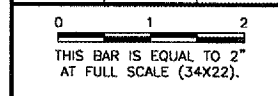
EXPANSION JOINTS

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY INSTALLED PER ARTICLE 420.05 JOINTS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAINED DURING CONSTRUCTION. TIE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
- CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS AT NO ADDITIONAL COST.



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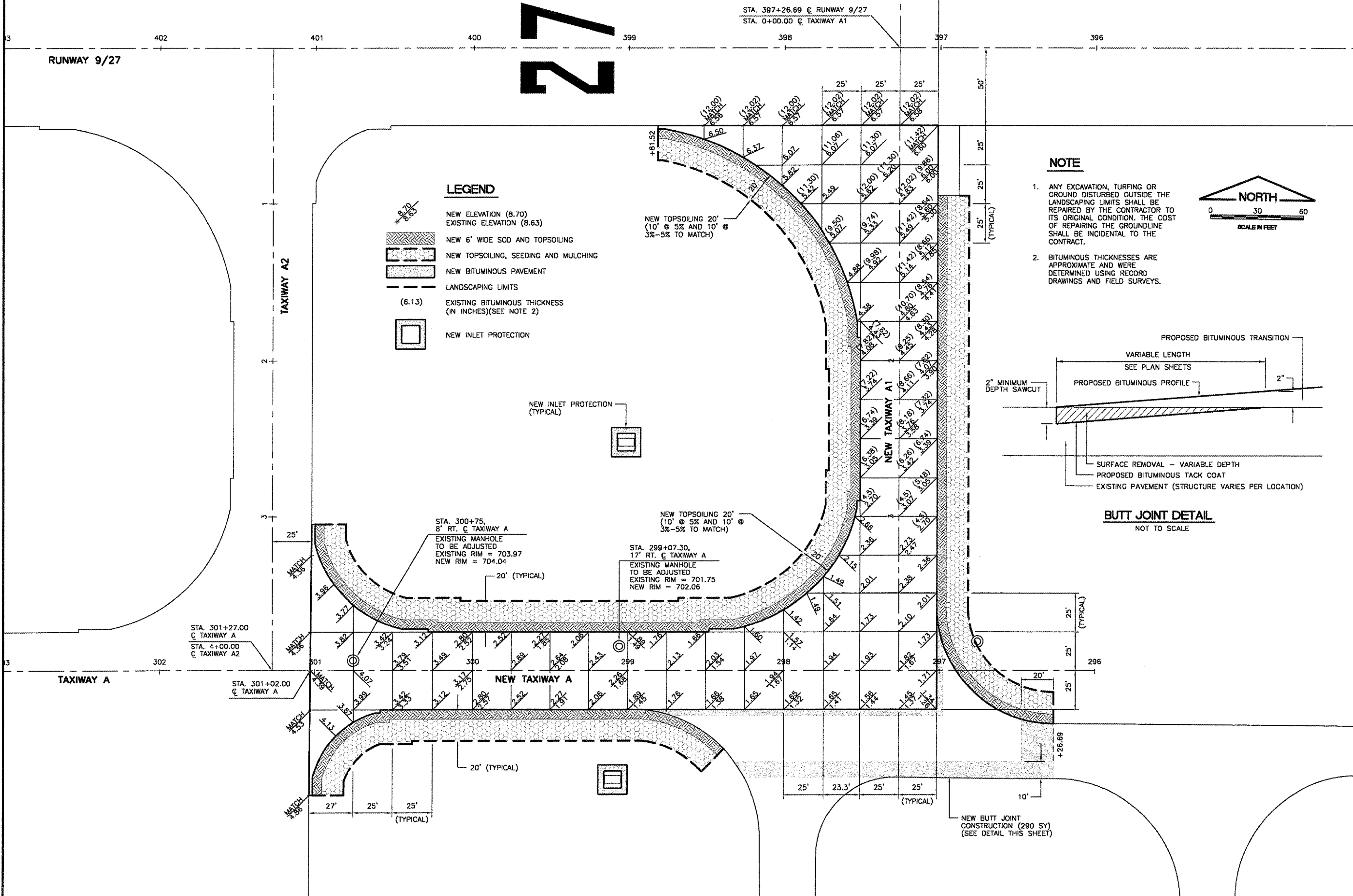


AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
REHABILITATE AND STRENGTHEN TAXIWAY ALPHA ENDS AND CONNECTING TAXIWAYS - PHASE 2
PAVEMENT JOINTING DETAILS

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 DRAWN BY: JRO
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 APPROVED BY:
 DATE: 10/31/07
 JOB No: 07285-04
 ILLINOIS PROJECT: ARR-3770
 A.I.P. PROJECT: 3-17-0003-836
FINAL SUBMITTAL
 SHEET 13 OF 23 SHEETS

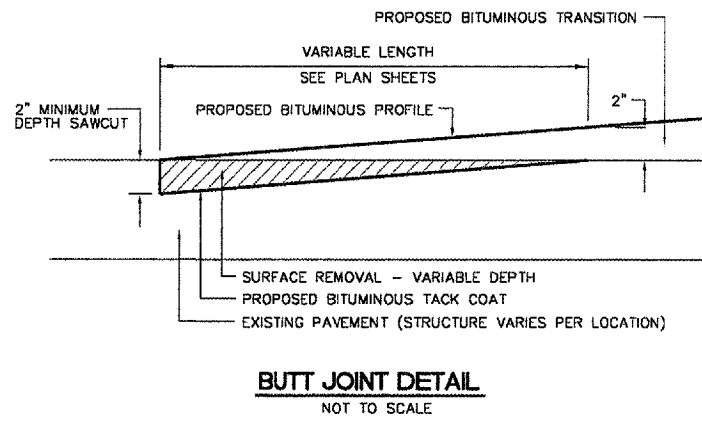


LEGEND

- NEW ELEVATION (8.70)
- EXISTING ELEVATION (8.63)
- NEW 6' WIDE SOD AND TOPSOILING
- NEW TOPSOILING, SEEDING AND MULCHING
- NEW BITUMINOUS PAVEMENT
- LANDSCAPING LIMITS
- (6.13) EXISTING BITUMINOUS THICKNESS (IN INCHES)(SEE NOTE 2)
- NEW INLET PROTECTION

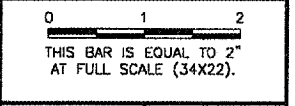
NOTE

1. ANY EXCAVATION, TURFING OR GROUND DISTURBED OUTSIDE THE LANDSCAPING LIMITS SHALL BE REPAIRED BY THE CONTRACTOR TO ITS ORIGINAL CONDITION. THE COST OF REPAIRING THE GROUNDLINE SHALL BE INCIDENTAL TO THE CONTRACT.
2. BITUMINOUS THICKNESSES ARE APPROXIMATE AND WERE DETERMINED USING RECORD DRAWINGS AND FIELD SURVEYS.



REVISIONS

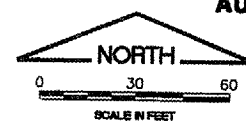
NUMBER	BY	DATE



AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
ENDS AND CONNECTING TAXIWAYS - PHASE 2
GRADING AND LANDSCAPING PLAN -
SHEET 1

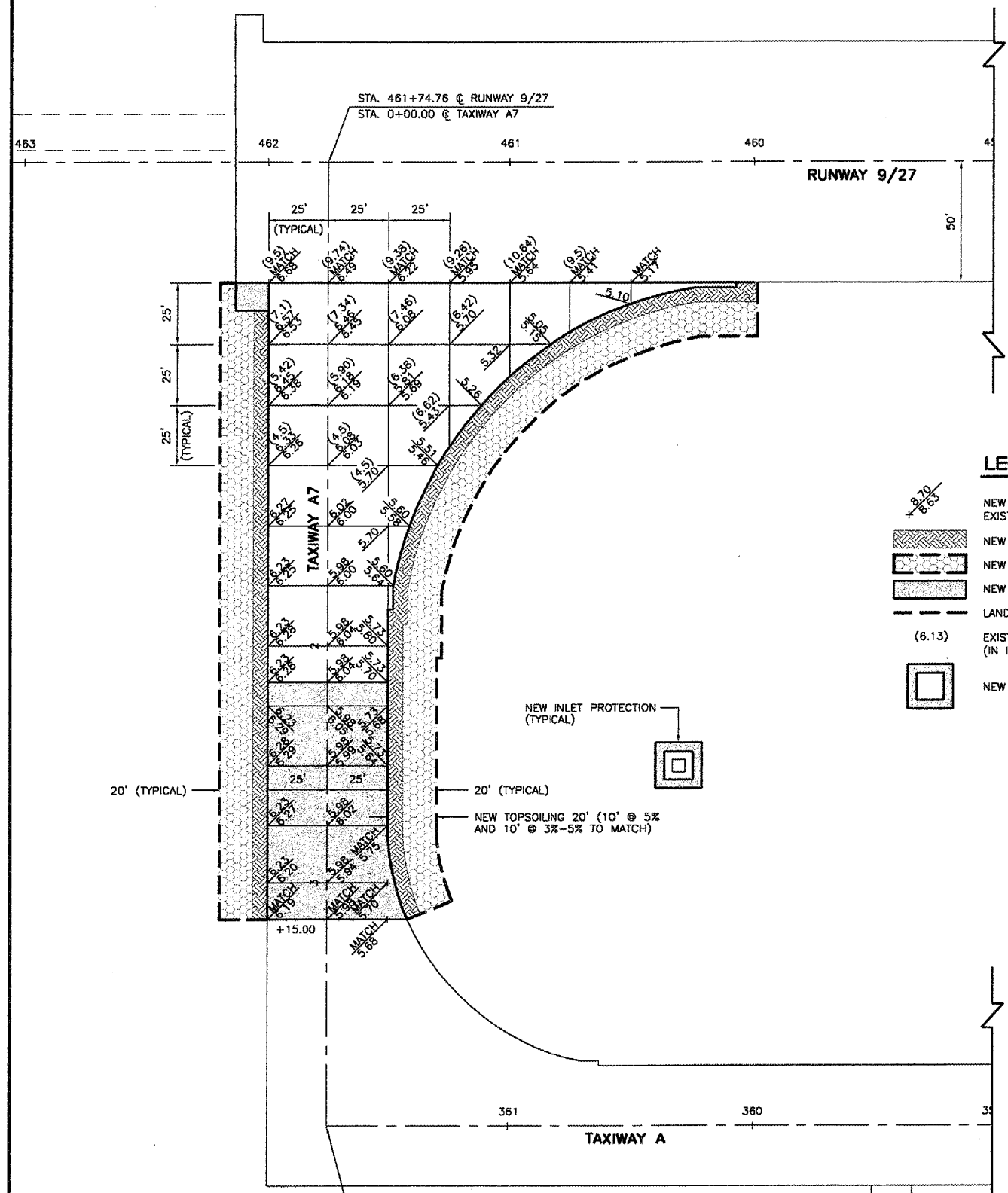
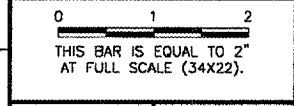
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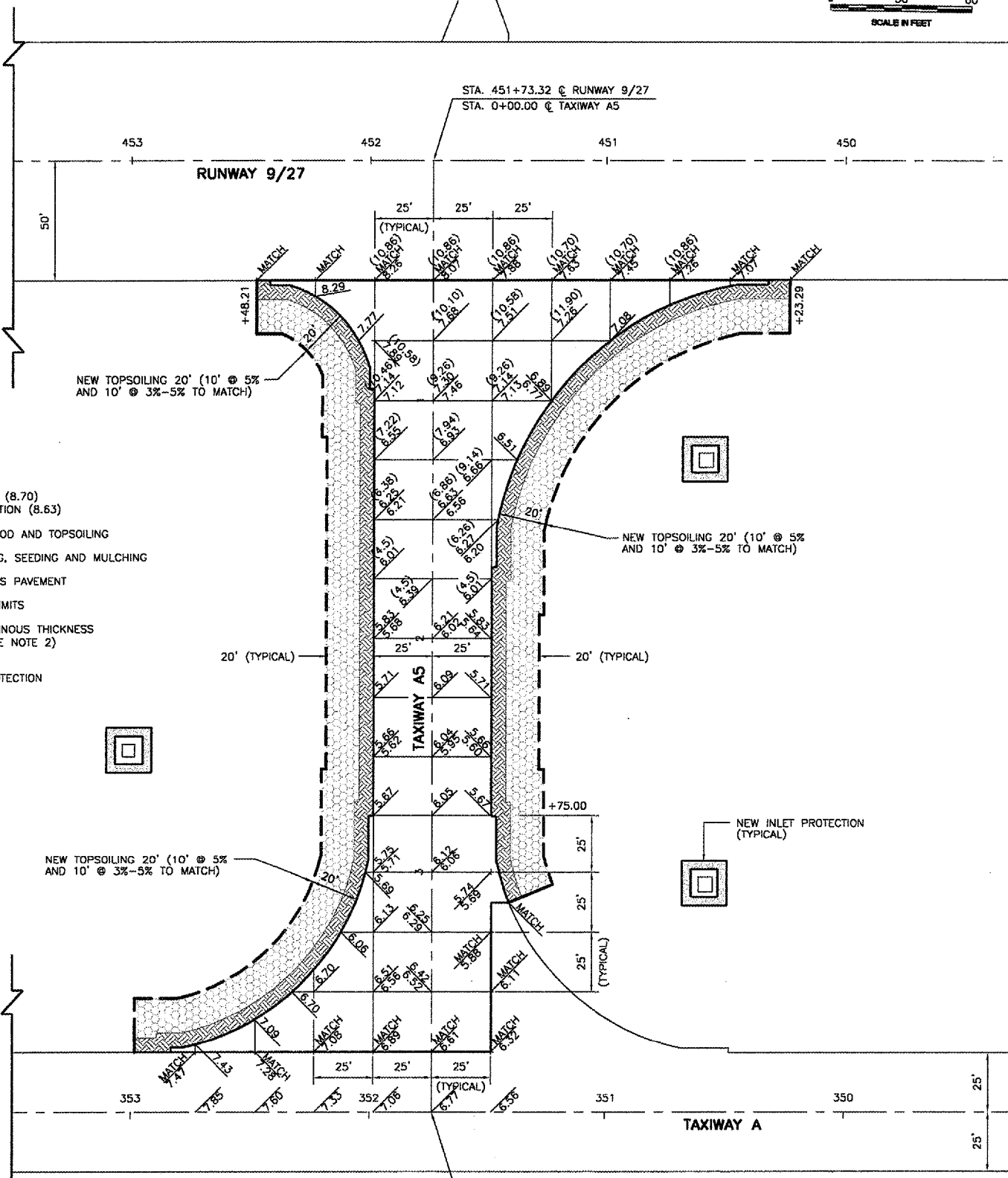
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LAYOUT: Layout1
UPDATE BY: mkatz
SURVEY BOOK #
DATE: Mon 2/4/08 2:58pm
XREF DWG: tbcint.dwg
tb.dwg
txyend-base.dwg

REVISIONS		
NUMBER	BY	DATE



LEGEND

- NEW ELEVATION (8.70)
EXISTING ELEVATION (8.63)
- NEW 6' WIDE SOD AND TOPSOILING
- NEW TOPSOILING, SEEDING AND MULCHING
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- LANDSCAPING LIMITS
- EXISTING BITUMINOUS THICKNESS (IN INCHES)(SEE NOTE 2)
- NEW INLET PROTECTION



STA. 361+74.76 @ NEW TAXIWAY A
STA. 4+00.00 @ TAXIWAY A7

STA. 351+73.32 @ NEW TAXIWAY A
STA. 4+00.00 @ TAXIWAY A5

NOTE

1. ANY EXCAVATION, TURFING OR GROUND DISTURBED OUTSIDE THE LANDSCAPING LIMITS SHALL BE REPAIRED BY THE CONTRACTOR TO ITS ORIGINAL CONDITION. THE COST OF REPAIRING THE GROUNDLINE SHALL BE INCIDENTAL TO THE CONTRACT.
2. BITUMINOUS THICKNESSES ARE APPROXIMATE AND WERE DETERMINED USING RECORD DRAWINGS AND FIELD SURVEYS.

**AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
ENDS AND CONNECTING TAXIWAYS - PHASE 2
GRADING AND LANDSCAPING PLAN -
SHEET 2**

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CHECKED BY:	CAL
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DATE:	10/31/07
JOB No:	07285-04
ILLINOIS PROJECT: ARR-3770 A.I.P. PROJECT: 3-17-0003-B36	
FINAL SUBMITTAL	
SHEET 15 OF 23 SHEETS	

K:\Aurora\0428505 Try A Ends Rehab\PH2\Drawings
FILE: ph2-stormnotes.dwg
LAYOUT: Model
UPDATE BY: mkatz
SURVEY BOOK #
DATE: Mon 2/4/08 3:08pm
XREF DWG: tbcint.dwg
tb.dwg

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATING THE WEST END OF TAXIWAY ALPHA AT THE AURORA AIRPORT. THE PROJECT INCLUDES EXCAVATION, DRAINAGE, VARIOUS PAVEMENT ITEMS, ELECTRICAL IMPROVEMENTS AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS INLET PROTECTION.
2. EXCAVATION WILL BE COMPLETED WITHIN THE PROJECT LIMITS TO GRADE OUT FOR A TAXIWAY FILLET WIDENING.
3. UNDERDRAIN INSTALLATION.
4. PAVEMENT CONSTRUCTION.
5. ELECTRICAL IMPROVEMENTS.
6. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
7. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SODDING.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 3.00 ACRES OF WHICH 1.20 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE WELCH CREEK THROUGH A STORM SEWER SYSTEM.

CONTROLS--EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

1. THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED. WHERE WORK IS COMPLETE, PERMANENT STABILIZATION SHALL OCCUR WITHIN 14 DAYS OF COMPLETION, AND IN AREAS WHERE WORK HAS TEMPORARILY CEASED FOR 21 DAYS OR MORE, TEMPORARY STABILIZATION SHALL OCCUR BY THE 14TH DAY AFTER WORK HAS CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER 1LR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES AND OTHER BUILDING MATERIALS TO REMAIN IN PLACE MORE THAN (3) DAYS SHALL BE FURNISHED WITH EROSION AND SEDIMENT CONTROL MEASURES AND SHALL BE INCIDENTAL TO THE CONTRACT. STOCKPILES SHALL BE TEMPORARILY SEEDED, WHICH WILL BE INCIDENTAL TO THE CONTRACT. IF THEY ARE TO REMAIN UNUSED FOR MORE THAN 30 DAYS, STOCKPILES SHALL NOT BE LOCATED IN SPECIAL MANAGEMENT AREAS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
 - A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
 - C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
 - D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
 - E. ANY WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION SHALL BE FILTERED.
4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.
6. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.
7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

CONTRACTORS

1. THE STORM WATER POLLUTION PREVENTION PLAN MUST CLEARLY IDENTIFY FOR EACH MEASURE IDENTIFIED IN THE PLAN, THE CONTRACTOR(S) OR SUBCONTRACTOR(S) THAT WILL IMPLEMENT THE MEASURE. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN THE PLAN MUST SIGN A COPY OF THE CERTIFICATION STATEMENT IN PARAGRAPH 2 BELOW IN ACCORDANCE WITH PART VI.G (SIGNATORY REQUIREMENTS) OF THIS PERMIT. ALL CERTIFICATIONS MUST BE INCLUDED IN THE STORM WATER POLLUTION PREVENTION PLAN EXCEPT FOR OWNERS THAT ARE ACTING AS CONTRACTOR.
2. CERTIFICATION STATEMENT. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN A STORM WATER POLLUTION PREVENTION PLAN IN ACCORDANCE WITH PARAGRAPH 1 ABOVE SHALL SIGN A COPY OF THE FOLLOWING CERTIFICATION STATEMENT BEFORE CONDUCTING ANY PROFESSIONAL SERVICE AT THE SITE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN:

"I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (1LR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

THE CERTIFICATION MUST INCLUDE THE NAME AND TITLE OF THE PERSON PROVIDING THE SIGNATURE IN ACCORDANCE WITH PART VI.G OF THIS PERMIT: THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE CONTRACTING FIRM; THE ADDRESS (OR OTHER IDENTIFYING DESCRIPTION) OF THE SITE; AND THE DATE THE CERTIFICATION IS MADE.

CONTRACTOR CERTIFICATION

"I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (1LR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

GENERAL CONTRACTOR

SIGNATURE TITLE DATE

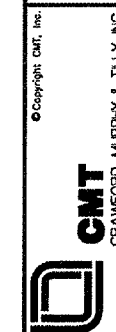
COMPANY

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2
 STORM WATER POLLUTION
 PREVENTION PLAN NOTES

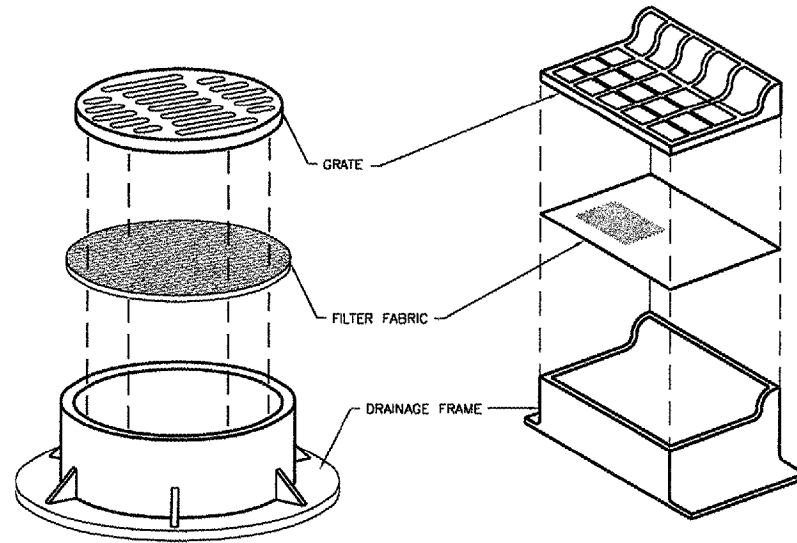


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 SHEET 16 OF 23 SHEETS

SOIL PROTECTION CHART

STABILIZATION TYPE	JAN	FEB	MAR	APR	MAY	JUNE	JULY	AUG	SEPT	OCT	NOV	DEC
PERMANENT SEEDING			+A		+A1					+A1		
DORMANT SEEDING	B										+B	
TEMPORARY SEEDING			+C			D						
SODDING			**E									
MULCHING	F											

- A KENTUCKY BLUEGRASS 100 LBS/ACRE
 PERENNIAL RYEGRASS 60 LBS/ACRE
 CREEPING RED FESCUE 40 LBS/ACRE
- B KENTUCKY BLUEGRASS 100 LBS/ACRE
 PERENNIAL RYEGRASS 60 LBS/ACRE
 CREEPING RED FESCUE 40 LBS/ACRES
- C SPRING OATS 100 LBS/ACRE
 WHEAT OR CEREAL RYE 150 LBS/ACRE
- D WHEAT OR CEREAL RYE 150 LBS/ACRE
- E SOD
- F HYDROMULCH 2 TON/ACRE
- * IRRIGATION NEEDED DURING JUNE AND JULY
- ** IRRIGATION NEEDED FOR A MINIMUM 3 TO 4 WEEKS AFTER APPLYING SOD

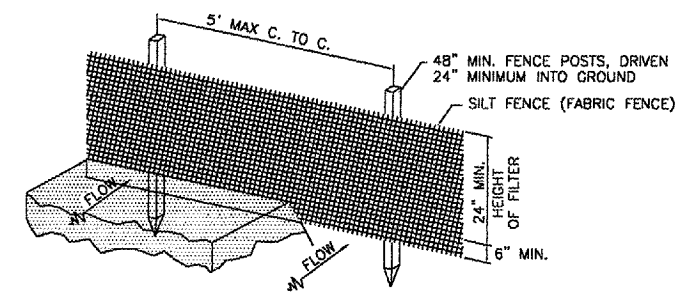


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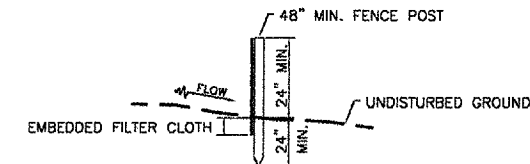
- FILTER WRAP TO BE PLACED IN ALL SLOPE BOXES, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
- FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR FABRIC FENCE.
- FABRIC SHALL OVERLAY FRAME BY 2-INCH (MINIMUM).
- CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
- FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MINIMUM OF 80% OF COVERAGE.
- COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO INLET PROTECTION.

DRAINAGE STRUCTURE FILTER WRAP

NOT TO SCALE



PERSPECTIVE VIEW



SECTION

EROSION CONTROL FABRIC FENCE DETAIL

NOT TO SCALE

CONSTRUCTION NOTES FOR SILT (FABRIC) FENCE

- WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVER-LAPPED BY 6-INCH MIN. AND FOLDED.
- MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.
- SILT FENCE SHALL BE INSTALLED PER GRADING AND LANDSCAPING PLAN OR AS DIRECTED BY THE ENGINEER.

**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2**

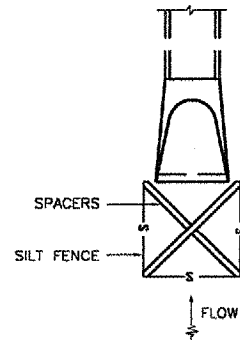
**STORM WATER POLLUTION
 PREVENTION PLAN DETAILS**

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DRAWN BY:	JRO
CHECKED BY:	
APPROVED BY:	
DATE:	10/31/07
JOB No:	07285-04
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A.I.P. PROJECT:	3-17-0003-B36
FINAL SUBMITTAL	
SHEET 17 OF 23 SHEETS	

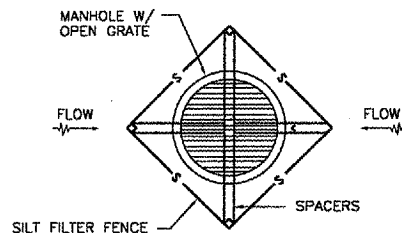
**NOTES FOR ALL
 INLET PROTECTION**

- SILT FENCE AND SPACERS SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT WILL BE INCLUDED IN THE UNIT PRICE FOR INLET PROTECTION.
- INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
- AFTER FINAL APPROVAL OF THE ENGINEER, SILT FENCE MAY BE REMOVED. CONTRACTOR SHALL PLACE SEED AND MULCH OVER THE DISTURBED AREAS.
- SILT FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR ITEM AR156000 EROSION CONTROL IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND REQUIRING SPECIAL PROVISIONS.



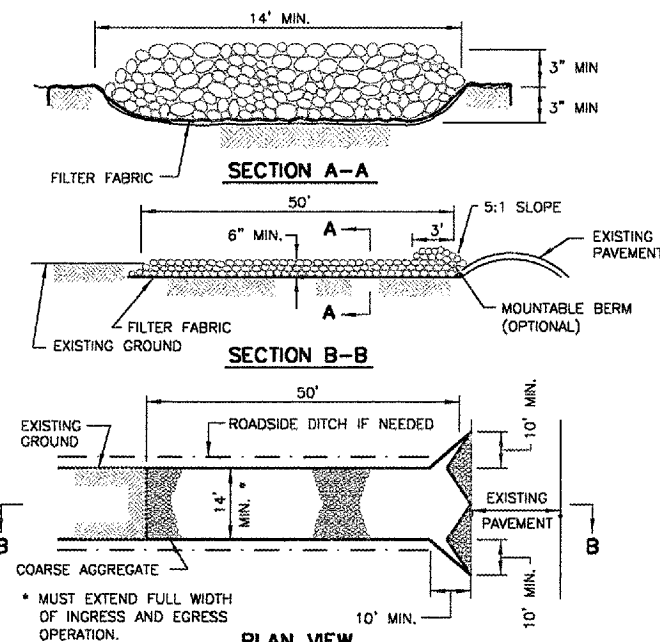
INLET PROTECTION (END SECTION)

NOT TO SCALE
 IDOT STANDARD 280001-03



INLET PROTECTION (INLET/MANHOLES)

NOT TO SCALE
 IDOT STANDARD 280001-03

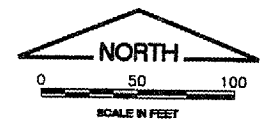
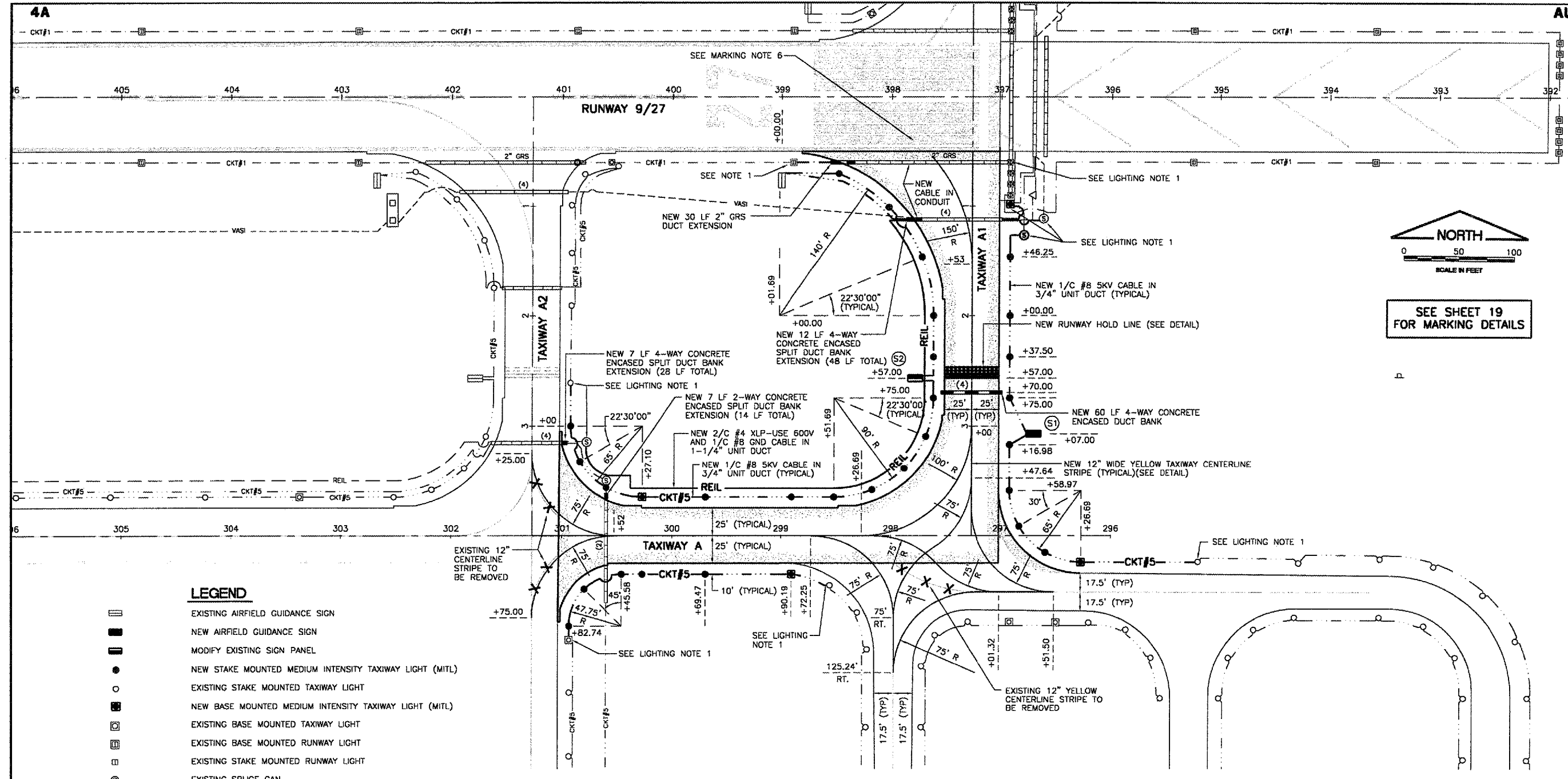


STABILIZED CONSTRUCTION ENTRANCE

FROM NRCS STANDARD DRAWING NO. IL-630

- FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR AR152540 IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND REQUIRING SPECIAL PROVISIONS.
- ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4.
- ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
- MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC. DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
- ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
- STABILIZED CONSTRUCTION ENTRANCE SHALL BE INCIDENTAL TO THE CONTRACT.

K:\Aurora\4\0428505 Try A Ends Rehab\PH2\Draw3
 FILE: ph2-light-east.dwg
 LAYOUT: Layout1
 UPDATE BY: mkatz
 SURVEY BOOK #
 DATE: Mon 2/4/08 3:02pm
 XREF DWG: tbclnt.dwg
 tb.dwg
 txyend-base.dwg



SEE SHEET 19
FOR MARKING DETAILS

LEGEND

- EXISTING AIRFIELD GUIDANCE SIGN
- NEW AIRFIELD GUIDANCE SIGN
- MODIFY EXISTING SIGN PANEL
- NEW STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT (MITL)
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- NEW BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT (MITL)
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING STAKE MOUNTED RUNWAY LIGHT
- EXISTING SPLICE CAN
- NEW SPLICE CAN
- LIMITS OF NEW P.C.C. PAVEMENT
- EXISTING CONCRETE ENCASED DUCT (NUMBER OF WAYS)
- NEW CONCRETE ENCASED/SPLIT DUCT BANK EXTENSION (NUMBER OF WAYS)
- EXISTING CIRCUIT #1 (RUNWAY 9/27)
- EXISTING CIRCUIT #4 (TAXIWAY A - WEST END)
- EXISTING CIRCUIT #5 (TAXIWAY A - EAST END)
- NEW CIRCUIT #4 (TAXIWAY A -WEST END)
1/C #8 5KV CABLE IN 3/4" UNIT DUCT
- NEW CIRCUIT #5 (TAXIWAY A -EAST END)
1/C #8 5KV CABLE IN 3/4" UNIT DUCT
- NEW CIRCUIT #1 (RUNWAY 9/27)
1/C #8 5KV CABLE IN 3/4" UNIT DUCT
- EXISTING 2/C #4 XLP AND GND.
- NEW 2-1/C #4 XLP-USE 600V AND 1/C #8 GND. CABLE
IN 1-1/4" UNIT DUCT
- EXISTING GRS CONDUIT
- NEW GRS CONDUIT
- NEW PAVEMENT MARKING
- EXISTING PAVEMENT MARKING

PAVEMENT MARKING NOTES

1. CURING COMPOUND ON CONCRETE PAVEMENTS SHALL BE REMOVED BY WATER BLASTING OR OTHER METHODS APPROVED BY THE ENGINEER PRIOR TO MARKING.
2. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
3. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE GAPPED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINES.
4. ALL RUNWAY MARKINGS ARE WHITE WITH A 6" BLACK BORDER. THE 6" BLACK BORDER WILL BE PAID FOR UNDER ITEM AR620525 - PAVEMENT MARKING - BLACK BORDER.
5. ALL TAXIWAY MARKINGS ARE YELLOW WITH A 6" BLACK BORDER. THE 6" BLACK BORDER WILL BE PAID FOR UNDER ITEM AR620525 - PAVEMENT MARKING - BLACK BORDER.
6. CONTRACTOR SHALL MAINTAIN EXISTING PAVEMENT MARKINGS TO ORIGINAL CONDITIONS. IF EXISTING MARKINGS ARE DAMAGED DURING CONSTRUCTION, THE CONTRACTOR SHALL RE-MARK AT OWN COST.

LIGHTING NOTES

1. CONNECT NEW CABLE TO EXISTING CABLE AT GUIDANCE SIGN, TAXIWAY LIGHT, RUNWAY LIGHT OR SPLICE CAN. THE COST OF SPLICING SHALL BE INCIDENTAL TO AIRFIELD LIGHTING CABLE INSTALLATION. CONTRACTOR SHALL LOCATE ENDS OF GRS CONDUIT AT NO ADDITIONAL COST TO THE CONTRACT.
2. THE CONTRACTOR SHALL VERIFY THAT THE EXISTING RUNWAY/TAXIWAY LIGHTING CIRCUITS ARE OPERATIONAL AT THE END OF EACH WORKING DAY.
3. THE ROUTING OF NEW AND EXISTING CABLE SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE FIELD COORDINATED WITH THE RESIDENT ENGINEER.
4. ANY EXISTING CABLE MARKERS THAT ARE DISTURBED SHALL BE REMOVED AND REINSTALLED AT THE SAME LOCATION. COST SHALL BE INCIDENTAL TO GRADING.
5. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS DIRECTED BY THE ENGINEER. ALL TEMPORARY CABLING AND SPLICING SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
6. AT ANY LOCATION WHERE THE NEW DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
7. CONTRACTOR SHALL PROVIDE PULL WIRE FOR ALL DUCT BANKS AND CAP THE UNUSED DUCT BANKS FOR FUTURE USE.
8. EACH CABLE SHALL HAVE A MINIMUM OF 10' SLACK IN EACH ELECTRICAL MANHOLE AND SHALL BE TAGGED AND PAID FOR PER LINEAL FOOT. ALL OTHER CABLE SLACK IN LIGHTS AND SPLICE CANS WILL BE INCIDENTAL TO THE LIGHT OR SPLICE CAN.
9. ALL DUCT BANKS SHALL EXTEND 5' BEYOND EDGE OF PAVEMENT.

REVISIONS

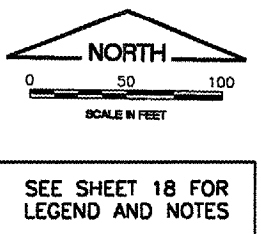
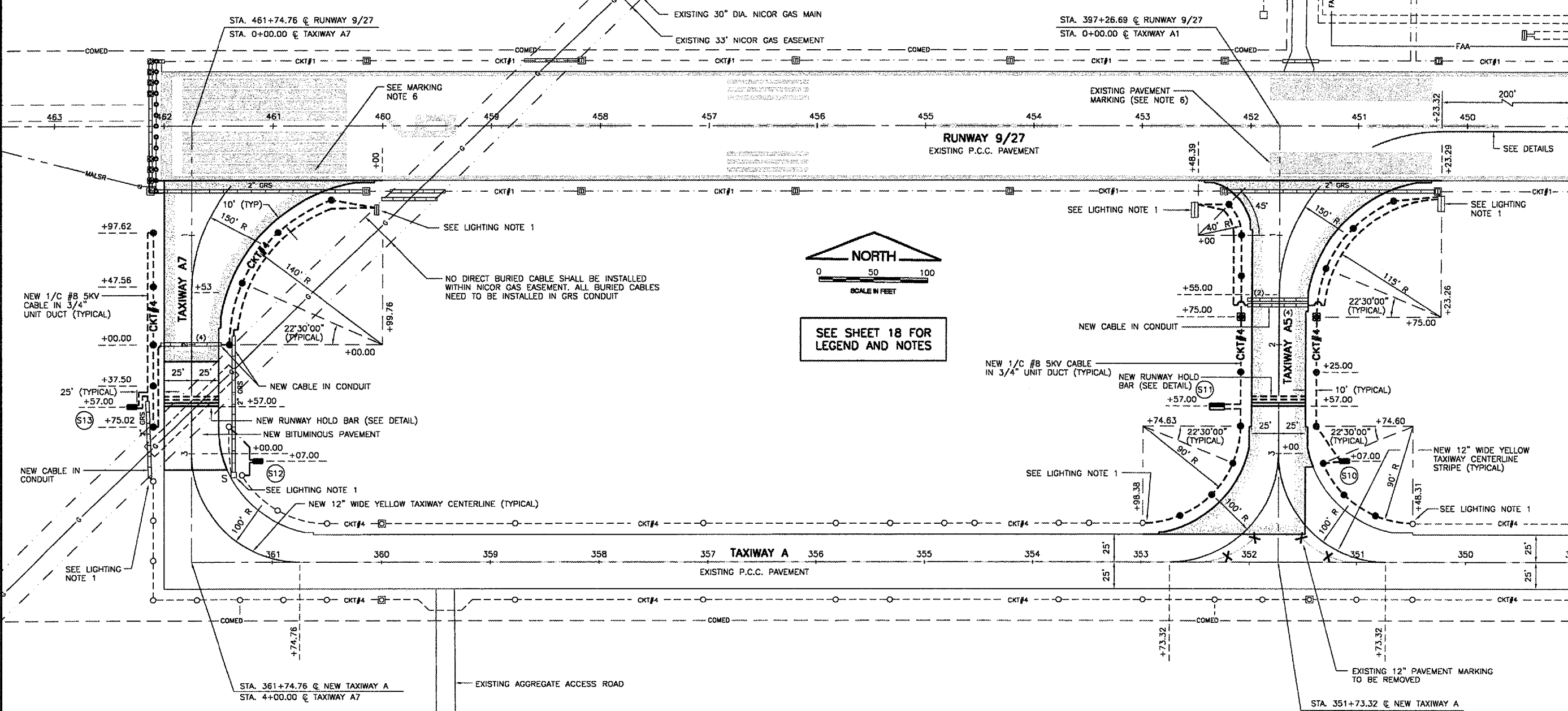
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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

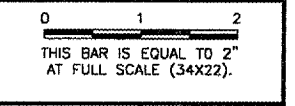
AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
ENDS AND CONNECTING TAXIWAYS - PHASE 2
LIGHTING AND PAVEMENT MARKING PLAN -
SHEET 1

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APPROVED BY:	
DATE:	10/31/07
JOB No:	07285-04
ILLINOIS PROJECT: ARR-3770 A.I.P. PROJECT: 3-17-0003-836 FINAL SUBMITTAL	
SHEET	18 OF 23 SHEETS

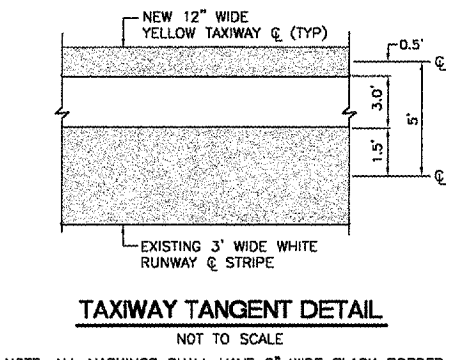
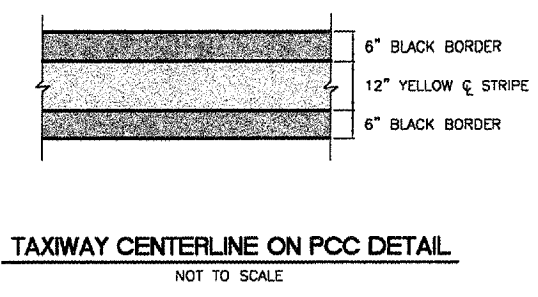


REVISIONS		
NUMBER	BY	DATE

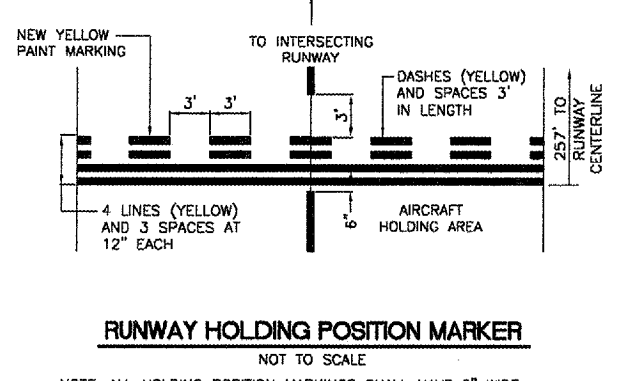


**AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
ENDS AND CONNECTING TAXIWAYS - PHASE 2**

**LIGHTING AND PAVEMENT MARKING PLAN -
SHEET 2**



NOTE: ALL MARKINGS SHALL HAVE 6" WIDE BLACK BORDER (BLACK BORDER PAID UNDER ITEM AR620525)









NOTE: ALL HOLDING POSITION MARKINGS SHALL HAVE 6" WIDE BLACK BORDER (BLACK BORDER PAID UNDER ITEM AR620525)

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ILLINOIS PROJECT:	ARR-3770
A.I.P. PROJECT:	3-17-0003-B36
FINAL SUBMITTAL	
SHEET 19 OF 23 SHEETS	

LEGEND

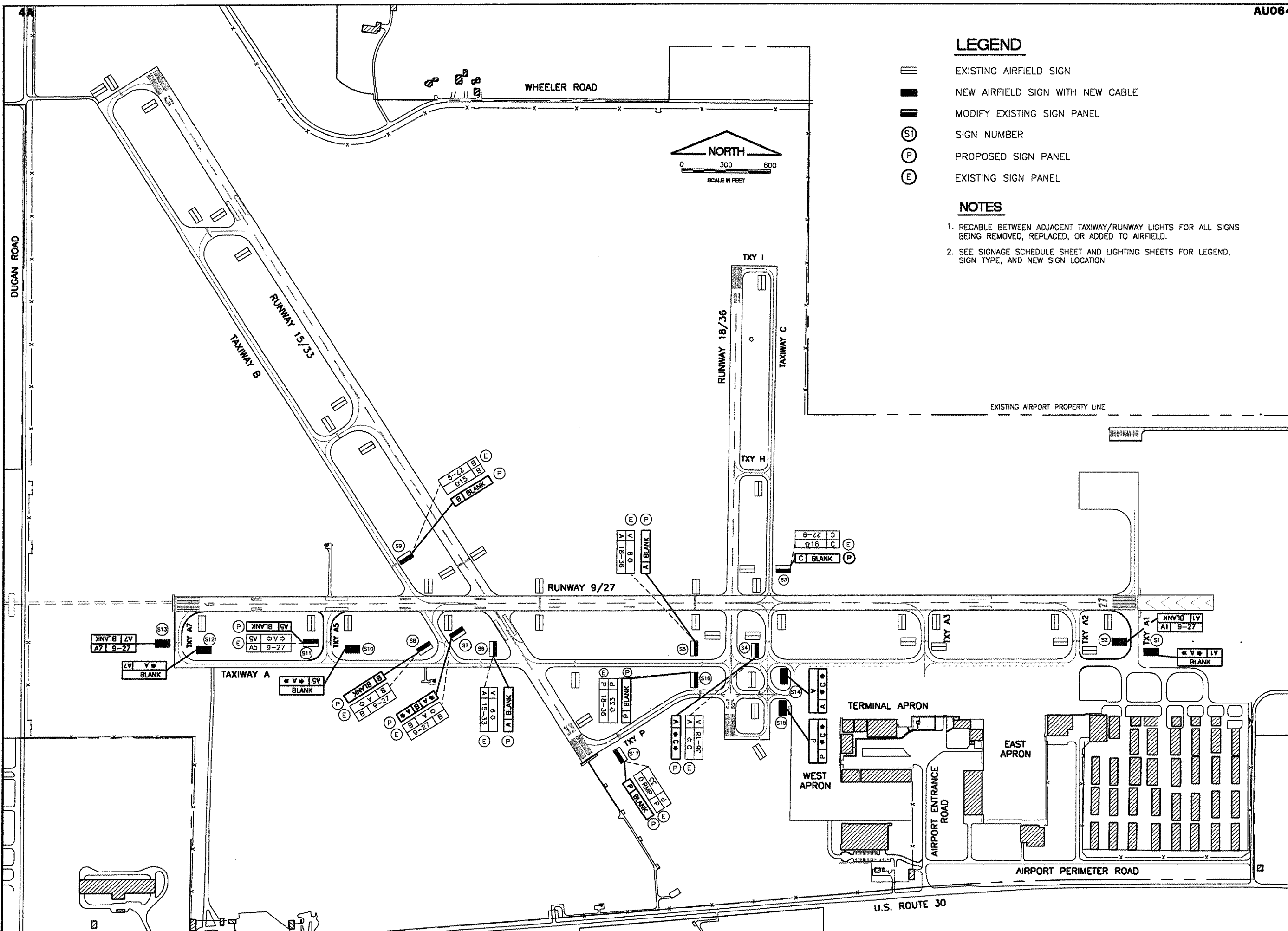
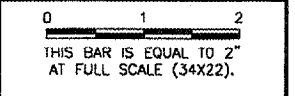
-  EXISTING AIRFIELD SIGN
-  NEW AIRFIELD SIGN WITH NEW CABLE
-  MODIFY EXISTING SIGN PANEL
-  SIGN NUMBER
-  PROPOSED SIGN PANEL
-  EXISTING SIGN PANEL

NOTES

1. RECABLE BETWEEN ADJACENT TAXIWAY/RUNWAY LIGHTS FOR ALL SIGNS BEING REMOVED, REPLACED, OR ADDED TO AIRFIELD.
2. SEE SIGNAGE SCHEDULE SHEET AND LIGHTING SHEETS FOR LEGEND, SIGN TYPE, AND NEW SIGN LOCATION

REVISIONS

NUMBER	BY	DATE
1	DKP	01/13/06
2	DKP	03/01/06



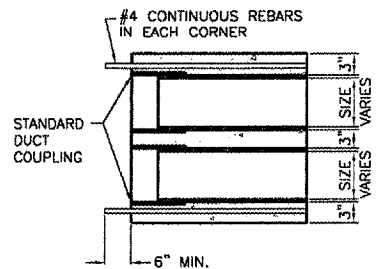
**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2**

AIRFIELD SIGNAGE PLAN

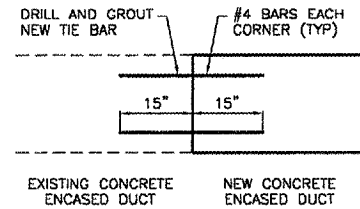
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A.I.P. PROJECT: 3-17-0003-B36	
FINAL SUBMITTAL	
SHEET	20 OF 23 SHEETS

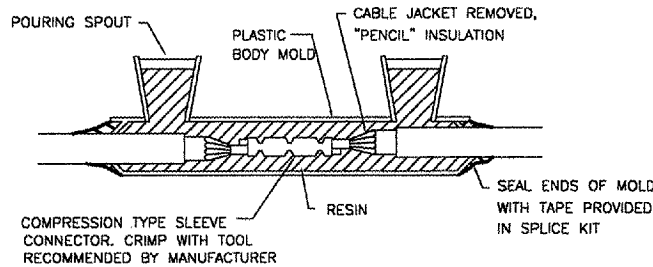
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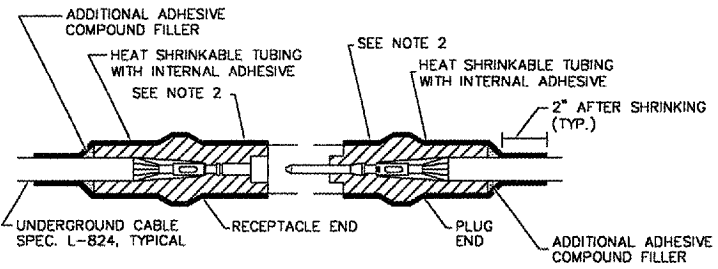
CONCRETE ENCASED DUCT END DETAIL
 NO SCALE



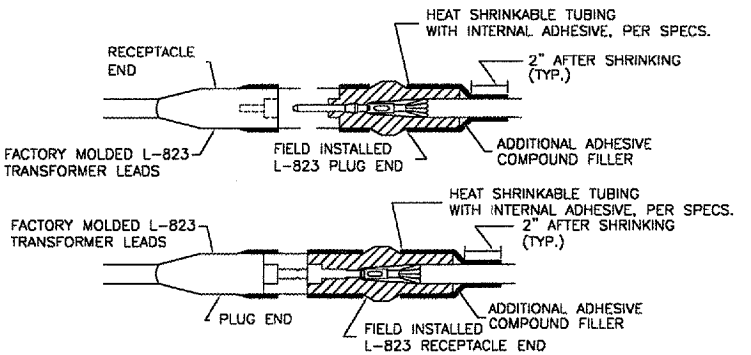
EXTENSION OF EXISTING DUCT
 NO SCALE
 NOTE: COST OF CONNECTION SHALL BE CONSIDERED INCIDENTAL TO NEW DUCT.



TYPE A - CABLE SPLICE
 FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY
 N.T.S.

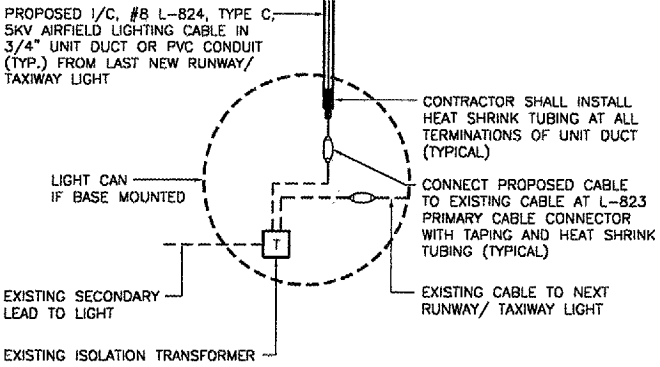


TYPE B - CABLE SPLICE
 FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT
 N.T.S.

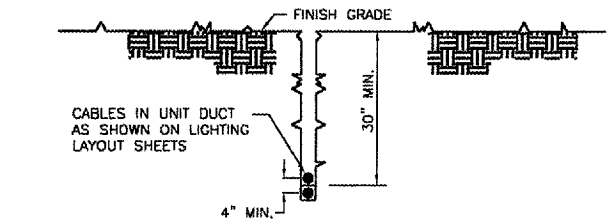


TYPE C AND D - CABLE SPLICE
 FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS
 N.T.S.

- NOTES**
- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
 - WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
 - THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
 - THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

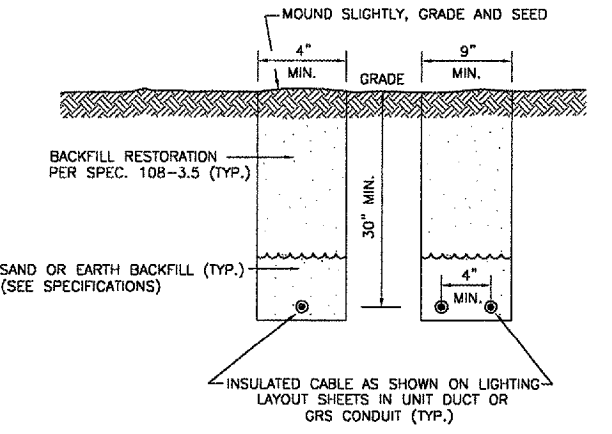


RUNWAY/TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL
 NOT TO SCALE



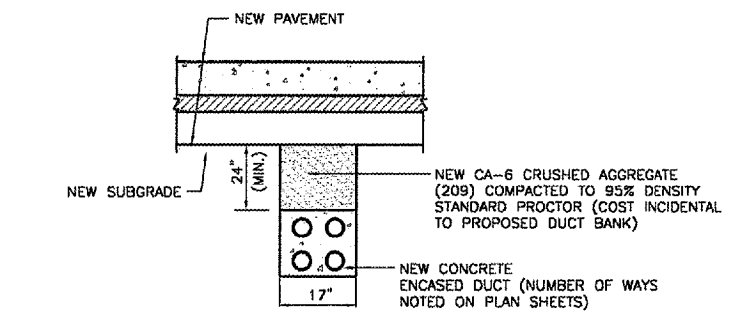
CABLE IN UNIT DUCT - PLOWED
 NOT TO SCALE

CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.

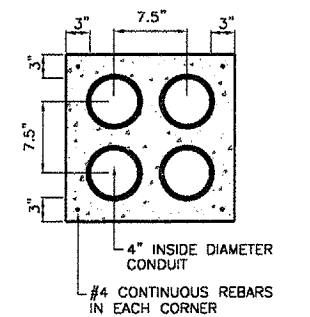


TRENCH DETAIL
 NOT TO SCALE

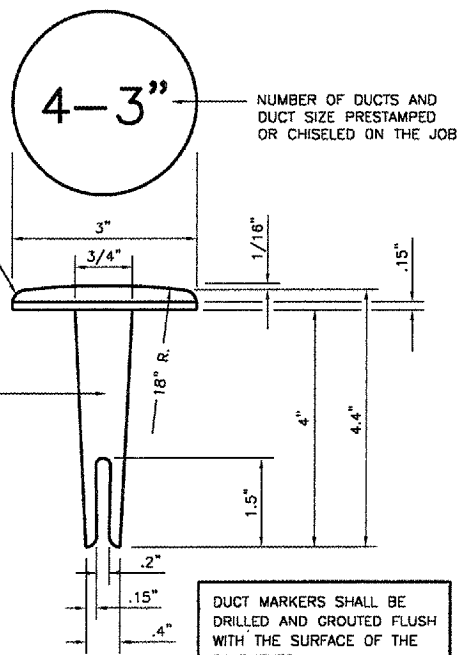
- NOTES**
- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
 - DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
 - SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
 - ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL.



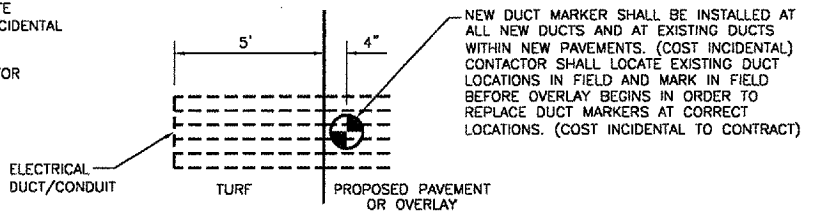
CONCRETE ENCASED DUCT BACKFILL
 NOT TO SCALE



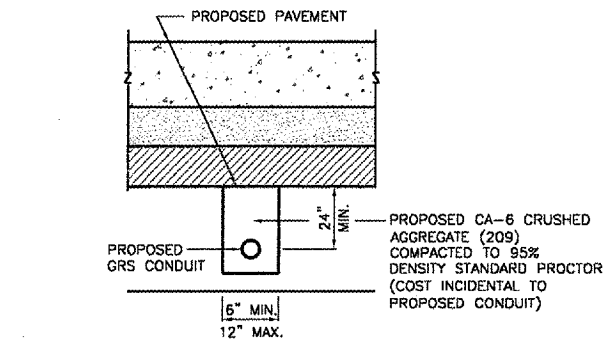
4-WAY CONCRETE ENCASED DUCT BANKS/SPLIT DUCT
 NOT TO SCALE



DUCT/CONDUIT MARKER DETAIL
 NOT TO SCALE



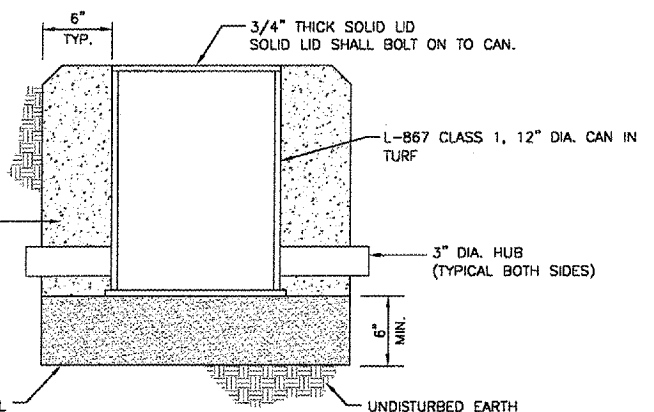
DUCT/CONDUIT MARKER DETAIL
 NOT TO SCALE



GRS CONDUIT UNDER P.C.C. PAVEMENT DETAIL
 NOT TO SCALE

NEW DUCT BANK/CONDUIT SHALL BE INSTALLED AT AN ELEVATION THAT WILL NOT CONFLICT WITH EXISTING OR NEW UTILITIES INCLUDING STORM SEWER, UNDERDRAIN, CONDUIT, DUCT, GAS, WATERMAIN, PHONE, ELECTRICAL AT NO ADDITIONAL COST TO THE CONTRACT.

- NOTES:**
- DIMENSIONS ARE MINIMUM.
 - CONCRETE SHALL CONFORM TO ITEM 610.
 - ALL CONDUIT SHALL BE SCHEDULE 40 PVC.
 - TOP OF CONCRETE ENCASEMENT IN TURF AREAS SHALL NOT BE LESS THAN 24" BELOW FINISHED GRADE.
 - 4" SPLIT DUCT SHALL BE CONCRETE ENCASED WITH 3" MINIMUM CONCRETE SURROUNDING 4" CONDUIT. COST INCIDENTAL TO SPLIT DUCT.
 - PROVIDE PULL STRINGS AND CAPS FOR UNUSED DUCTS.



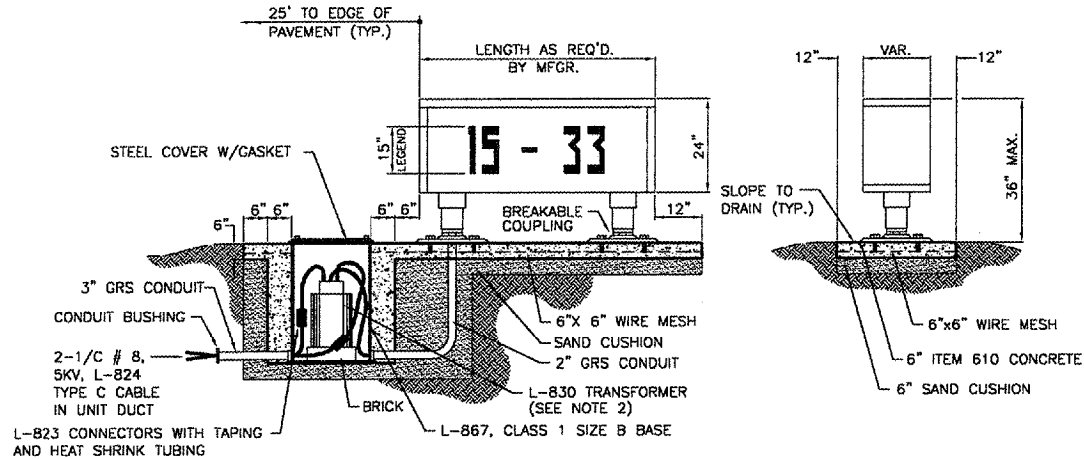
SPLICE CAN DETAIL IN TURF
 NOT TO SCALE

AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
ENDS AND CONNECTING TAXIWAYS - PHASE 2

ELECTRICAL DETAILS - SHEET 1

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ILLINOIS PROJECT:	ARR-3770
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FINAL SUBMITTAL	
SHEET	21 OF 23 SHEETS

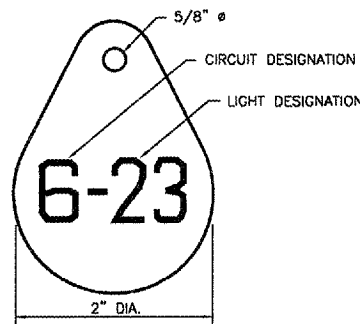


HOLD LINE / TAXIWAY GUIDANCE SIGN L-858, SIZE 2, STYLE 2, CLASS 2

NOT TO SCALE

SIGNAGE NOTES

- ALL SIGNS ARE 2-SIDED SIGNS.
- TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER.
- LIGHTED SIGNS SHALL BE BASE MOUNTED ONLY.
- UNIT DUCT SHALL BE TERMINATED IN THE CAN AND SEALED TO THE CABLE WITH HEAT SHRINK AS SPECIFIED.
- THE NUMBER OF MODULES PER SIGN SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWING INCLUDING SIGN, COLOR, SIZE AND PROPOSED LEGEND, IN ENOUGH DETAIL AND DETERMINE PROPOSED SPACING AND OTHER INFORMATION REQUIRED BY SPECIAL PROVISIONS. CONTRACTOR TO VERIFY PROPOSED SIGN LOCATIONS AND ORIENTATIONS WITH RESIDENT ENGINEER PRIOR TO INSTALLATION.



LIGHT IDENTIFICATION DETAIL

NOT TO SCALE

NOTES:

- INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (NEW OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

AIRFIELD SIGNAGE SCHEDULE						
SIGN NUMBER	PROPOSED SIGN FACE	PROPOSED SIGN LEGEND	PROPOSED SIGN TYPE	SIGN PANEL SIZE	EXISTING SIGN LEGEND	COMMENTS
S1	N S	LV BLANK	3,2 0	SEE NOTE 2	N/A	NEW SIGN PANEL 5 CHARACTER
S2	N S	LV A1 9-27	0,2 2,1	SEE NOTE 2	LV A1 27	NEW SIGN PANEL 6 CHARACTER
S3	N S	C/N C BLANK	N/C 2,0	9'x2'	6-22 18 C	MODIFY EXISTING SIGN PANEL
S4	E W	C/N A C	N/C 2,3	9'x2'	6-22 18-36 A C	MODIFY EXISTING SIGN PANEL
S5	E W	LV N/C	0,2 N/C	9'x2'	6 A 18-36	MODIFY EXISTING SIGN PANEL
S6	E W	LV N/C	0,2 N/C	11'x2'	6 A 15-33	MODIFY EXISTING SIGN PANEL
S7	N S	LV N/C	3,2,3 N/C	9'x2'	6 9-27 B	MODIFY EXISTING SIGN PANEL
S8	N S	LV N/C	0,2 N/C	9'x2'	6 9-27 B	MODIFY EXISTING SIGN PANEL
S9	N S	C/N B BLANK	N/C 2,0	12'x2'	6-22 15 B	MODIFY EXISTING SIGN PANEL
S10	N S	LV BLANK	3,2 0	SEE NOTE 2	N/A	NEW SIGN PANEL 5 CHARACTER
S11	N S	LV N/C	0,2 N/C	9'x2'	LV A1 9-27	MODIFY EXISTING SIGN PANEL
S12	N S	LV BLANK	2,3 0	SEE NOTE 2	N/A	NEW SIGN PANEL 4 CHARACTER
S13	N S	LV A7 9-27	0,2 2,1	SEE NOTE 2	LV A7 9-27	NEW SIGN PANEL 6 CHARACTER
S14	E W	C/N A	3,2 2	SEE NOTE 2	N/A	NEW SIGN PANEL 4 CHARACTER
S15	E W	C/N P	3,2 2	SEE NOTE 2	N/A	NEW SIGN PANEL 4 CHARACTER
S16	E W	LV N/C	0,2 N/C	11'x2'	6 P 18-36	MODIFY EXISTING SIGN PANEL
S17	E W	C/N P BLANK	N/C 2,0	9'x2'	6 RMP P	MODIFY EXISTING SIGN PANEL

PROPOSED SIGN TYPE LEGEND

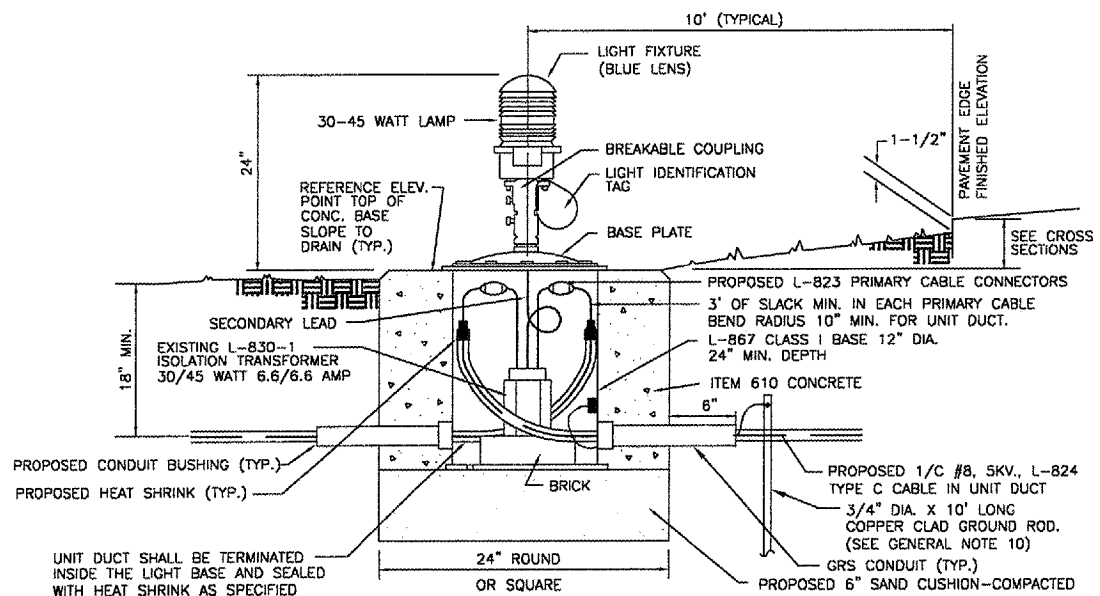
- 0 — BLANK PANEL — BLACK
- 1 — RUNWAY/TAXIWAY HOLDLINE — WHITE LEGEND ON RED BACKGROUND
- 2 — LOCATION SIGN — YELLOW LEGEND ON BLACK BACKGROUND
- 3 — DIRECTION SIGN — BLACK LEGEND ON YELLOW BACKGROUND
- 4 — REMAINING DISTANCE — WHITE LEGEND ON BLACK BACKGROUND
- N/C — NO CHANGE

NOTES

- EXCHANGING OF EXISTING PANELS BETWEEN SIGNS WILL BE INCIDENTAL TO THE COST OF RELOCATING OR ADJUSTING THE SIGN. ANY EXISTING PANELS THAT ARE NOT USED SHALL BE TURNED OVER TO THE AIRPORT.
- PROPOSED GUIDANCE SIGNS PANEL SIZE WILL BE BASED ON THE MANUFACTURER'S RECOMMENDATION. CONTRACTOR TO VERIFY SIZE OF PANEL BEFORE SUBMITTING SHOP DRAWINGS OR ORDERING MATERIALS.
- EXISTING SIGN TO BE REMOVED AND REPLACED WITH NEW SIGN.

GENERAL NOTES:

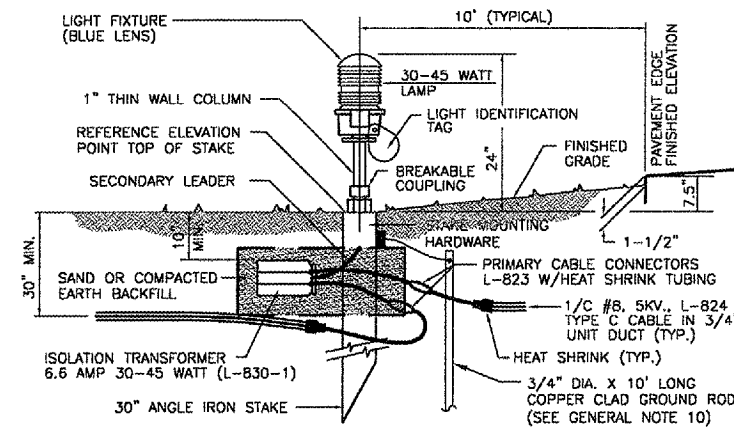
- THE CONCRETE BASE FOR BASE MTD. LIGHTS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (610).
- TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. B, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL SIGNS, LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT DIRECTOR, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE AT NO ADDITIONAL COST.
- TAXIWAY LIGHTS SHALL HAVE A BLUE LENS, RUNWAY LIGHTS SHALL HAVE A CLEAR OR 180° AMBER/CLEAR LENS AS DESIGNATED ON PLANS.
- DUCT MARKERS SHALL BE INSTALLED AT EVERY NEW DUCT AND AT EVERY EXISTING DUCT USED FOR THIS PROJECT.
- CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.
- INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN OR STAKE AND EXOTHERMICALLY WELDED TO GROUND ROD.



NEW/ADJUSTED/RELOCATED BASE MOUNTED

MEDIUM INTENSITY TAXIWAY LIGHT

NOT TO SCALE



NEW/ADJUSTED/RELOCATED STAKE MOUNTED

MEDIUM INTENSITY TAXIWAY LIGHT

NOT TO SCALE

AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
ENDS AND CONNECTING TAXIWAYS - PHASE 2

ELECTRICAL DETAILS - SHEET 2



DESIGN BY: AB

DRAWN BY: JRO

CHECKED BY: AB

APPROVED BY:

DATE: 10/31/07

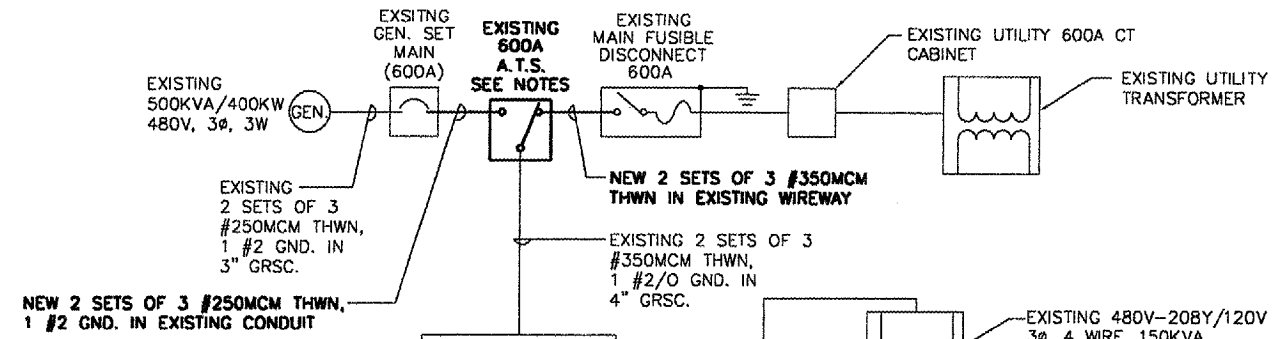
JOB No: 07285-04

ILLINOIS PROJECT: ARR-3770

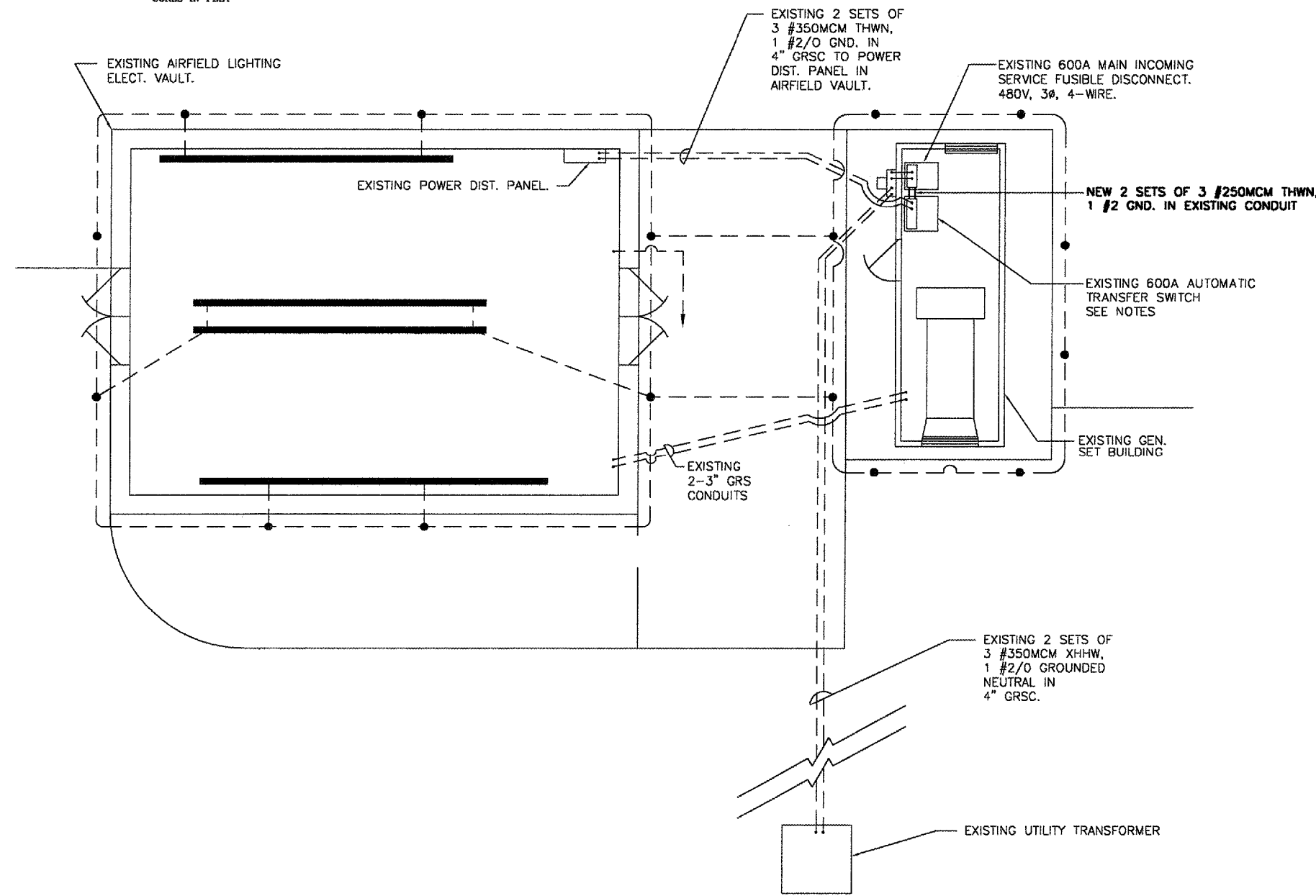
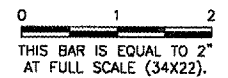
A.I.P. PROJECT: 3-17-0003-836

FINAL SUBMITTAL

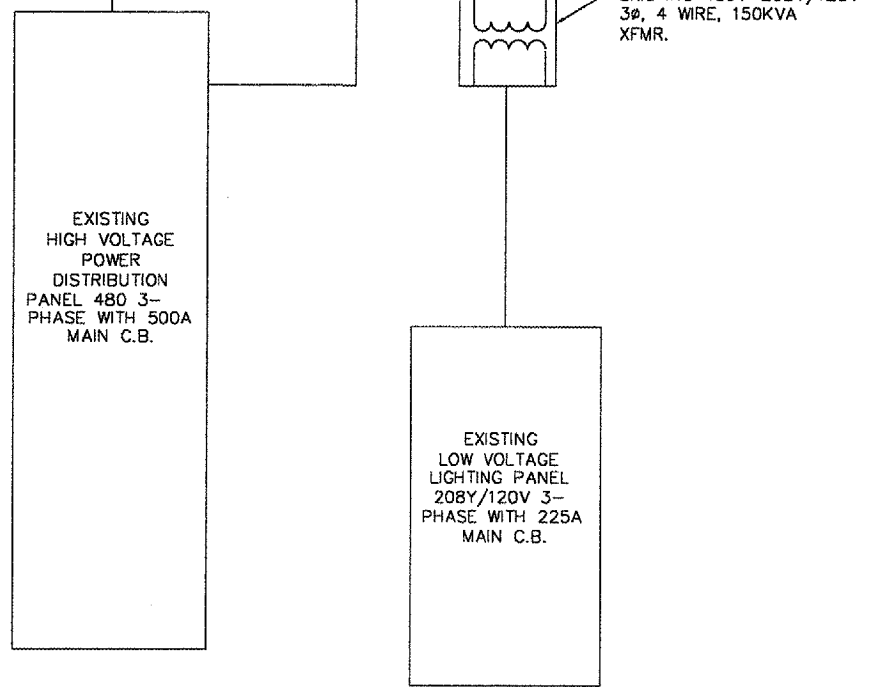
SHEET 22 OF 23 SHEETS



REVISIONS		
NUMBER	BY	DATE



EXISTING VAULT/GENERATOR DETAIL



EXISTING POWER DISTRIBUTION ONE-LINE

NOTES:

1. REMOVE AND REPLACE EXISTING TRANSFER SWITCH. NEW A.T.S. SHALL BE 600A, 480V, 3-POLE IN NEMA 12 ENCLOSURE.
2. CONTRACTOR TO VERIFY TOP AND BOTTOM ENTRY OF NORMAL, LOAD AND EMERGENCY CABLES. ALIGN NEW A.T.S. WITH EXISTING CONDUIT STUBS AND WIREWAY. USE EXISTING CABLE SLACK TO PROVIDE REQUIRED BENDING RADIUS.
3. AUTOMATIC TRANSFER SWITCH SHALL BE MODEL NO. 7ATS-A-3-600-N-5X-L AS MANUFACTURED BY ASCO OR APPROVED EQUAL

AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 REHABILITATE AND STRENGTHEN TAXIWAY ALPHA
 ENDS AND CONNECTING TAXIWAYS - PHASE 2

VAULT MODIFICATION DETAIL

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DESIGN BY:	AB
DRAWN BY:	JRO
CHECKED BY:	AB
APPROVED BY:	
DATE:	10/31/07
JOB No:	07285-04
ILLINOIS PROJECT: ARR-3770 A.I.P. PROJECT: J-17-0003-B36	
FINAL SUBMITTAL	
SHEET 23 OF 23 SHEETS	