

DUPAGE AIRPORT AUTHORITY WEST CHICAGO, ILLINOIS

CONSTRUCTION PLANS FOR DUPAGE AIRPORT

RE-MARK RWY 10-28 AND RE-LIGHT RWY END 28 THRESHOLD

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LIGHTING PLAN	
7	LIGHTING PLAN

A.I.P. PROJECT: 3-17-0017-B21
ILLINOIS PROJECT: DPA-3695

MARCH 7, 2008

ITEM NO.	DESCRIPTION	UNIT	QUANTITY	RECORD QUANTITY
AR108158	1/C #8 5 KV UG CABLE IN UD	LF	420	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	6	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	50910	
AR620525	PAVEMENT MARKING - BLACK BORDER	SF	23811	
AR620900	PAVEMENT MARKING REMOVAL	SF	42727	
AR800175	RETROFIT EXISTING ELEVATED LIGHT	EACH	22	
AR800176	RETROFIT EXISTING IN-PAVEMENT LIGHT	EACH	2	

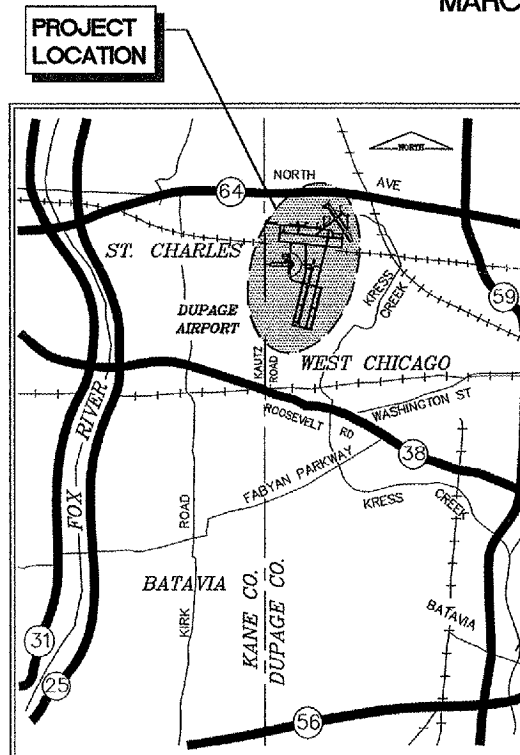
DUPAGE AIRPORT

TOWNSHIP: 40 NORTH WAYNE TOWNSHIP
RANGE: 9 EAST (SECTIONS: 31)
DUPAGE COUNTY

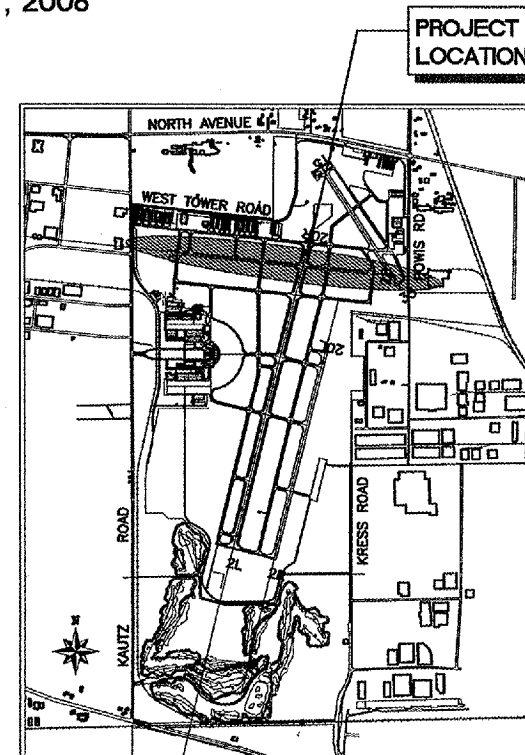
RE-MARK RWY 10-28 AND RE-LIGHT
RWY END 28 THRESHOLD

DESIGN AIRCRAFT APPROACH CATEGORY B
DESIGN AIRCRAFT GROUP II

CALL JULIE
BEFORE EXCAVATING
1-800-892-0123



LOCATION MAP



SITE PLAN

CMT 07257-04
CRANFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
800 N. COMMONS DRIVE
SUITE 107
AURORA, IL 60504
PHONE (630) 820-1022
FAX (630) 820-0950

SUBMITTED BY *Jeremy R. Linke*
JEREMY R. LINKE, P.E.
DATE *1/21/08*

DPA
DuPage Airport
2700 INTERNATIONAL DRIVE
SUITE 200
WEST CHICAGO, IL 60185

APPROVED BY *David Bird*
DAVID BIRD - EXECUTIVE DIRECTOR
DATE *1/9/08*

DATE: Wednesday, January 09, 2008 4:54:47 PM
 FILE: K:\Dupage\A0725704_Rwy 28_PAV\Draw\Sheets\Re-mark And Re-light Rwy 10-28_Tid\mark-Site_Plan.dwg
 UPDATE BY: Jeremy Linke
 LAYOUT: Layout1
 XREF: DWG: post-base.dwg
 IMAGE FILES:

HORIZONTAL CONTROL (NAD 27)

NO.	DESCRIPTION	NORTHING	EASTING
1	TAXIWAY G/E9 AND RUNWAY 10/28 INTERSECTION	1911021.627	520416.973
2	TAXIWAY E8 AND RUNWAY 10/28 INTERSECTION	1910994.196	520679.544
3	TAXIWAY E6 AND RUNWAY 10/28 INTERSECTION	1910878.699	521785.067
4	TAXIWAY W AND RUNWAY 10/28 INTERSECTION	1910809.875	522443.843
5	RUNWAY 2L/20R AND RUNWAY 10/28 INTERSECTION	1910707.252	523426.135
6	TAXIWAY C AND RUNWAY 10/28 INTERSECTION	1910646.109	524011.384
7	TAXIWAY E1 AND RUNWAY 10/28 INTERSECTION	1910533.207	525092.069
8	RUNWAY 2L/20R AND TAXIWAY E WEST INTERSECTION	1911251.143	523561.382
9	RUNWAY 2L/20R AND TAXIWAY G INTERSECTION	1910021.263	523255.554
10	RUNWAY 2L/20R AND TAXIWAY E EAST INTERSECTION	1911256.043	523562.600
11	END OF RUNWAY 10	1911024.225	520392.108
12	END OF RUNWAY 28	1910530.609	525116.936

LEGEND

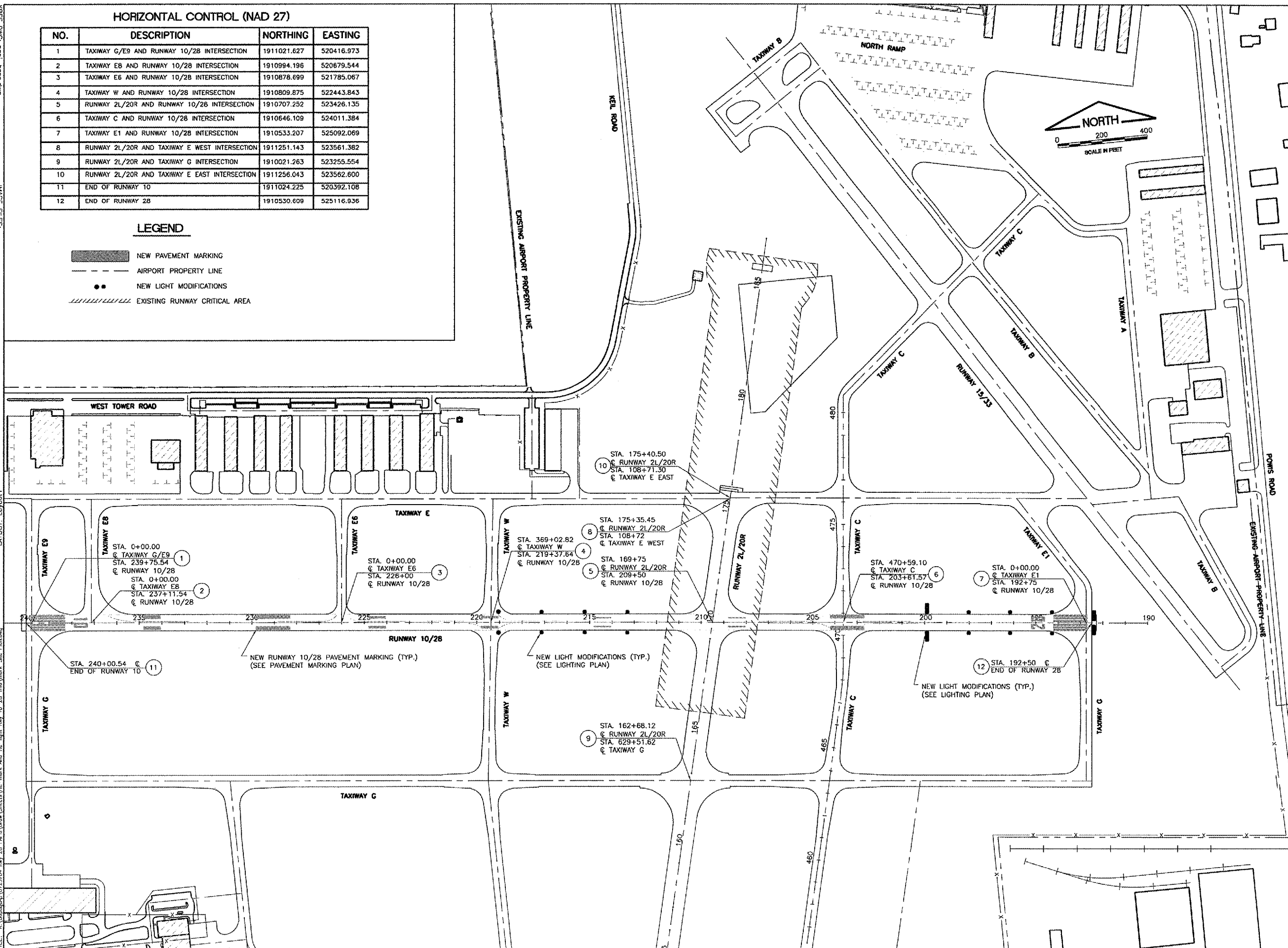
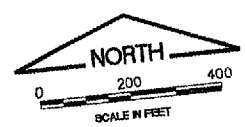
- NEW PAVEMENT MARKING
- AIRPORT PROPERTY LINE
- NEW LIGHT MODIFICATIONS
- EXISTING RUNWAY CRITICAL AREA

IL CONTRACT: **DU076**
 IL LETTING ITEM: **2A**
 IL PROJECT: **DPA-3695**
 A.I.P. PROJECT: **3-17-0017-B21**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



**DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS**

**RE-MARK Rwy 10-28 AND RE-LIGHT Rwy END 28 THRESHOLD
 SITE PLAN - PROJECT LAYOUT**

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DESIGN BY:	JP / JRL
DRAWN BY:	JRO
CHECKED BY:	JRL
APPROVED BY:	DLP
DATE:	MARCH 7, 2008
JOB No:	07257-04
SHEET 2 OF 7 SHEETS	

DATE: Wednesday, January 09, 2008 4:54:52 PM
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 UPDATE BY: Jeremy Linke
 LAYOUT: Layout1
 XREF DWG: papi-base.dwg
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 IMAGE FILES:

**PHASE 1A AND 1B
SUGGESTED SEQUENCE OF CONSTRUCTION
(OUTSIDE A.O.A. OF RUNWAY 2L/20R)**

- CLOSE RUNWAY 10/28. PLACE RUNWAY CLOSURE MARKERS ON RUNWAY 10/28.
- PLACE LATHE AND RIBBON AT AIR OPERATIONS AREA (A.O.A.) LIMITS AND PLACE BARRICADES AS SHOWN ON THE PLANS.
- REMOVE EXISTING PAVEMENT MARKING.
- CONSTRUCT NEW PAVEMENT MARKING AND RELOCATED THRESHOLD.
- CONSTRUCT RUNWAY LIGHT MODS.

**PHASE 2
SUGGESTED SEQUENCE OF CONSTRUCTION
(WITHIN A.O.A. OF RUNWAY 2L/20R)**

- CLOSE RUNWAY 10/28 AND RUNWAY 2L/20R. PLACE RUNWAY CLOSURE MARKERS ON RUNWAY 10/28 AND RUNWAY 2L/20R.
- REMOVE EXISTING PAVEMENT MARKING.
- CONSTRUCT NEW PAVEMENT MARKING.
- OPEN RUNWAY 10/28 AND RUNWAY 2L/20R.

NOTES

1. RUNWAY 10/28 SHALL BE CLOSED FOR THE ENTIRE DURATION OF THE PROJECT (7 CALENDAR DAYS).
2. CONTRACTOR SHALL PLACE BARRICADES AND TEMPORARILY CLOSE TAXIWAYS AND RUNWAYS DURING CONSTRUCTION IF REQUIRED BY THE AIRPORT AND ENGINEER. OTHER METHODS AND SEQUENCING SHALL BE APPROVED BY THE FAA ATCT, AIRPORT, AND ENGINEER PRIOR TO START OF CONSTRUCTION.
3. CONSTRUCTION OF NEW PAVEMENT MARKING AND LIGHT MODS. SHALL BE COORDINATED WITH ENGINEER AND AIRPORT. CONSTRUCTION SHALL BE EXPEDITED TO MINIMIZE THE CLOSURE OF ALL RUNWAYS AND DISRUPTION TO AIRCRAFT OPERATIONS.

LEGEND

- ➔ CONTRACTOR'S ACCESS TO SITE
- ✈ AIRCRAFT MOVEMENT AREA
- AOA— AIRCRAFT OPERATIONS AREA (A.O.A.)
- PHASE 1A BARRICADES WITH FLASHING RED LIGHTS
- ▣ PHASE 1B BARRICADES WITH FLASHING RED LIGHTS
- PHASE 2 BARRICADES WITH FLASHING RED LIGHTS
- - - AIRPORT PROPERTY LINE
- X CLOSED RUNWAY MARKER
- ////// EXISTING RUNWAY CRITICAL AREA
- ▨ PHASE 1A CONSTRUCTION
- ▩ PHASE 1B CONSTRUCTION
- ▧ PHASE 2 CONSTRUCTION
- NEW LIGHT MODIFICATIONS

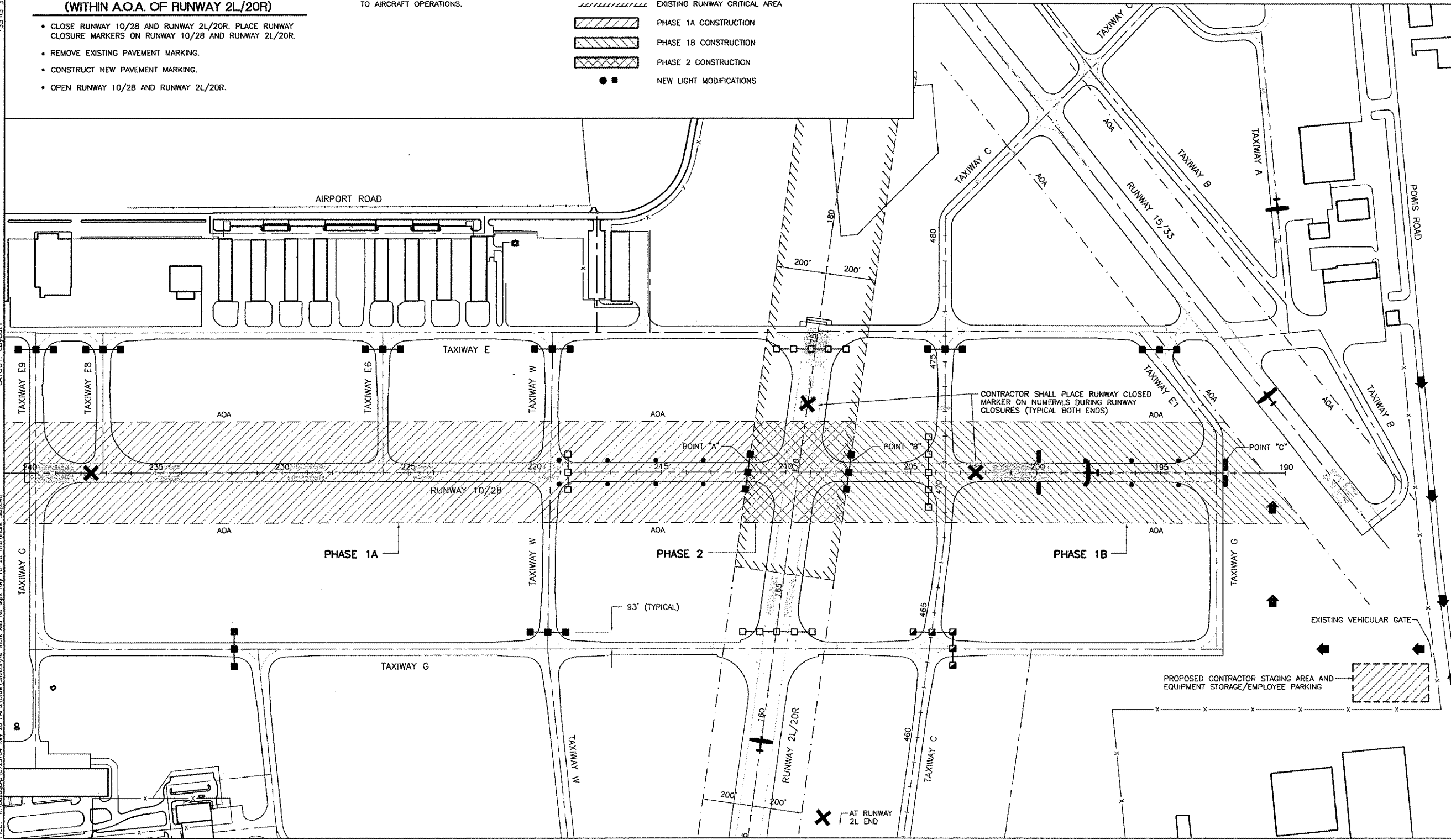
PHASE	WORK AREA	ALLOWABLE WORK PERIODS	AIRPORT OPERATIONAL RESTRICTIONS
1A	RUNWAY 10/28 FOR REMARKING AND LIGHT MODIFICATIONS	UNRESTRICTED FOR 4 CALENDAR DAYS MAXIMUM	RUNWAY 10/28 WILL BE CLOSED. AIRCRAFT MAY USE RUNWAY 10/28 FOP TAXIING AS APPROVED.
1B	RUNWAY 10/28 FOR REMARKING AND LIGHT MODIFICATIONS	UNRESTRICTED FOR 2 CALENDAR DAYS MAXIMUM	RUNWAY 10/28 WILL BE CLOSED. AIRCRAFT MAY USE RUNWAY 10/28 FOP TAXIING AS APPROVED.
2	INTERSECTION OF RUNWAY 10/28 AND RUNWAY 2L/20R FOR REMARKING	RESTRICTED TO 1 CALENDAR DAY MAXIMUM. CLOSURE OF RUNWAY 2L/20R ONLY PERMITTED BETWEEN 10:00 A.M. AND 2:00 P.M. WEEKDAYS.	RUNWAY 10/28 AND RUNWAY 2L/20R WILL BE CLOSED. AIRCRAFT MAY USE RUNWAY 10/28 FOP TAXIING AS APPROVED.

IL CONTRACT: **DU076**
 IL LETTING ITEM: **2A**
 IL PROJECT: **DPA-3695**
 A.I.P. PROJECT: **3-17-0017-B21**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



**DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS**

**RE-MARK Rwy 10-28 AND RE-LIGHT Rwy END 28 THRESHOLD
SEQUENCE OF CONSTRUCTION
PER AC 150-5370-2E**

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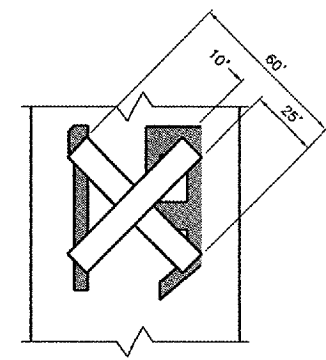
DESIGN BY: JP / JRL
 DRAWN BY: JRO
 CHECKED BY: JRL
 APPROVED BY: DLP
 DATE: MARCH 7, 2008
 JOB No: 07257-04

DATE: Wednesday, January 09, 2008 4:54:55 PM
 FILE: K:\dupage\0725704 Rev 28 P\0725704\Draw\Sheets\06-mark And Re-light Rwy 10-28 Thd\mark-Sig Notes.dwg
 UPDATE BY: Jeremy Linke
 LAYOUT: Layout1
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GENERAL NOTES:

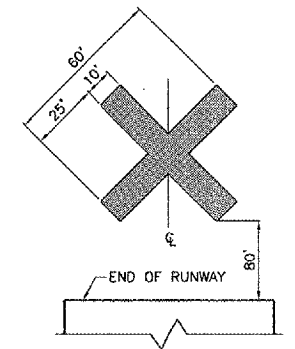
- ALL CONSTRUCTION SEQUENCING AND OPERATIONS SHALL CONFORM TO THE APPLICABLE PROVISIONS OF AC 150/5370-2E OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION (LATEST EDITION). **PAYMENT FOR MAINTENANCE OF TRAFFIC AND REQUIREMENTS OF THE SEQUENCE OF CONSTRUCTION SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.**
- THE CONTRACTOR MUST STAGE CONSTRUCTION AROUND AIRPORT OPERATIONS. STAGING SHOWN IS SUGGESTED AND IS INTENDED TO PROVIDE THE CONTRACTOR WITH MAJOR WORK AREAS WHILE MINIMIZING DISRUPTIONS TO AIRPORT OPERATIONS. THE CONTRACTOR MAY USE ALTERNATE STAGING PLANS; HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DIRECTOR AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER TWO WORKING DAYS IN ADVANCE OF ANY STAGING CHANGES WHICH WILL REQUIRE CHANGES IN AIRCRAFT MOVEMENT. THE RESIDENT ENGINEER SHALL THEN NOTIFY THE AIRPORT WHO WILL ISSUE APPROPRIATE NOTAMS. THE MOVING AND MAINTENANCE OF ALL TRAFFIC CONTROL SIGNS, BARRICADES, LATHE/RIBBON AT A.O.A. LIMITS ETC. SHALL BE THE CONTRACTOR'S RESPONSIBILITY, AND SHALL BE INCIDENTAL TO THE CONTRACT.
- BARRICADES AT 10' CENTERS SHALL BE PLACED AT THE LOCATIONS SHOWN OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHED TO PREVENT BLOWING OVER, HAVE A FLASHING RED LIGHT AND CONFORM TO IDOT STANDARD 702001, TYPE II. ROPE WITH HIGH VISIBILITY ORANGE FLAGGING SHALL BE INSTALLED BETWEEN ALL BARRICADES.
- THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING PAVEMENTS, EXCEPT AS SHOWN OR WITH THE PRIOR APPROVAL OF THE AIRPORT DIRECTOR.
- EXISTING AREAS BEYOND PROJECT LIMITS INCLUDING THE HAUL ROAD(S) DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO PRE-CONSTRUCTION CONDITION AT NO ADDITIONAL COST TO THE OWNER AND TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR.
- EACH DAY AT THE COMPLETION OF WORK, OR MORE FREQUENTLY AS DETERMINED BY THE AIRPORT DIRECTOR, THE CONTRACTOR SHALL BE REQUIRED TO USE A PICKUP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AIRFIELD PAVEMENT AREAS AND HAUL ROADS. THE COST OF SWEEPING SHALL BE INCIDENTAL TO THE CONTRACT.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- CONTRACTOR'S WORK WITHIN AIRCRAFT OPERATIONS AREA (A.O.A.) SHALL BE EXPEDITED TO MINIMIZE ANY TEMPORARY CLOSURE TIME.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.
- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND DIRECTOR OF OPERATIONS AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE A DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST OF CROSSING GUARDS, PAVEMENT REPAIR OR ANY NECESSARY ITEMS ASSOCIATED WITH THE SEQUENCE OR STAGING SHALL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- IF REQUESTED BY THE AIRPORT, FAA, OR ENGINEER, REMOVAL ITEMS/MATERIALS SHALL BE DELIVERED TO A DESIGNATED LOCATION AT THE AIRPORT MAINTENANCE BUILDING. IF NOT REQUESTED, MATERIALS REMOVED FROM THE PROJECT SHALL BE DISPOSED OF OFF OF AIRPORT PROPERTY (INCIDENTAL).
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, BARRICADES, SIGNING, RUNWAY CLOSE MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR WILL BE REQUIRED TO PLACE A LINE OF LATHE AND RIBBON ALONG THE AIR OPERATIONS AREA (A.O.A.) LIMIT AND ALONG DESIGNATED LIMITS OF CONSTRUCTION AT LOCATIONS AS SHOWN ON THE PLANS.

- THE CONTRACTOR SHALL CONTACT THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR A MINIMUM OF FIVE (5) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS.
- ORANGE CONES SHALL BE PLACED AT 25' CENTERS ALONG THE PAVEMENT EDGE DURING PAVING OPERATIONS OF THE CLOSURE LANES TO PREVENT VEHICLES FROM ENTERING.
- VEHICLES AND EQUIPMENT OPERATING ON THE AIRPORT SHALL DISPLAY ORANGE AND WHITE FLAGS (SEE DETAIL THIS SHEET) AND HAVE BEACON LIGHTS.
- ANY PAVEMENT DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR AT NO ADDITIONAL COST TO THE OWNER. THE PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE A DEBRIS-FREE SURFACE DURING HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED AT ALL TIMES UNLESS THE GATE IS MANNED WITH A GUARD (COST INCIDENTAL). THE CONTRACTOR SHALL PROVIDE AN "AUTHORIZED PERSONNEL ONLY" SIGN AT THE ACCESS GATE. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE, THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COSTS RELATING TO THE CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BECOME NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, HE SHALL DO SO AT NO EXTRA COST TO THE CONTRACT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A COMBINATION LOADER, WHICH HAS A MAXIMUM HEIGHT OF 15 FEET WITH THE BUCKET IN THE FULLY EXTENDED POSITION.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION, THE CONTRACTOR SHALL PLACE "RUNWAY CLOSED" MARKERS OVER THE NUMERALS AS DETAILED IN THE PLANS. OTHERWISE, "RUNWAY CLOSED" MARKERS SHALL BE PLACED IN THE TURF AT THE ENDS OF THE RUNWAY AS DETAILED IN THE PLANS.
- RUNWAYS SHALL BE CLOSED DURING ANY TIME THE CONTRACTOR IS WORKING WITHIN THE RUNWAY A.O.A. AND/OR OPEN EXCAVATIONS NOT MEETING THE RUNWAY GRADING CRITERIA EXIST WITHIN 200' OF THE AFFECTED RUNWAY CENTERLINE.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.



- CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
- MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

CLOSED RUNWAY MARKER DETAIL
ON PAVEMENT - NO SCALE



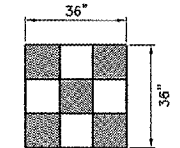
- CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
- MARKERS SHALL BE PLACED 80'-FEET OFF THE RUNWAY-END OR AS NOTED.
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

CLOSED RUNWAY MARKER DETAIL
OFF PAVEMENT - NO SCALE

LIMITATIONS ON CONSTRUCTION WITHIN AIRPORT OPERATIONS AREA (AOA)

RUNWAYS:
THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND DIRECTOR OF OPERATIONS TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION PHASE CHANGES. WORK SHALL BE EXPEDITED IN RUNWAY 2L / 20R A.O.A. AND NEAR OPEN TAXIWAYS. THE CONTRACTOR SHALL BE GIVEN 7 CALENDAR DAYS TO COMPLETE THIS WORK, IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT.

TAXIWAYS:
CONSTRUCTION WILL BE ALLOWED UP TO THE EDGE OF THE TAXIWAY PAVEMENTS WITHOUT CLOSURE ON A LIMITED BASIS. WORK WITHIN THE TAXIWAY A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.



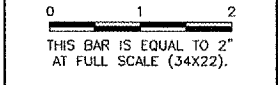
CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
ORANGE/WHITE

DESIGN AIRCRAFT APPROACH CATEGORY: B DESIGN AIRPORT GROUP: II	
MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 15' POINT "A" @ PAVEMENT MARKING EAST SIDE CLOSEST POINT OF CONSTRUCTION TO RUNWAY 2L/20R (10/28 CLOSED) STA. 211+46.77, 36.5' RT. RUNWAY 10/28 CENTERLINE GEOGRAPHIC NAD 83: LATITUDE: 41°54'45.94" LONGITUDE: 88°14'52.97" POTENTIAL OBJECT ELEVATION: 771.23 = (GROUND ELEVATION 756.23 + COMBO 15')	MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 15' POINT "B" @ PAVEMENT MARKING WEST SIDE CLOSEST POINT OF CONSTRUCTION TO RUNWAY 2L/20R (10/28 CLOSED) STA. 207+42.83, 36.5' RT. RUNWAY 10/28 CENTERLINE GEOGRAPHIC NAD 83: LATITUDE: 41°54'45.52" LONGITUDE: 88°14'47.66" POTENTIAL OBJECT ELEVATION: 771.23 = (GROUND ELEVATION 756.23 + COMBO 15')
MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 15' POINT "C" @ 28 THRESHOLD LIGHT CLOSEST POINT OF CONSTRUCTION TO RUNWAY 15/33 STA. 192+36.89, 50' RT. RUNWAY 10/28 CENTERLINE GEOGRAPHIC NAD 83: LATITUDE: 41°54'44.09" LONGITUDE: 88°14'27.83" POTENTIAL OBJECT ELEVATION: 771 = (GROUND ELEVATION 756 + COMBO 15')	
CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS/SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT: <ul style="list-style-type: none"> INSTALL RUNWAY 28 PAPI / REMOVE VASI REHABILITATION OF RUNWAY 2L/20R - PHASE 1 AIRFIELD SIGNAGE UPDATE - PHASE 1 	

IL. CONTRACT: **DU076**
 IL. LETTING ITEM: **2A**
 IL. PROJECT: **DPA-3695**
 A.I.P. PROJECT: **3-17-0017-B21**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



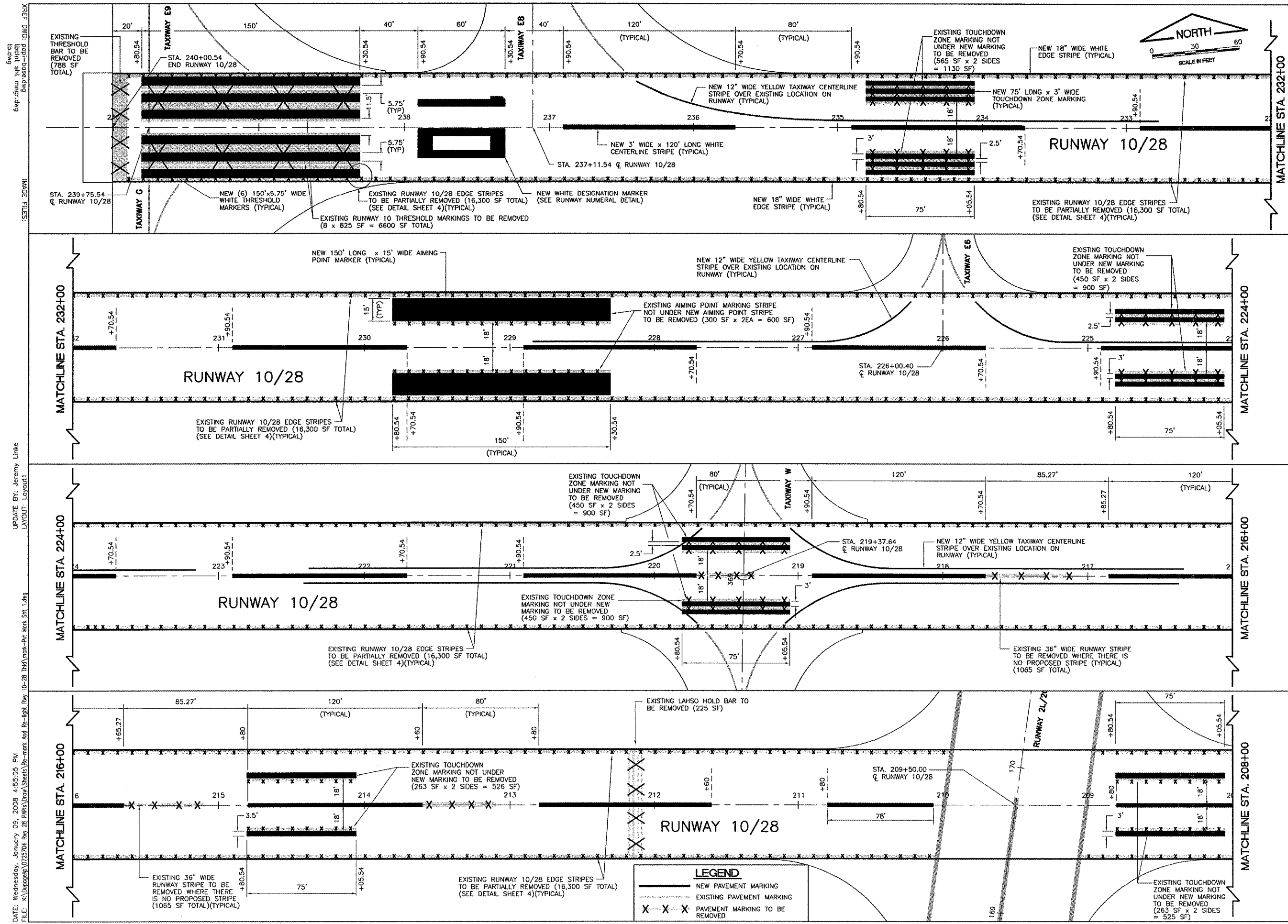
DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
RE-MARK Rwy 10-28 AND RE-LIGHT Rwy END 28 THRESHOLD
SEQUENCE OF CONSTRUCTION PER AC 150-5370-2E
GENERAL NOTES AND DETAILS

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DESIGN BY:	JRL
DRAWN BY:	JRC
CHECKED BY:	DLP
APPROVED BY:	DLP
DATE:	MARCH 7, 2008
JOB No:	07257-04



IL CONTRACT: **DU076**
 IL LETTING ITEM: **2A**
 IL PROJECT: **DPA-3695**
 A.I.P. PROJECT: **3-17-0017-B21**

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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

**DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS**

**RE-MARK RWY 10-28 AND RE-LIGHT RWY END 28 THRESHOLD
 PAVEMENT MARKING PLAN - SHEET 1
 STA. 240+00.54 TO STA. 208+00**

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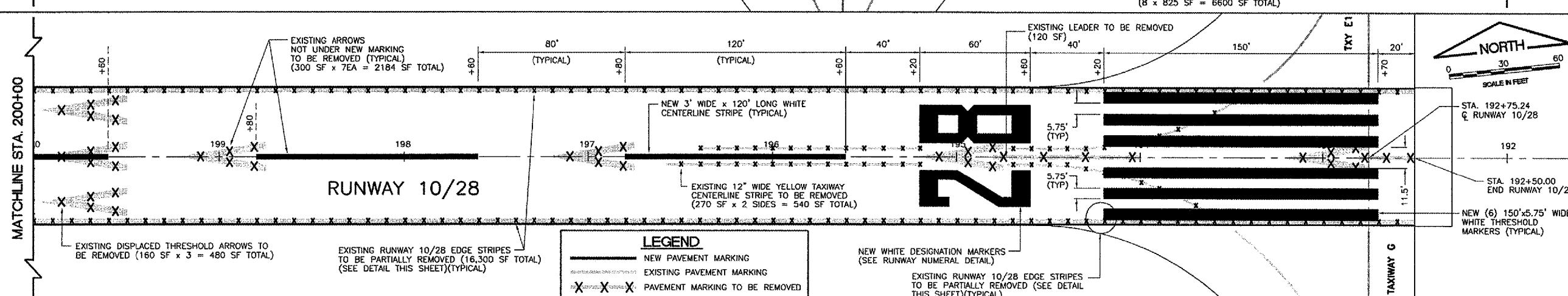
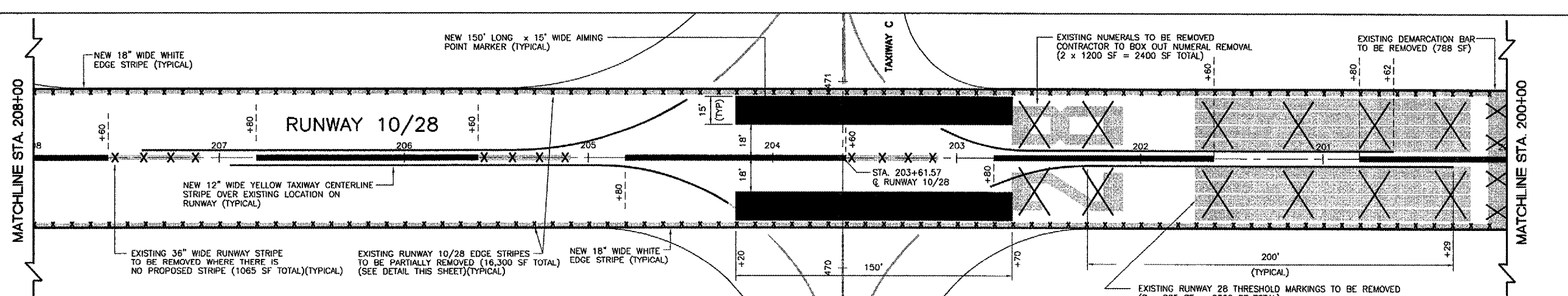
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CHECKED BY:	DLP
APPROVED BY:	DLP
DATE:	MARCH 7, 2008
JOB No:	07257-04

SHEET 5 OF 7 SHEETS

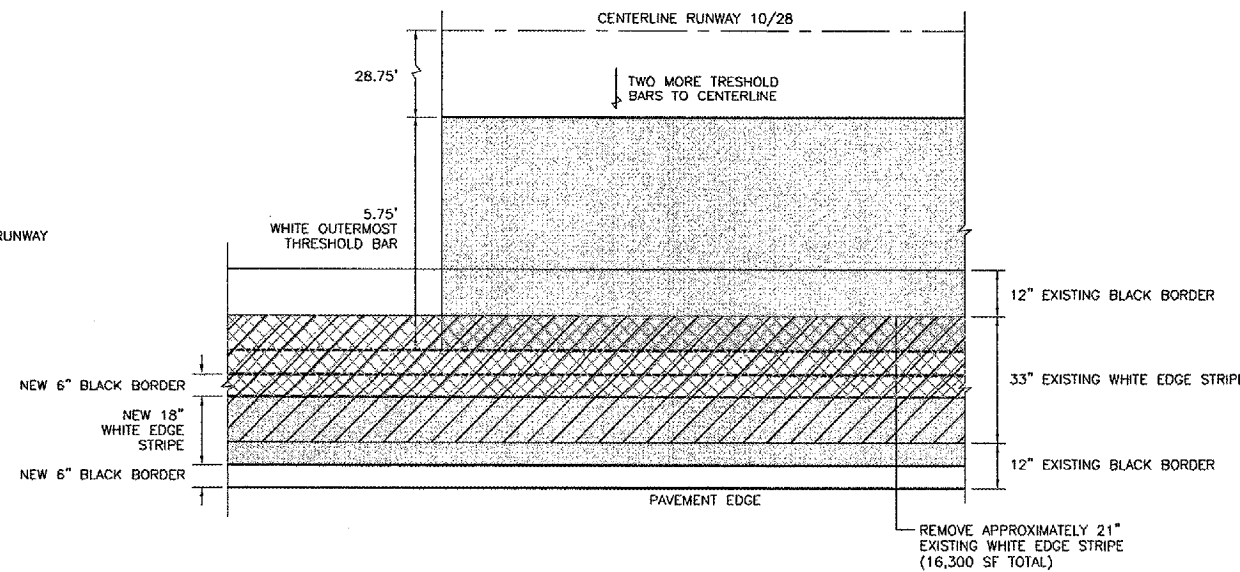
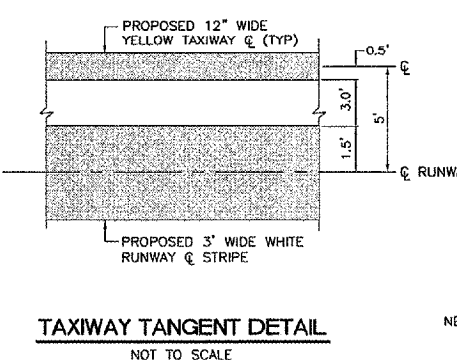
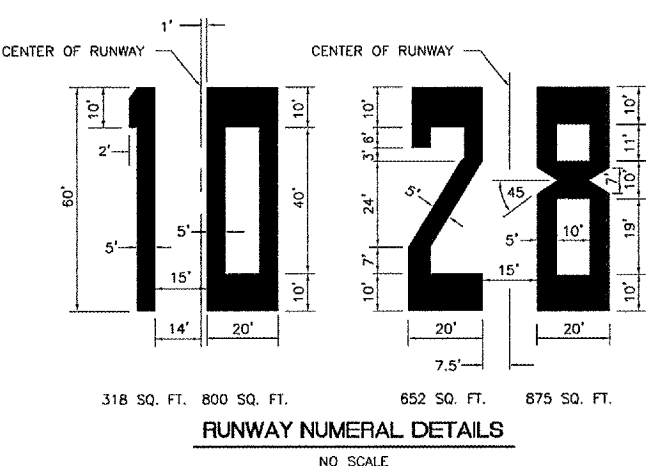
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LEGEND

	NEW PAVEMENT MARKING
	EXISTING PAVEMENT MARKING
	PAVEMENT MARKING TO BE REMOVED



- PAVEMENT MARKING NOTES**
1. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
 2. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE GAPPED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINES.
 3. ALL RUNWAY MARKINGS ARE WHITE WITH A 6" BLACK BORDER. THE 6" BLACK BORDER WILL BE PAID FOR UNDER ITEM AR620525 - PAVEMENT MARKING - BLACK BORDER.
 4. ALL TAXIWAY MARKINGS ARE YELLOW WITH A 6" BLACK BORDER. THE 6" BLACK BORDER WILL BE PAID FOR UNDER ITEM AR620525 - PAVEMENT MARKING - BLACK BORDER.

IL. CONTRACT: **DU076**
 IL. LETTING ITEM: **2A**
 IL. PROJECT: **DPA-3695**
 A.I.P. PROJECT: **3-17-0017-B21**

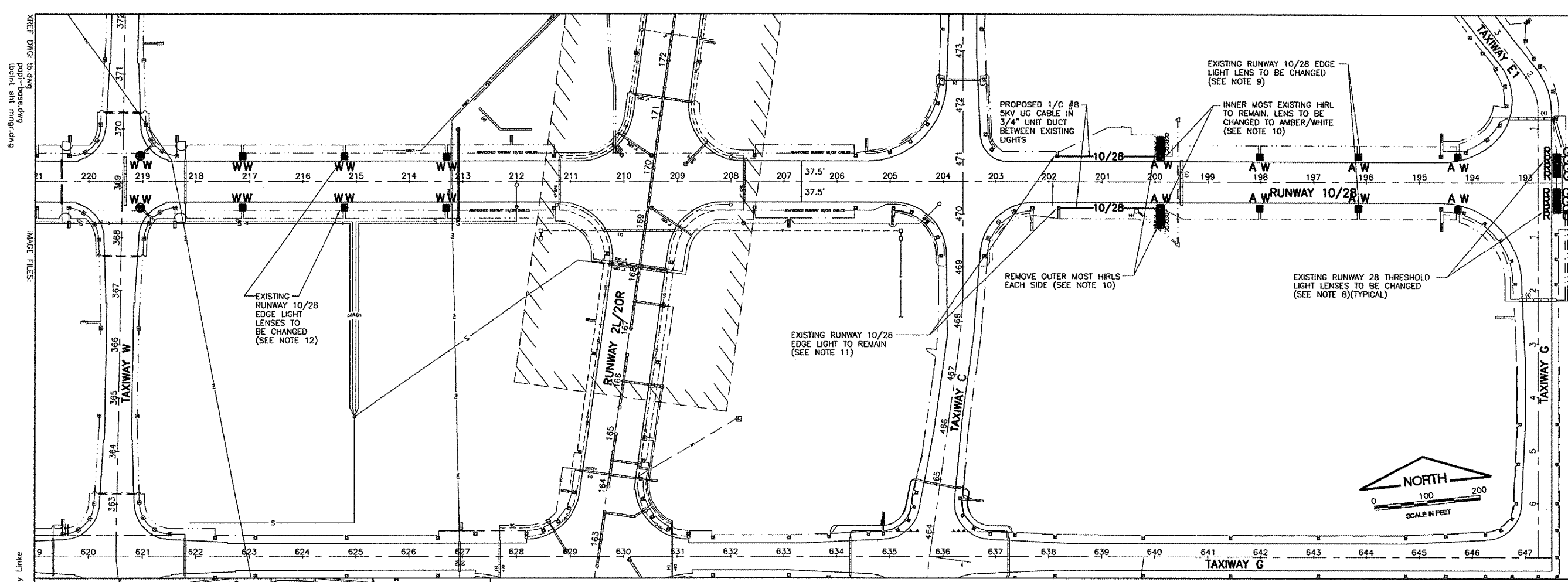
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DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
RE-MARK Rwy 10-28 AND RE-LIGHT Rwy END 28 THRESHOLD
PAVEMENT MARKING PLAN - SHEET 2
STA. 208+00 TO STA. 192+50.00

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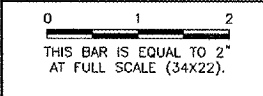
DESIGN BY: JRL
 DRAWN BY: JRO
 CHECKED BY: DLP
 APPROVED BY: DLP
 DATE: MARCH 7, 2008
 JOB No: 07257-04



IL CONTRACT: **DU076**
 IL LETTING ITEM: **2A**
 IL PROJECT: **DPA-3695**
 A.I.P. PROJECT: **3-17-0017-B21**

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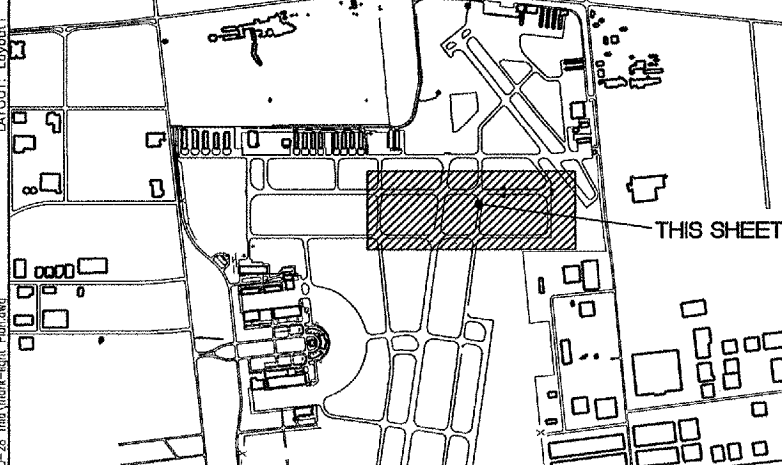
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**DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS**

**RE-MARK Rwy 10-28 AND RE-LIGHT Rwy End 28 THRESHOLD
 LIGHTING PLAN**

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 UPDATE BY: Jeremy Linke
 LAYOUT: Layout1
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 IMAGE FILES:



LEGEND

A	NEW AMBER LENS
G	NEW GREEN LENS
R	NEW RED LENS
W	NEW WHITE LENS
X	REMOVAL ITEM
-10/28-	NEW RUNWAY 10/28 CIRCUIT 1/C #8 5 KV UG CABLE IN UD
---	EXISTING AIRFIELD CIRCUIT
○	EXISTING RUNWAY LIGHT
○/○	RUNWAY LIGHT TO BE MODIFIED

LIGHTING NOTES

1. THE ROUTING OF PROPOSED AND EXISTING CABLE SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE FIELD VERIFIED.
2. PRIOR TO BEGINNING ANY EXCAVATION, THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGEMENT, LOCAL FAA PERSONNEL, UTILITY COMPANY TO COORDINATE CONSTRUCTION ACTIVITIES AND TRENCHING OF UNDERGROUND CABLING.
3. THE CONTRACTOR SHALL VERIFY THAT THE EXISTING RUNWAY LIGHTING CIRCUIT IS OPERATIONAL AT THE END OF EACH WORKING DAY. ANY ELECTRICAL CABLE USED AS A TEMPORARY JUMPER CABLE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. ANY EXISTING CABLE MARKERS THAT ARE DISTURBED SHOULD BE REMOVED AND REINSTALLED AT THE SAME LOCATION. COST SHALL BE INCIDENTAL TO GRADING.
5. THE EXISTING RUNWAY LIGHTING CIRCUIT, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS DIRECTED BY THE ENGINEER. ALL TEMPORARY CABLING AND SPLICING REQUIRED SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
6. AT ANY LOCATION WHERE THE PROPOSED DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
7. ALL SPLICES OF PROPOSED AND EXISTING CABLES IN ELECTRICAL HANDHOLES AND LIGHT BASES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. THE EXISTING EIGHT RUNWAY 10/28 THRESHOLD LIGHTS SHALL BE CHANGED TO EMIT RED LIGHT IN THE DIRECTION OF THE RUNWAY 28 END AND GREEN LIGHT OUTWARD.
9. SIX EXISTING RUNWAY 10/28 EDGE LIGHTS SHALL BE CHANGED TO EMIT WHITE LIGHT IN THE DIRECTION OF RUNWAY 28 THRESHOLD AND AMBER LIGHT IN THE OPPOSITE DIRECTION FROM STA. 194+27.50 TO STA. 198+02.50.
10. SIX OUTER MOST DISPLACED THRESHOLD LIGHTS TO BE REMOVED (3 EACH SIDE). TWO INNER MOST HIRLS TO REMAIN (1 EACH SIDE) AND TO BE CHANGED TO EMIT WHITE LIGHT TOWARD THE 28 END OF THE RUNWAY AND AMBER IN THE OPPOSITE DIRECTION. REPLACE EXISTING LAMPS WITH 120W LAMP.
11. EXISTING RUNWAY EDGE LIGHT LENSES TO REMAIN AMBER/WHITE FROM STA. 201+81.43 TO STA. 211+28.
12. EIGHT EXISTING RUNWAY 10/28 EDGE LIGHTS TO BE CHANGED TO EMIT WHITE LIGHT IN BOTH DIRECTIONS FROM STA. 213+25 TO STA. 219+04.29 INCLUDING TWO IN-PAVEMENT LIGHTS.
13. THE CONTRACTOR SHALL CHANGE LENSES IN A TOTAL OF TWENTY-FOUR LIGHTS ON RUNWAY 10/28. TWENTY TWO BASE MOUNTED LIGHT LENS CHANGES TO BE PAID FOR AS AR800205 RETROFIT EXISTING ELEVATED LIGHT. TWO IN-PAVEMENT LIGHT LENS CHANGES TO BE PAID FOR AS AR800206 RETROFIT EXISTING IN-PAVEMENT LIGHT.

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