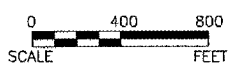


STAGE 1 AIRFIELD STATUS

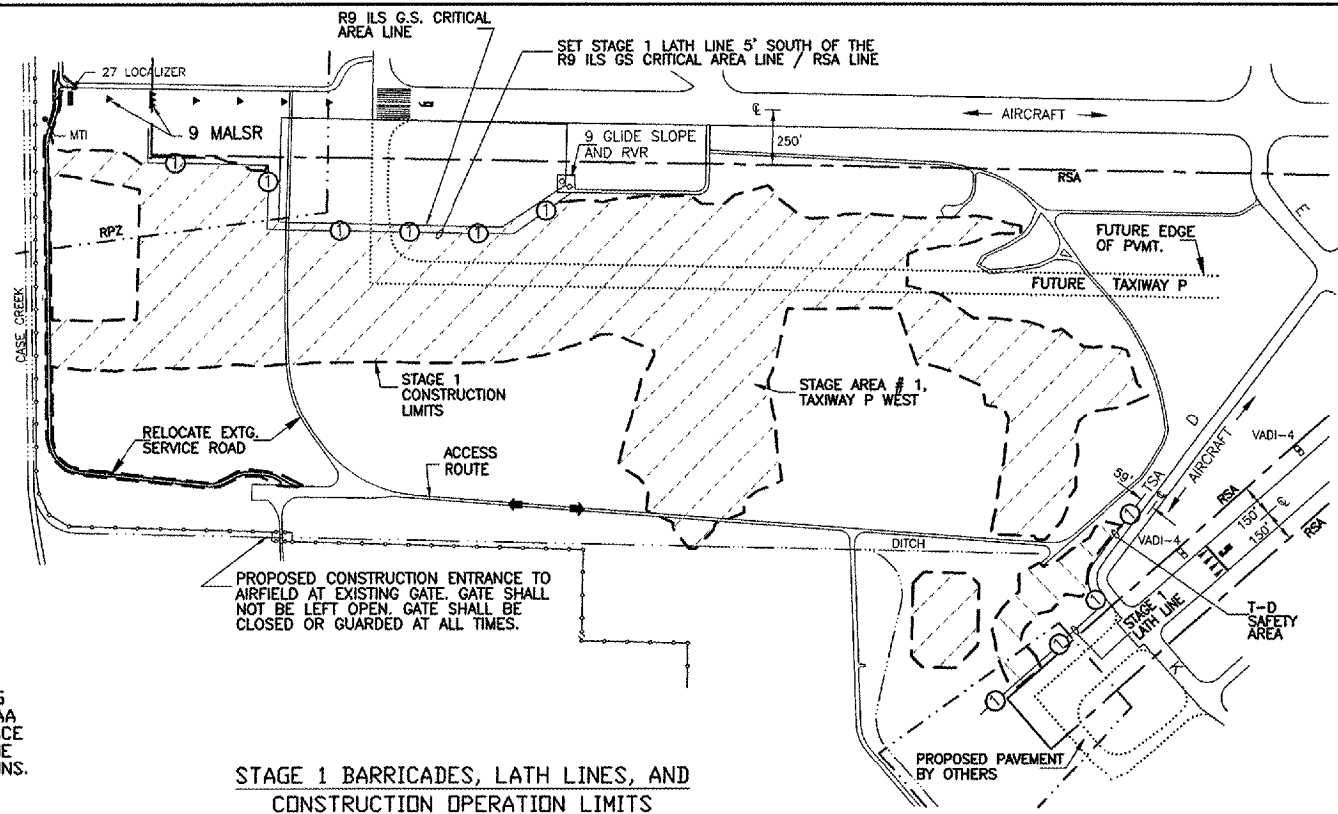
1. ALL AIRFIELD PAVEMENTS OPEN TO AIRCRAFT.
2. CONTRACTOR SHALL NOT ENTER ANY SAFETY AREA.
2. CONTRACTOR SHALL NOT ENTER THE R9 ILS G.S. CRITICAL AREA.



STAGE 1 NOTES:

1. INSTALL STAGE 1 LATH LINES PRIOR TO THE START OF WORK.
2. CONTRACTOR TO REMAIN CLEAR OF ALL AIRFIELD SAFETY AREA.
3. CONTRACTOR TO NOT STOCKPILE MATERIALS ABOVE THE FAA PART 77 CIVIL AIRPORT IMAGINARY SURFACE FOR RUNWAY 9-27 AS DETERMINED BY THE RESIDENT ENGINEER.
4. CONTRACTOR TO REMAIN BELOW (INCLUDING TOP OF CONSTRUCTION EQUIPMENT) THE FAA PART 77 CIVIL AIRPORT IMAGINARY SURFACE FOR RUNWAY 9-27 (AS DETERMINED BY THE RESIDENT ENGINEER) DURING IFR CONDITIONS.

○ = PROPOSED BARRICADES



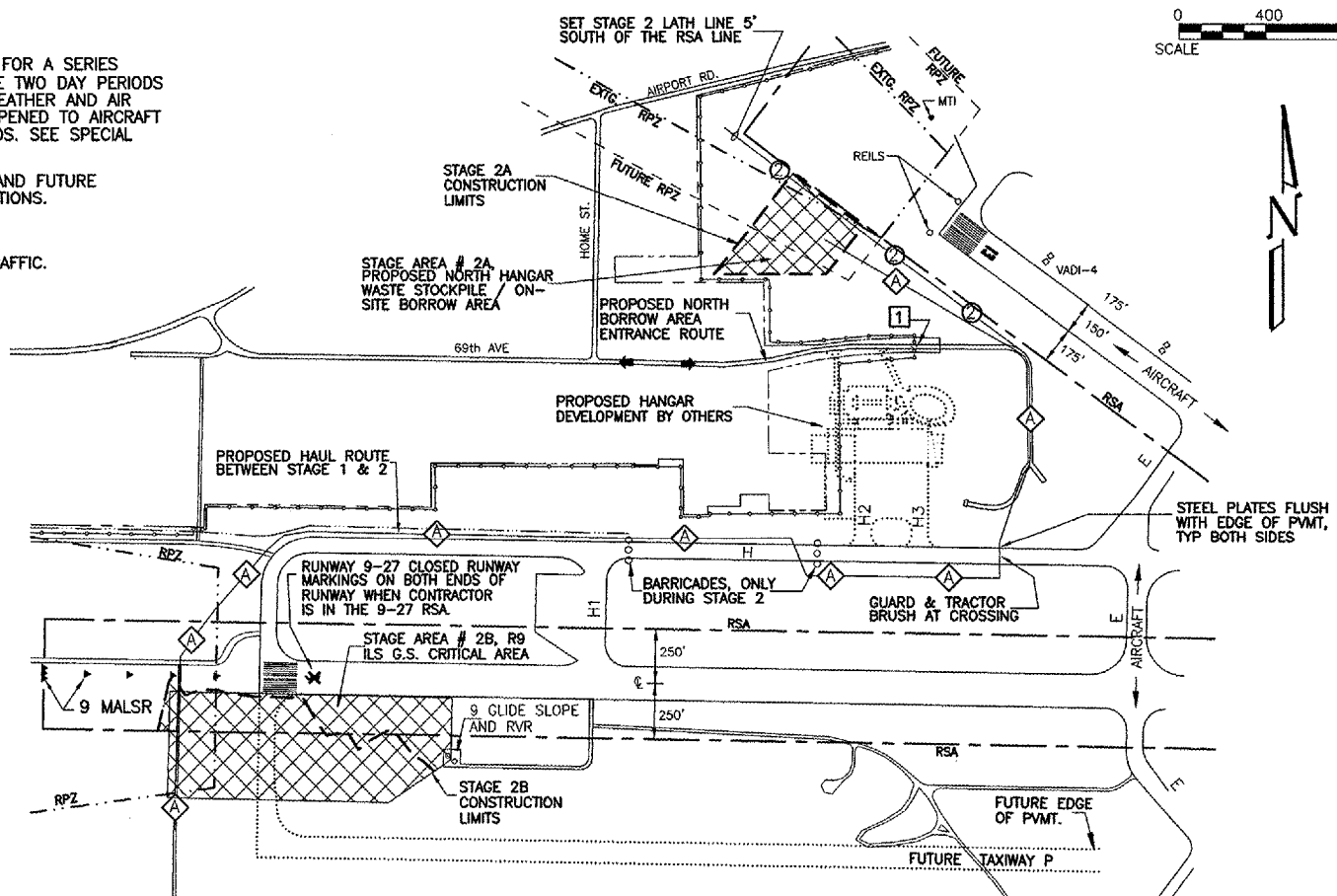
STAGE 1 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 2 AIRFIELD STATUS

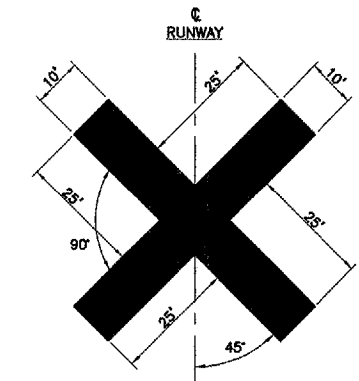
1. RUNWAY 9-27 CLOSED TO AIRCRAFT TRAFFIC FOR A SERIES OF NON-CONSECUTIVE TWO DAY PERIODS. THE TWO DAY PERIODS WILL BE SELECTED BY THE MAA BASED ON WEATHER AND AIR TRAFFIC CONDITIONS. RUNWAY 9-27 TO BE OPENED TO AIRCRAFT TRAFFIC BETWEEN THE TWO DAY WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. TAXIWAY H CLOSED BETWEEN RUNWAY 9-27 AND FUTURE TAXIWAY H2 DURING STAGE 2 HAULING OPERATIONS.
3. RUNWAY 5-23 AND RUNWAY 13-31 OPEN TO AIRCRAFT TRAFFIC.
4. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT TRAFFIC.

STAGE 2 NOTES:

1. COMPLETE STAGE 2 WORK AND OPEN RUNWAY 9-27 TO AIRCRAFT TRAFFIC PRIOR TO STARTING STAGES 3 / 4.
2. RUNWAY 9-27 CLOSED RUNWAY MARKINGS IN PLACE WHEN CONTRACTOR IS IN THE R9-27 RSA.
3. RUNWAY 13-31 SHALL BE OPEN TO AIRCRAFT WHEN RUNWAY 9-27 IS CLOSED.
4. CONTRACTOR SHALL MINIMIZE THE TIME SPENT IN THE RUNWAY 9-27 SAFETY AND G.S. CRITICAL AREAS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
5. ALL PROPOSED WORK IN THE STAGE 2 AREAS SHALL BE BROKEN DOWN INTO UNITS OF WORK WHICH CAN BE ACCOMPLISHED IN SEPARATE TWO DAY PERIODS. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO STOP WORK AND REDPEN THE RUNWAY AT ANY TIME DURING THE WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
6. AT THE END OF EVERY WORK PERIOD, CONTRACTOR SHALL REMOVE ALL ITEMS FROM THE SAFETY / CRITICAL AREAS, REGRADE THE SAFETY / CRITICAL AREAS AND REDPEN THE PAVEMENTS TO AIRCRAFT TRAFFIC. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



STAGE 2 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS



CLOSED RUNWAY MARKING DETAIL

CLOSED RUNWAY MARKINGS NOTES:

1. SOLID CROSS FOR CLOSED RUNWAY IS REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES AND SHALL BE PLACED DIRECTLY OVER THE RUNWAY IDENTIFICATION NUMERALS.
2. THE CONTRACTOR SHALL INSTALL, REMOVE AND REINSTALL THE CROSSES AS REQUIRED BY WORKING CONDITIONS AND AS APPROVED BY THE RESIDENT ENGINEER.
3. COLOR OF ALL CROSSES SHALL BE AVIATION YELLOW.
4. SOLID CROSSES ARE TO BE CONSTRUCTED OF PLYWOOD, CANVAS, OR ANY OTHER APPROVED SOLID MATERIALS AND SHALL BE ADHERED TO THE RUNWAY IN A MANNER ACCEPTABLE TO THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN THE CROSSES IN EXCELLENT CONDITION.

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