

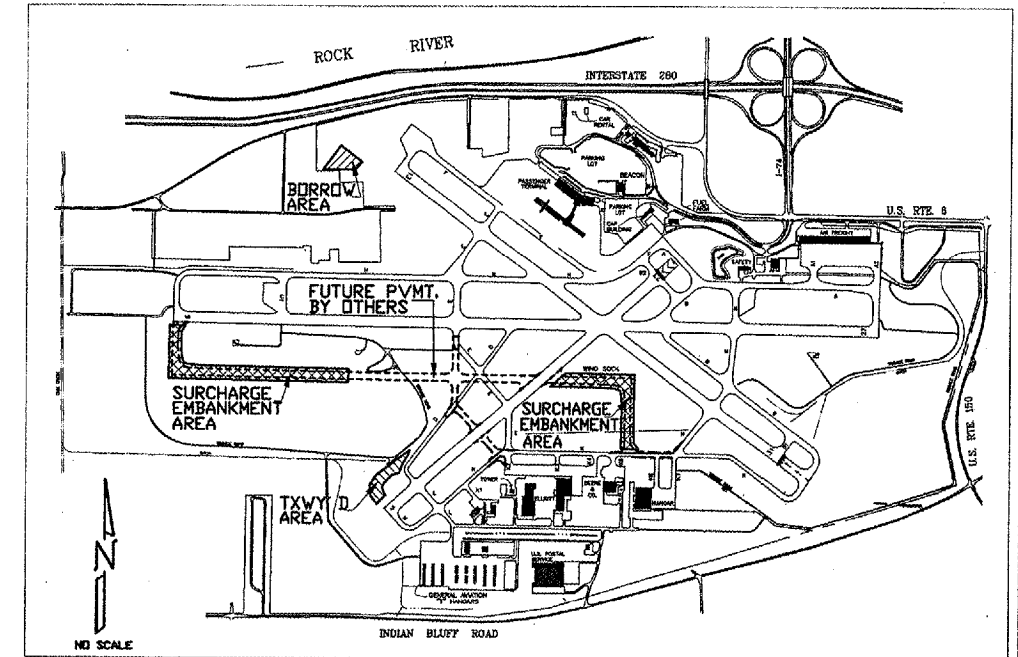
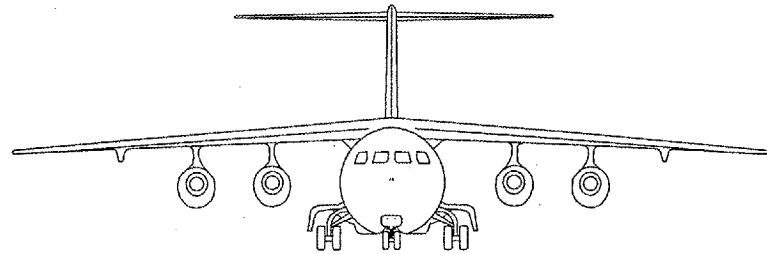
ITEM 8A

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF AERONAUTICS
CONSTRUCTION PLANS

FOR

QUAD-CITY
INTERNATIONAL
AIRPORT

ROCK ISLAND COUNTY, ILLINOIS



PROJECT LOCATION MAP

WARNING



CALL BEFORE
YOU DIG

TAXIWAY P, PHASE I - EARTHWORK

EXCAVATION AND CONSTRUCTION OF 8' SURCHARGE EMBANKMENT FOR FUTURE TAXIWAY P (6,502' X 100'), WEST CONNECTOR TAXIWAY (645' X 75'), AND EAST CONNECTOR TAXIWAY (980' X 75') ALONG WITH ASSOCIATED UTILITY ADJUSTMENTS AND TURFING.

ILLINOIS PROJECT MLI-3790
A.I.P. PROJECT NO. 3-17-0068-XX
AIRPORT CLASSIFICATION - AIR CARRIER

AIRCRAFT APPROACH CATEGORY - C
AIRPLANE DESIGN GROUP - III
LATITUDE 41°26'52.4", LONGITUDE 90°30'33.9", ELEVATION 589'M.S.L.

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CONSULTING ENGINEERS
MISSMAN, STANLEY & ASSOC.

ENGINEER'S SIGNATURE *Dennis R. Martin*

SEALED & SIGNED 4/18/08

DATE OF LICENSE EXPIRATION 11/30/09

METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY, ILLINOIS

Bruce Carter

4/18/08 DATE APPROVED

APRIL 15, 2008
OFFICIAL DATE OF PLANS

PREPARED BY
MISSMAN, STANLEY & ASSOCIATES
Consulting Civil Engineers
ROCK ISLAND, ILLINOIS

SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITIES	
			AS AWARDED	AS BUILT
BASE BID:				
AR107920	REPLACE WIND CONE	EACH	1	
AR108658	3/C #8 600 V UG CABLE IN UD	L.F.	700	
AR110214	4" STEEL DUCT, DIRECT BURY	L.F.	95	
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	236,000	
AR152441	ON-SITE BORROW	C.Y.	60,800	
AR152515	SUBGRADE UNDERCUT	C.Y.	13,435	
AR152540	SOIL STABILIZATION FABRIC	S.Y.	11,710	
AR156500	TEMPORARY EROSION CONTROL	L.S.	1	
AR156543	RIPRAP - GRADATION NO. 3	S.Y.	7,500	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	5,360	
AR209511	CRUSHED AGGREGATE BASE, (CA-1)	TON	100	
AR209600	GEOTEXTILE FABRIC	S.Y.	4,400	
AR701430	30" RCP, CLASS III	L.F.	168	
AR701442	42" RCP, CLASS III	L.F.	218	
AR701512	12" RCP, CLASS IV	L.F.	350	
AR701515	15" RCP, CLASS IV	L.F.	272	
AR701518	18" RCP, CLASS IV	L.F.	467	
AR701524	24" RCP, CLASS IV	L.F.	705	
AR701710	RCEP SPAN 23 RISE 14	L.F.	213	
AR701713	RCEP SPAN 30 RISE 19	L.F.	366	
AR701719	RCEP SPAN 38 RISE 24	L.F.	790	
AR701900	REMOVE PIPE	L.F.	75	
AR705508	8" PERFORATED UNDERDRAIN	L.F.	820	
AR705610	CONCRETE HEADWALL FOR UNDERDRAIN	EACH	25	
AR751410	INLET	EACH	4	
AR751411	INLET - TYPE A	EACH	1	
AR751415	INLET - SPECIAL	EACH	1	
AR751540	MANHOLE 4'	EACH	3	
AR751550	MANHOLE 5'	EACH	1	
AR751560	MANHOLE 6'	EACH	1	
AR751903	REMOVE MANHOLE	EACH	3	
AR752424	PRECAST REINFORCED CONC. FES 24"	EACH	6	
AR752442	PRECAST REINFORCED CONC. FES 42"	EACH	2	
AR752524	GRATING FOR CONC. FES 24"	EACH	2	
AR752730	P R CONC. FES EQ. ROUND SIZE 30"	EACH	1	
AR770508	8" SANITARY SEWER	L.F.	1,288	
AR770704	SANITARY MANHOLE 4'	EACH	2	
AR770925	REPLACE SANITARY MANHOLE	EACH	1	
AR801604	SETTLEMENT PLATES	EACH	10	
AR901510	SEEDING	ACRE	109	
AR908513	MULCHING - METHOD 3	ACRE	109	
AR908520	EXCELSIOR BLANKET	S.Y.	1,500	
ADDITIVE ALTERNATE 1 (TAXIWAY D AREA):				
AS152420	COMMON EXCAVATION	C.Y.	12,002	
AS901510	SEEDING	ACRE	3	
AS908513	MULCHING - METHOD 3	ACRE	3	
ADDITIVE ALTERNATE 2 (RUNWAY 5 WASTE STORAGE AREA):				
AT152420	COMMON EXCAVATION	C.Y.	30,366	
AT901510	SEEDING	ACRE	3	
AT908513	MULCHING - METHOD 3	ACRE	3	

GENERAL NOTES:

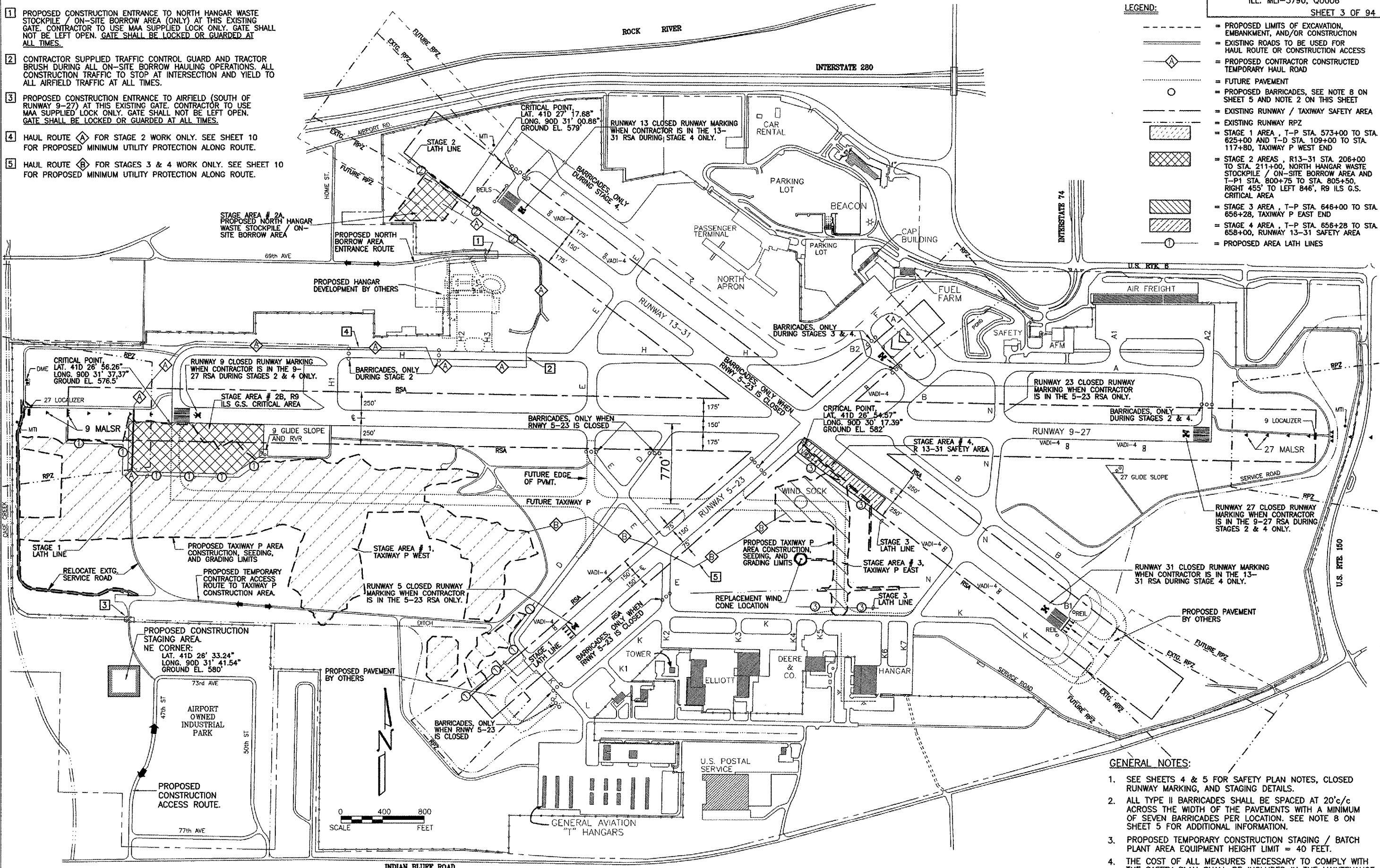
1. THE CONTRACTOR SHALL SALVAGE EXISTING AIRFIELD LIGHTING EQUIPMENT AS DETAILED IN THE CONSTRUCTION PLANS AND SPECIAL PROVISIONS PRIOR TO THE START OF EARTHWORK AND/OR PAVING ACTIVITIES. SALVAGED EQUIPMENT SHALL BE CLEANED AND REUSED ON THE AIRPORT OR DELIVERED TO THE METROPOLITAN AIRPORT AUTHORITY.
2. CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS APPROVED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
3. THE CONTRACTOR SHALL EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALES, AS REQUIRED BY THE RESIDENT ENGINEER, TO CONTROL STORM WATER RUN-OFF.
4. THE CONTRACT 152 - EARTHWORK ITEMS SHALL INCLUDE ALL COSTS ASSOCIATED WITH EXCAVATION OF SOILS, HAULING OF SOILS, STOCKPILING SOILS, INSTALLATION OF SOILS, COMPACTION OF SOILS, GRADING OF SOILS, INSTALLATION AND REMOVAL OF HAUL ROADS OR ROUTES, RESTORATION OF HAUL ROADS OR ROUTES, DISPOSAL OF WASTE SOILS, CLEANING OF PAVEMENTS, AND ALL OTHER ITEMS THAT ARE REQUIRED TO COMPLETE THE EARTHWORK. THESE ITEMS SHALL BE PAID FOR BASED UPON THE CUBIC YARDS OF MATERIALS REMOVED AND/OR REQUIRED AS ACCEPTED BY THE ENGINEER. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
5. AFTER CONSTRUCTION HAS BEEN COMPLETED, THE CONTRACTOR SHALL SEED AND HYDRAULIC MULCH ALL DISTURBED AREAS PER SPECIAL PROVISIONS 901 AND 908. ONLY SEEDING AND MULCHING AREAS WITHIN THE LIMITS OF CONSTRUCTION/SEEDING WILL BE ELIGIBLE FOR PAYMENT UNDER THESE CONTRACT PAY ITEMS. AREAS DISTURBED OUTSIDE OF THE LIMITS OF CONSTRUCTION/SEEDING SHALL BE SEEDED AND MULCHED BY THE CONTRACTOR PER SPECIAL PROVISION 901/908, BUT SHALL NOT BE MEASURED FOR PAYMENT.
6. ITEM AR908513 MULCHING - METHOD 3 SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE IDOT-DOA SUPPLEMENTAL SPECIFICATIONS AND IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, METHOD 3 (HYDRAULIC MULCH).
7. ITEM AR908520 EXCELSIOR BLANKET SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, EROSION CONTROL BLANKET (EXCELSIOR BLANKET). THE LOCATION OF THE PROPOSED EXCELSIOR BLANKET SHALL BE DETERMINED BY THE RESIDENT ENGINEER, IN THE FIELD, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

NUMBERED LEGEND:

- 1 PROPOSED CONSTRUCTION ENTRANCE TO NORTH HANGAR WASTE STOCKPILE / ON-SITE BORROW AREA (ONLY) AT THIS EXISTING GATE. CONTRACTOR TO USE MAA SUPPLIED LOCK ONLY. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES.
- 2 CONTRACTOR SUPPLIED TRAFFIC CONTROL GUARD AND TRACTOR BRUSH DURING ALL ON-SITE BORROW HAULING OPERATIONS. ALL CONSTRUCTION TRAFFIC TO STOP AT INTERSECTION AND YIELD TO ALL AIRFIELD TRAFFIC AT ALL TIMES.
- 3 PROPOSED CONSTRUCTION ENTRANCE TO AIRFIELD (SOUTH OF RUNWAY 9-27) AT THIS EXISTING GATE. CONTRACTOR TO USE MAA SUPPLIED LOCK ONLY. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES.
- 4 HAUL ROUTE (A) FOR STAGE 2 WORK ONLY. SEE SHEET 10 FOR PROPOSED MINIMUM UTILITY PROTECTION ALONG ROUTE.
- 5 HAUL ROUTE (B) FOR STAGES 3 & 4 WORK ONLY. SEE SHEET 10 FOR PROPOSED MINIMUM UTILITY PROTECTION ALONG ROUTE.

LEGEND:

- - - - - PROPOSED LIMITS OF EXCAVATION, EMBANKMENT, AND/OR CONSTRUCTION
- - - - - EXISTING ROADS TO BE USED FOR HAUL ROUTE OR CONSTRUCTION ACCESS
- ◇ PROPOSED CONTRACTOR CONSTRUCTED TEMPORARY HAUL ROAD
- FUTURE PAVEMENT
- - - - - PROPOSED BARRICADES, SEE NOTE 8 ON SHEET 5 AND NOTE 2 ON THIS SHEET
- - - - - EXISTING RUNWAY / TAXIWAY SAFETY AREA
- - - - - EXISTING RUNWAY RPZ
- ▨ STAGE 1 AREA, T-P STA. 573+00 TO STA. 625+00 AND T-D STA. 109+00 TO STA. 117+80, TAXIWAY P WEST END
- ▩ STAGE 2 AREAS, R13-31 STA. 206+00 TO STA. 211+00, NORTH HANGAR WASTE STOCKPILE / ON-SITE BORROW AREA AND T-P1 STA. 800+75 TO STA. 805+50, RIGHT 455' TO LEFT 846', R9 ILS G.S. CRITICAL AREA
- ▨ STAGE 3 AREA, T-P STA. 646+00 TO STA. 656+28, TAXIWAY P EAST END
- ▨ STAGE 4 AREA, T-P STA. 656+00 TO STA. 658+00, RUNWAY 13-31 SAFETY AREA
- ① PROPOSED AREA LATH LINES



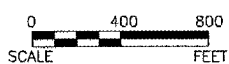
GENERAL NOTES:

- 1. SEE SHEETS 4 & 5 FOR SAFETY PLAN NOTES, CLOSED RUNWAY MARKING, AND STAGING DETAILS.
- 2. ALL TYPE II BARRICADES SHALL BE SPACED AT 20'C/C ACROSS THE WIDTH OF THE PAVEMENTS WITH A MINIMUM OF SEVEN BARRICADES PER LOCATION. SEE NOTE 8 ON SHEET 5 FOR ADDITIONAL INFORMATION.
- 3. PROPOSED TEMPORARY CONSTRUCTION STAGING / BATCH PLANT AREA EQUIPMENT HEIGHT LIMIT = 40 FEET.
- 4. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

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STAGE 1 AIRFIELD STATUS

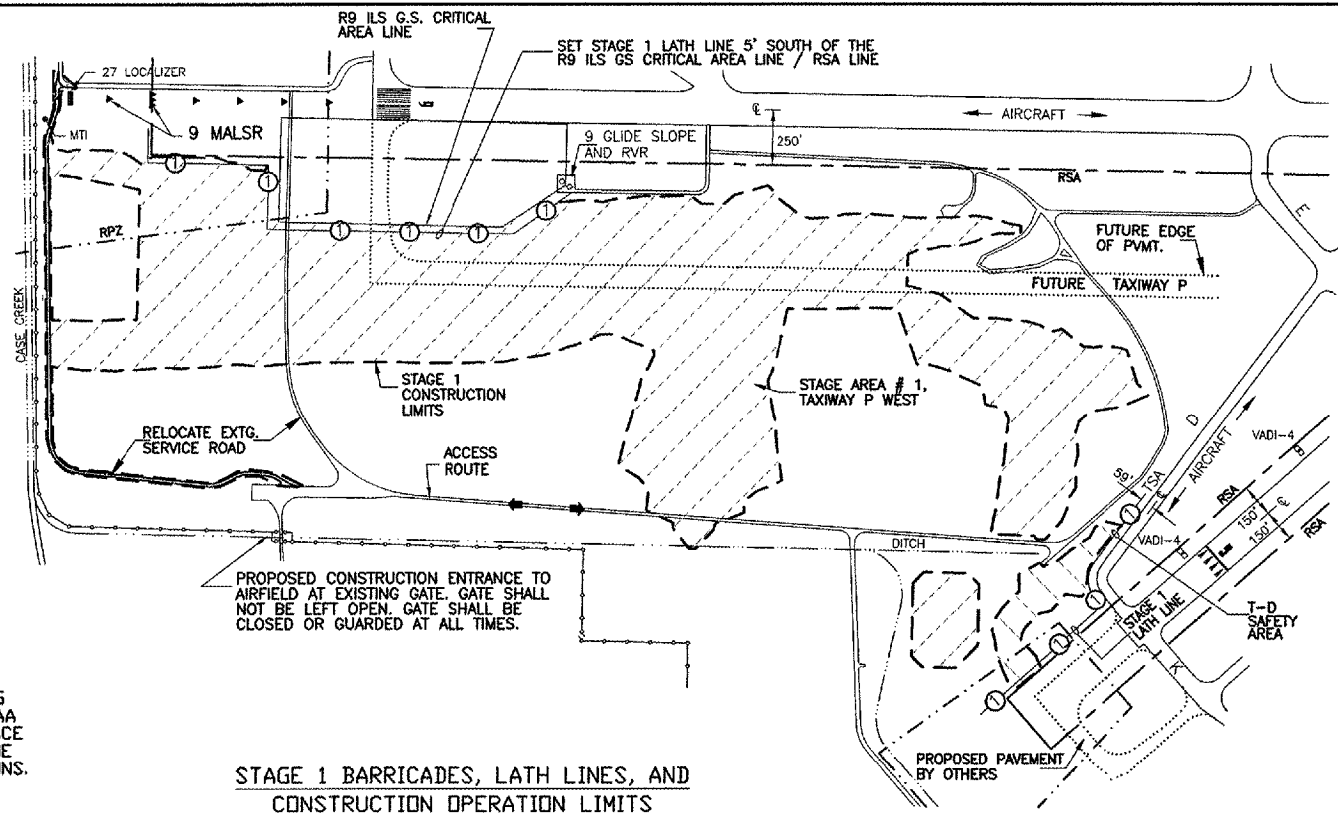
1. ALL AIRFIELD PAVEMENTS OPEN TO AIRCRAFT.
2. CONTRACTOR SHALL NOT ENTER ANY SAFETY AREA.
2. CONTRACTOR SHALL NOT ENTER THE R9 ILS G.S. CRITICAL AREA.



STAGE 1 NOTES:

1. INSTALL STAGE 1 LATH LINES PRIOR TO THE START OF WORK.
2. CONTRACTOR TO REMAIN CLEAR OF ALL AIRFIELD SAFETY AREA.
3. CONTRACTOR TO NOT STOCKPILE MATERIALS ABOVE THE FAA PART 77 CIVIL AIRPORT IMAGINARY SURFACE FOR RUNWAY 9-27 AS DETERMINED BY THE RESIDENT ENGINEER.
4. CONTRACTOR TO REMAIN BELOW (INCLUDING TOP OF CONSTRUCTION EQUIPMENT) THE FAA PART 77 CIVIL AIRPORT IMAGINARY SURFACE FOR RUNWAY 9-27 (AS DETERMINED BY THE RESIDENT ENGINEER) DURING IFR CONDITIONS.

○ = PROPOSED BARRICADES



STAGE 1 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

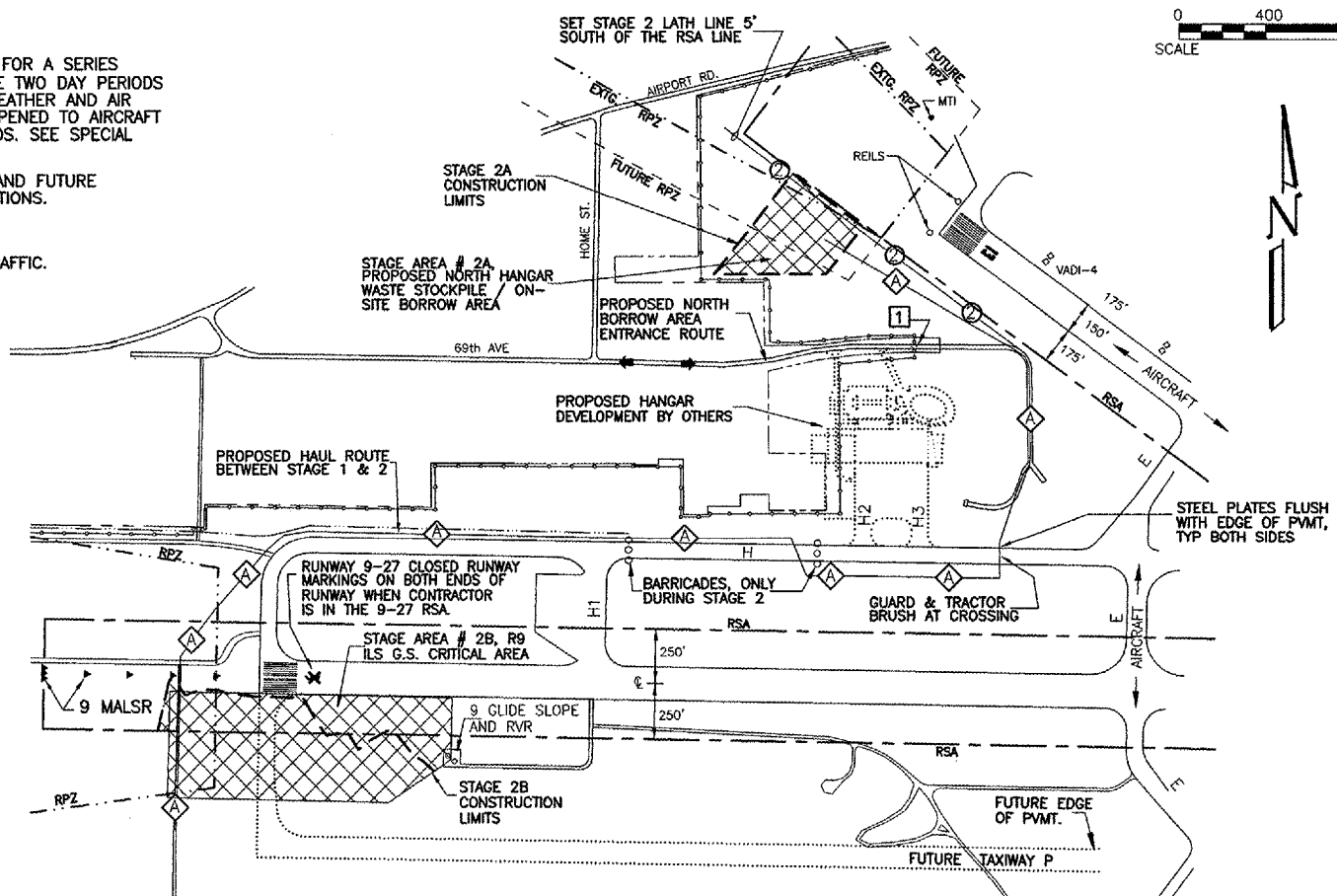
STAGE 2 AIRFIELD STATUS

1. RUNWAY 9-27 CLOSED TO AIRCRAFT TRAFFIC FOR A SERIES OF NON-CONSECUTIVE TWO DAY PERIODS. THE TWO DAY PERIODS WILL BE SELECTED BY THE MAA BASED ON WEATHER AND AIR TRAFFIC CONDITIONS. RUNWAY 9-27 TO BE OPENED TO AIRCRAFT TRAFFIC BETWEEN THE TWO DAY WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. TAXIWAY H CLOSED BETWEEN RUNWAY 9-27 AND FUTURE TAXIWAY H2 DURING STAGE 2 HAULING OPERATIONS.
3. RUNWAY 5-23 AND RUNWAY 13-31 OPEN TO AIRCRAFT TRAFFIC.
4. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT TRAFFIC.

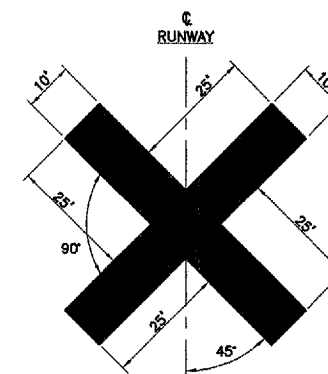


STAGE 2 NOTES:

1. COMPLETE STAGE 2 WORK AND OPEN RUNWAY 9-27 TO AIRCRAFT TRAFFIC PRIOR TO STARTING STAGES 3 / 4.
2. RUNWAY 9-27 CLOSED RUNWAY MARKINGS IN PLACE WHEN CONTRACTOR IS IN THE R9-27 RSA.
3. RUNWAY 13-31 SHALL BE OPEN TO AIRCRAFT WHEN RUNWAY 9-27 IS CLOSED.
4. CONTRACTOR SHALL MINIMIZE THE TIME SPENT IN THE RUNWAY 9-27 SAFETY AND G.S. CRITICAL AREAS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
5. ALL PROPOSED WORK IN THE STAGE 2 AREAS SHALL BE BROKEN DOWN INTO UNITS OF WORK WHICH CAN BE ACCOMPLISHED IN SEPARATE TWO DAY PERIODS. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO STOP WORK AND REDPEN THE RUNWAY AT ANY TIME DURING THE WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
6. AT THE END OF EVERY WORK PERIOD, CONTRACTOR SHALL REMOVE ALL ITEMS FROM THE SAFETY / CRITICAL AREAS, REGRADE THE SAFETY / CRITICAL AREAS AND REDPEN THE PAVEMENTS TO AIRCRAFT TRAFFIC. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



STAGE 2 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS



CLOSED RUNWAY MARKING DETAIL

CLOSED RUNWAY MARKINGS NOTES:

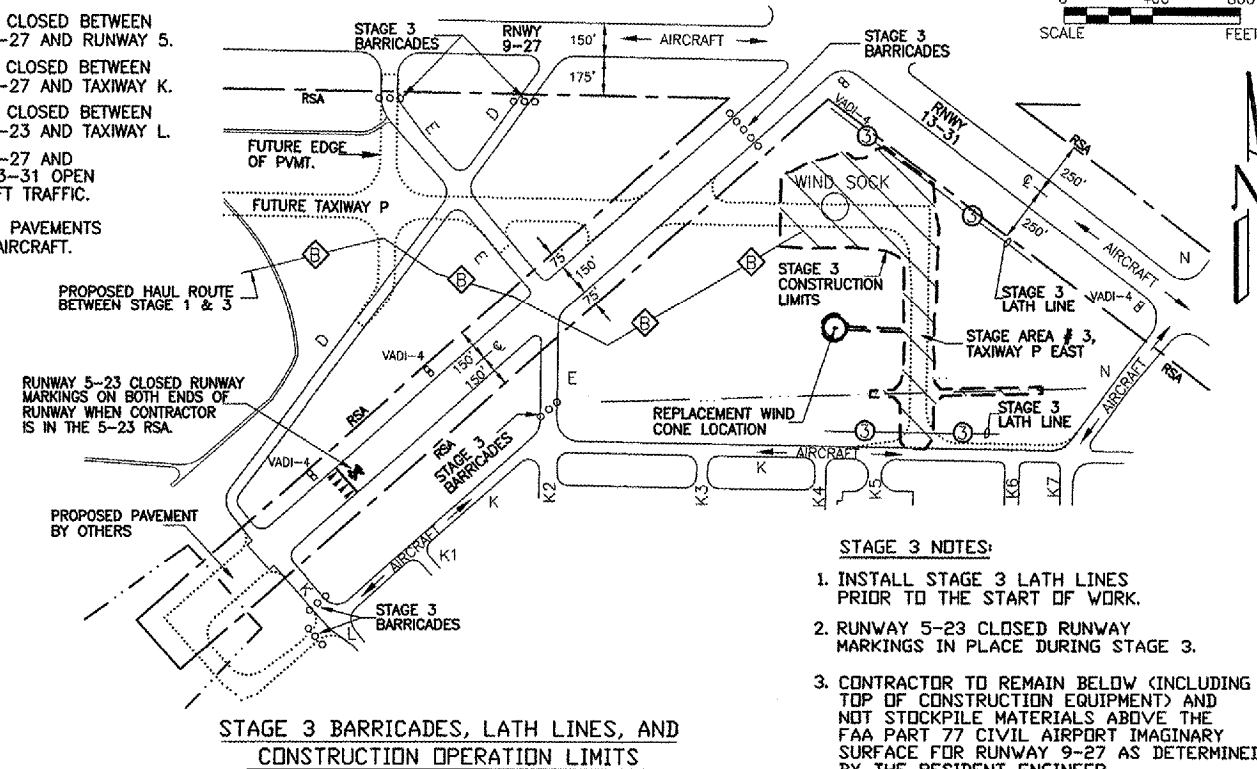
1. SOLID CROSS FOR CLOSED RUNWAY IS REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES AND SHALL BE PLACED DIRECTLY OVER THE RUNWAY IDENTIFICATION NUMERALS.
2. THE CONTRACTOR SHALL INSTALL, REMOVE AND REINSTALL THE CROSSES AS REQUIRED BY WORKING CONDITIONS AND AS APPROVED BY THE RESIDENT ENGINEER.
3. COLOR OF ALL CROSSES SHALL BE AVIATION YELLOW.
4. SOLID CROSSES ARE TO BE CONSTRUCTED OF PLYWOOD, CANVAS, OR ANY OTHER APPROVED SOLID MATERIALS AND SHALL BE ADHERED TO THE RUNWAY IN A MANNER ACCEPTABLE TO THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN THE CROSSES IN EXCELLENT CONDITION.

SAFETY PLAN NOTES:

1. THE METROPOLITAN AIRPORT AUTHORITY (MAA) SHALL DETERMINE WHEN AND WHERE THE CONTRACTOR WILL BE ALLOWED TO WORK. THE CONTRACTOR SHALL BE PREPARED TO EXIT THE RESTRICTED AREAS, SAFETY AREAS AND/OR AIR FIELD AT ALL TIMES WHEN DIRECTED TO DO SO BY THE MAA. ALL PAVEMENTS SHALL BE OPEN TO AIRCRAFT TRAFFIC UNLESS "NOTAMED" OTHERWISE DURING THE CONSTRUCTION OF THIS PROJECT. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION ON WORKING RESTRICTIONS AND CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF ALL BARRICADES AND LATH LINES AS SHOWN, PRIOR TO ANY EQUIPMENT ENTERING THE AIRFIELD OUTSIDE OF THE CONSTRUCTION STAGING AREA.
2. SOLID CLOSED RUNWAY MARKING CROSSES ARE REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES. SEE SPECIAL PROVISIONS FOR DETAILS OF CROSSES.
3. THE CONTRACTOR SHALL NOT TRAVEL IN OR THROUGH THE RESTRICTED AREAS AND/OR SAFETY AREAS UNLESS PERMISSION IS RECEIVED AND CONTACT HAS BEEN MADE WITH THE FAA CONTROL TOWER.
4. THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PROPOSED CONSTRUCTION STAGING AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY CONTRACTOR'S MARKED VEHICLES AND EQUIPMENT SHALL BE ALLOWED ON THE AIRFIELD. ALL CONSTRUCTION VEHICLES AND EQUIPMENT, EXCEPT THE PAVING TRAIN, SHALL BE PARKED IN THE CONSTRUCTION STAGING AREA DURING ALL NON-WORKING HOUR. THE PAVING TRAIN MAY BE PARKED ON THE AIRFIELD OUTSIDE ALL RESTRICTED AREAS IN A LOCATION AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL LOCATE HIS TRAILER, THE ENGINEER'S FIELD OFFICE, AND ALL OTHER NECESSARY FACILITIES AND MATERIALS IN THE PROPOSED CONSTRUCTION STAGING AREA. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONSTRUCT WHATEVER ACCESS ROAD HE DEEMS NECESSARY BETWEEN THE EXISTING ROADS AND THE CONSTRUCTION AREAS. OVERNIGHT PARKING OF EMPLOYEE VEHICLES IN THE CONSTRUCTION STAGING AREA WILL NOT BE ALLOWED UNLESS AUTHORIZED BY THE MAA.
5. THE CONTRACTOR SHALL PROCURE ENOUGH QUAD CITY AIRPORT SECURITY/IDENTIFICATION BADGES FOR HIS EMPLOYEES AND SUBCONTRACTOR EMPLOYEES FROM THE AIRPORT AUTHORITY TO GUARANTEE AT LEAST ONE MEMBER OF EACH CONSTRUCTION CREW WILL HAVE A BADGE. ANY CONSTRUCTION CREW WITHOUT A BADGED MEMBER SHALL NOT BE ALLOWED ON THE AIRFIELD SITE. ALL INDIVIDUALS WHO ARE ISSUED SECURITY BADGES MUST CORRECTLY WEAR THEIR OWN BADGE WHILE ON THE AIRFIELD. BADGES MAY BE OBTAINED, AFTER MEETING SECURITY REQUIREMENTS, FROM THE SECURITY OFFICE AT THE QCI AIRPORT. A FIFTY DOLLAR (\$50.00) REFUNDABLE DEPOSIT IS REQUIRED FOR EACH BADGE.
6. THE CONTRACTOR SHALL USE THE DESIGNATED HAUL ROUTES, CONSTRUCTION STAGING AREA, AND ENTRANCE TO THE AIRFIELD AS SHOWN ON THE SAFETY PLAN SHEET. NO CONSTRUCTION TRAFFIC SHALL BE ALLOWED ON THE AIRFIELD OUTSIDE THE HAUL ROUTES AND WORK AREAS UNLESS OTHERWISE AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL SUPPLY AND INSTALL TEMPORARY LOCKS ON EXISTING GATES AT THE PROPOSED CONSTRUCTION ENTRANCE. THE CONTRACTOR SHALL BE RESPONSIBLE TO KEEP THE CONSTRUCTION ENTRANCE GATE CLOSED AT ALL TIMES. VIOLATIONS ARE SUBJECT TO FINES/PENALTIES AND THE CONTRACTOR SHALL PAY ANY FINES INCURRED, INCLUDING FINES INCURRED BY THE RESIDENT ENGINEER AND/OR MAA DUE TO THE CONTRACTOR'S NEGLIGENCE. ALL VEHICULAR TRAFFIC SHALL BE KEPT TO A MINIMUM. ALL VEHICLES ON THE APRONS, RAMPS, TAXIWAYS, OR RUNWAYS REQUIRE THE APPROVAL OF THE RESIDENT ENGINEER / MAA.
7. THE CONTRACTOR AND EACH SUBCONTRACTOR SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WORK OR EQUIPMENT. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
8. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO INSTALL TYPE II BARRICADES EQUIPPED WITH FLASHING RED LIGHTS AND 20" X 20" ORANGE FLAGS AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR WILL FURNISH, MAINTAIN, AND MOVE THE BARRICADES AS REQUIRED BY THE RESIDENT ENGINEER. THE BARRICADES SHALL BE SUFFICIENTLY WEIGHTED WITH SANDBAGS OR OTHER APPROPRIATE METHOD TO WITHSTAND HIGH WINDS AND/OR JET BLAST WITHOUT DISLOCATION. BARRICADES SHALL BE CHECKED DAILY BY THE CONTRACTOR FOR PROPER PLACEMENT, ADEQUATE BALLAST, PROPER LIGHTING, PROPER FLAGGING AND WORKING BATTERIES. ALL DISCREPANCIES SHALL BE CORRECTED IMMEDIATELY. BARRICADES SHALL BE RELOCATED UPON THE COMPLETION OF EACH STAGE OF CONSTRUCTION. COST TO BE INCLUDED IN MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
9. WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRFIELD, THEY SHALL BE PROPERLY MARKED. THE MARKING SHALL CONSIST OF A THREE FOOT (3') SQUARE FLAG WITH A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN ONE FOOT (1') ON EACH SIDE, DISPLAYED IN FULL VIEW ABOVE THE VEHICLE OR EQUIPMENT. EACH VEHICLE SHALL HAVE A FLASHING YELLOW LIGHT MOUNTED ON TOP OF THE ROOF.
10. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE THE CONSTRUCTION STAGING AREA, SERVICE ROADS, ACCESS ROADS, AND HAUL ROADS TO THEIR ORIGINAL CONDITIONS FOLLOWING COMPLETION OF CONSTRUCTION. THIS MAY INCLUDE, BUT NOT BE LIMITED TO, REGRADING, FERTILIZING, SEEDING AND MULCHING OF EARTH SURFACES AND/OR REGRADING, GRAVELING & SEAL COATING OF TREATED SURFACES, AS REQUIRED, TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL GROUND SURFACES, GRAVEL ROADS, PAVEMENTS, AND OTHER FACILITIES DAMAGED BY THE CONTRACTOR WHILE COMPLETING THE PROPOSED WORK SHALL BE REPAIRED OR RETURNED TO ITS ORIGINAL STATE. COST TO BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
11. THE CONTRACTOR SHALL IMMEDIATELY SWEEP OR PICK UP ANY SOIL, DEBRIS, AGGREGATE CHIPS OR ROCK, OR LOOSE MATERIALS WHICH HAS BEEN DROPPED ONTO AIRPORT ROADS, RUNWAYS, TAXIWAYS, OR SODDED AREAS.
12. THE DISPOSAL OF ALL MATERIALS NOT TO BE INCORPORATED IN EMBANKMENTS ON THE PROJECT SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT A LOCATION OFF AIRPORT PROPERTY.
13. THE SEQUENCE OF CONSTRUCTION OPERATIONS AND DESCRIPTION OF CONDITIONS ARE OUTLINED IN THE PROJECT SPECIAL PROVISIONS. THE CONTRACTOR SHALL GIVE THE MAA A 96 HOUR NOTICE PRIOR TO THE START OF ANY WORK REQUIRING THE CLOSING OF ANY PAVEMENTS TO AIRCRAFT TRAFFIC SO THAT A NOTAM CAN BE ISSUED.
14. WORKING HOURS OF THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL CONFORM TO ALL APPLICABLE LOCAL LAWS, INCLUDING ANY NOISE CONTROL.
15. NO MOUNDS OF DIRT OR IRREGULARITIES GREATER THAN 3" WHICH, IN THE OPINION OF THE RESIDENT ENGINEER, COULD INTERFERE WITH ANY AIRFIELD OPERATIONS WILL BE PERMITTED ON THE AIRFIELD. NO EXPOSED FACES IN EXCESS OF ONE AND ONE-HALF (1-1/2) INCHES IN HEIGHT AND 2:1 SLOPES ON ANY EXCAVATION WILL BE PERMITTED WITHIN THE RESTRICTED AREAS.
16. DUST ABATEMENT MEASURES WILL BE REQUIRED, WHEN IN THE OPINION OF THE RESIDENT ENGINEER, A HAZARD TO AIR TRAFFIC, LOCAL RESIDENCES, OR CONSTRUCTION PROJECT PERSONNEL EXISTS. PREVENTIVE MEASURES TO BE ACCOMPLISHED BY THE CONTRACTOR SHALL INCLUDE, BUT NOT BE LIMITED TO, WATERING AND TREATMENT WITH CALCIUM CHLORIDE.
17. BY THE END OF EACH WORK DAY AND PRIOR TO LEAVING THE AIRFIELD, THE CONTRACTOR SHALL HAVE THOROUGHLY SWEEPED THE AIR TRAFFIC CORRIDORS ADJACENT TO THE WORK AREAS TO REMOVE DUST AND DEBRIS. IN ADDITION, ALL AIR TRAFFIC AREAS USED BY CONSTRUCTION PERSONNEL AND EQUIPMENT MUST BE CONTINUOUSLY SWEEPED AND MAINTAINED FREE OF DEBRIS. SWEEPERS SHALL BE PROVIDED BY THE CONTRACTOR FOR THE ENTIRE LENGTH OF THE CONTRACT AND SHALL BE OF A TYPE CAPABLE OF REMOVING ALL DUST AND DEBRIS TO THE SATISFACTION OF THE MAA. SWEEPERS MUST BE COMMERCIAL QUALITY AND APPROVED BY THE RESIDENT ENGINEER AND MAA PRIOR TO THE START OF CONSTRUCTION.
18. THE CONTRACTOR SHALL INSTALL AND MAINTAIN LATH LINES DURING THE LENGTH OF THE PROJECT AS SHOWN OR DIRECTED BY THE RESIDENT ENGINEER. SEE PROJECT SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
19. NO CHANGES SHALL BE MADE IN ANY PROVISIONS OF THIS SAFETY PLAN UNLESS APPROVED IN WRITING BY THE METROPOLITAN AIRPORT AUTHORITY, THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS, AND THE FEDERAL AVIATION ADMINISTRATION. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

STAGE 3 AIRFIELD STATUS

1. RUNWAY 5-23 CLOSED TO AIRCRAFT TRAFFIC.
2. TAXIWAY D CLOSED BETWEEN RUNWAY 9-27 AND RUNWAY 5.
3. TAXIWAY E CLOSED BETWEEN RUNWAY 9-27 AND TAXIWAY K.
4. TAXIWAY K CLOSED BETWEEN RUNWAY 5-23 AND TAXIWAY L.
5. RUNWAY 9-27 AND RUNWAY 13-31 OPEN TO AIRCRAFT TRAFFIC.
6. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.



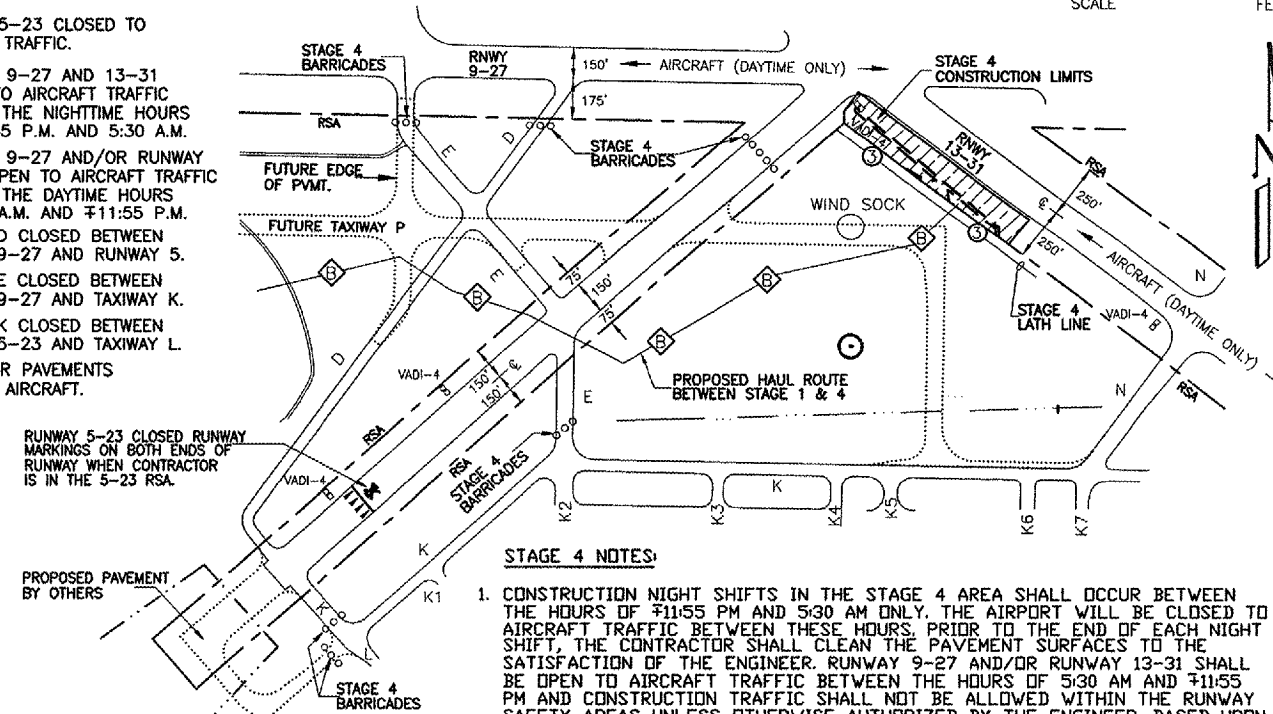
STAGE 3 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 3 NOTES:

1. INSTALL STAGE 3 LATH LINES PRIOR TO THE START OF WORK.
2. RUNWAY 5-23 CLOSED RUNWAY MARKINGS IN PLACE DURING STAGE 3.
3. CONTRACTOR TO REMAIN BELOW (INCLUDING TOP OF CONSTRUCTION EQUIPMENT) AND NOT STOCKPILE MATERIALS ABOVE THE FAA PART 77 CIVIL AIRPORT IMAGINARY SURFACE FOR RUNWAY 9-27 AS DETERMINED BY THE RESIDENT ENGINEER.

STAGE 4 AIRFIELD STATUS

1. RUNWAY 5-23 CLOSED TO AIRCRAFT TRAFFIC.
2. RUNWAYS 9-27 AND 13-31 CLOSED TO AIRCRAFT TRAFFIC BETWEEN THE NIGHTTIME HOURS OF 7:11:55 P.M. AND 5:30 A.M.
3. RUNWAYS 9-27 AND/OR RUNWAY 13-31 OPEN TO AIRCRAFT TRAFFIC BETWEEN THE DAYTIME HOURS OF 5:30 A.M. AND 7:11:55 P.M.
4. TAXIWAY D CLOSED BETWEEN RUNWAY 9-27 AND RUNWAY 5.
5. TAXIWAY E CLOSED BETWEEN RUNWAY 9-27 AND TAXIWAY K.
6. TAXIWAY K CLOSED BETWEEN RUNWAY 5-23 AND TAXIWAY L.
7. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.








STAGE 4 NOTES:

1. CONSTRUCTION NIGHT SHIFTS IN THE STAGE 4 AREA SHALL OCCUR BETWEEN THE HOURS OF 7:11:55 PM AND 5:30 AM ONLY. THE AIRPORT WILL BE CLOSED TO AIRCRAFT TRAFFIC BETWEEN THESE HOURS. PRIOR TO THE END OF EACH NIGHT SHIFT, THE CONTRACTOR SHALL CLEAN THE PAVEMENT SURFACES TO THE SATISFACTION OF THE ENGINEER. RUNWAY 9-27 AND/OR RUNWAY 13-31 SHALL BE OPEN TO AIRCRAFT TRAFFIC BETWEEN THE HOURS OF 5:30 AM AND 7:11:55 PM AND CONSTRUCTION TRAFFIC SHALL NOT BE ALLOWED WITHIN THE RUNWAY SAFETY AREAS UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER. BASED UPON AIRCRAFT LANDING REQUIREMENTS, THE MAA SHALL DETERMINE WHICH RUNWAY OR RUNWAYS ARE TO BE OPEN DURING THE DAYTIME HOURS. SEE THE PROJECT SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. CONTRACTOR SHALL MINIMIZE THE TIME SPENT IN THE RUNWAY 9-27 AND 13-31 SAFETY AREAS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
3. AT THE END OF EVERY WORK PERIOD, CONTRACTOR SHALL REGRADE THE RSA / TSA AND REDOEN THE RUNWAYS / TAXIWAYS TO AIRCRAFT TRAFFIC. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
4. CLOSED RUNWAY MARKINGS SHALL BE IN PLACE WHEN THE CONTRACTOR IS IN THE RUNWAY SAFETY AREAS (ALL RUNWAYS).

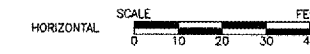
STAGE 4 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

SOIL MATERIAL LEGEND:

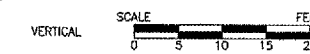
-  PROPOSED UNDERCUT / ROCK BACKFILL AREA
-  UNCOMPACTED SAND OR CLAY FILL MATERIALS
-  COMPACTED SAND FILL MATERIALS
-  COMPACTED SAND OR CLAY FILL MATERIALS
-  GROUND STRIPPING MATERIALS

PROPOSED TYPICAL SECTION - NO SURCHARGE, FUTURE RUNWAY SAFETY AREA

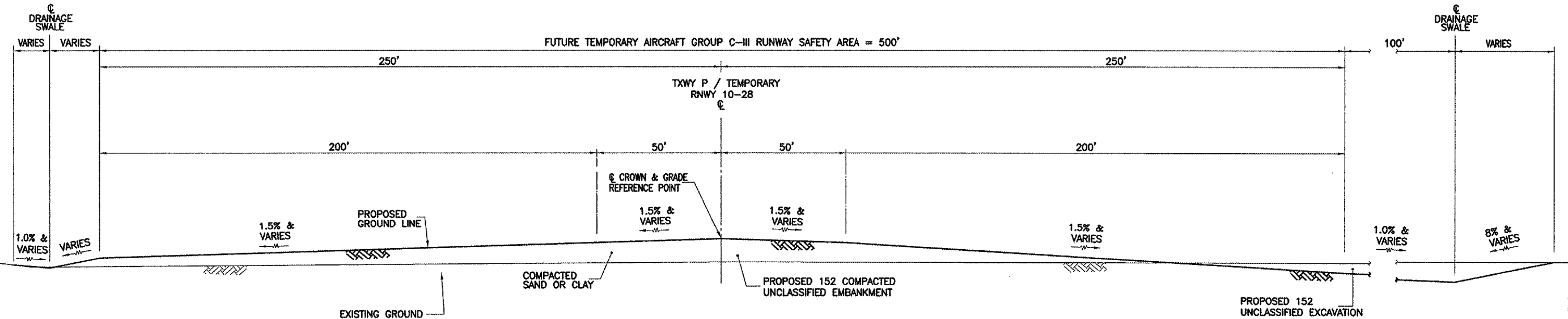
TAXIWAY P / TEMPORARY RUNWAY 10-28, STA. 574+50 TO STA. 588+71.9



SCALES:



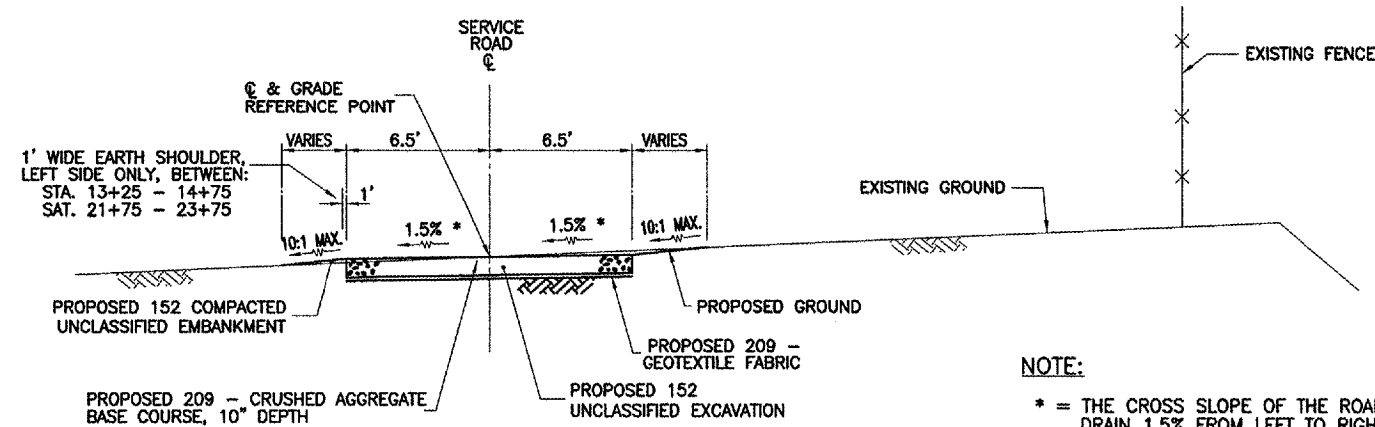
AIRCRAFT APPROACH CATEGORY C
AIRPLANE DESIGN GROUP III
NON-PRECISION RUNWAY



NOTES:

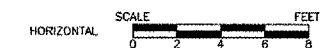
- ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

PROPOSED TYPICAL SECTION
FLUSH CRUSHED AGGREGATE SERVICE ROAD
STA. 10+44 TO STA. 39+05

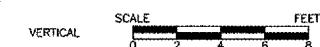


NOTE:

- * = THE CROSS SLOPE OF THE ROADWAY SHALL DRAIN 1.5% FROM LEFT TO RIGHT BETWEEN: STA. 10+44 TO STA. 12+50. STA. 24+00 TO STA. 32+00.








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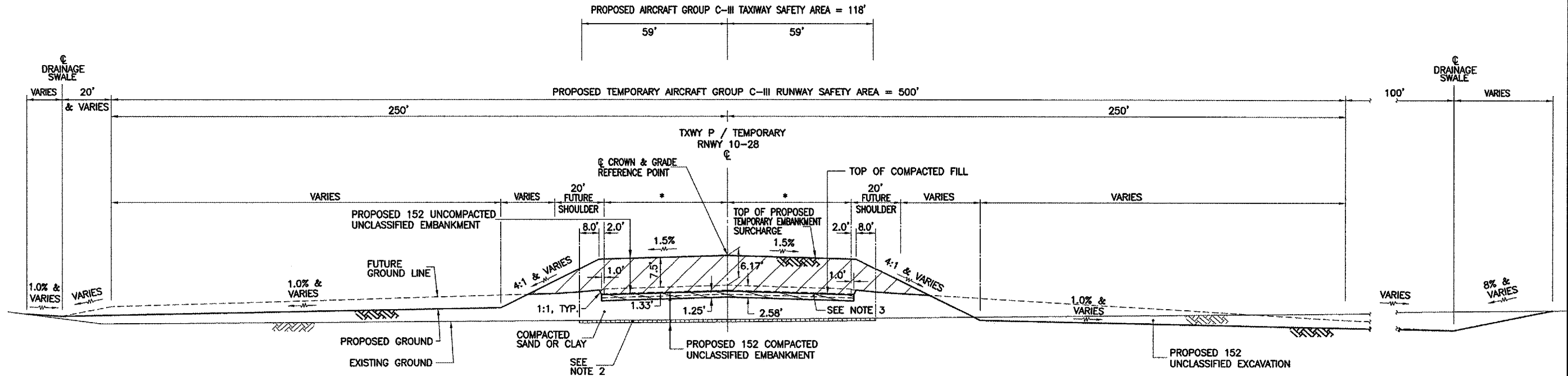
PROPOSED TYPICAL SECTION - EMBANKMENT SURCHARGE

TAXIWAY P / TEMPORARY RUNWAY 10-28, STA. 587+71.9 TO STA. 613+44.9
 TAXIWAY P / TEMPORARY RUNWAY 10-28, STA. 647+06.6 TO STA. 654+40
 SURVEY LINE P1 (WEST CONNECTOR), STA. 805+45.81 TO STA. 807+20
 SURVEY LINE P2 (EAST CONNECTOR), STA. 900+50 TO STA. 909+95

SOIL MATERIAL LEGEND:

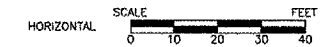
-  PROPOSED UNDERCUT / ROCK BACKFILL AREA
-  UNCOMPACTED SAND OR CLAY FILL MATERIALS
-  COMPACTED SAND FILL MATERIALS
-  COMPACTED SAND OR CLAY FILL MATERIALS
-  GROUND STRIPPING MATERIALS

AIRCRAFT APPROACH CATEGORY C
 AIRPLANE DESIGN GROUP III
 NON-PRECISION RUNWAY

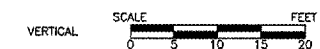


NOTES:

1. ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. THE EXISTING GROUND MATERIALS UNDER THE FUTURE PAVEMENT SURFACE SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
3. INSTALL COMPACTED SAND FILL MATERIALS UNDER (AND WITHIN 1.0' OF) THE FUTURE PAVEMENT FROM A POINT 1.33' BELOW THE FUTURE PAVEMENT SURFACE TO A POINT 2.58' BELOW THE FUTURE PAVEMENT SURFACE.
4. UNCOMPACTED EMBANKMENT SURCHARGE WILL NOT BE REQUIRED BETWEEN TAXIWAY P STATIONS 613+44.9 AND 647+06.6.
5. * = TAXIWAY P : 50' & VARIES.
 WEST CONNECTOR (SURVEY LINE P1) : 37.5' & VARIES.
 EAST CONNECTOR (SURVEY LINE P2) : 37.5' & VARIES.

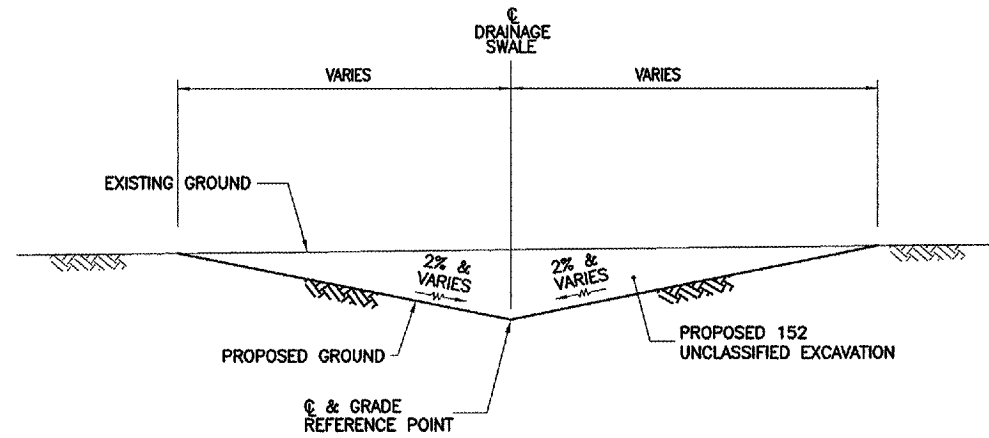


SCALES:



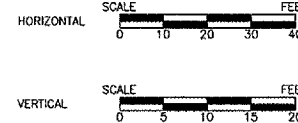
PROPOSED TYPICAL SECTION - WEST & EAST DITCHES

WEST DITCH: TAXIWAY P, STA. 604+00, RIGHT 350' TO RIGHT 1,230'
EAST DITCH: TAXIWAY P, STA. 620+00, RIGHT 280' TO RIGHT 1,140'

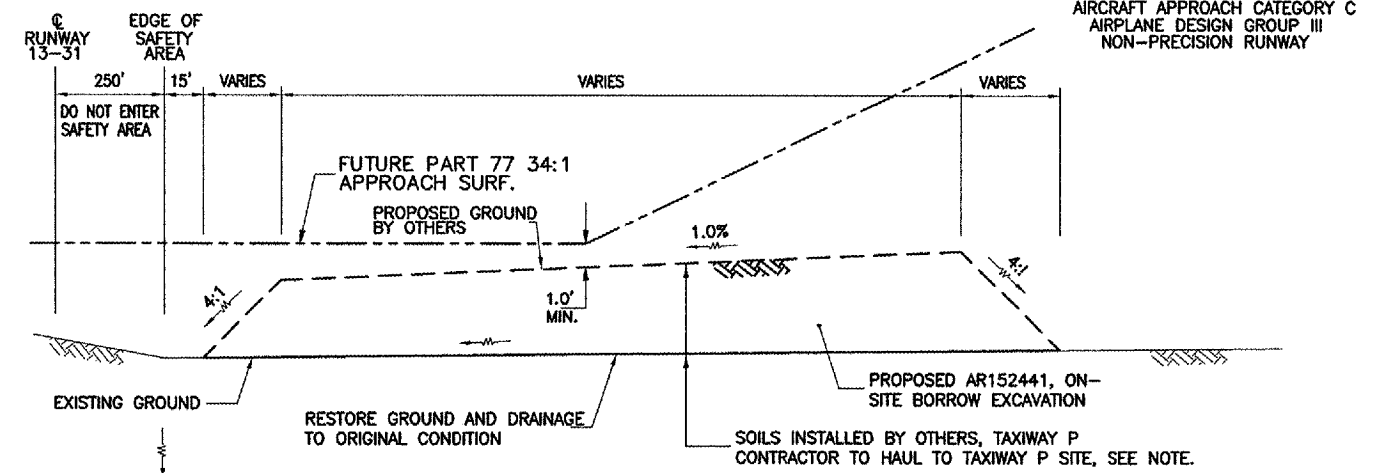


NOTES:

WEST DITCH : 300+XX SERIES STATIONING.
EAST DITCH : 200+XX SERIES STATIONING.



PROPOSED TYPICAL SECTION - ON-SITE BORROW AREA



NOTE:

PROPOSED NORTH HANGAR SITE CONTRACTOR (OTHERS) TO STOCK PILE SANDY WASTE SOILS AT THIS LOCATION. TAXIWAY P EARTHWORK CONTRACTOR (THIS CONTRACT) TO HAUL AND INSTALL STORED SOILS TO THE TAXIWAY P CONSTRUCTION SITE AREA. SOILS TO BE HAULED FROM THIS ON-SITE BORROW AREA TO TAXIWAY P UNDER CONTRACT ITEM AR152441 ON-SITE BORROW.



PROPOSED TYPICAL SECTION - SUBGRADE UNDERCUT DETAILS

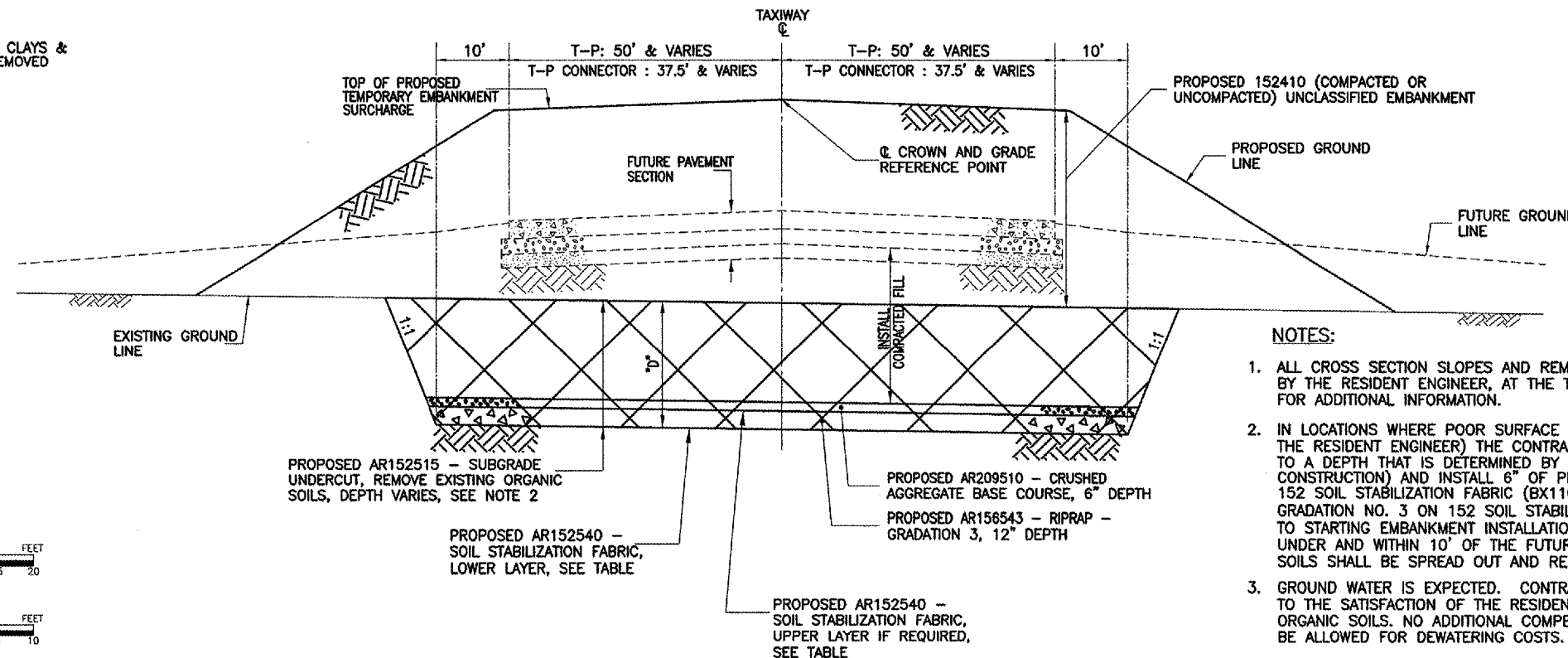
TAXIWAY P, STA. 652+00 TO STA. 654+10, TAXIWAY P WEST CONNECTOR, STA. 802+80 TO STA. 805+80, TAXIWAY P EAST CONNECTOR, STA. 900+50 TO STA. 901+00, AND TAXIWAY P EAST CONNECTOR, STA. 907+33 TO STA. 907+67

SOIL MATERIAL LEGEND:



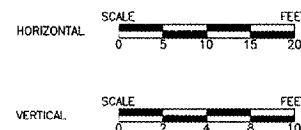
PROPOSED AR152540 - SOIL STABILIZATION FABRIC TABLE			
TENSAR BIAXIAL GEOGRID (OR APPROVED EQUAL)			
LOCATION	LOWER LAYER	UPPER LAYER	
652+00 - 654+10	BX1300	BX1100	
802+80 - 805+80	BX1300	NOT REQ'D	
900+50 - 901+00	BX1300	BX1100	
907+33 - 907+67	BX1300	BX1100	

APPROXIMATE "D"	
LOCATION	FEET
652+00 - 654+10	7
802+80 - 805+80	3
900+50 - 901+00	7
907+33 - 907+67	4.5



NOTES:






- ALL CROSS SECTION SLOPES AND REMOVAL AREAS ARE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
- IN LOCATIONS WHERE POOR SURFACE / SUB-SURFACE SOILS EXIST (AS DETERMINED BY THE RESIDENT ENGINEER) THE CONTRACTOR SHALL REMOVE THE EXISTING SOIL MATERIALS TO A DEPTH THAT IS DETERMINED BY THE RESIDENT ENGINEER (ON SITE, AT THE TIME OF CONSTRUCTION) AND INSTALL 6" OF PROPOSED 209 CRUSHED AGGREGATE BASE COURSE ON 152 SOIL STABILIZATION FABRIC (BX1100 OR APPROVED EQUAL) ON 12" OF 156 RIP RAP - GRADATION NO. 3 ON 152 SOIL STABILIZATION FABRIC (BX1300 OR APPROVED EQUAL) PRIOR TO STARTING EMBANKMENT INSTALLATION. THE PROPOSED MATERIALS SHALL BE INSTALLED UNDER AND WITHIN 10' OF THE FUTURE PAVED AREA. THE REMOVED EXISTING ORGANIC SOILS SHALL BE SPREAD OUT AND REUSED AS SHOULDER FILL / TOPSOIL.
- GROUND WATER IS EXPECTED. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DEWATERING, TO THE SATISFACTION OF THE RESIDENT ENGINEER, TO INSURE PROPER REMOVAL OF ALL ORGANIC SOILS. NO ADDITIONAL COMPENSATION BEYOND THE CONTACT UNIT PRICES WILL BE ALLOWED FOR DEWATERING COSTS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

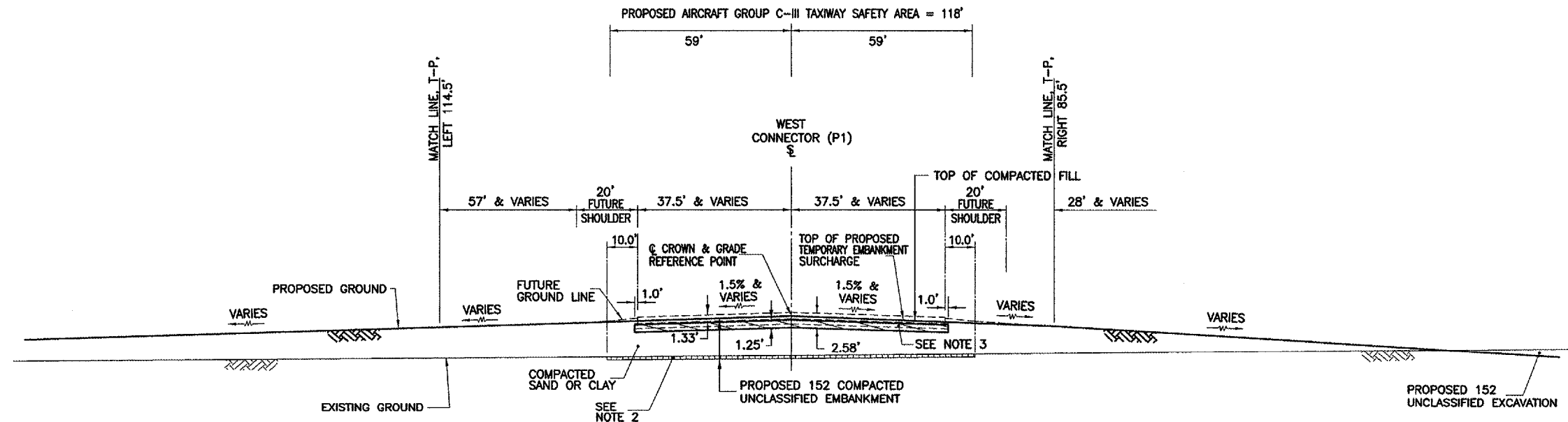


PROPOSED TYPICAL SECTION - PARTIAL SURCHARGE, FUTURE TAXIWAY

SURVEY LINE P1 (WEST CONNECTOR), STA. 801+05.61 TO STA. 805+45.81

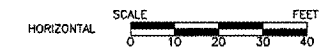
SOIL MATERIAL LEGEND:

-  PROPOSED UNDERCUT / ROCK BACKFILL AREA
-  UNCOMPACTED SAND OR CLAY FILL MATERIALS
-  COMPACTED SAND FILL MATERIALS
-  COMPACTED SAND OR CLAY FILL MATERIALS
-  GROUND STRIPPING MATERIALS

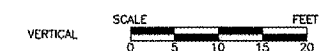


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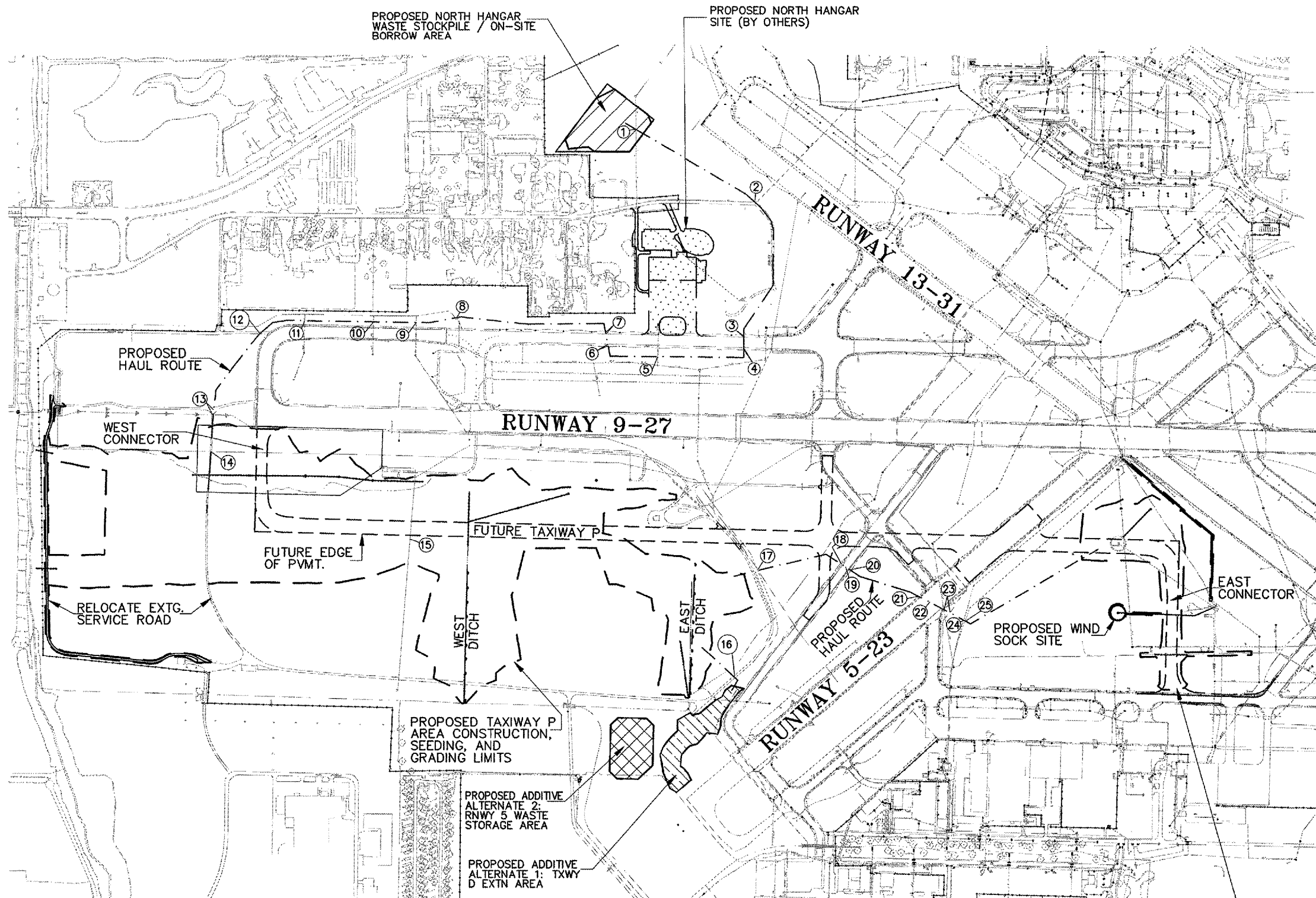
1. ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. THE EXISTING GROUND MATERIALS UNDER THE FUTURE PAVEMENT SURFACE SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
3. INSTALL COMPACTED SAND FILL MATERIALS UNDER (AND WITHIN 1.0' OF) THE FUTURE PAVEMENT FROM A POINT 1.33' BELOW THE FUTURE PAVEMENT SURFACE TO A POINT 2.58' BELOW THE FUTURE PAVEMENT SURFACE.



SCALES:



QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE I - EARTHWORK
ILL. MLI-3790, QU006
SHEET 10 OF 94

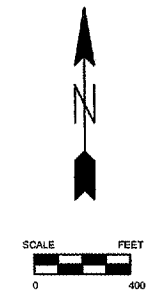


PROPOSED MINIMUM UTILITY PROTECTION*		
LOCATION NUMBER	UTILITY DESCRIPTION	MINIMUM PROTECTION
①	FIBER OPTIC TELEPHONE	STEEL PLATES
②	4" GAS MAIN	STEEL PLATES
③	EDGE LIGHT CABLE / EDGE OF PCC PVMT	STEEL PLATES
④	EDGE LIGHT CABLE / EDGE OF PCC PVMT	STEEL PLATES
⑤	15" STM SEW	STEEL PLATES
⑥	EDGE LIGHT CABLE / EDGE OF PCC PVMT	STEEL PLATES
⑦	EDGE LIGHT CABLE / EDGE OF PCC PVMT	STEEL PLATES
⑧	36" STM SEW	STEEL PLATES
⑨	18" STM SEW	STEEL PLATES
⑩	18" STM SEW	STEEL PLATES
⑪	18" STM SEW	STEEL PLATES
⑫	SERVICE ROAD	STEEL PLATES W/ 3" SOIL COVER
⑬	MALSR CABLE / SERVICE ROAD	STEEL PLATES
⑭	LOCALIZER CONTROL CABLE	STEEL PLATES W/ 3" SOIL COVER
⑮	TRANSFORMER POWER CABLE	STEEL PLATES W/ 3" SOIL COVER
⑯	SERVICE ROAD	STEEL PLATES W/ 3" SOIL COVER
⑰	SERVICE ROAD / R9 ILS CONTROL CABLE / SCAN CABLE	STEEL PLATES W/ 3" SOIL COVER
⑱	EDGE LIGHT CABLE	STEEL PLATES
⑲	BIT. TXWY. PVMT.	STEEL PLATES W/ 3" SOIL COVER
⑳	EDGE LIGHT CABLE	STEEL PLATES
㉑	EDGE LIGHT CABLE / EDGE OF PCC PVMT	STEEL PLATES
㉒	PCC RNWY PVMT	3" SOIL COVER
㉓	EDGE LIGHT CABLES (4EA.) / EDGE OF PCC PVMT	STEEL PLATES
㉔	FAA CONTROL CABLE	STEEL PLATES
㉕	FAA FIBER OPTIC CABLE	STEEL PLATES

* = MINIMUM THICKNESS OF ALL STEEL PLATES = 1" (ONE INCH), ALL STEEL PLATES TO HAVE A MINIMUM OF 3" SOIL COVER TO HOLD PLATES IN PLACE.

LEGEND:

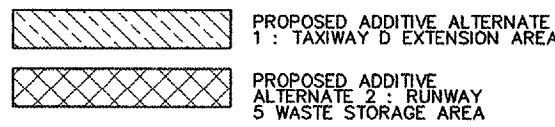
- ⊙ EDGE LIGHT
- GUIDANCE SIGN
- ELECTRICAL CABLE
- STORM SEWER OR UNDERDRAIN
- ▣ INLET
- ▽ OUTLET
- ⊙ STORM MANHOLE
- ⊙ SANITARY MANHOLE
- DRAINAGE DITCH
- CONDUIT OR DUCT BANK
- FUTURE PAVEMENT
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
- ▨ PROPOSED NORTH HANGAR SITE (BY OTHERS)
- ▨ PROPOSED NORTH HANGAR WASTE STOCKPILE / ON-SITE BORROW AREA
- ① = PROPOSED UTILITY PROTECTION LOCATION NUMBER
- PROPOSED HAUL ROUTE



TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM 200	587.10	HUB AT SE COR OF TXY H BORROW AREA, N1742722.2338, E2199623.7550, STATION 107+06.74, LT. 739.58
TBM K	578.34	TOP OF RCP, S SIDE OF SERVICE RD, N1741508.6290, E2201629.7690, STATION 127+42.16, RT. 424.08
TBM R	581.52	NE COR CONC BASE ELECT TRAN, S SIDE 9-27, BETWEEN SIGNS ② - ⑦, N1741569.4966, E2200420.1701, STATION 115+31.40, RT. 393.12
TBM T	576.35	CHIS. □ SE COR CONC PAD W SIDE GS BLDG, N1741651.7212, E2198675.6748, STATION 97+85.40, RT. 354.03
TBM U	583.18	CHIS. □ HOLD SIGN BASE, NE COR, S SIDE R9, N1741775.92035, E2203365.21359, STATION 144+70.44, RT. 113.99

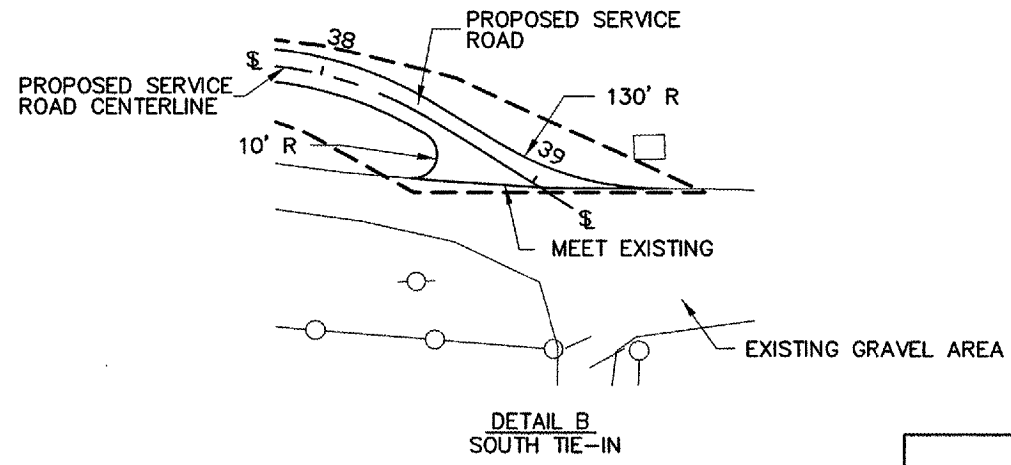
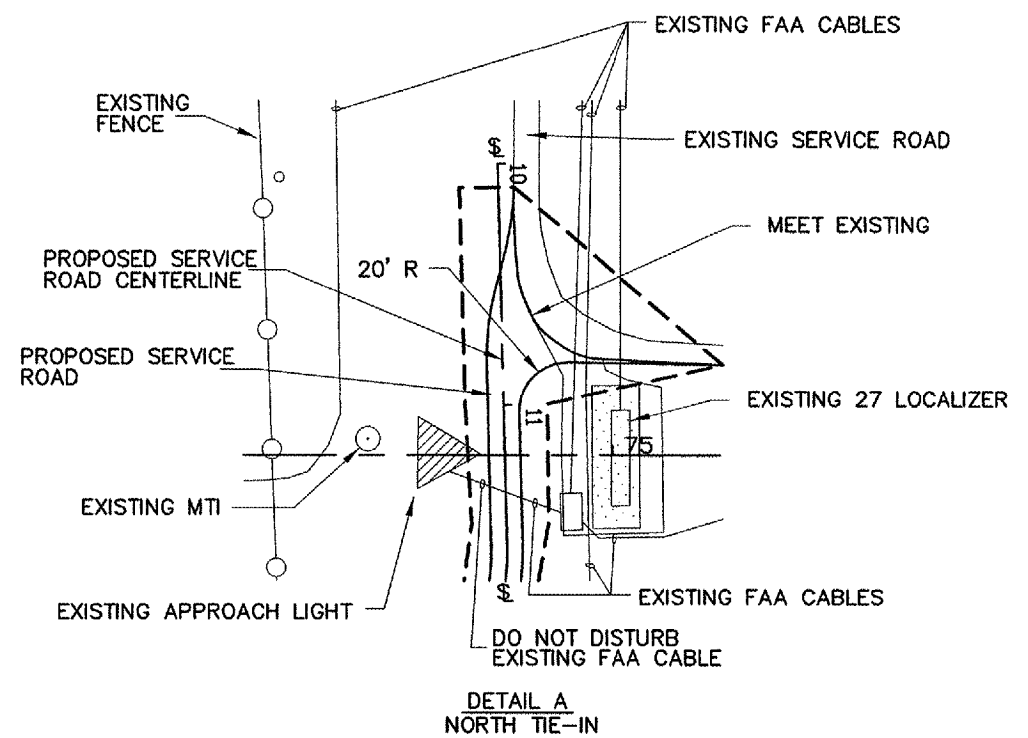
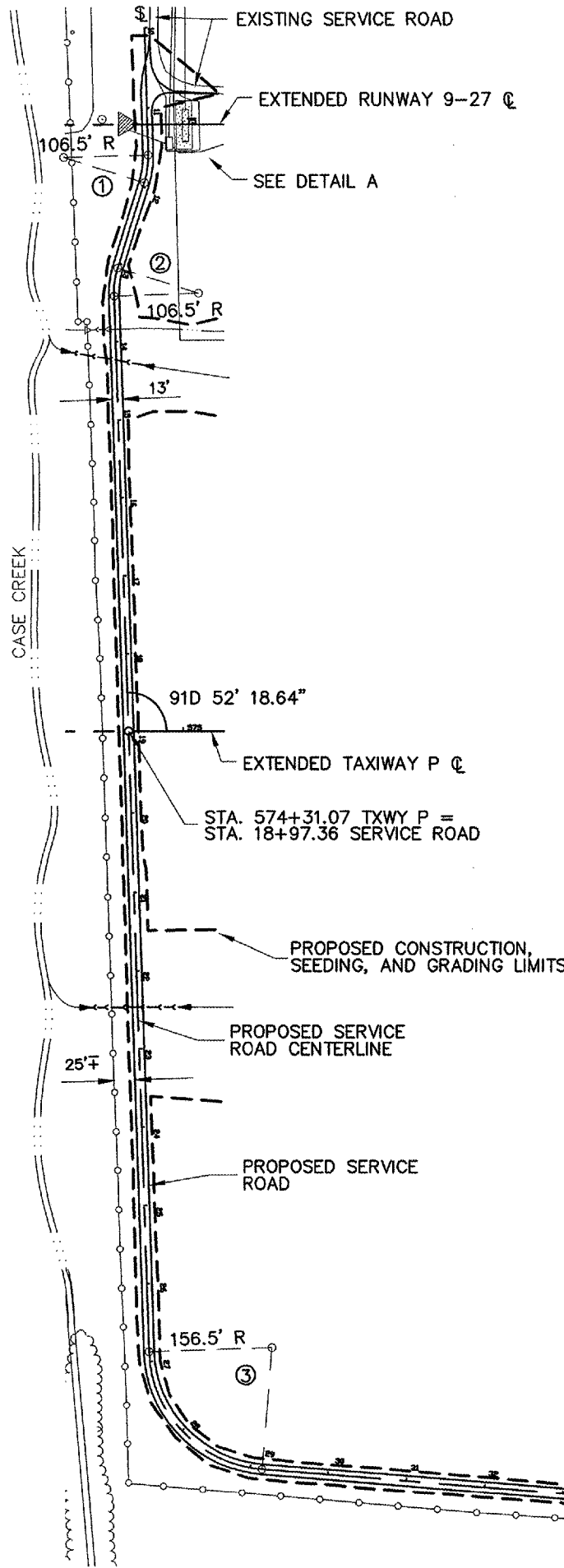
PROJECT CONTROL POINTS			
NUMBER	LOCATION	ELEV.	REMARK
5	STA. 108+94.45, RT. 339.57 N1741638.7660, E2199784.7412	580.05	
7	STA. 121+74.76, RT. 449.70 N1741496.8955, E2201061.9416	577.67	
8	STA. 127+73.19, RT. 413.69 N1741318.2450, E2201661.0745	579.45	
13	STA. 143+25.87, RT. 1163.46 N1740729.9858, E2203194.7470	574.40	1/2" REBAR
2000	STA. 107+69.21, LT. 736.44 N1742717.7720, E2199686.1370	587.72	SPK

- NOTES:**
- SEE PLAN & PROFILE SHEETS FOR LOCATION OF EXISTING UNDERGROUND ELECTRICAL AND FIELD TILES.
 - CONTRACTOR SHALL REPAIR OR REPLACE ANY IMPROVEMENTS DAMAGED DURING CONSTRUCTION AT HIS OWN EXPENSE.

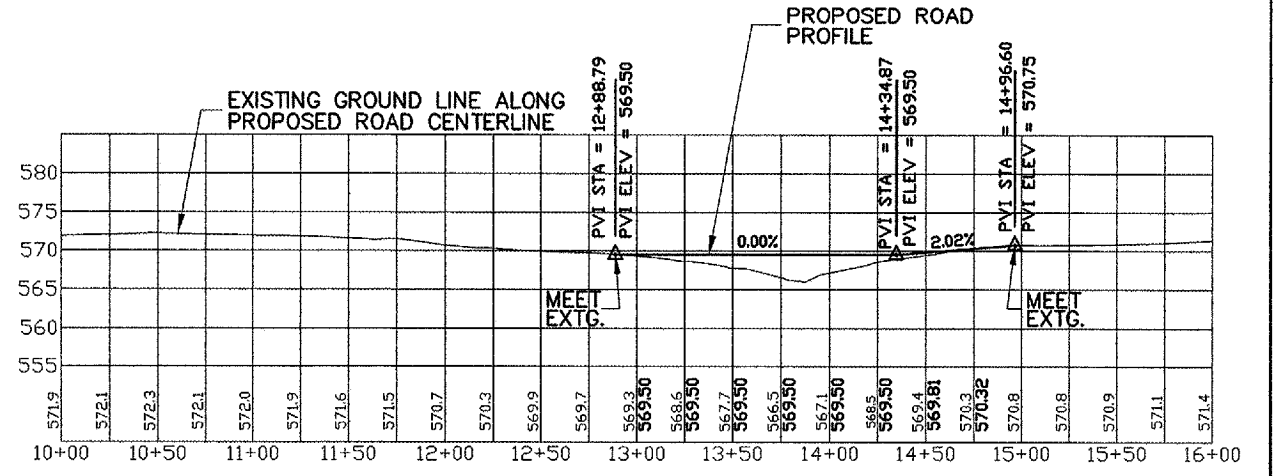
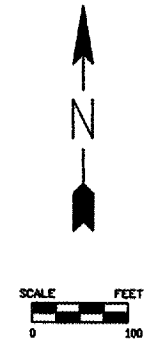


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QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE I - EARTHWORK
ILL. MLI-3790, QU006
SHEET 11 OF 94



- LEGEND:**
- ☼ EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - ▨ INLET
 - ▽ OUTLET
 - ⊙ STORM MANHOLE
 - ⊙ SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - SECURITY FENCE
 - ⊙ CENTERLINE CURVE NUMBER
 - ⊙ POWER POLE



NOTE: PROPOSED PROFILE FOLLOWS EXISTING GROUND BETWEEN STATIONS 10+80 AND 39+05 (EXCEPT BETWEEN STATIONS 12+88.79 AND 14+96.60 AS SHOWN ABOVE).

SERVICE ROAD PROFILE
STA. 10+00 - 16+00 ONLY

SERVICE ROAD ☐ CURVE DATA						
CURVE NUMBER	①	②	③	④	⑤	⑥
I	18D 48' 57.20"	19D 16' 18.55"	83D 38' 41.72"	36D 53' 23.73"	34D 10' 05.61"	31D 12' 09.24"
D	53D 47' 55.94"	53D 47' 55.94"	36D 36' 38.64"	36D 36' 38.64"	36D 36' 38.64"	36D 36' 38.64"
T	17.65'	18.08'	140.04'	52.20'	48.10'	42.17'
L	34.97'	35.82'	228.47'	100.76'	93.33'	85.23'
R	106.50'	106.50'	156.50'	156.50'	156.50'	156.50'
E	1.45'	1.52'	53.51'	8.48'	7.22'	5.58'
P.C. STA.	11+59.16	13+06.45	26+87.34	34+93.59	36+14.65	37+68.06
P.I. STA.	11+76.81	13+24.53	28+27.38	35+45.79	36+62.75	38+10.23
P.T. STA.	11+94.13	13+42.27	29+15.81	35+94.35	37+07.98	38+53.29

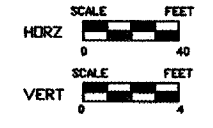
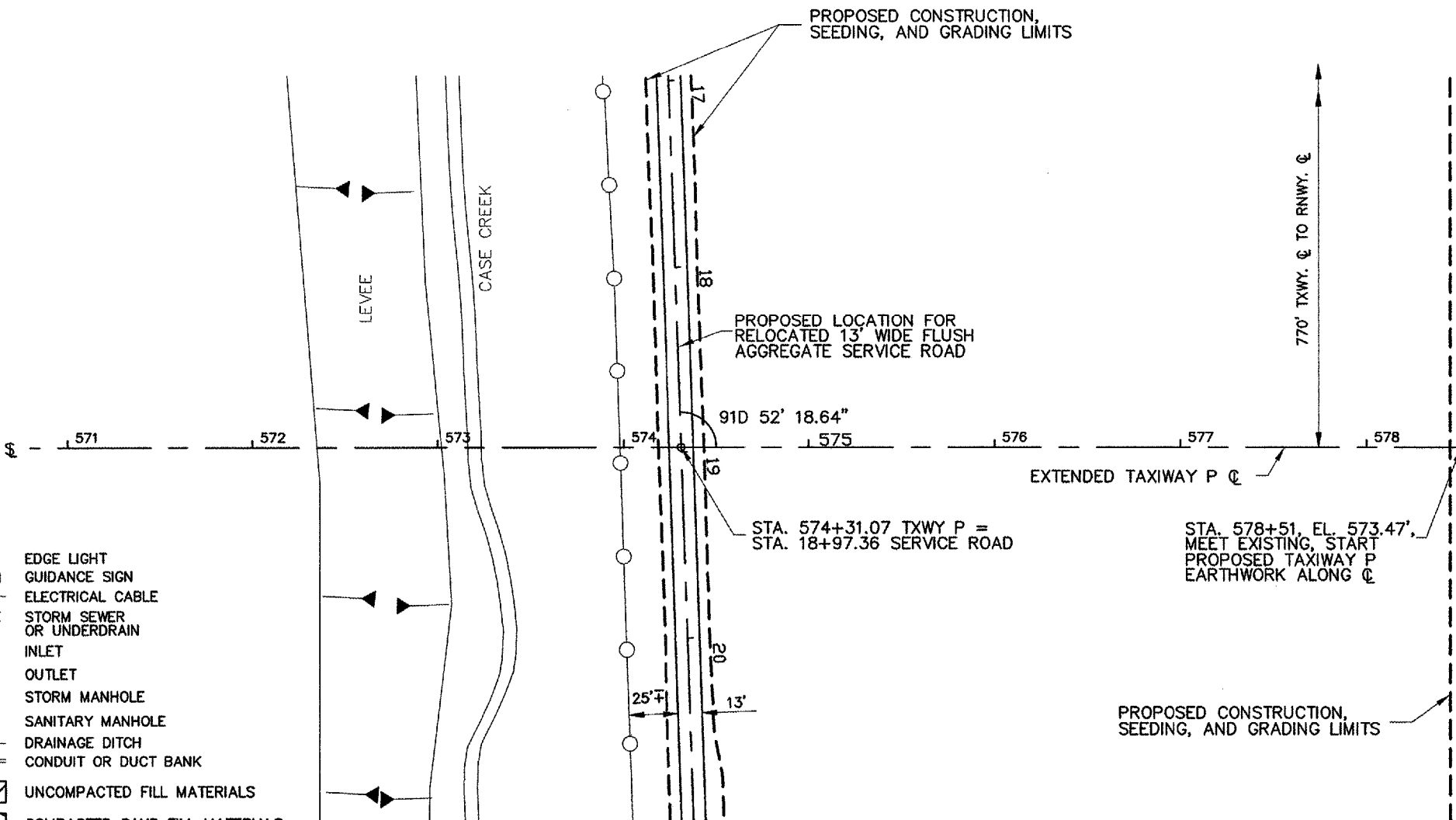
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QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE I - EARTHWORK
ILL. MLI-3790, QU006
SHEET 12 OF 94

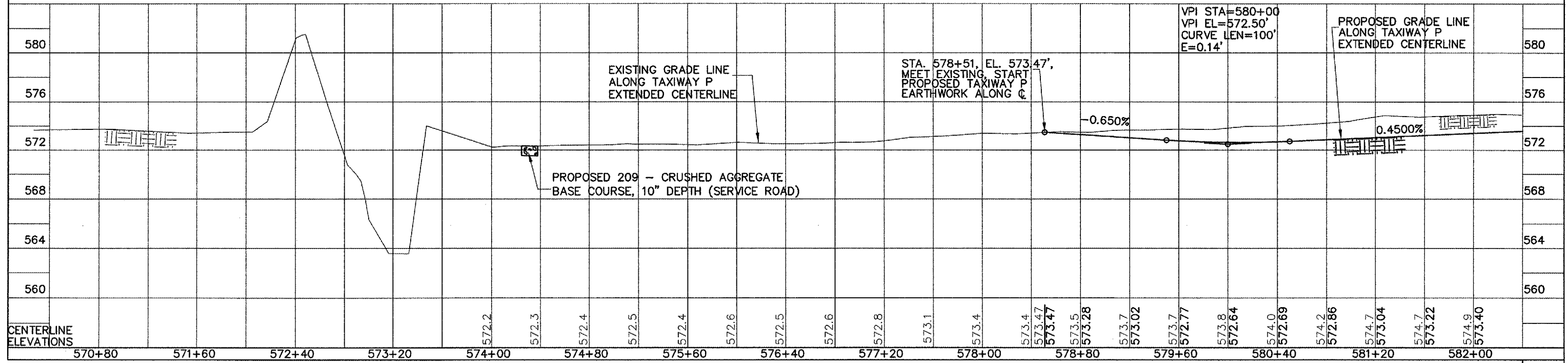
PROJECT CONTROL POINTS			
NUMBER	LOCATION	ELEV.	REMARK
5	STA. 108+94.45, RT. 339.57' N1741638.7660, E2199784.7412	580.05	
7	STA. 121+74.76, RT. 449.70' N1741496.8955, E2201061.9416	577.67	
8	STA. 127+73.19, RT. 413.69' N1741518.2450, E2201661.0745	579.45	
13	STA. 143+25.87, RT. 1163.46' N1740729.9858, E2203194.7470	574.40	1/2' REBAR
2000	STA. 107+69.21, LT. 736.44' N1742717.7720, E2199686.1370	587.72	SPK

TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM 200	587.10	HUB AT SE COR OF TXY H BORROW AREA, N1742722.2338, E2199623.7530, STATION 107+06.74, LT. 739.38'
TBM K	578.34	TOP OF RCP, S SIDE OF SERVICE RD, N1741508.6290, E2201629.7690, STATION 127+42.16, RT. 424.08'
TBM R	581.52	NE COR CONC BASE ELECT TRAN, S SIDE 9-27, BETWEEN SIGNS [B] - [Z] N1741569.4966, E2200420.1701, STATION 115+31.40, RT. 393.12'
TBM T	576.35	CHIS. □, SE COR CONC PAD V SIDE GS BLDG, N1741651.7212, E2198675.6748, STATION 97+85.40, RT. 354.03'
TBM U	583.18	CHIS. □, HOLD SIGN BASE, NE COR, S SIDE R9, N1741775.9203, E2203365.2135, STATION 144+70.44, RT. 113.99'

- LEGEND:**
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - INLET
 - OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - UNCOMPACTED FILL MATERIALS
 - COMPACTED SAND FILL MATERIALS
 - COMPACTED FILL MATERIALS
 - PROPOSED UNDERCUT / ROCK BACKFILL AREA



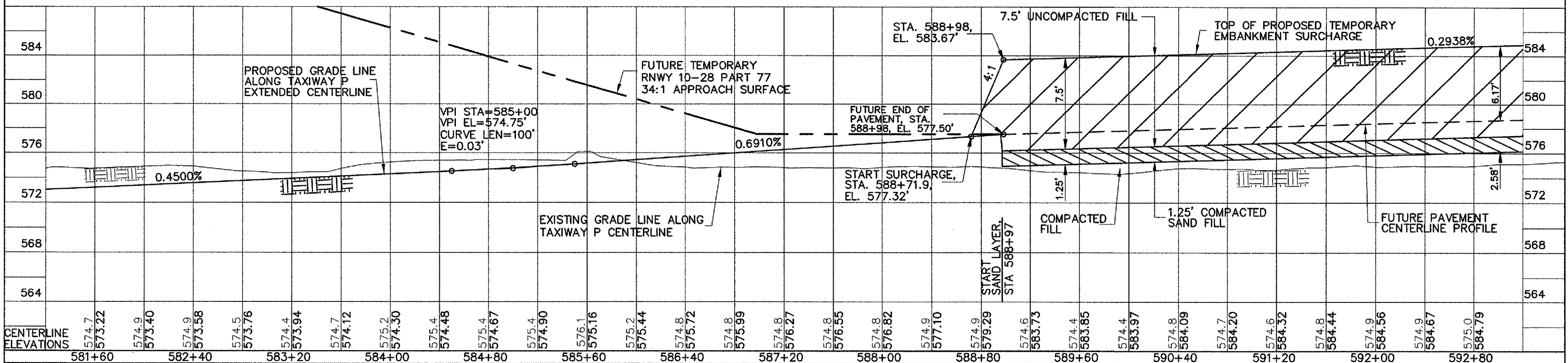
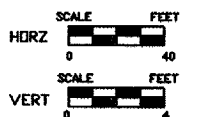
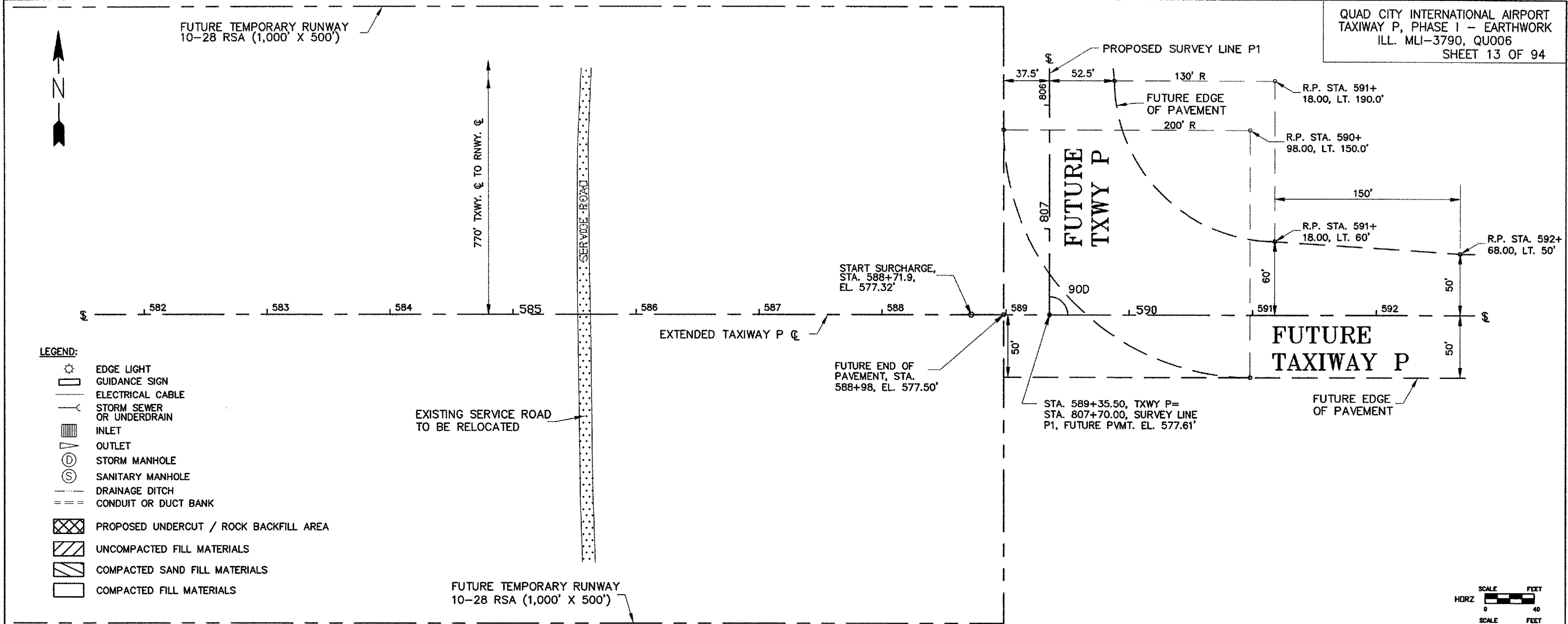
FUTURE TEMPORARY RUNWAY
10-28 RSA (1,000' X 500')



TAXIWAY P PLAN & PROFILE. STA 571+00 TO STA 581+80

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QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE I - EARTHWORK
ILL. MLI-3790, QU006
SHEET 13 OF 94

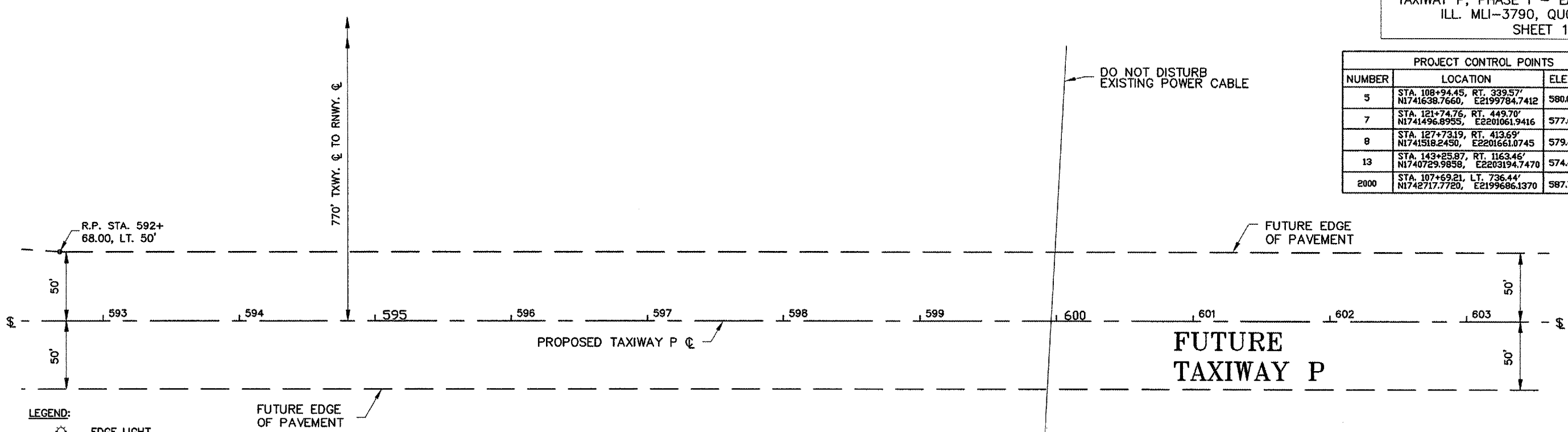


TAXIWAY P PLAN & PROFILE STA 581+80 TO STA 592+60 13/94

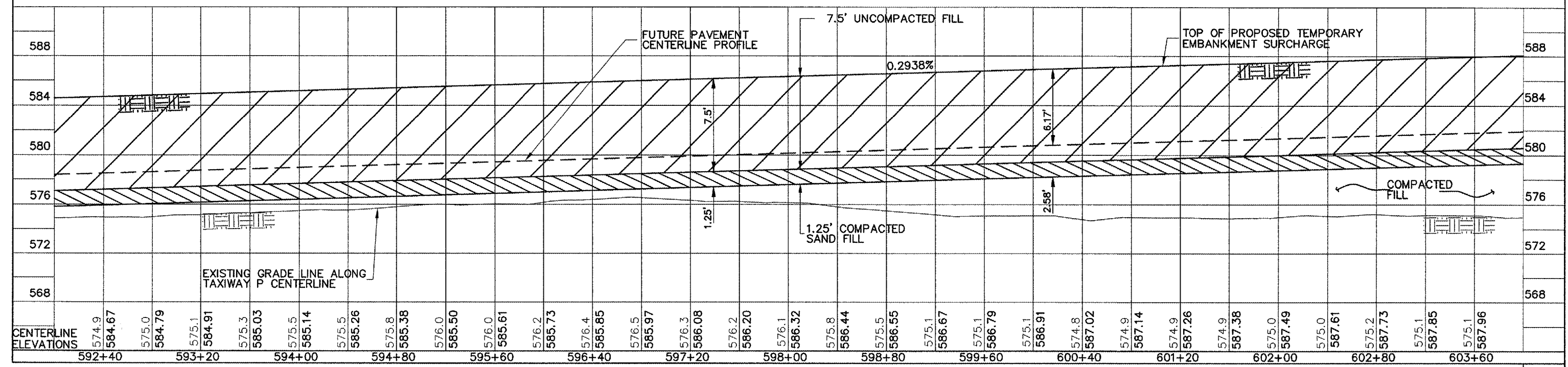
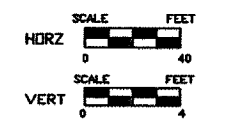
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QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE I - EARTHWORK
ILL. MLI-3790, QU006
SHEET 14 OF 94

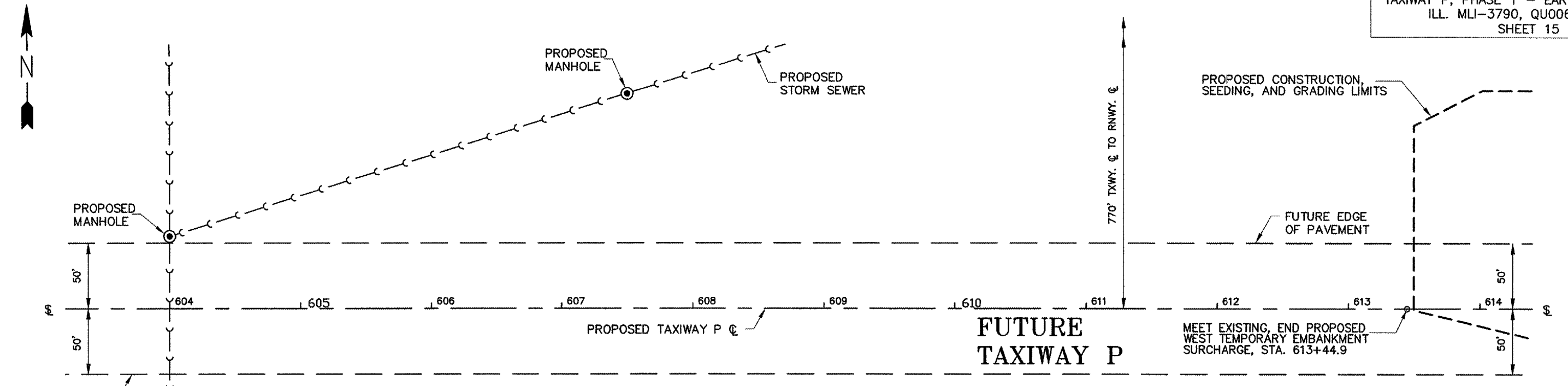
PROJECT CONTROL POINTS			
NUMBER	LOCATION	ELEV.	REMARK
5	STA. 108+94.45, RT. 339.57' N1741638.7660, E2199784.7412	580.05	
7	STA. 121+74.76, RT. 449.70' N1741496.8955, E2201061.9416	577.67	
8	STA. 127+73.19, RT. 413.69' N1741518.2450, E2201661.0745	579.45	
13	STA. 143+25.87, RT. 1163.46' N1740729.9858, E2203194.7470	574.40	1/2" REBAR
2000	STA. 107+69.21, LT. 736.44' N1742717.7720, E2199686.1370	587.72	SPK



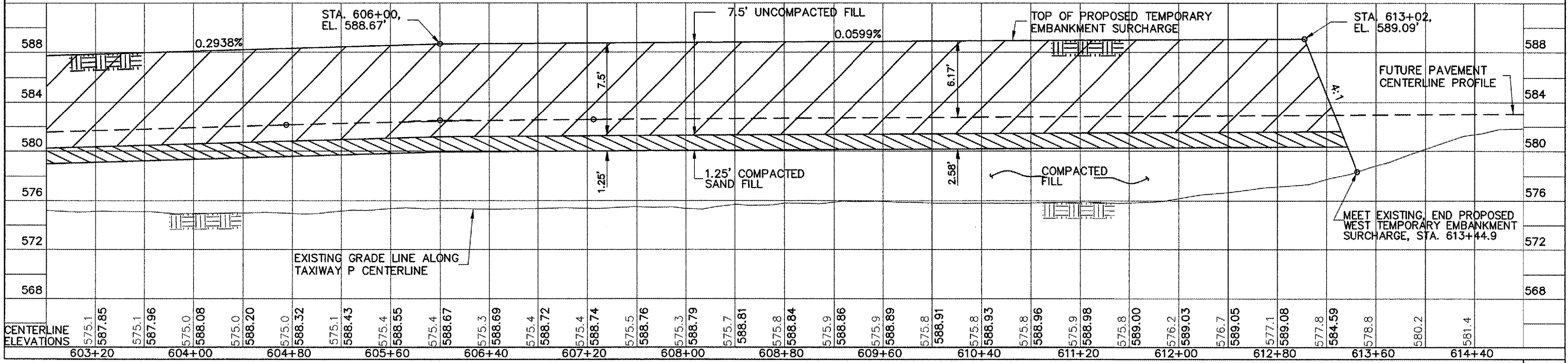
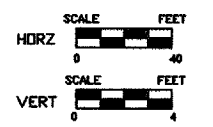
TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM 200	587.10	HUB AT SE COR OF TXY H BORROW AREA, N1742722.2338, E2199623.7550, STATION 107+06.74, LT. 739.58'
TBM K	578.34	TOP OF RCP, S SIDE OF SERVICE RD, N1741508.6290, E2201629.7690, STATION 127+42.16, RT. 424.08'
TBM R	581.52	NE COR CONC BASE ELECT TRAN, S SIDE 9-27, BETWEEN SIGNS (R) - (L) N1741565.4966, E2200420.1701, STATION 115+31.40, RT. 393.12'
TBM T	576.35	CHIS. □, SE COR CONC PAD W SIDE GS BLDG, N1741651.7212, E2198675.6748, STATION 97+85.40, RT. 354.03'
TBM U	583.18	CHIS. □, HOLD SIGN BASE, NE COR, S SIDE R9, N1741775.92035, E2203365.21359, STATION 144+70.44, RT. 113.99'



TAXIWAY P PLAN & PROFILE. STA 592+60 TO STA 603+40 14/94



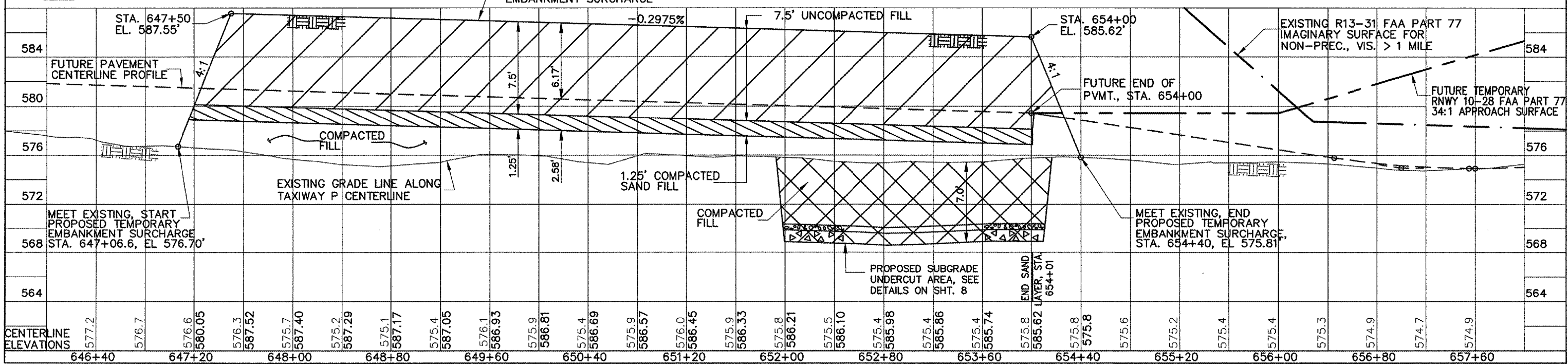
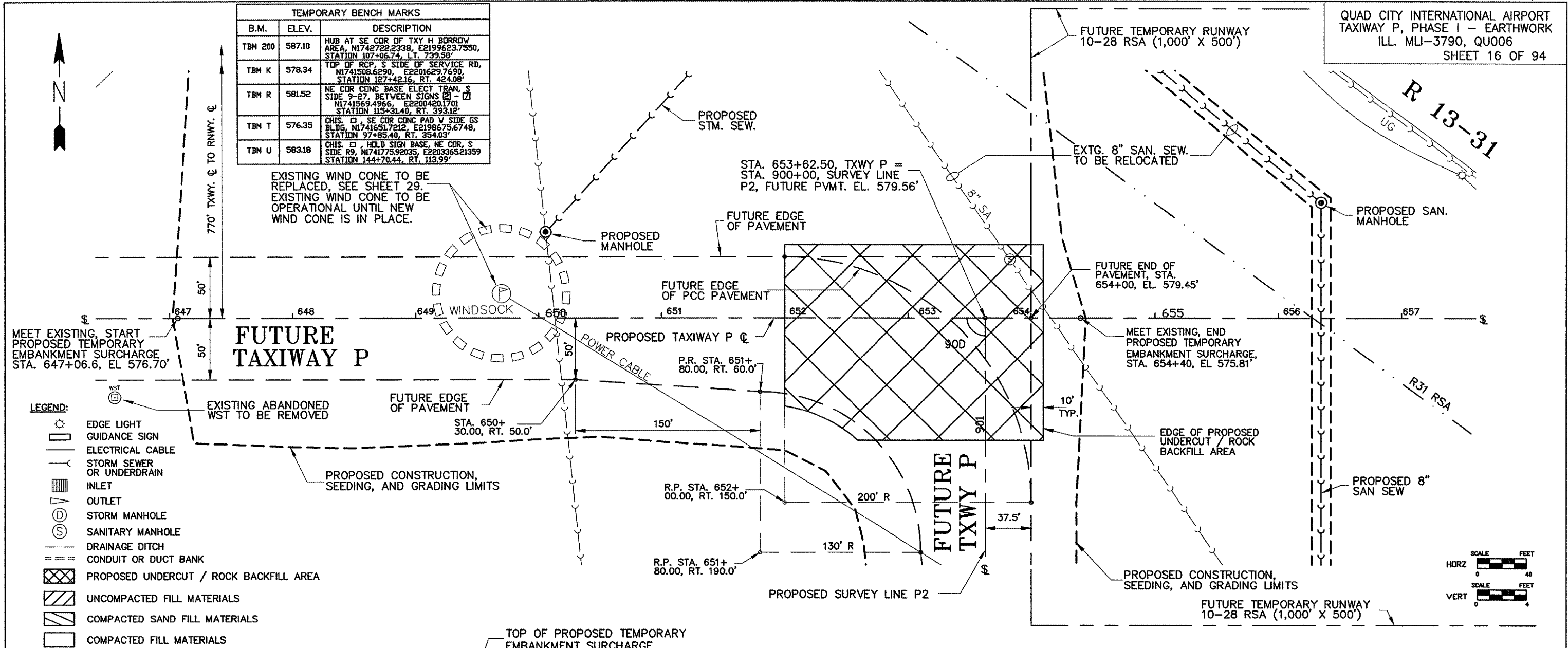
- LEGEND:**
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - INLET
 - OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - PROPOSED UNDERCUT / ROCK BACKFILL AREA
 - UNCOMPACTED FILL MATERIALS
 - COMPACTED SAND FILL MATERIALS
 - COMPACTED FILL MATERIALS



TAXIWAY P PLAN & PROFILE. STA 603+40 TO STA 614+20

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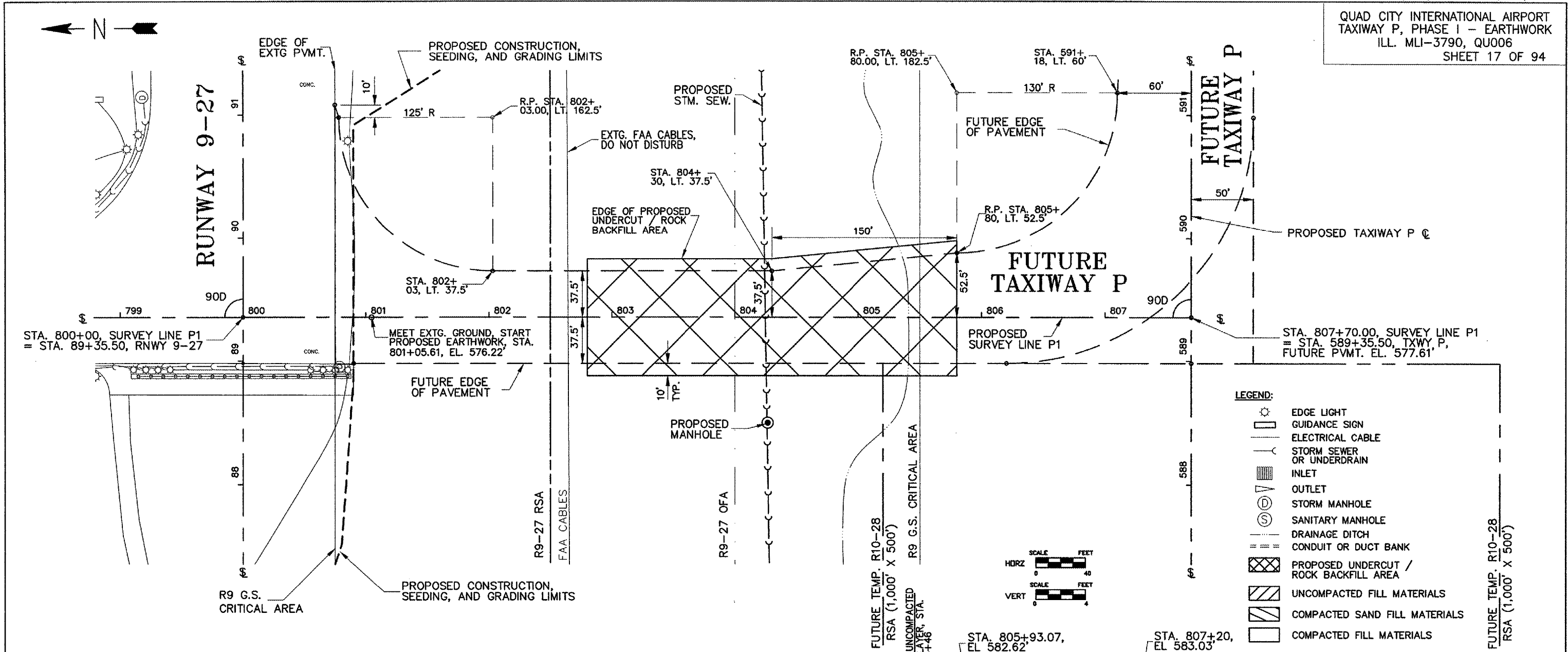
TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM 200	587.10	HUB AT SE COR OF TXY H BORROW AREA, N1742722.2338, E2199623.7550, STATION 107+06.74, LT. 739.58'
TBM K	578.34	TOP OF RCP S SIDE OF SERVICE RD, N1741508.6290, E2201629.7690, STATION 127+42.16, RT. 424.08'
TBM R	581.52	NE COR CONC BASE ELECT TRAN, S SIDE 9-27 BETWEEN SIGNS [] - [] N1741569.4966, E2200420.1701, STATION 115+31.40, RT. 393.12'
TBM T	576.35	CHIS. □ SE COR CONC PAD W SIDE GS BLDG, N1741651.7212, E2198675.6748, STATION 97+85.40, RT. 354.03'
TBM U	583.18	CHIS. □ HOLD SIGN BASE, NE COR, S SIDE R9, N1741775.92035, E2203365.21359, STATION 144+70.44, RT. 113.99'



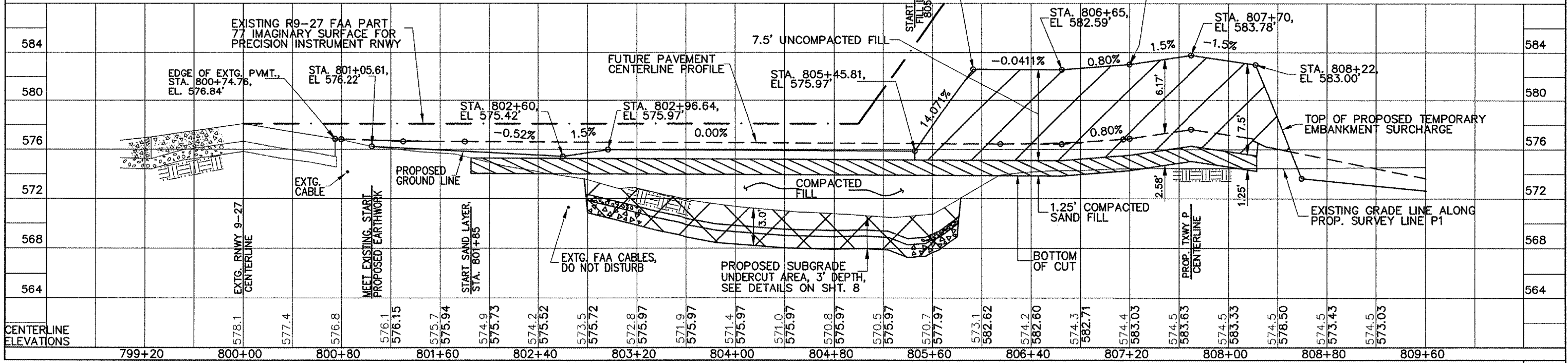
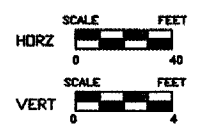
TAXIWAY P PLAN & PROFILE, STA 646+60 TO STA 657+40 16/94

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QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE I - EARTHWORK
ILL. MLI-3790, QU006
SHEET 17 OF 94



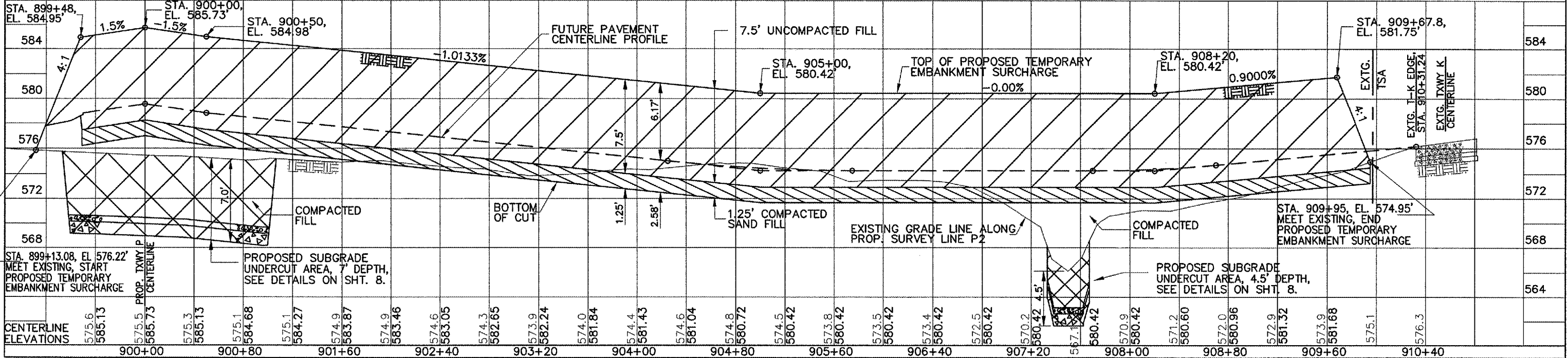
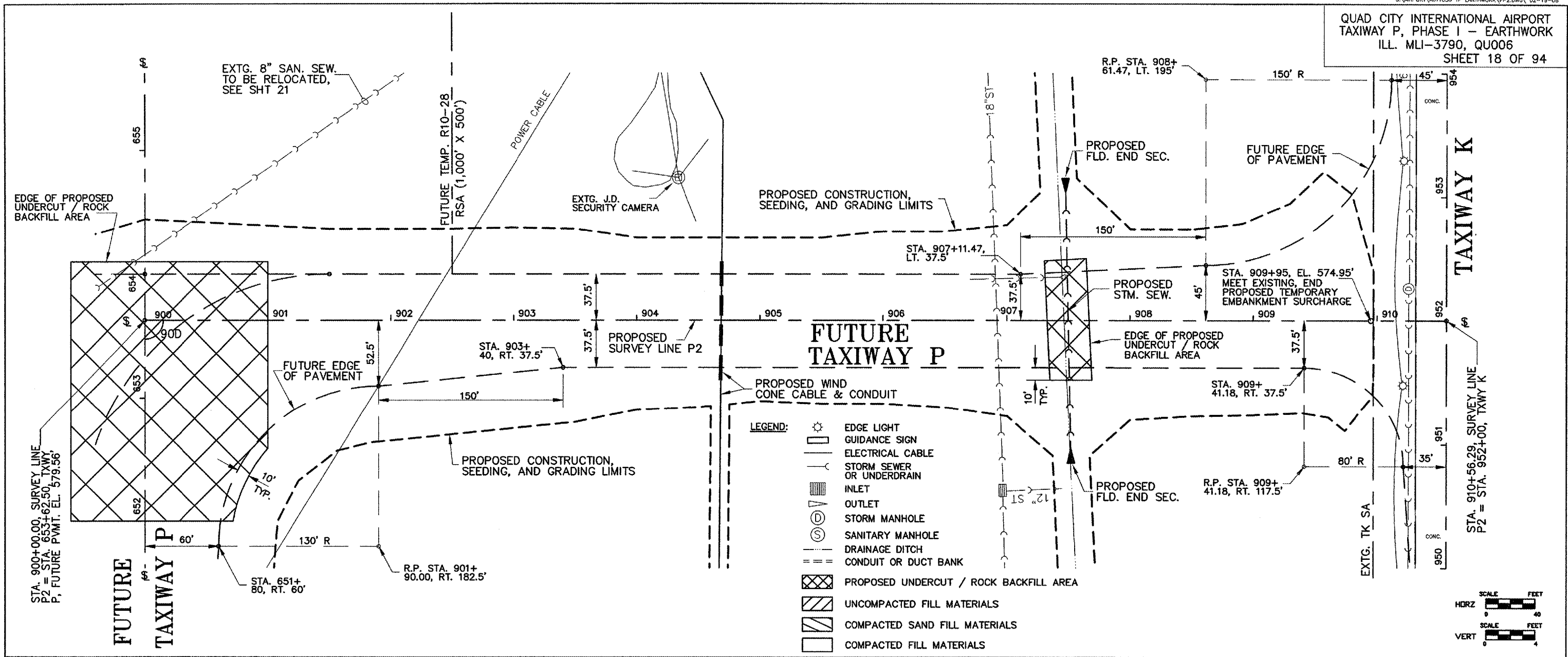
- LEGEND:**
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - INLET
 - OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - PROPOSED UNDERCUT / ROCK BACKFILL AREA
 - UNCOMPACTED FILL MATERIALS
 - COMPACTED SAND FILL MATERIALS
 - COMPACTED FILL MATERIALS



SURVEY LINE P1 PLAN & PROFILE. STA 799+20 TO STA 809+60 17/94

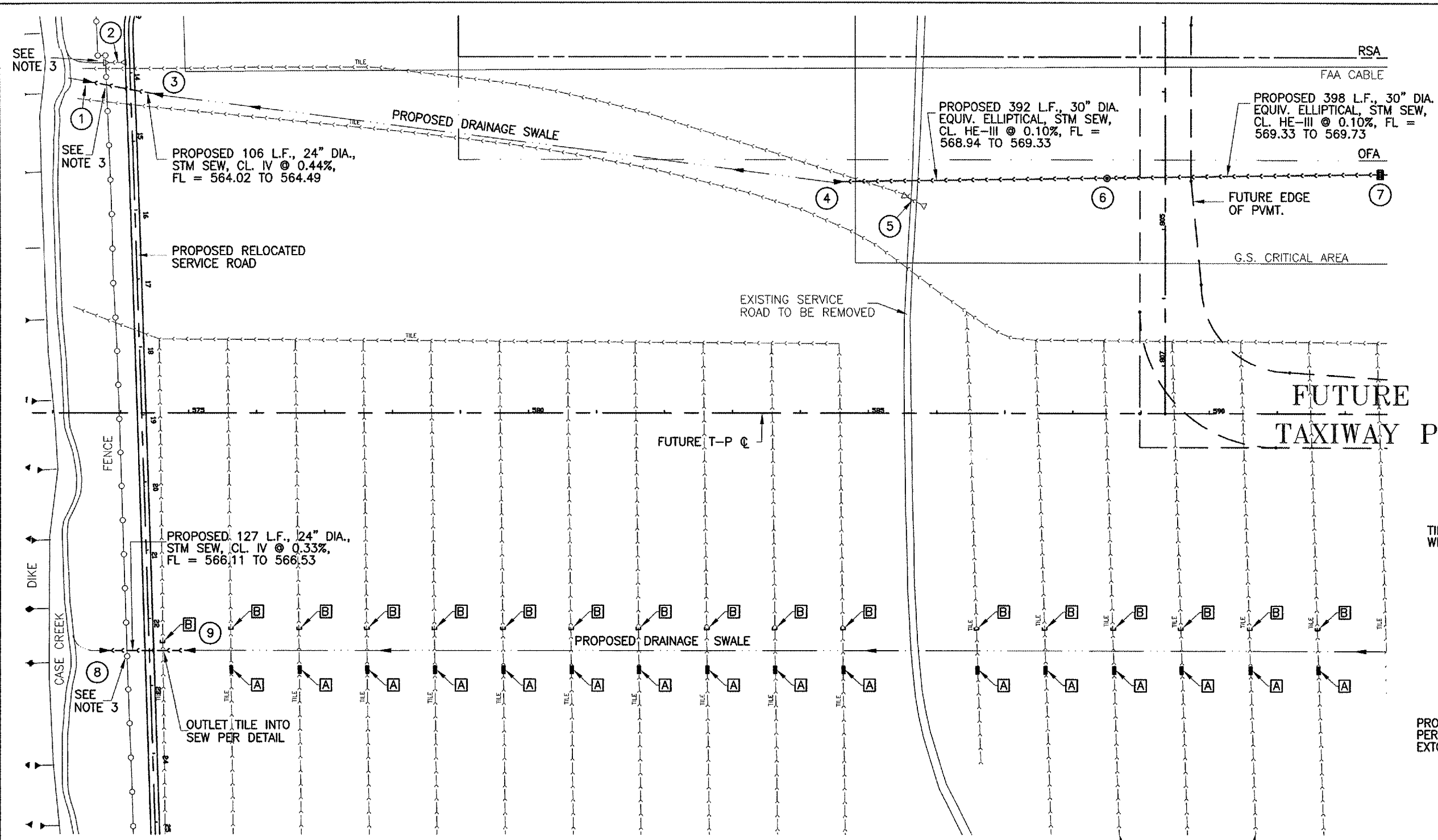
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QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE I - EARTHWORK
ILL. MLI-3790, QU006
SHEET 18 OF 94

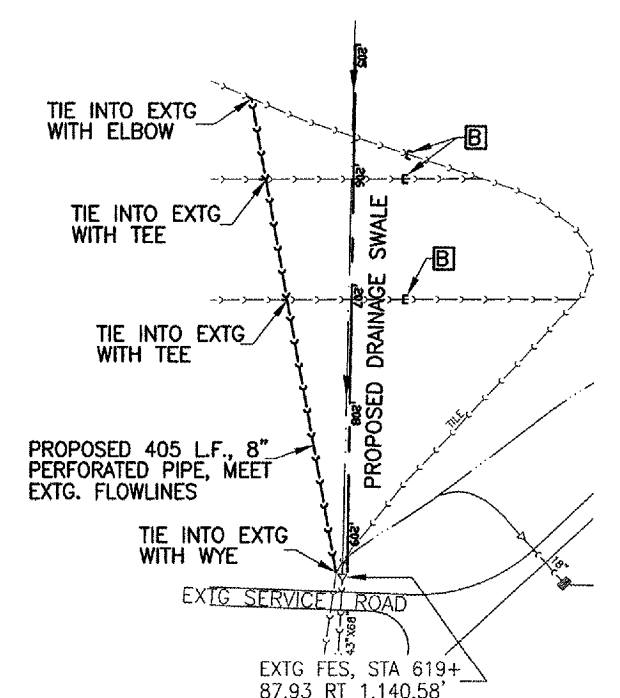


SURVEY LINE P2 PLAN & PROFILE STA 900+00 TO STA 910+40 18/94

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- LEGEND:**
- EXTG EDGE LIGHT
 - EXTG GUIDANCE SIGN
 - EXTG ELECTRICAL CABLE
 - EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
 - EXTG INLET
 - EXTG OUTLET
 - EXTG STORM MANHOLE
 - EXTG SANITARY MANHOLE
 - EXTG DRAINAGE DITCH
 - EXTG CONDUIT OR DUCT BANK
 - PROPOSED MANHOLE
 - PROPOSED INLET
 - PROPOSED FLARED END SECTION
 - PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
 - PROPOSED / EXISTING STRUCTURE NUMBER

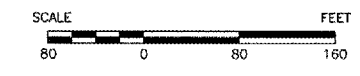


STRUCTURE SCHEDULE

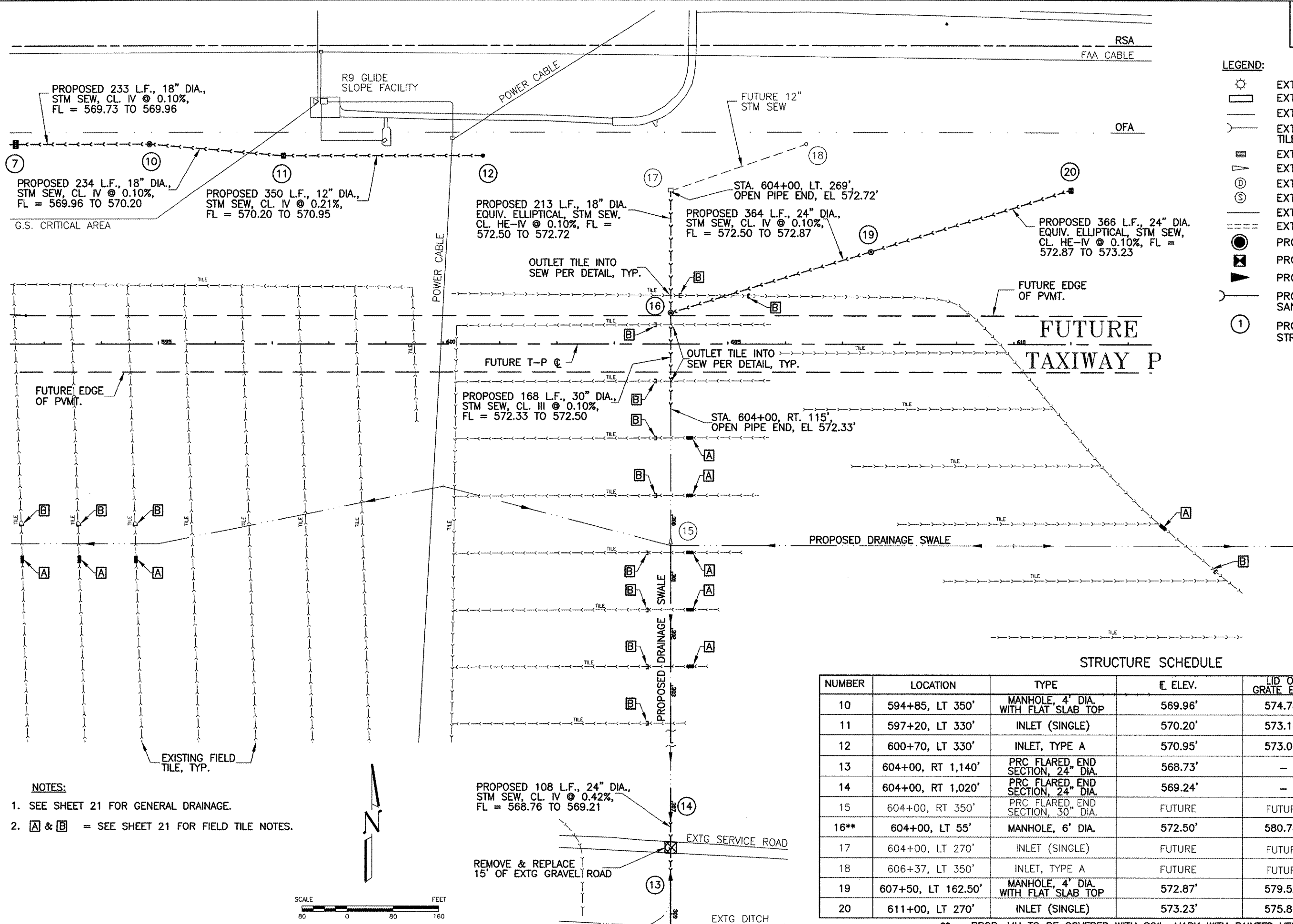
NUMBER	LOCATION	TYPE	ℓ ELEV.	LID OR GRATE ELEV.	REMARK
1*	573+46.46, LT 485.81'	PRC FLARED, END SECTION, 24" DIA.	564.00'	-	SUPPLY GRATING, AR752424 & AR752524
2*	573+93, LT 512'	EXTG. 24" DIA. PVC	565.20'	-	REMOVE EXTG. PIPE & END SECTIONS AS REQD, AR701900
3	574+62.42, LT 464.51'	PRC FLARED, END SECTION, 24" DIA.	564.51'	-	AR752424
4	584+50, LT 338'	PRC FLARED END SECTION, ELLIPTICAL, EQ ROUND SIZE 30"	568.93'	-	AR752730
5	585+67, LT 310'	EXTG. 18" DIA. PVC	569.00'	-	REMOVE EXTG. PIPE & END SECTIONS AS REQD, AR701900
6**	588+50, LT 343.50'	MANHOLE, 5' DIA. WITH FLAT SLAB TOP	569.33'	574.61'	AR751550
7	592+50, LT 350'	INLET, SPECIAL (DOUBLE)	569.73'	572.90'	NEENAH R-3475-3, AR751415
8*	573+69.88, RT 350'	PRC FLARED, END SECTION, 24" DIA.	566.09'	-	SUPPLY GRATING, AR752424 & AR752524
9	575+09.04, RT 350'	PRC FLARED, END SECTION, 24" DIA.	566.55'	-	AR752424

* = SEE NOTE 3. ** = PROP. MH TO BE COVERED WITH SOIL, MARK WITH PAINTED VERTICAL 2" X 4".

- NOTES:**
- SEE SHEET 21 FOR GENERAL DRAINAGE.
 - [A]** & **[B]** = SEE SHEET 21 FOR FIELD TILE NOTES.
 - THE CONTRACTOR SHALL NOT REMOVE ANY AIRPORT PERIMETER SECURITY FENCING. WHERE PROPOSED PIPES TRAVERSE UNDER THE EXISTING SECURITY FENCE, THE CONTRACTOR SHALL BORE UNDERNEATH THE FENCE WITHOUT DAMAGING THE FENCE. SHOULD THE CONTRACTOR DAMAGE THE FENCE OR FENCE MATERIALS, HE SHALL REPLACE THEM AT HIS OWN COST. THE CONTRACTOR SHALL NOT CREATE A BREACH THROUGH OR UNDER THE EXISTING SECURITY FENCE. IN LOCATIONS WHERE EXISTING PIPES ARE REMOVED, CONTRACTOR SHALL BACKFILL VOID MADE BY PIPE REMOVAL WITH COMPACTED SAND. INSTALL 4" OF COMPACTED AR209510 BASE ROCK AROUND BOTTOM OF FENCE TO MATCH EXISTING. SEE SAFETY PLAN FOR ADDITIONAL INFORMATION.



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- LEGEND:**
- EXTG EDGE LIGHT
 - EXTG GUIDANCE SIGN
 - EXTG ELECTRICAL CABLE
 - EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
 - EXTG INLET
 - EXTG OUTLET
 - EXTG STORM MANHOLE
 - EXTG SANITARY MANHOLE
 - EXTG DRAINAGE DITCH
 - EXTG CONDUIT OR DUCT BANK
 - PROPOSED MANHOLE
 - PROPOSED INLET
 - PROPOSED FLARED END SECTION
 - PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
 - PROPOSED / EXISTING STRUCTURE NUMBER

STRUCTURE SCHEDULE

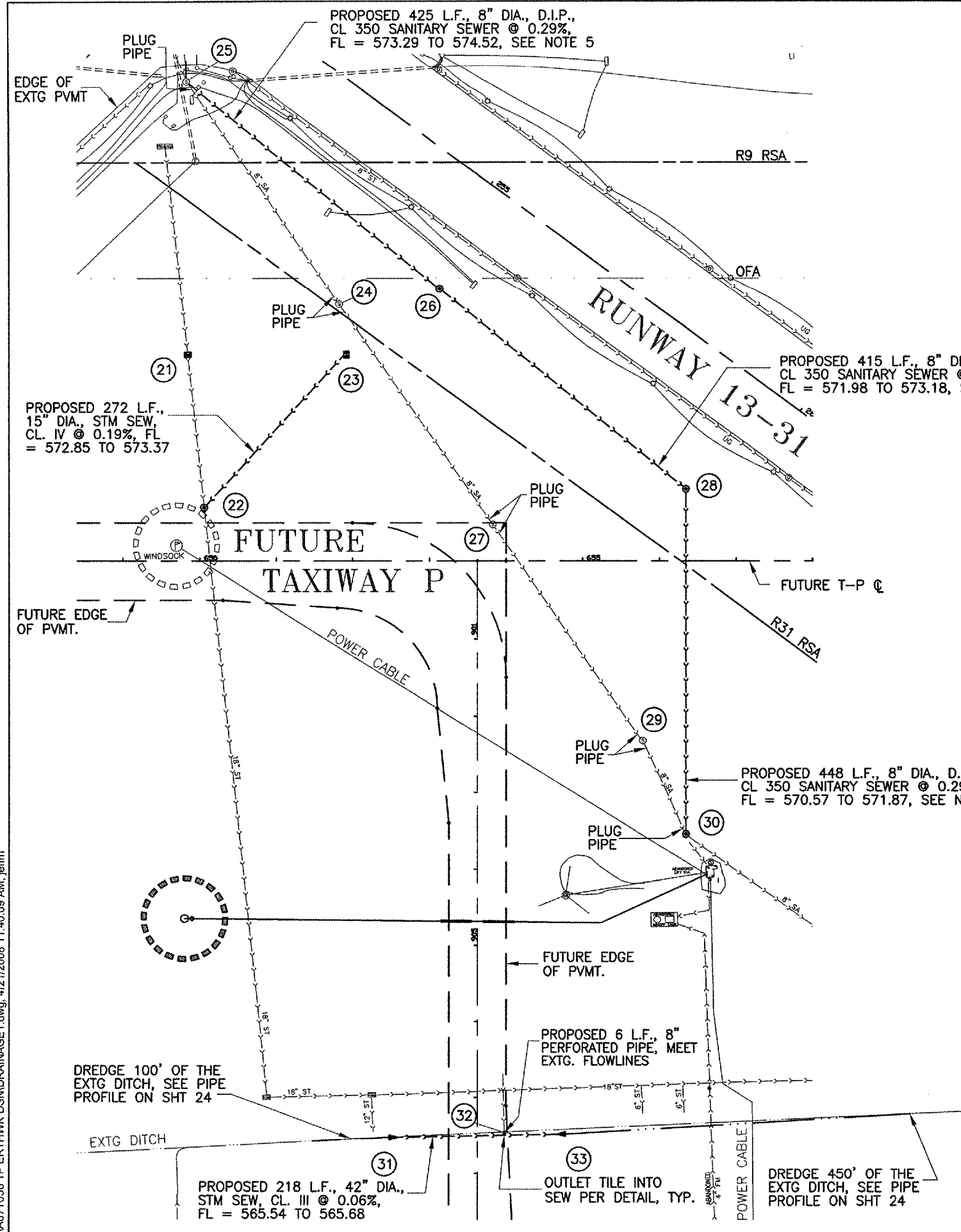
NUMBER	LOCATION	TYPE	E. ELEV.	LID OR GRATE ELEV.	REMARK
10	594+85, LT 350'	MANHOLE, 4' DIA. WITH FLAT SLAB TOP	569.96'	574.78'	NEENAH R-3493-A FR & SOLID LID, AR751540
11	597+20, LT 330'	INLET (SINGLE)	570.20'	573.14'	NEENAH R-3475, AR751410
12	600+70, LT 330'	INLET, TYPE A	570.95'	573.00'	NEENAH R-2535-A, OPEN GRATE, AR751411
13	604+00, RT 1,140'	PRC FLARED END SECTION, 24" DIA.	568.73'	-	AR752424
14	604+00, RT 1,020'	PRC FLARED END SECTION, 24" DIA.	569.24'	-	AR752424
15	604+00, RT 350'	PRC FLARED END SECTION, 30" DIA.	FUTURE	FUTURE	FUTURE
16**	604+00, LT 55'	MANHOLE, 6' DIA.	572.50'	580.78'	NEENAH R-3493-A FR & SOLID LID, AR751560
17	604+00, LT 270'	INLET (SINGLE)	FUTURE	FUTURE	FUTURE
18	606+37, LT 350'	INLET, TYPE A	FUTURE	FUTURE	FUTURE
19	607+50, LT 162.50'	MANHOLE, 4' DIA. WITH FLAT SLAB TOP	572.87'	579.52'	NEENAH R-3493-A FR & SOLID LID, AR751540
20	611+00, LT 270'	INLET (SINGLE)	573.23'	575.84'	NEENAH R-3475, AR751410

** = PROP. MH TO BE COVERED WITH SOIL, MARK WITH PAINTED VERTICAL 2" X 4".

- NOTES:**
- SEE SHEET 21 FOR GENERAL DRAINAGE.
 - A** & **B** = SEE SHEET 21 FOR FIELD TILE NOTES.



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GENERAL DRAINAGE NOTES:

1. CONTRACTOR SHALL HAND DIG AROUND ALL EXISTING UNDERGROUND UTILITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR COST OF REPAIRING ALL DAMAGED UTILITIES.
2. CONTRACTOR TO EXCAVATE AND EXPOSE EXISTING STORM PIPES, ELEC. CABLES, AND ELEC. DUCT BANKS AT PROPOSED MANHOLE, INLET, AND PIPE CROSSING LOCATIONS TO FIELD VERIFY VERTICAL LOCATIONS OF EXISTING UTILITIES. ENGINEER MAY ADJUST SLOPES AND TIE-INS AT THE TIME OF CONSTRUCTION AS REQUIRED.
3. OUTLET PROPOSED / EXISTING UNDERDRAIN / FIELD TILE INTO EXISTING / PROPOSED STORM SEWER PER DETAIL.
4. THE LOCATIONS OF THE EXISTING FIELD TILES SHOWN ON THESE DRAWINGS WERE TAKEN FROM AIRPORT RECORDS AND ARE APPROXIMATE. ACTUAL LOCATIONS TO BE DETERMINED BY THE CONTRACTOR IN THE FIELD AT THE TIME OF CONSTRUCTION. CONTRACTOR SHALL EXCAVATE AND EXPOSE THE EXISTING FIELD TILES, AS DIRECTED BY THE ENGINEER, PRIOR TO ORDERING MATERIALS. ENGINEER SHALL SET SLOPES AND TIE-INS AT THE TIME OF CONSTRUCTION AS REQUIRED. THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT RATES.
5. ALL CONSTRUCTION WORK LOCATED WITHIN THE EXISTING RUNWAY 13-31 SAFETY AREA SHALL BE ACCOMPLISHED DURING NIGHT SHIFTS. SEE THE SAFETY PLAN AND PROJECT SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

LEGEND:

- EXTG EDGE LIGHT
- EXTG GUIDANCE SIGN
- EXTG ELECTRICAL CABLE
- EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
- EXTG INLET
- EXTG OUTLET
- EXTG STORM MANHOLE
- EXTG SANITARY MANHOLE
- EXTG DRAINAGE DITCH
- EXTG CONDUIT OR DUCT BANK
- PROPOSED MANHOLE
- PROPOSED INLET
- PROPOSED FLARED END SECTION
- PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
- PROPOSED / EXISTING STRUCTURE NUMBER

FIELD TILE NOTES:

- A** LOCATE EXISTING FIELD TILES, INSTALL PROPOSED HEADWALLS ON EXTG. FIELD TILES AND START FIELD TILE REMOVAL AT THESE LOCATIONS. EXACT LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER AT THE TIME OF CONSTRUCTION. COSTS OF LOCATING TILES SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. COSTS OF REMOVING FIELD TILES BETWEEN POINTS A AND B SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES.
- B** LOCATE EXISTING FIELD TILES, INSTALL PROPOSED END CAPS ON EXTG. FIELD TILES AND END FIELD TILE REMOVAL AT THESE LOCATIONS. EXACT LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER AT THE TIME OF CONSTRUCTION. COSTS OF LOCATING TILES SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES.
- C** CONTRACTOR SHALL REPAIR AND / OR REPLACE ANY FIELD TILE DAMAGED DURING CONSTRUCTION. COSTS OF REPAIRS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES.

STRUCTURE SCHEDULE

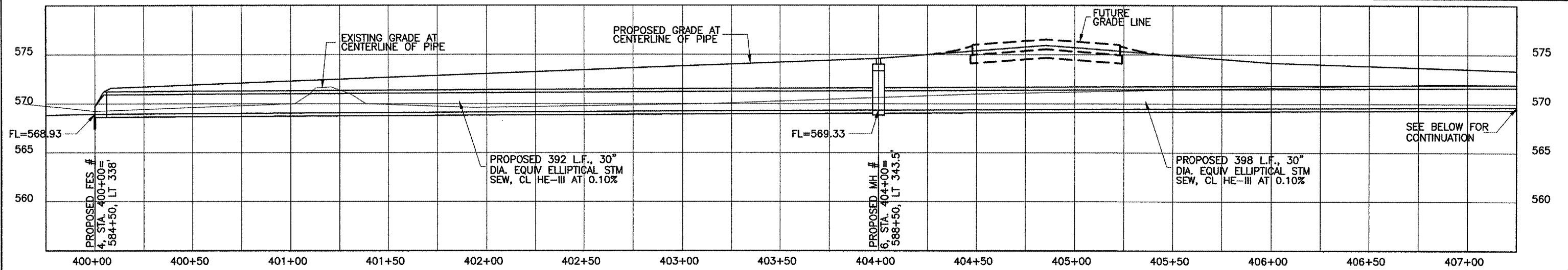
NUMBER	LOCATION	TYPE	E ELEV.	LID OR GRATE ELEV.	REMARK
21	649+84.33, LT 270'	INLET (SINGLE)	MEET EXTG FLOWLINE, $\mp 573.83'$	576.10'	NEENAH R-3475, AR751410
22**	650+05.76, LT 70'	MANHOLE, 4' DIA. WITH FLAT SLAB TOP	MEET EXTG FLOWLINE, $\mp 572.83'$	578.97'	NEENAH R-3493-A FR & SOLID LID, AR751540
23	651+92, LT 270'	INLET (SINGLE)	573.37'	575.50'	NEENAH R-3475, AR751410
24***	651+82.38, LT 335.80'	EXTG. SANITARY SEWER MANHOLE	573.15'	578.16'	REMOVE, AR751903
25***	649+83.12, LT 624.17'	EXTG. SANITARY SEWER MANHOLE	574.52'	581.70'	NO CHANGE
26***	653+12.93, LT 356.12'	SANITARY MANHOLE, 4' DIA WITH FLAT SLAB TOP	NW = 573.29' SE = 573.18'	MEET EXTG GROUND $\mp 578.10'$	NEENAH R-3493-A FR & SOLID LID, AR770704
27	653+82.65, LT 47.91'	EXTG. SANITARY SEWER MANHOLE	571.76'	576.31'	REMOVE, AR751903
28***	656+34.41, LT 94.21'	SANITARY MANHOLE, 4' DIA WITH FLAT SLAB TOP	NW = 571.98' S = 571.87'	MEET EXTG GROUND $\mp 575.36'$	NEENAH R-3493-A FR & SOLID LID, AR770704
29	655+78.37, RT 232.31'	EXTG. SANITARY SEWER MANHOLE	570.45'	574.55'	REMOVE, AR751903
30	656+34.41, RT 353.98'	EXTG. SANITARY SEWER MANHOLE	PROP. N = 570.57' EXTG. SE = 570.45'	575.15'	REPLACE, 4'DIA., AR770925
31	907+52.35, RT 115'	PRC FLARED END SECTION, 42" DIA.	565.68'	-	AR752442
32	907+47.70, LT 35.21'	EXTG. PRC FLARED END SECTION	EXTG FLOWLINE, EL $\mp 568.00'$	-	REMOVE, INCLUDE REMOVAL COSTS IN PIPE CONTRACT UNIT PRICES
33	907+47.84, LT 115'	PRC FLARED END SECTION, 42" DIA.	565.54'	-	AR752442'

** = PROP. MH TO BE COVERED WITH SOIL, MARK WITH PAINTED VERTICAL 2" X 4".

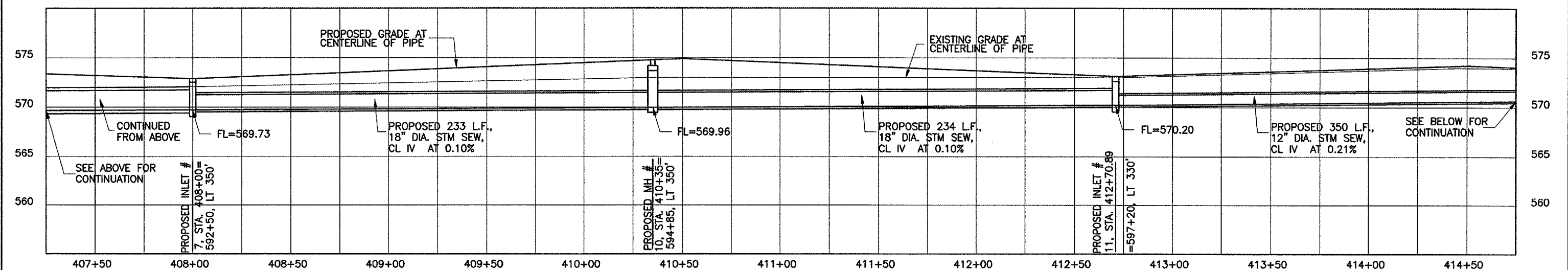
*** = SEE NOTE 5.

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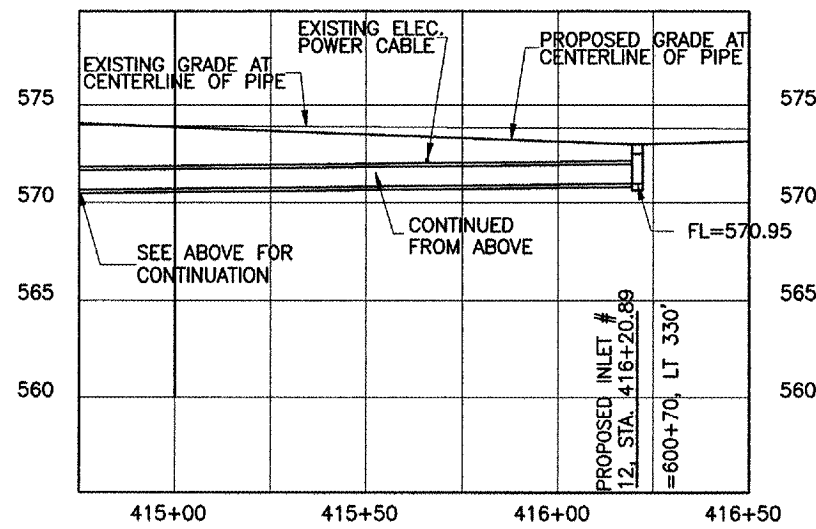
TAXIWAY P, STA. 584+50 TO 600+70 LEFT STORM SEWER CENTERLINE PROFILE



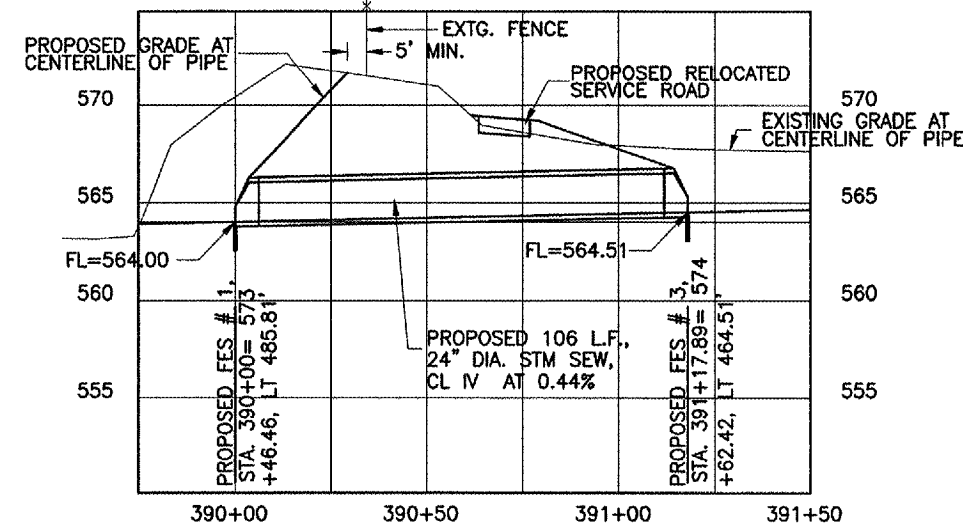
TAXIWAY P, STA. 584+50 TO 600+70 LEFT STORM SEWER CENTERLINE PROFILE, CONTINUED



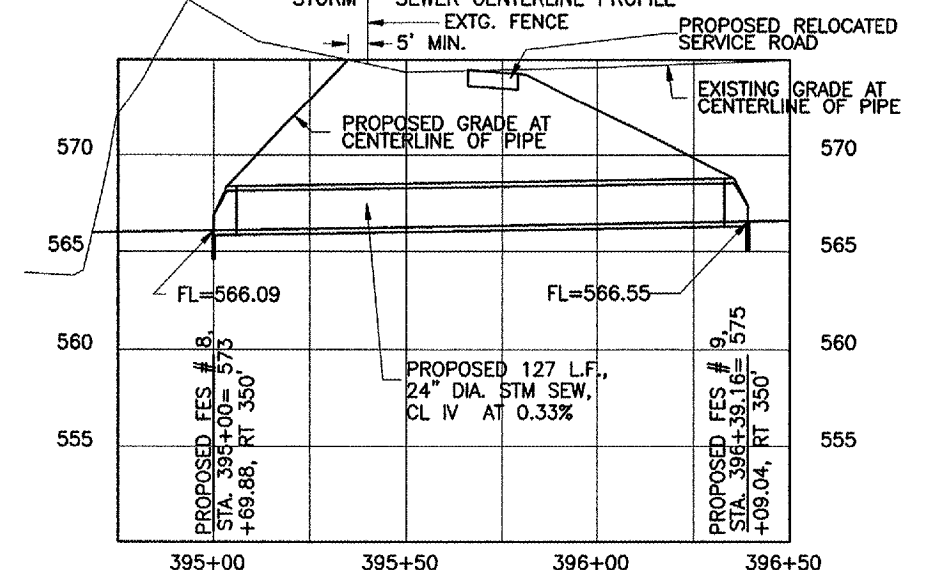
TAXIWAY P, STA. 584+50 TO 600+70 LEFT STORM SEWER CENTERLINE PROFILE, CONTINUED



TAXIWAY P, STA. 573+46 TO 574+65 LEFT STORM SEWER CENTERLINE PROFILE

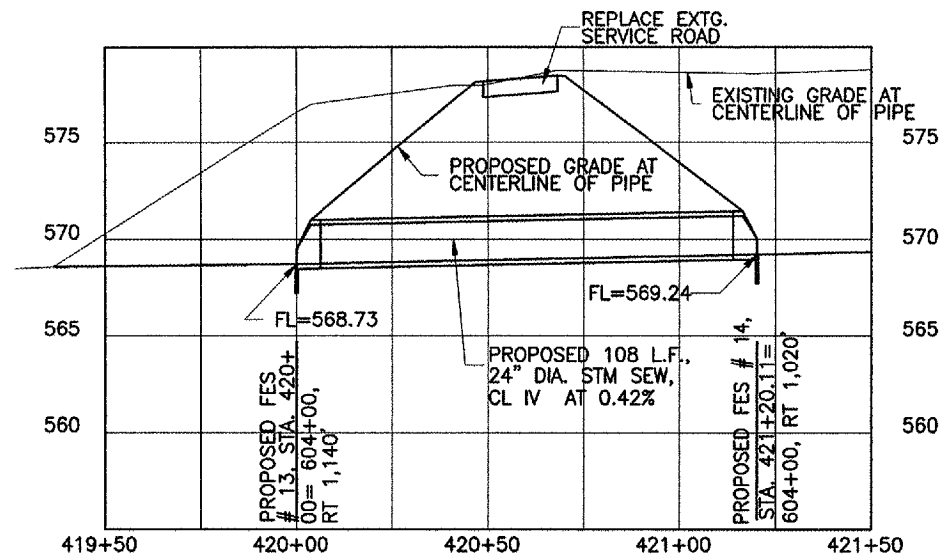


TAXIWAY P, STA. 573+70 TO 575+10 RIGHT 350' STORM SEWER CENTERLINE PROFILE

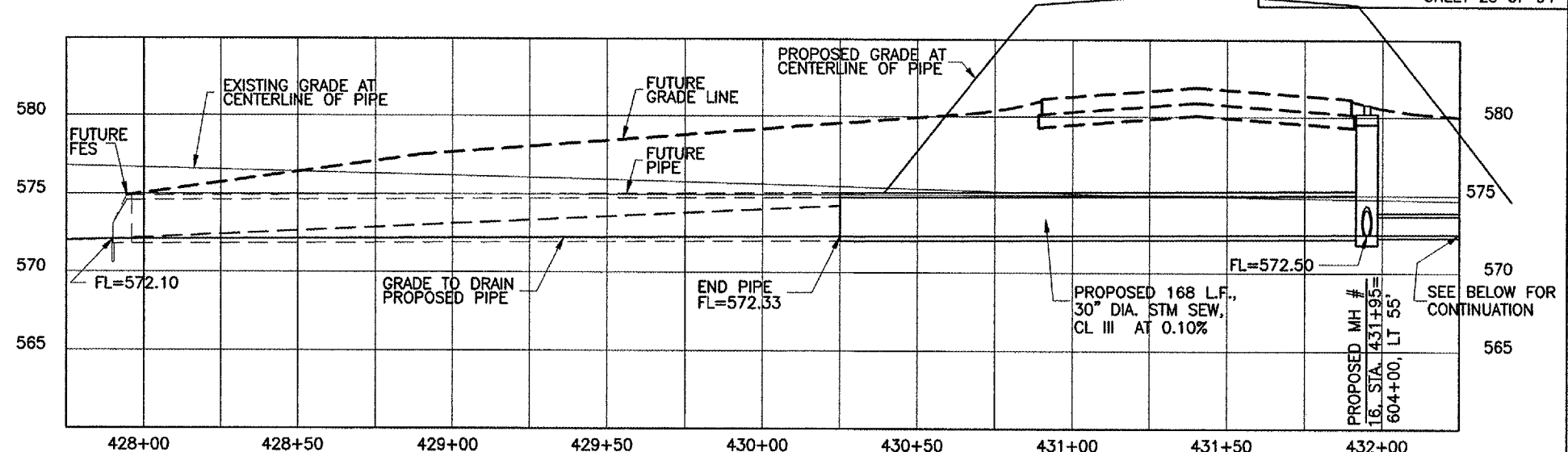


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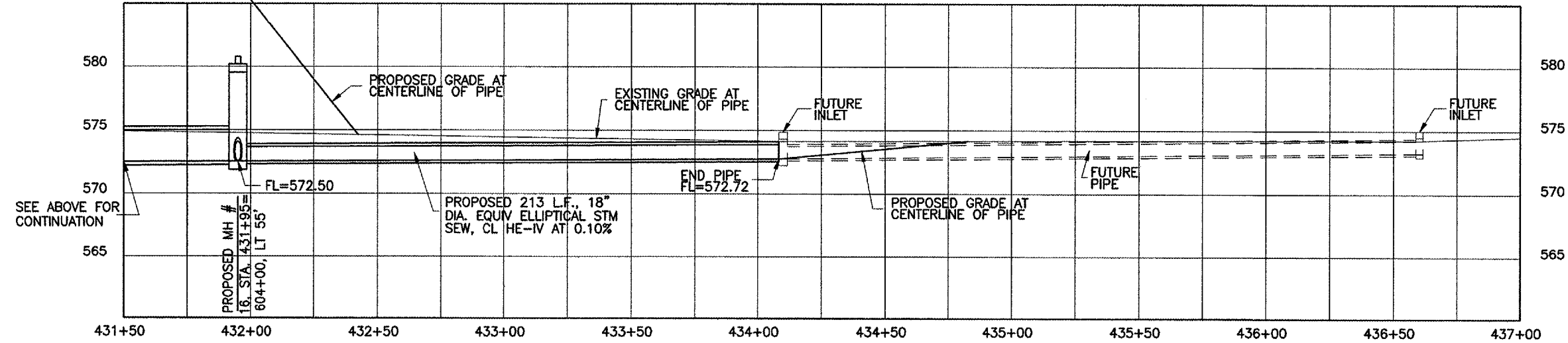
TAXIWAY P, STA. 604+00 RIGHT 1,020' TO 1,140'
STORM SEWER CENTERLINE PROFILE



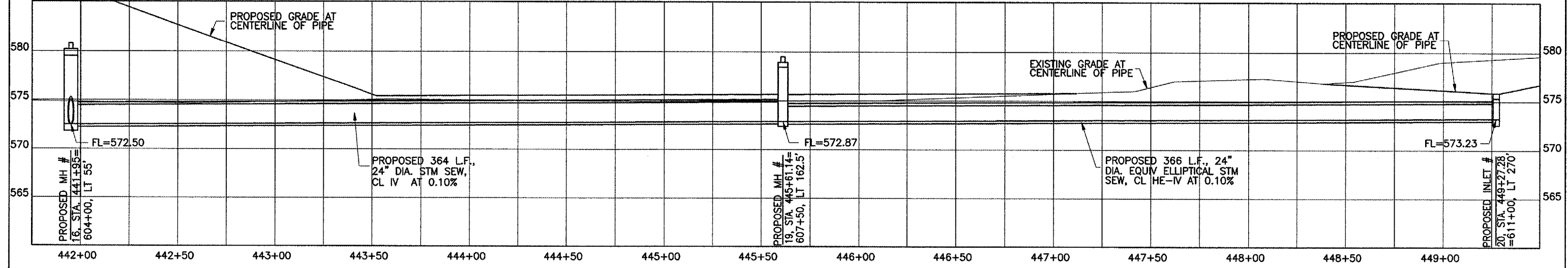
TAXIWAY P, STA. 604+00 RIGHT 350' TO LEFT 55'
STORM SEWER CENTERLINE PROFILE



TAXIWAY P, STA. 604+00 LEFT 55' TO STA. 606+37 LEFT 350'
STORM SEWER CENTERLINE PROFILE

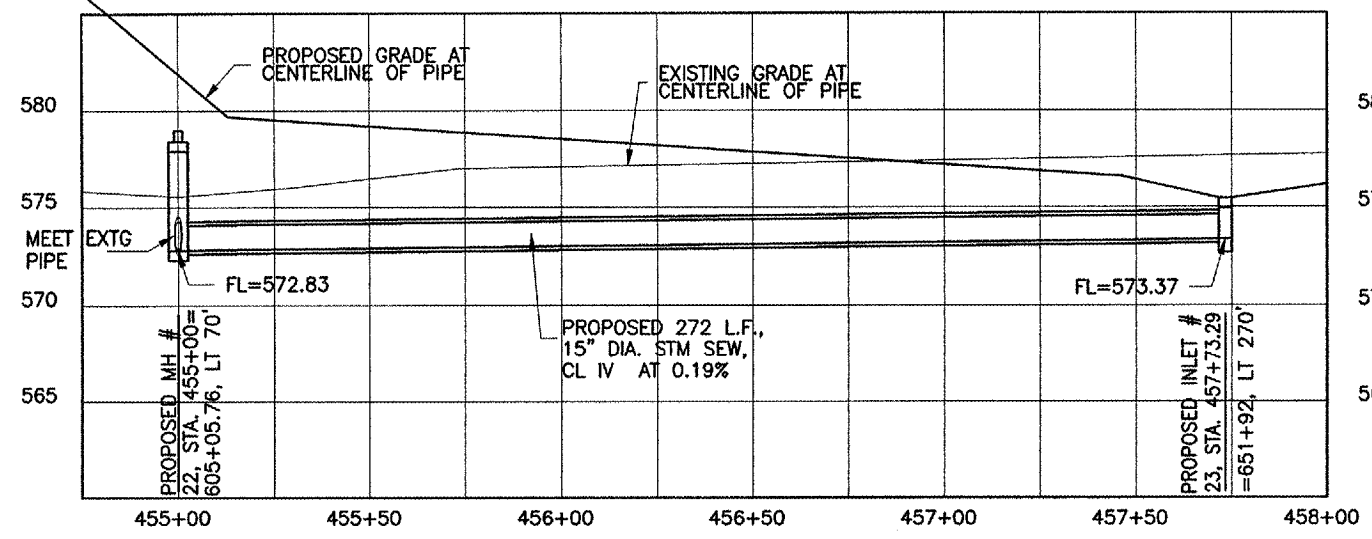


TAXIWAY P, STA. 604+00 LEFT 55' TO STA. 611+00 LEFT 270'
STORM SEWER CENTERLINE PROFILE

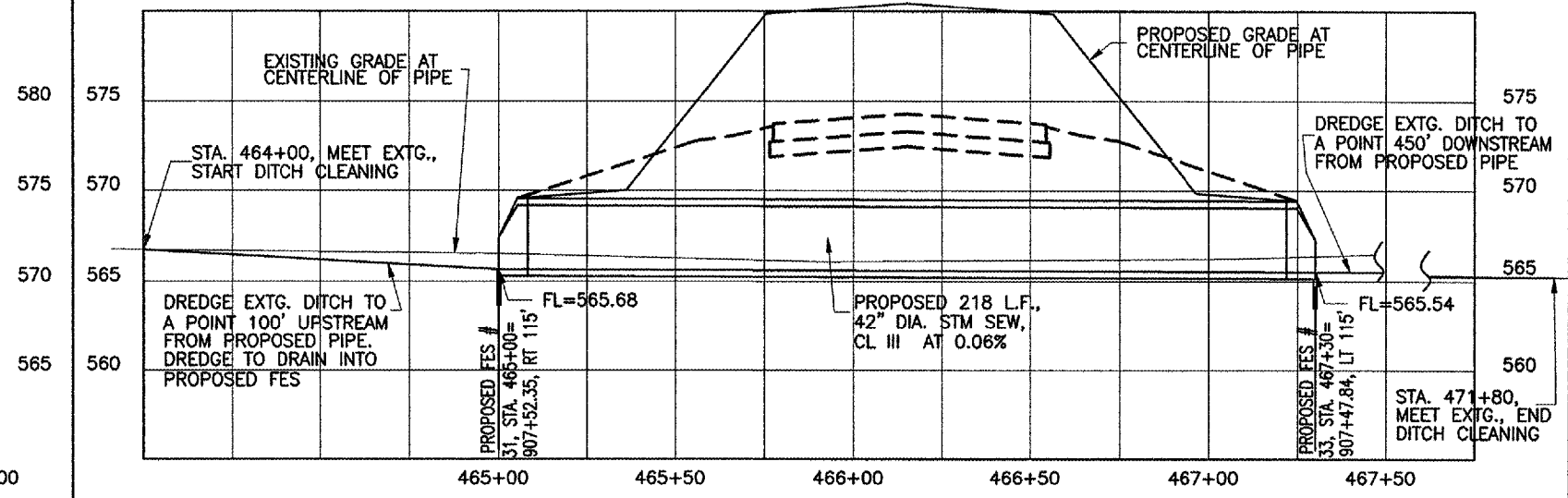


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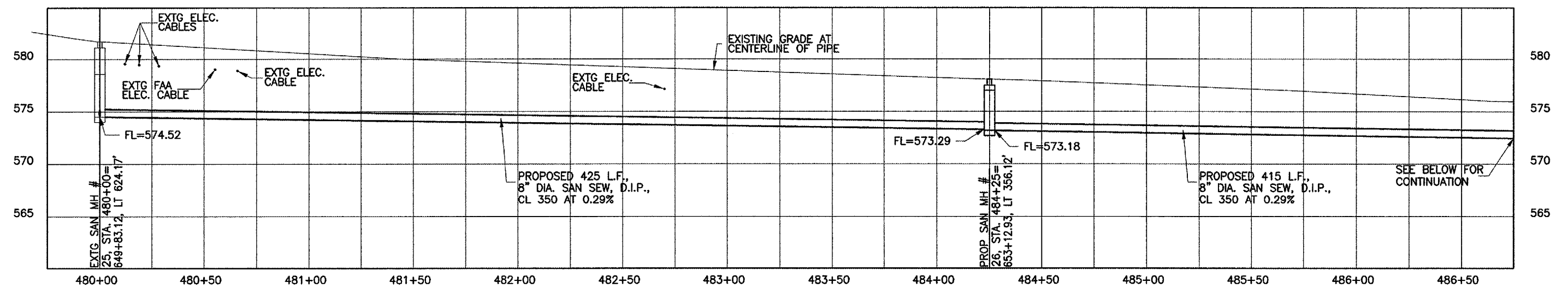
TAXIWAY P, STA. 605+05.76 LEFT 70' TO STA. 651+92 LEFT 270'
STORM SEWER CENTERLINE PROFILE



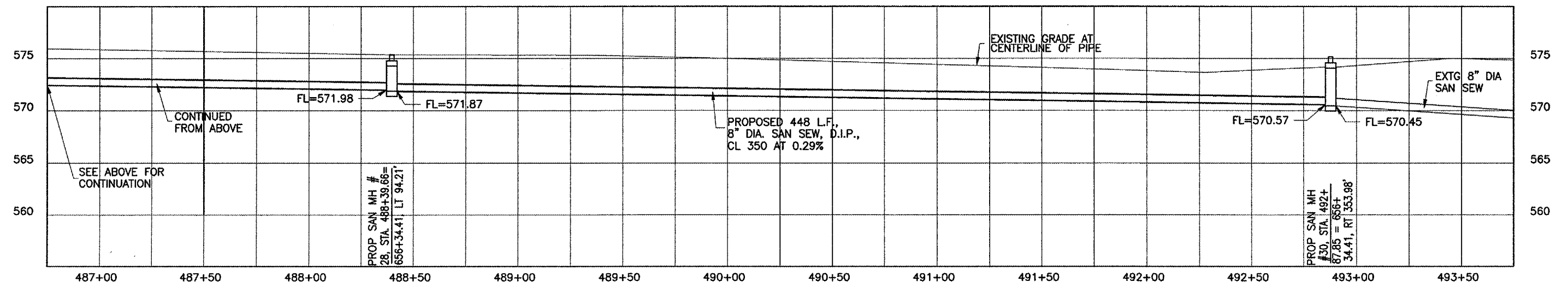
TAXIWAY P2, STA. 907+52.35 RIGHT 115' TO STA. 907+47.84 LEFT 115'
STORM SEWER CENTERLINE PROFILE



TAXIWAY P, STA. 649+83.12 LEFT 624.17' TO STA. 656+34.41 RIGHT 353.98'
SANITARY SEWER CENTERLINE PROFILE

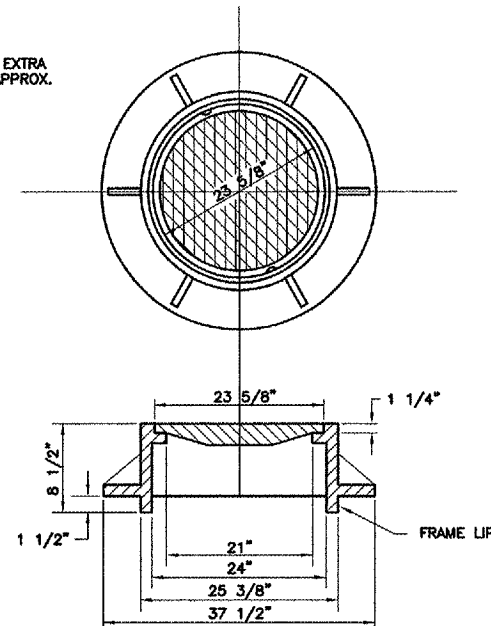


TAXIWAY P, STA. 649+83.12 LEFT 624.17' TO STA. 656+34.41 RIGHT 353.98'
SANITARY SEWER CENTERLINE PROFILE, CONTINUED



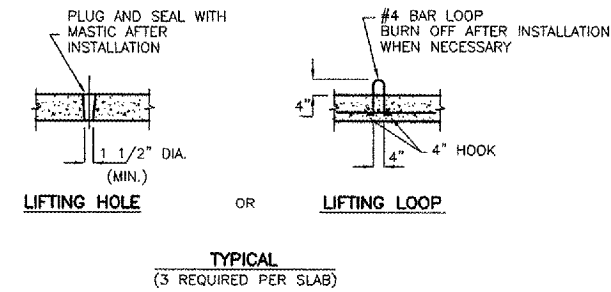
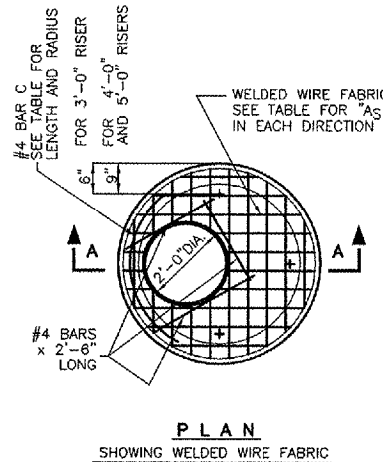
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NEENAH R-3493-A OR EQUAL EXTRA HEAVY DUTY FRAME AND LID. APPROX. WEIGHT 440 POUNDS.

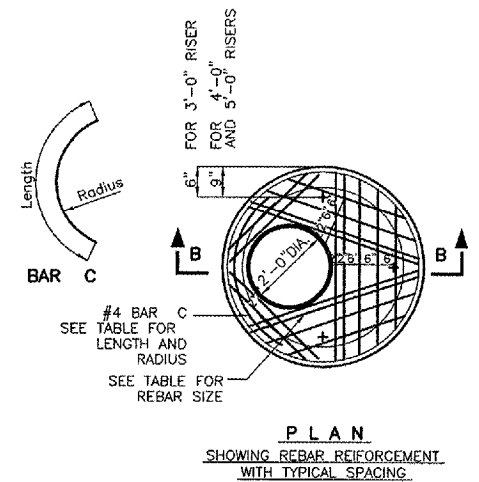


- NOTES:
1. USE FOR ALL MANHOLES (UNLESS NOTED OTHERWISE ON THE PLAN SHEETS).
 2. REMOVE BOLTS THAT HOLD THE GRATE TO THE FRAME AND DO NOT REINSTALL.

MANHOLE FRAME AND LID

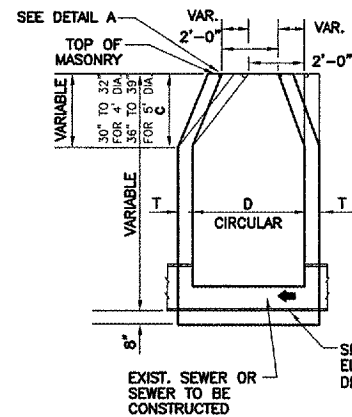


D	T	D ₀ (MIN)	f	REINFORCEMENT		#4 BAR C	
				"A" S" W.W.F. EACH DIRECTION	OR BAR SIZE	LENGTH	RADIUS
3'-0"	See Standards 1514, 1526, 1527, and 1885	D + 2"	6"	.20 sq.in./lin.ft.	#4	4'-0"	1'-7"
4'-0"			.35 sq.in./lin.ft.	#5	4'-6"	2'-2"	
5'-0"			.35 sq.in./lin.ft.	#5	5'-0"	2'-8"	

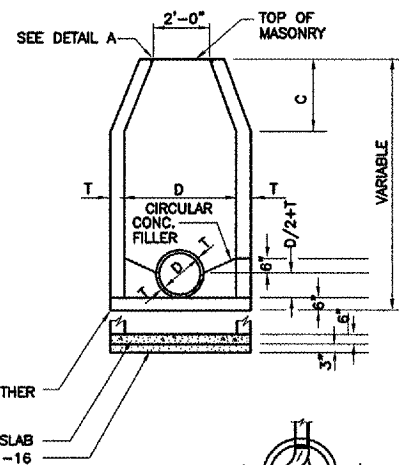


ALT. MATERIAL FOR WALLS	D	C	T
PRECAST REINFORCED CONCRETE RINGS	4'	2'-6"	5"
	5'	3'-0"	6"
MONOLITHIC CONCRETE	4'	2'-6"	6"
	5'	3'-0"	8"

FOR 6" DIAMETER MANHOLES, SEE IDOT STANDARD 602406. MANHOLE FRAME LIP NOTCH OUT DETAIL AND ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 602406.



- NOTES:
- PRECAST REINFORCED CONCRETE RINGS AND MANHOLE FRAMES SHALL BE LAID IN FULL MOTAR BEDS WITH FLUSH JOINTS.
 - MOTAR MIX SHALL CONSIST OF 1 PART PORTLAND CEMENT & 3 PARTS SAND.
 - ALL CONCRETE SHALL HAVE A MIN. STRENGTH AT 28 DAYS OF 3500 P.S.I.
 - THE CONTRACT UNIT PRICE FOR MANHOLE SHALL INCLUDE INSTALLATION OF THE FRAME AND LID.



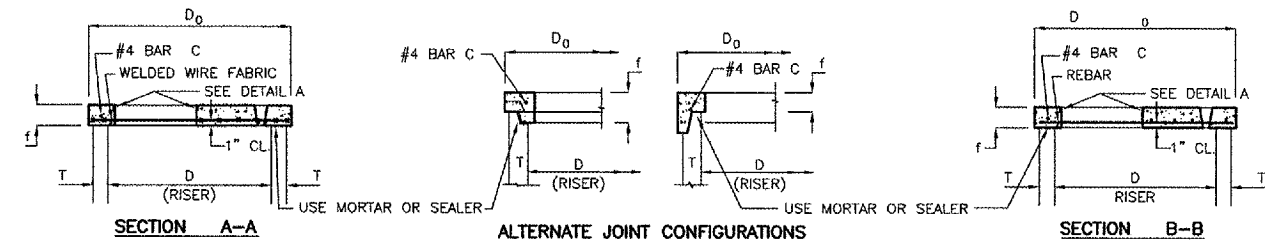
SEE PLAN SHEETS FOR ELEVATION OF INVERT AND DIAMETER OF SEWER

BOTTOM TO BE EITHER PLAIN CONCRETE OR PRECAST REINFORCED CONCRETE SLAB IDOT CA-14 OR -16

FURNISHING AND INSTALLING SAND CUSHION, FRAME, AND LID TO BE INCLUDED IN THE CONTRACT UNIT PRICE

NOTE:
THE CONE OF THE MANHOLE SHALL BE CONSTRUCTED AS SHOWN BY THE DOTTED LINES ONLY WHEN THERE IS INTERFERENCE WITH UNDERGROUND CONDITIONS AND THESE CONDITIONS CAN NOT BE ALTERED.

DETAIL OF STORM MANHOLE
ITEMS AR751540, AR751550, & AR751560

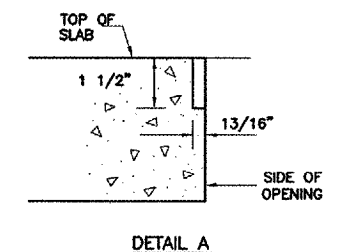


PRECAST REINFORCED CONCRETE
FLAT SLAB TOP
FOR MANHOLES, CATCH BASINS
AND VALVE VAULTS

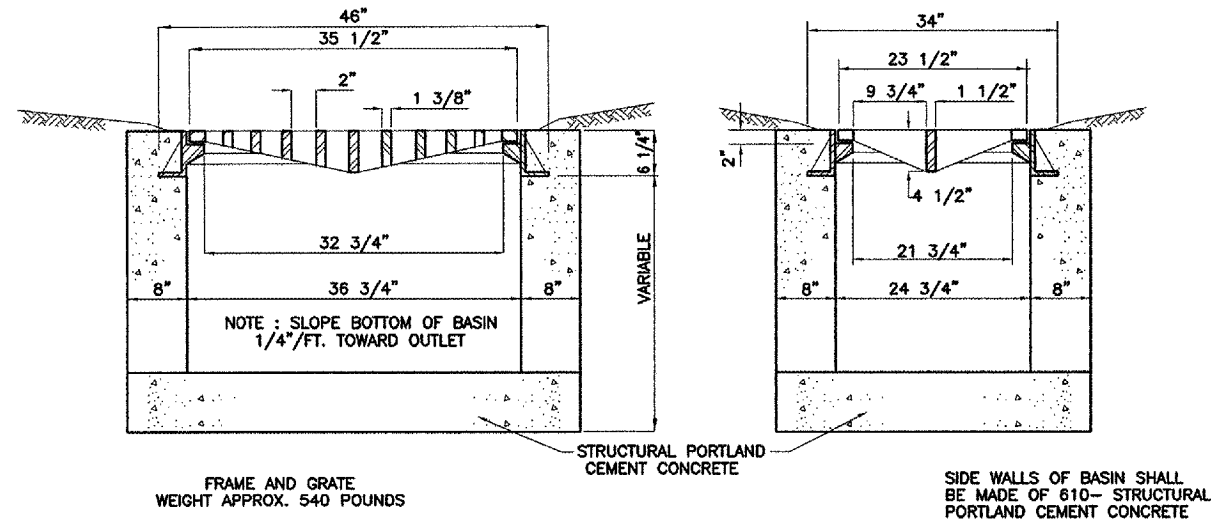
MODIFIED I.D.O.T.
STANDARD 602601

NOTES:

1. PRECAST FLAT SLAB TOPS SHALL CONFORM TO SECTION 602 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
2. REINFORCEMENT BARS OR WELDED WIRE FABRIC SHALL BE IN ACCORDANCE WITH ARTICLE 1006.04 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
3. JOINT CONFIGURATION AND DIMENSIONS SHALL MATCH AND FIT THE RISER JOINT DETAIL.
4. LIFTING DEVICES OTHER THAN SHOWN MAY BE USED SUBJECT TO APPROVAL BY THE ENGINEER.
5. THE FLAT SLAB TOP MAY BE USED IN LIEU OF THE TAPERED TOPS SHOWN ON STANDARDS 602001, 602011, 602306, 602401, OR 602501 AT THE OPTION OF THE CONTRACTOR OR WHEN FIELD CONDITIONS PROHIBIT THE USE OF TAPERED TOPS.
6. THE COST OF FURNISHING AND INSTALLING THE FLAT SLAB TOP SHALL BE INCLUDED IN THE UNIT PRICE FOR CATCH BASINS, MANHOLES, OR VALVE VAULTS.



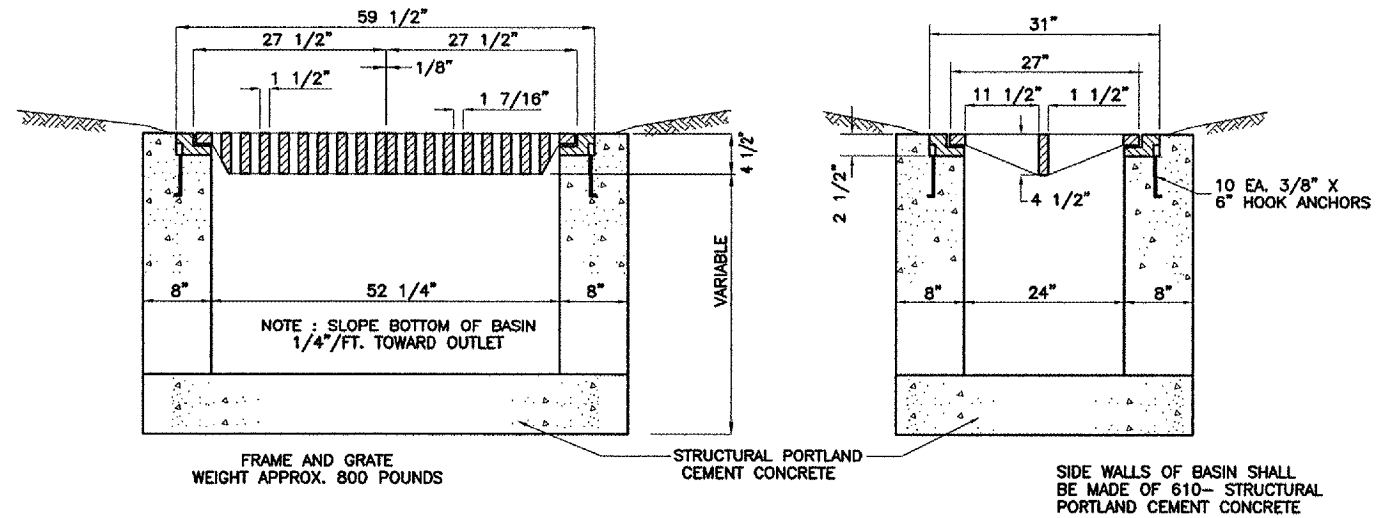
FOR 6" DIAMETER MANHOLES, SEE IDOT STANDARD 602406. MANHOLE FRAME LIP NOTCH OUT DETAIL AND ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 602406.



INLET (SINGLE), ITEM 751410

NOTES :

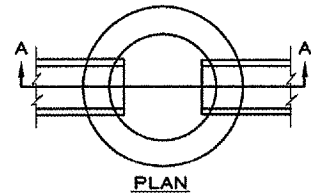
1. INLET SHALL INCLUDE FRAME WITH SINGLE GRATE OF TYPE SIMILAR AND EQUAL TO NO. R 3475 AS SHOWN BY CATALOG "R" ELEVENTH EDITION OF NEENAH FOUNDRY CO.
2. REMOVE BOLTS THAT HOLD THE GRATE TO THE FRAME AND DO NOT REINSTALL.



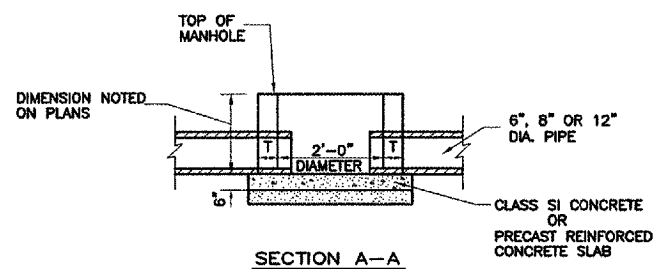
INLET SPECIAL (DOUBLE), ITEM 751415

NOTES :

1. INLET SPECIAL SHALL INCLUDE FRAME WITH SINGLE GRATE OF TYPE SIMILAR AND EQUAL TO NO. R 3475-3 AS SHOWN BY CATALOG "R" ELEVENTH EDITION OF NEENAH FOUNDRY CO.
2. REMOVE BOLTS THAT HOLD THE GRATE TO THE FRAME AND DO NOT REINSTALL.

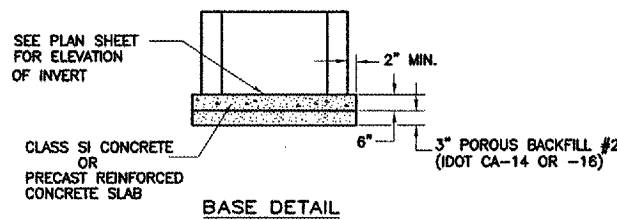


ALTERNATE MATERIALS FOR WALLS	T
PRECAST REINFORCED CONCRETE SECTION	3"
PRECAST REINFORCED CONCRETE RINGS	5"
CAST IN PLACE CONCRETE	6"



NOTES :

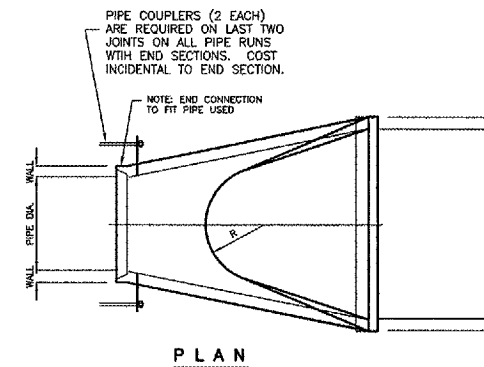
- 1) PRECAST REINFORCED CONCRETE RINGS AND MANHOLE FRAMES SHALL BE LAID IN FULL MORTAR BEDS WITH FLUSH JOINTS.
- 2) MORTAR MIX SHALL CONSIST OF 1 PART PORTLAND CEMENT AND 3 PARTS SAND.
- 3) ALL CONCRETE SHALL HAVE A MIN. STRENGTH AT 28 DAYS OF 3500 P.S.I.
- 4) THE CONTRACT UNIT PRICE FOR MANHOLE SPECIAL SHALL INCLUDE FURNISHING AND INSTALLING THE FRAME AND LID, THE SAND CUSHION, AND COMPACTING THE BACKFILL MATERIAL. SEE PLAN SHEETS FOR FRAME AND LID TYPE.



DETAIL OF INLET, TYPE A
ITEM AR751411

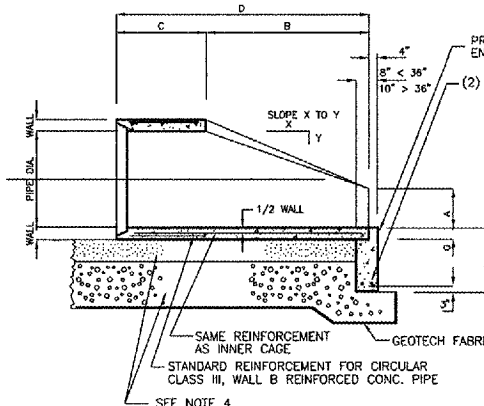
MODIFIED IDOT
STANDARD 602301

PRECAST REINFORCED CONCRETE FLARED END SECTION

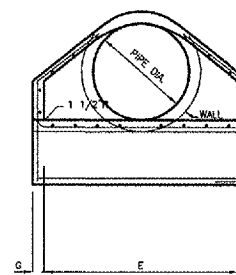


PIPE DIA.	APPROX. WT. (LBS.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4'	2'-0"	4'-0-7/8"	6'-0-7/8"	2'-0"	2'	9'	3:1
15"	740	2-1/4"	6'	2'-3"	3'-10"	6'-1"	2'-6"	2-1/4"	11'	3:1
18"	950	2-1/2"	9'	2'-3"	3'-10"	6'-1"	3'-0"	2-1/2"	12'	3:1
21"	1280	2-3/4"	9'	2'-11"	3'-2"	6'-1"	3'-6"	2-3/4"	13'	3:1
24"	1520	3"	9-1/2"	3'-7-1/2"	2'-6"	6'-1-1/2"	4'-0"	3"	14'	3:1
27"	1930	3-1/4"	10-1/2"	4'-0"	2'-1-1/2"	6'-1-1/2"	4'-6"	3-1/4"	14-1/2"	3:1
30"	2190	3-1/2"	1'-0"	4'-6"	1'-7-3/4"	6'-1-3/4"	5'-0"	3-1/2"	15'	3:1
33"	3200	3-3/4"	1'-1-1/2"	4'-10-1/2"	3'-3-1/4"	8'-1-3/4"	5'-6"	3-3/4"	17-1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10-3/4"	8'-1-3/4"	6'-0"	4"	20'	3:1
42"	5380	4-1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4-1/2"	22'	3:1
48"	6590	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22'	3:1
54"	8240	5-1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5-1/2"	24'	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6-1/2"	6'-0"	2'-6"	2'-3"	8'-3"	8'-6"	5-1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.85:1
78"	14770	7-1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6-1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6-1/2"	1'-9"	9-3-1/2"	10'-0"	6-1/2"	*	1.5:1

*RADIUS AS FURNISHED BY MANUFACTURER



LONGITUDINAL SECTION



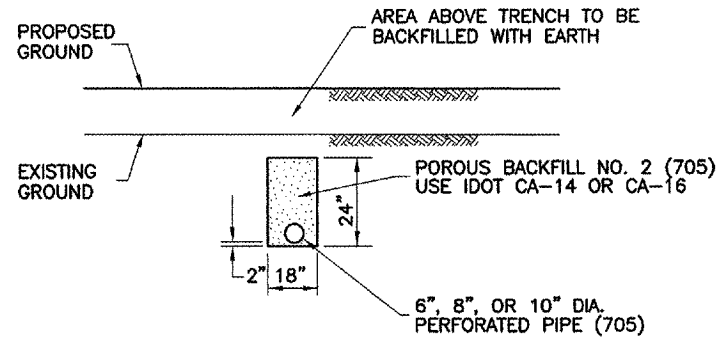
END SECTION

NOTES :

1. PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
2. PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
3. THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ART. 701-3.7 OF THE STANDARD SPECIFICATIONS, COST INCIDENTAL TO END SECTION.
4. BEDDING MATERIALS AND GEOTECH FABRIC PER 701 PIPE SPECIFICATION IN THE SPECIAL PROVISIONS AS SHOWN IN THIS DETAIL SHALL BE INCLUDED IN THE 752 PIPE END SECTION CONTRACT UNIT PRICE.

MODIFIED I.D.O.T.
STANDARD 542301

FOR EQUIV ELLIPTICAL PRECAST, SEE IDOT STANDARD 542306.
ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET
SHALL BE APPLIED TO IDOT STANDARD 542306.

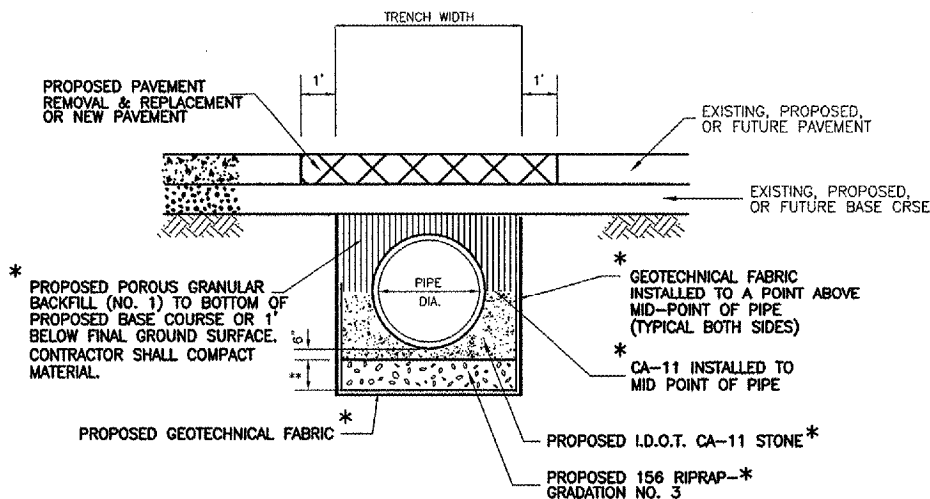


NOTES :

- 1) PIPE UNDERDRAIN MATERIAL SHALL MEET THE MATERIAL REQUIREMENTS OF ITEM 705 AND THE SPECIAL PROVISIONS.
- 2) COST OF POROUS BACKFILL NO. 2 (CA-14 OR CA-16), BENDS AND FITTINGS TO BE INCLUDED IN THE UNIT PRICE FOR UNDERDRAINS.
- 3) NO ADDITIONAL COMPENSATION SHALL BE MADE FOR CHANGES IN ELEVATIONS MADE BY THE RESIDENT ENGINEER.

PERFORATED PIPE UNDERDRAIN DETAIL

PROPOSED TYPICAL SECTION - PIPE TRENCH (ALL PROPOSED PIPES)

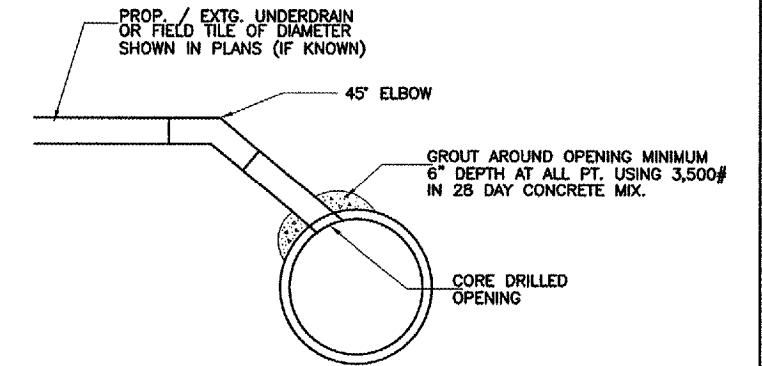


* = COST OF THESE ITEMS SHALL BE INCLUDED IN THE 701 PIPE CONTRACT UNIT PRICES

GENERAL PIPE NOTES:

1. GROUND WATER IS EXPECTED. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DEWATERING, TO THE SATISFACTION OF THE ENGINEER, TO INSURE PROPER INSTALLATION OF PIPES. NO ADDITIONAL COMPENSATION BEYOND THE CONTRACT UNIT PRICES WILL BE ALLOWED FOR DEWATERING COSTS.
2. COMPACTED BEDDING AND BACKFILL MATERIALS SHALL BE REQUIRED AS SHOWN IN DETAIL. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

PIPE DIA.	TRENCH WIDTH	PVMT. RMVL. WIDTH	FOUNDATION DEPTH
8"	3.75'	6'	10"
12"	4.17'	6.5'	10"
15"	4.46'	6.75'	10"
18"	4.75'	7'	10"
ELLIPTICAL 18" EQUIV	5.21'	7.5'	10"
24"	5.33'	7.5'	18"
ELLIPTICAL 24" EQUIV	5.87'	8'	18"
30"	5.92'	8'	18"
ELLIPTICAL 30" EQUIV	6.63'	9'	18"
42"	7.08'	9.5'	18"

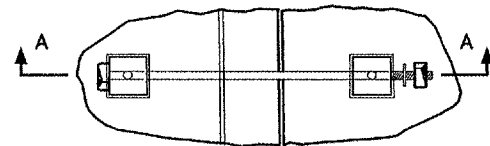


DETAIL OF PIPE UNDERDRAIN / FIELD TILE OUTLET INTO STORM SEWER OR PIPE CULVERT

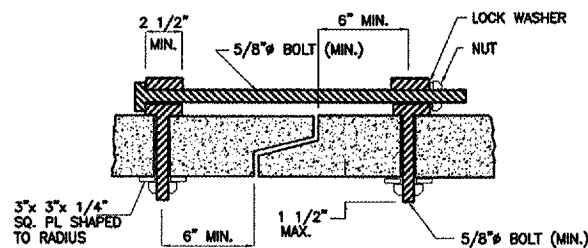
NOTES :

COST OF NEW PIPE, ELBOW AND GROUTING TO BE INCLUDED IN CONTRACT UNIT PRICES. ANY DAMAGE TO EXISTING STORM SEWER, TILE, OR PIPE CULVERT SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT ENGINEER AT THE CONTRACTOR'S EXPENSE.

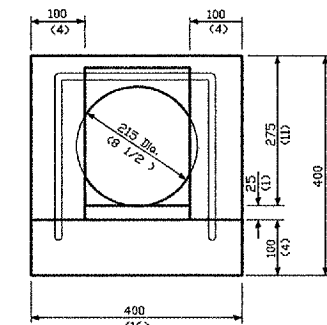
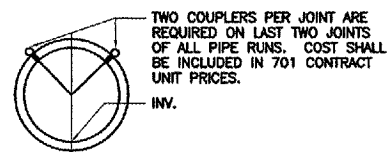
PROPOSED PIPE COUPLERS



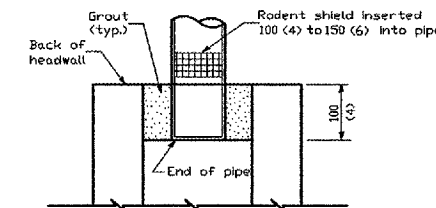
PLAN OF ONE COUPLER



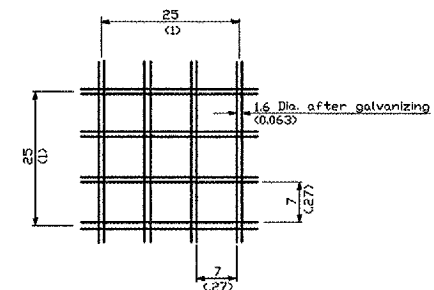
SECTION A-A



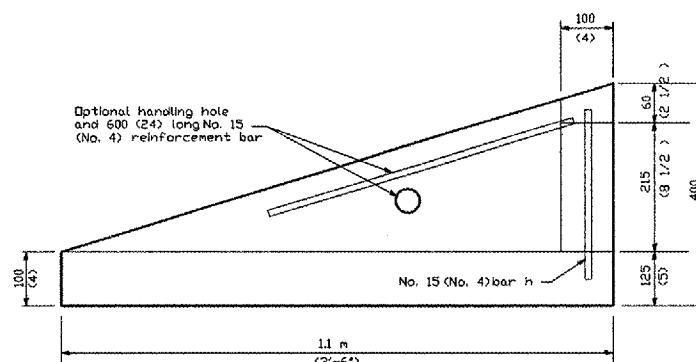
FRONT VIEW



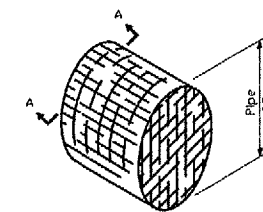
RODENT SHIELD PLACEMENT



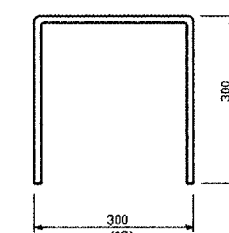
SECTION A-A



SIDE VIEW



DETAIL OF RODENT SHIELD



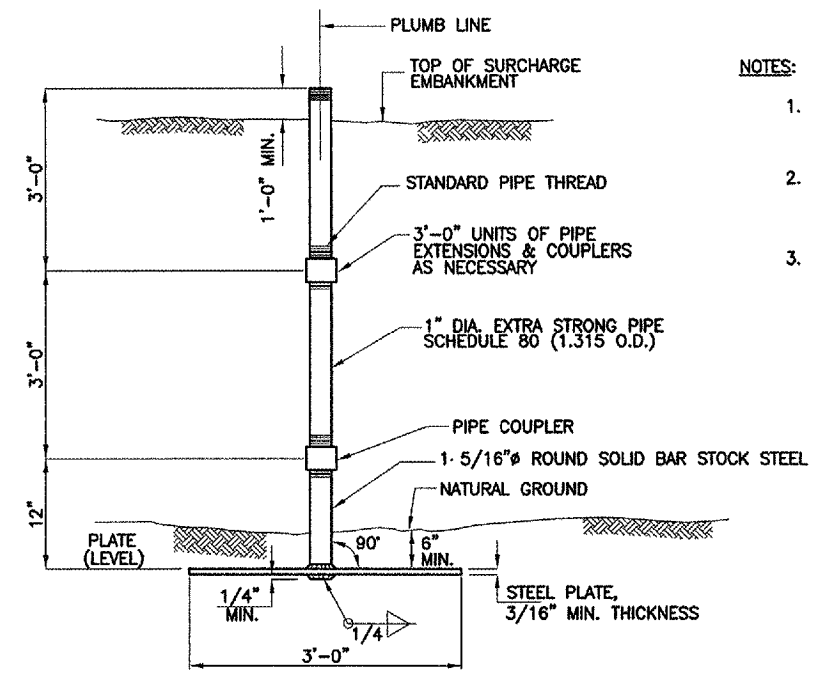
BAR h

CONCRETE HEADWALL FOR PIPE DRAIN

MODIFIED I.D.O.T. STANDARD 601101

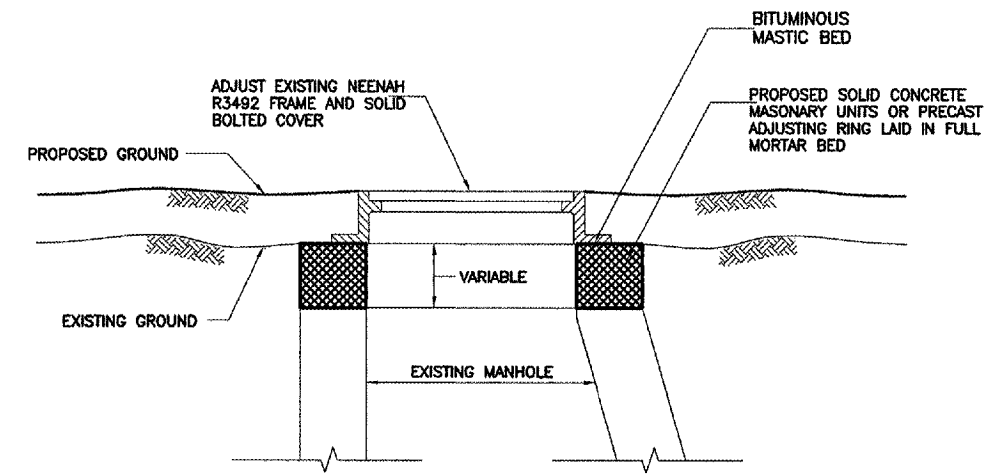
GENERAL NOTES

An alternate paved invert meeting the approval of the Engineer may be substituted for that shown in side view.

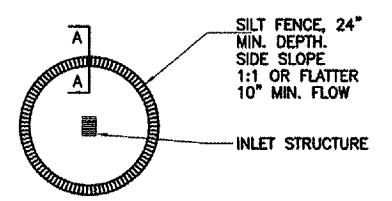


- NOTES:**
1. INSTALL PROPOSED SETTLEMENT PLATES PRIOR TO INSTALLING THE EMBANKMENT SURCHARGE SOILS.
 2. SEE SPECIFICATIONS FOR ADDITIONAL INSTALLATION DETAILS. PLATES TO BE INSTALLED IN THE LOCATIONS SELECTED BY THE ENGINEER.
 3. INSTALL ADDITIONAL 3' LONG PIPE SECTIONS AS REQUIRED.

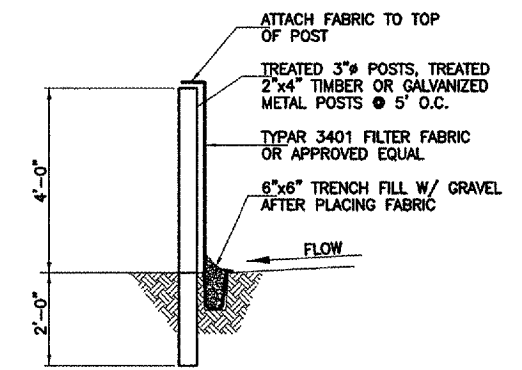
PROPOSED SETTLEMENT PLATES
(10 EACH REQ'D)



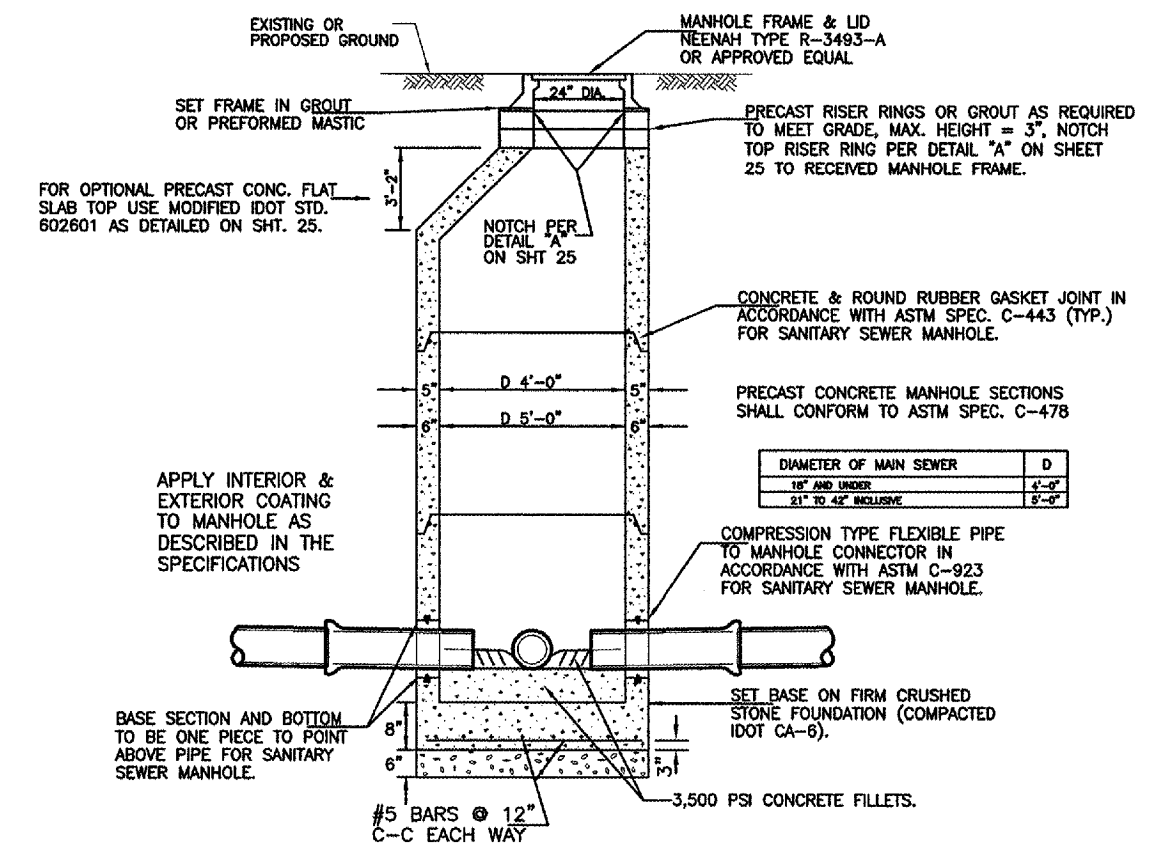
TYPICAL DETAIL - MANHOLE
ADJUST - IN TURF



TEMPORARY SEDIMENT TRAP
AT ALL INLET STRUCTURES

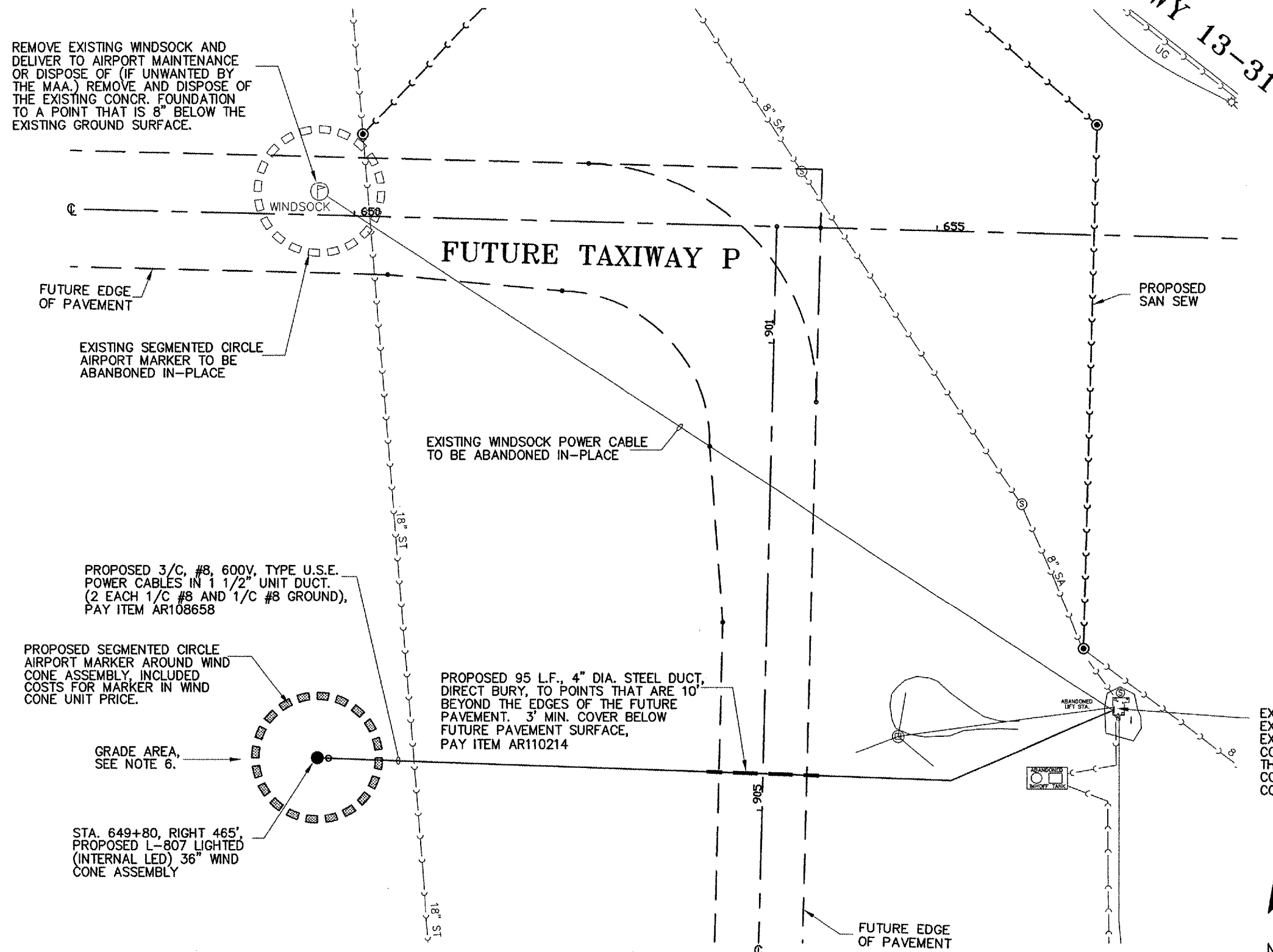


SECTION A-A
SILT FENCE DETAIL
NOT TO SCALE



ITEM AR770704 SANITARY MANHOLE 4'
(MANHOLES FOR SANITARY SEWERS)
NO SCALE

FURNISHING AND INSTALLING CRHD STONE FOUNDATION, FRAME, AND LID TO BE INCLUDED IN THE CONTRACT UNIT PRICE.

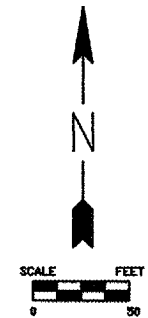


GENERAL NOTES:

1. THE CONTRACTOR SHALL SALVAGE THE EXISTING WINDSOCK SYSTEM EQUIPMENT AS DETAILED IN THE CONSTRUCTION PLANS AND SPECIAL PROVISIONS. SALVAGED EQUIPMENT SHALL BE CLEANED AND DELIVERED TO THE MAA. IF UNWANTED BY THE MAA, THE CONTRACTOR SHALL DISPOSE OF ITEMS.
2. THE REMOVAL OF THE EXISTING WIND SOCK SYSTEM AND SUPPLYING / INSTALLATION OF THE PROPOSED WIND CONE SYSTEM (INCLUDING, BUT NOT LIMITED TO, WIRING, CABLES, GROUNDING, CABLE TRENCHING, ELECTRICAL EQUIPMENT, CONDUIT, DUCT, FITTINGS, REBAR, CONCRETE, CRUSHED AGGREGATE ROCK, TESTING, REMOVAL AND DISPOSAL OF EXISTING FOUNDATIONS, NEW SEGMENTED CIRCLE AIRPORT MARKERS AND ALL OTHER ASSOCIATED WORK) SHALL BE INCLUDED AND PAID FOR UNDER CONTRACT ITEM AR107920, "REPLACE WIND CONE" UNLESS NOTED OTHERWISE.
3. CABLE MARKERS AS DETAILED ON SHEET 30 SHALL BE INSTALLED OVER THE WIND CONE CABLES. THE LEGEND ON THE MARKERS SHALL BE AS DIRECTED BY THE MAA AND/OR THE RESIDENT ENGINEER. CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUNS. REMOVE & DISPOSE OF THE EXISTING OLD WINDSOCK CABLE MARKERS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAID FOR.
5. EXISTING WIND CONE TO BE OPERATIONAL UNTIL NEW WIND CONE IS IN PLACE AND OPERATIONAL.
6. GRADE AREA PRIOR TO INSTALLING WIND CONE AND SEGMENTED CIRCLE MARKERS PER SECTION CC ON SHEET 31.
7. SEE PLAN & PROFILE SHEETS FOR LOCATION OF EXISTING UNDERGROUND ELECTRICAL, UTILITIES, AND FIELD TILES.
8. SEE SHEETS 30 AND 31 FOR ELECTRICAL NOTES AND DETAILS.

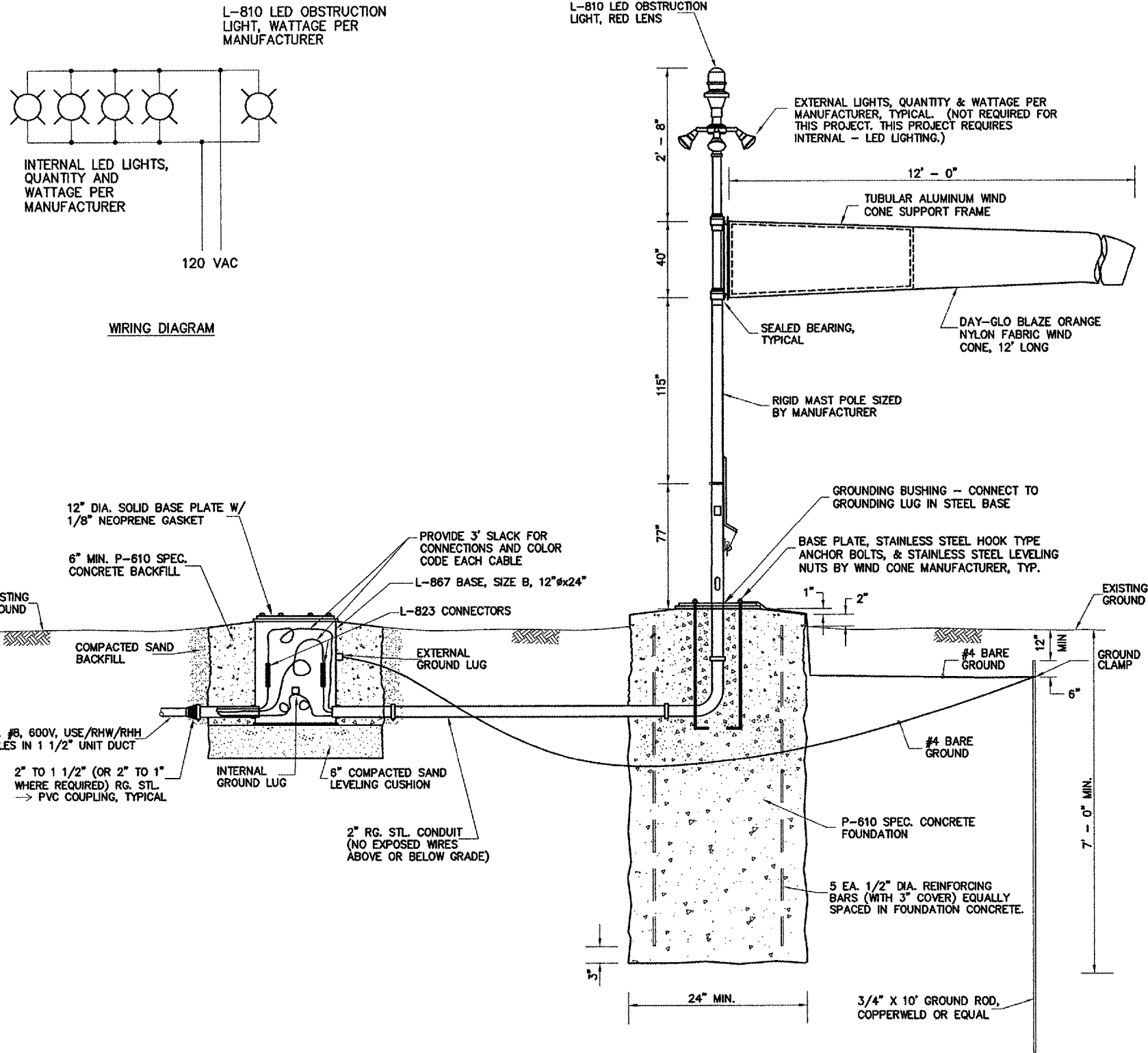
SYMBOL LEGEND

- EXISTING RWY / TAXIWAY EDGE LIGHT
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED 3/C, #8, 600V, TYPE U.S.E. POWER CABLES IN 1 1/2" UNIT DUCT. (2 EACH 1/C #8 AND 1/C #8 GROUND)
- PROPOSED L-807 LIGHTED (INTERNAL - LED) 36" DIA. WIND CONE ASSEMBLY



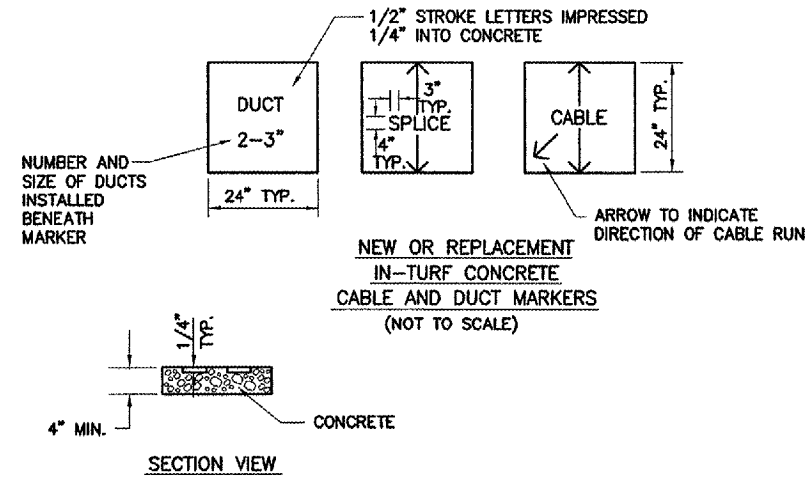
AIRFIELD L-807 WIND CONE LOCATION PLAN

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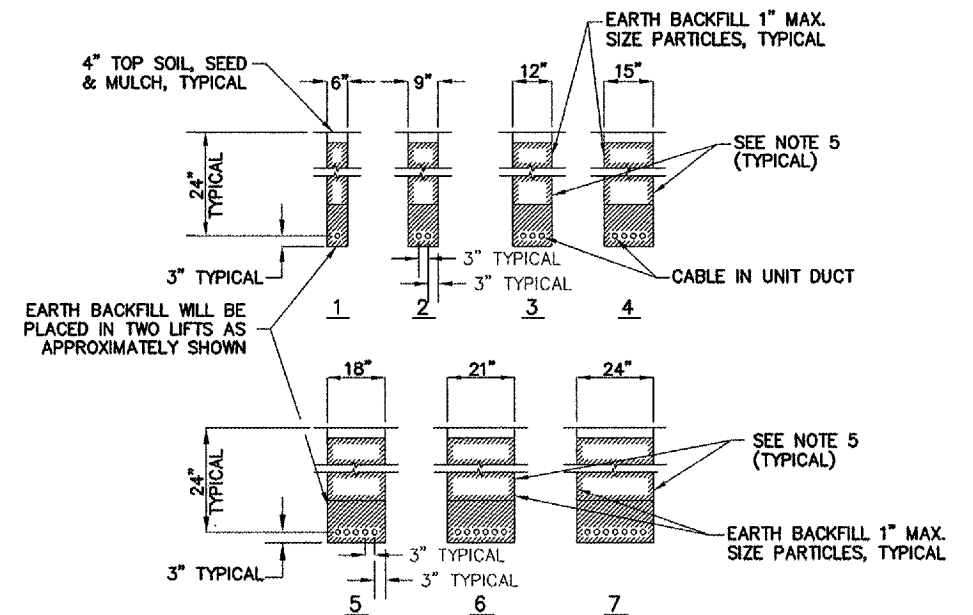
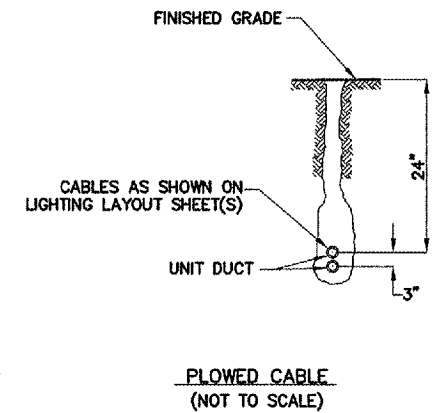
PROFILE VIEW

L-807 LIGHTED (INTERNAL LED) 36" WIND CONE ASSEMBLY DETAILS
NON-FRANGIBLE - BASE MOUNTED
120 VAC POWER SOURCE
NOT TO SCALE



NOTES:

1. NEW MARKERS ARE REQUIRED ON ALL FAA CABLES. CABLE MARKERS SHALL BE INSTALLED BY THE CONTRACTOR AT ALL LOCATIONS SELECTED BY THE RESIDENT ENGINEER AND / OR THE FAA. THE CONTRACTOR SHALL BE REQUIRED TO ADJUST THE ELEVATION OF EXISTING MARKERS AND / OR REPLACE EXISTING MARKERS DAMAGED DURING CONSTRUCTION.
2. COST OF CONCRETE MARKERS IS INCIDENTAL TO THE ASSOCIATED ITEMS OF DUCT OR CABLE.
3. EDGE EXPOSED CONCRETE WITH A 1/4" RADIUS TOOL.
4. WHERE ADDITIONAL SPACE TO FIT THE LEGEND IS REQUIRED, SOME OF THE FOLLOWING METHODS SHALL BE EMPLOYED.
 - A. REDUCE LETTER SIZE TO 3" HIGH, 2" WIDE.
 - B. INCREASE THE MARKER SIZE TO 30" X 30" MAX.
 - C. PROVIDE ADDITIONAL MARKERS PLACED SIDE BY SIDE.



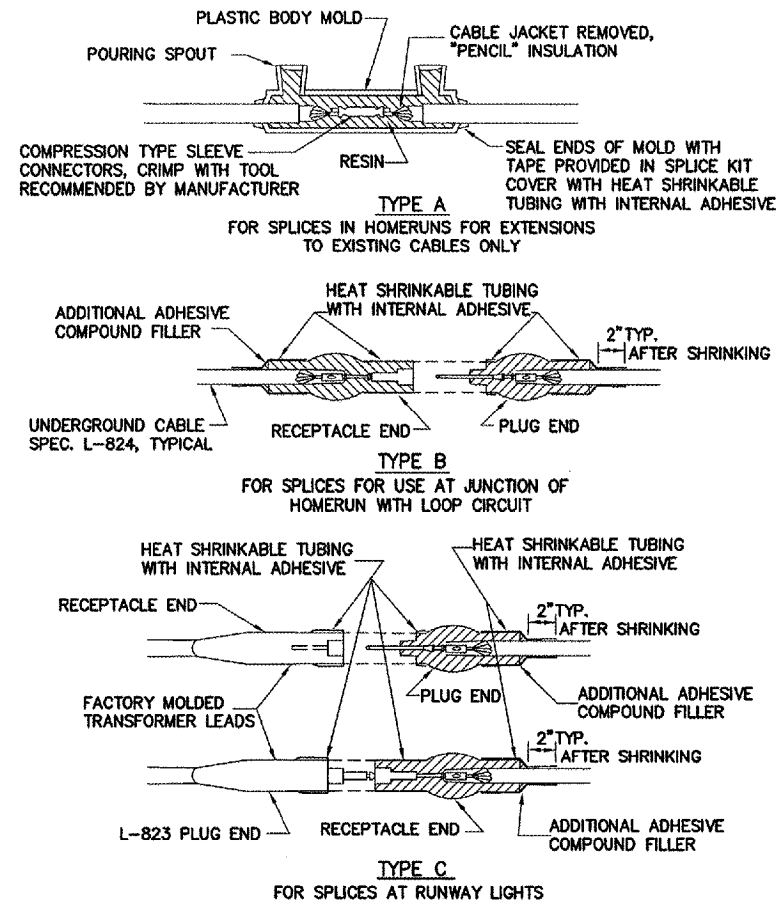
CABLE TRENCH NOTES :

1. DETAIL NUMBERS INDICATE NO. OF CABLES.
2. TRENCHES WITH MORE THAN 7 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE; IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
3. DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH. RETURNING MATERIALS AND RATES MAY BE SHOWN ON THE PLANS.
5. INSTALL YELLOW PLASTIC WARNING RIBBON IN TRENCH 9" ABOVE CABLES (TYPICAL ALL TRENCHES).

CABLE TRENCHES
(NOT TO SCALE)

GENERAL ELECTRICAL NOTES:

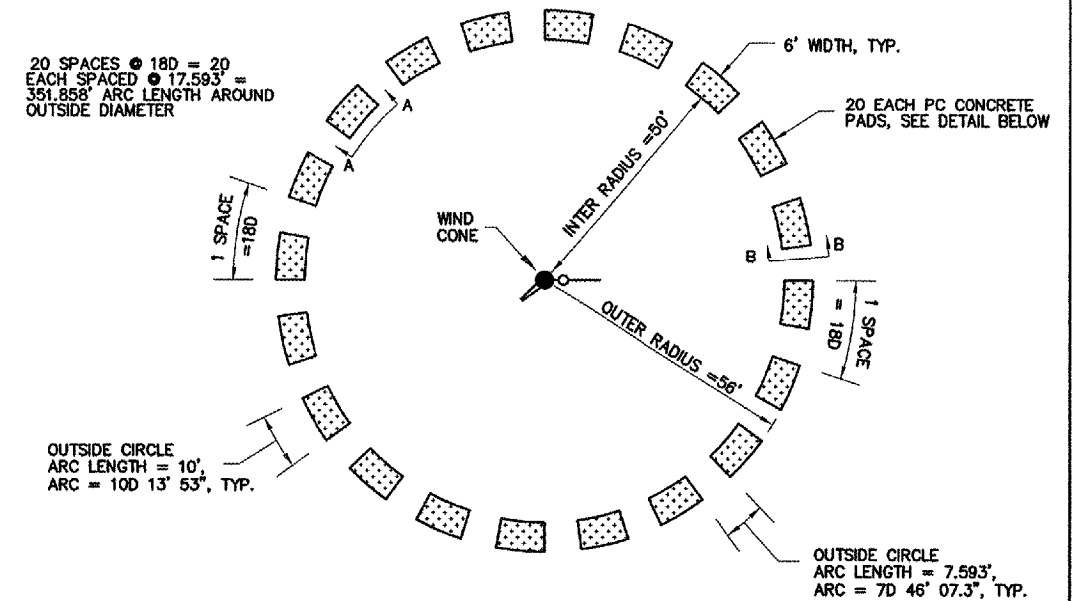
1. ALL EXCAVATION SHALL BE DONE VERY CAREFULLY. EXCAVATION BY HAND DIGGING SHALL BE REQUIRED AROUND ALL EXISTING DUCT BANKS, SPLICE CANS, MANHOLES, AND EXISTING CABLES. MANY EXISTING ACTIVE UNDERGROUND CABLES, WHOSE EXACT LOCATIONS CANNOT BE DETERMINED, ARE FOUND IN THE PROJECT AREA. IN ORDER TO AVOID EXISTING UNDERGROUND CABLES, THE CONTRACTOR SHALL CONNECT A THUMPER TO ALL EXISTING CIRCUITS AFTER WHICH THEY SHALL BE STAKED IN ALL AREAS REQUIRING TRENCHING OR EXCAVATION. CONTRACTOR SHALL ALSO NOTE THAT LOW VOLTAGE, FAA CABLES ALSO RUN UNDERGROUND THROUGHOUT THESE AREAS. ANY CABLE DAMAGED SHALL BE REPAIRED OR REPLACED TO ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
2. A MINIMUM OF 3 FEET OF SLACK SHALL BE PROVIDED IN THE CABLES AT EACH TRANSFORMER, CONNECTOR, OR SPLICE POINT. ALL CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS OR SPLICE CANS, UNLESS NOTED OTHERWISE.
3. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE AND LOCAL REGULATIONS.
4. ALL MANUFACTURERS FOR SUPPLYING AIRPORT LIGHTING EQUIPMENT SHALL APPEAR ON THE CURRENT FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-53B. THE EQUIPMENT SHALL COMPLY WITH THE APPLICABLE CURRENT FAA ADVISORY CIRCULAR LISTED IN THE FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-2 (AIRPORTS ELECTRONIC BULLETIN BOARD NUMBER 14).
5. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM (INCLUDING FAA APPROVED EQUIPMENT) ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
6. IF THE CONTRACTOR SELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT WHICH REQUIRES ADDITIONAL WIRING, TRANSFORMERS, ADAPTERS, MOUNTINGS, ETC., BEYOND THAT SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATIONS, THEN THE COST FOR THE ADDITIONAL ITEMS SHALL BE INCLUDED TO THE CONTRACT UNIT PRICES.
7. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
8. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES, STYLES, CLASSES, ETC. MAY BE FAA APPROVED.
9. ALL CONCRETE FOR ELECTRICAL EQUIPMENT SHALL COMPLY WITH SPECIFICATION 610-STRUCTURAL PC CONCRETE 3500 PSI AT 28 DAYS, AIR ENTRAINED CONCRETE MIX SHALL BE USED.
10. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL HAVE A 1/4" DIAMETER OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE CONNECTOR INTO THE BASE.
11. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1 1/2" ABOVE THE EDGE OF THE COVER IN THE CASE OF A BASE MOUNTED COUPLING.
12. ALL PERMANENT CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS, OR SPLICE CANS, UNLESS NOTED OTHERWISE.
13. MIMIC PANEL COLORS : RUNWAY R9-27 CIRCUIT = WHITE, RUNWAY R13-31 CIRCUIT = WHITE, RUNWAY R5-23 CIRCUIT = WHITE AND TAXIWAY B CIRCUIT = PINK.



CABLE SPLICES
(NOT TO SCALE)

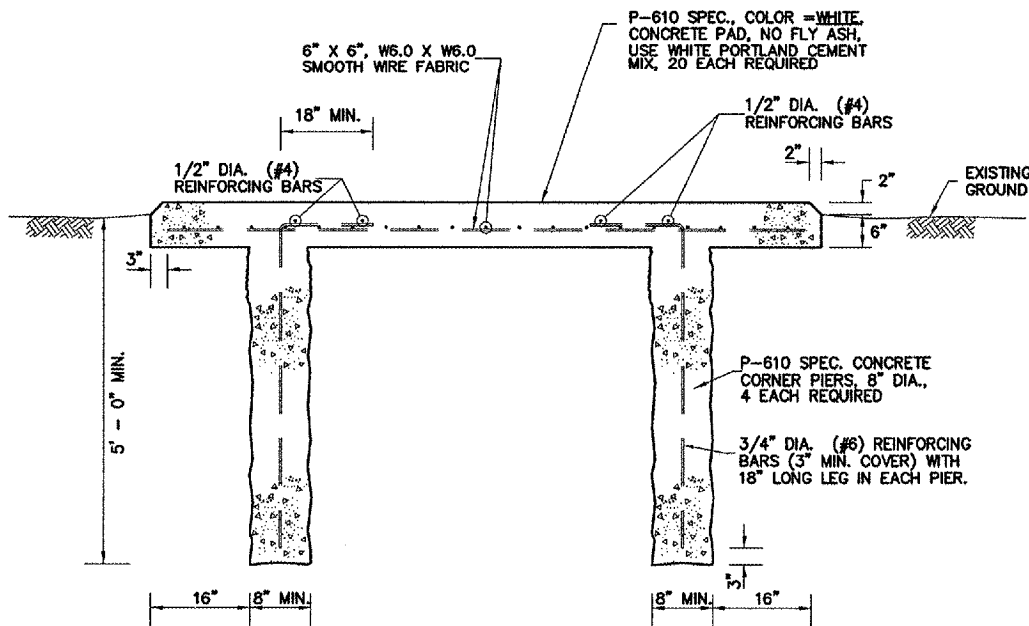
- NOTES :
1. SEE LIGHTING LAYOUT SHEET(S) FOR SPLICE TYPE
 2. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE I - EARTHWORK
ILL. MLI-3790, QU006
SHEET 31 OF 94

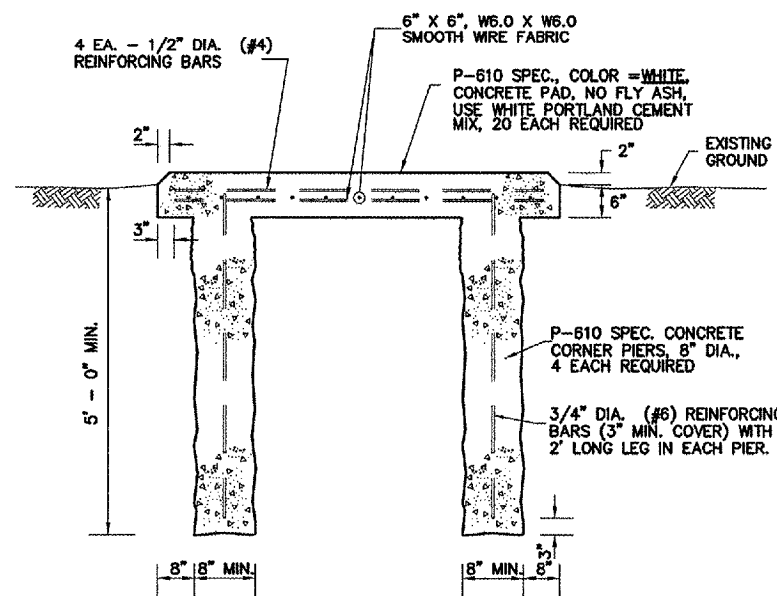


NOTE:
STA. 649+80, RIGHT 465', CENTER OF
SEGMENTED CIRCLE AIRPORT MARKER.

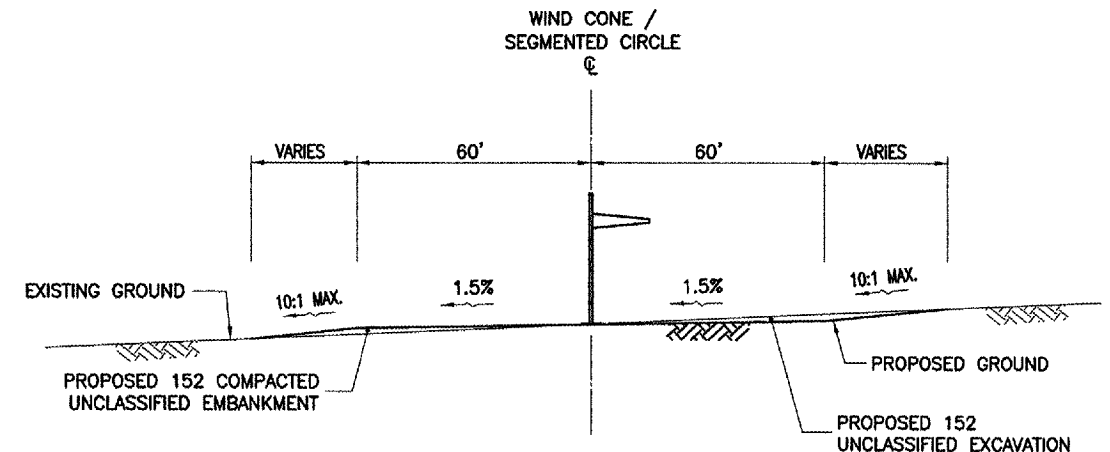
**PROPOSED SEGMENTED CIRCLE
AIRPORT MARKER DETAIL**



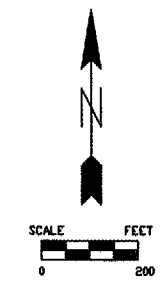
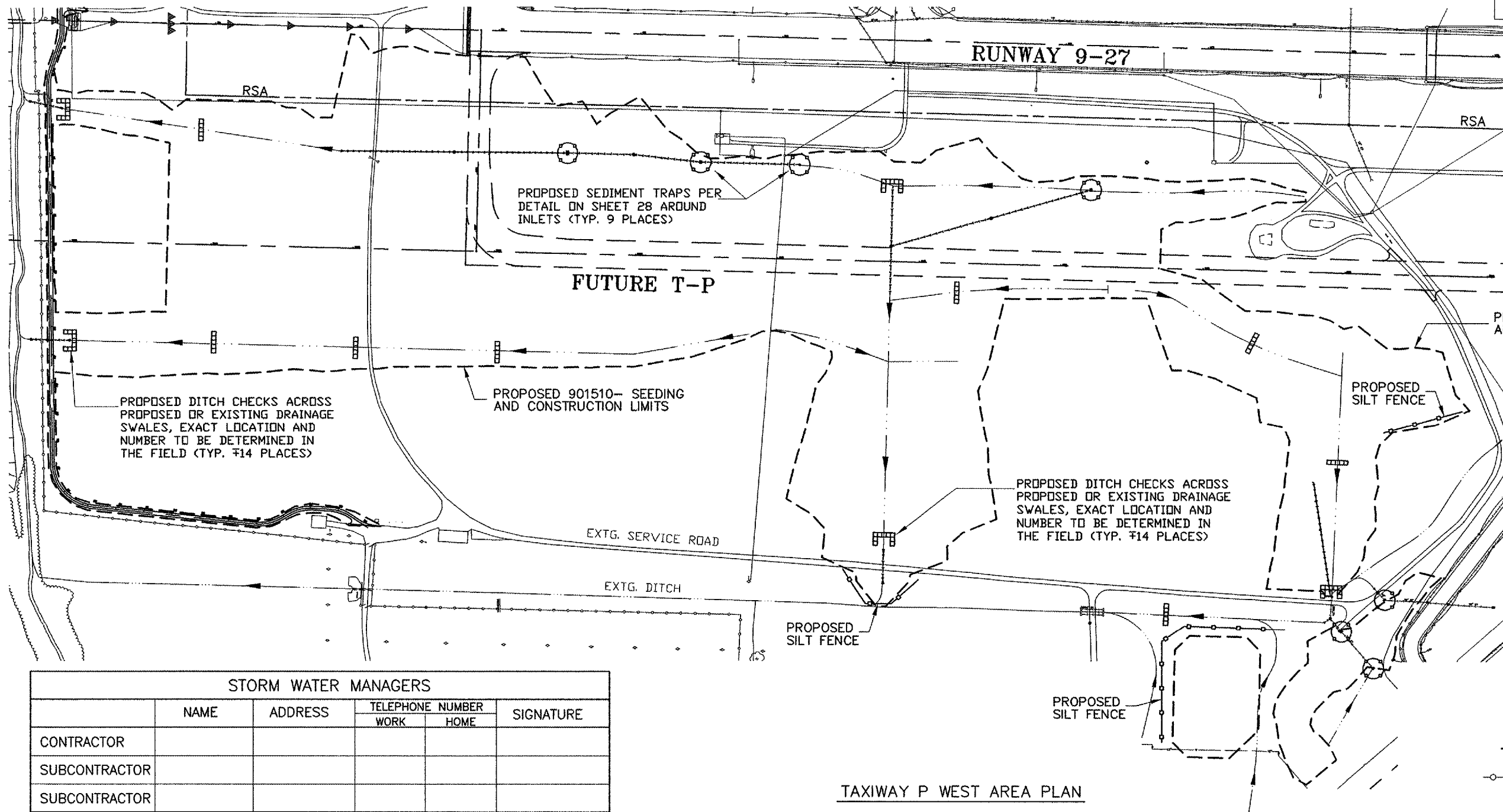
SECTION AA
ONE PC CONCRETE PAD
FOR SEGMENTED CIRCLE MARKER
NO SCALE



SECTION BB
ONE PC CONCRETE PAD
FOR SEGMENTED CIRCLE MARKER
NO SCALE



SECTION CC
AREA GRADING FOR
SEGMENTED CIRCLE MARKER
NO SCALE



STORM WATER MANAGERS					
	NAME	ADDRESS	TELEPHONE NUMBER		SIGNATURE
			WORK	HOME	
CONTRACTOR					
SUBCONTRACTOR					
SUBCONTRACTOR					

EROSION AND SEDIMENT CONTROL MEASURES		
ITEM	LOCATION	SPECIAL PROVISION/ PAY ITEM
TEMPORARY DITCH CHECKS	TAXIWAY P, STA. 574+65, LT. 465'; 575+10, RT. 350'; 580+00, LT. 580+00, RT. 585+00, RT. 590+00, RT. 604+00 LT. 270'; 604+00 RT. 1,000'; 606+50 RT. 616+80 RT. 620+00 RT. 670'; 620+00 RT. 1,130'; T-P2, STA. 907+40 LT. 580'; AND ADDITIVE ALT. 2, STA. 1209+00 LT. & RT.	156500
TEMPORARY SILT FENCE	AT ALL SEDIMENT TRAP & DITCH CHECK LOCATIONS AND T-P, STA. 602+50 RT. 1,090' TO STA. 605+25 RT. 1,120'; T-P, STA. 621+50 RT. TO STA. 624+25 RT.; T-P STA. 647+30 RT. TO STA. 652+90 RT.; T-P2 STA. 907+50 RT.; T-P2 STA. 907+50 LT. AND RNWY 31 / NORTH ON-SITE BORROW AREA STA. 206+40 RT. 780' TO STA. 210+50 RT. 265'; ALT. 2, STA. RT. 1209+00 TO 1212+50.	156500
TEMPORARY SEDIMENT TRAPS	TAXIWAY D, STA. 114+64, LT. & RT.; TXWY D STA. 116+50, LT.; TAXIWAY P, STA. 592+50, LT. 350'; 597+20, LT. 330'; 600+70, LT. 330'; 611+00, LT. 270'; 649+85, LT. 270'; AND 651+92, LT. 270'.	156500

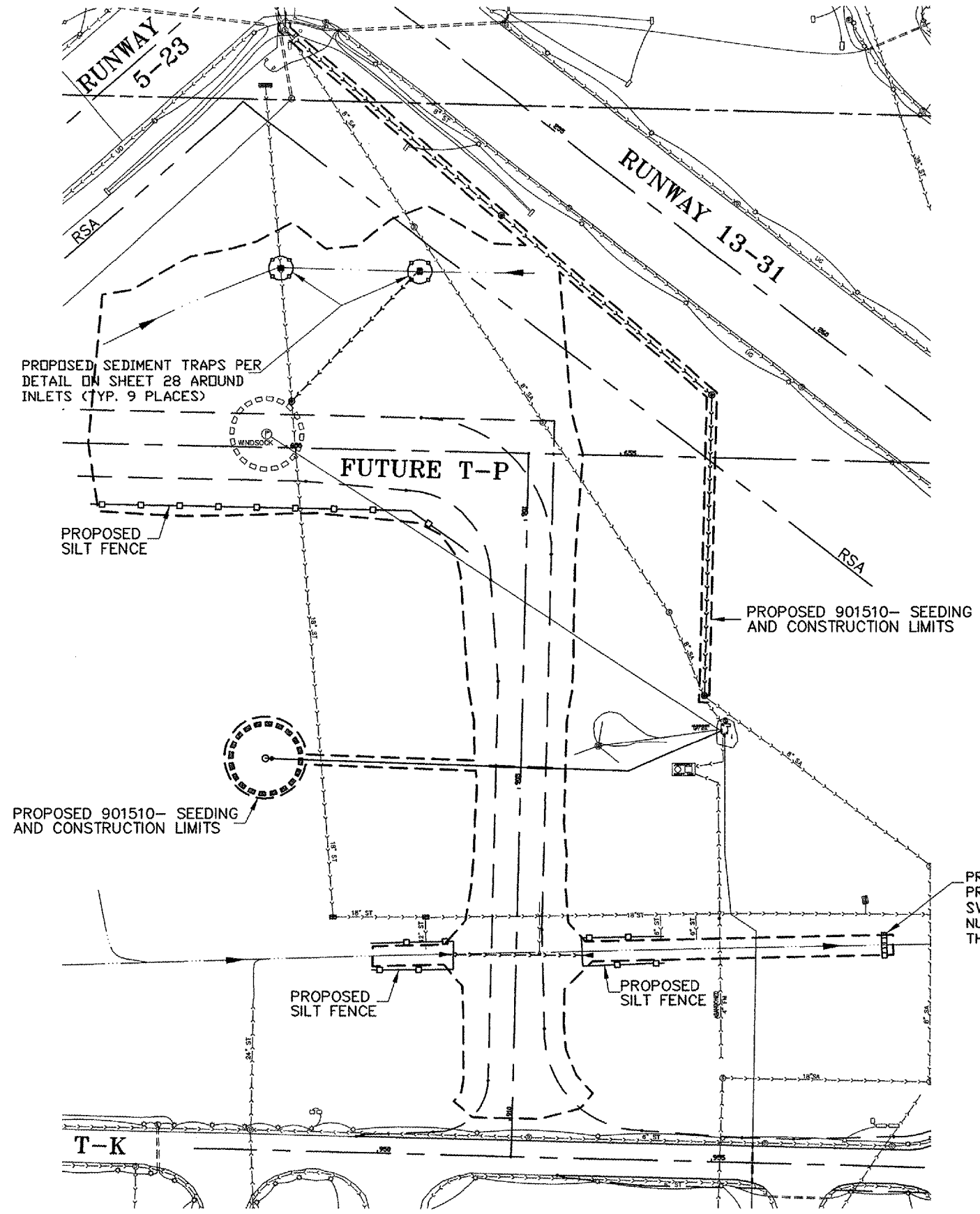
NOTE:
SEE SHEET 33 FOR STORM WATER POLLUTION PREVENTION GENERAL NOTES.

- LEGEND:**
- EXTG. EDGE LIGHT
 - EXTG. GUIDANCE SIGN
 - EXTG. ELECTRICAL CABLE
 - EXTG. STORM SEWER OR UNDERDRAIN
 - EXTG. INLET
 - EXTG. OUTLET
 - EXTG. MANHOLE
 - EXTG. DRAINAGE DITCH
 - PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
 - EXISTING FENCE
 - EXISTING OR PROPOSED DRAINAGE SWALE
 - PROPOSED SILT FENCE
 - PROPOSED TEMPORARY STRAW BALE DITCH CHECK
 - PROPOSED SEDIMENT TRAP

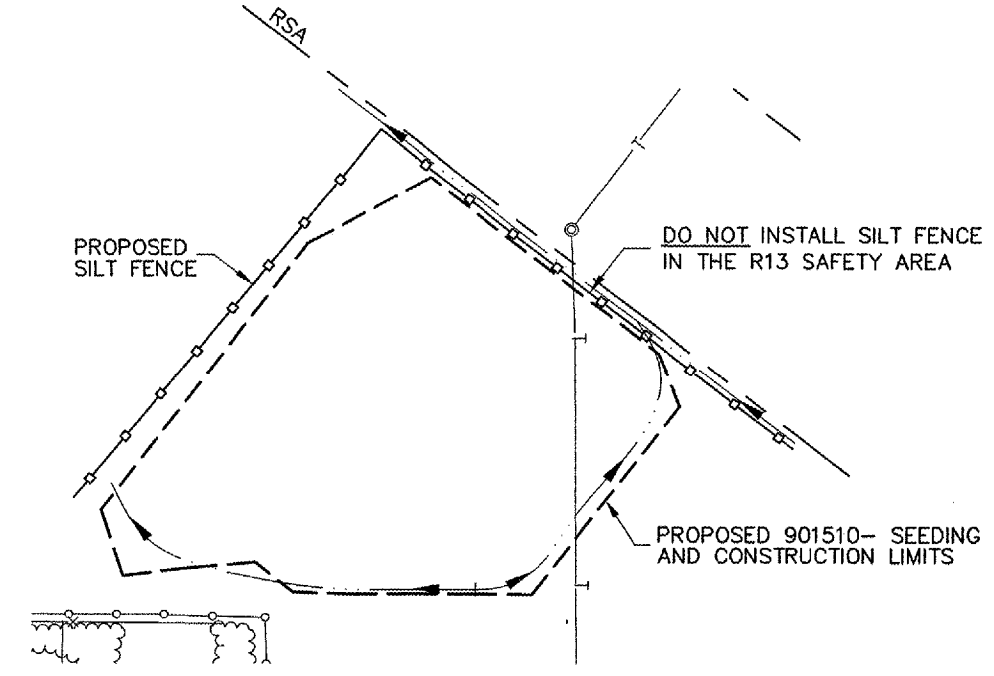
TAXIWAY P WEST AREA PLAN

ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE		WEEK														
NO.	ITEM DESCRIPTION	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
1	INSTALL SEDIMENT/EROSION CONTROLS	█														
2	CLEARING AND GRADING		█													
3	EXCAVATION AND EMBANKMENT			█												
4	TURFING				█											
5	MAINTAIN SEDIMENT/EROSION CONTROL	█	█	█	█	█	█	█	█	█	█	█	█	█	█	█
6	CLEAN-UP															

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TAXIWAY P EAST AREA PLAN



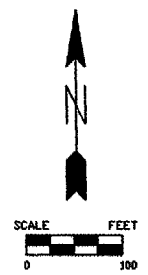
NORTH ON-SITE BORROW AREA PLAN

LEGEND:

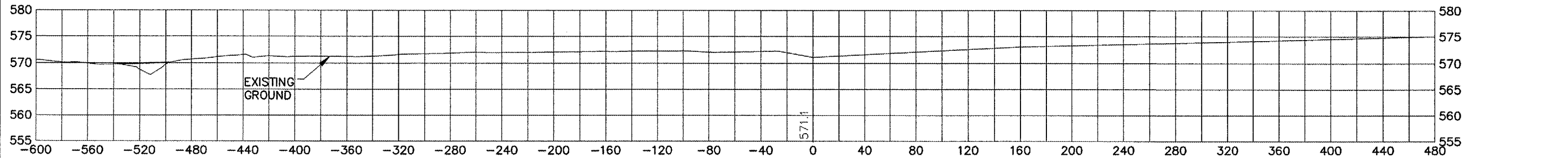
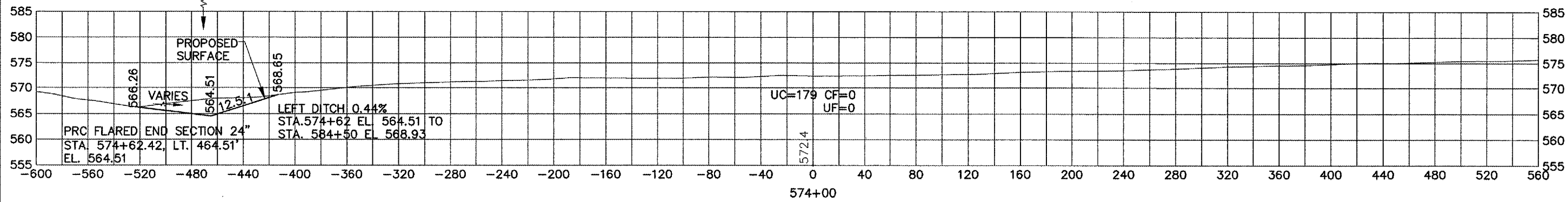
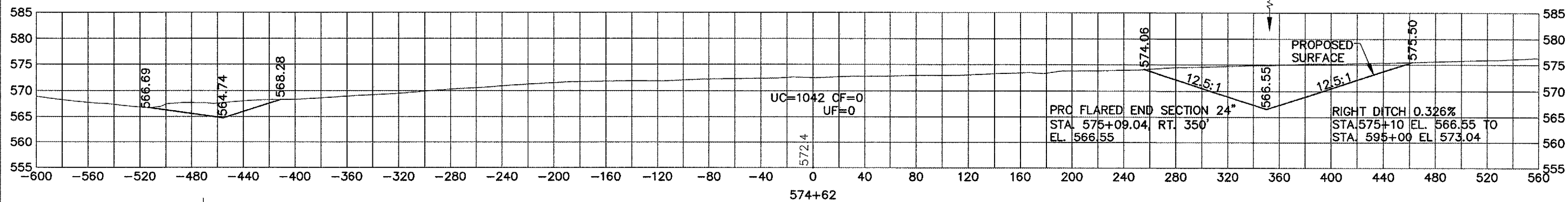
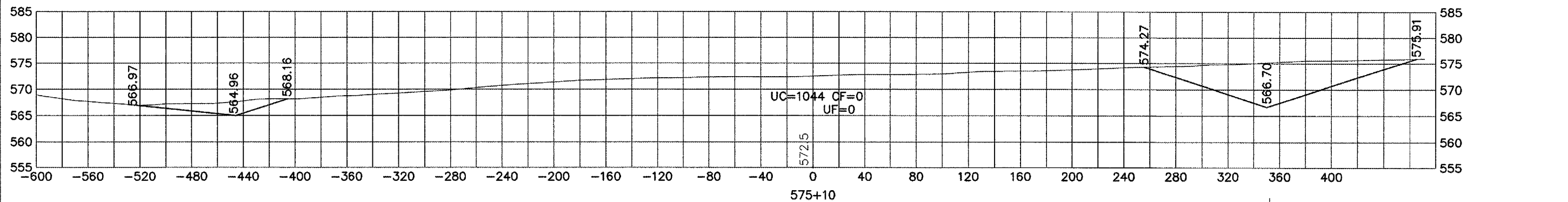
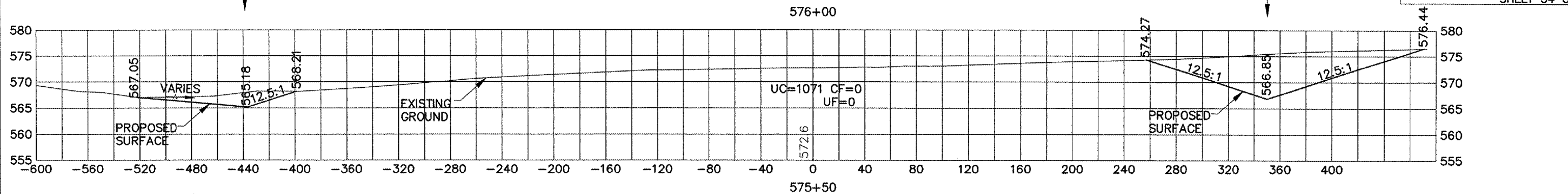
	EXTG. EDGE LIGHT
	EXTG. GUIDANCE SIGN
	EXTG. ELECTRICAL CABLE
	EXTG. STORM SEWER OR UNDERDRAIN
	EXTG. INLET
	EXTG. OUTLET
	EXTG. MANHOLE
	EXTG. DRAINAGE DITCH
	PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
	EXISTING FENCE
	EXISTING OR PROPOSED DRAINAGE SWALE
	PROPOSED SILT FENCE
	PROPOSED TEMPORARY STRAW BALE DITCH CHECK
	PROPOSED SEDIMENT TRAP

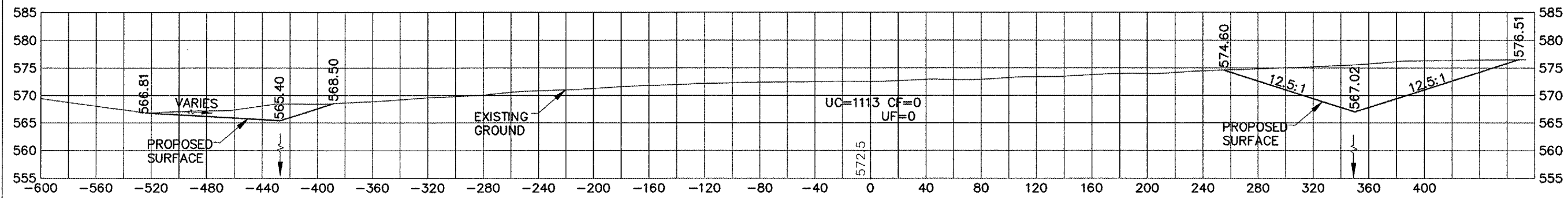
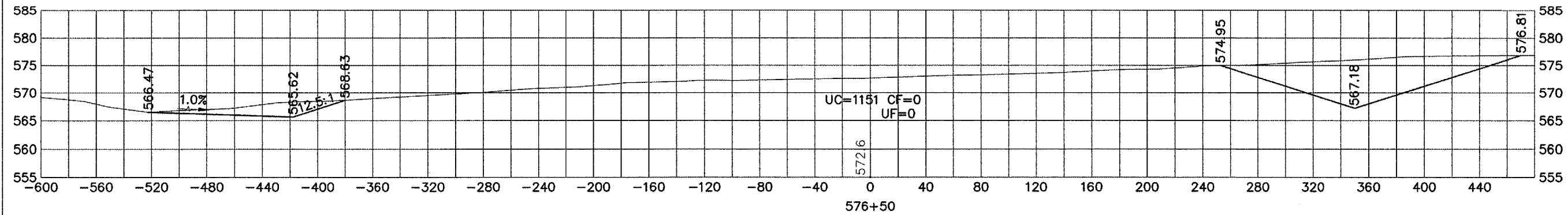
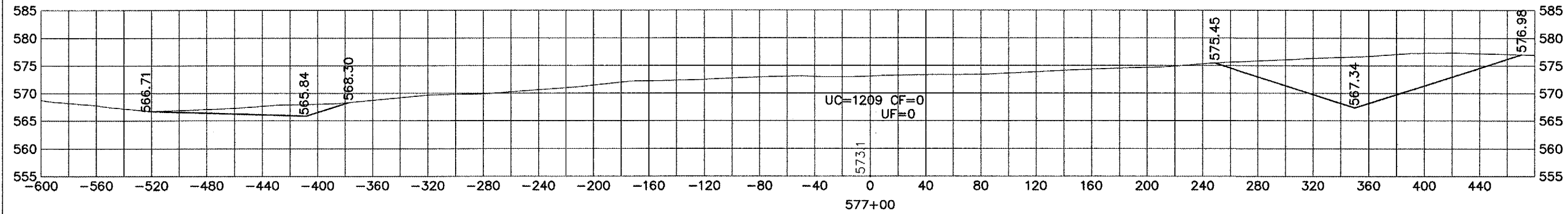
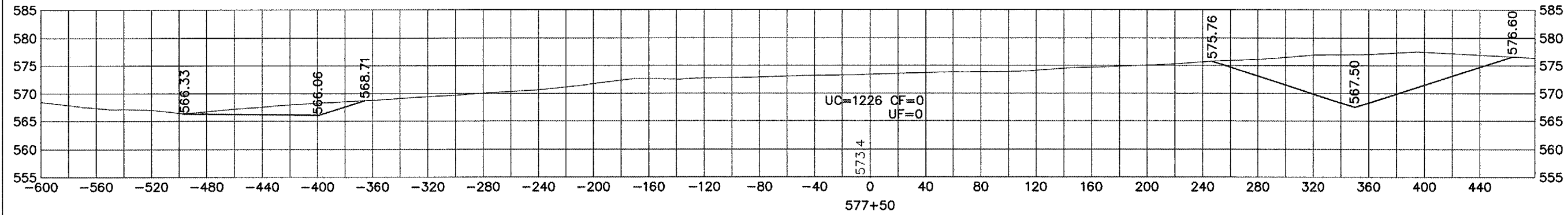
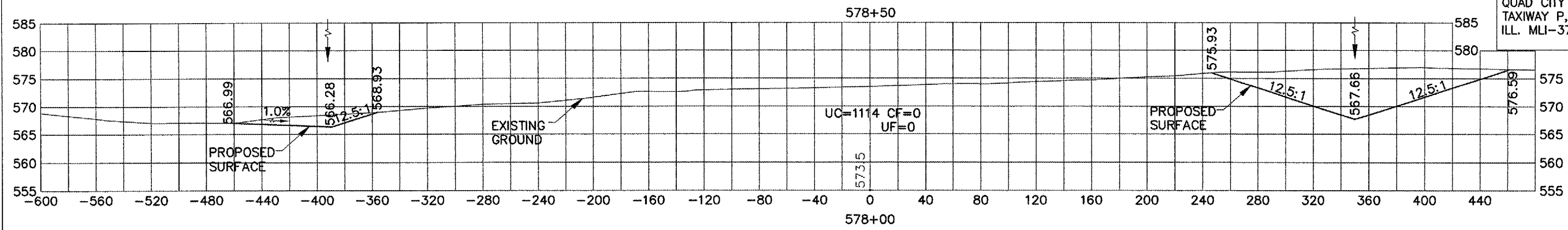
GENERAL NOTES:

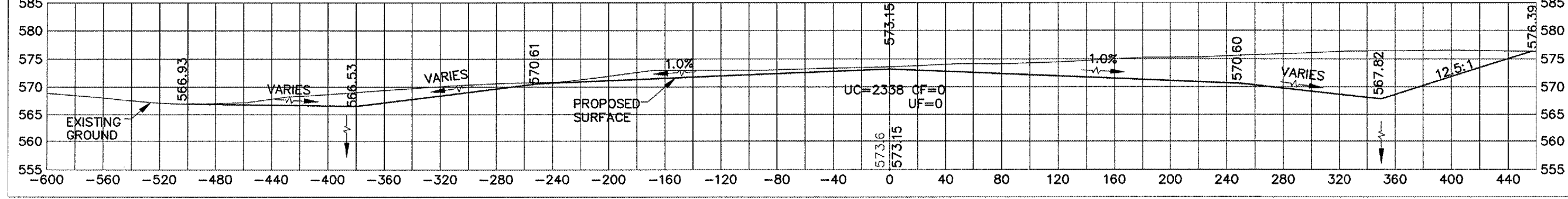
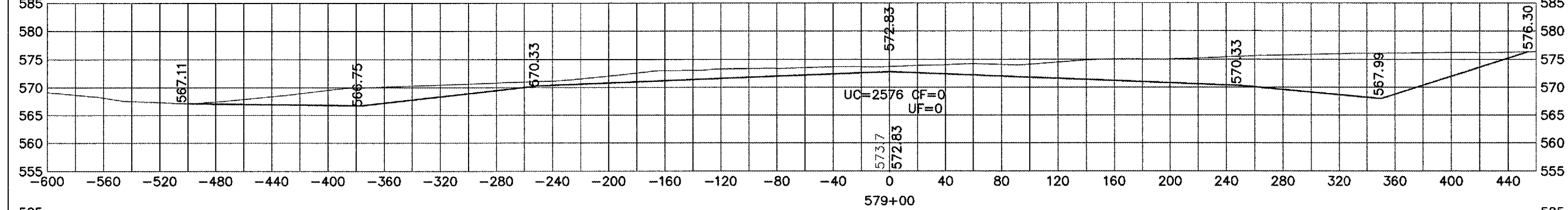
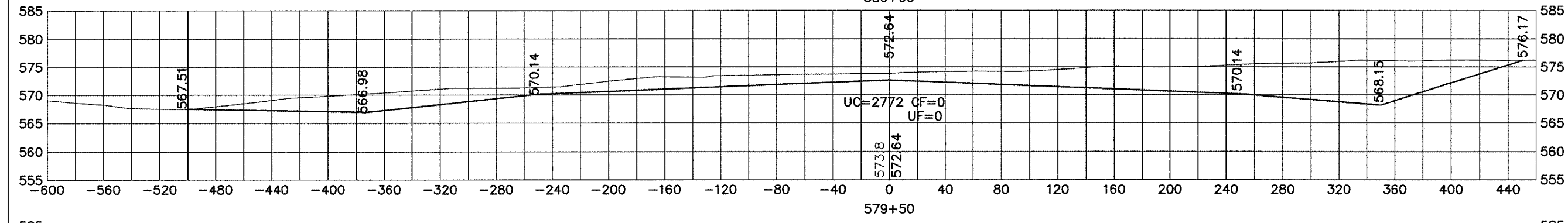
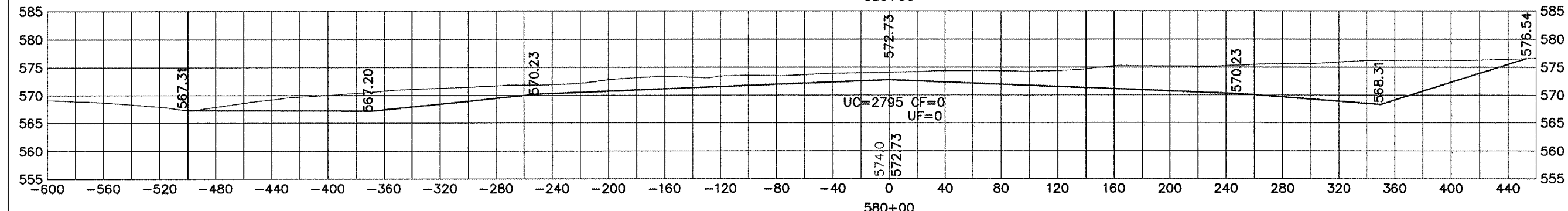
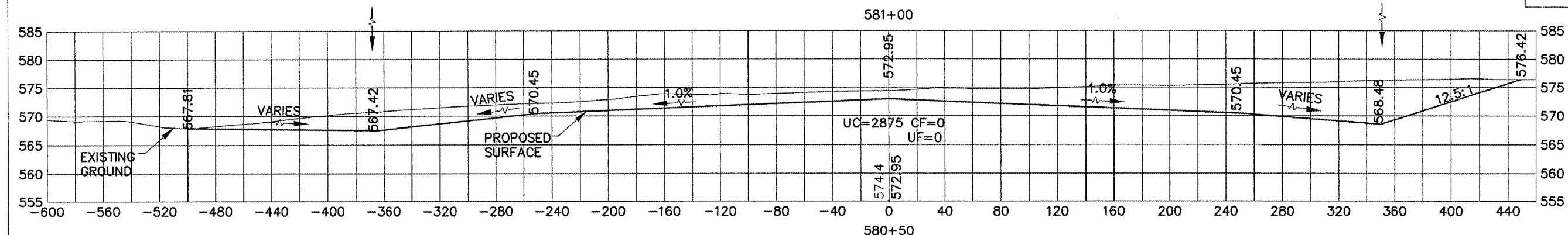
- TEMPORARY DITCH CHECKS - TWO BALES HIGH WITH SILT FENCING SHALL BE REQUIRED PER IDOT STANDARD 280001 - TEMPORARY EROSION SITE CONTROL SYSTEM DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING, MAINTAINING, AND REMOVING DITCH CHECKS, SILT FENCE, AND SEDIMENT TRAPS TO THE SATISFACTION OF THE RESIDENT ENGINEER. THIS INCLUDES, BUT IS NOT LIMITED TO, CLEANING EROSION SOILS AS REQUIRED.
- LOCATION OF THE DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHOWN ARE APPROXIMATE. ACTUAL LOCATIONS TO BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.
- ONCE CONSTRUCTION HAS BEEN COMPLETED, OR TEMPORARILY SUSPENDED FOR LONGER THAN 21 DAYS (SUCH AS A WINTER SHUTDOWN), THE CONTRACTOR SHALL SEED ALL AREAS DISTURBED IN ACCORDANCE WITH ITEM 901510 WITHIN 14 DAYS OF THE LAST DISTURBANCE. DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE CONTRACTOR ESTABLISHES A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR AND EACH SUBCONTRACTOR RESPONSIBLE FOR WATER POLLUTION CONTROL SHALL DESIGNATED, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WATER POLLUTION CONTROL ITEMS. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE TO REPAIR AND MAINTAIN WATER POLLUTION CONTROL DEVICES ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
- CONTRACTOR TO EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALE AS REQUIRED TO PREVENT RAIN WATER PONDING AND TO CONTROL STORM WATER RUN-OFF.
- CONTRACTOR SHALL ADHERE TO THE CITY OF MOLINE'S EROSION AND SEDIMENT CONTROL REGULATIONS AND THE ILLINOIS MANUAL ON EROSION AND SEDIMENT CONTROL.
- SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY CONSTRUCTION THAT DISTURBS EXISTING STORM WATER RUN-OFF CONDITIONS AND/OR GROUND VEGETATION.
- EROSION CONTROL MEASURES SHALL BE INSPECTED WEEKLY AND AFTER EACH PRECIPITATION EVENT AND REPAIRED OR REPLACED AS NECESSARY.
- RESIDENT ENGINEER SHALL CHECK THAT ALL FILL AREAS ARE TO A MINIMUM COMPACTION OF 95% OF THE MATERIALS STANDARD PROCTOR MAXIMUM DRY DENSITY.
- SILT FENCE, SEDIMENT TRAPS, AND HAY BALES SHALL BE CLEANED OR REPLACED WHEN SILT BUILDS UP TO WITHIN ONE FOOT OF THE TOP OF THE SILT FENCE OR HAY BALES.
- ADDITIONAL EROSION CONTROL DEVICES SHALL BE USED AS REQUIRED. THE COSTS OF ALL MEASURES NECESSARY TO COMPLY WITH THIS STORM WATER POLLUTION PREVENTION PLAN SHALL BE INCLUDED IN THE ITEM 156500 - TEMPORARY EROSION CONTROL LUMP SUM PRICES.
- SEE SHEET 32 FOR STORM WATER MANAGERS SIGNATURE CHART, ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE, AND EROSION / SEDIMENT CONTROL MEASURES LIST.



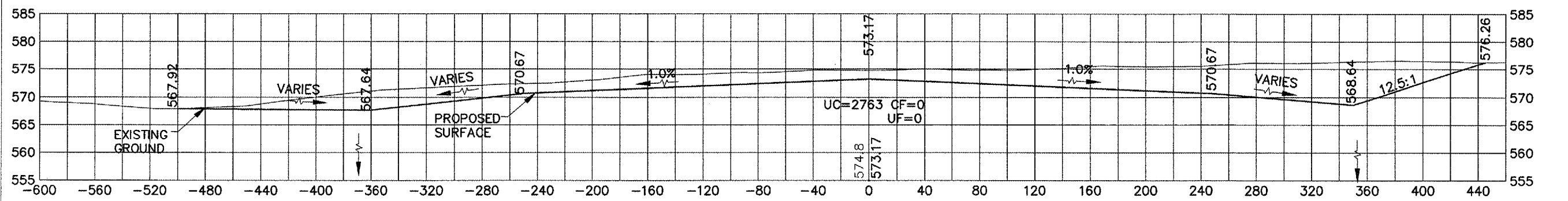
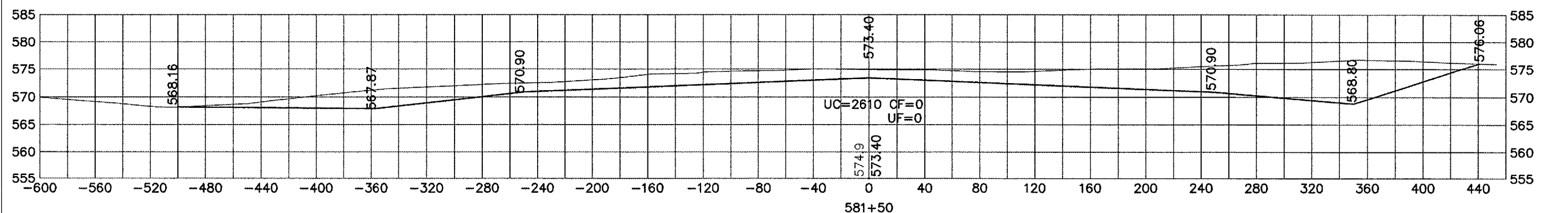
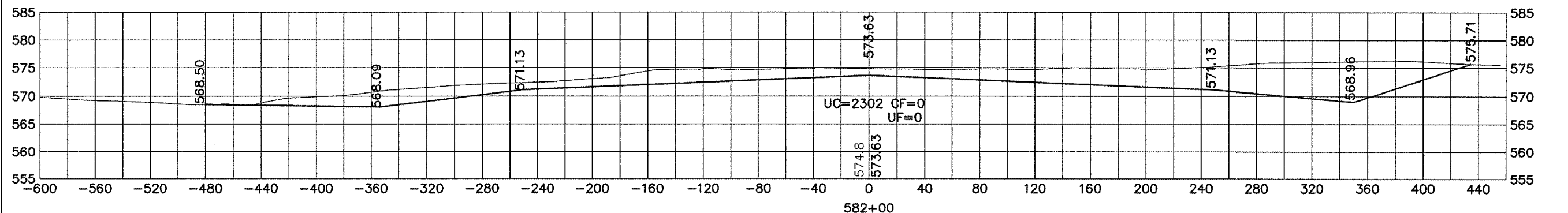
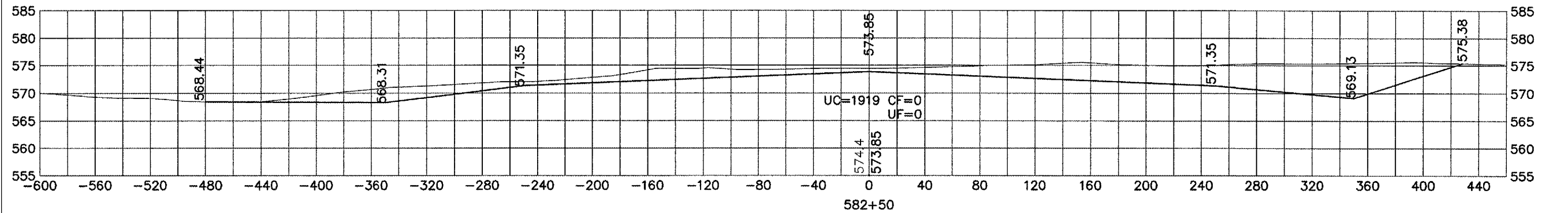
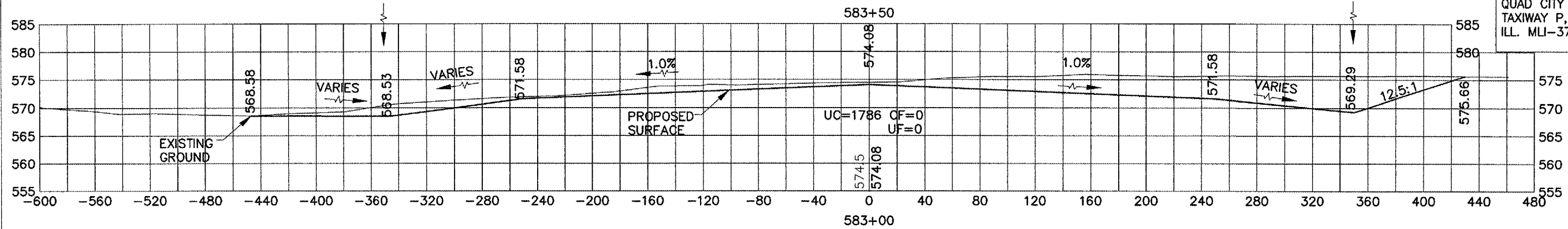
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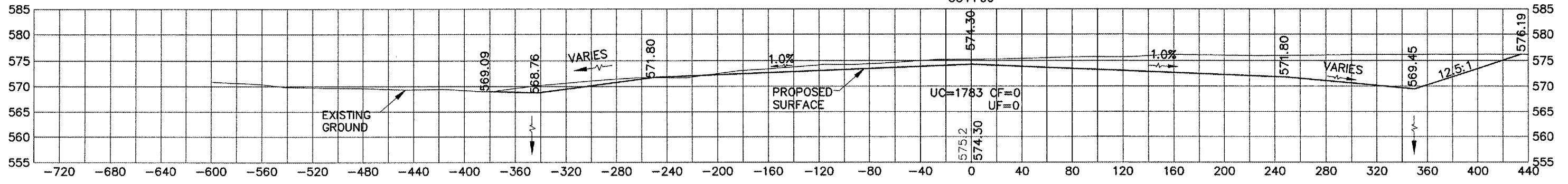
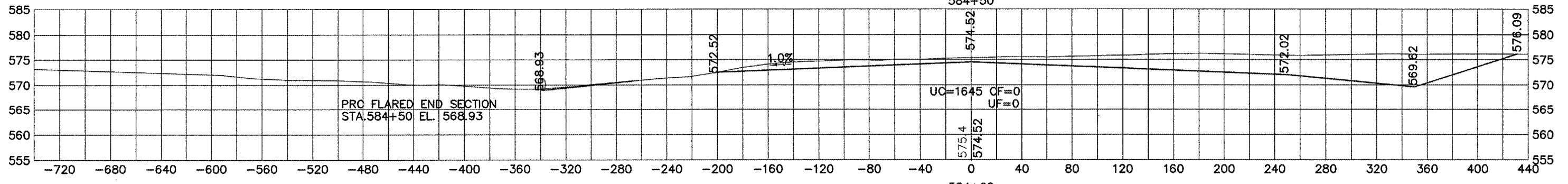
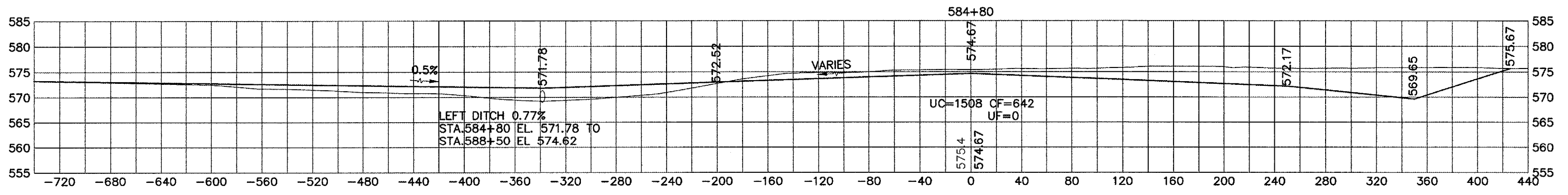
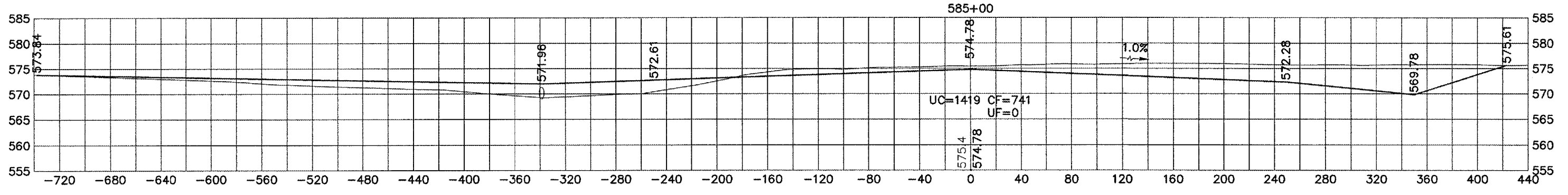
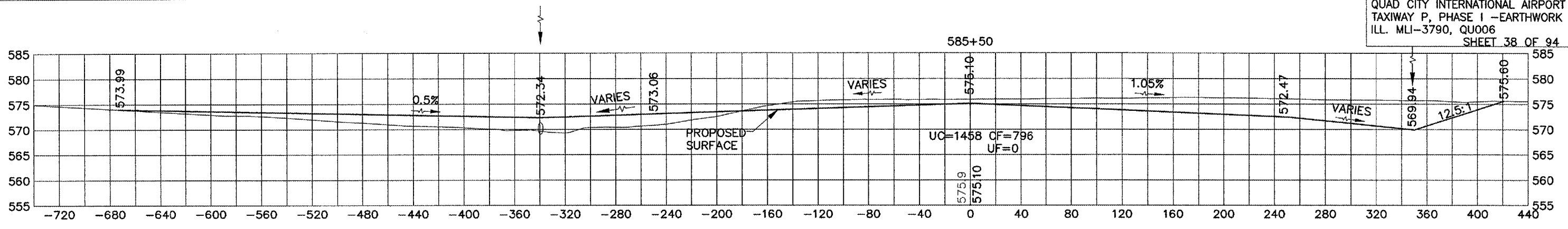




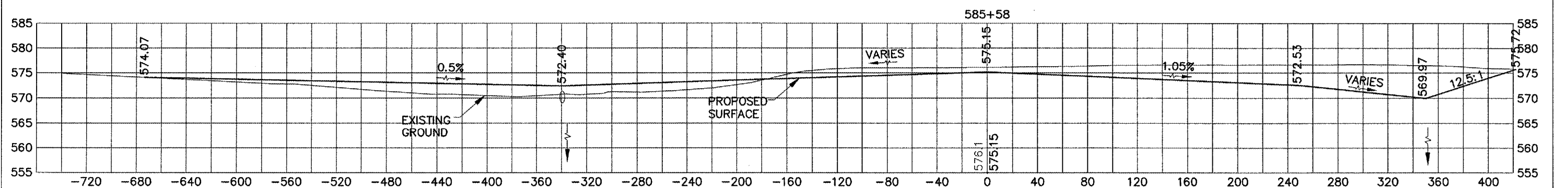
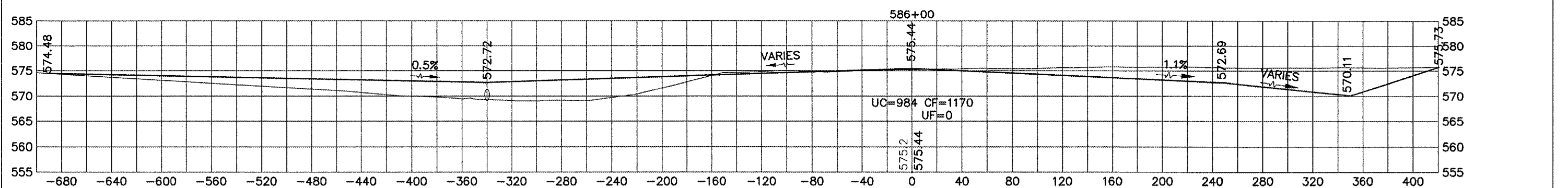
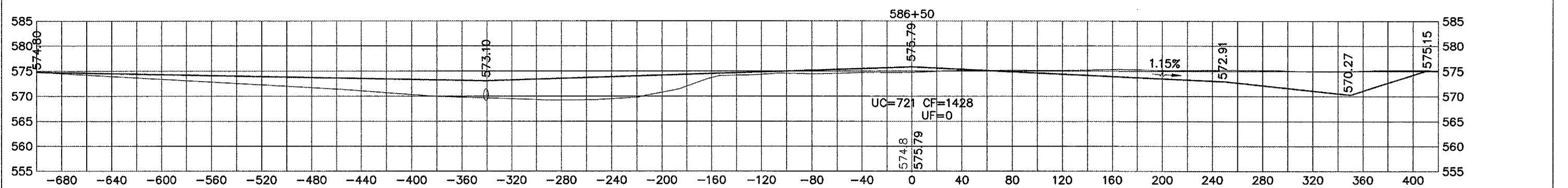
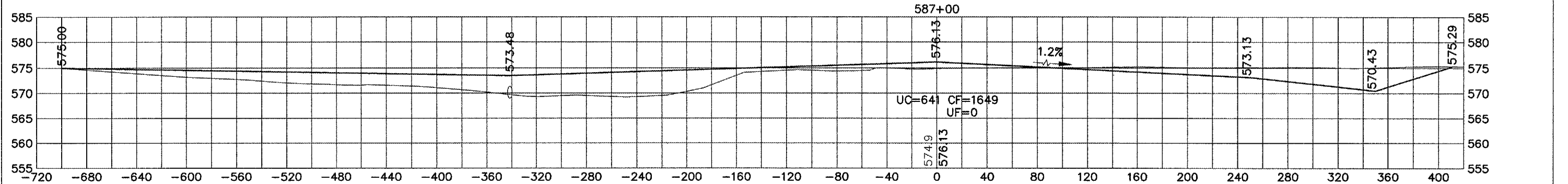
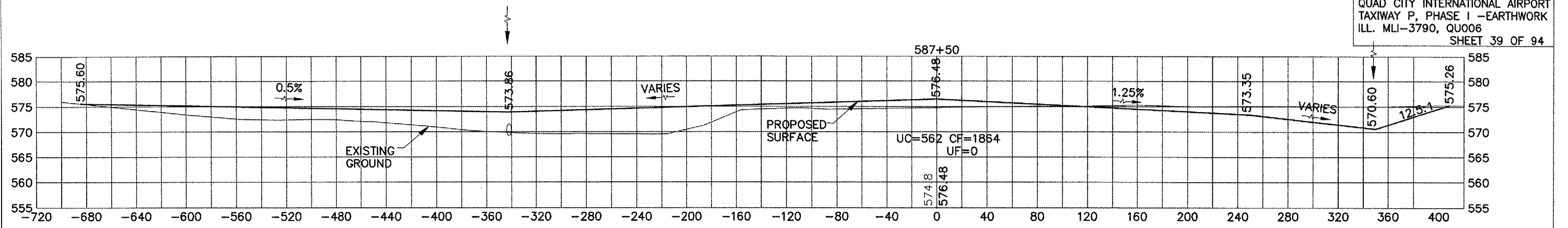


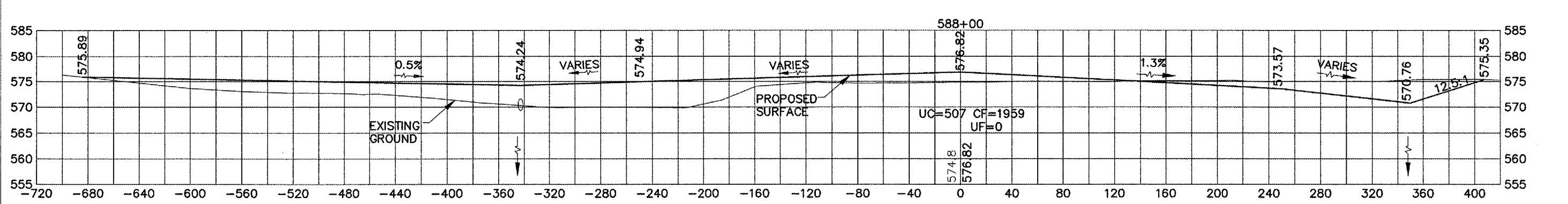
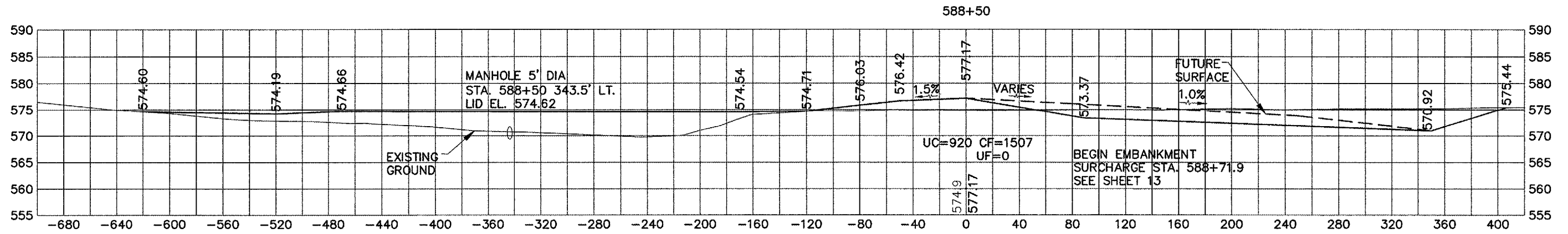
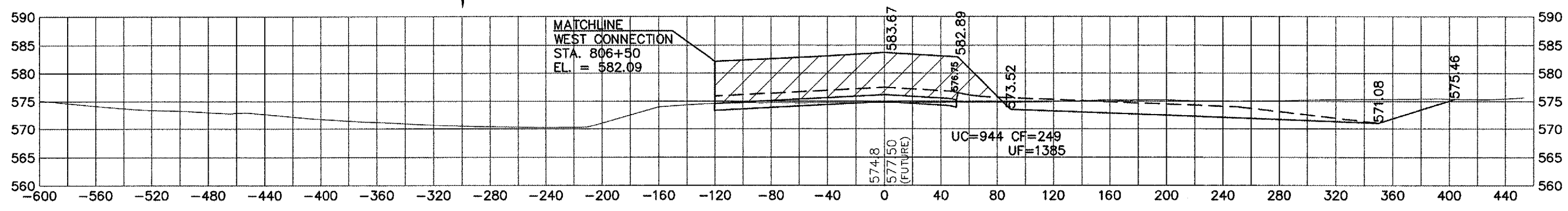
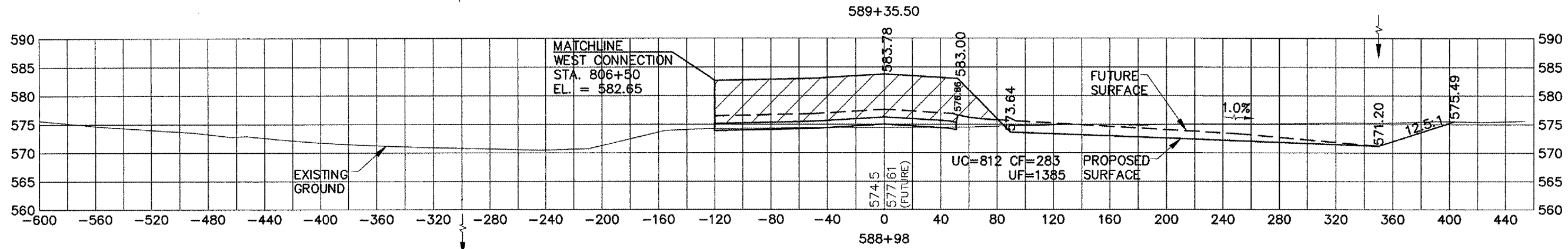
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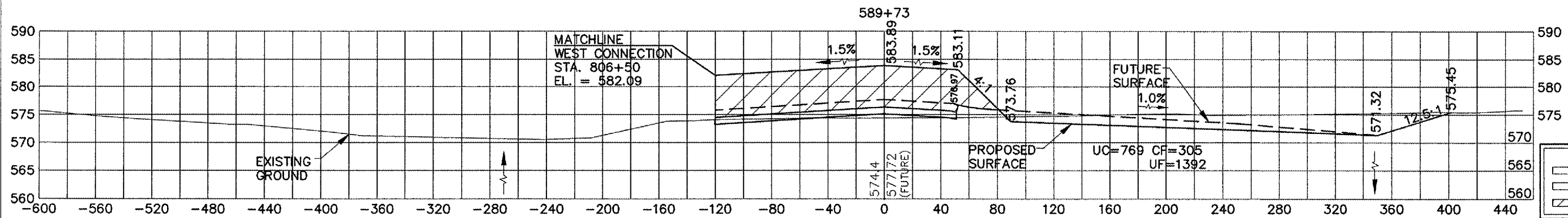
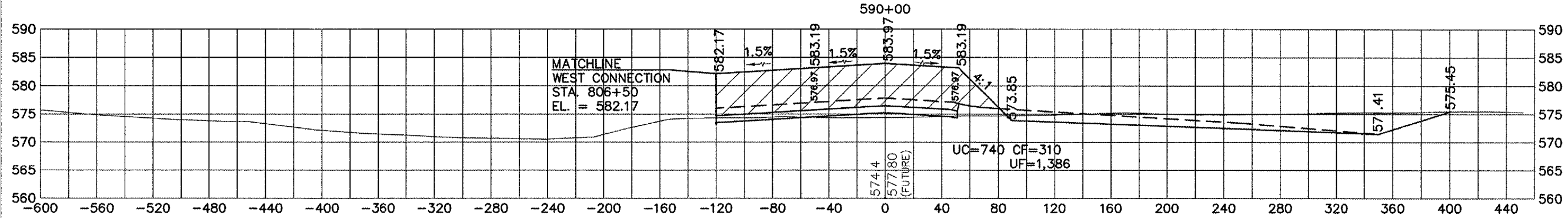
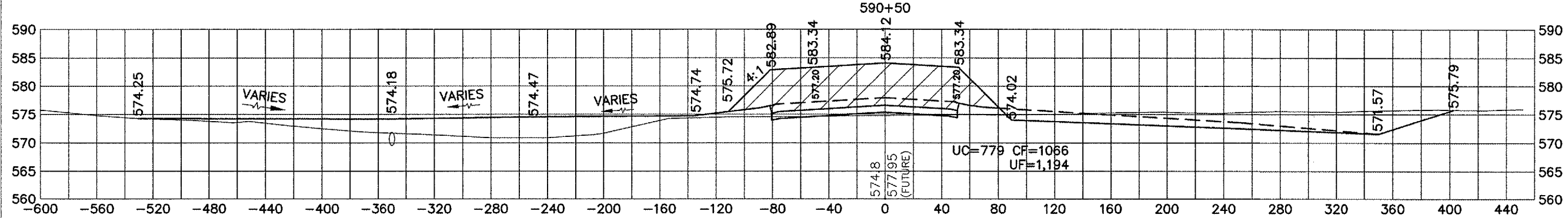
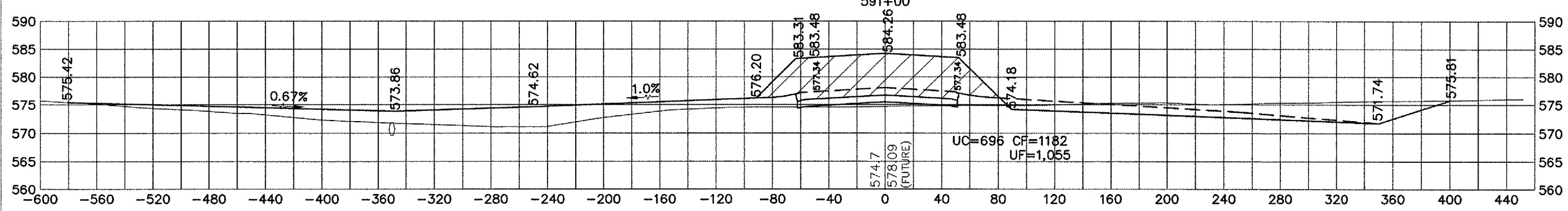
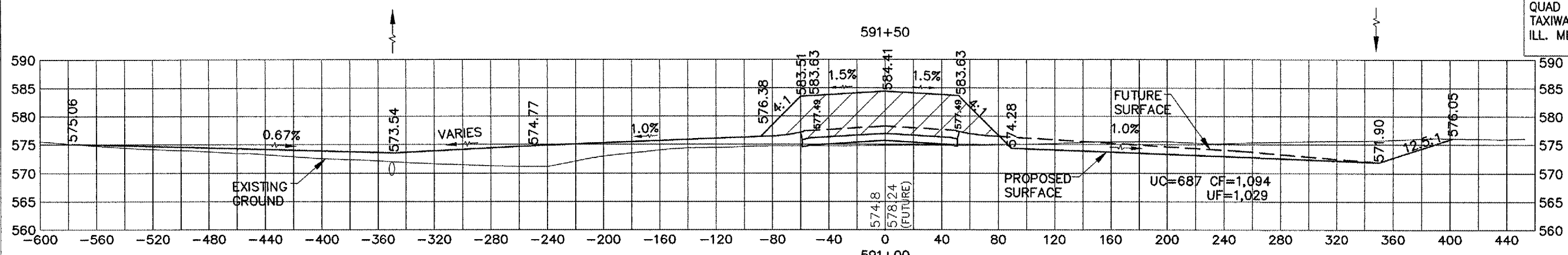




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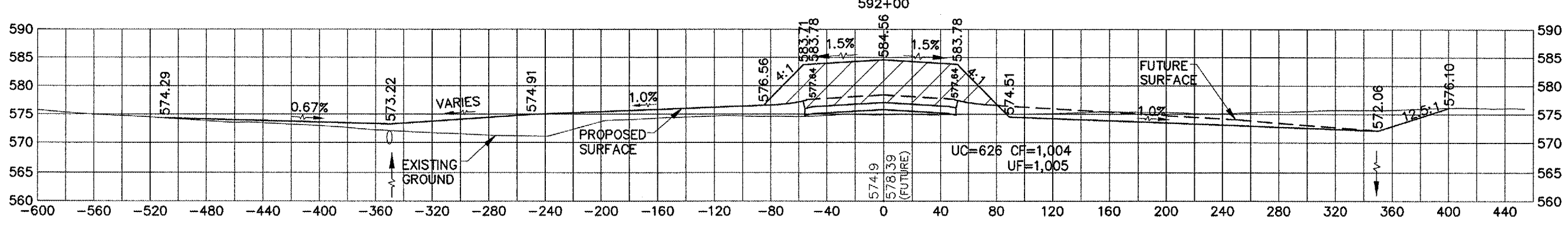
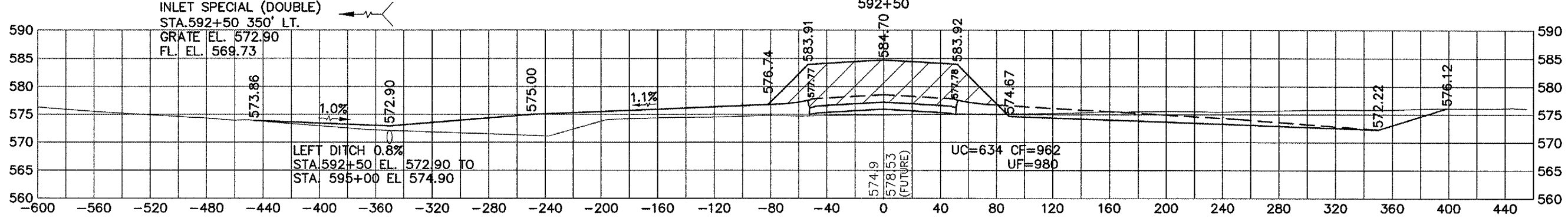
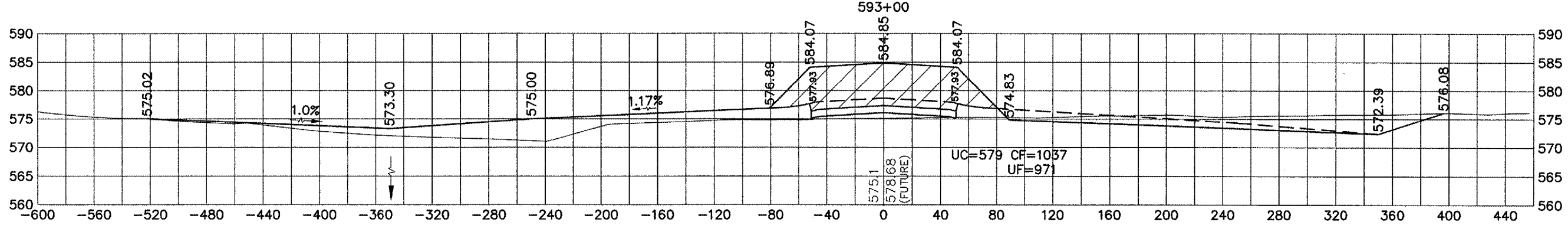
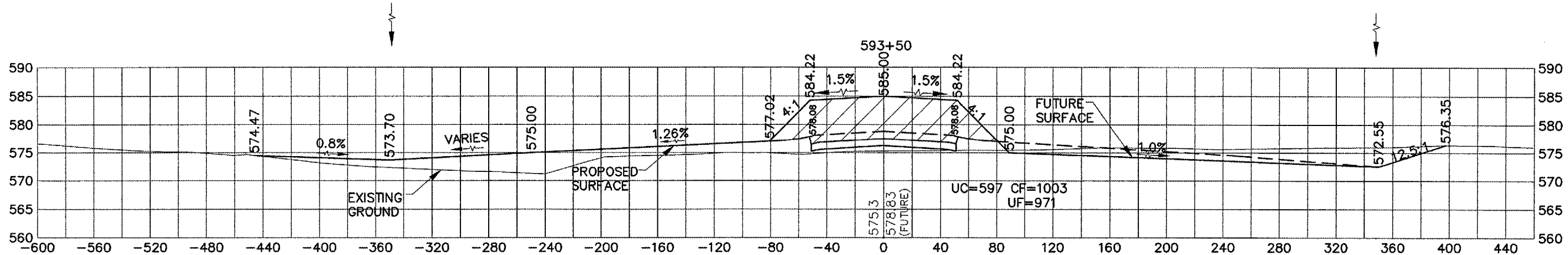
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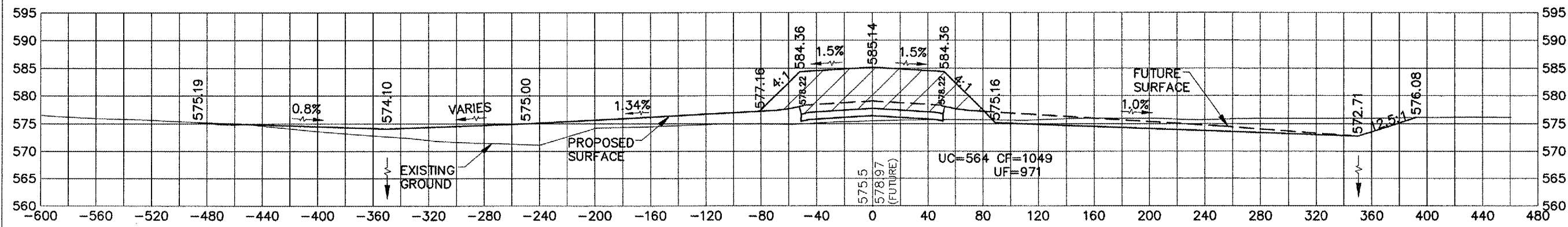
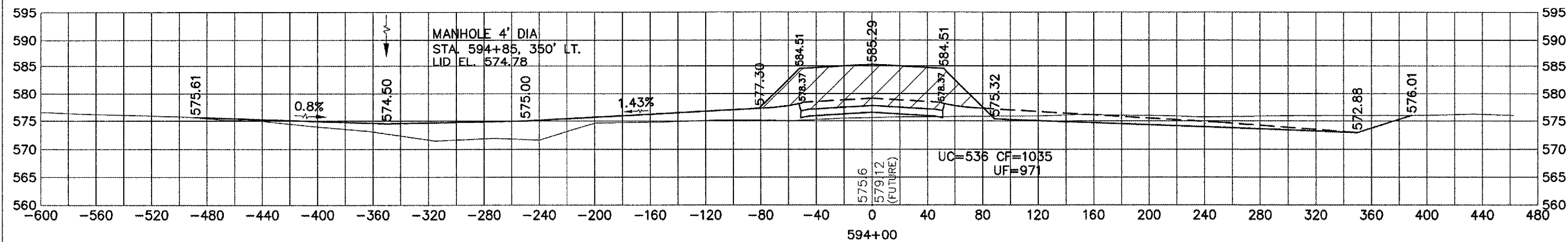
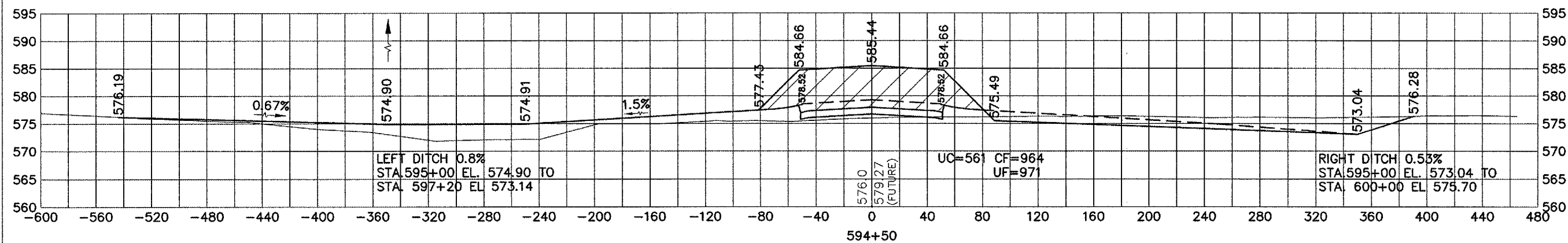
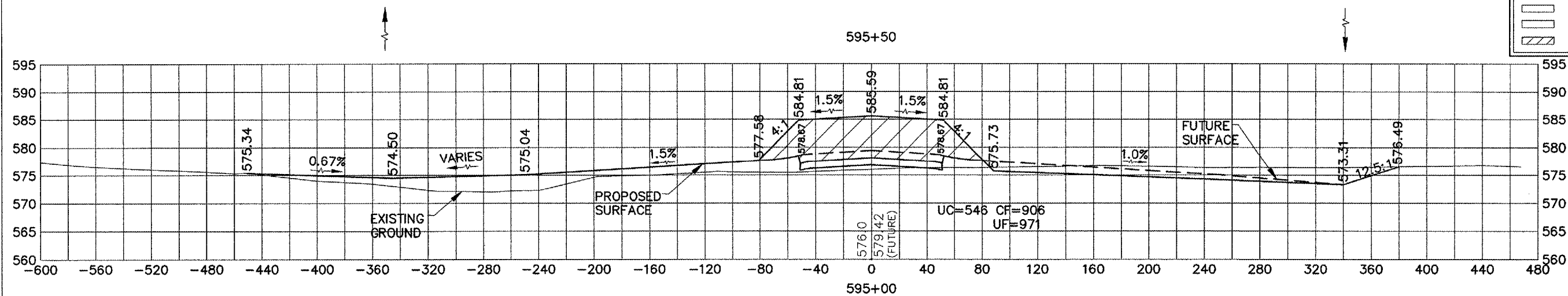
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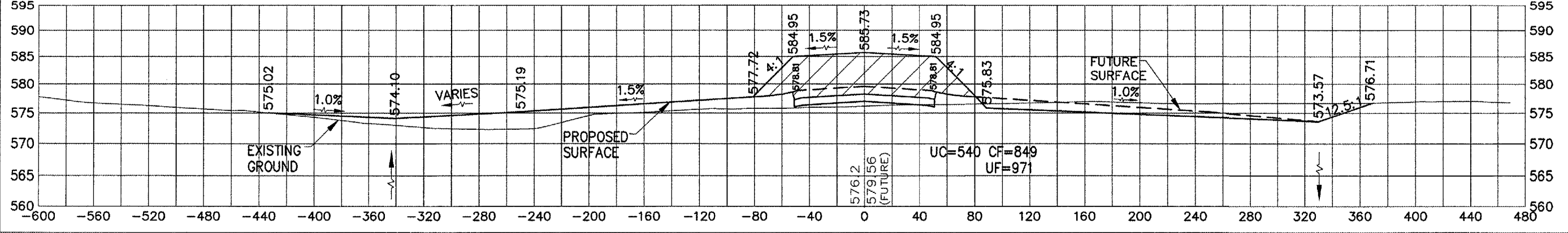
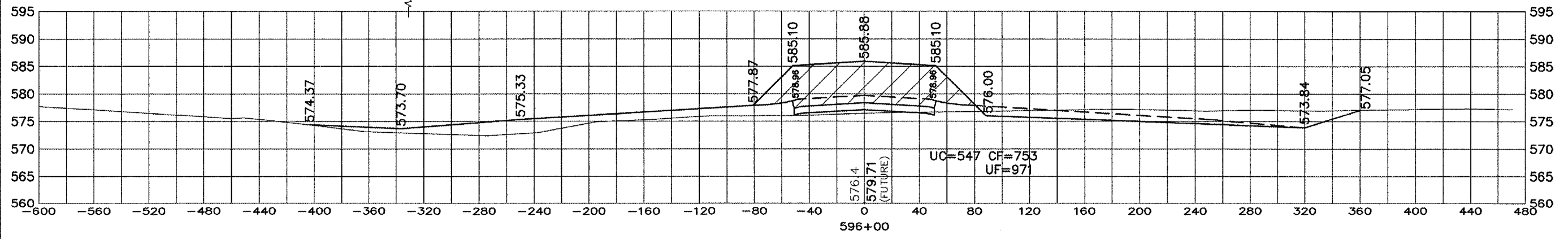
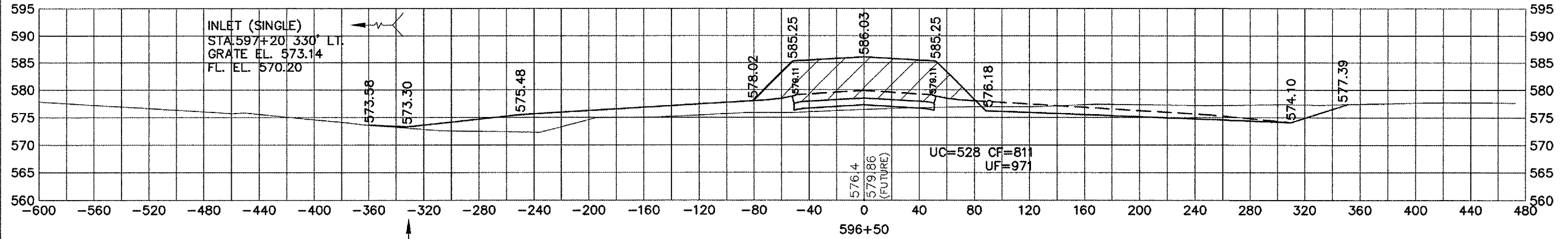
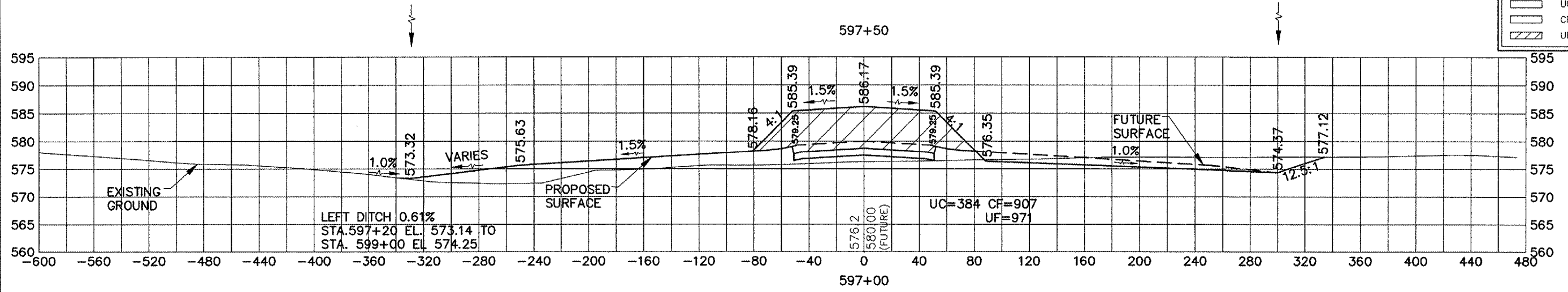
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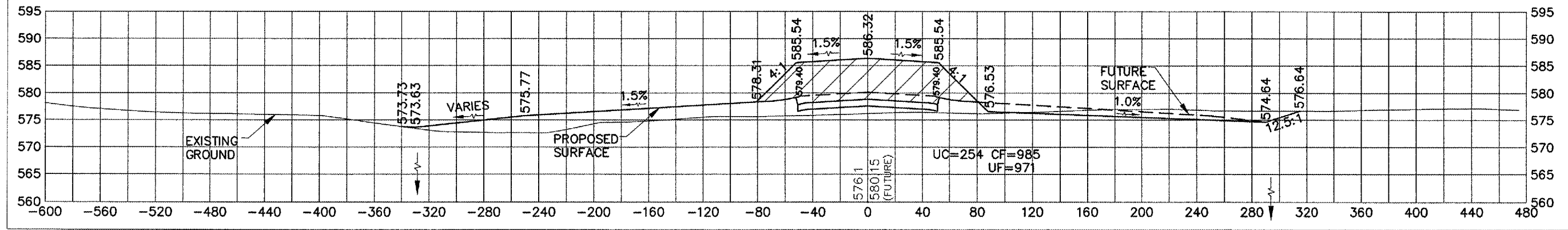
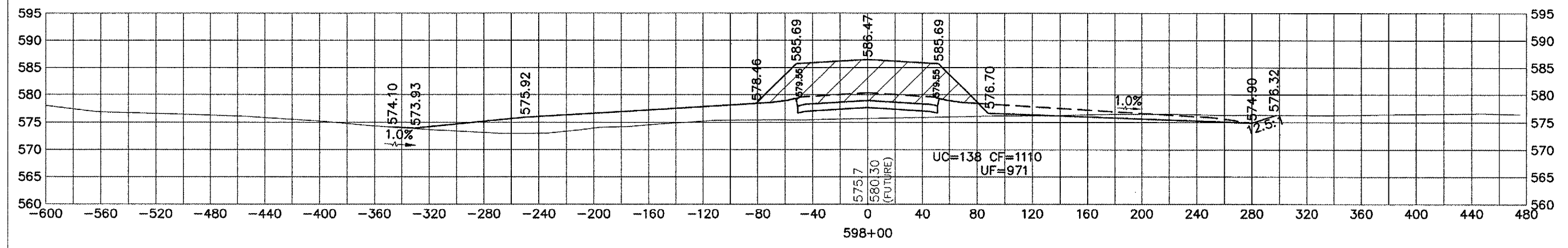
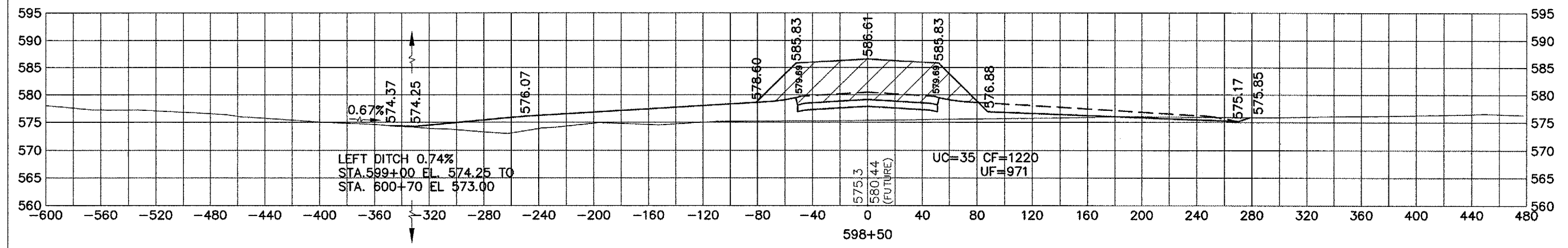
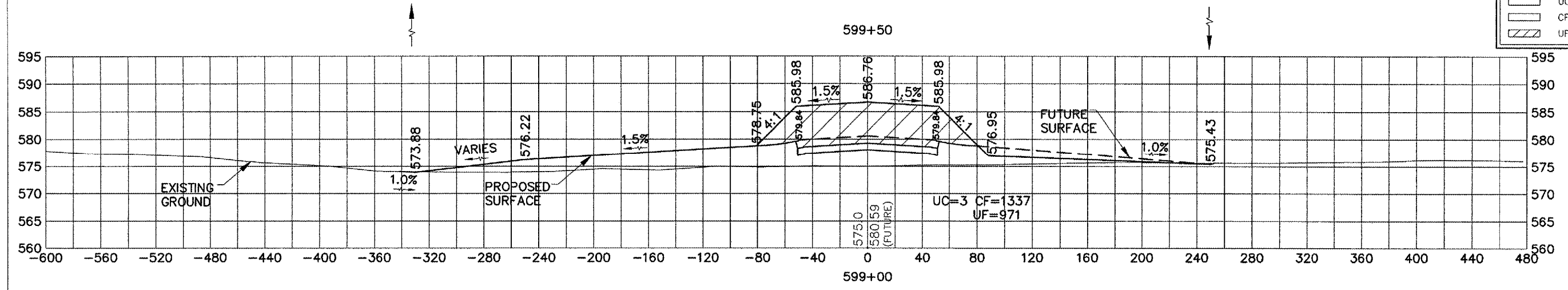
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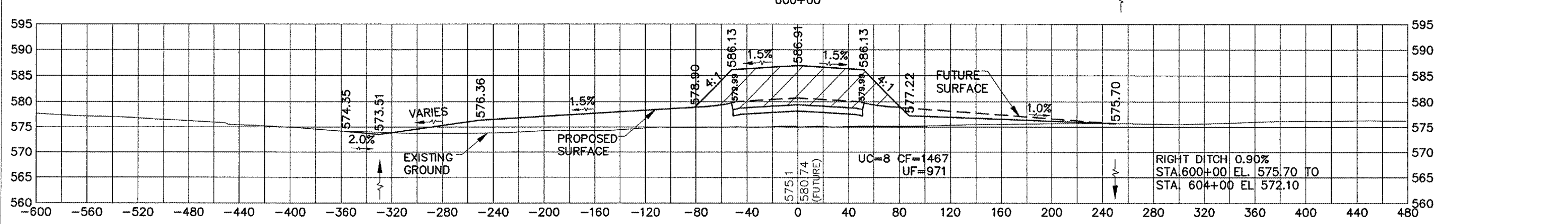
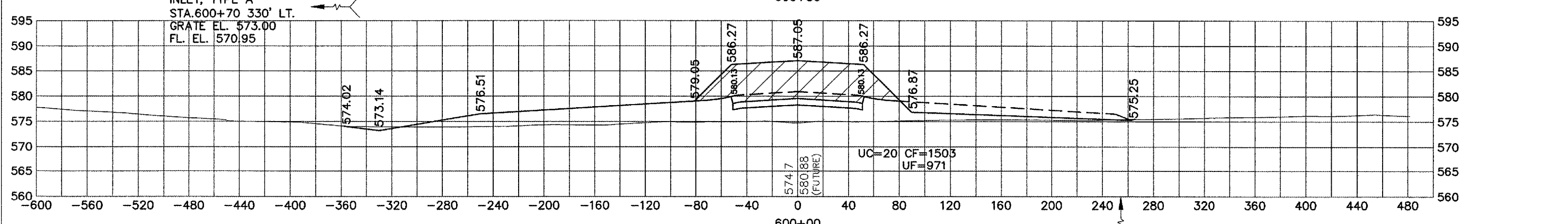
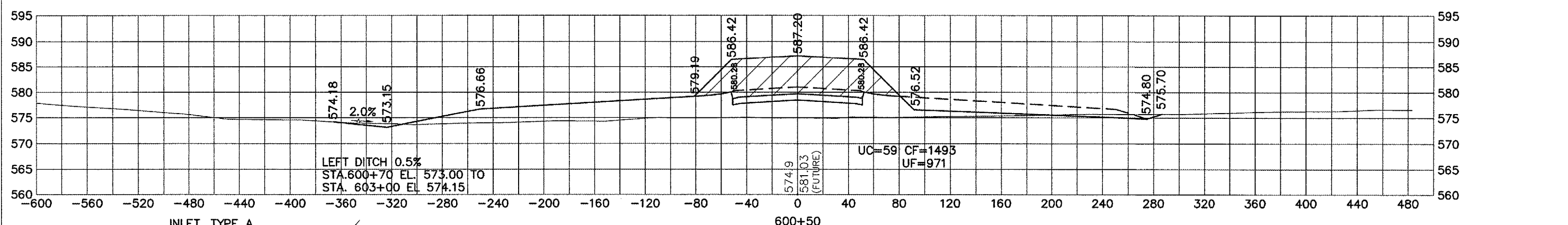
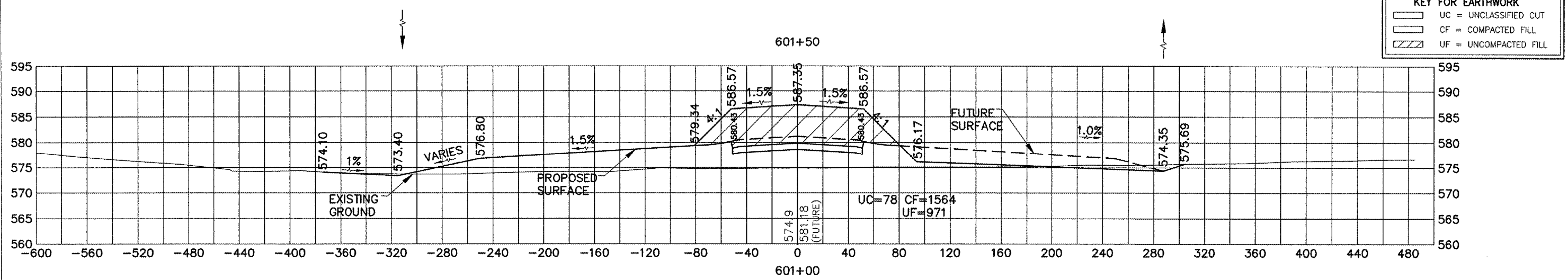
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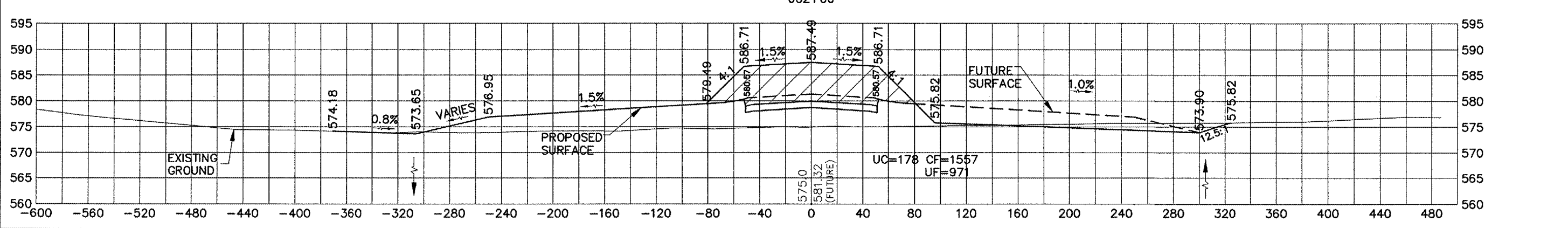
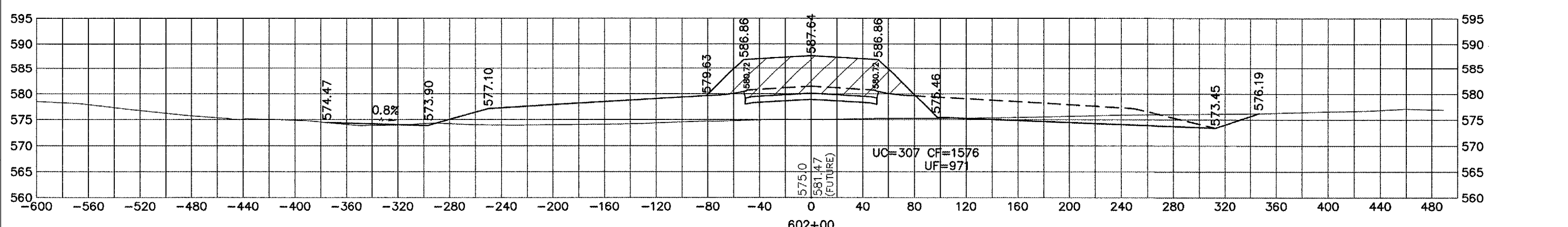
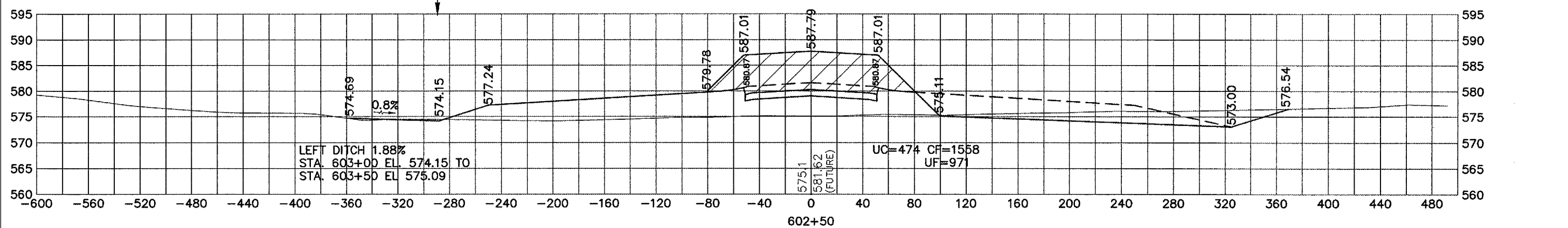
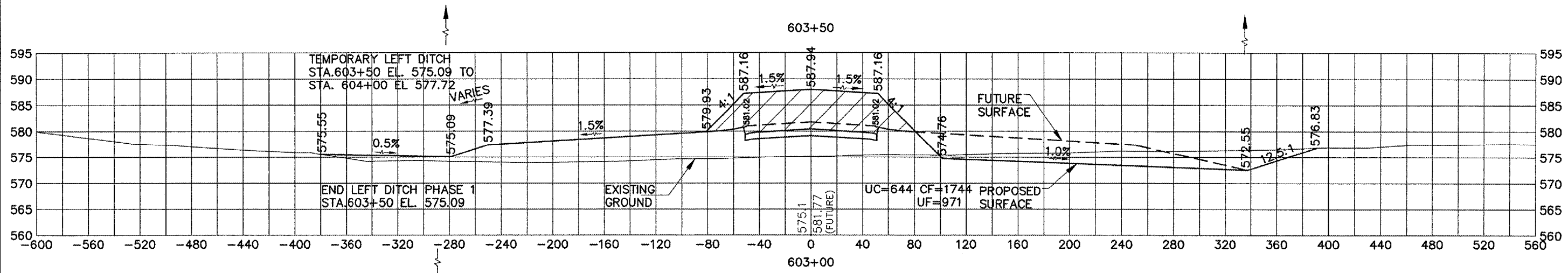
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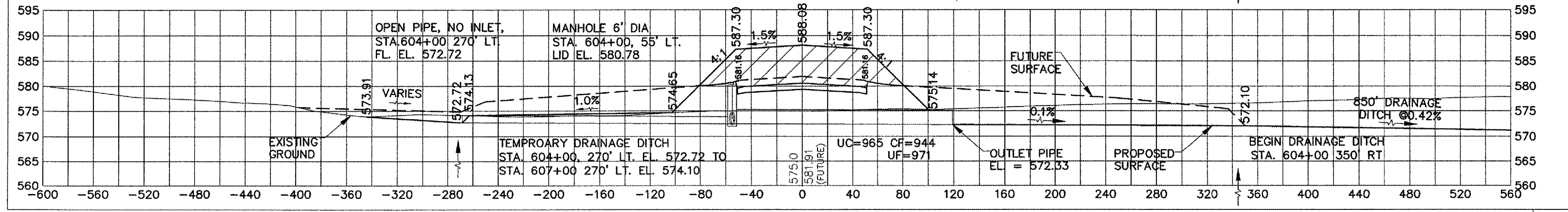
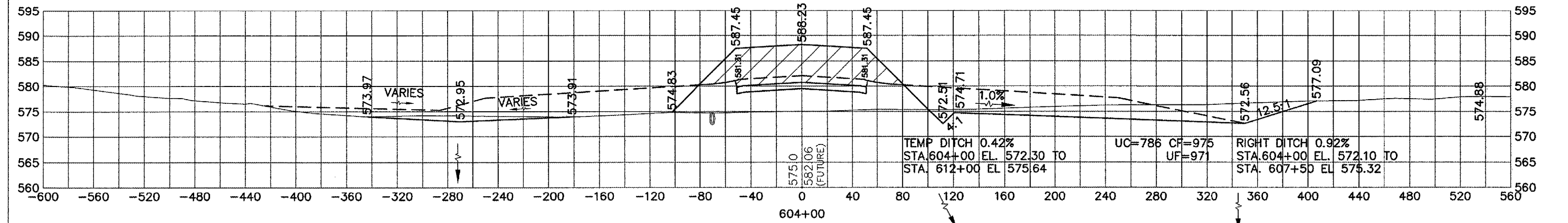
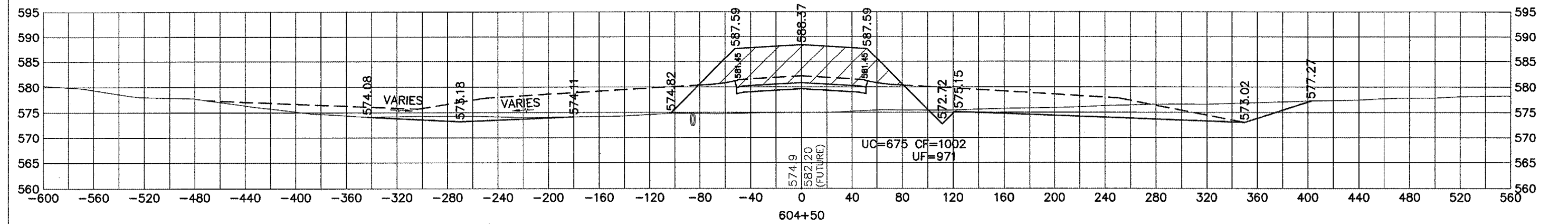
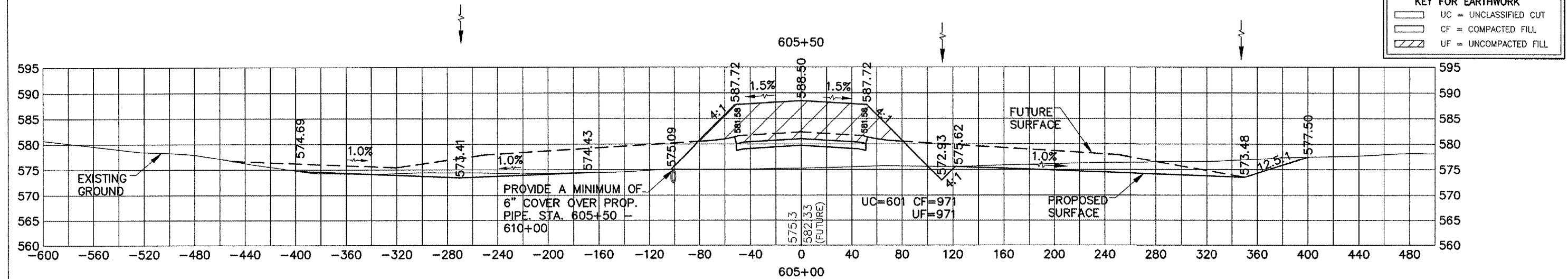
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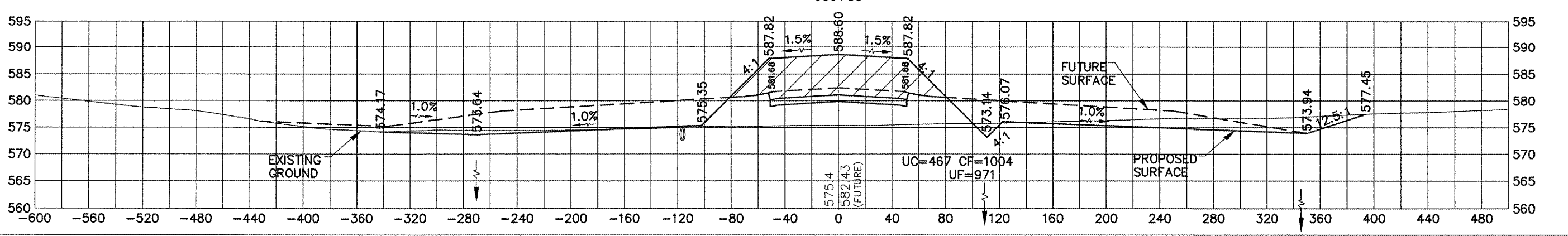
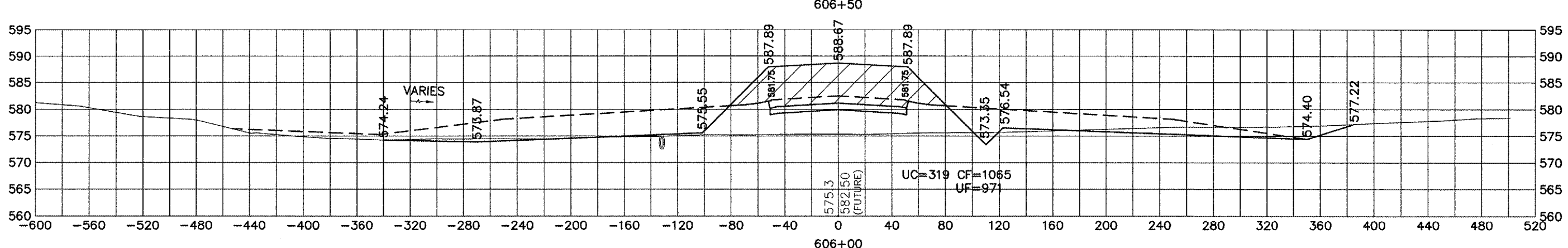
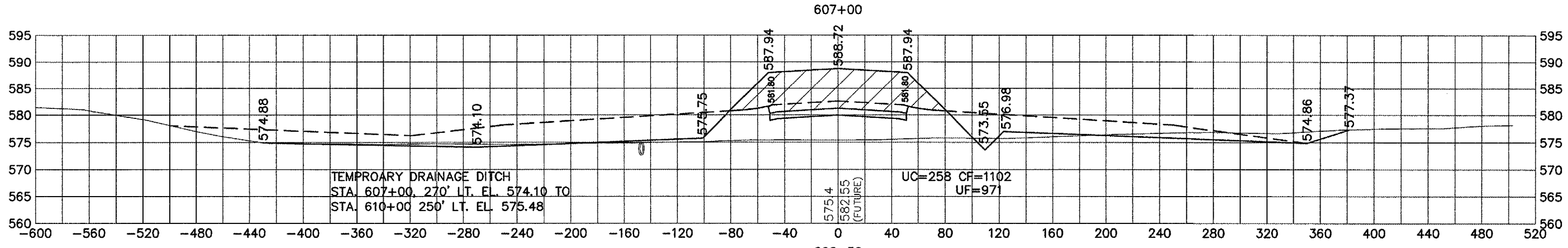
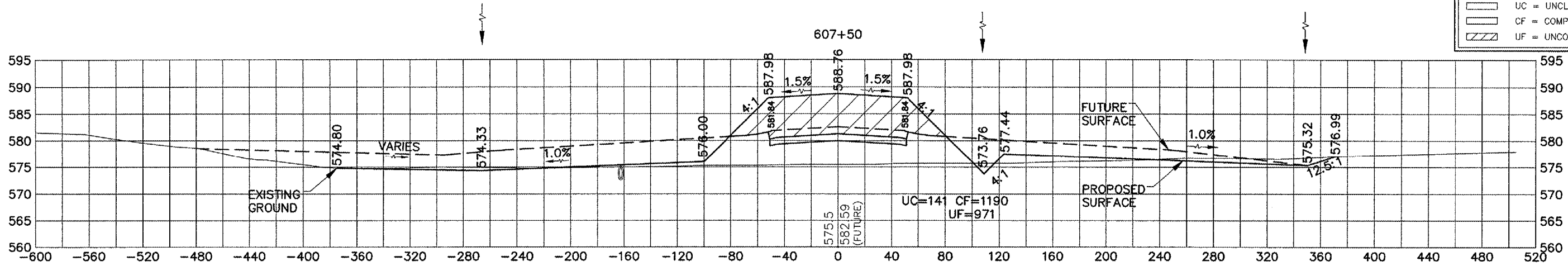


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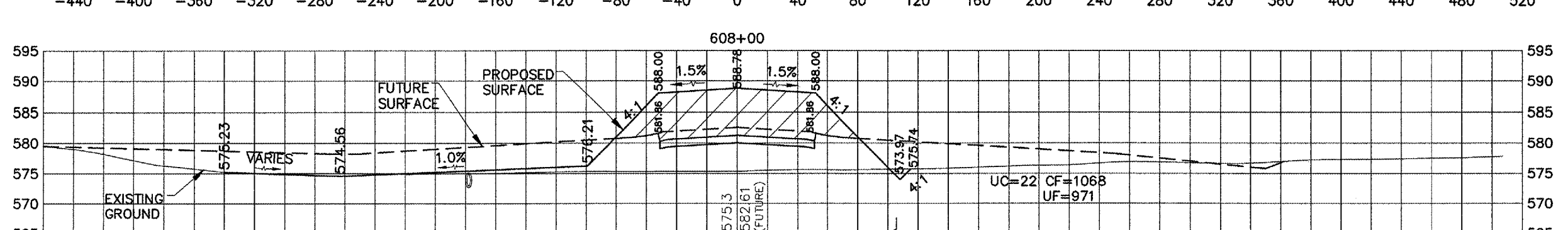
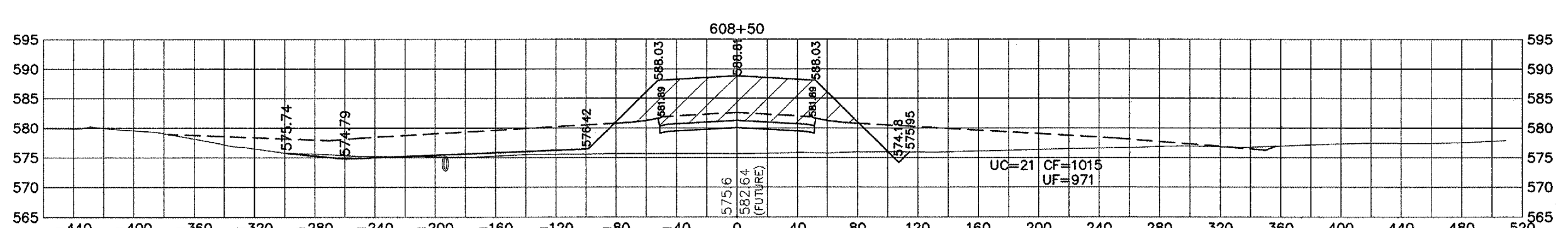
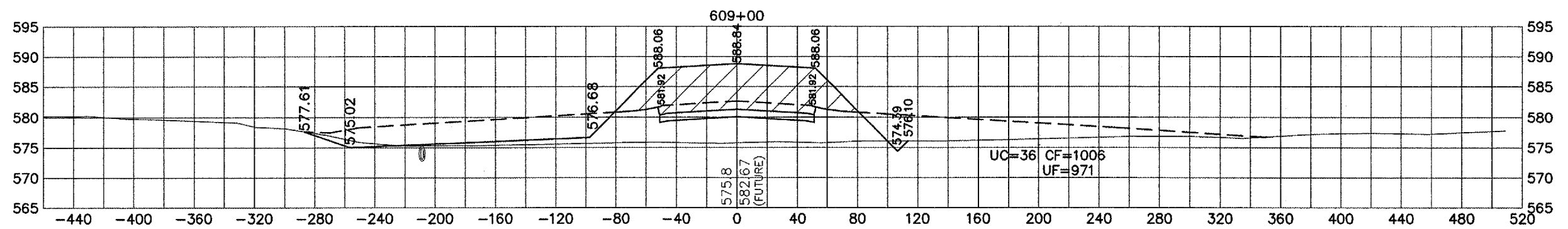
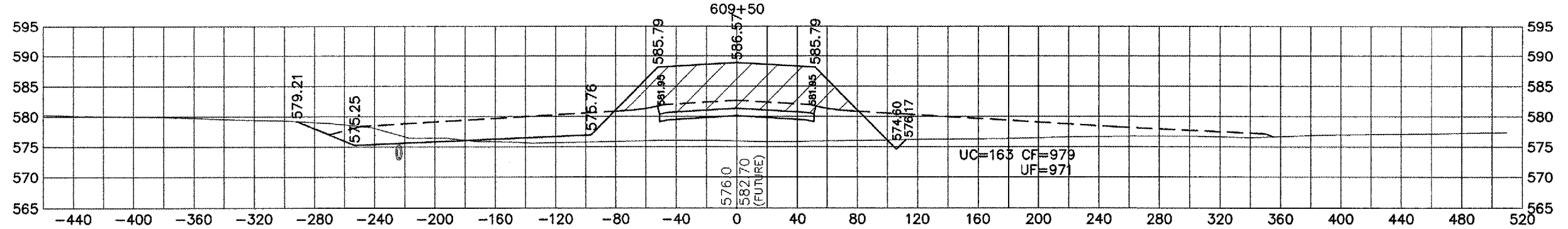
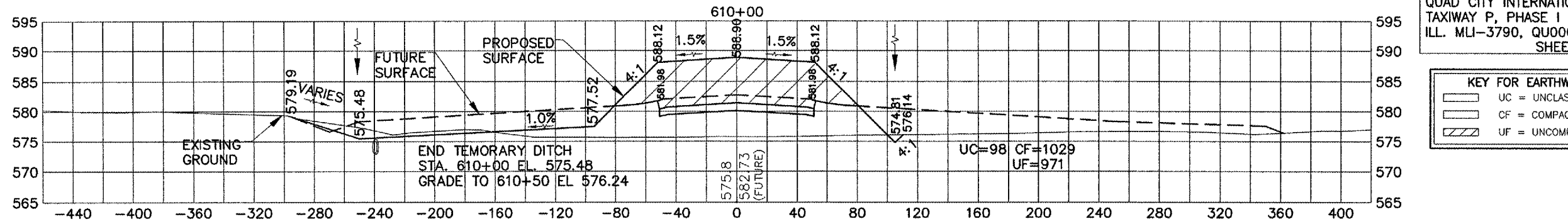
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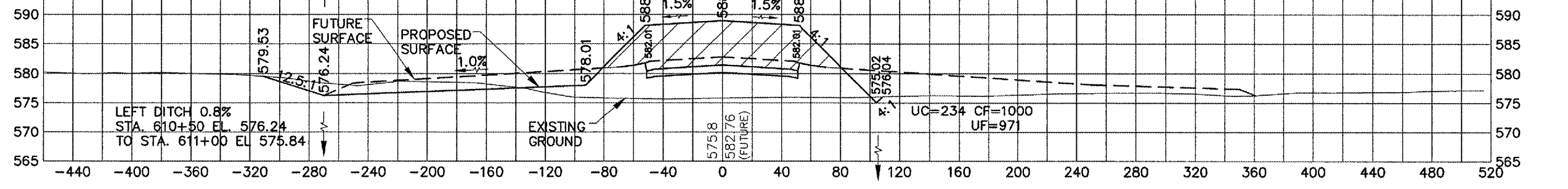
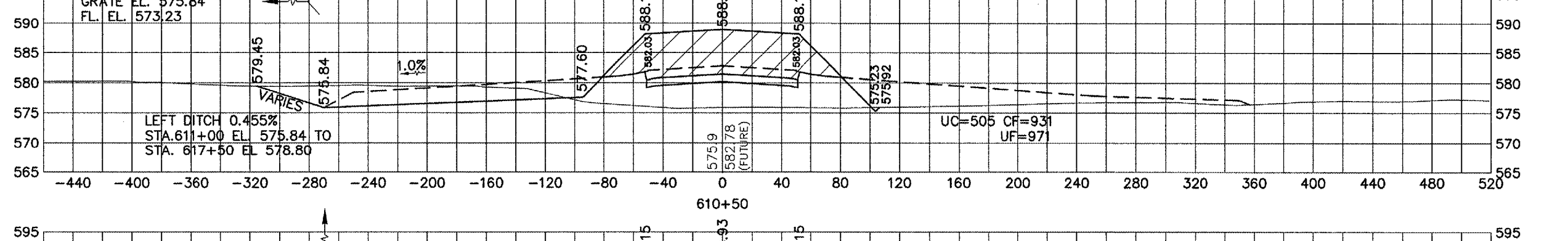
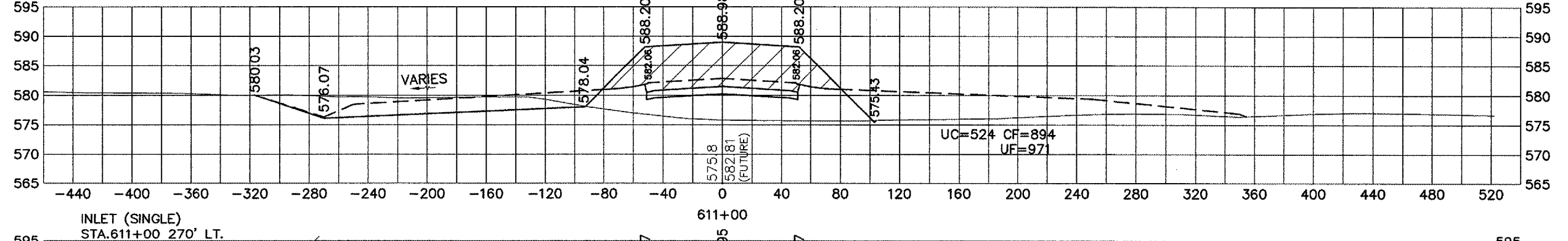
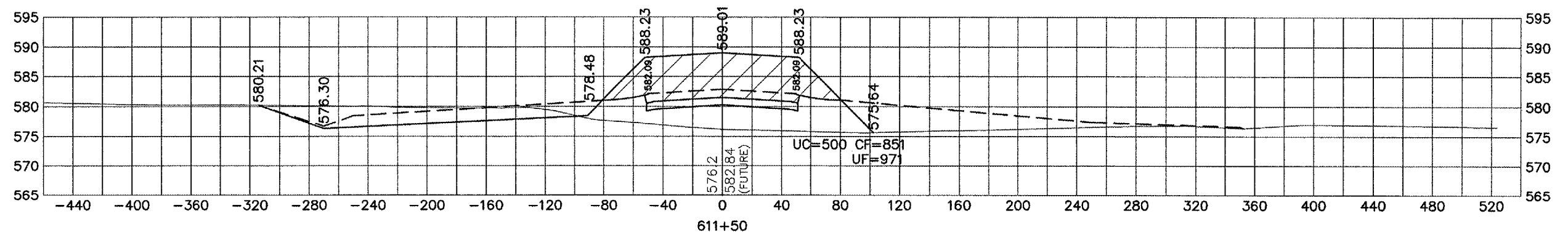
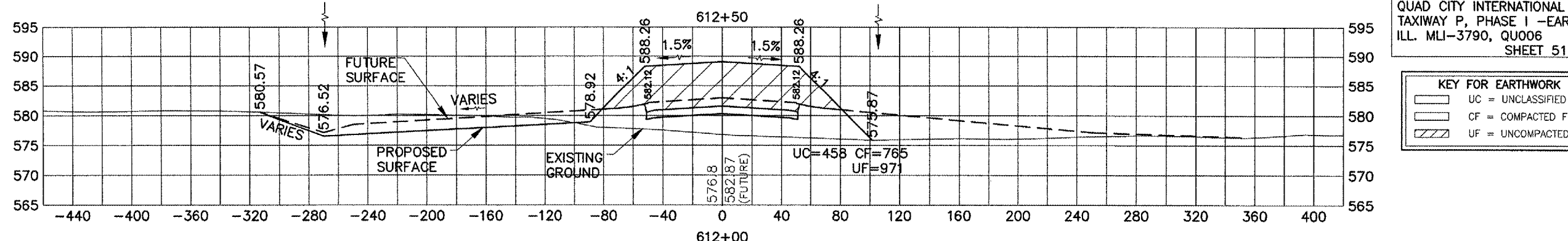
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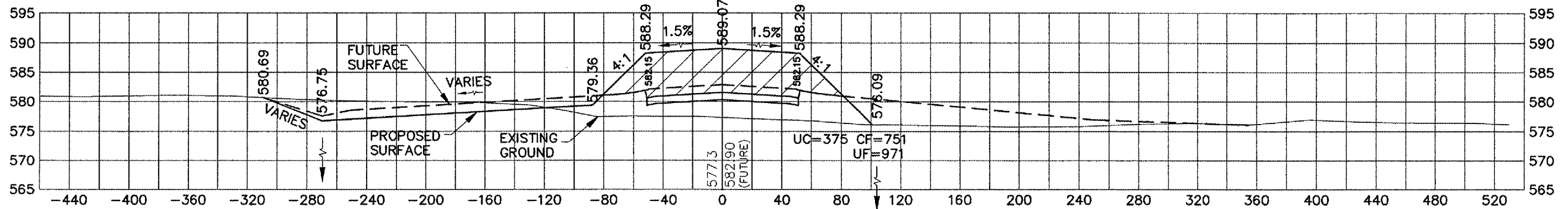
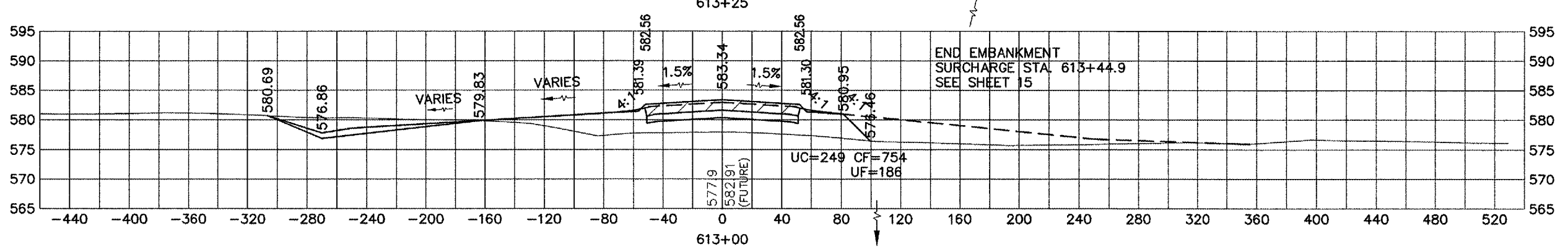
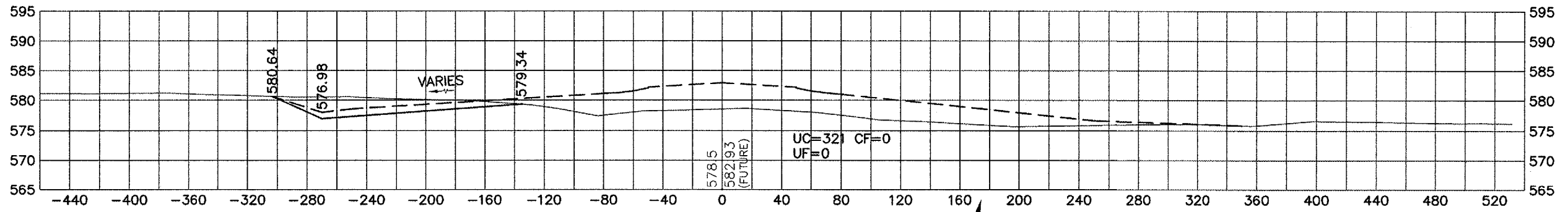
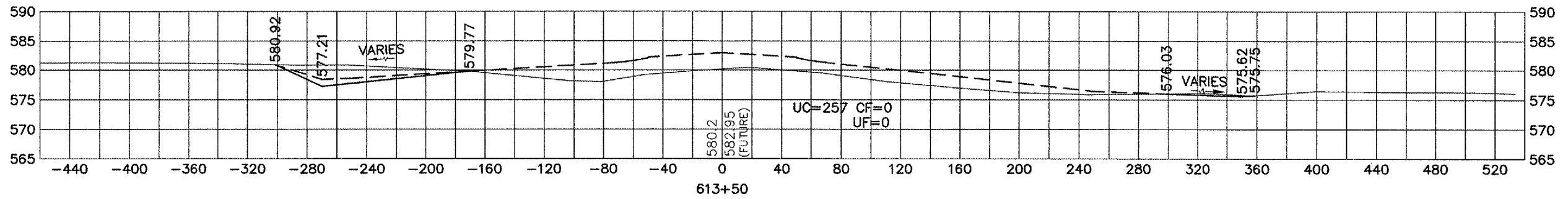
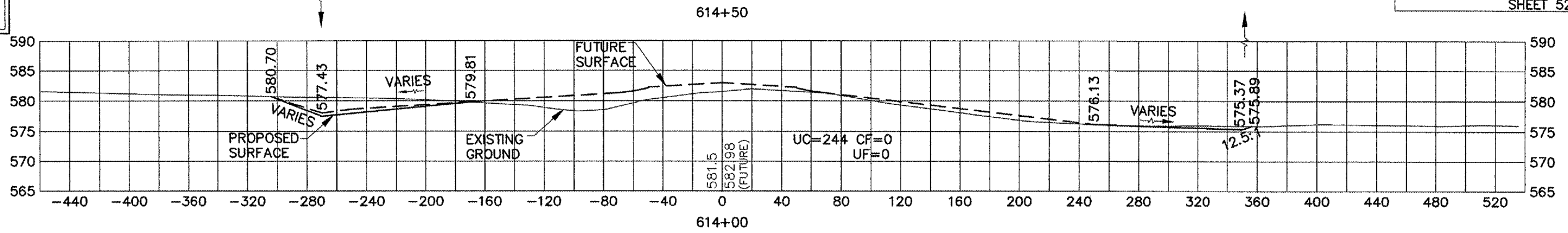
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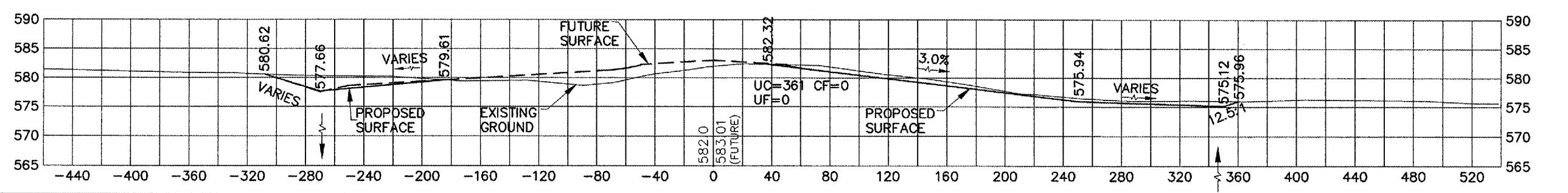
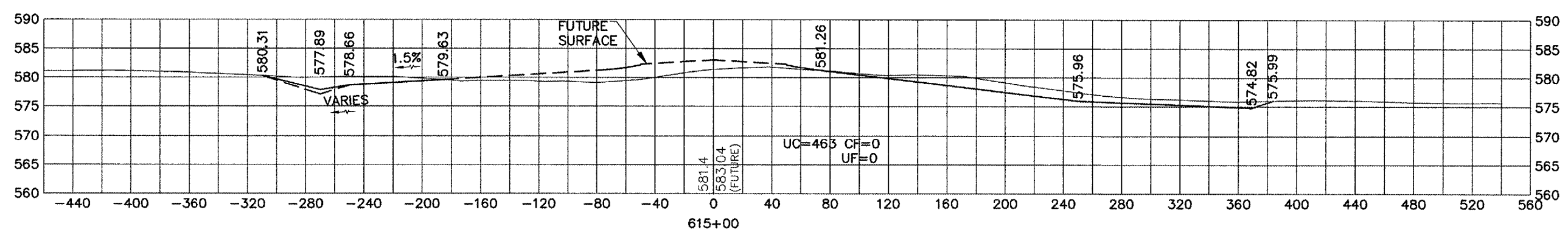
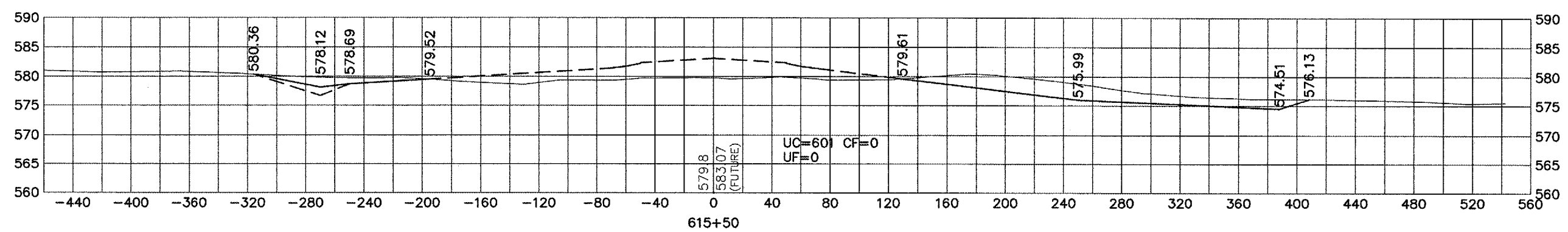
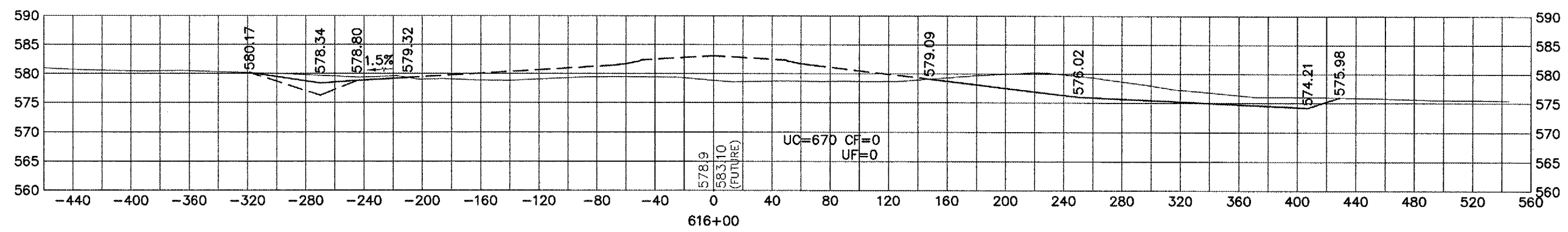
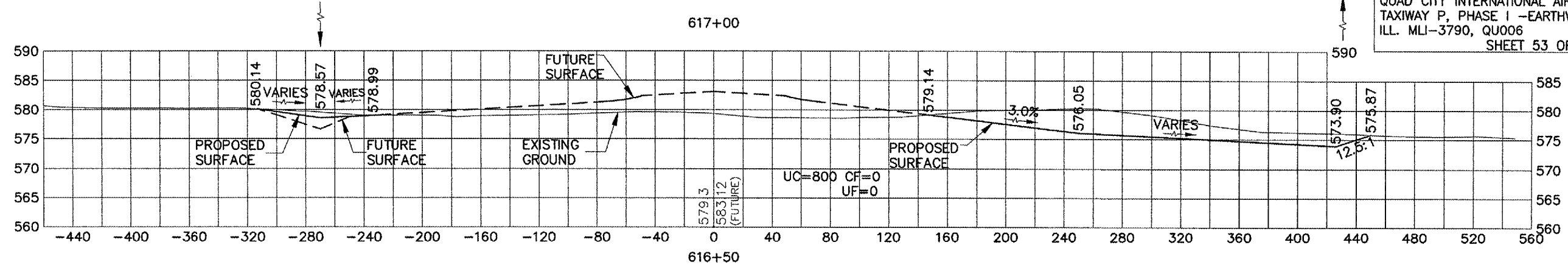


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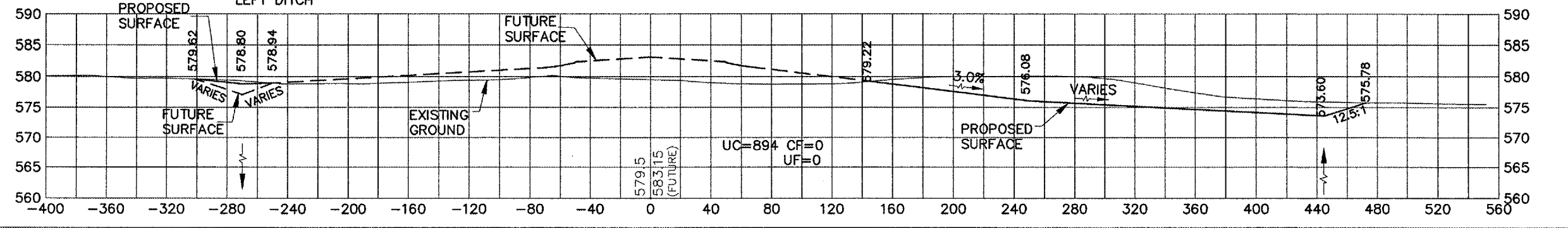
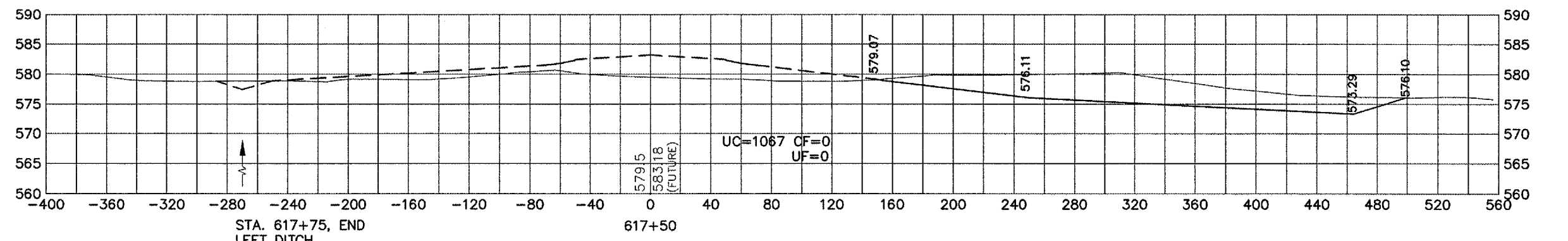
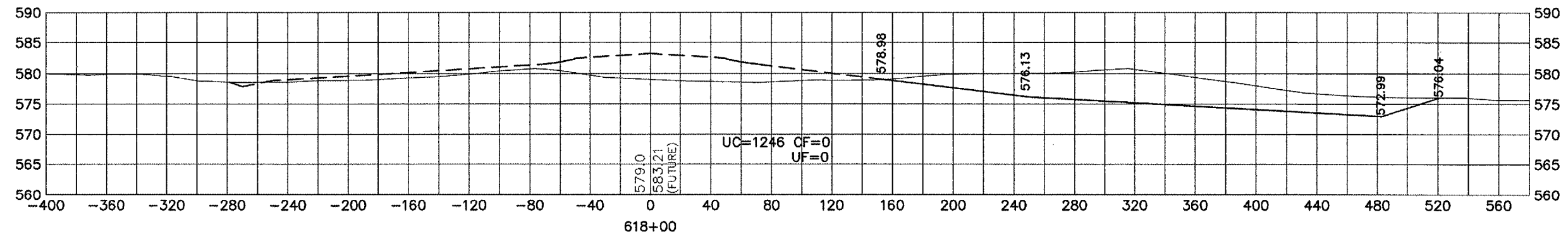
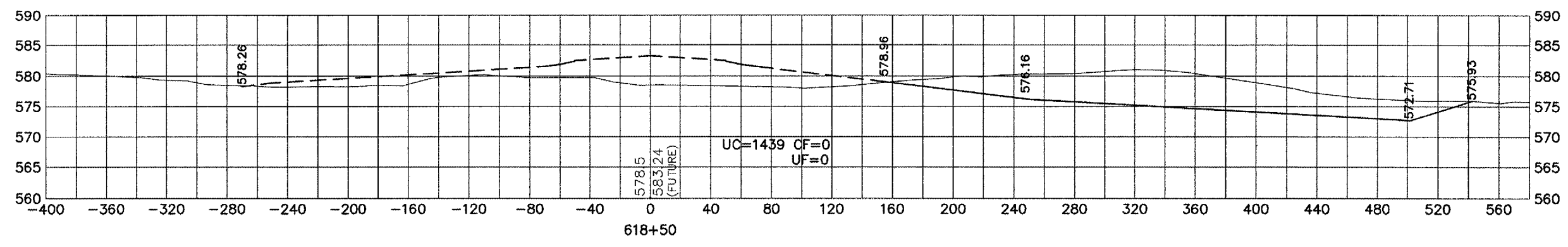
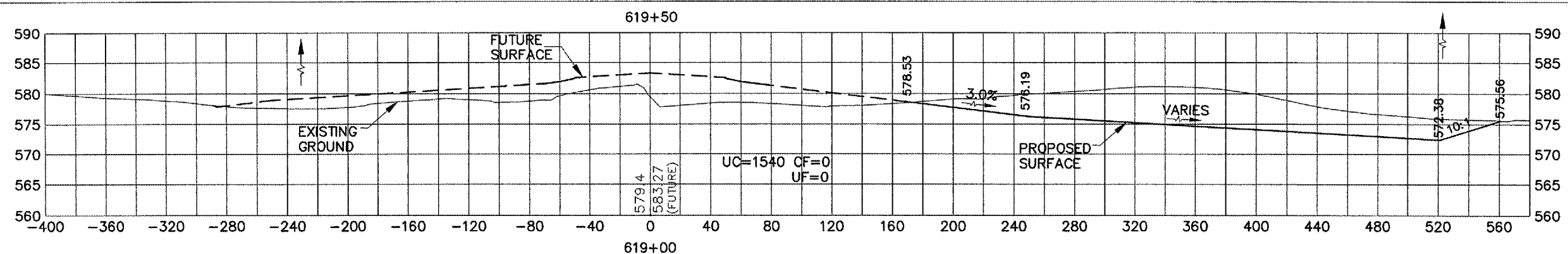
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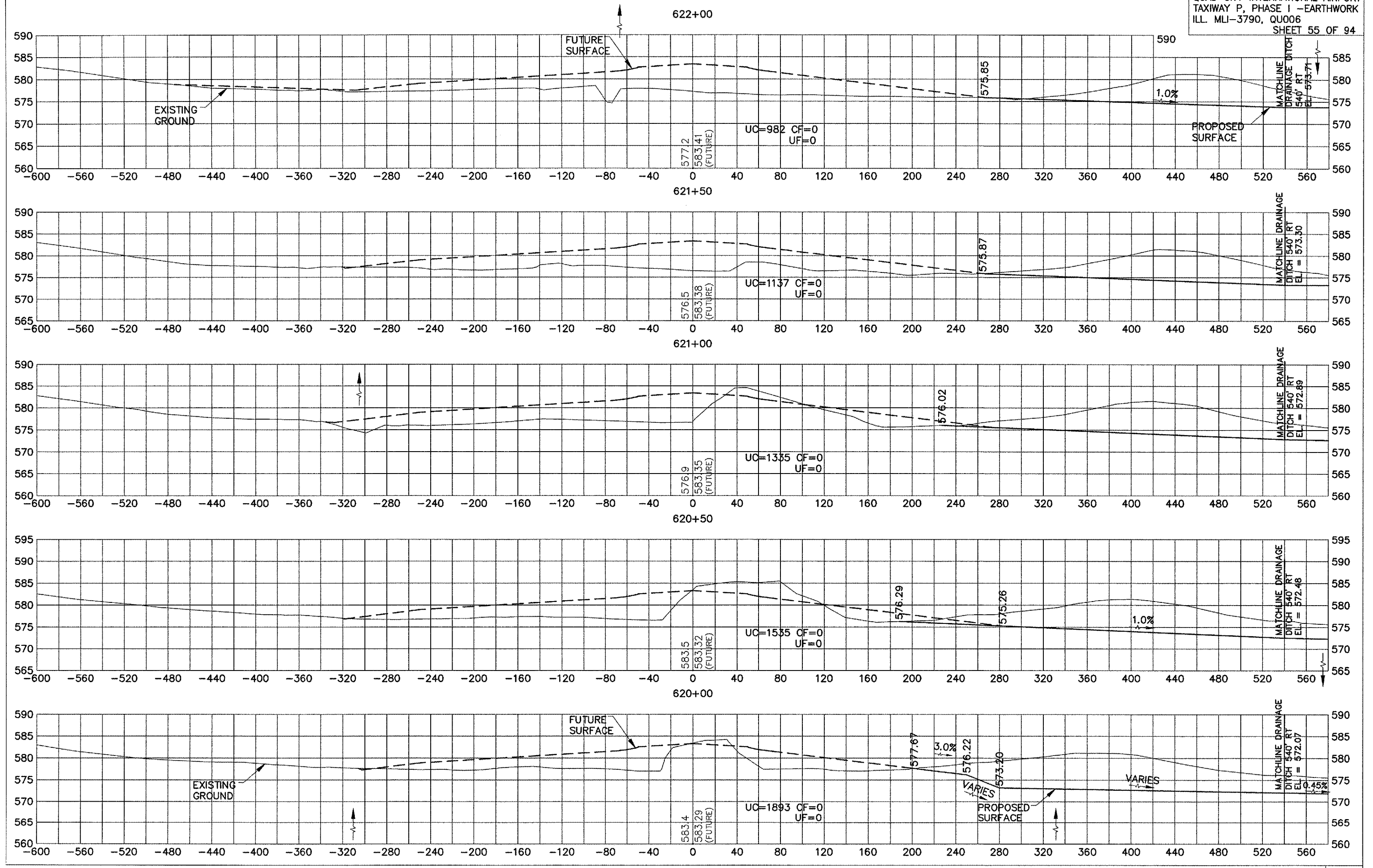




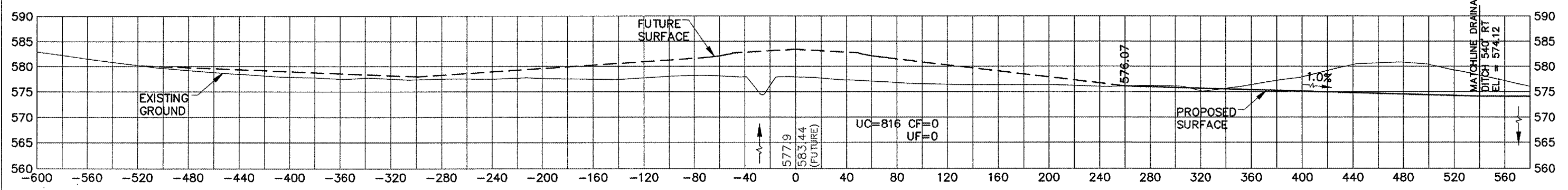
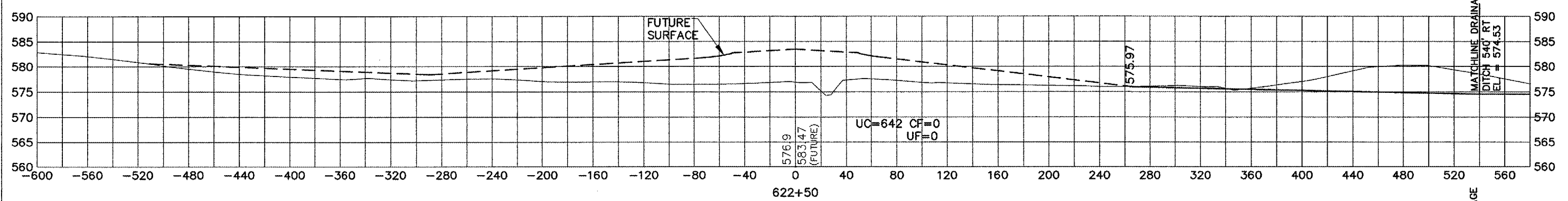
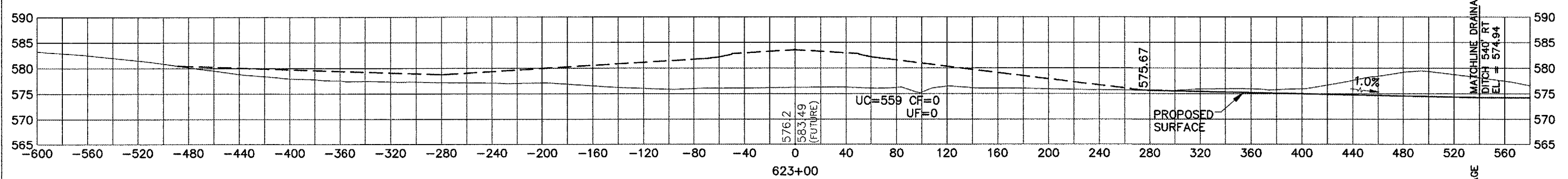
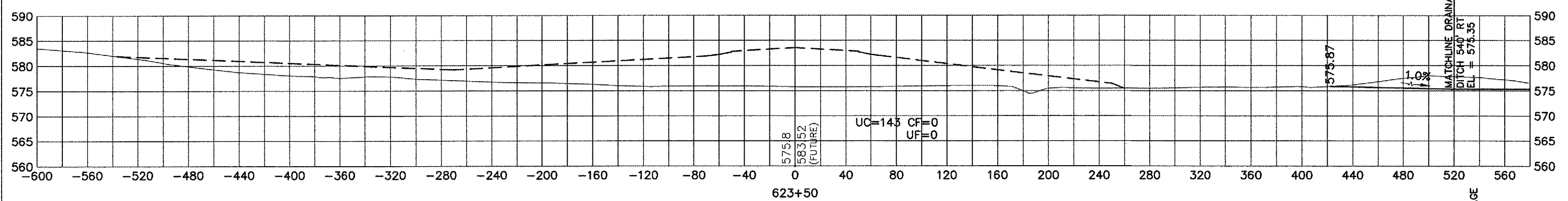
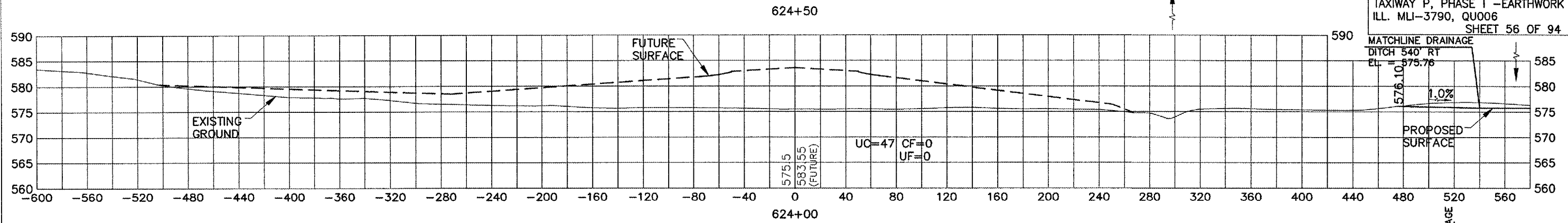
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

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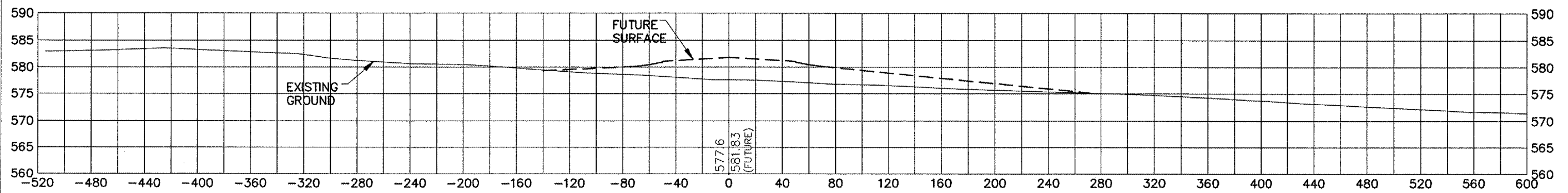
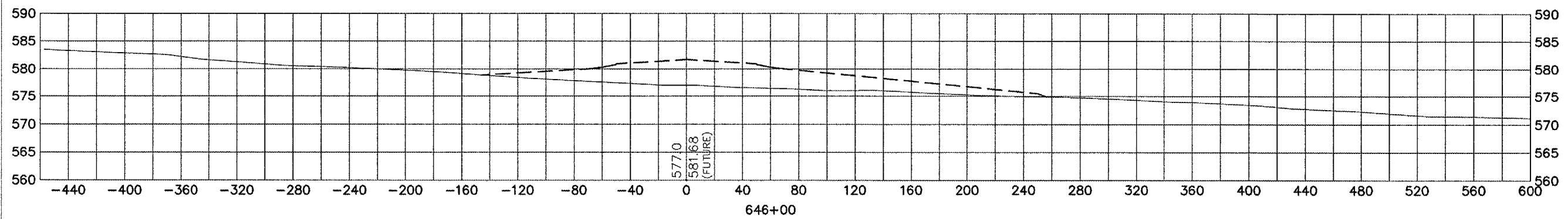
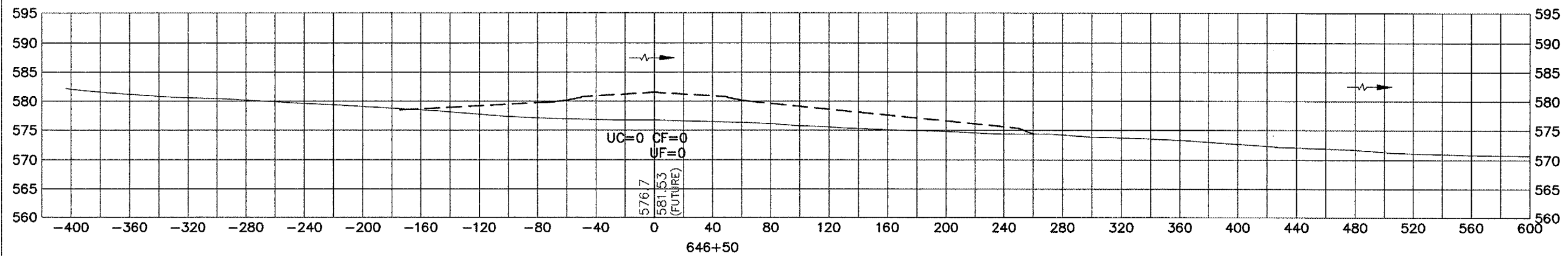
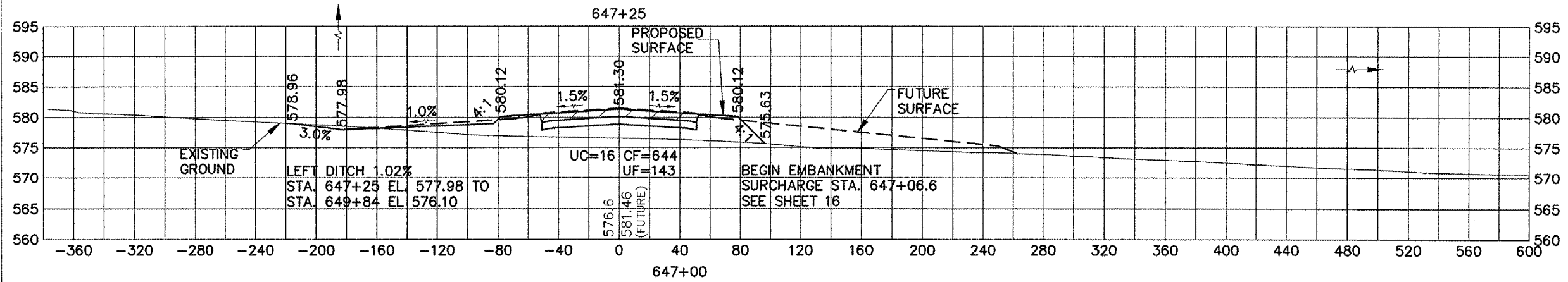


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



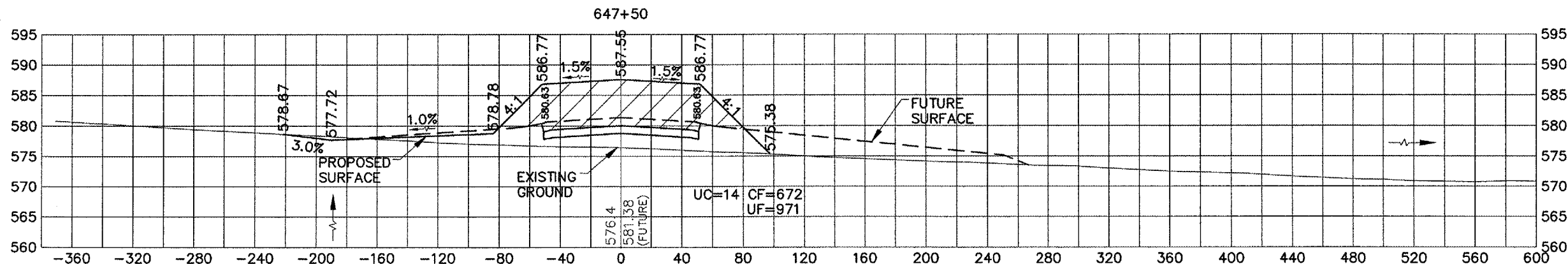
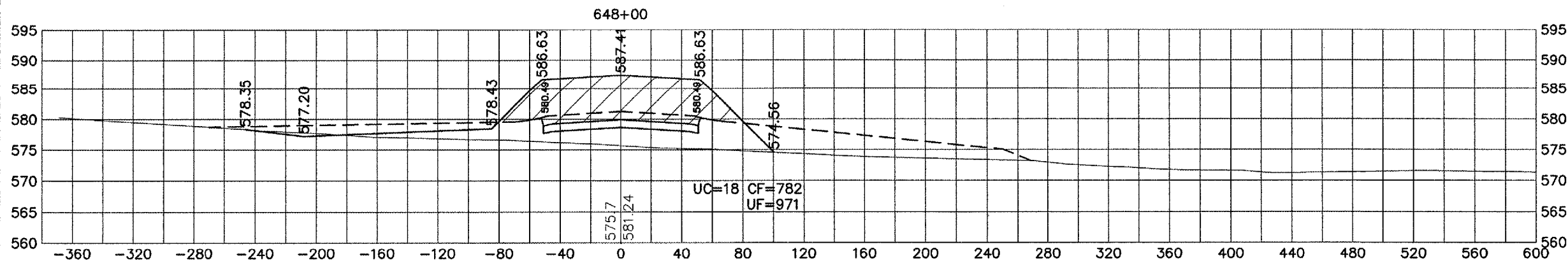
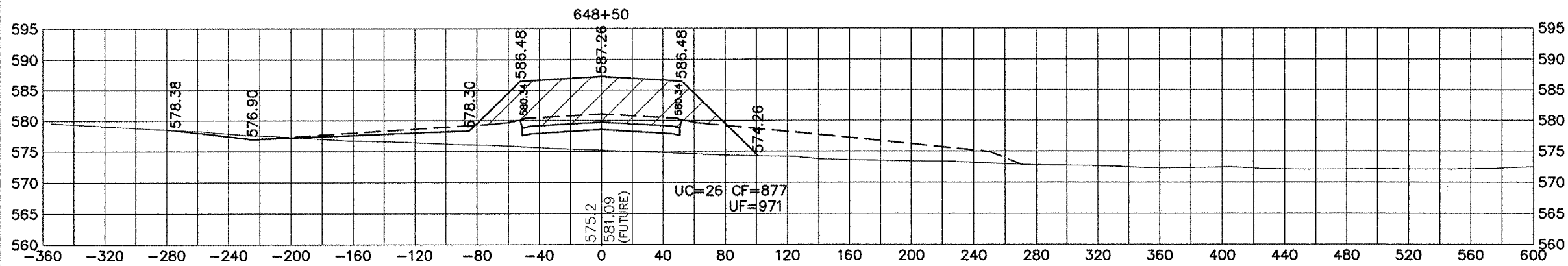
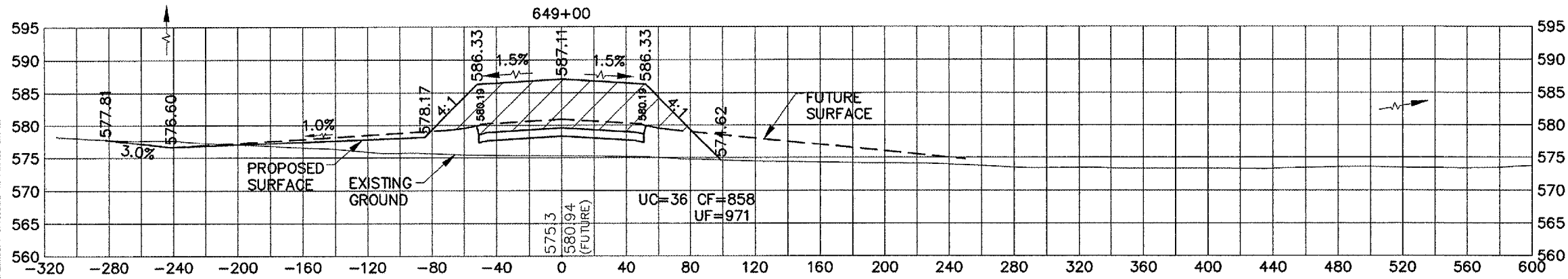
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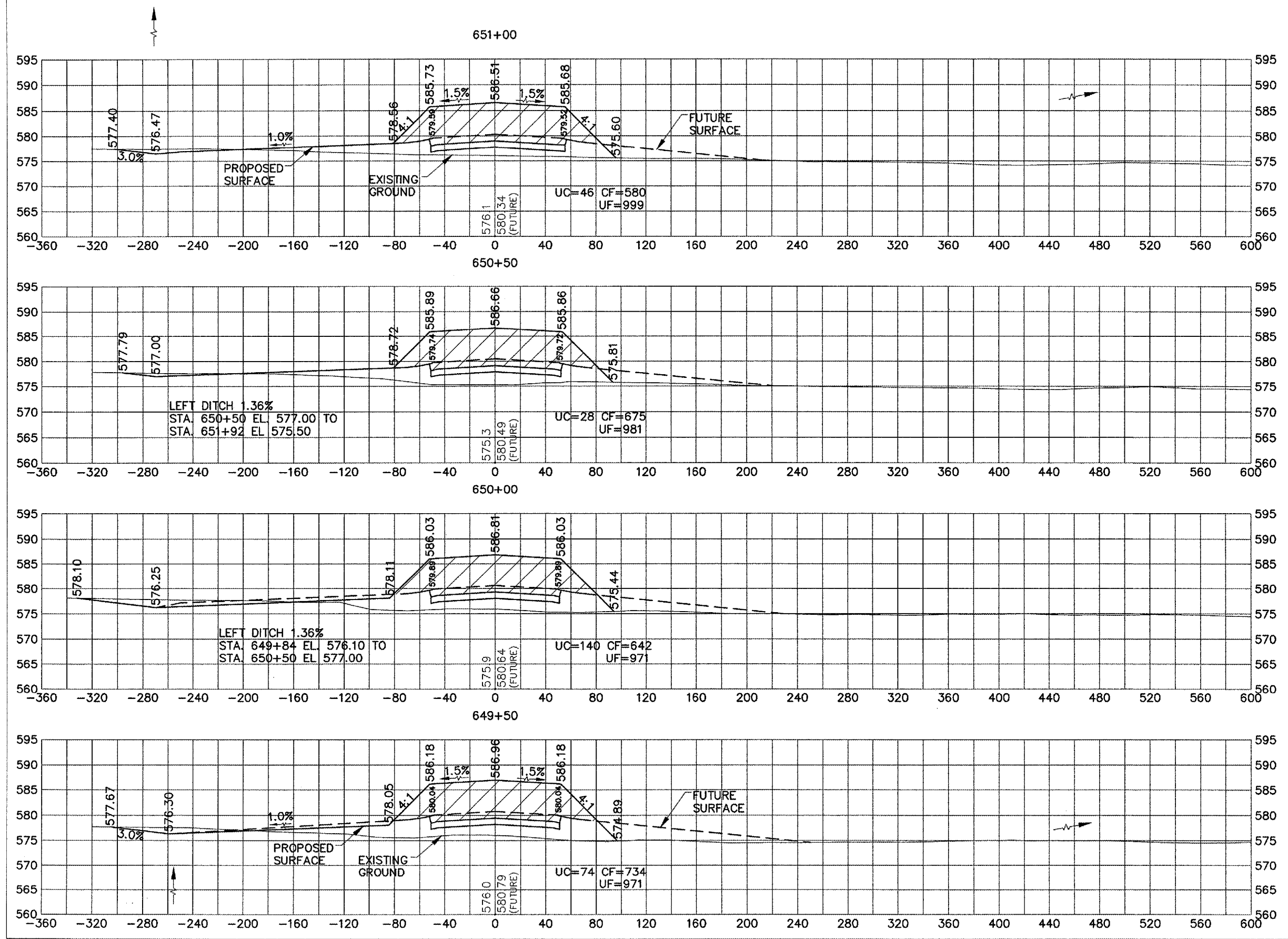


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KEY FOR EARTHWORK	
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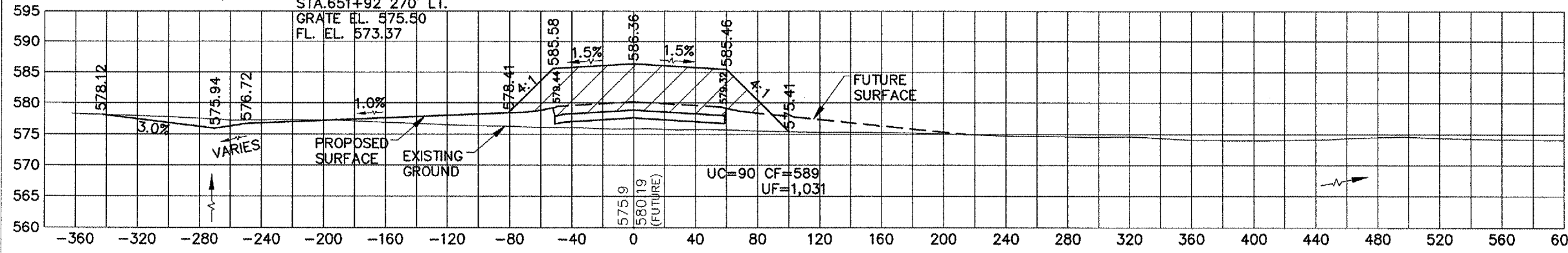
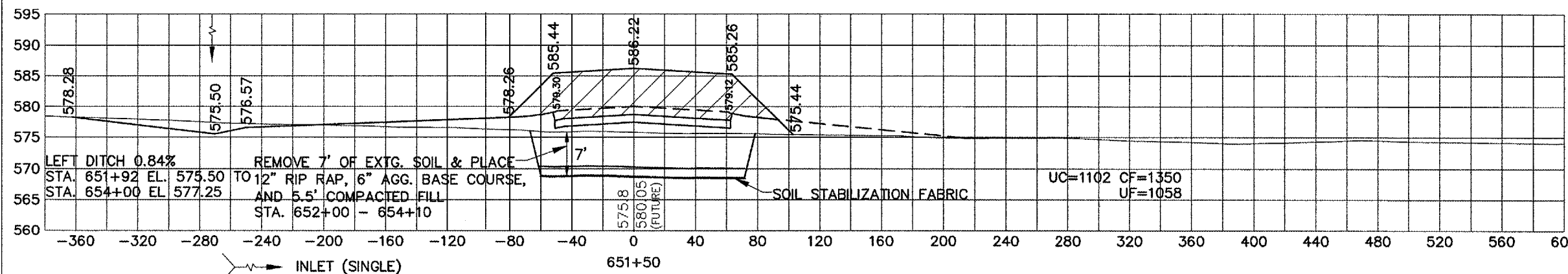
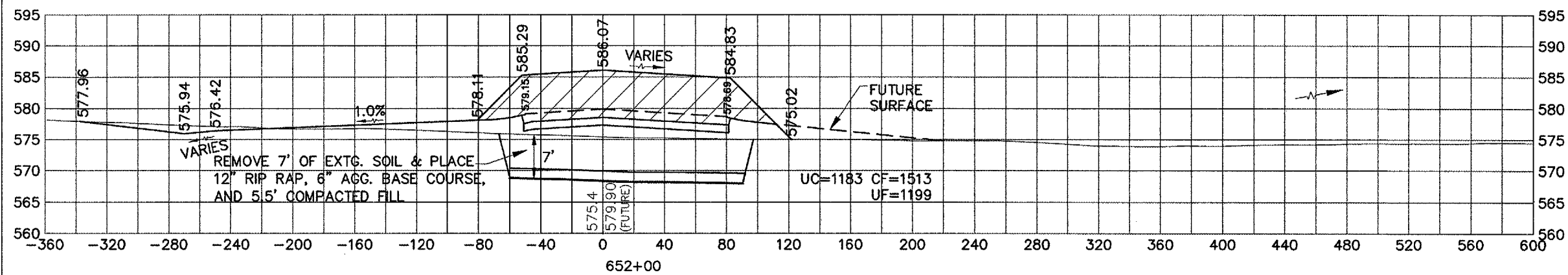
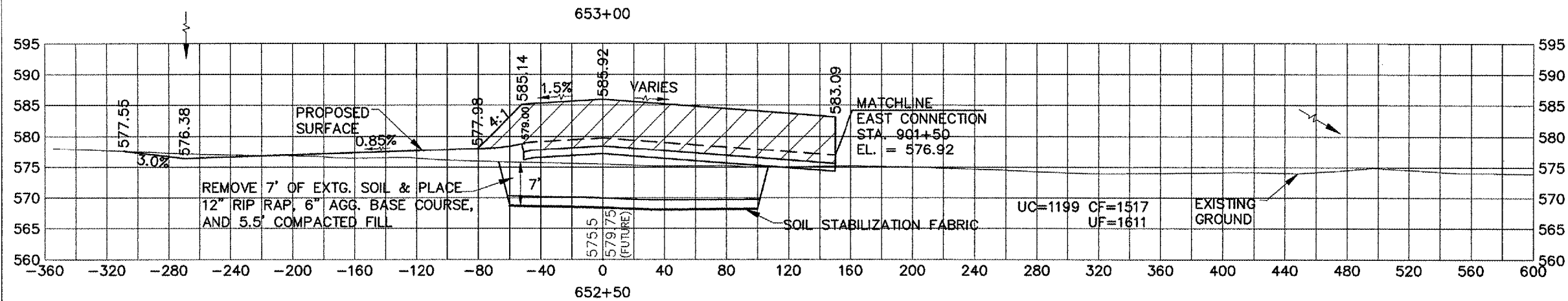


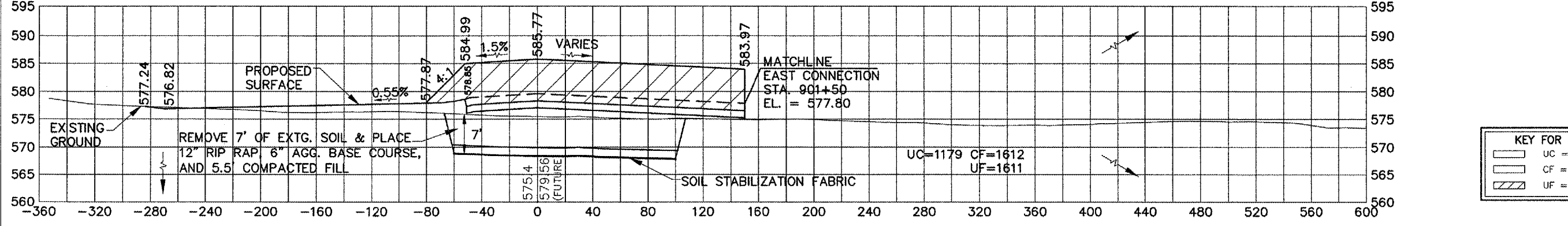
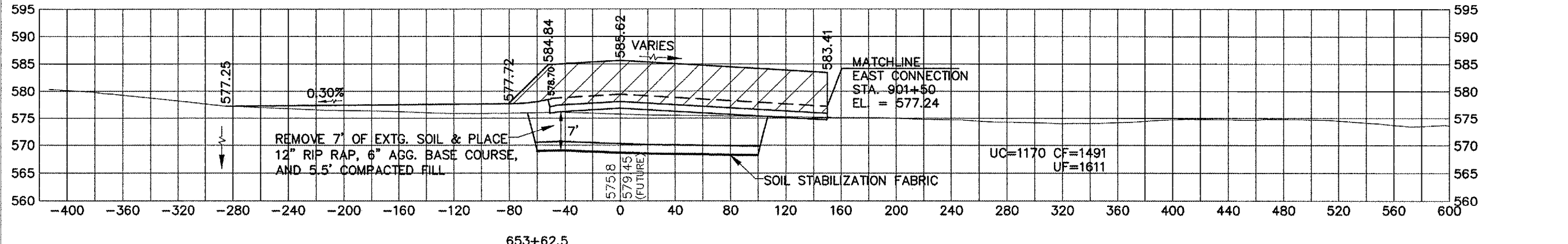
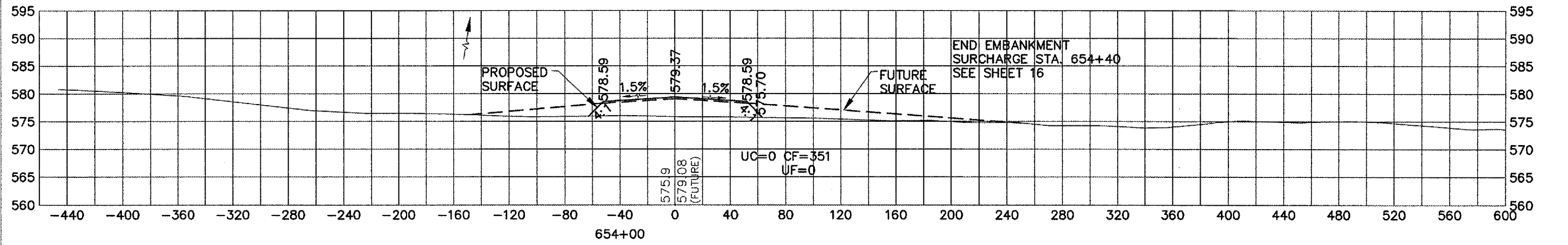
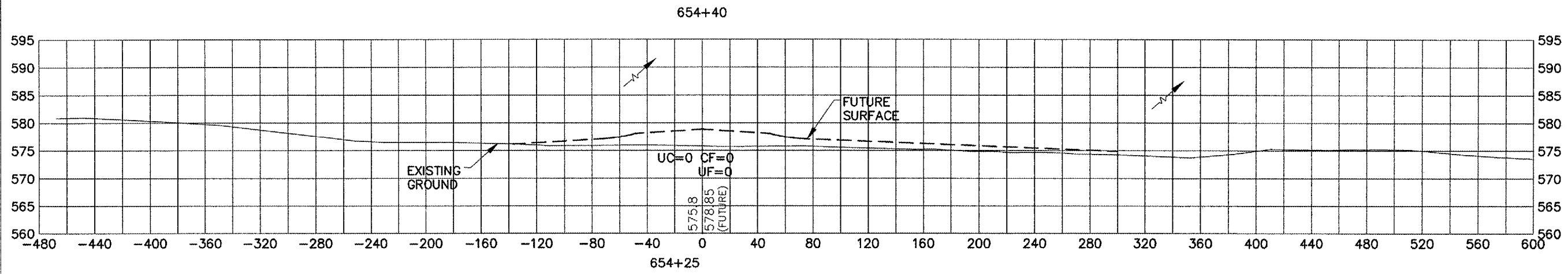
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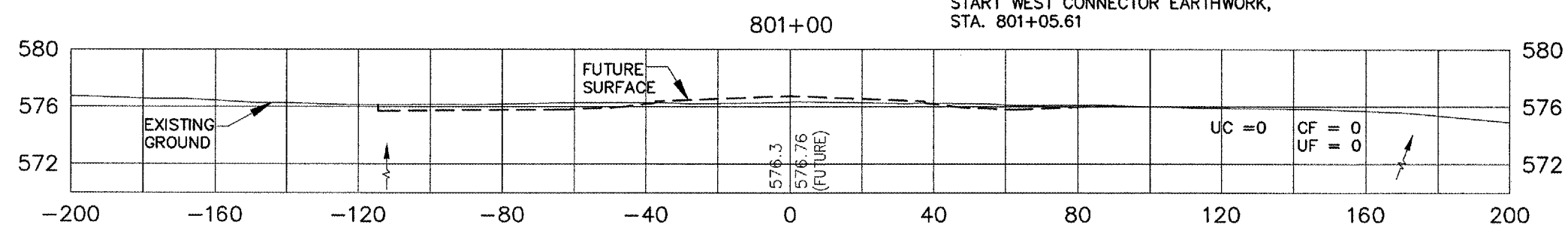
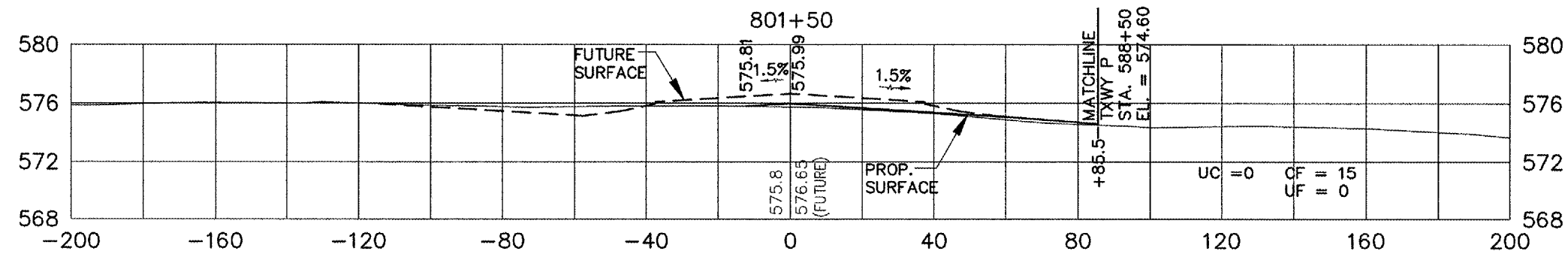
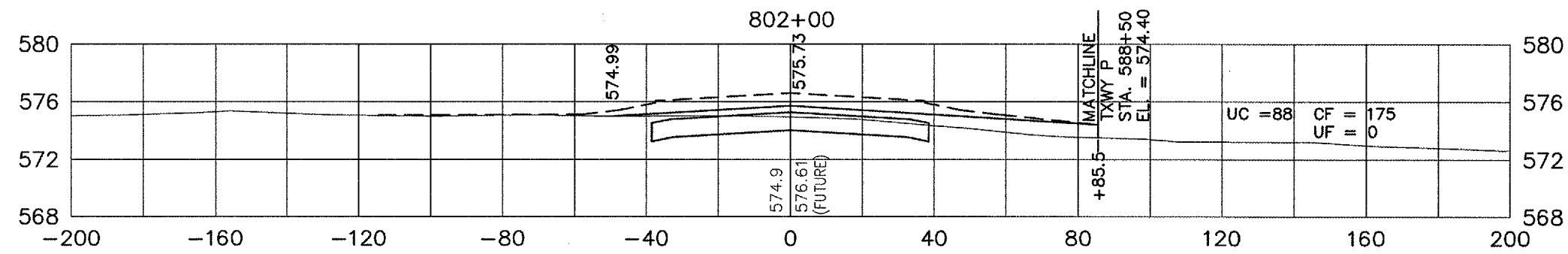
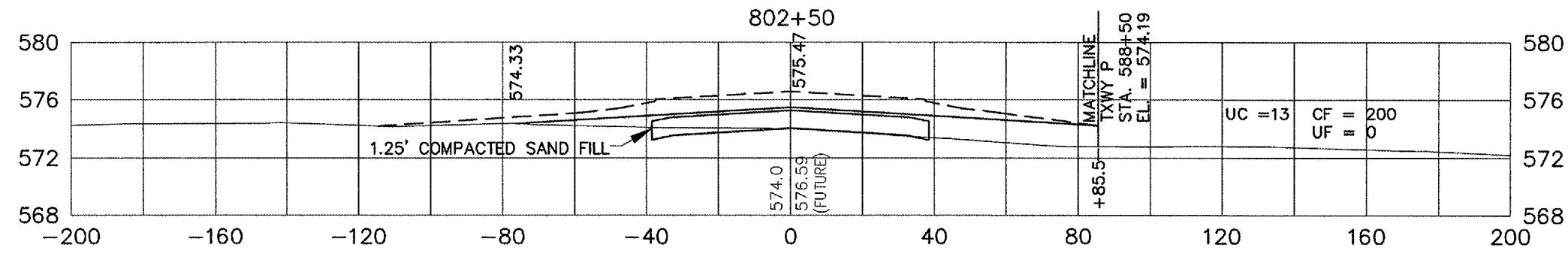
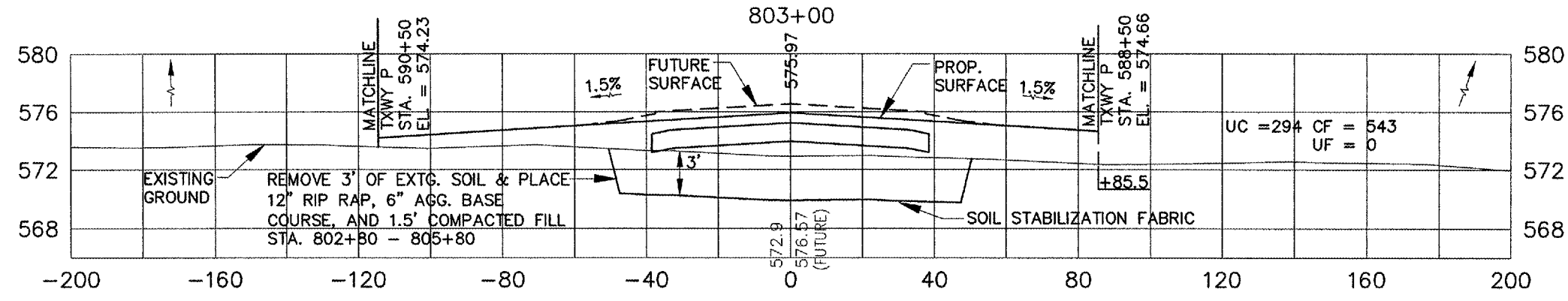
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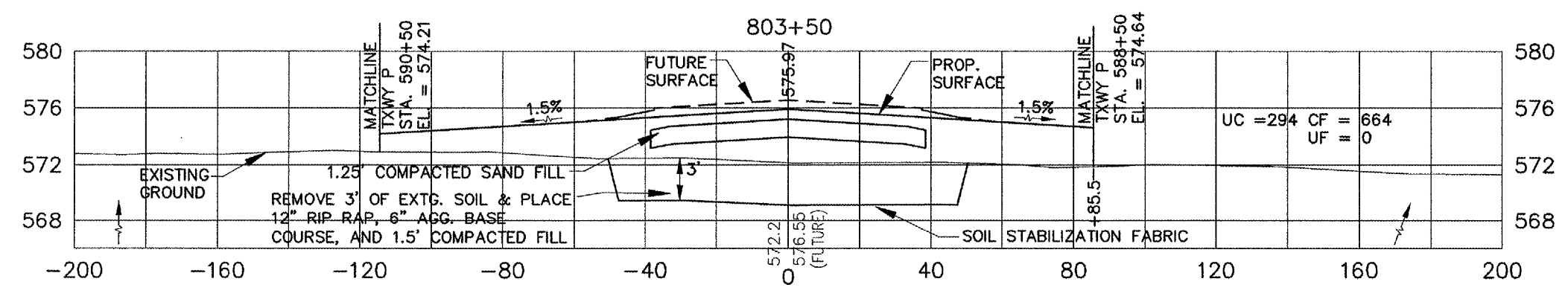
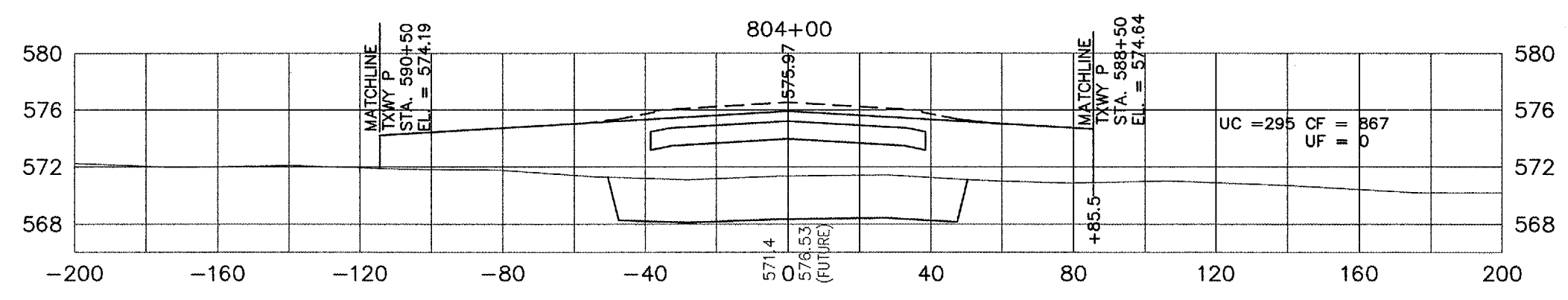
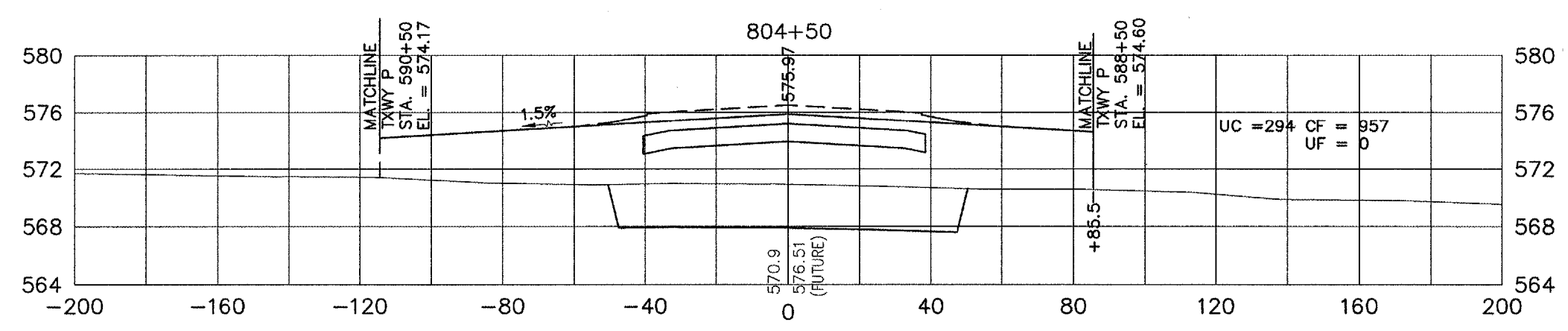
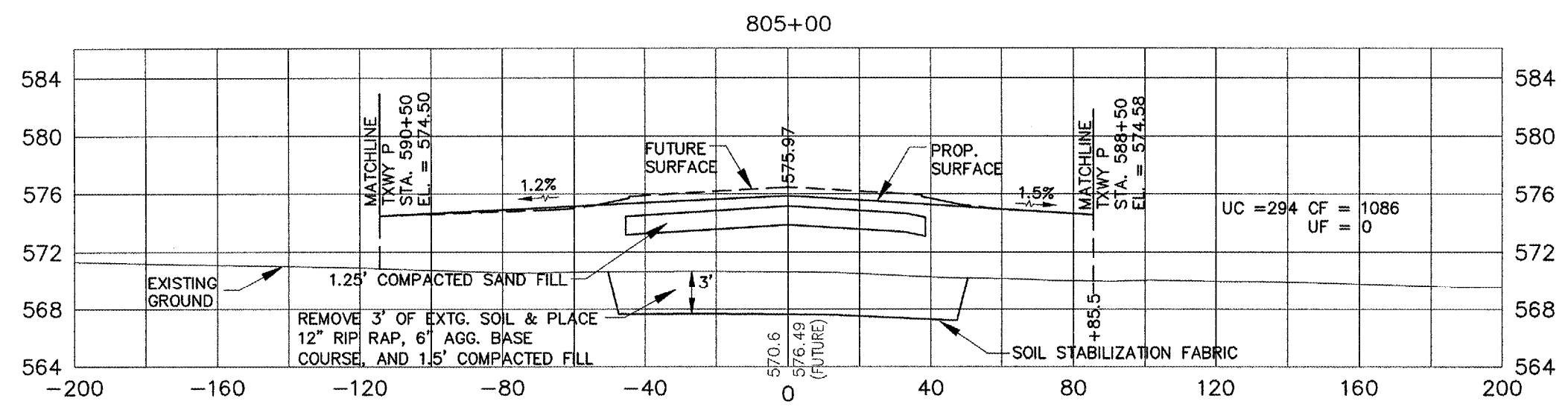
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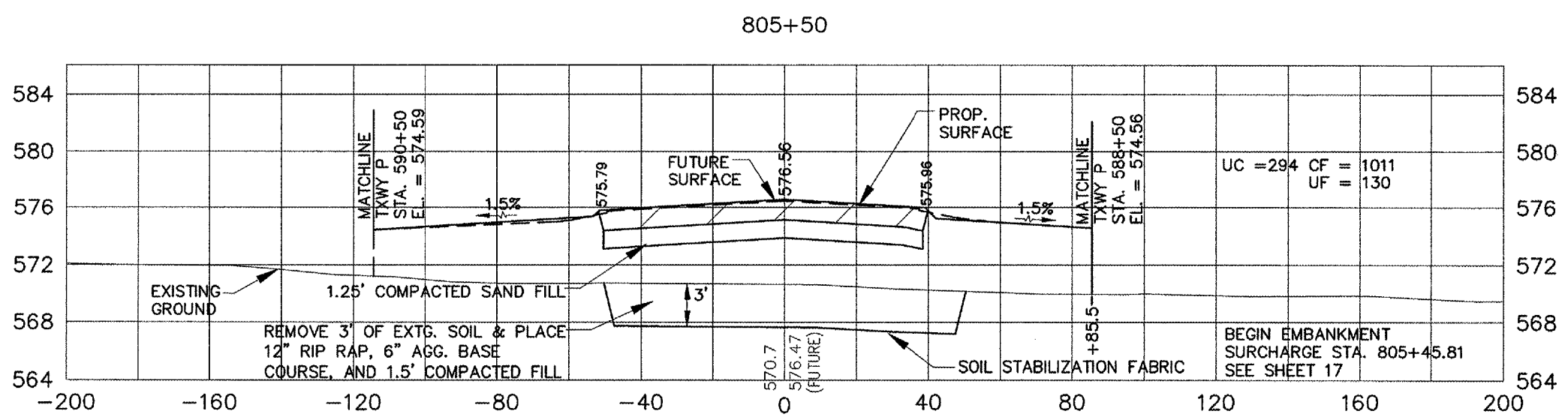
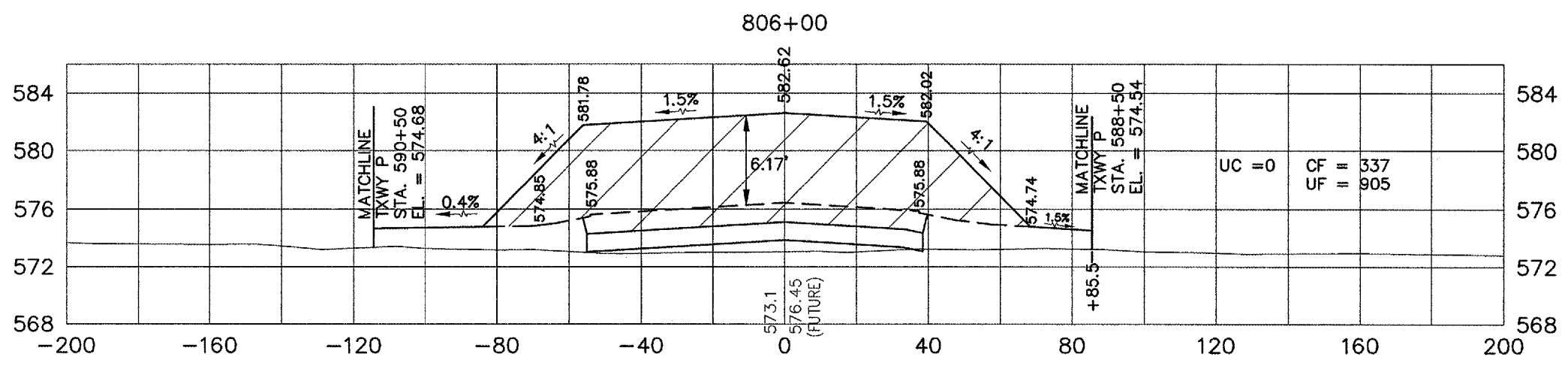
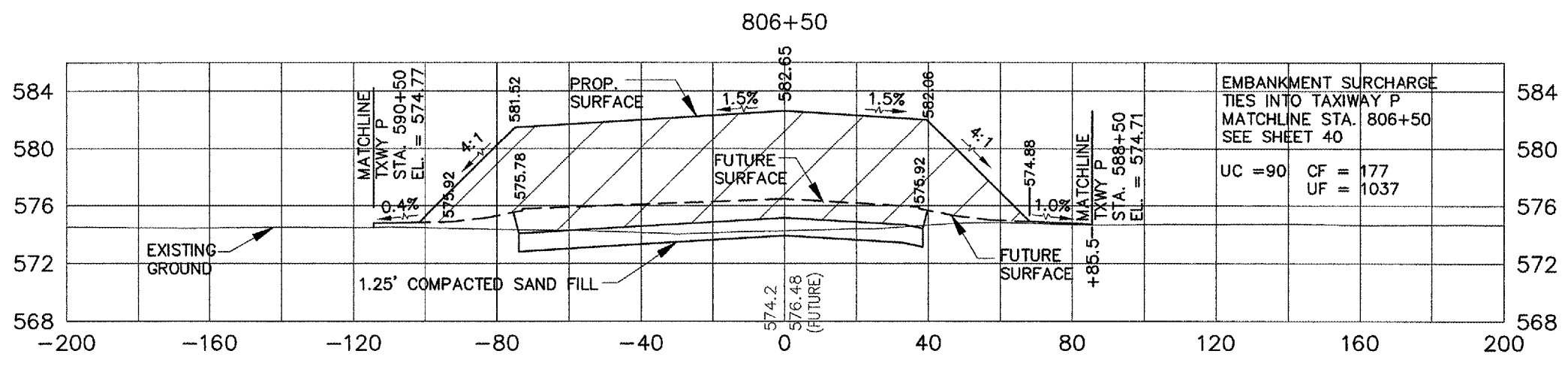
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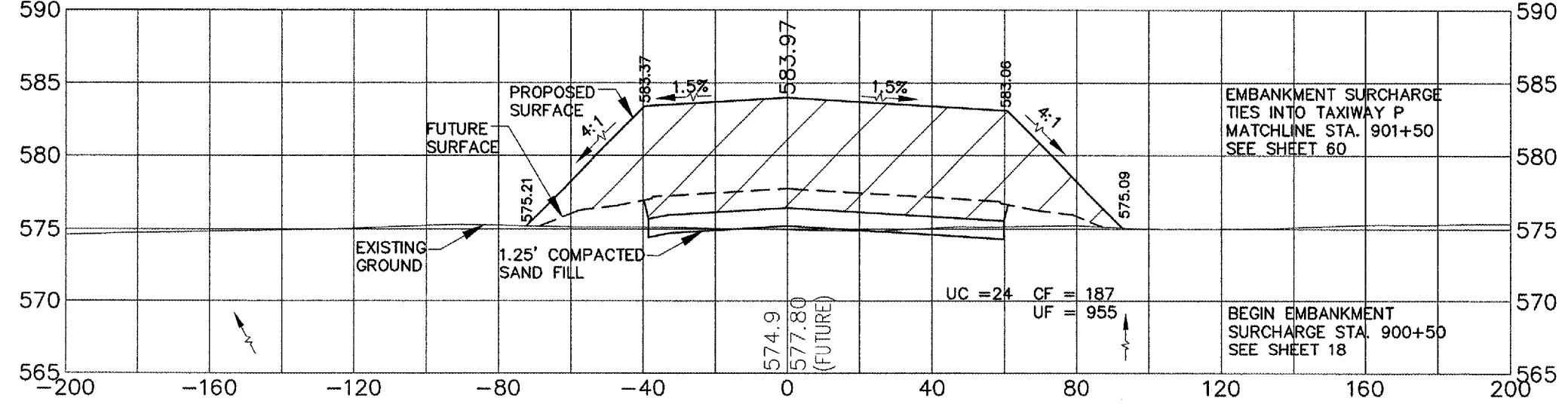
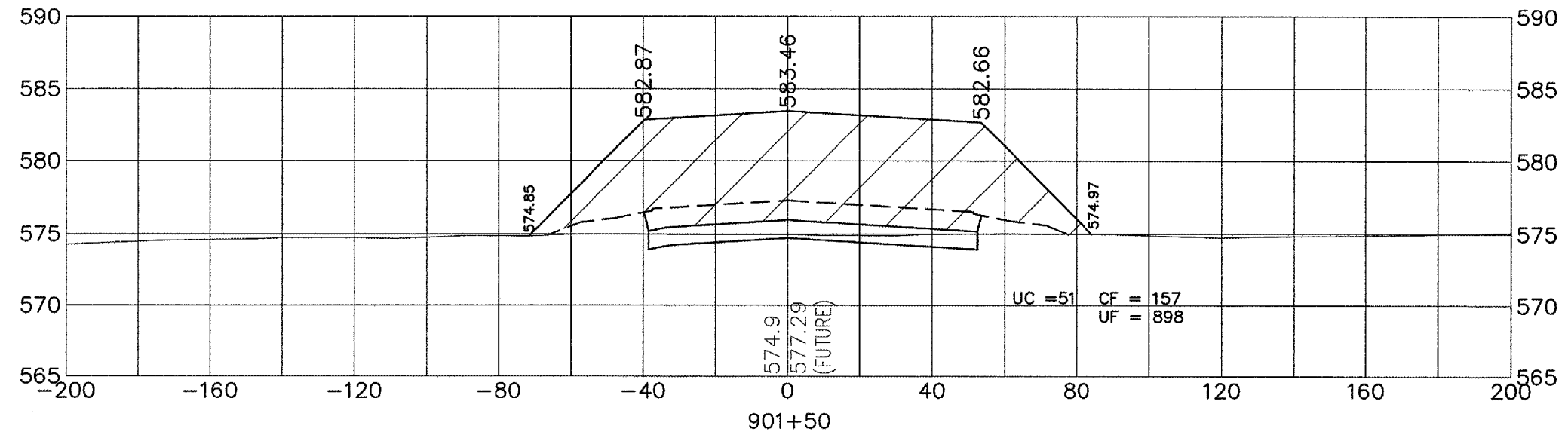
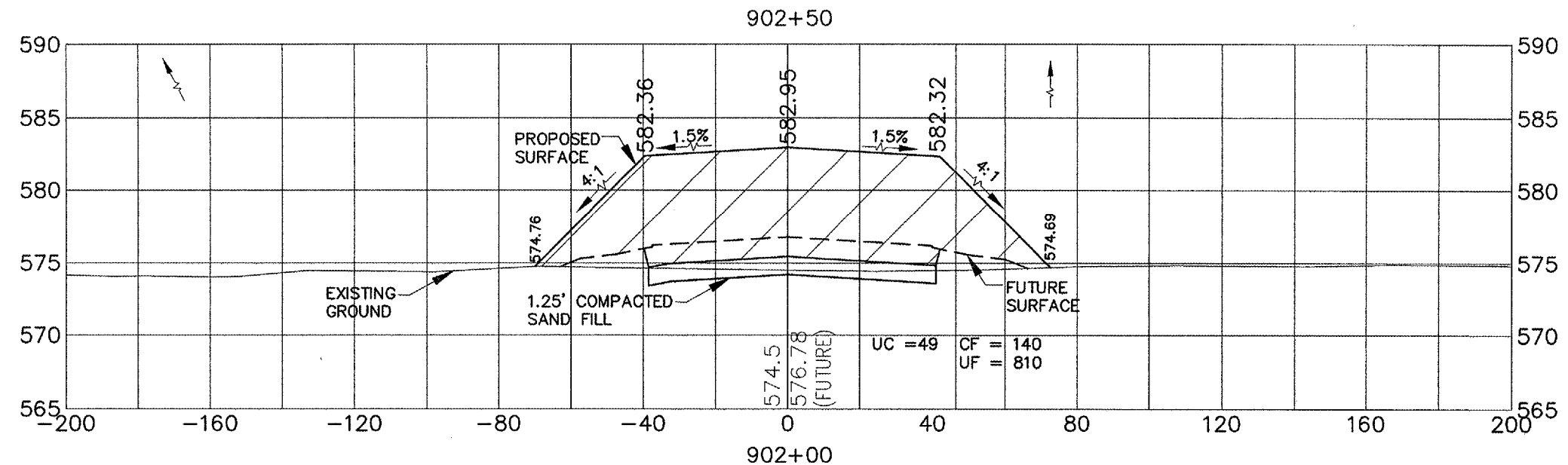
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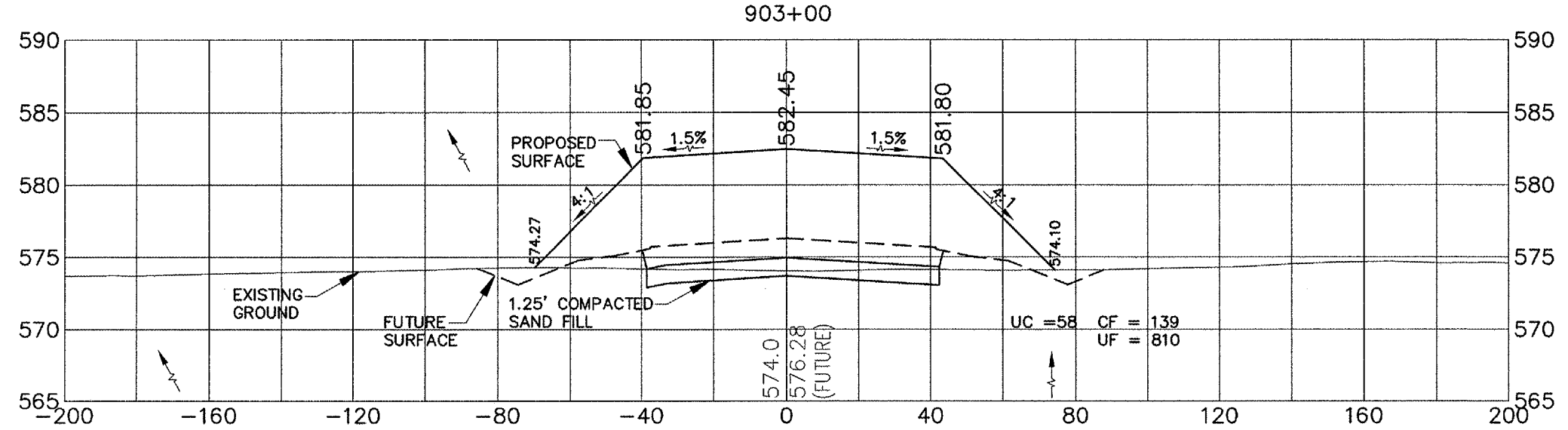
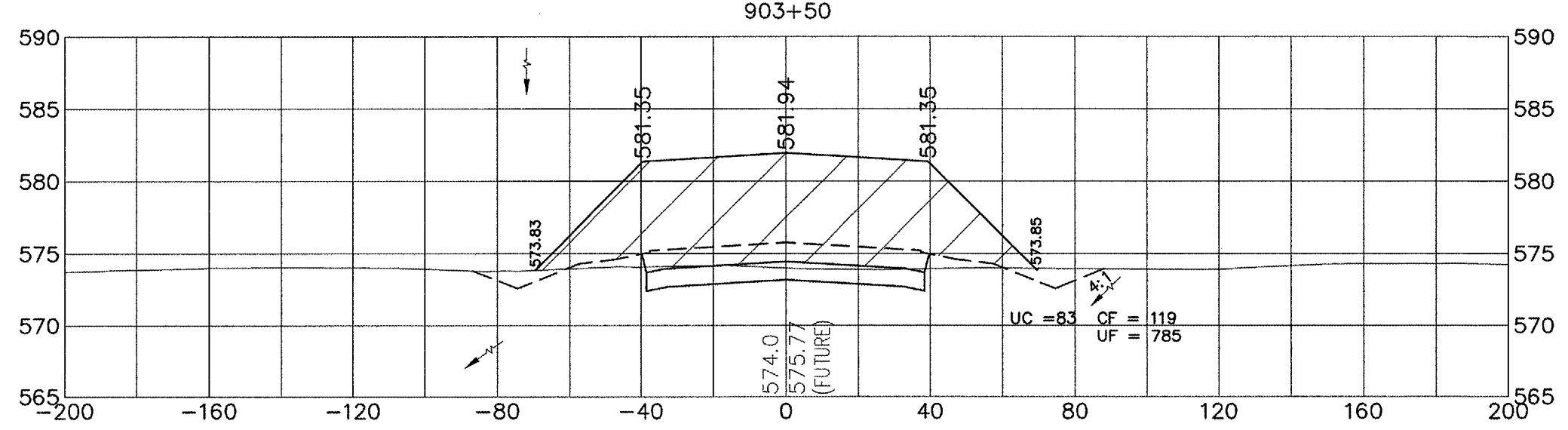
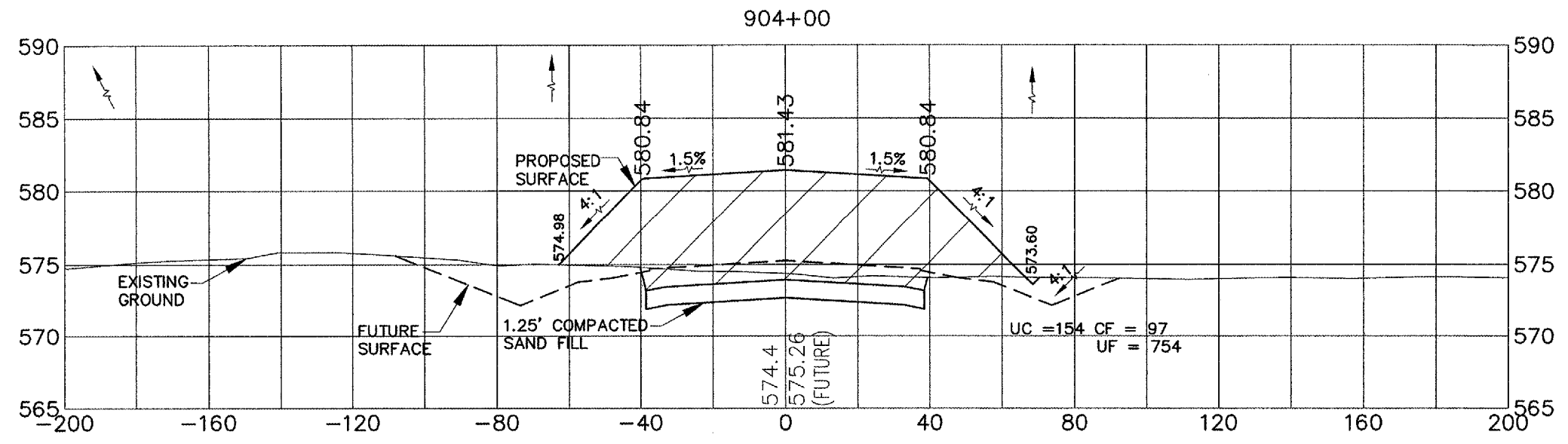
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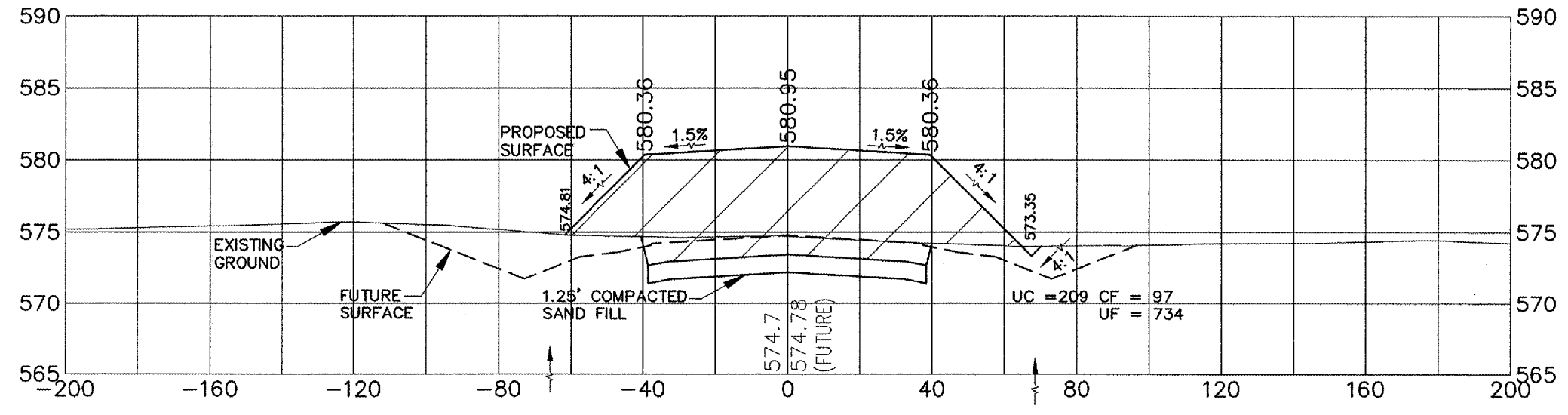
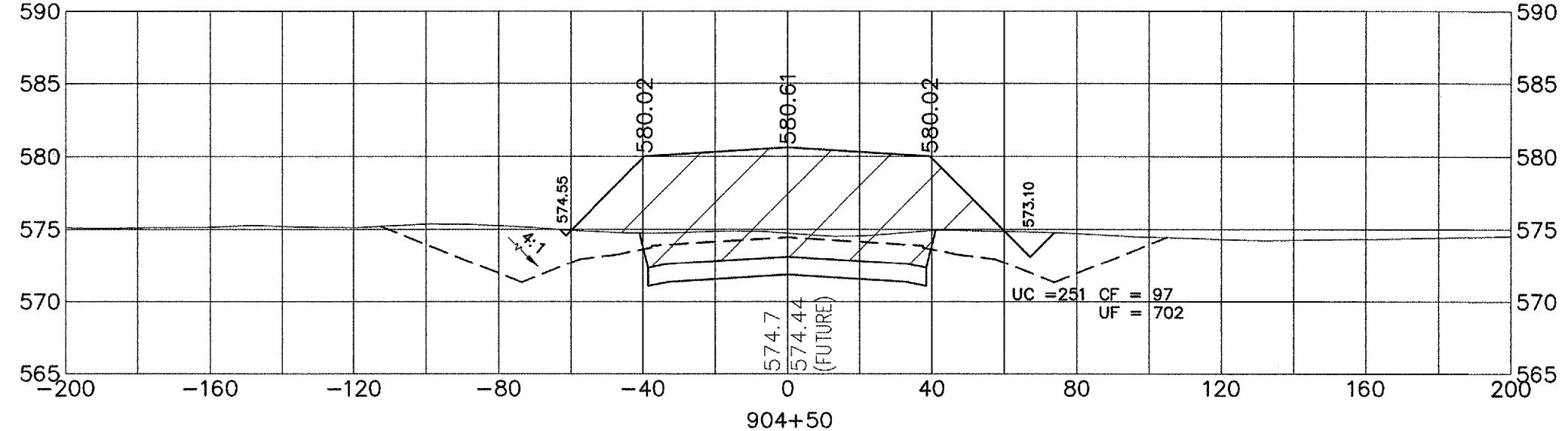
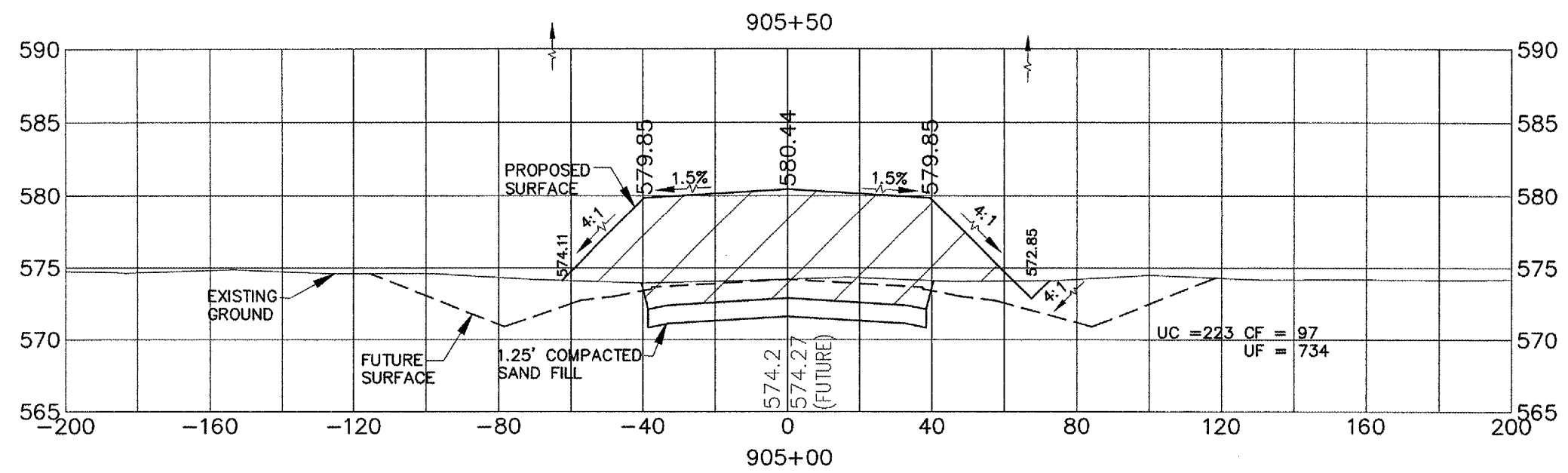
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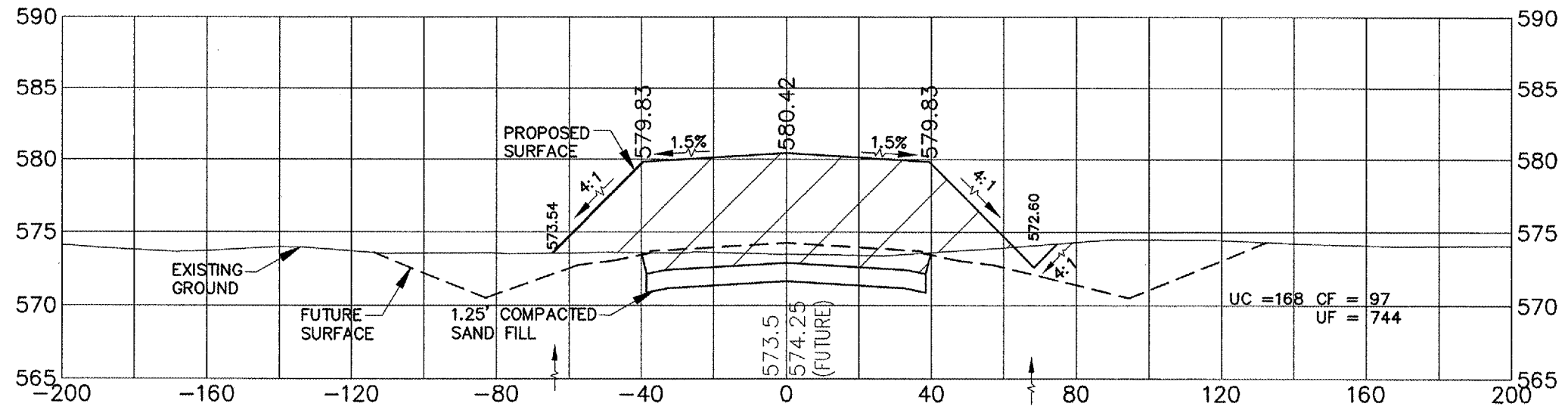
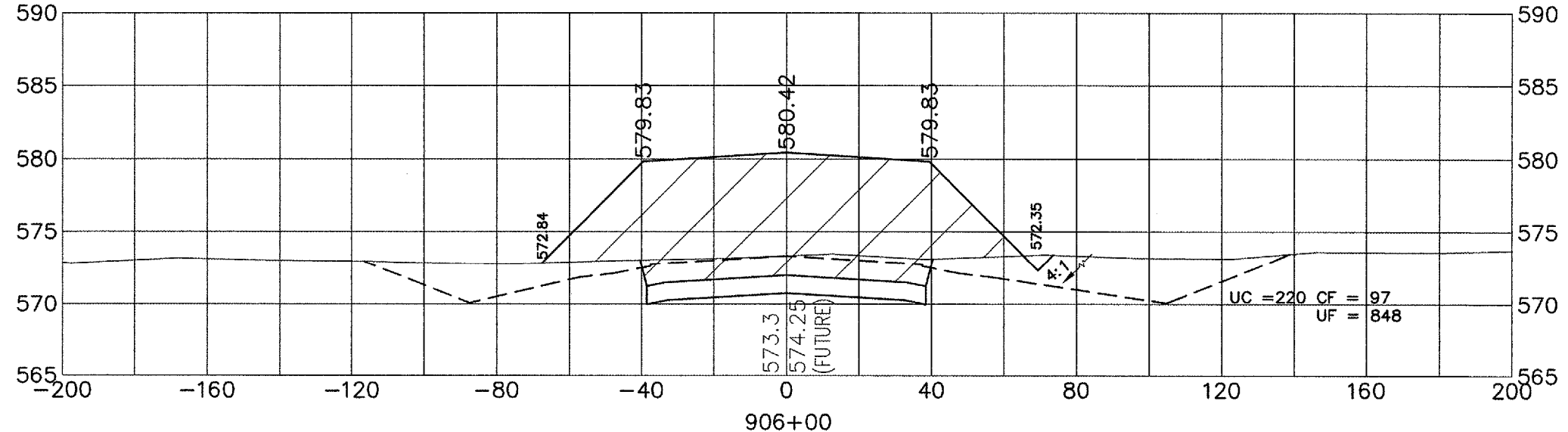
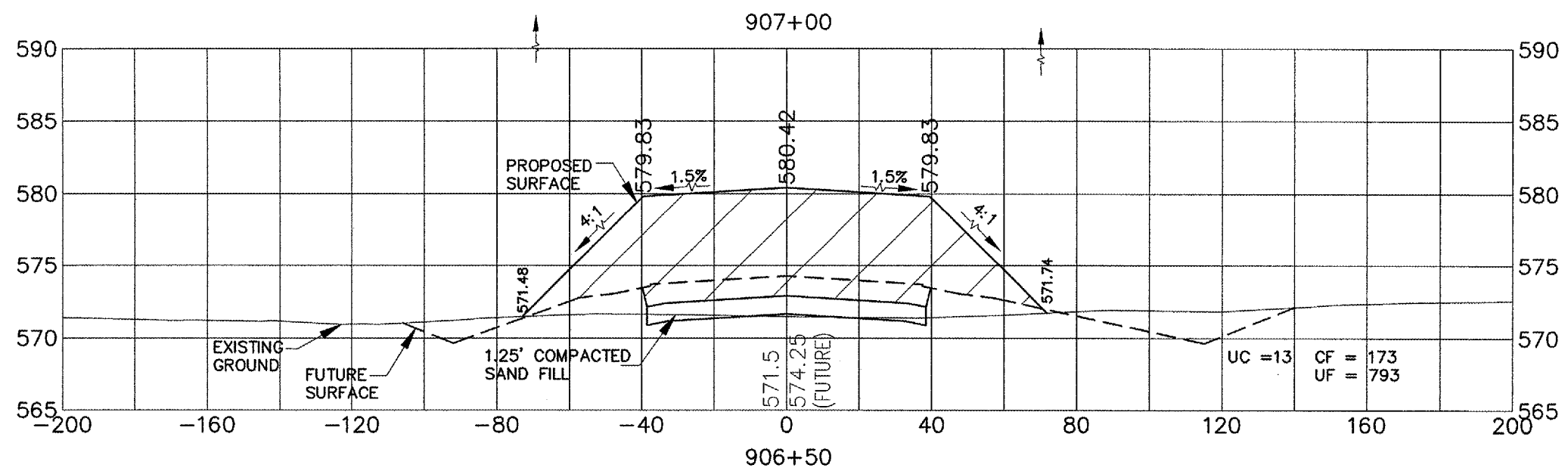
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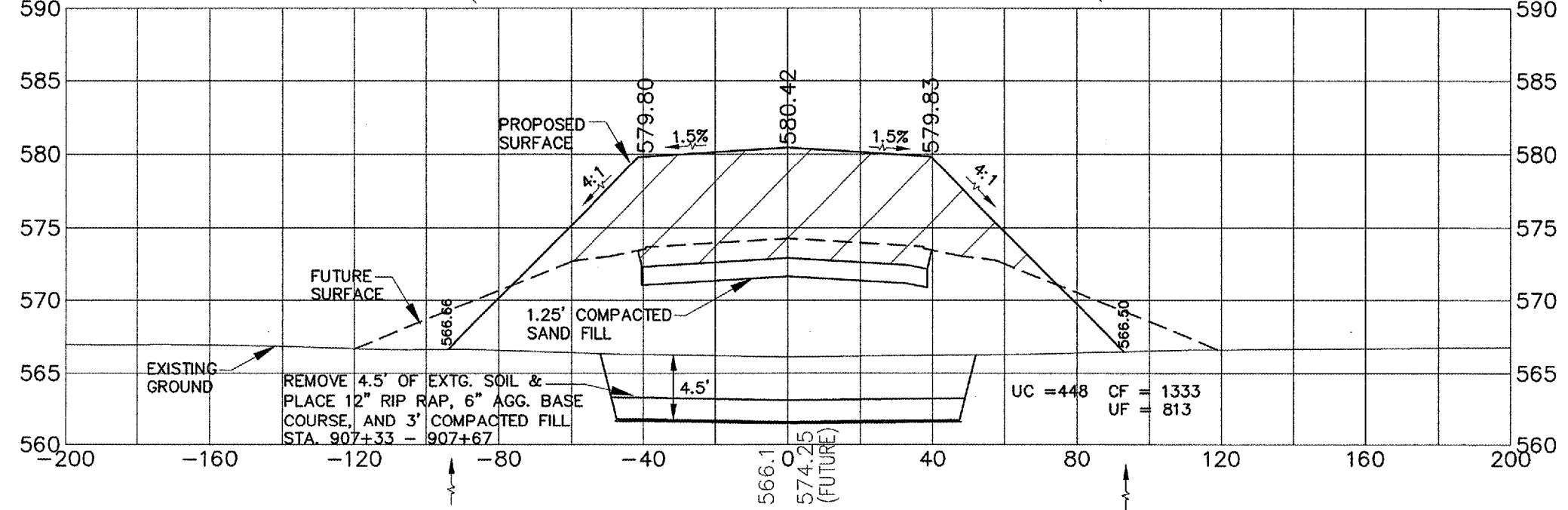
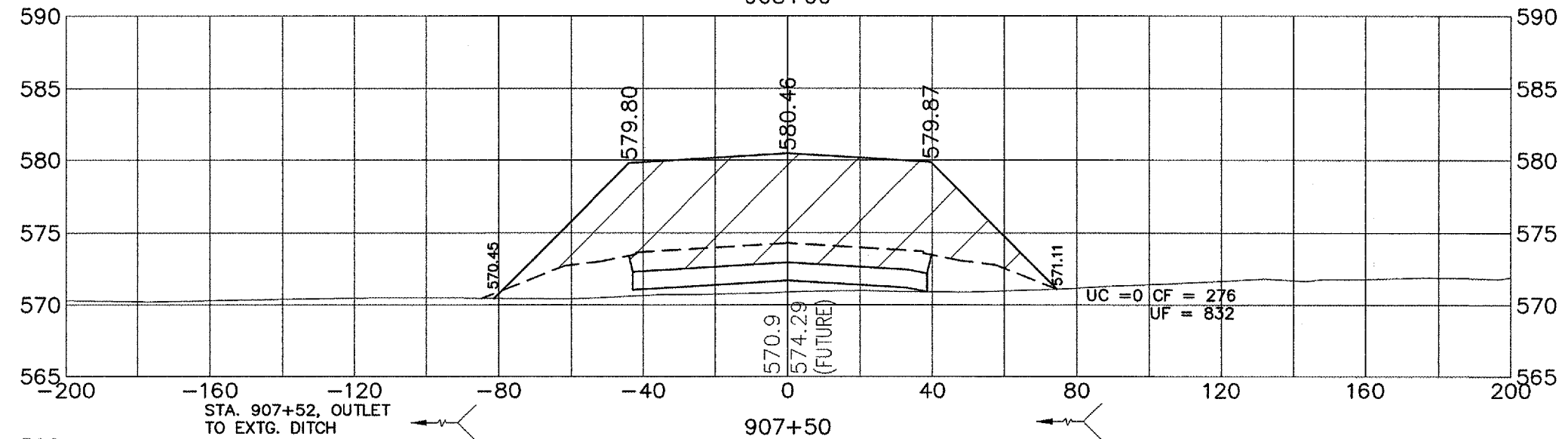
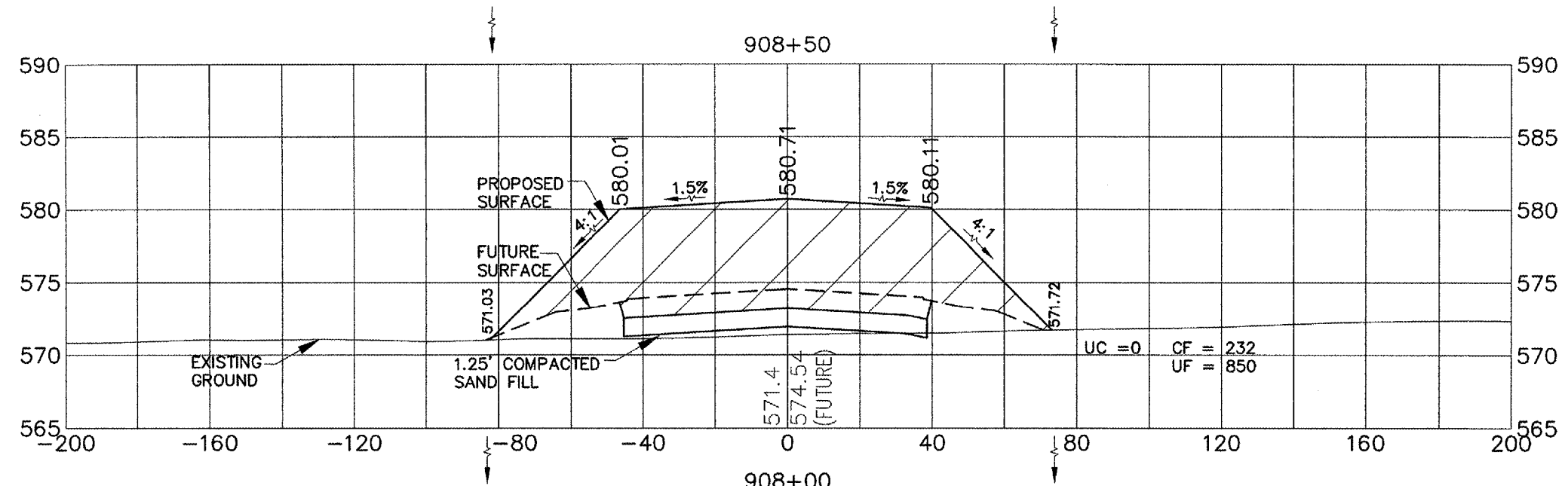
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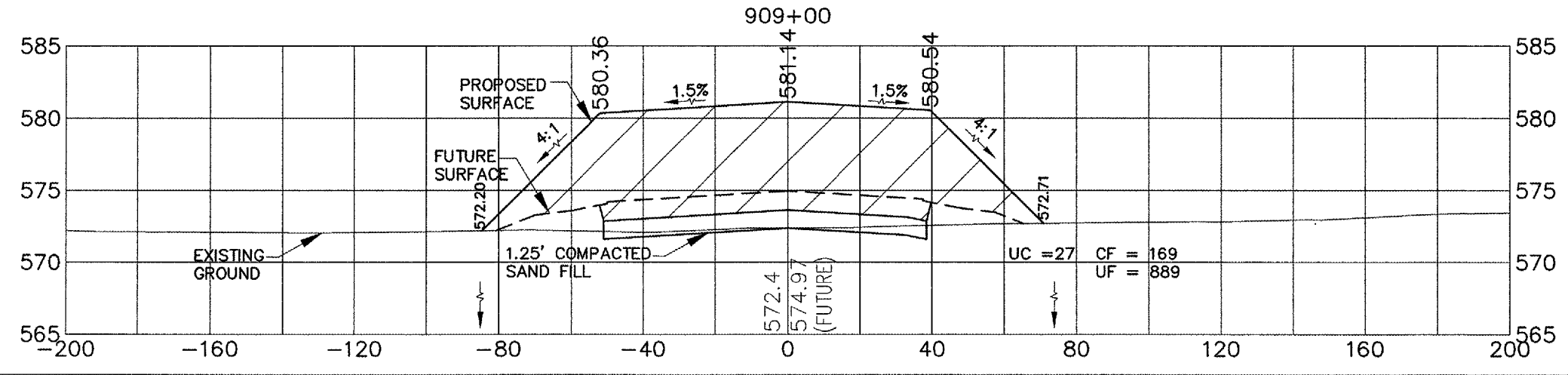
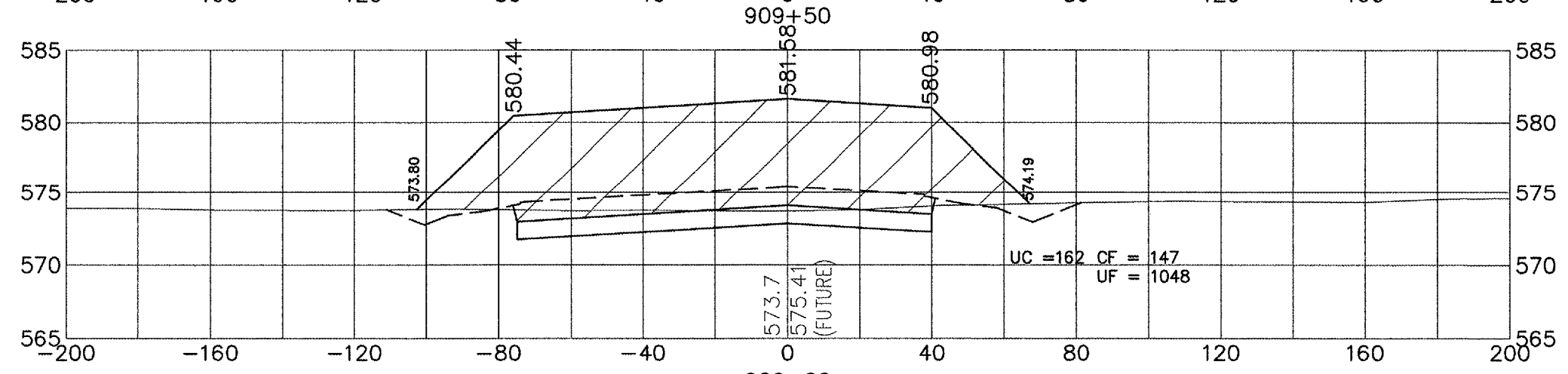
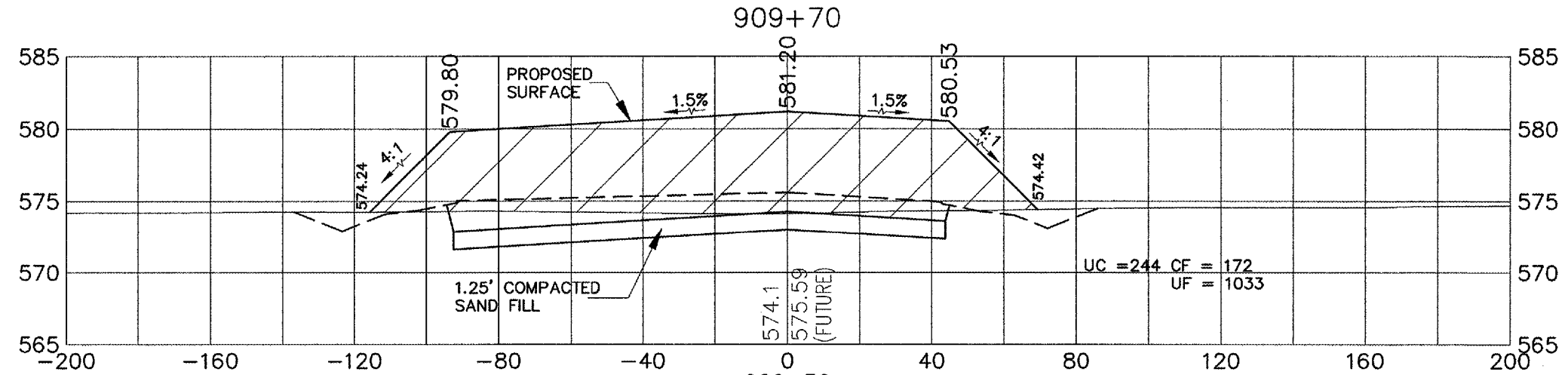
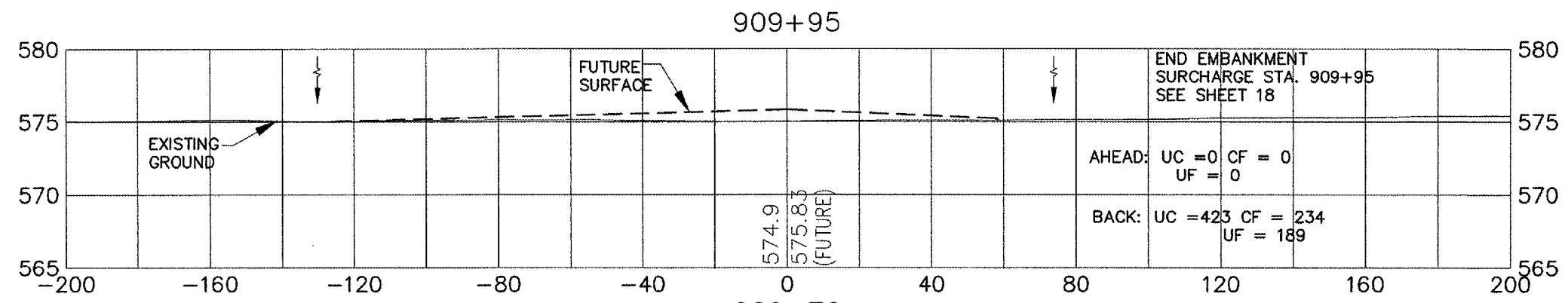
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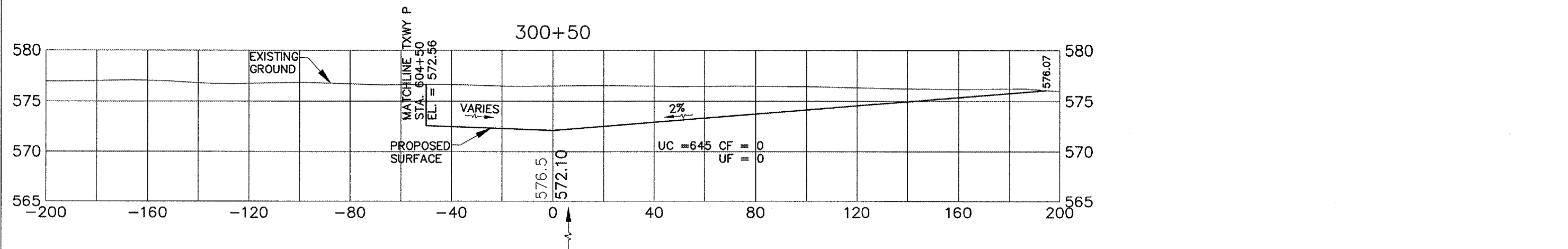
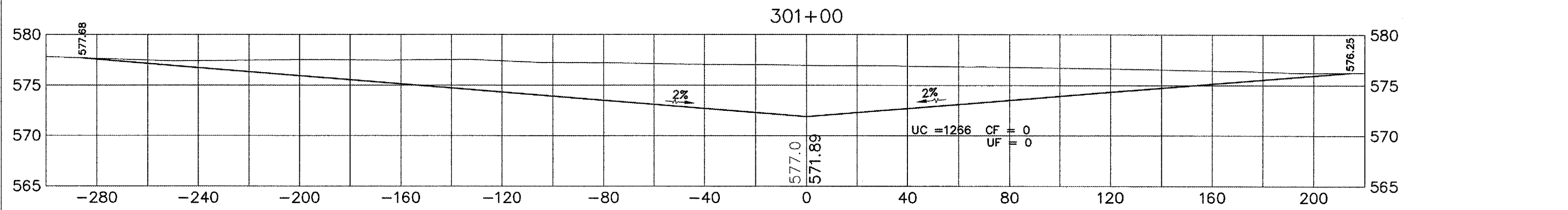
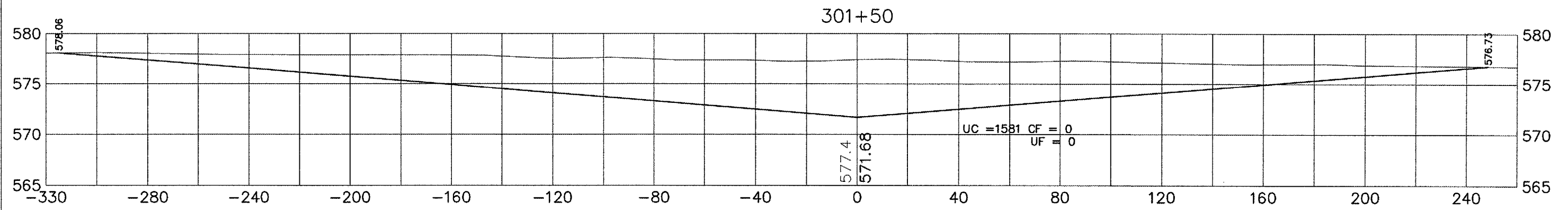
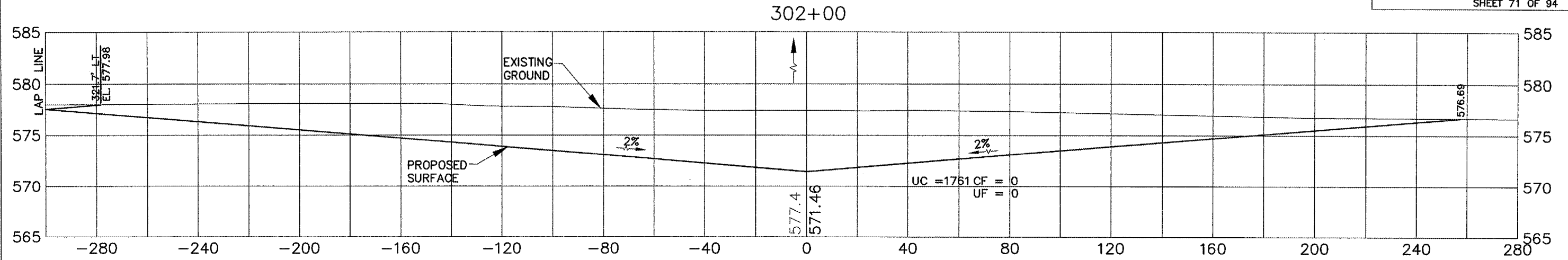
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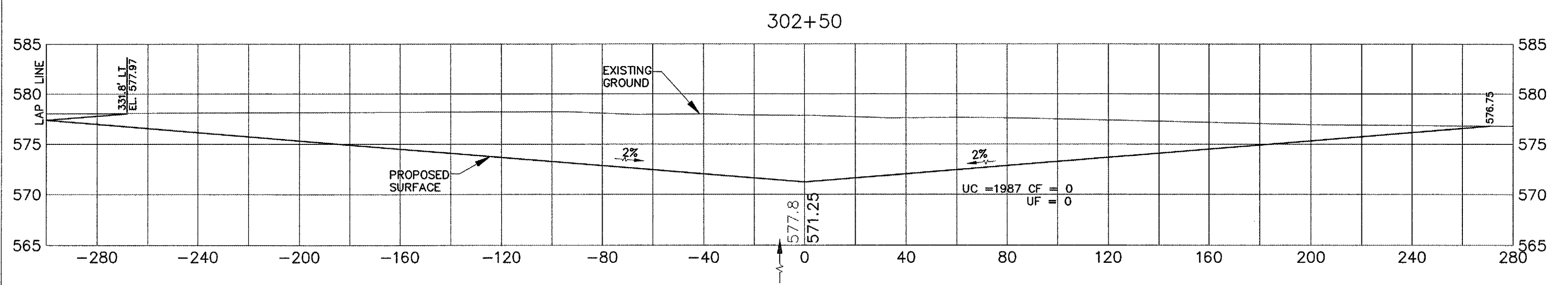
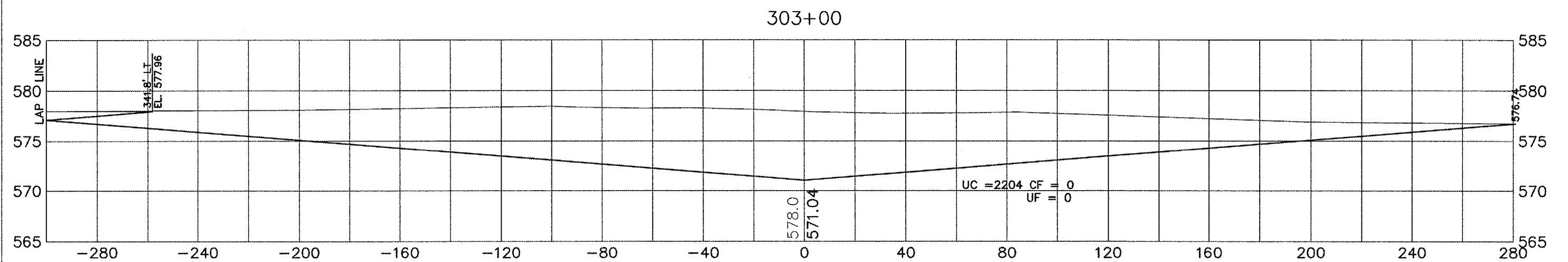
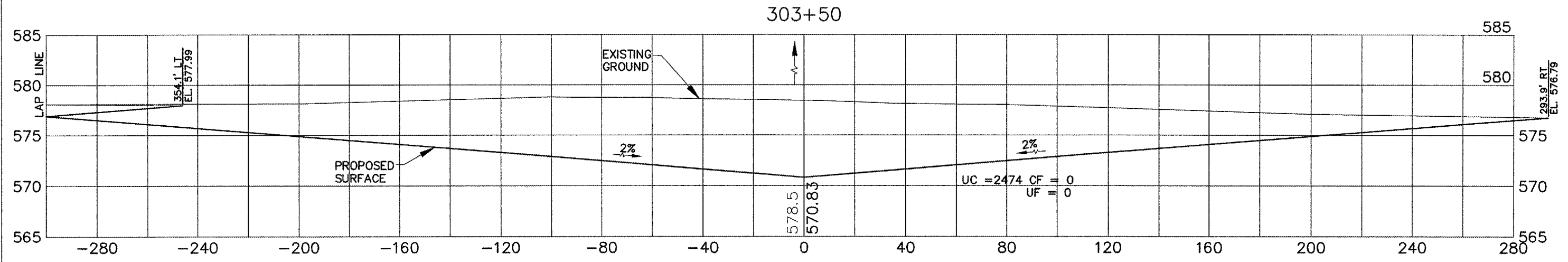
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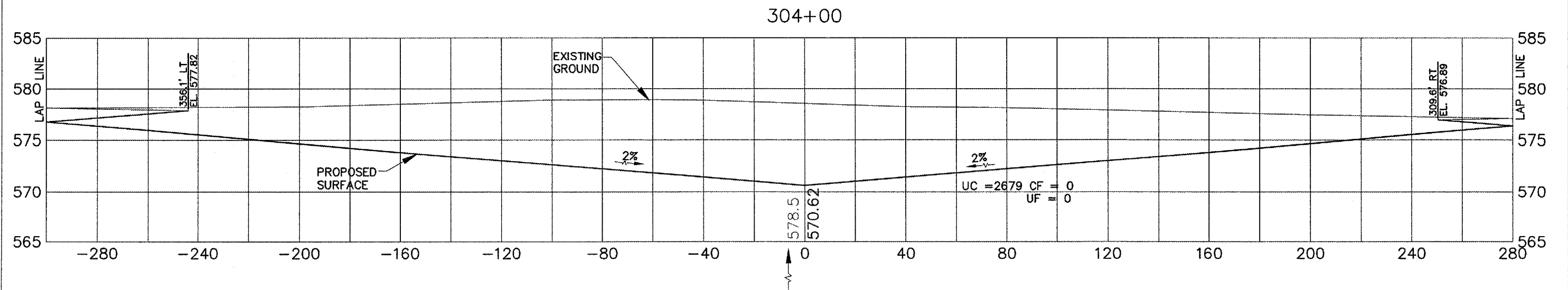
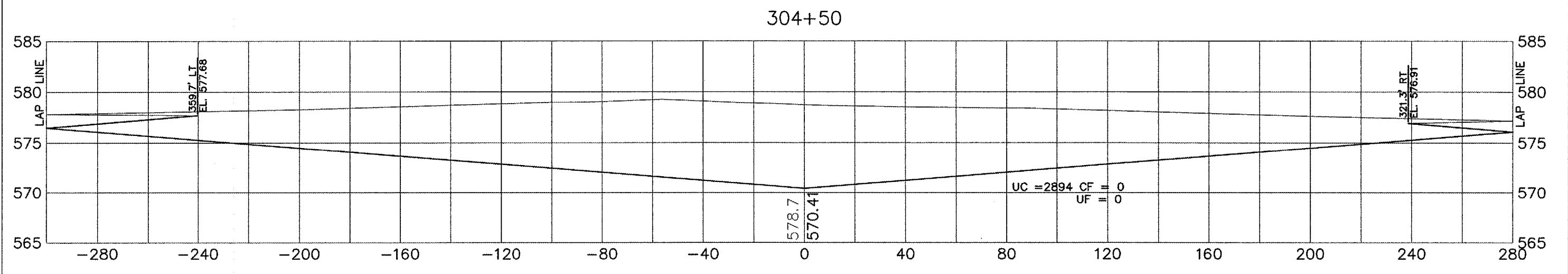
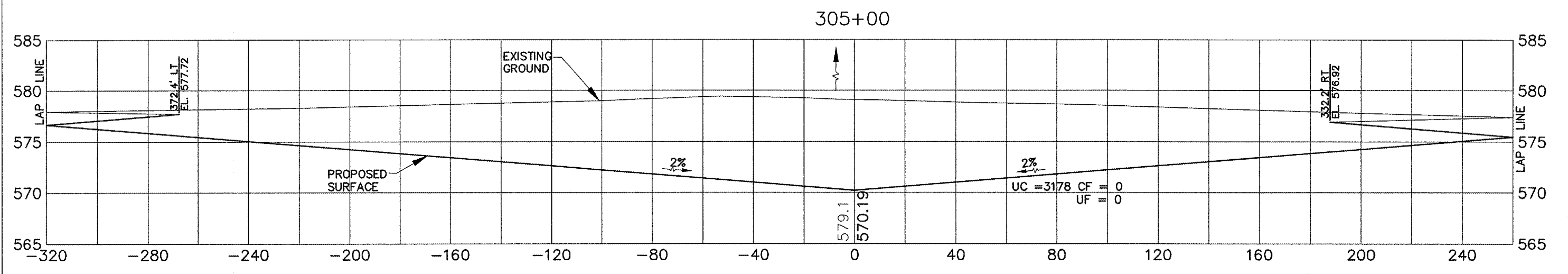
KEY FOR EARTHWORK	
	UC = UNCLASSIFIED CUT
	CF = COMPACTED FILL
	UF = UNCOMPACTED FILL

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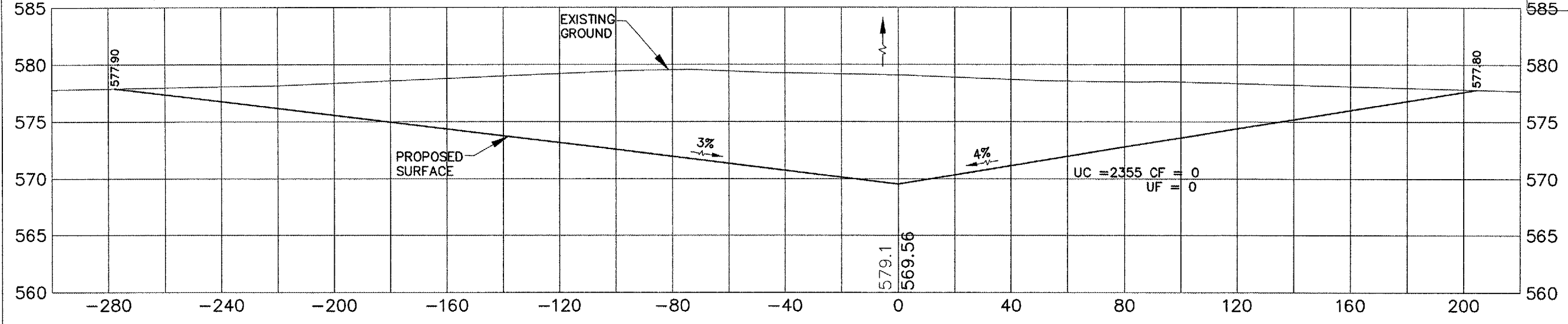


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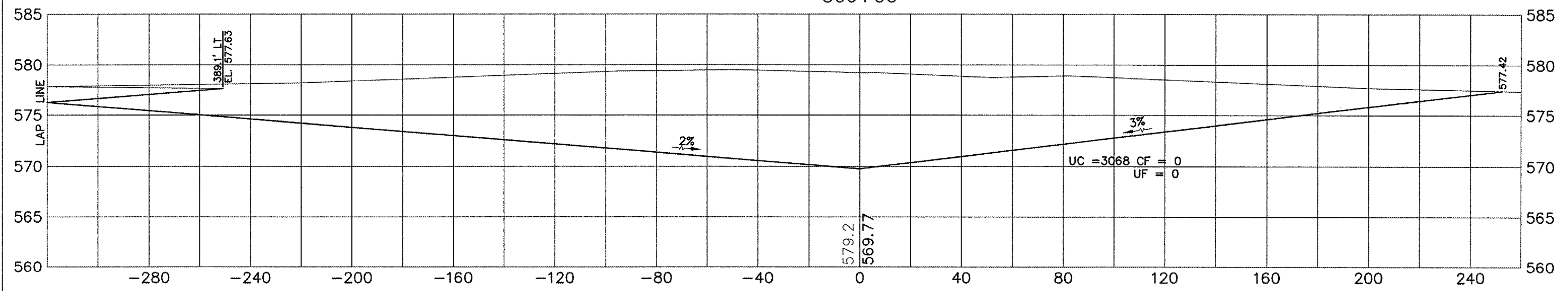


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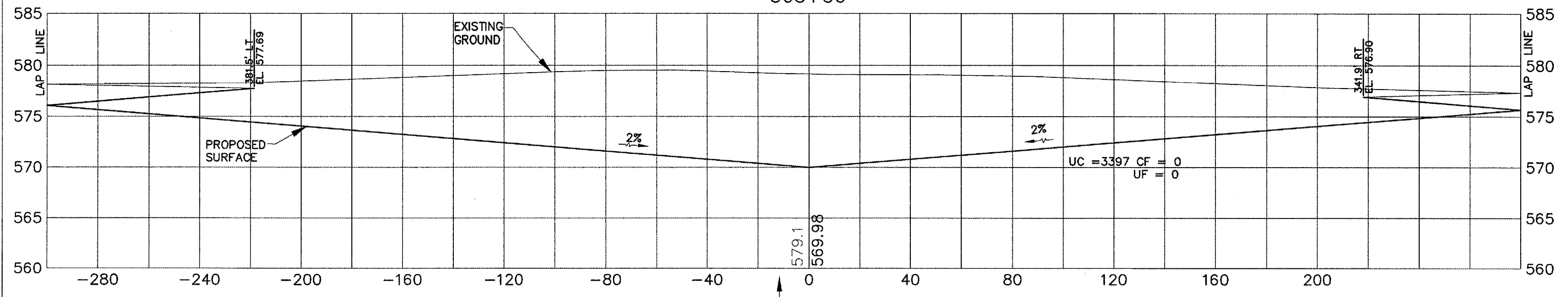
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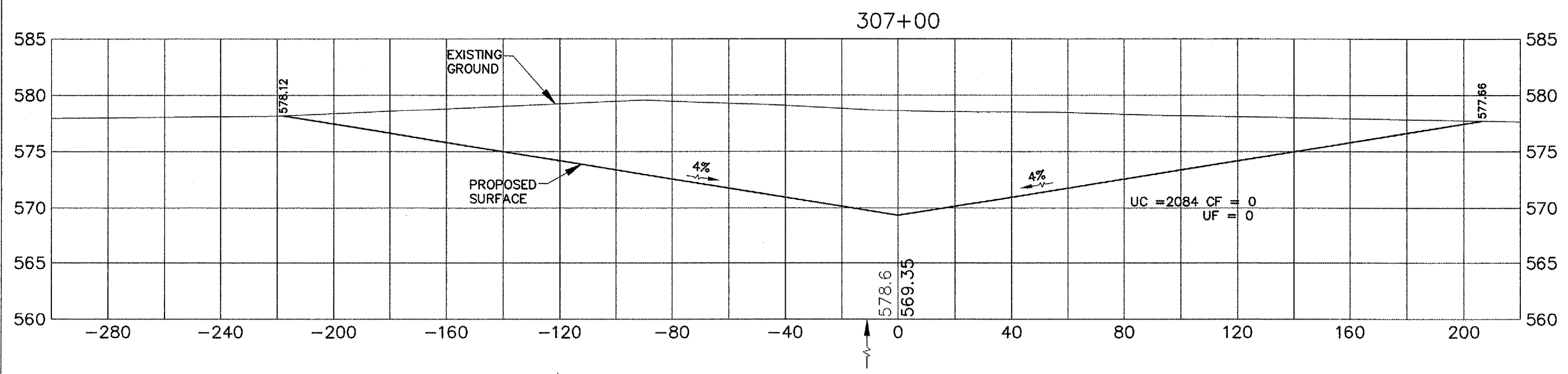
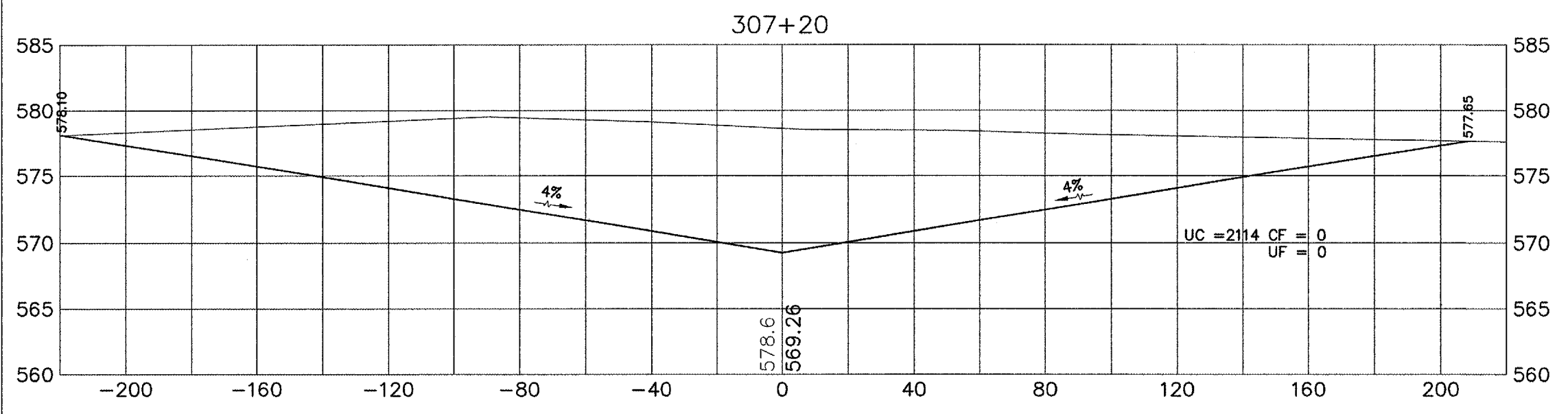
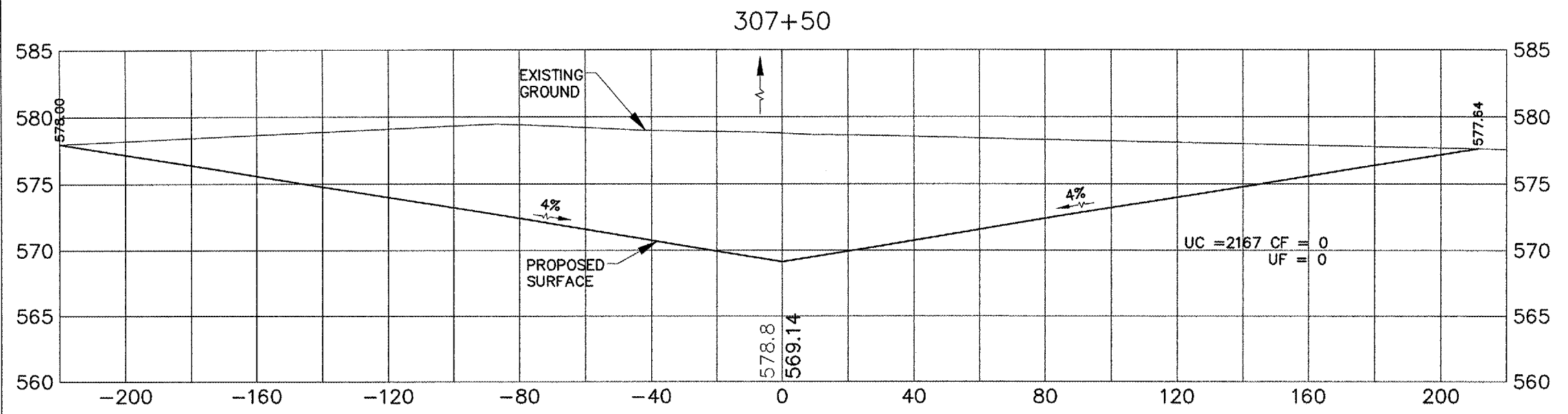
306+00



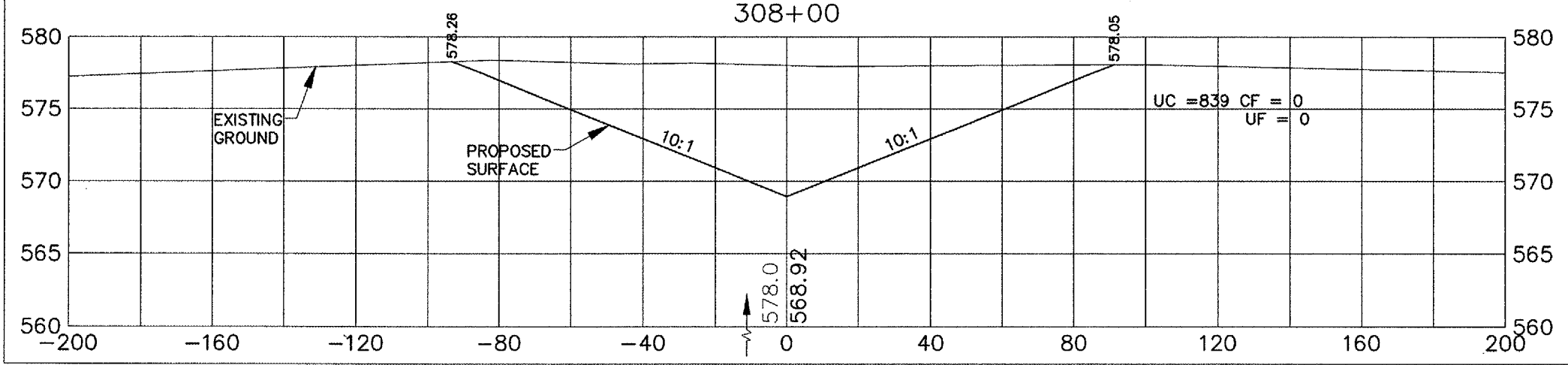
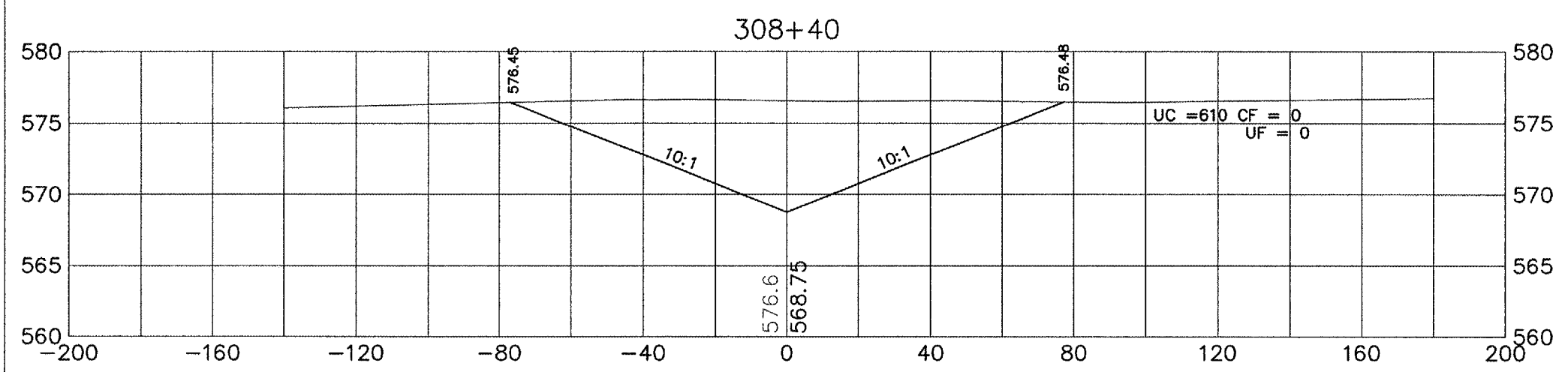
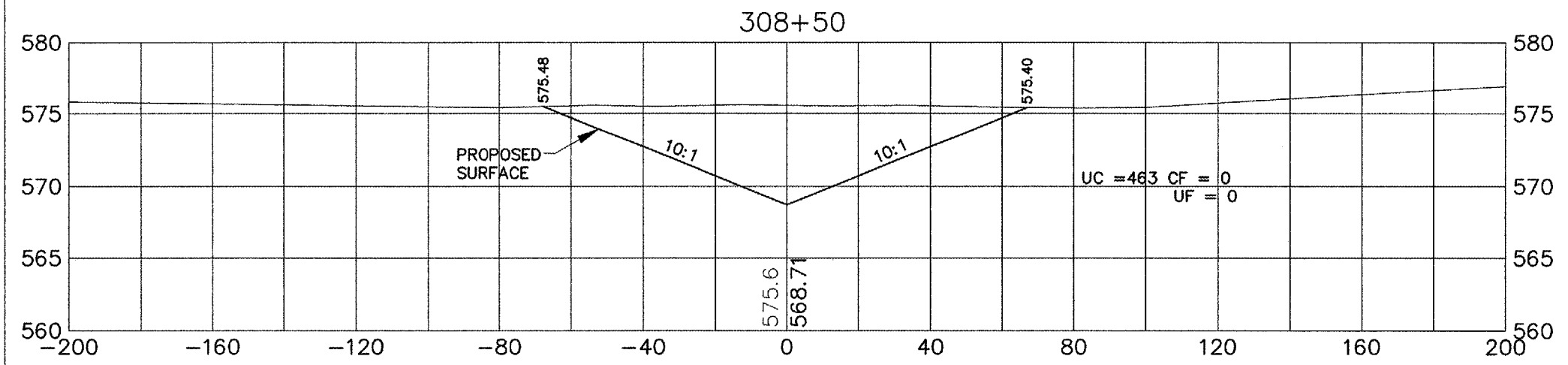
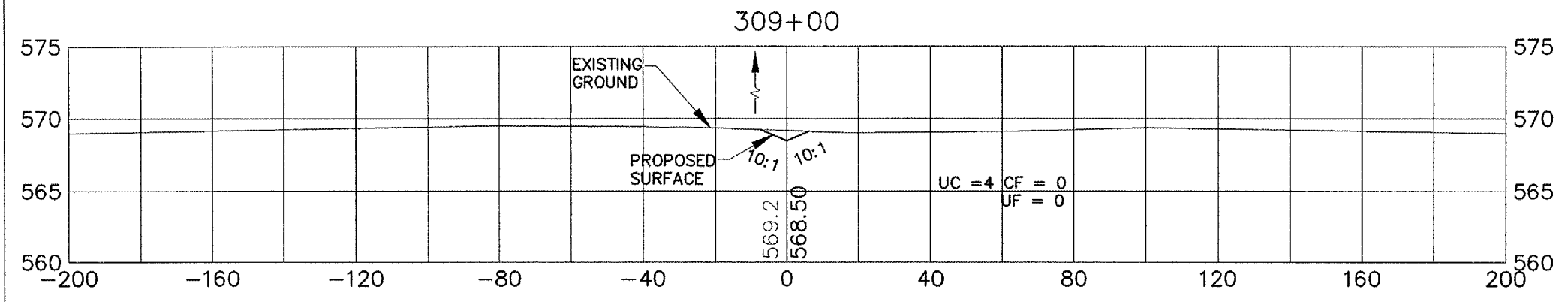
305+50



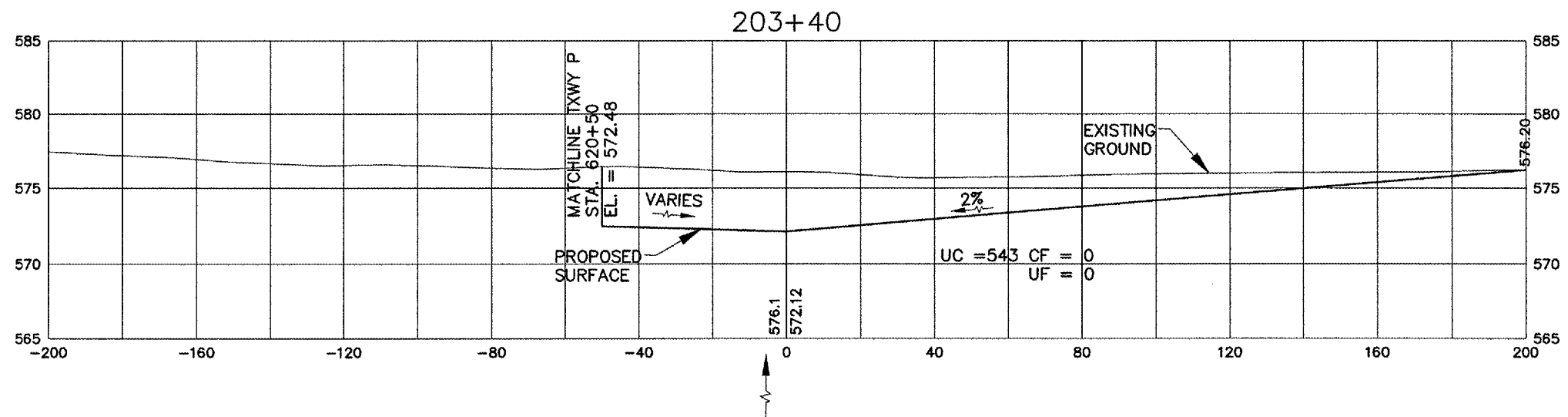
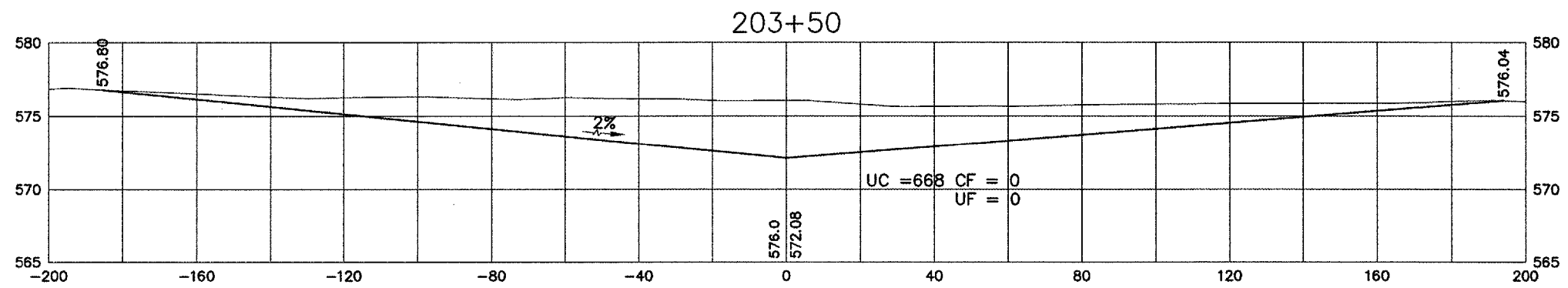
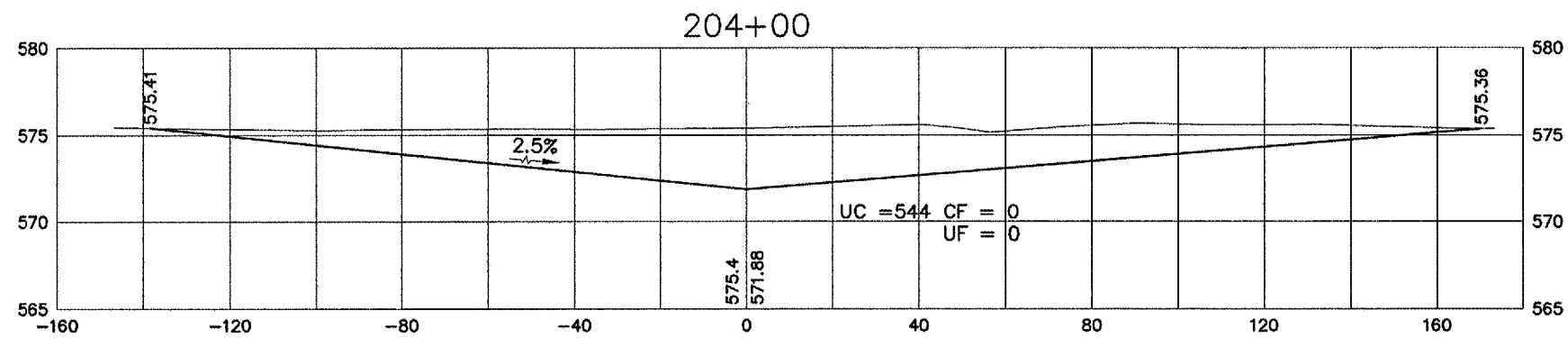
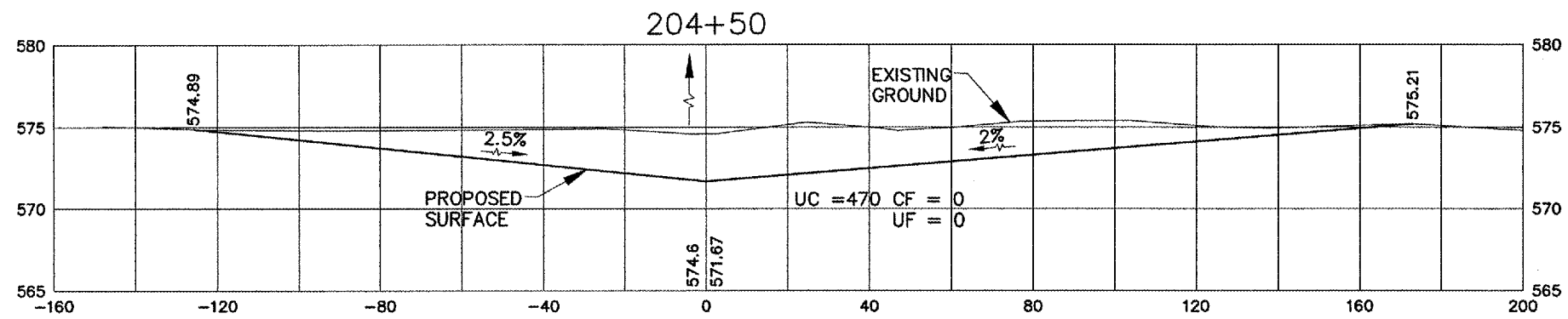
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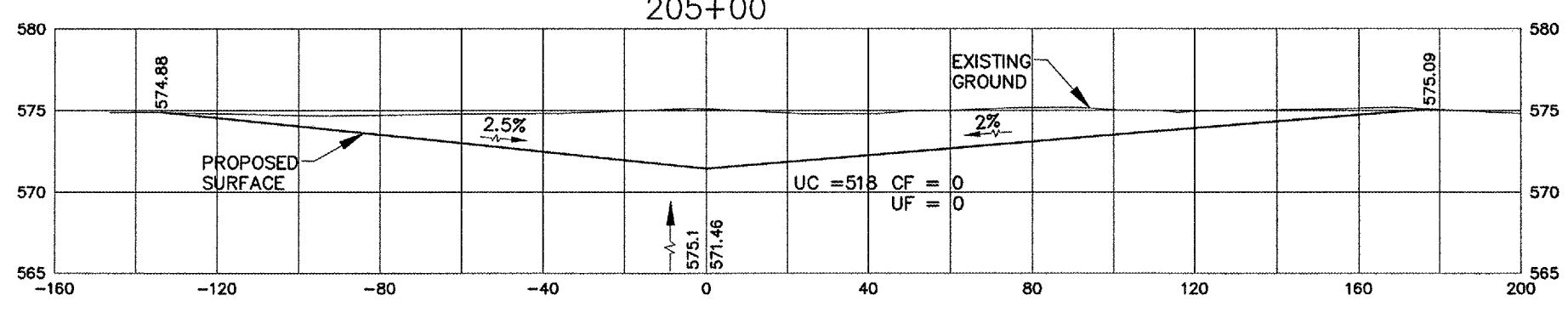
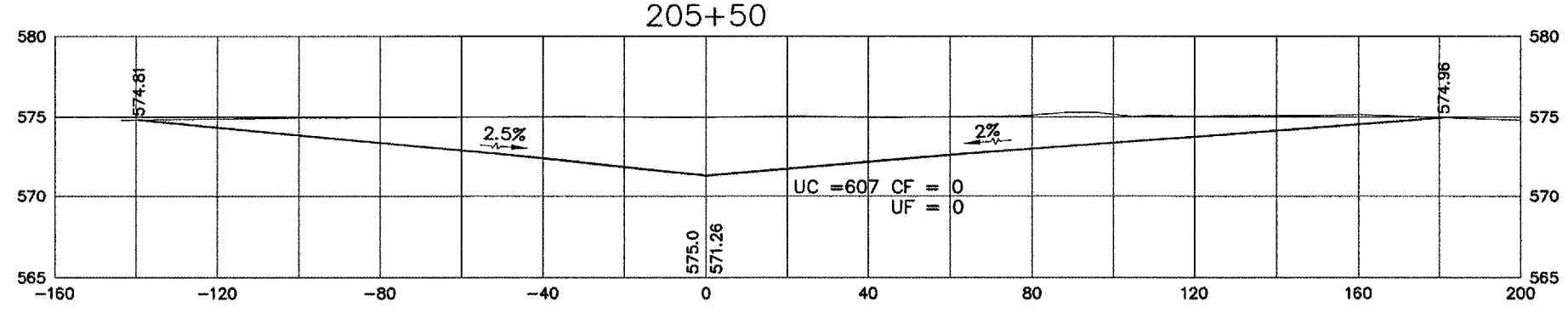
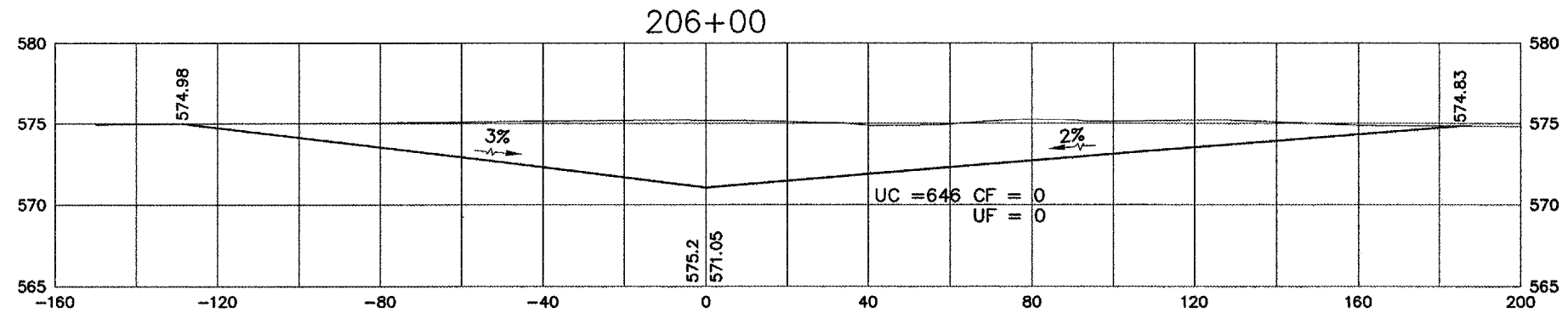
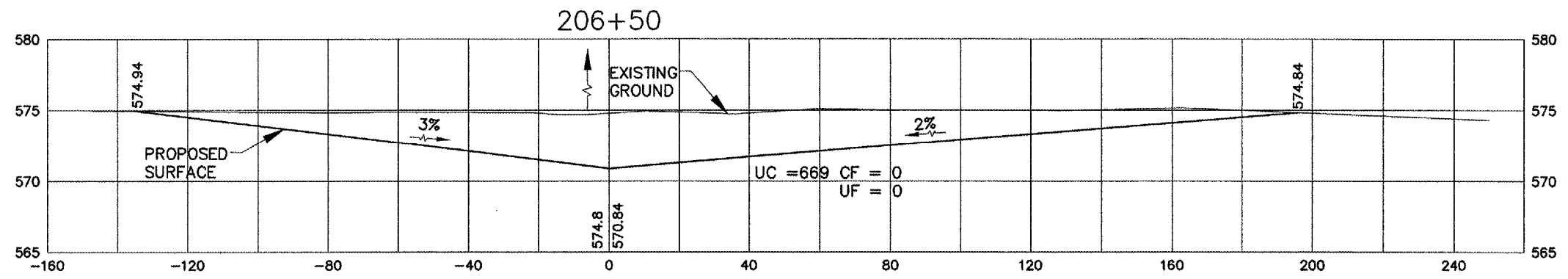


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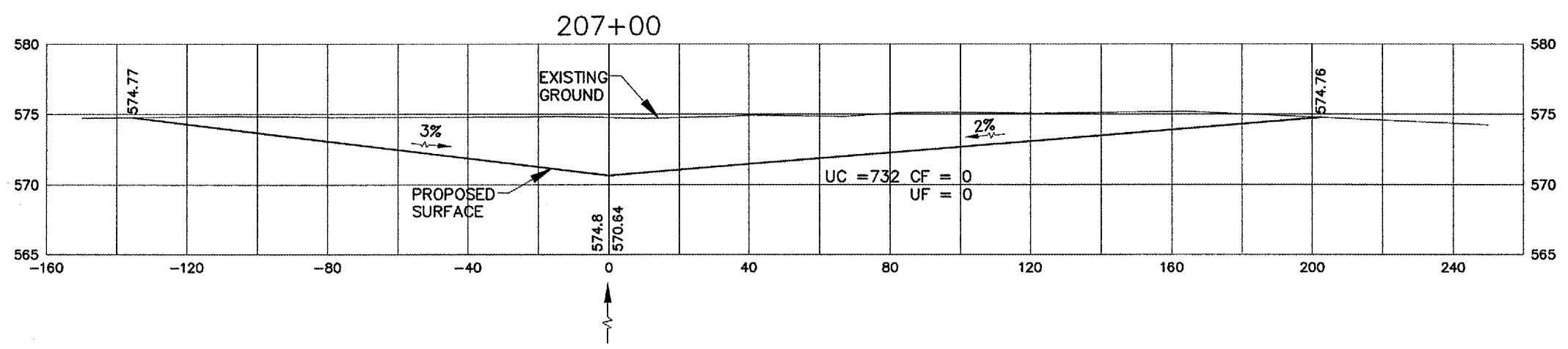
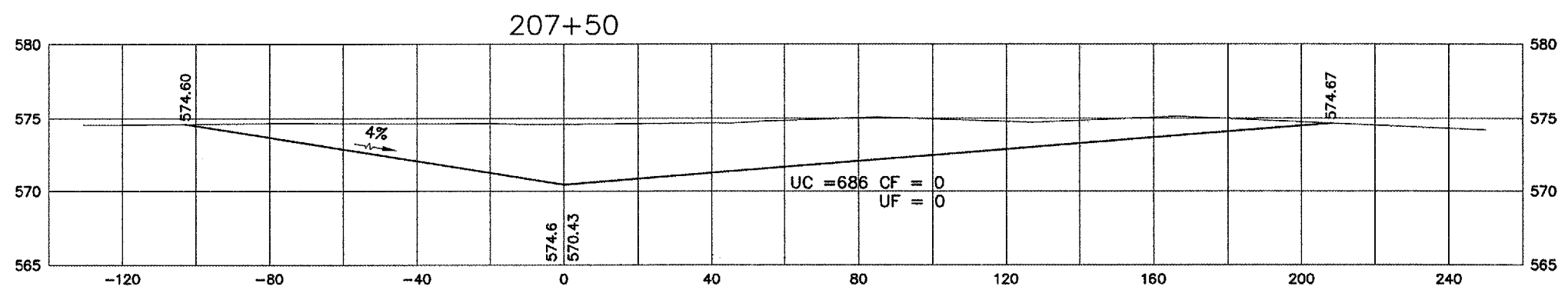
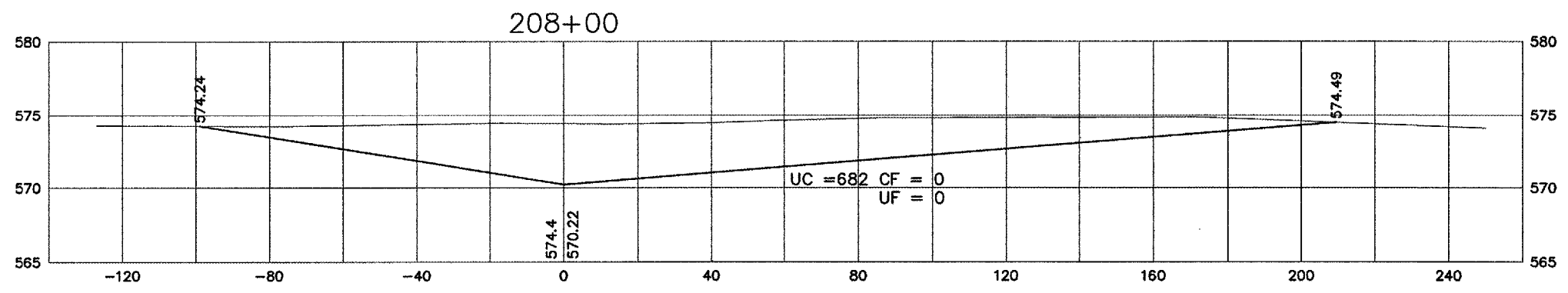
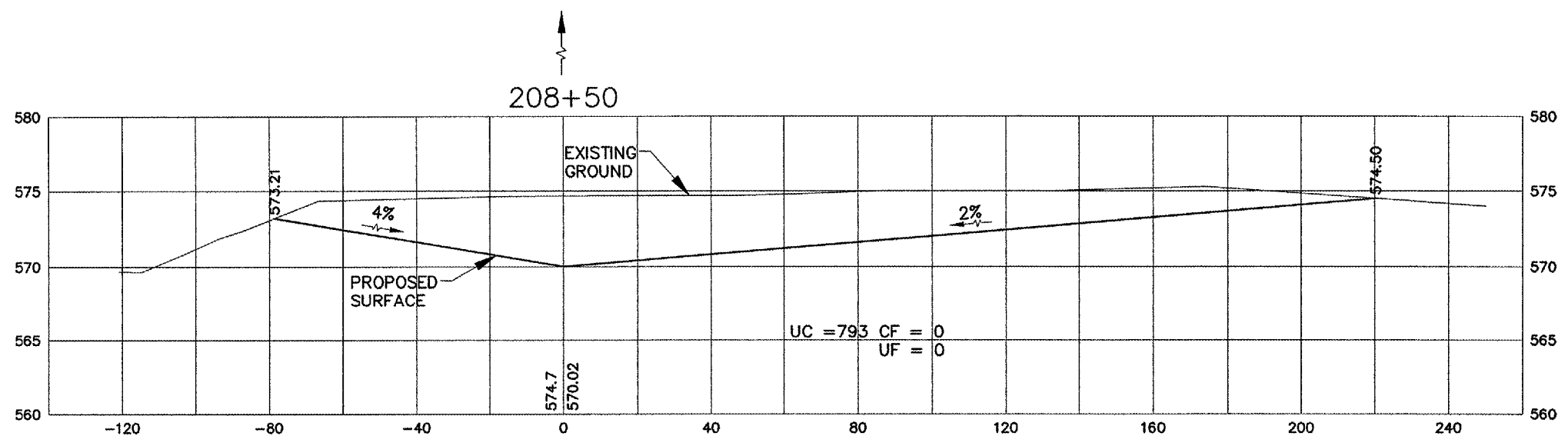


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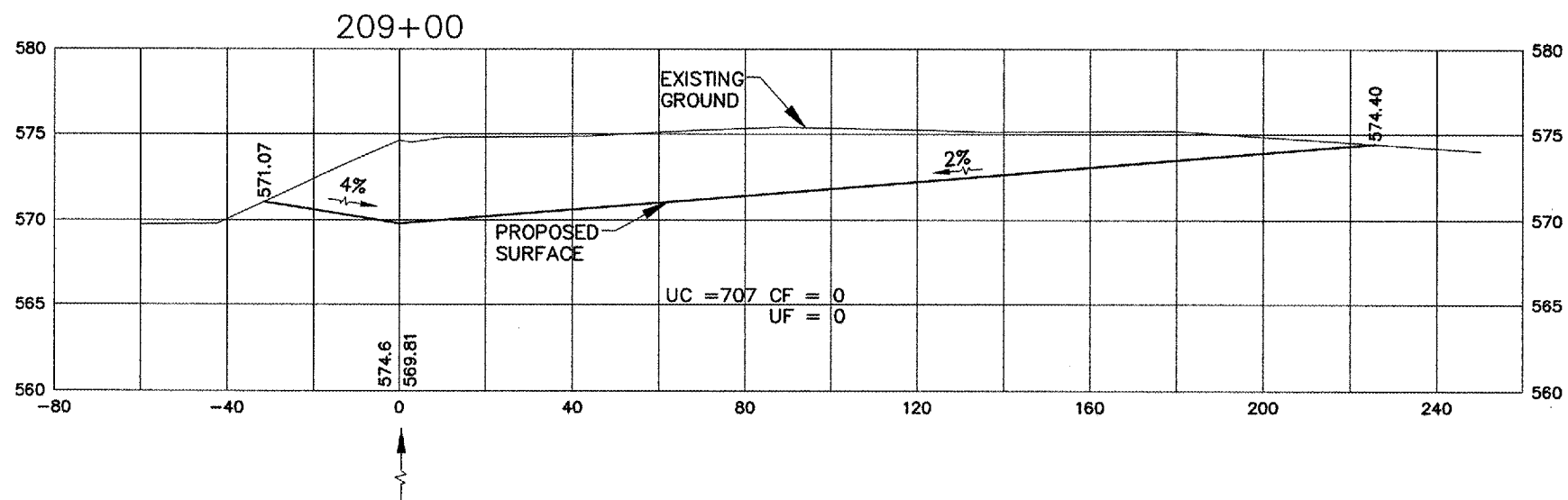
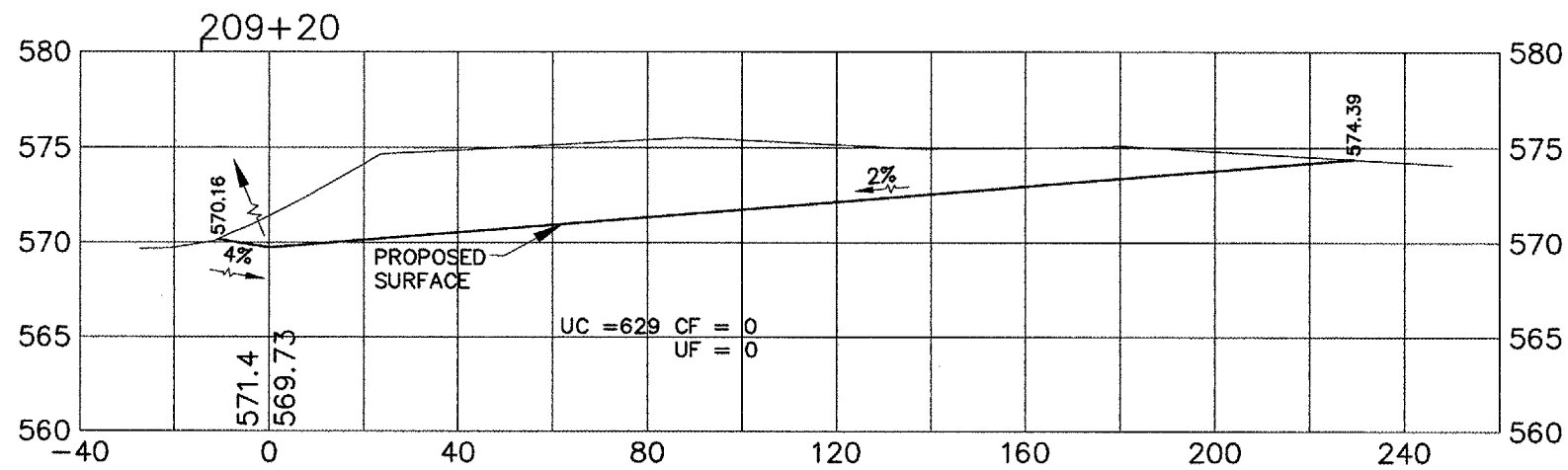
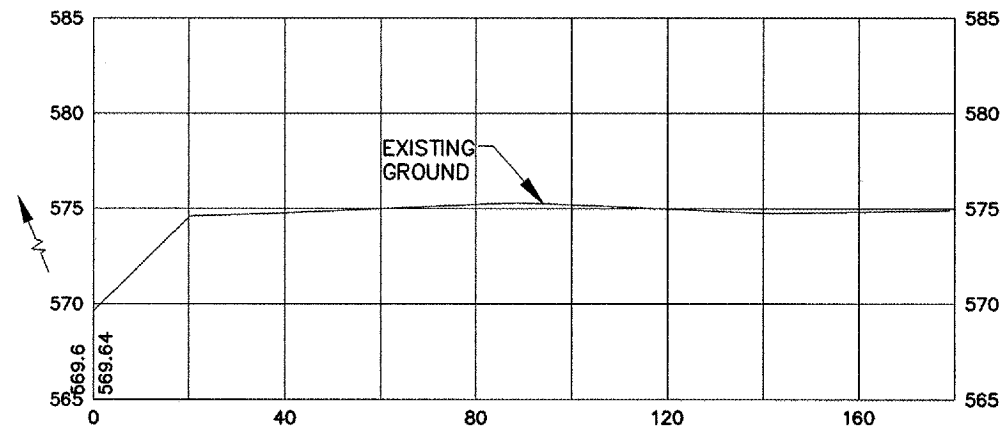


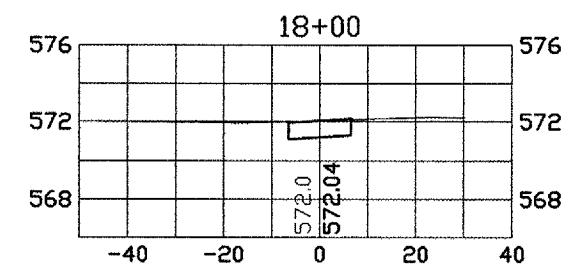
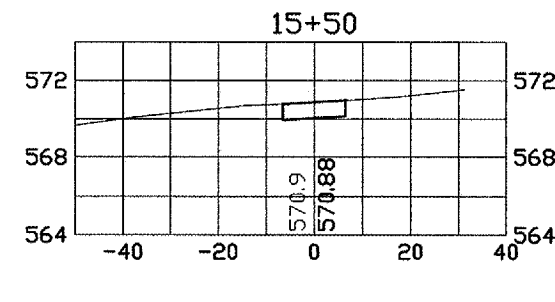
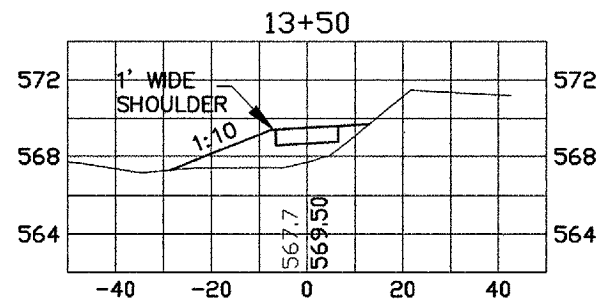
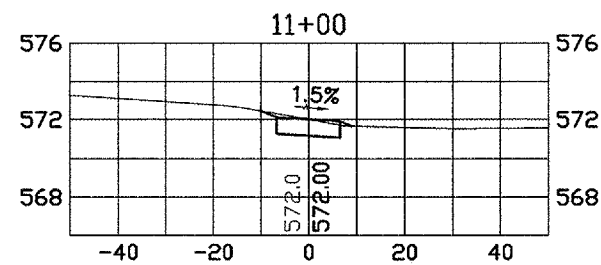
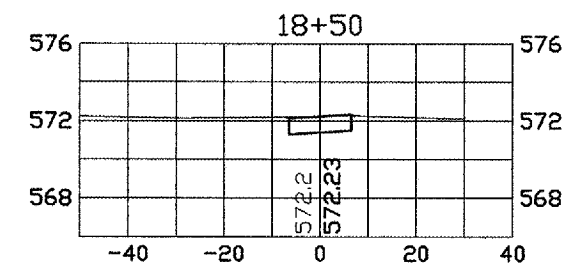
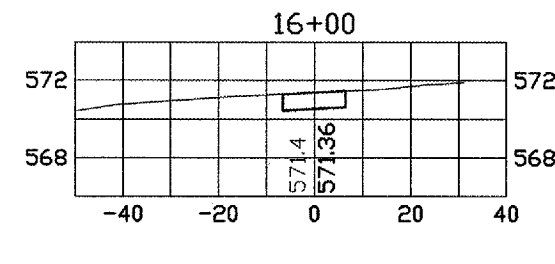
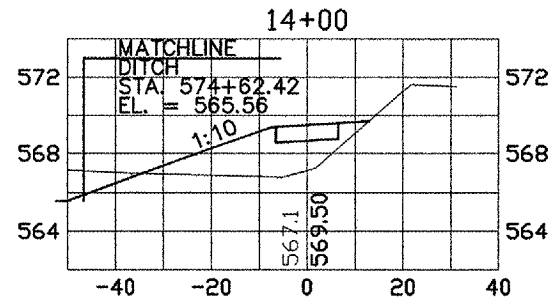
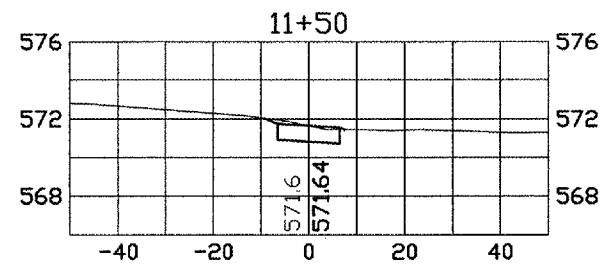
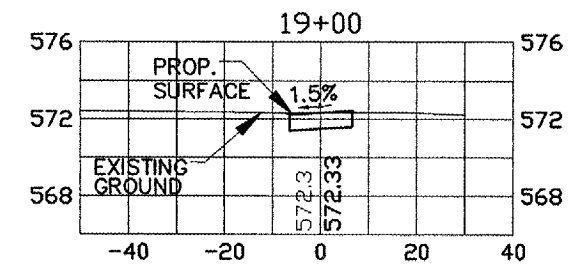
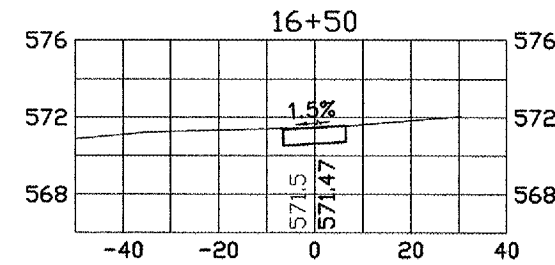
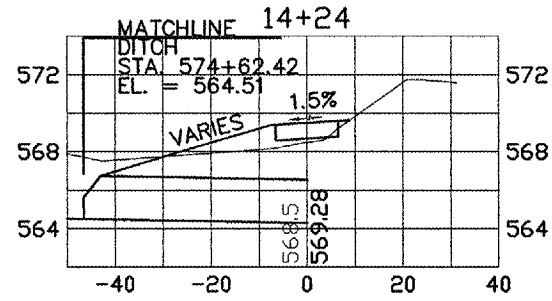
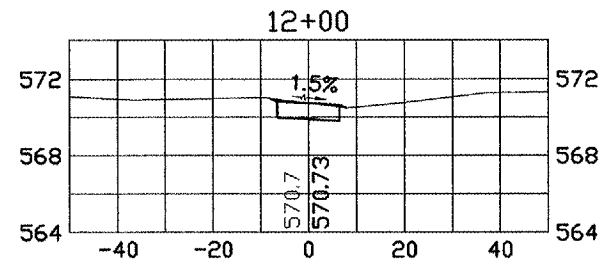
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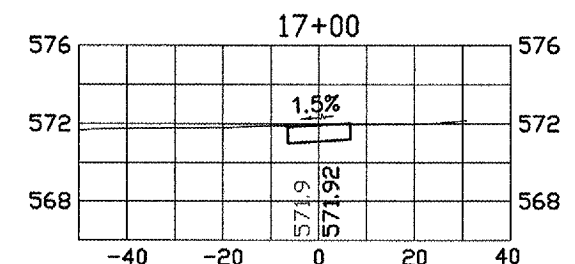
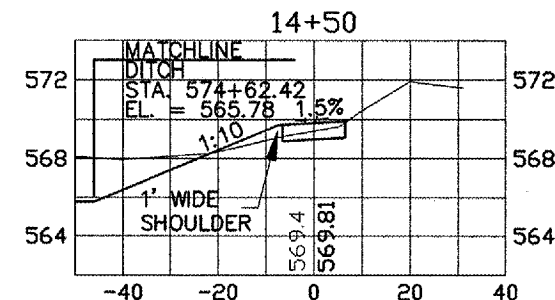
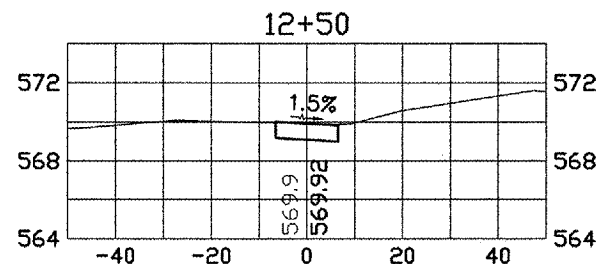
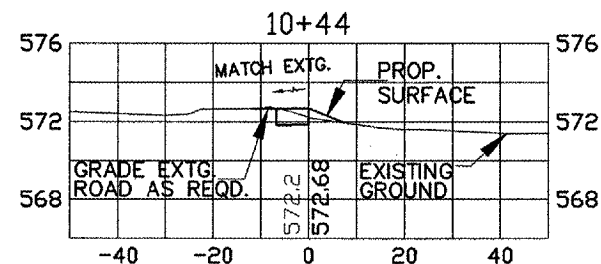
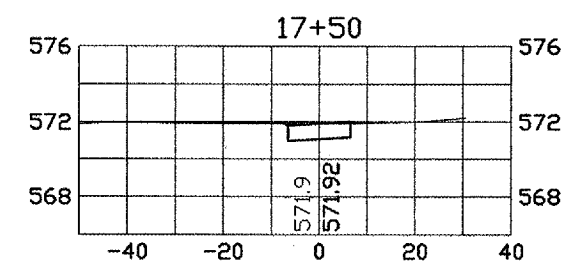
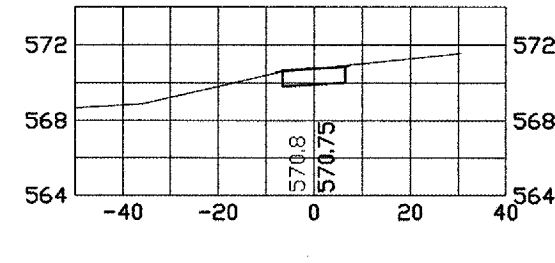
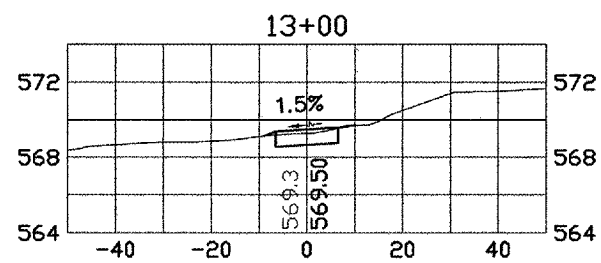
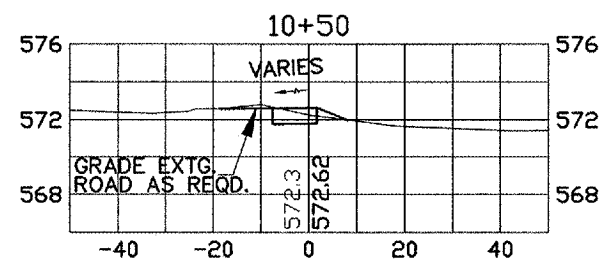
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209+40.67





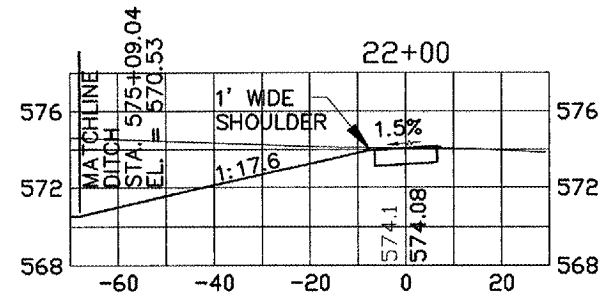
PROVIDE A 1.0' WIDE SHOULDER IN FILL AREA, STA. 13+25 TO 14+75



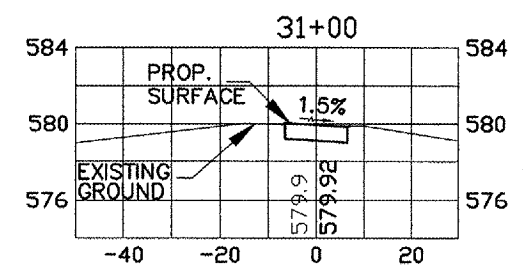
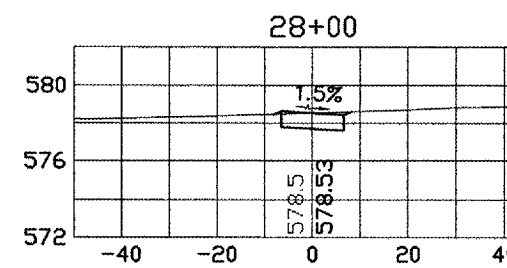
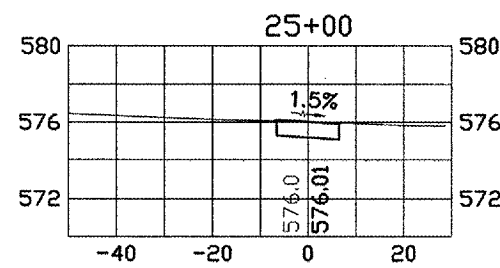
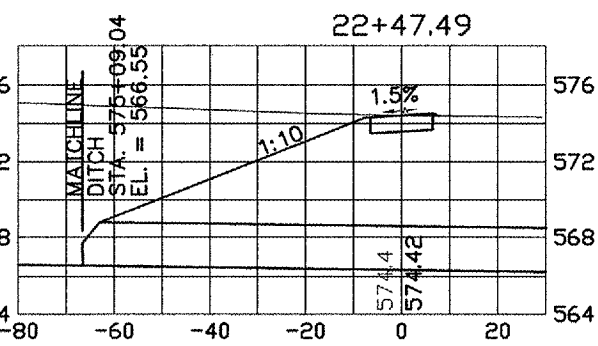
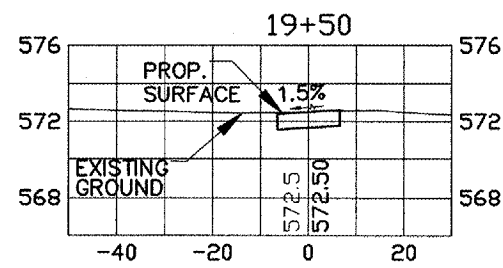
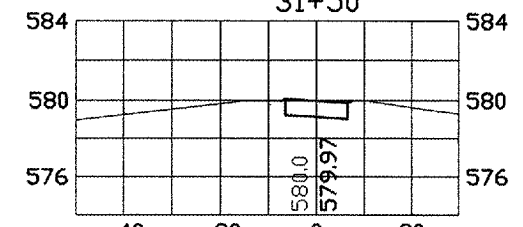
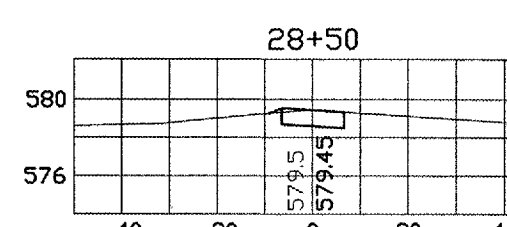
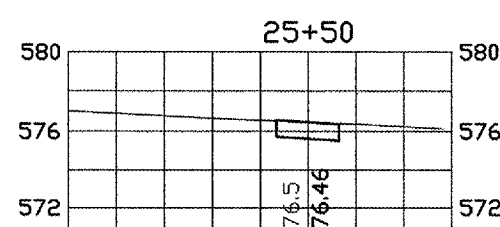
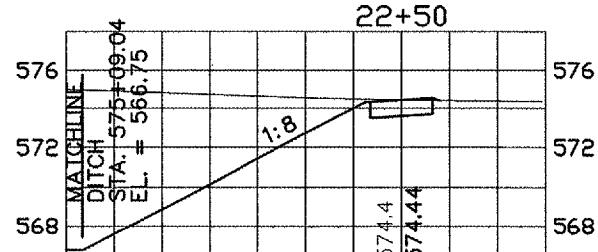
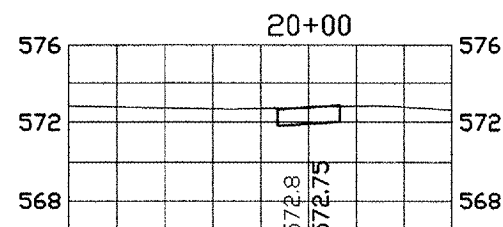
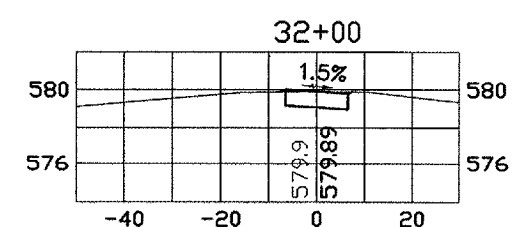
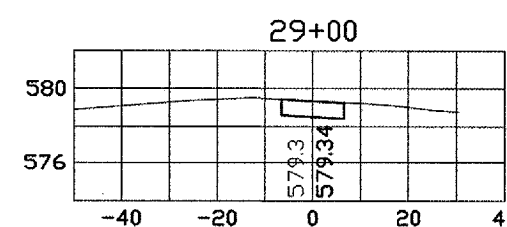
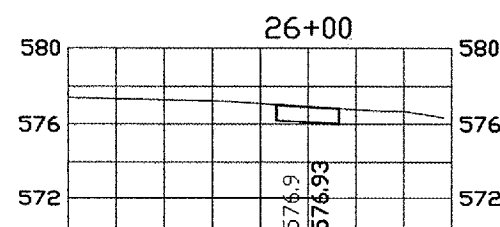
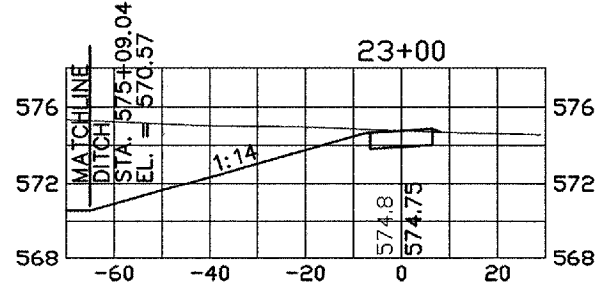
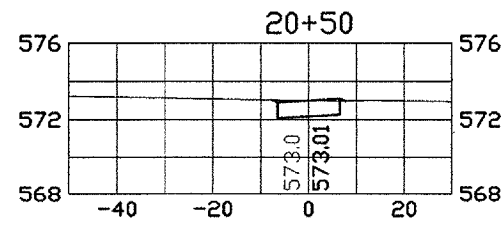
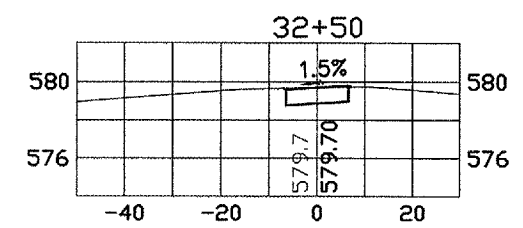
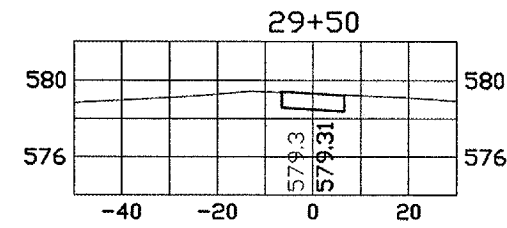
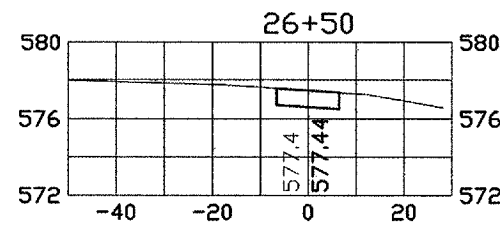
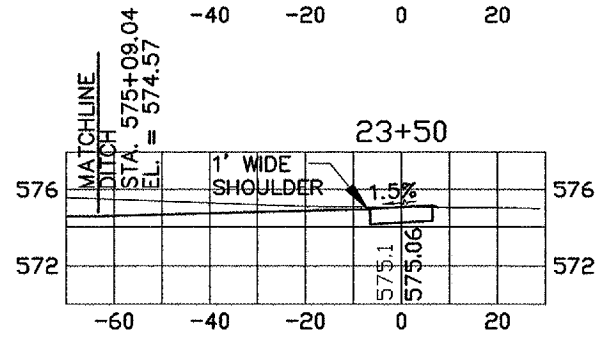
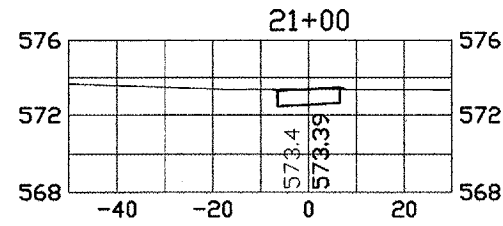
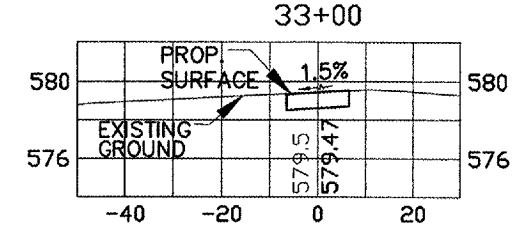
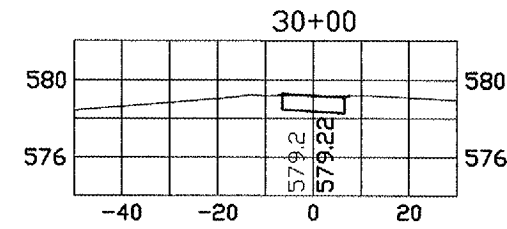
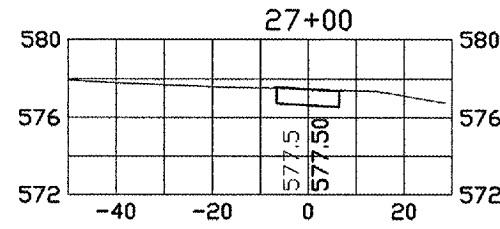
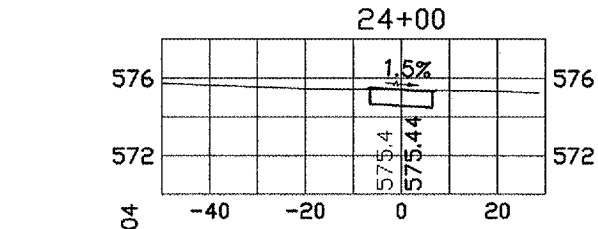
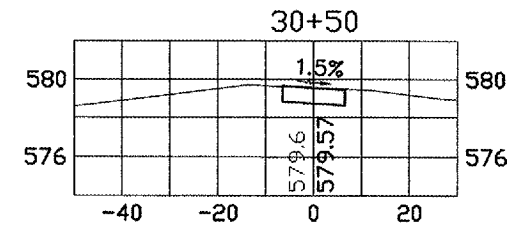
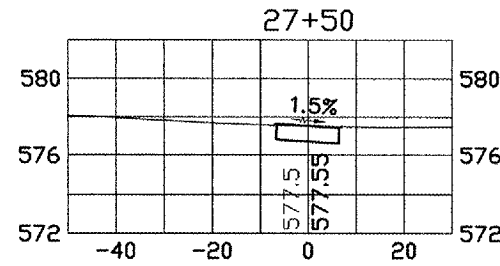
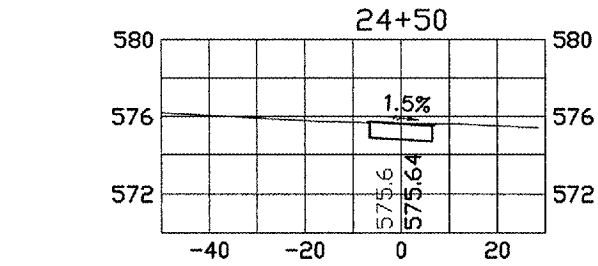
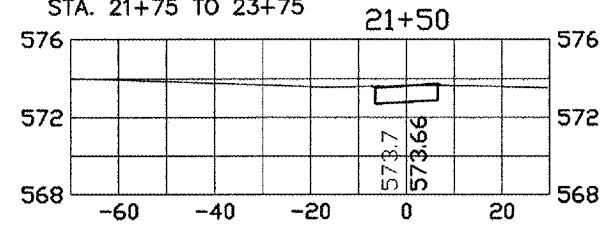
KEY FOR EARTHWORK

	UC = UNCLASSIFIED CUT
	CF = COMPACTED FILL
	UF = UNCOMPACTED FILL

G:\Airport\A07T030 TP EARTHWORK DSN\Sections Service Road.dwg, 4/21/2008 11:45:41 AM, jefim



PROVIDE A 1.0' WIDE SHOULDER IN CUT AREA, STA. 21+75 TO 23+75



QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE I - EARTHWORK
ILL. MLI-3790, QU006
SHEET 83 OF 94

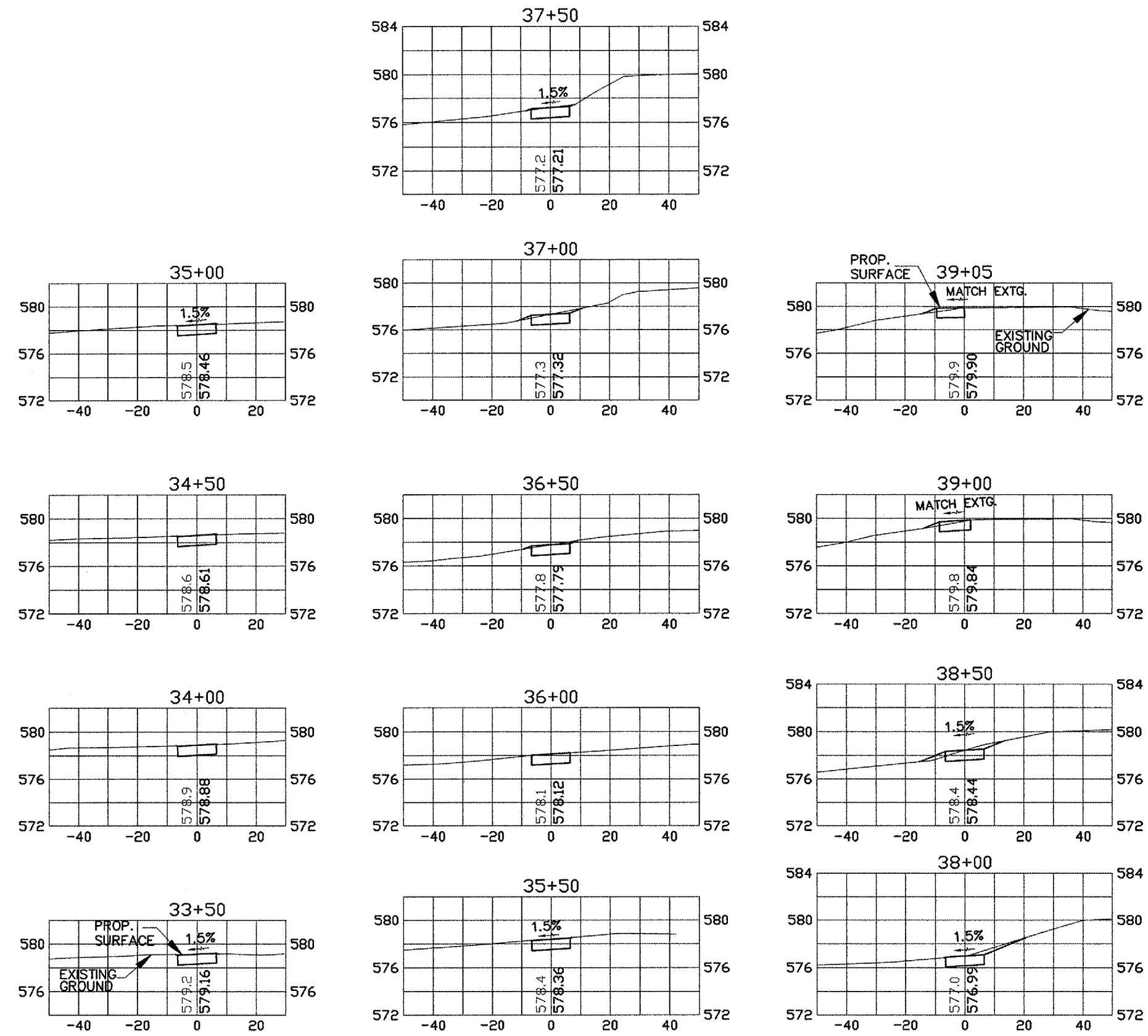
SERVICE ROAD
EARTHWORK DATA STA. 10+44 TO 39+05

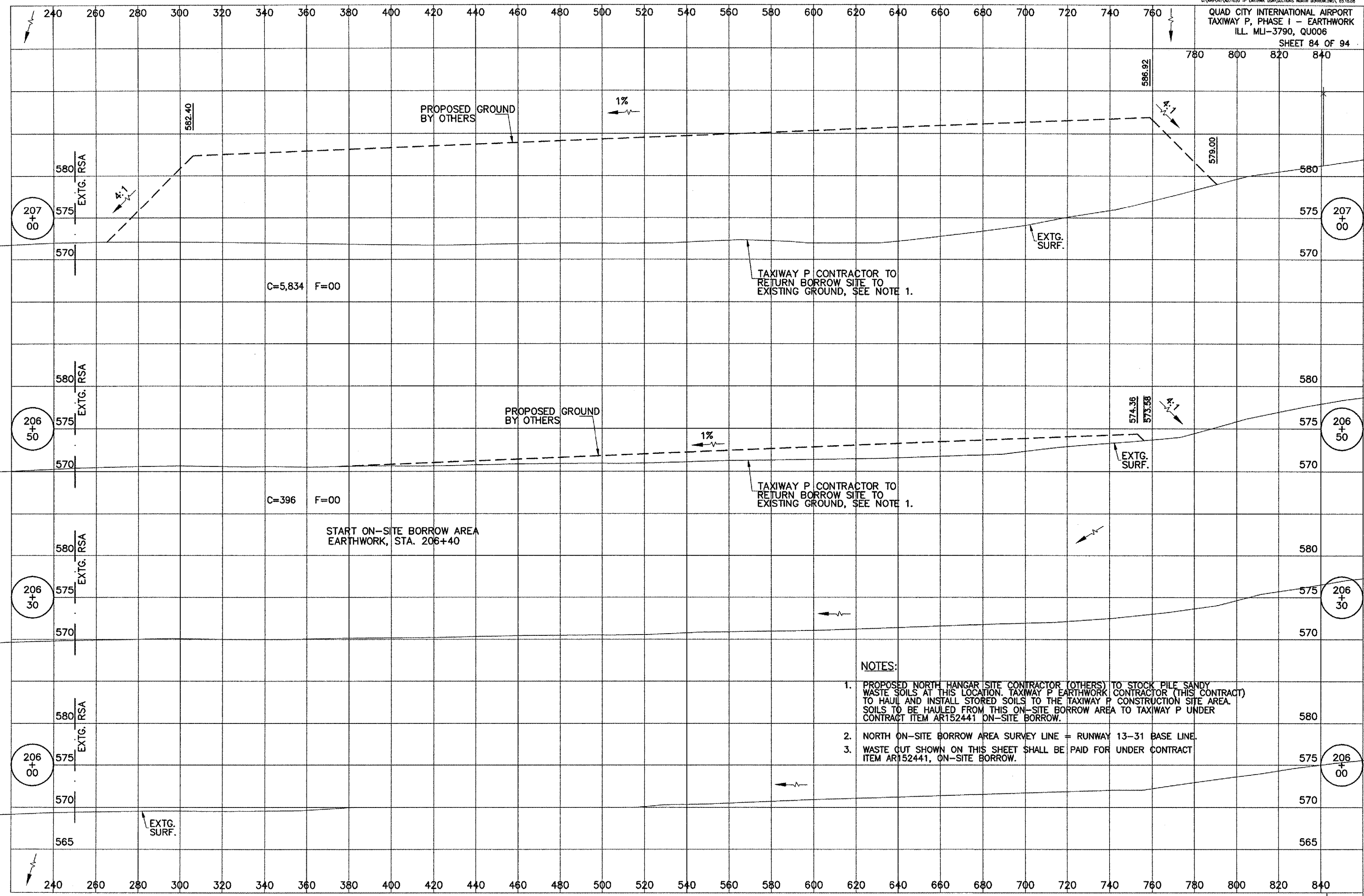
STATION	AREAS		VOLUMES		CUMULATIVE VOLUMES	
	CUT	FILL	CUT	FILL	CUT	FILL
10+44	12.2623	0.1779	2.6843	0.0305	2.6843	0.0305
10+50	11.8962	0.0967	21.3100	0.3256	23.9943	0.3561
11+00	11.1186	0.2549	20.7509	0.2776	44.7452	0.6337
11+50	11.2923	0.0449	20.6596	0.0989	65.4048	0.7326
12+00	10.9168	0.0693	20.1553	0.0797	85.5601	0.8123
12+50	10.8509	0.0167	20.7381	0.0155	106.2982	0.8278
13+00	11.5463	0.0000	10.7056	22.3751	117.0038	23.2028
13+50	0.0000	25.0129	0.0000	63.5430	117.0038	86.7459
14+00	0.0000	43.6136	0.5594	24.3858	117.5633	111.1316
14+24	1.2587	11.2545	3.3169	7.4493	120.8801	118.5809
14+50	5.6301	4.2171	15.2699	3.9370	136.1501	122.5179
15+00	10.8614	0.0349	20.0885	0.0323	156.2385	122.5503
15+50	10.8341	0.0000	20.0644	0.0012	176.3029	122.5515
16+00	10.8354	0.0013	20.1466	0.0018	196.4495	122.5533
16+50	10.9229	0.0007	20.1768	0.0320	216.6262	122.5853
17+00	10.8680	0.0339	20.1380	0.0748	236.7642	122.6601
17+50	10.8810	0.0469	20.1169	0.0537	256.8911	122.7138
18+00	10.8452	0.0111	20.0940	0.0309	276.9751	122.7447
18+50	10.8563	0.0223	20.1206	0.0576	297.0957	122.8023
19+00	10.8740	0.0399	20.1327	0.0548	317.2284	122.8570
19+50	10.8693	0.0192	20.1107	0.0328	337.3391	122.8998
20+00	10.8503	0.0161	20.1488	0.0857	357.4879	122.9784
20+50	10.9105	0.0764	20.1419	0.1245	377.6298	123.0999
21+00	10.8428	0.0580	20.0909	0.0845	397.7207	123.1844
21+50	10.8554	0.0333	20.1391	0.0949	417.8598	123.2793
22+00	10.8948	0.0692	19.1789	0.1302	437.0387	123.4096
22+47.49	10.9131	0.0789	1.0145	0.0073	438.0532	123.4169
22+50	10.9130	0.0789	20.2734	0.1909	458.3266	123.6078
23+00	10.9823	0.1273	20.2536	0.1711	478.5802	123.7789
23+50	10.8916	0.0575	20.1309	0.0826	498.7111	123.8616
24+00	10.8497	0.0318	20.1778	0.0405	518.8889	123.9020
24+50	10.9422	0.0119	20.1294	0.0271	539.0182	123.9292
25+00	10.7975	0.0173	20.0325	0.0215	559.0508	123.9507
25+50	10.8376	0.0059	20.1007	0.0055	579.1515	123.9561
26+00	10.8712	0.0000	20.0986	0.0011	599.2501	123.9572
26+50	10.8353	0.0012	20.0705	0.0118	619.3206	123.9690
27+00	10.8408	0.0116	20.1065	0.0276	639.4271	123.9986
27+50	10.8525	0.0196	20.2408	0.1277	659.6678	124.1243
28+00	10.9623	0.1247	19.6114	0.3545	679.2792	124.4789
28+50	10.1683	0.2766	19.4734	0.2451	698.7528	124.7239
29+00	10.8433	0.0003	20.0292	0.0082	718.7818	124.7322
29+50	10.7883	0.0086	20.0622	0.0495	738.8441	124.7816
30+00	10.8790	0.0448	20.1091	0.0460	758.9532	124.8276
30+50	10.8389	0.0048	20.0828	0.0196	779.0360	124.8472
31+00	10.8505	0.0164	20.1292	0.0660	799.1652	124.9132
31+50	10.8890	0.0549	20.0820	0.1112	819.2472	125.0244
32+00	10.7995	0.0652	20.0477	0.0769	839.2949	125.1013
32+50	10.8520	0.0179	20.0811	0.0179	859.3760	125.1192
33+00	10.8356	0.0014	20.0974	0.0334	879.4734	125.1526
33+50	10.8696	0.0346	20.1286	0.0389	899.6020	125.1915
34+00	10.8693	0.0074	20.1028	0.0155	919.7048	125.2070
34+50	10.8417	0.0093	20.0906	0.0202	939.7954	125.2272
35+00	10.8561	0.0125	20.2854	0.0118	960.0808	125.2390
35+50	11.0598	0.0000	20.2752	0.0001	980.3560	125.2391
36+00	10.8342	0.0001	20.1969	0.2168	1000.5528	125.4559
36+50	11.0188	0.2241	20.5178	0.7134	1021.0706	126.1693
37+00	11.2440	0.5102	20.5263	0.5561	1041.5969	126.7254
37+50	10.9244	0.0903	22.9123	0.0990	1064.5092	126.8243
38+00	14.0300	0.0120	23.6154	2.2364	1088.1246	129.0607
38+50	11.8084	2.2826	20.6273	2.4253	1108.7519	131.4861
39+00	10.4691	0.3367	1.9387	0.0624	1110.6906	131.5484
39+05	10.4691	0.3367	0.0000	0.0000	1110.6906	131.5484
GRAND TOTAL			1111	132 *		

* COMPACTED FILL
UNADJUSTED VALUE

KEY FOR EARTHWORK

	UC = UNCLASSIFIED CUT
	CF = COMPACTED FILL
	UF = UNCOMPACTED FILL





C=5,834 F=00

C=396 F=00

START ON-SITE BORROW AREA
EARTHWORK, STA. 206+40

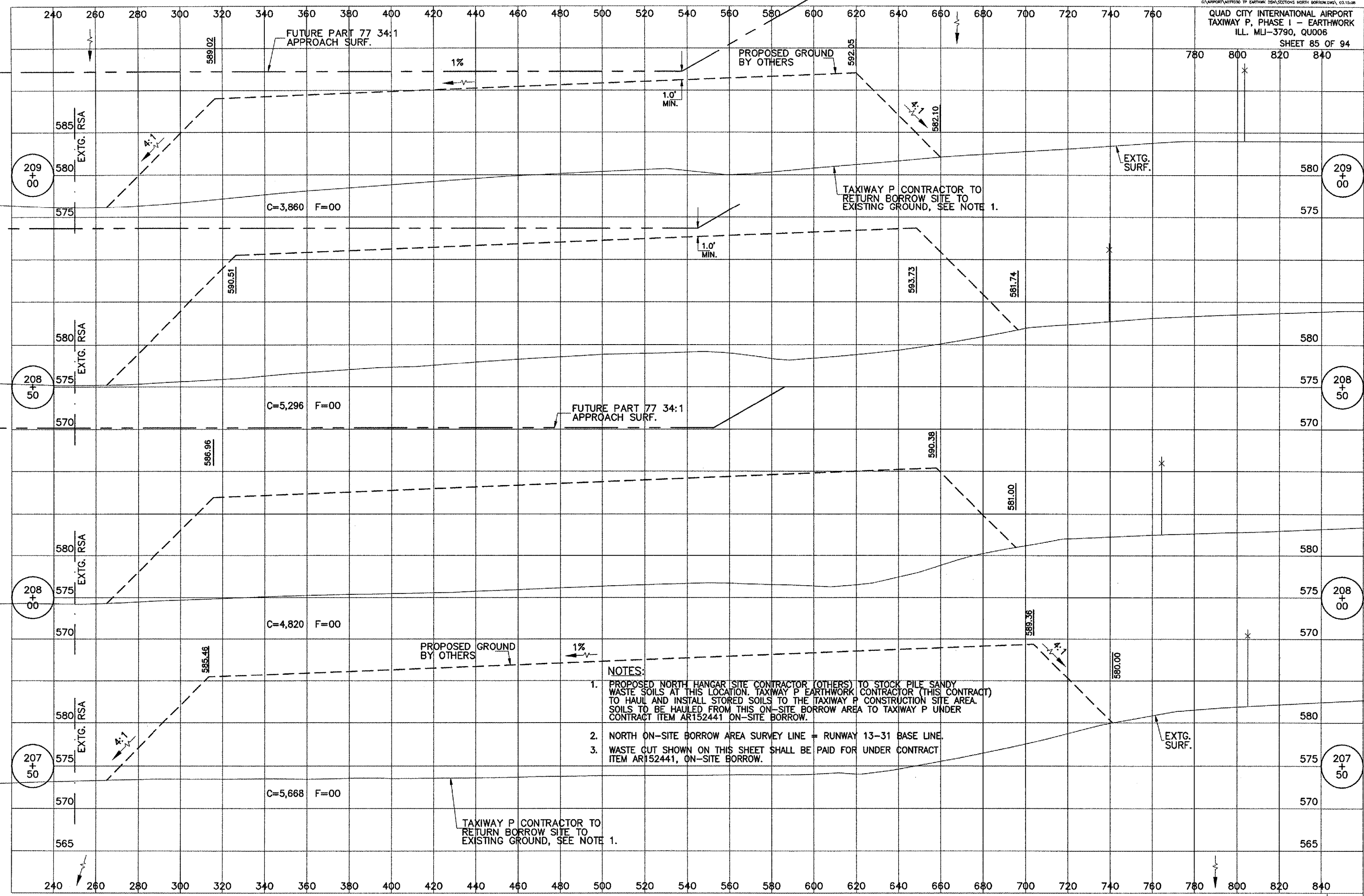
TAXIWAY P CONTRACTOR TO
RETURN BORROW SITE TO
EXISTING GROUND, SEE NOTE 1.

TAXIWAY P CONTRACTOR TO
RETURN BORROW SITE TO
EXISTING GROUND, SEE NOTE 1.

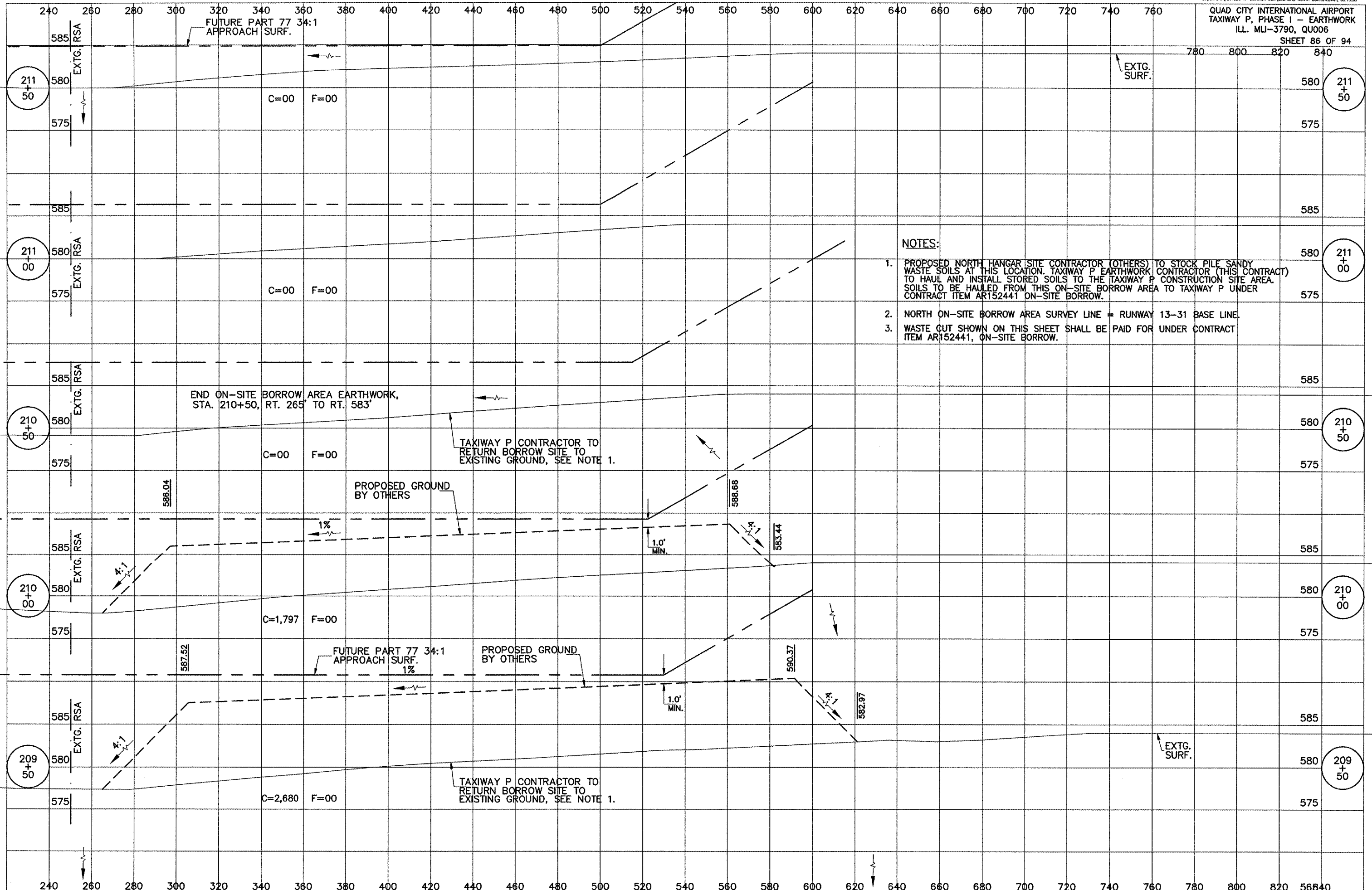
NOTES:

1. PROPOSED NORTH HANGAR SITE CONTRACTOR (OTHERS) TO STOCK PILE SANDY WASTE SOILS AT THIS LOCATION. TAXIWAY P EARTHWORK CONTRACTOR (THIS CONTRACT) TO HAUL AND INSTALL STORED SOILS TO THE TAXIWAY P CONSTRUCTION SITE AREA. SOILS TO BE HAULED FROM THIS ON-SITE BORROW AREA TO TAXIWAY P UNDER CONTRACT ITEM AR152441 ON-SITE BORROW.
2. NORTH ON-SITE BORROW AREA SURVEY LINE = RUNWAY 13-31 BASE LINE.
3. WASTE CUT SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER CONTRACT ITEM AR152441, ON-SITE BORROW.

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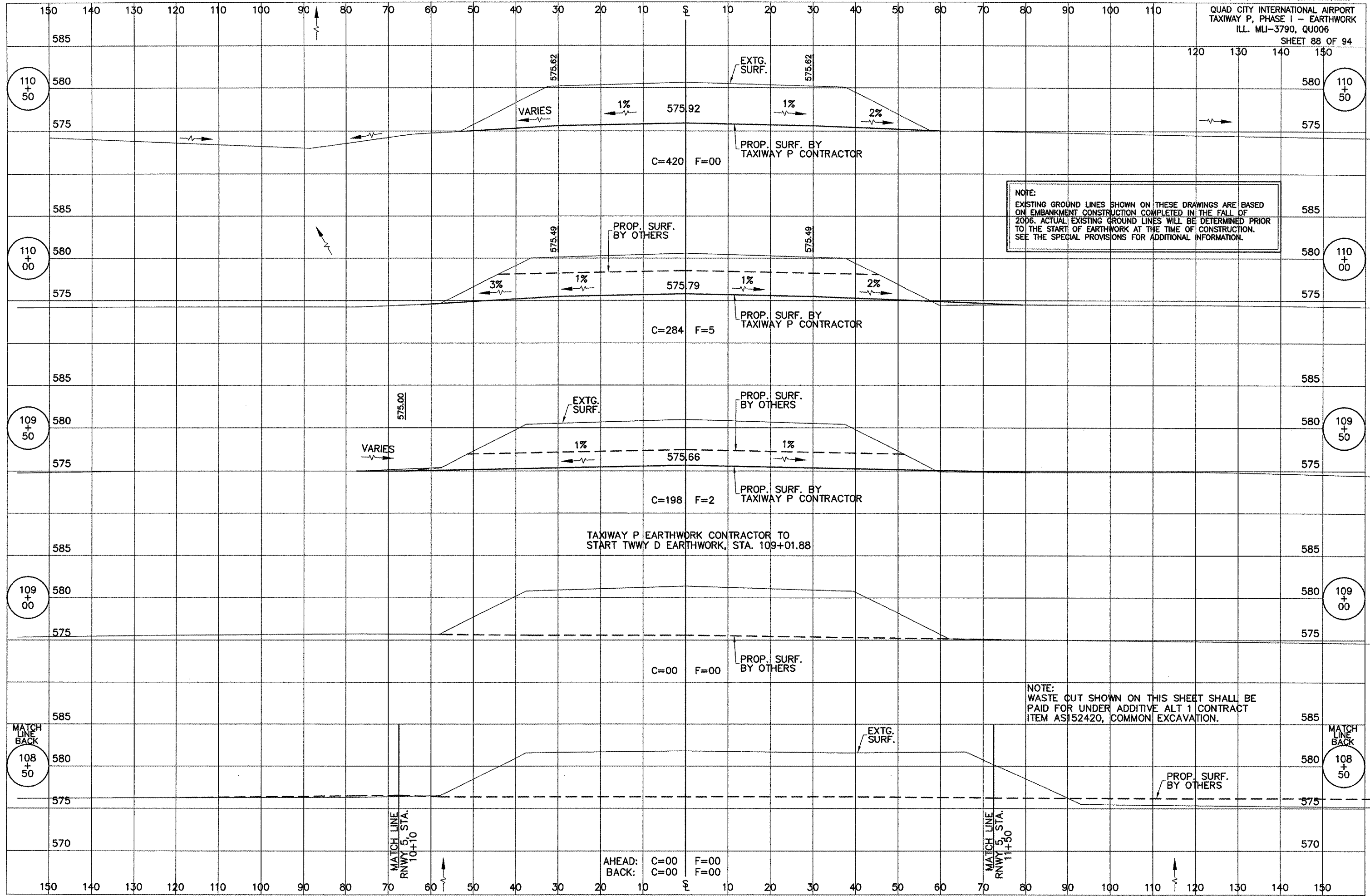


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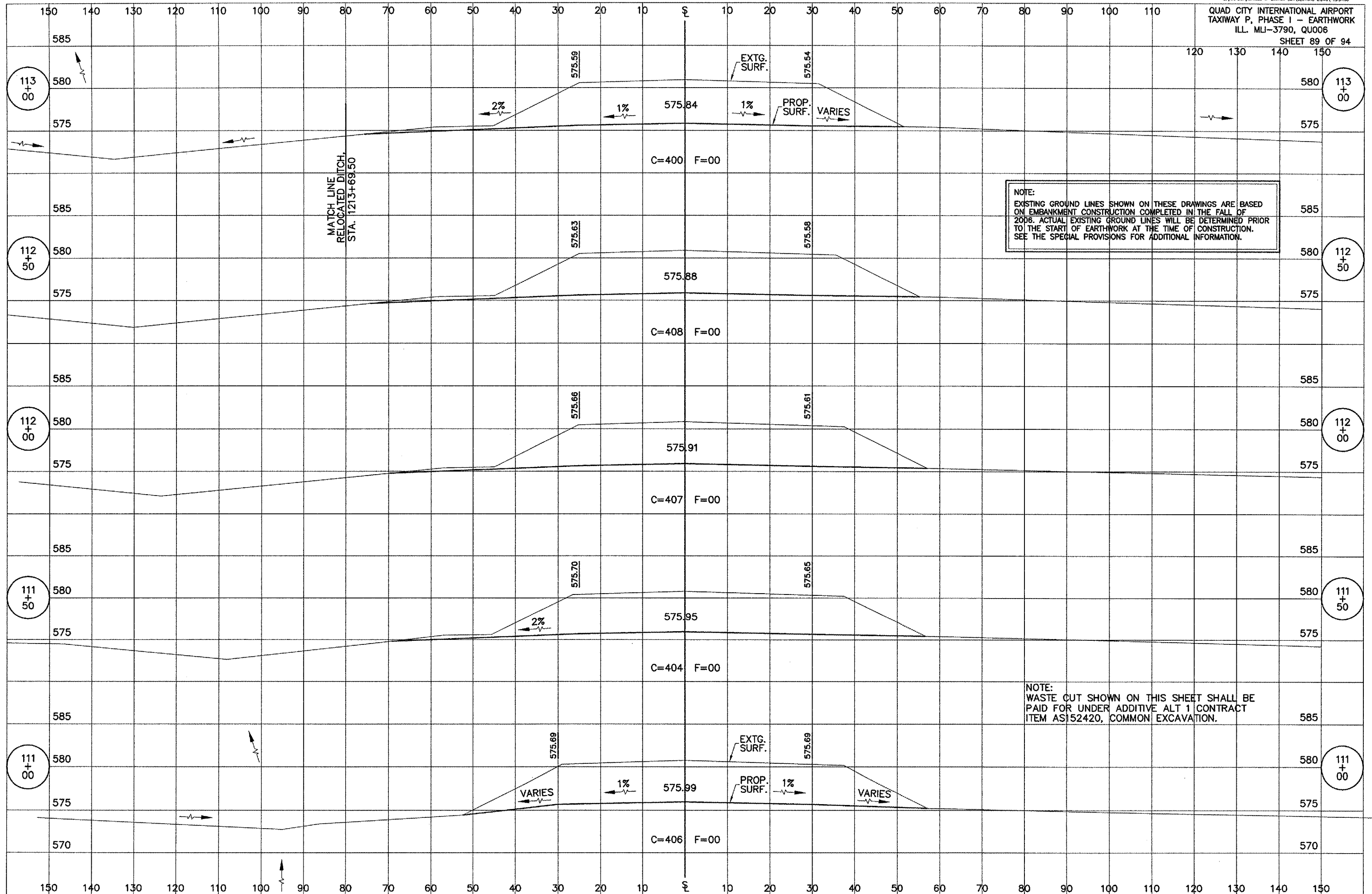


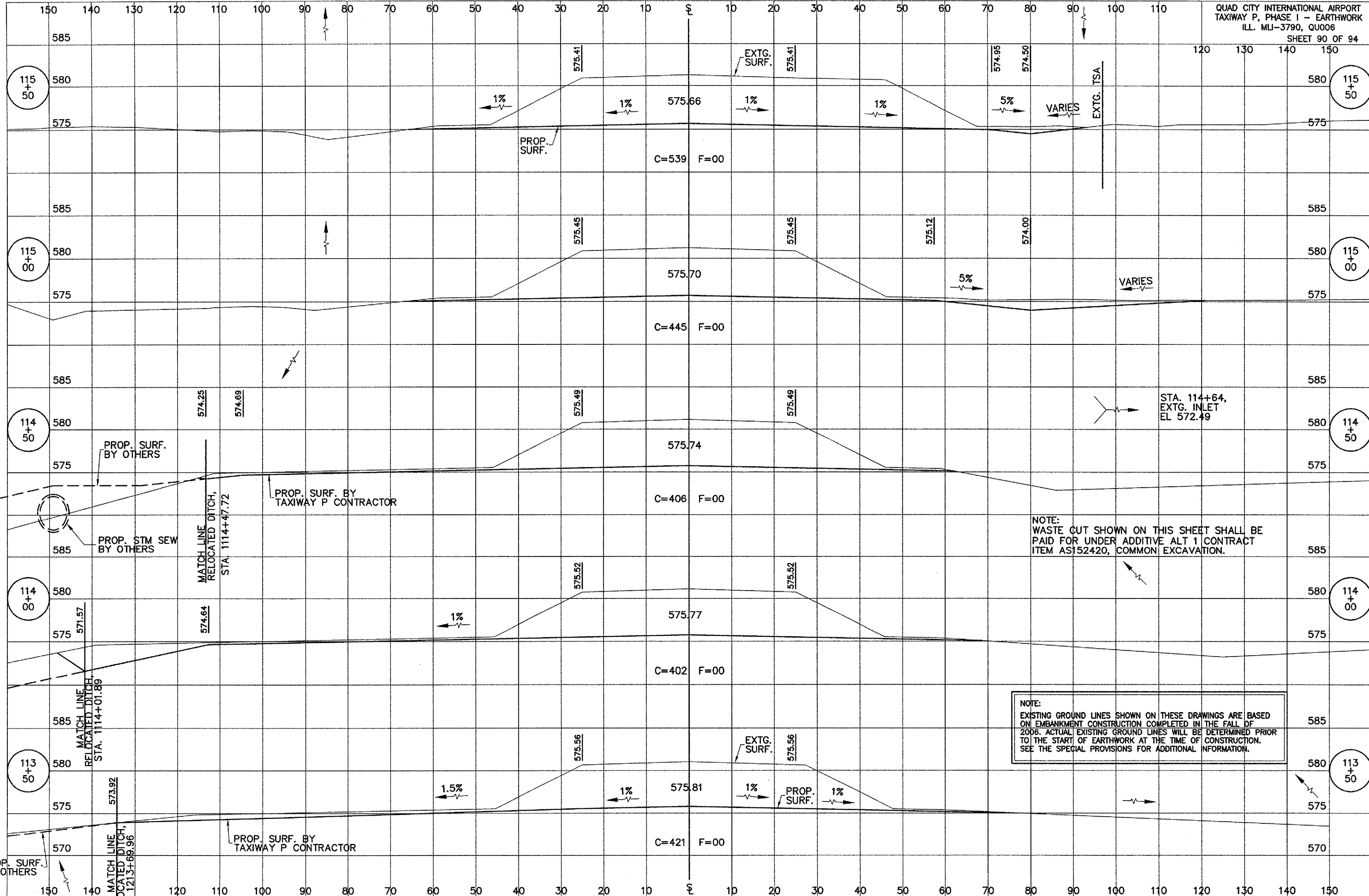
- NOTES:**
1. PROPOSED NORTH HANGAR SITE CONTRACTOR (OTHERS) TO STOCK PILE SANDY WASTE SOILS AT THIS LOCATION. TAXIWAY P EARTHWORK CONTRACTOR (THIS CONTRACT) TO HAUL AND INSTALL STORED SOILS TO THE TAXIWAY P CONSTRUCTION SITE AREA. SOILS TO BE HAULED FROM THIS ON-SITE BORROW AREA TO TAXIWAY P UNDER CONTRACT ITEM AR152441 ON-SITE BORROW.
 2. NORTH ON-SITE BORROW AREA SURVEY LINE = RUNWAY 13-31 BASE LINE.
 3. WASTE CUT SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER CONTRACT ITEM AR152441, ON-SITE BORROW.

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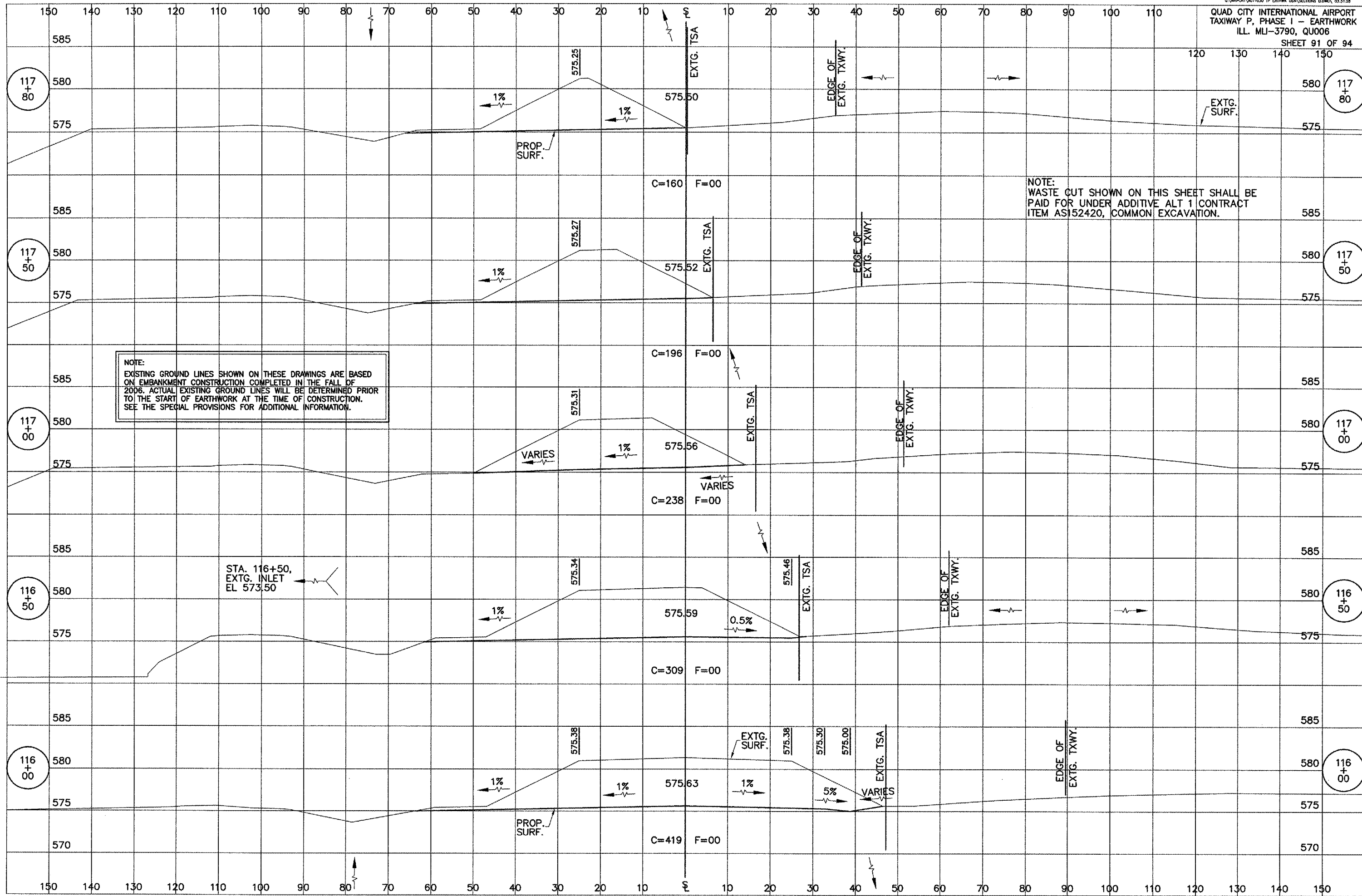




NOTE:
WASTE CUT SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER ADDITIVE ALT 1 CONTRACT ITEM AS152420, COMMON EXCAVATION.

NOTE:
EXISTING GROUND LINES SHOWN ON THESE DRAWINGS ARE BASED ON EMBANKMENT CONSTRUCTION COMPLETED IN THE FALL OF 2006. ACTUAL EXISTING GROUND LINES WILL BE DETERMINED PRIOR TO THE START OF EARTHWORK AT THE TIME OF CONSTRUCTION. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

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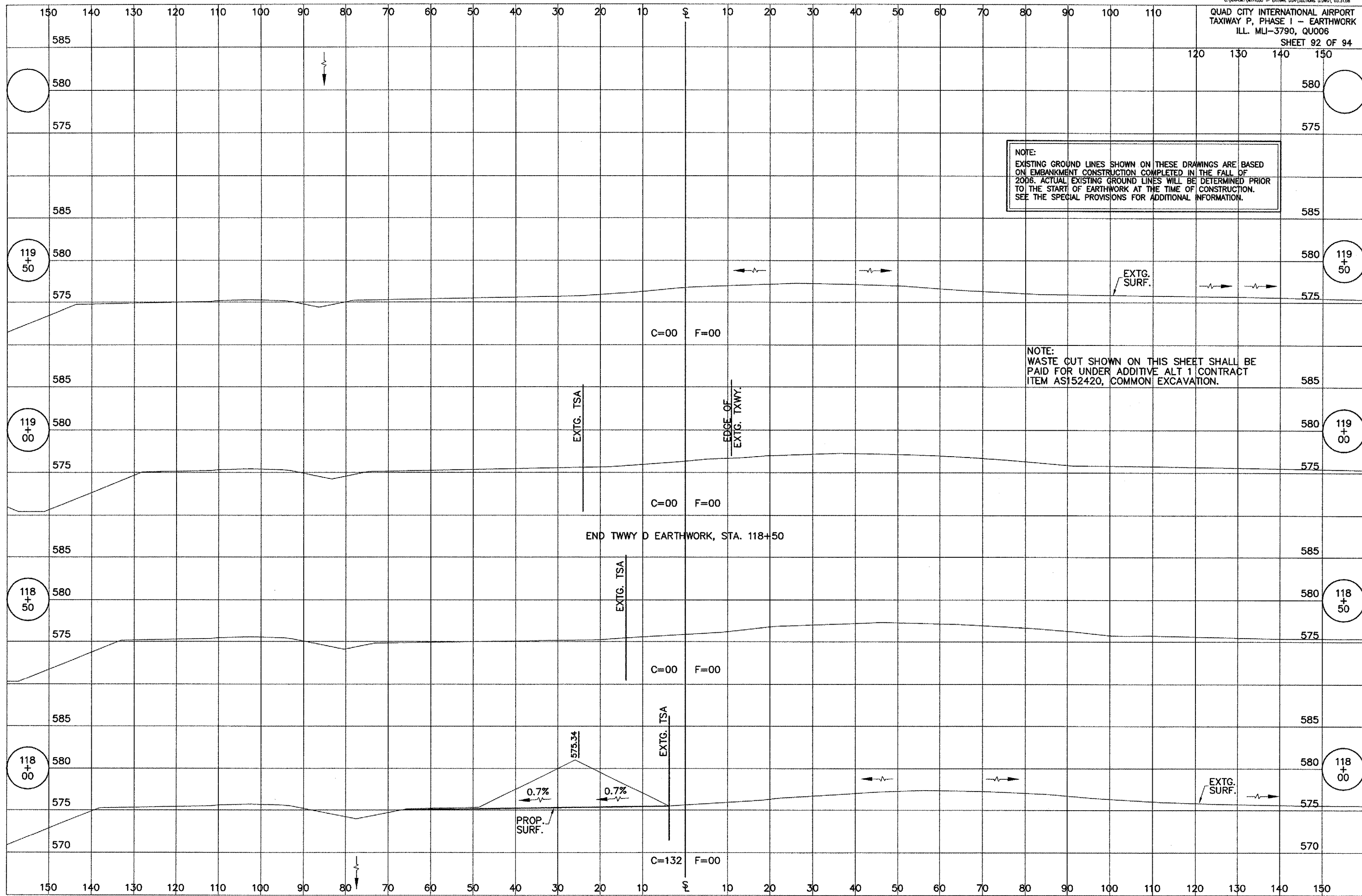


NOTE:
EXISTING GROUND LINES SHOWN ON THESE DRAWINGS ARE BASED ON EMBANKMENT CONSTRUCTION COMPLETED IN THE FALL OF 2006. ACTUAL EXISTING GROUND LINES WILL BE DETERMINED PRIOR TO THE START OF EARTHWORK AT THE TIME OF CONSTRUCTION. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

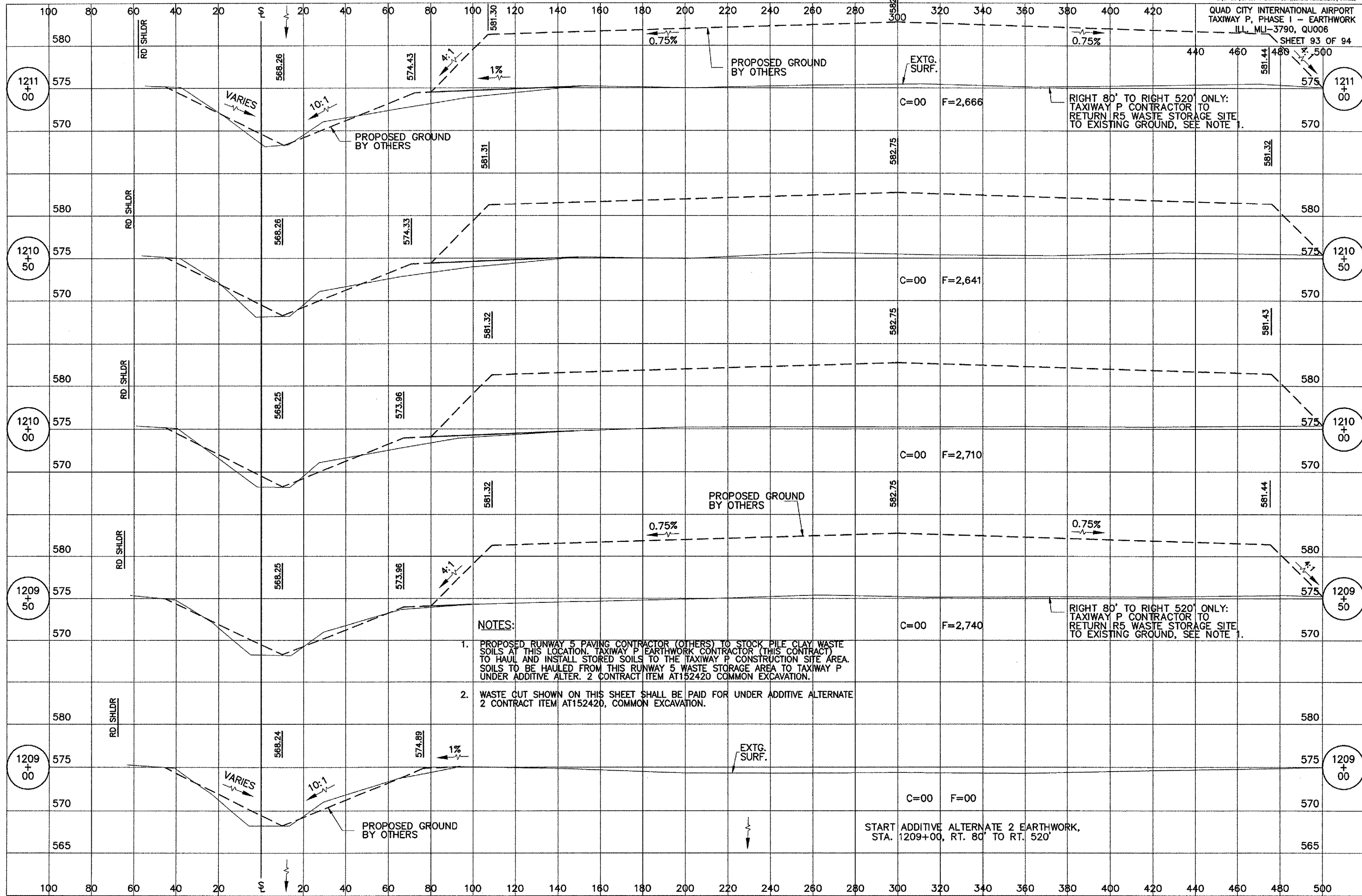
NOTE:
WASTE CUT SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER ADDITIVE ALT 1 CONTRACT ITEM ASI52420, COMMON EXCAVATION.

STA. 116+50,
EXTG. INLET
EL 573.50

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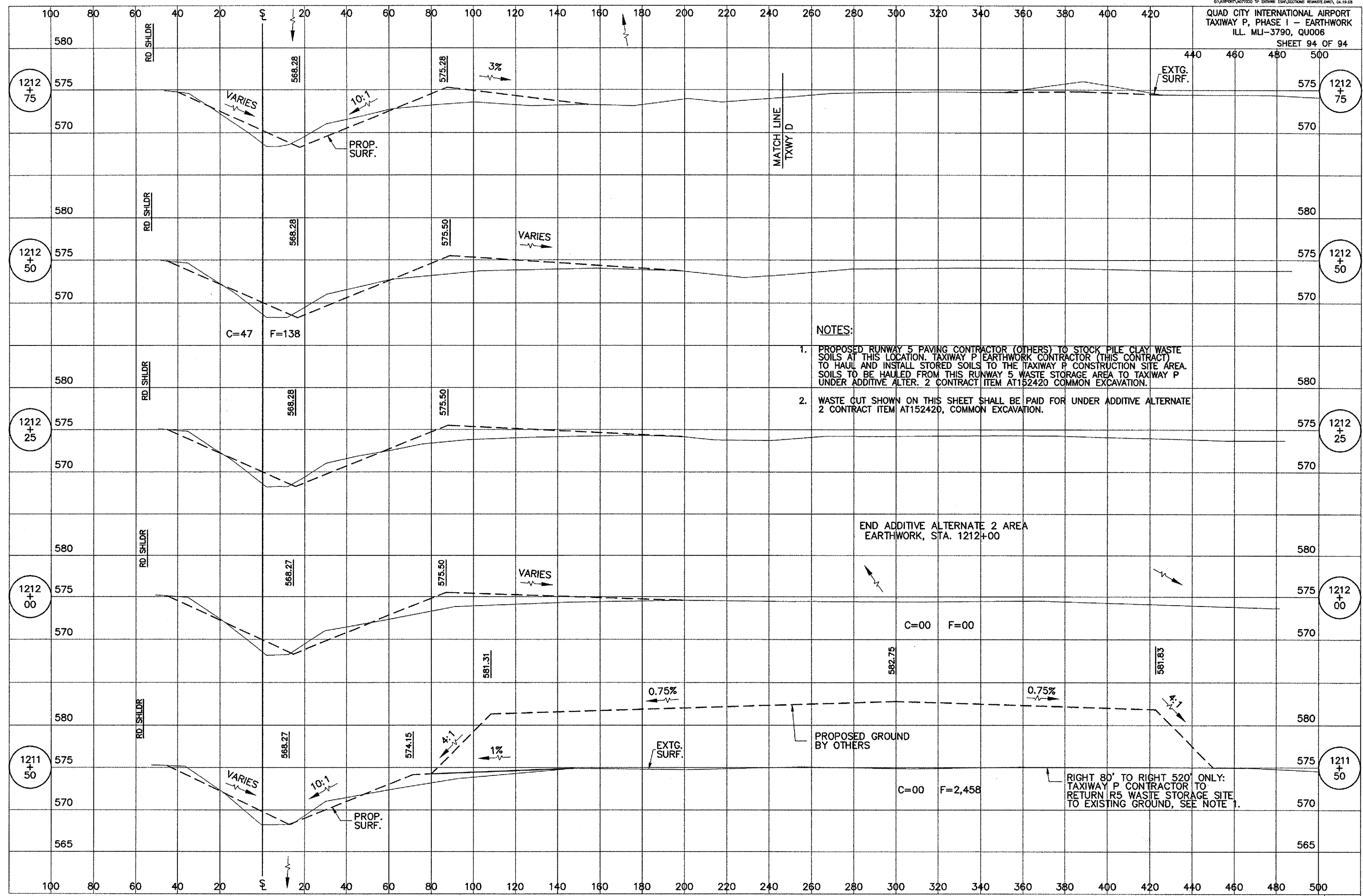
- NOTES:**
1. PROPOSED RUNWAY 5 PAVING CONTRACTOR (OTHERS) TO STOCK PILE CLAY WASTE SOILS AT THIS LOCATION. TAXIWAY P EARTHWORK CONTRACTOR (THIS CONTRACT) TO HAUL AND INSTALL STORED SOILS TO THE TAXIWAY P CONSTRUCTION SITE AREA. SOILS TO BE HAULED FROM THIS RUNWAY 5 WASTE STORAGE AREA TO TAXIWAY P UNDER ADDITIVE ALTER. 2 CONTRACT ITEM AT152420 COMMON EXCAVATION.
 2. WASTE CUT SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER ADDITIVE ALTERNATE 2 CONTRACT ITEM AT152420, COMMON EXCAVATION.

RIGHT 80' TO RIGHT 520' ONLY: TAXIWAY P CONTRACTOR TO RETURN R5 WASTE STORAGE SITE TO EXISTING GROUND, SEE NOTE 1.

RIGHT 80' TO RIGHT 520' ONLY: TAXIWAY P CONTRACTOR TO RETURN R5 WASTE STORAGE SITE TO EXISTING GROUND, SEE NOTE 1.

START ADDITIVE ALTERNATE 2 EARTHWORK, STA. 1209+00, RT. 80' TO RT. 520'

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NOTES:

1. PROPOSED RUNWAY 5 PAVING CONTRACTOR (OTHERS) TO STOCK PILE CLAY WASTE SOILS AT THIS LOCATION. TAXIWAY P EARTHWORK CONTRACTOR (THIS CONTRACT) TO HAUL AND INSTALL STORED SOILS TO THE TAXIWAY P CONSTRUCTION SITE AREA. SOILS TO BE HAULED FROM THIS RUNWAY 5 WASTE STORAGE AREA TO TAXIWAY P UNDER ADDITIVE ALTER. 2 CONTRACT ITEM AT152420 COMMON EXCAVATION.
2. WASTE CUT SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER ADDITIVE ALTERNATE 2 CONTRACT ITEM AT152420, COMMON EXCAVATION.

END ADDITIVE ALTERNATE 2 AREA
EARTHWORK, STA. 1212+00

RIGHT 80' TO RIGHT 520' ONLY:
TAXIWAY P CONTRACTOR TO
RETURN R5 WASTE STORAGE SITE
TO EXISTING GROUND, SEE NOTE 1.

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