

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	100 (I&2) I-9	WILL	59	51
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		MOT-01 OF 02		

## SUGGESTED STAGES OF CONSTRUCTION

### PRE-STAGE

1. INSTALL "CHANGEABLE MESSAGE BOARDS" PRIOR TO THE START OF CONSTRUCTION ACTIVITY ON FAI - 55.
2. REMOVE EXISTING PAVEMENT MARKING AND INSTALL TEMPORARY PAVEMENT MARKING AS SHOWN ON THE STAGED TYPICAL SECTIONS.

### STAGE I

1. ESTABLISH TRAFFIC CONTROL AS SHOWN ON STAGE I TYPICAL SECTION (NIGHT TIME ONLY-OPERATION). THIS WORK SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
2. MILL, PATCH, AND RESURFACE THE OUTSIDE SHOULDERS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
3. INSTALL PERMANENT PAVEMENT MARKING AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER, AND REOPEN ALL LANES TO TRAFFIC.

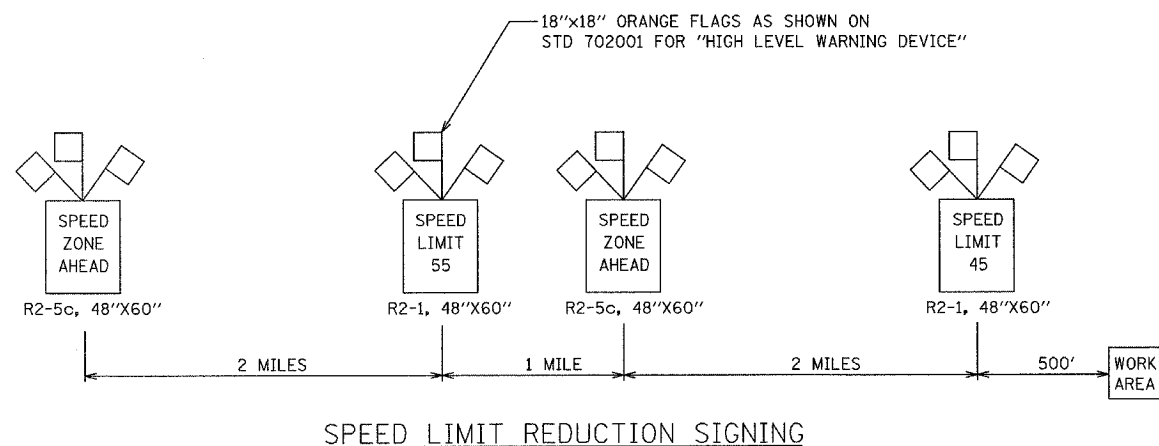
## LIST OF TRAFFIC CONTROL STATE AND DISTRICT 1 STANDARDS

### STATE

- 701101 OFF-ROAD OPERATIONS, MULTILANE, 4.5m (15') TO 600mm (24") FROM PAVEMENT EDGE
- 701400 APPROACH TO LANE CLOSURE FREEWAY/EXPRESSWAY
- 701401 LANE CLOSURE, FREEWAY/EXPRESSWAY
- 701411 LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS  $\geq$  45MPH
- 702001 TRAFFIC CONTROL DEVICES

### DISTRICT 1

- TC12 MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS
- TC13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- TC18 SIGNING FOR FLAGGING OPERATIONS AT WORK AREA OPENINGS
- TC22 TEMPORARY INFORMATION SIGNING



## TRAFFIC CONTROL - GENERAL NOTES

1. ONCE THE OUTSIDE SHOULDER IS MILLED, THE CONTRACTOR SHALL PATCH THE SHOULDER AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FILL ALL PATCH HOLES BEFORE OPENING THE ADJACENT LANE CLOSURE TO TRAFFIC. THIS WORK SHALL BE PAID FOR AS CLASS D PATCHES OF THE TYPE REQUIRED.

2. AT THE END OF EACH NIGHTLY SHIFT, THE CONTRACTOR SHALL SWEEP THE PAVEMENT SURFACE CLEAN AND THEN OPEN THE OUTSIDE LANE TO TRAFFIC BY THE TIME SPECIFIED IN THE SPECIAL PROVISION "KEEPING THE EXPRESSWAY OPEN TO TRAFFIC". SHOULDERS WHICH HAVE BEEN MILLED SHALL REMAIN CLOSED FROM THE START OF SHOULDER MILLING OPERATIONS UNTIL THE SHOULDER RESURFACING IS COMPLETED, OR AS DIRECTED BY THE ENGINEER.

3. THE REGULATORY SPEED LIMIT SHALL BE REDUCED TO 45 MPH IN THE DIRECTION OF TRAFFIC ADJACENT TO THE WORK AREA AS FOLLOWS:

A. IN CONJUNCTION WITH IMPLEMENTING A LANE OR SHOULDER CLOSURE, THE CONTRACTOR SHALL:

- SIMULTANEOUSLY COVER THE EXISTING REGULATORY SPEED LIMIT SIGNS AND ERECT 45 MPH REGULATORY SPEED LIMIT SIGNS (R2-1, 48" X 60") ADJACENT TO ALL EXISTING SPEED LIMIT SIGNS WITHIN THE WORK AREA LIMITS AS DIRECTED BY THE ENGINEER.

- SIMULTANEOUSLY ERECT SIGNS AS SHOWN IN THE "SPEED LIMIT REDUCTION SIGNING" DIAGRAM ON BOTH SIDES OF THE TRAVELED WAY AND COVER THE EXISTING REGULATORY SPEED LIMIT SIGNS WITHIN THE LIMITS OF THE "SPEED LIMIT REDUCTION SIGNING".

B. THE EXISTING SPEED LIMIT SIGNING OUTSIDE OF THE WORK AREA LIMITS AND BEYOND THE "SPEED LIMIT REDUCTION SIGNING" SHALL BE MAINTAINED, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

C. THE CONTRACTOR SHALL COVER THE 45 MPH REGULATORY SPEED LIMIT SIGNS DURING THE LABOR DAY HOLIDAY PERIOD BETWEEN FRIDAY, SEPTEMBER 1, 2006 AND MONDAY, SEPTEMBER 4, 2006. THE CONTRACTOR SHALL UNCOVER THE EXISTING SPEED LIMIT SIGNS SO THAT THESE SIGNS WILL BE VISIBLE TO THE MOTORING PUBLIC DURING THIS PERIOD. ONCE THE LABOR DAY HOLIDAY PERIOD IS OVER, THE CONTRACTOR SHALL COVER THE EXISTING SPEED LIMIT SIGNS AND UNCOVER THE WORK AREA REGULATORY 45 MPH SPEED LIMIT SIGNS AS DIRECTED BY THE ENGINEER.

D. THE CONTRACTOR SHALL UNCOVER THE EXISTING SPEED LIMIT SIGNS AND REMOVE THE 45 MPH REGULATORY SPEED LIMIT SIGNS SIMULTANEOUS WITH THE REMOVAL OF TRAFFIC CONTROL DEVICES IN ANY SEGMENT OF ROADWAY WHERE TRAFFIC CONTROL IS NO LONGER NEEDED.

E. THE "WORK ZONE SPEED LIMIT 55 RESUMES" SIGNING SHOWN ON STANDARD 701401 SHALL NOT BE ERECTED.

F. THE COST OF THIS WORK (NOTE 3) SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".

4. THE CONTRACTOR SHALL ERECT REGULATORY HIGHWAY SIGNS R4-5 (48"x60") "TRUCKS USE LEFT LANE" AS FOLLOWS:

- 1/2 MILE AND 1 MILE ON BOTH SIDES OF THE TRAFFIC LANES IN ADVANCE OF THE WORK AREA.

- 1/2 MILE INTERVALS ON BOTH SIDES OF THE TRAFFIC LANES ADJACENT TO THE WORK AREA AND AT EACH ENTRANCE RAMP.

- THE PLACEMENT OF THE SIGNS SHALL BE AS DIRECTED BY THE ENGINEER AND THE COST OF THIS WORK WILL BE PAID FOR AS "TEMPORARY INFORMATION SIGNING"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55  
US RTE. 30 TO WEBER ROAD  
SUGGESTED MAINTENANCE OF  
TRAFFIC CONTROL AND PROTECTION  
GENERAL NOTES AND DETAILS

SCALE: NONE  
DATE: 03-15-06

DRAWN BY: DM  
CHECKED BY: DVS



FINAL