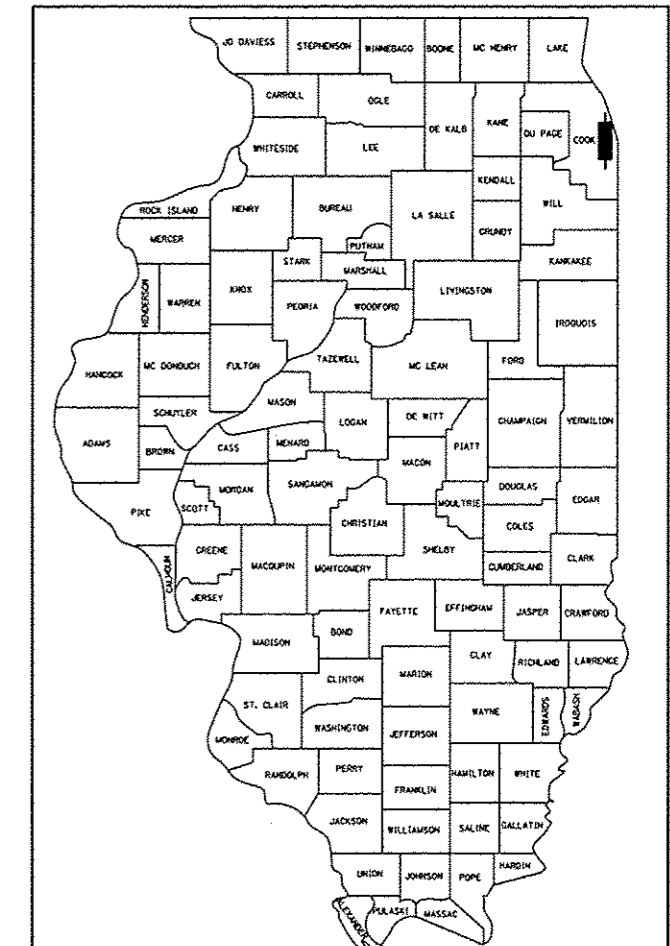


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.1RS-5	COOK	34	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 62B59		

D-91-061-16



LOCATION OF SECTION INDICATED THUS: - [Symbol]

**PROPOSED
HIGHWAY PLANS**

**FAI ROUTE 94: (BISHOP FORD EXPRESSWAY)
INTERSTATE 57 TO
MARTIN LUTHER KING DRIVE
RESURFACING
SECTION: 1516.1RS-5
PROJECT: ACNHPP-0094(405)
COOK COUNTY
C-91-061-16**

LOCATION MAP

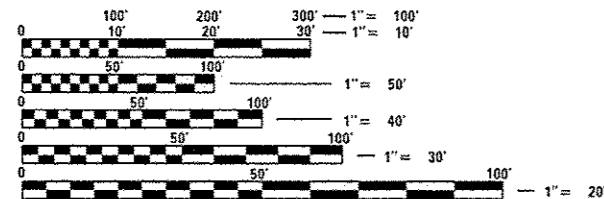
N.T.S.

FOR INDEX OF SHEETS, SEE SHEET NO. 2

TRAFFIC DATA

SPEED LIMIT: 55 M.P.H.
ADT: 149,200 (2014)

PROJECT LOCATED IN
THE CITY OF CHICAGO



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

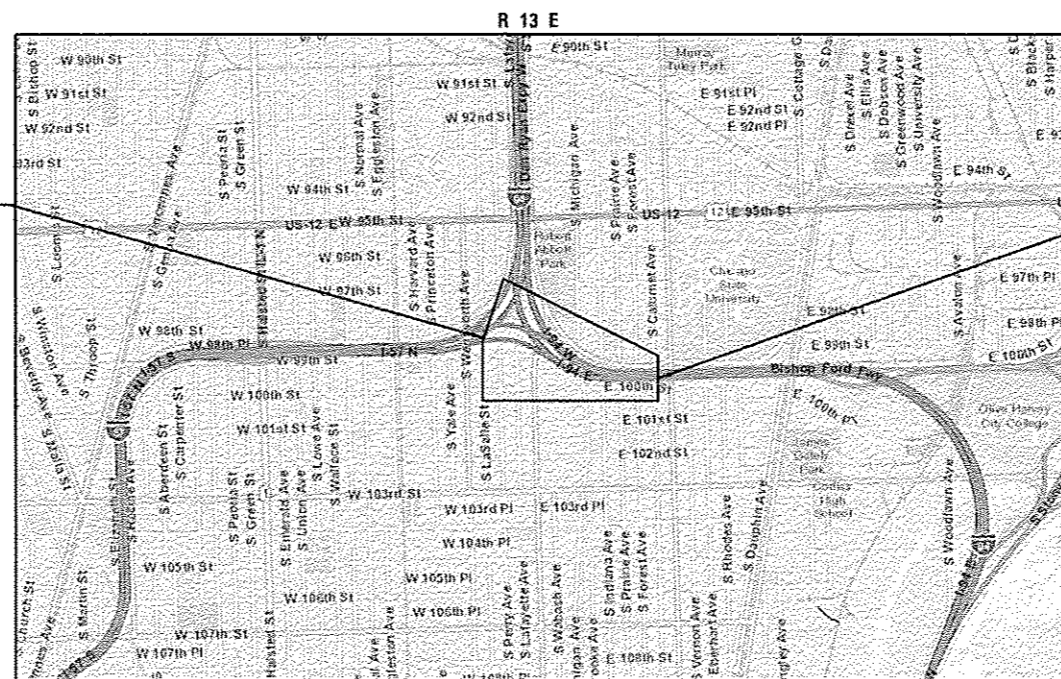
DIGGER
CHICAGO UTILITY ALERT NETWORK
1-312-744-7000

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: BENJAMIN HARTMAN, P.E. (815) 385-1778

PROJECT MANAGER: KARI SMITH, P.E. (847) 705-4437

CONTRACT NO. 62B59



PROJECT BEGINS
STA. 204+62 EB
STA. 124+60 WB

PROJECT ENDS
STA. 245+45 EB
STA. 143+05 WB

GROSS LENGTH OF PROJECT = 5,925 FT = 1.123 MI
NET LENGTH OF PROJECT = 5,925 FT = 1.123 MI

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 17, 2016
John Fortman
REGIONAL ENGINEER

Mary Le 2016
Mahreen M. Addis, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

Mary Le 2016
Tracey
DIRECTOR OF PROGRAM DEVELOPMENT

PROFESSIONAL ENGINEER'S SIGN & SEAL

Benjamin D. Hartman
BENJAMIN D. HARTMAN, P.E.
EXPIRES: 11-30-17

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

1	TITLE SHEET
2	INDEX OF SHEETS, LIST OF STATE STANDARDS, DISTRICT ONE DETAILS AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-6	TYPICAL SECTIONS
7-8	SUGGESTED MAINTENANCE OF TRAFFIC
9-11	REMOVAL PLANS
12-14	RESURFACING PLANS
15-17	PAVEMENT MARKING PLANS
18	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD08)
19	PAVEMENT PATCHING FOR HMA SURFACE PAVEMENT (BD22)
20	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD 24)
21	BUTT JOINT AND HMA TAPER DETAILS (BD32)
22	ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC08)
23	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE (TC09)
24-25	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS (TC12)
26	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC17)
27	FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS (TC18)
28	EXISTING ROUND LOOP INSTALLATION (TY-ITSC-418*1)
29	EXISTING ROUND LOOP INSTALLATION TYPICALS (TY-ITSC-418*2)
30	RECTANGULAR INDUCTION LOOP TYPICAL (TY-ITSC-418*3)
31	LOOP STATUS REPORT (TY-ITSC-418*7)
32	DIVE HOLE DUCT SYSTEM (TY-ITSC-418*8)
33	ONE LANE COUNT SYSTEM (TY-ITSC-400*31)
34	THREE LANE COUNT STATION (TY-ITSC-400*32A)

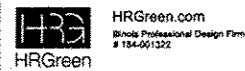
LIST OF STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
642001-02	SHOULDER RUMBLE STRIPS, 16 IN.
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701400-08	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-09	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45 MPH
701428-01	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701446-07	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
701901-05	TRAFFIC CONTROL DEVICES

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 OR DIGGER AT 312-744-700 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- REMOVAL OF DAMAGED MATERIALS WILL INCLUDE DISMANTLING, LOADING, HAULING AND DISPOSAL. THE WORK SITE WILL BE LEFT IN A SAFE AND ORDERLY CONDITION.
- NO MATERIAL SHOULD BE LEFT ON OR NEAR THE ROADWAY DURING NON-WORKING HOURS.
- ANY NECESSARY TRAFFIC CONTROL WILL BE PROVIDED BY THE CONTRACTOR.
- FOR PATCHING OPERATIONS, THE CONTRACTOR SHALL PATCH FIRST PER DISTRICT ONE DETAIL (BD-22).
- VERTICAL CLEARANCES OF BRIDGES OVER I-94 SHALL BE MAINTAINED. HMA SURFACE REMOVAL AND PROPOSED HMA SURFACE THICKNESS WILL BE VARIED AS DIRECTED BY THE ENGINEER IF THE EXISTING HMA DEPTH IS LESS THAN 2". THE CONTRACTOR SHALL VERIFY ALL BRIDGE CLEARANCES BEFORE AND AFTER CONSTRUCTION AND FURNISH RESULTS TO THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE LUMP SUM PRICE FOR CONSTRUCTION LAYOUT.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE RESIDENT ENGINEER SHALL CONTACT THE EXPRESSWAY FIELD ENGINEER AT 847-705-4153 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT 847-705-4155 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS WHEN USING ARTIFICIAL LIGHTING DURING NIGHT OPERATIONS TO PREVENT ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- BUTT JOINTS WILL BE INSTALLED AT THE END OF ALL RESURFACING SECTIONS TO MATCH EXISTING PAVEMENT. SEE DISTRICT ONE DETAIL (BD-32) - BUTT JOINT AND HMA TAPER DETAILS FOR ALL FCC AND HMA BUTT JOINTS IDENTIFIED IN THE RESURFACING PLANS.
- TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORSEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL RESPOND TO THE ENGINEER'S REQUEST WITHIN THIRTY (30) MINUTES FROM THE TIME OF THE NOTIFICATION.
- THE CONTRACTOR MAY WORK IN THE EASTBOUND AND WESTBOUND LANES SIMULTANEOUSLY.
- THE CONTRACTOR SHALL SWEEP THE PAVEMENT SURFACE, PLACE TEMPORARY PAVEMENT MARKINGS AND REMOVE TRAFFIC CONTROL AT THE END OF EACH NIGHT SHIFT PER ARTICLE 107.15 OF THE STANDARD SPECIFICATIONS. VERTICAL BARRICADES WILL REMAIN IN PLACE ALONG THE EDGES OF PAVEMENT AS SHOWN IN THE SUGGESTED MAINTANANCE OF TRAFFIC SHEETS UNTIL THE SURFACE COURSE AND PROPOSED PAVEMENT MARKING EDGE LINES HAVE BEEN COMPLETED.
- STRIP REFLECTIVE CRACK CONTROL TREATMENT WILL BE INSTALLED IMMEDIATELY IN FRONT OF THE HMA SURFACING OPERATION SO TRAFFIC WILL NOT DISTURB IT BEFORE BEING COVERED WITH HMA. ANY CRACK CONTROL TREATMENT DISTURBED BY TRAFFIC WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS WAS DEVELOPED FROM PREVIOUS PLANIMETRICS AND AERIAL PHOTOGRAPHY FURNISHED BY THE DEPARTMENT AND IS NOT THE RESULT OF A GROUND SURVEY. THEREFORE, THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS IS FOR REFERENCE PURPOSES ONLY. THE RELATIVE ACCURACY OF THE INFORMATION IS UNKNOWN AND CANNOT BE GUARANTEED. THE CONTRACTOR MAY BE REQUIRED TO ADJUST LAYOUT TO MATCH ACTUAL FIELD CONDITIONS AND THE INTENT OF THE PLANS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING CORRECTIVE MEASURES TO ALLEVIATE ANY POTENTIAL DRAINAGE ISSUES OR CONCERNS DURING MILLING AND PAVING OPERATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE VARIOUS SURFACE REMOVAL AND ASPHALT ITEMS.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: bh@hr.com
 CLIENT: ILLINOIS
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS,
 DISTRICT ONE DETAILS AND GENERAL NOTES**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	2
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	

REV.

PAYCODE	ITEM DESCRIPTION	UNIT	URBAN	90% FEDERAL
			TOTAL QUANTITIES	10% STATE 0005
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	100	100
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	2	2
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	2	2
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	2	2
25200110	SODDING, SALT TOLERANT	SO YD	100	100
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	50	50
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	22,819	22,819
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	120	120
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	368	368
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1,010	1,010
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SO YD	12,000	12,000
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	10,750	10,750
44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SO YD	18,100	18,100
44002236	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 9"	SO YD	730	730
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	150	150
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	290	290
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	290	290
44300200	STRIP-REFLECTIVE CRACK CONTROL TREATMENT	FOOT	11,585	11,585
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	20	20
60404910	FRAMES AND GRATES, TYPE 20	EACH	8	8
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	2	2
63500105	DELINEATORS	EACH	45	45
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	9,250	9,250
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6
67100100	MOBILIZATION	LSUM	1	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	8	8
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	34,925	34,925

27

* SPECIALTY ITEMS

COMPANY NAME: HRGreen
PROJECT CONTACT: HRGreen.com
CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION
FILE NAME: 06150046-05-aht-sum.dgn
PLOT DATE: 3/18/2016



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

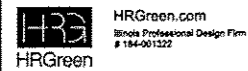
SUMMARY OF QUANTITIES	
SCALE: N.T.S.	SHEET NO. 1 OF 2 SHEETS
STA. TO STA.	

F.A.I. RTE. 94	SECTION 1516.1RS-5	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 3
CONTRACT NO. 62B59			ILLINOIS FED. AID PROJECT	

PAYCODE	ITEM DESCRIPTION	UNIT	URBAN TOTAL QUANTITIES	90% FEDERAL 10% STATE 0005
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	19,295	19,295
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	2,520	2,520
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	970	970
* 78004220	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	FOOT	1,710	1,710
* 78004240	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	FOOT	495	495
* 78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	31,840	31,840
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	280	280
* 78005120	EPOXY PAVEMENT MARKING - LINE 5"	FOOT	3,110	3,110
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	20	20
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	280	280
* 81028730	UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 1 1/4" DIA.	FOOT	50	50
* X0322247	MAINTENANCE OF EXISTING TRAFFIC SURVEILLANCE	LSUM	1	1
X4060002	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, 12.5, N80	TON	2,030	2,030
X4060006	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 12.5, N80	TON	3,230	3,230
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	5	5
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	LSUM	1	1
X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	31,840	31,840
X7030035	WET REFLECTIVE TEMPORARY TAPE TYPE III, 5 INCH	FOOT	3,110	3,110
X7030045	WET REFLECTIVE TEMPORARY TAPE TYPE III, 8 INCH	FOOT	8,680	8,680
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	1,710	1,710
* X7830075	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	495	495
* X8730312	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 18 4/C, TWISTED, SHIELDED	FOOT	3,227	3,227
* X8850102	INDUCTION LOOP	FOOT	247	247
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	580	580
Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	70	70
Z0034105	MATERIAL TRANSFER DEVICE	TON	5,260	5,260

* SPECIALTY ITEMS

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 PROJECT CONTACT: bhartma
 CLIENT: HRGreen
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

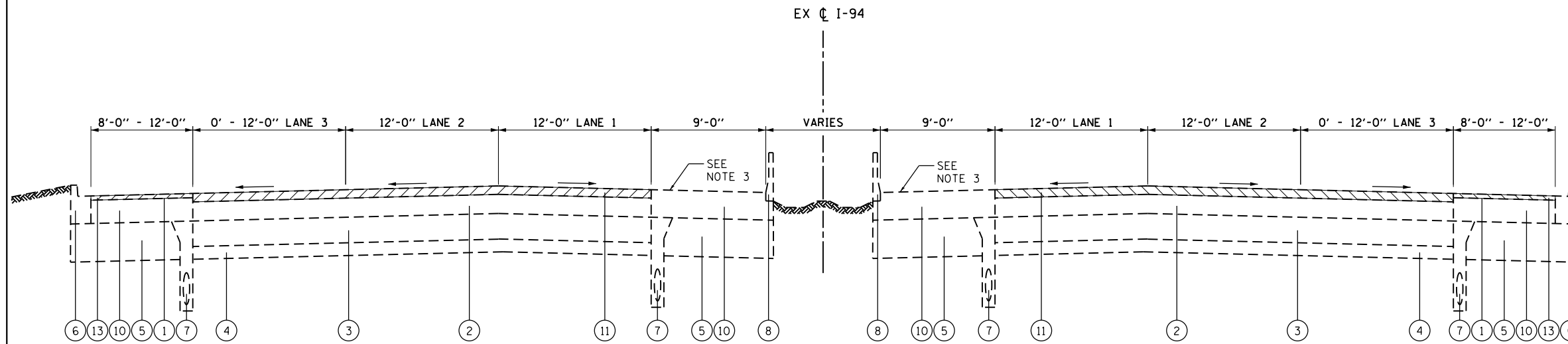
SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516, IRS-5	COOK	34	4
FED. ROAD DIST. NO.			CONTRACT NO. 62B59	
ILLINOIS FED. AID PROJECT				

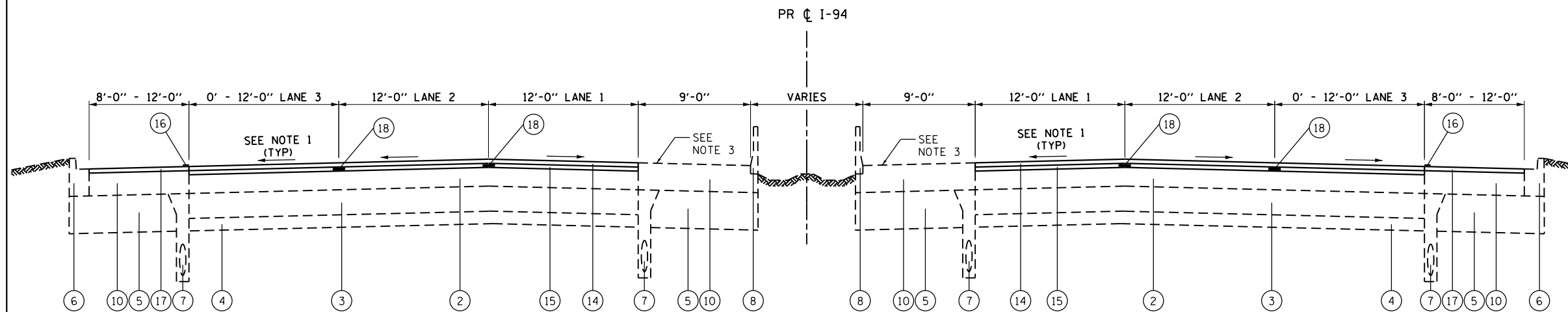
LEGEND

- ① EXISTING HMA OVERLAY, VARIES 1.5"
- ② EXISTING HMA OVERLAY, VARIES 7"-10"
- ③ EXISTING P.C.C. BASE, 10"
- ④ EXISTING GRANULAR SUB-BASE
- ⑤ EXISTING CRUSHED STONE, THICKNESS VARIES
- ⑥ EXISTING CURB & GUTTER, TYPE VARIES
- ⑦ EXISTING 6" PIPE UNDERDRAIN
- ⑧ EXISTING SINGLE FACE CONCRETE BARRIER WALL
- ⑨ EXISTING HMA SHOULDER, VARIES 10"-15"
- ⑩ EXISTING P.C.C. SHOULDER, 9"
- ⑪ HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- ⑫ HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- ⑬ HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ⑭ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 12.5, N80; 2"
- ⑮ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, 12.5, N80; 2"
- ⑯ PROPOSED SHOULDER RUMBLE STRIP (STD. 642001-01)
- ⑰ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑱ STRIP REFLECTIVE CRACK CONTROL TREATMENT



I-94 EXISTING TYPICAL SECTION

STA. 124+60 TO STA. 143+05 (4" SURF REM)
 STA. 220+25 TO STA. 245+45 (4" SURF REM)
 STA. 334+50 TO STA. 342+13 (4" SURF REM)
 STA. 415+55 TO STA. 440+89 (4" SURF REM)



I-94 PROPOSED TYPICAL SECTION

STA. 124+60 TO STA. 143+05 (4" SURF REM)
 STA. 220+25 TO STA. 245+45 (4" SURF REM)
 STA. 334+50 TO STA. 342+13 (4" SURF REM)
 STA. 415+55 TO STA. 440+89 (4" SURF REM)

NOTES:

1. THE CONTRACTOR SHALL MAINTAIN THE EXISTING CROSS SLOPES.
2. THE COST OF HAND REMOVAL OF HMA SURFACE AROUND CASTINGS SHALL BE INCLUDED IN THE COST OF HMA SURFACE REMOVAL OF THE THICKNESS SPECIFIED.
3. SEE RESURFACING PLANS FOR LOCATIONS OF PCC AND HMA SHOULDERS.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD./IN.

THE AC TYPE FOR POLYMERIZED HMA MIXTURES SHALL BE "SBS/SBR PG 76 -22", FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (OMP)
MAINLINE RESURFACING		
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 12.5, N80; 2"	3.5% @ 80 GYR.	OCP
POLYMERIZED HMA BINDER COURSE, STONE MATRIX ASPHALT, 12.5, N80; 2"	3.5% @ 80 GYR.	OCP
MAINLINE SHOULDER RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm); 1 1/2"	4.0% @ 70 GYR.	OCP
RAMP RESURFACING		
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 12.5, N80; 2"	3.5% @ 80 GYR.	OCP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm); 10"	4.0% @ 70 GYR.	QC/OA
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm); 9"	4.0% @ 70 GYR.	QC/OA
OMP DESIGNATIONS: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (OCP)		

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

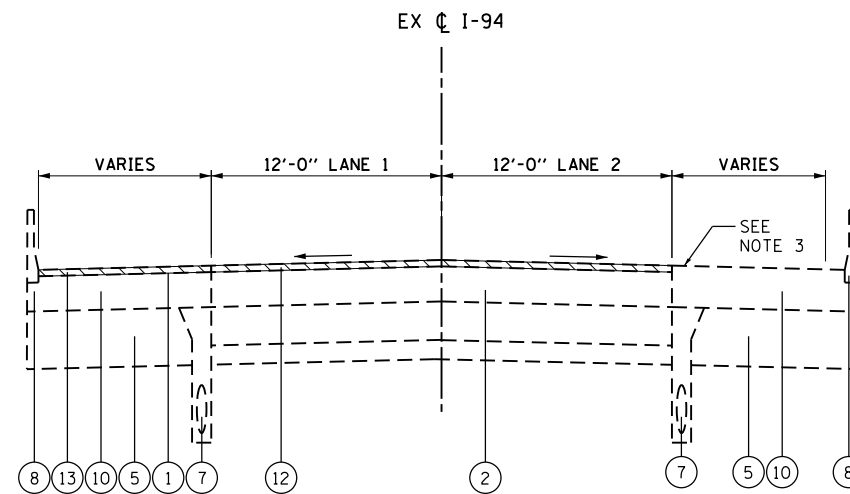
**EXISTING & PROPOSED TYPICAL SECTIONS
 BISHOP FORD EXPRESSWAY (I-94)**

SCALE: N.T.S. SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	5
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	

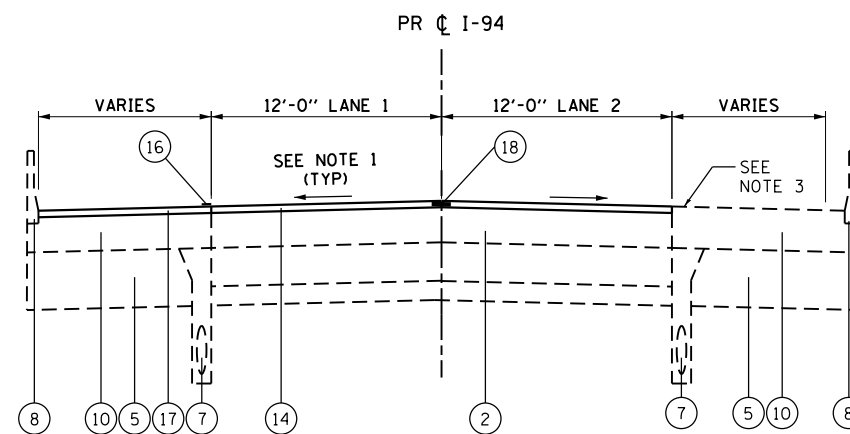
LEGEND

- ① EXISTING HMA OVERLAY, VARIES 1.5"
- ② EXISTING HMA OVERLAY, VARIES 7"-10"
- ③ EXISTING P.C.C. BASE, 10"
- ④ EXISTING GRANULAR SUB-BASE
- ⑤ EXISTING CRUSHED STONE, THICKNESS VARIES
- ⑥ EXISTING CURB & GUTTER, TYPE VARIES
- ⑦ EXISTING 6" PIPE UNDERDRAIN
- ⑧ EXISTING SINGLE FACE CONCRETE BARRIER WALL
- ⑨ EXISTING HMA SHOULDER, VARIES 10"-15"
- ⑩ EXISTING P.C.C. SHOULDER, 9"
- ⑪ HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- ⑫ HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- ⑬ HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ⑭ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 12.5, N80; 2"
- ⑮ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, 12.5, N80; 2"
- ⑯ PROPOSED SHOULDER RUMBLE STRIP (STD. 642001-01)
- ⑰ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑱ STRIP REFLECTIVE CRACK CONTROL TREATMENT



I-94 EXISTING TYPICAL SECTION

STA. 204+62 TO STA. 220+25 (2" SURF REM)
 STA. 311+09 TO STA. 334+50 (2" SURF REM)
 STA. 407+20 TO STA. 415+55 (2" SURF REM)



I-94 PROPOSED TYPICAL SECTION

STA. 204+62 TO STA. 220+25 (2" SURF REM)
 STA. 311+09 TO STA. 334+50 (2" SURF REM)
 STA. 407+20 TO STA. 415+55 (2" SURF REM)

NOTES:

1. THE CONTRACTOR SHALL MAINTAIN THE EXISTING CROSS SLOPES.
2. THE COST OF HAND REMOVAL OF HMA SURFACE AROUND CASTINGS SHALL BE INCLUDED IN THE COST OF HMA SURFACE REMOVAL OF THE THICKNESS SPECIFIED.
3. SEE RESURFACING PLANS FOR LOCATIONS OF PCC AND HMA SHOULDERS.

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen
 DATE PLOTTED: 3/18/2016 12:08:38 PM
 FILE NAME: 86150046-05-sht-typ-02.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: plot.tbl



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PLOT SCALE = N.T.S.	CHECKED - TH	REVISED -
PLOT DATE = 3/18/2016	DATE - 03/18/16	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

EXISTING & PROPOSED TYPICAL SECTIONS BISHOP FORD EXPRESSWAY (I-94)	
SCALE: N.T.S.	SHEET NO. 2 OF 2 SHEETS
STA.	TO STA.



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	6
CONTRACT NO.			62B59	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

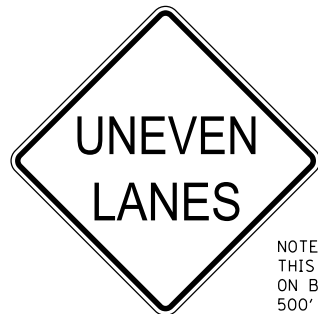
NOTES:

- SEQUENCE OF CONSTRUCTION: PAVEMENT PATCHING, HMA SURFACE REMOVAL AND PROPOSED HMA COURSES AS DETAILED IN EACH OPERATION SECTION. WORK CAN OCCUR CONCURRENTLY, BUT SEQUENCE SHALL BE MAINTAINED AND NOT CONFLICT. THE TWO LANE SECTION OF I-94 WILL FOLLOW THE SEQUENCE OF LANES 1 AND 3 IN THE OPERATION SECTIONS. RAMP WORK WILL BE COMPLETED USING DISTRICT DETAIL TC-8 - FREEWAY AND EXIT RAMP CLOSURE DETAILS.
- LANE AND RAMP CLOSURES ALLOWED AS STIPULATED IN THE SPECIAL PROVISIONS.
- LANE CLOSURES PER STANDARDS 701400, 701401 AND 701446.
- REDUCE LANE WIDTH USING 55:1 TAPER PER DISTRICT DETAIL TC-9 - TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE AND MULTI-LANE WEAVE. A W5-1 (48 x 48) ROAD NARROWS SIGN WILL BE ERECTED 500' IN ADVANCE OF THE TAPER.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF UP TO 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED AT A MINIMUM OF 1V:3H.
- PARTIAL MILLING OF PAVEMENT FOR AN OPERATION WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE PAID FOR AT THE FINAL MILLING THICKNESS SHOWN ON THE TYPICAL SECTIONS.
- REMOVAL OF TEMPORARY PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "SHORT TERM PAVEMENT MARKING REMOVAL".
- EXISTING OR TEMPORARY PAVEMENT MARKINGS REMOVED DURING MILLING OPERATIONS WILL NOT BE PAID FOR SEPARATELY.
- CASTINGS AND CONCRETE HANDHOLES EXPOSED THAT ARE NOT BEING ADJUSTED WITH "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)" IN TRAVEL LANES SHALL BE PROTECTED PER APPLICABLE PORTIONS OF ARTICLE 603.07 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE VARIOUS HMA BINDER AND SURFACE COURSES PLACED.
- THE CONTRACTOR SHALL INSTALL AND MAINTAIN FOUR CHANGEABLE MESSAGE SIGNS FOR THIS CONTRACT.

LEGEND

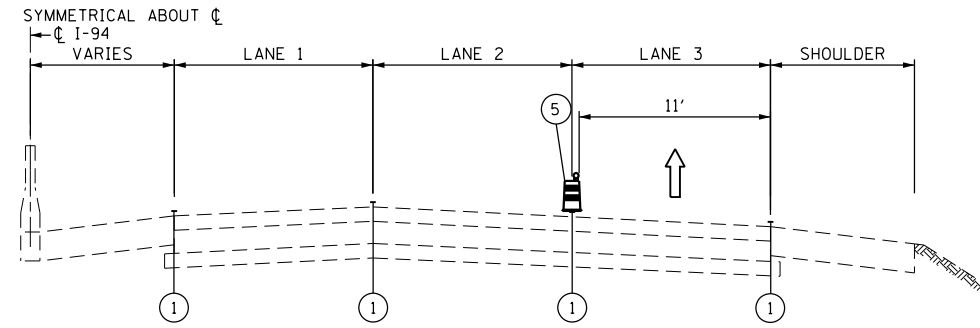
- ① EXISTING PAVEMENT MARKING
- ② EPOXY PAVEMENT MARKING, LINE 5" (10' DASH - 30' SKIP)
- ③ EPOXY PAVEMENT MARKING, LINE 4" (YELLOW LEFT & WHITE RIGHT) INSTALLED WITHIN 24 HOURS OF MILLING OR BINDER PLACEMENT.
- ④ PROPOSED PAVEMENT MARKING. SEE PAVEMENT MARKING PLANS.
- ⑤ VERTICAL BARRICADES WITH STEADY BURNING MONO DIRECTIONAL LIGHT AT 100' C-C SPACING. VERTICAL BARRICADES ALONG SHOULDERS WILL REMAIN IN PLACE BETWEEN OPERATIONS TO PREVENT SHOULDER RIDING. DURING EACH OPERATION VERTICAL BARRICADES WILL THEN BE USED TO SEPARATE THE WORK ZONE AND OPERATING LANES.

-  HMA SURFACE REMOVAL
-  HMA SURFACE COURSE

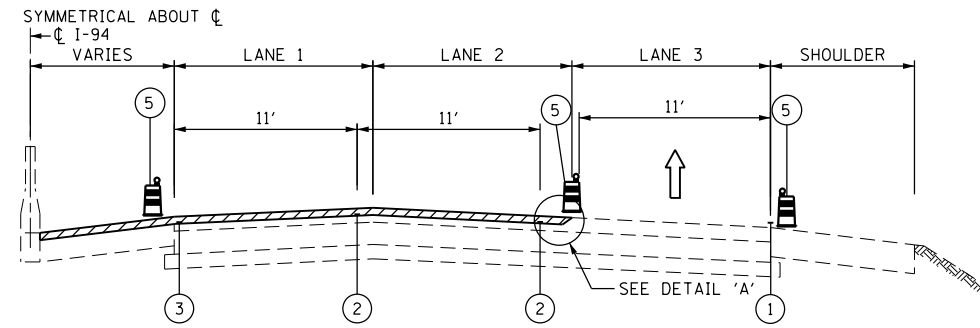


W8-11
48" x 48"

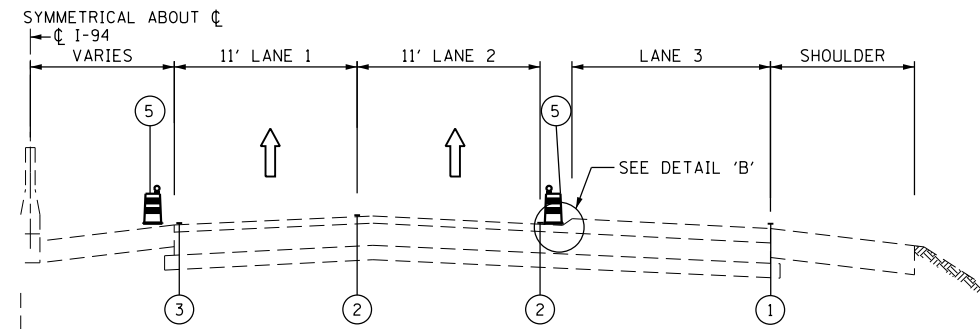
NOTE:
THIS SIGN SHALL BE INSTALLED ON BOTH SIDES OF THE ROADWAY 500' IN ADVANCE OF AREAS WHERE THERE IS A GRADE DIFFERENTIAL BETWEEN LANES, AFTER EACH ENTRANCE RAMP AND A MINIMUM OF EVERY MILE.



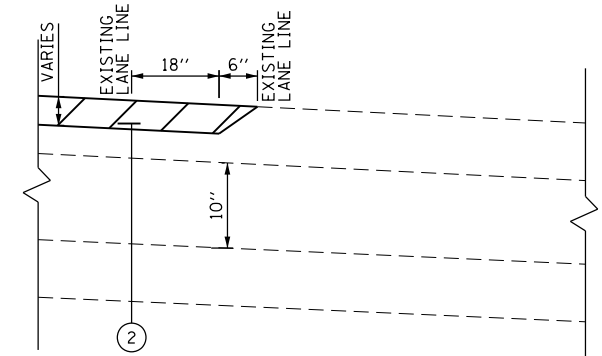
OPERATION ONE - I-94
PATCH LANES 1 & 2 AND INSIDE SHOULDER



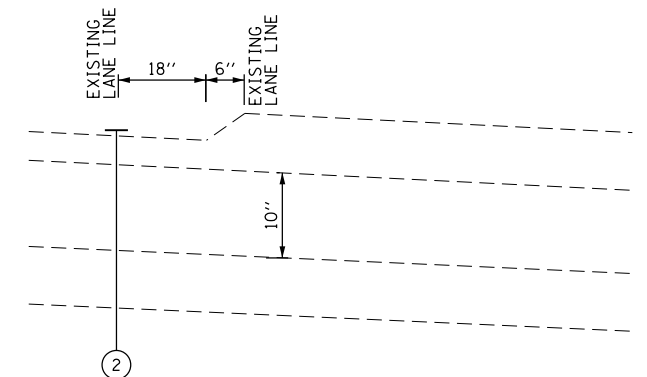
OPERATION TWO - I-94
HMA SURFACE REMOVAL - LANES 1 & 2 & INSIDE SHOULDER



OPERATION THREE - I-94
PATCH LANE 3 AND OUTSIDE SHOULDER



DETAIL 'A'



DETAIL 'B'

COMPANY NAME: HRGreen.com
PROJECT CONTACT: bhartma
DATE PLOTTED: 4/20/2016 12:23:54 PM
FILE NAME: 86150046-05-sht-mot-01.dgn
PLOT DRIVER: ILpdf.plt
PEN TABLE: plot.tbl



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PLOT SCALE = N.T.S.	CHECKED - TH	REVISED -
PLOT DATE = 4/20/2016	DATE - 03/18/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED MAINTENANCE OF TRAFFIC
BISHOP FORD EXPRESSWAY (I-94)

SCALE: N.T.S. SHEET NO. 1 OF 2 SHEETS STA. TO STA.



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	7
CONTRACT NO.			62B59	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

NOTES:

- SEQUENCE OF CONSTRUCTION: PAVEMENT PATCHING, HMA SURFACE REMOVAL AND PROPOSED HMA COURSES AS DETAILED IN EACH OPERATION SECTION. WORK CAN OCCUR CONCURRENTLY, BUT SEQUENCE SHALL BE MAINTAINED AND NOT CONFLICT. THE TWO LANE SECTION OF I-94 WILL FOLLOW THE SEQUENCE OF LANES 1 AND 3 IN THE OPERATION SECTIONS. RAMP WORK WILL BE COMPLETED USING DISTRICT DETAIL TC-8 - FREEWAY AND EXIT RAMP CLOSURE DETAILS.
- LANE AND RAMP CLOSURES ALLOWED AS STIPULATED IN THE SPECIAL PROVISIONS.
- LANE CLOSURES PER STANDARDS 701400, 701401 AND 701446.
- REDUCE LANE WIDTH USING 55:1 TAPER PER DISTRICT DETAIL TC-9 - TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE AND MULTI-LANE WEAVE. A W5-1 (48 x 48) ROAD NARROWS SIGN WILL BE ERECTED 500' IN ADVANCE OF THE TAPER.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF UP TO 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED AT A MINIMUM OF 1V:3H.
- PARTIAL MILLING OF PAVEMENT FOR AN OPERATION WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE PAID FOR AT THE FINAL MILLING THICKNESS SHOWN ON THE TYPICAL SECTIONS.
- REMOVAL OF TEMPORARY PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "SHORT TERM PAVEMENT MARKING REMOVAL".
- EXISTING OR TEMPORARY PAVEMENT MARKINGS REMOVED DURING MILLING OPERATIONS WILL NOT BE PAID FOR SEPARATELY.
- CASTINGS AND CONCRETE HANDHOLES EXPOSED THAT ARE NOT BEING ADJUSTED WITH "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)" IN TRAVEL LANES SHALL BE PROTECTED PER APPLICABLE PORTIONS OF ARTICLE 603.07 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE VARIOUS HMA BINDER AND SURFACE COURSES PLACED.
- THE CONTRACTOR SHALL INSTALL AND MAINTAIN FOUR CHANGEABLE MESSAGE SIGNS FOR THIS CONTRACT.

LEGEND

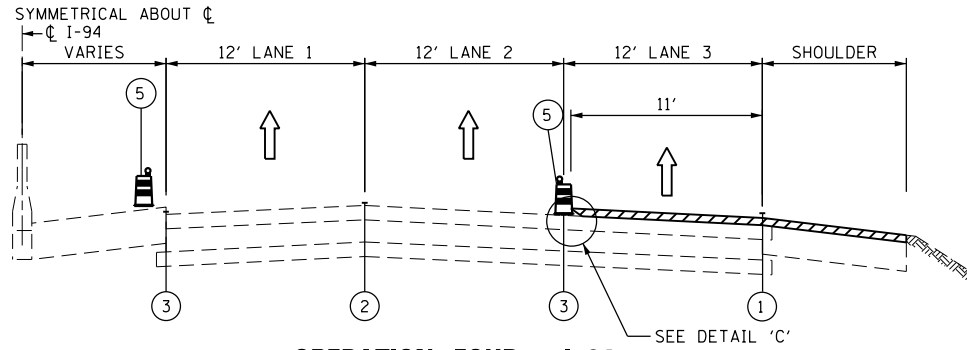
- ① EXISTING PAVEMENT MARKING
- ② EPOXY PAVEMENT MARKING, LINE 5" (10' DASH - 30' SKIP)
- ③ EPOXY PAVEMENT MARKING, LINE 4" (YELLOW LEFT & WHITE RIGHT) INSTALLED WITHIN 24 HOURS OF MILLING OR BINDER PLACEMENT.
- ④ PROPOSED PAVEMENT MARKING. SEE PAVEMENT MARKING PLANS.
- ⑤ VERTICAL BARRICADES WITH STEADY BURNING MONO DIRECTIONAL LIGHT AT 100' C-C SPACING. VERTICAL BARRICADES ALONG SHOULDERS WILL REMAIN IN PLACE BETWEEN OPERATIONS TO PREVENT SHOULDER RIDING. DURING EACH OPERATION VERTICAL BARRICADES WILL THEN BE USED TO SEPARATE THE WORK ZONE AND OPERATING LANES.

-  HMA SURFACE REMOVAL
-  HMA BINDER/SURFACE COURSE



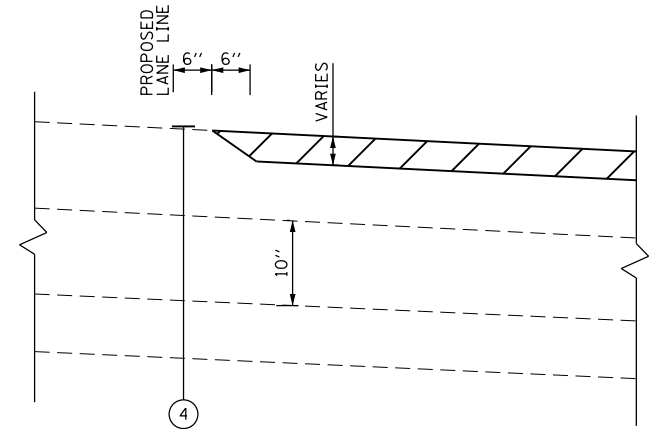
W8-11
48" x 48"

NOTE:
THIS SIGN SHALL BE INSTALLED ON BOTH SIDES OF THE ROADWAY 500' IN ADVANCE OF AREAS WHERE THERE IS A GRADE DIFFERENTIAL BETWEEN LANES, AFTER EACH ENTRANCE RAMP AND A MINIMUM OF EVERY MILE.

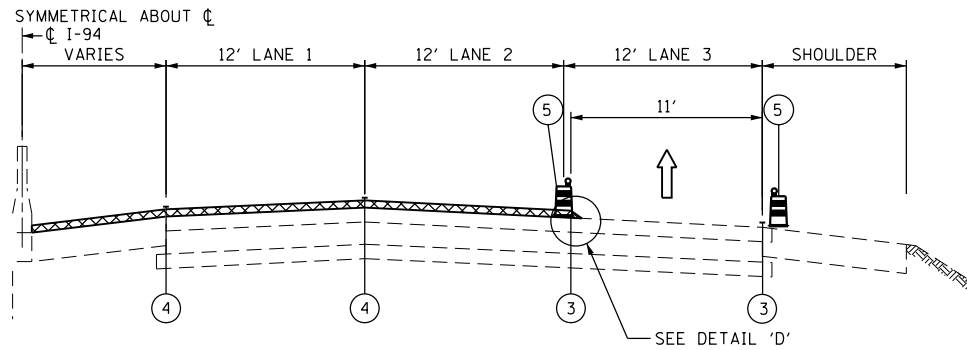


OPERATION FOUR - I-94

HMA SURFACE REMOVAL - LANE 3 & OUTSIDE SHOULDER

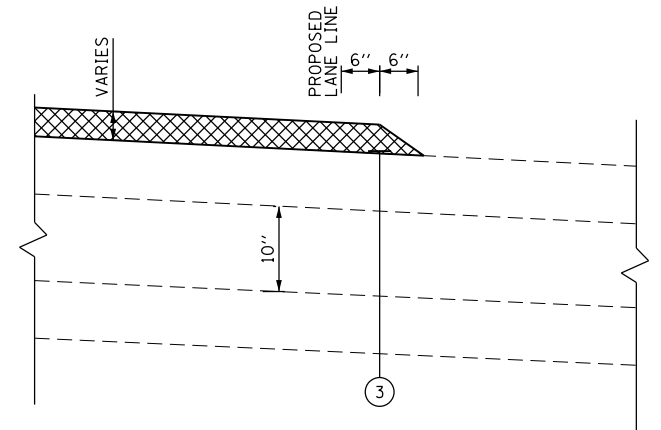


DETAIL 'C'

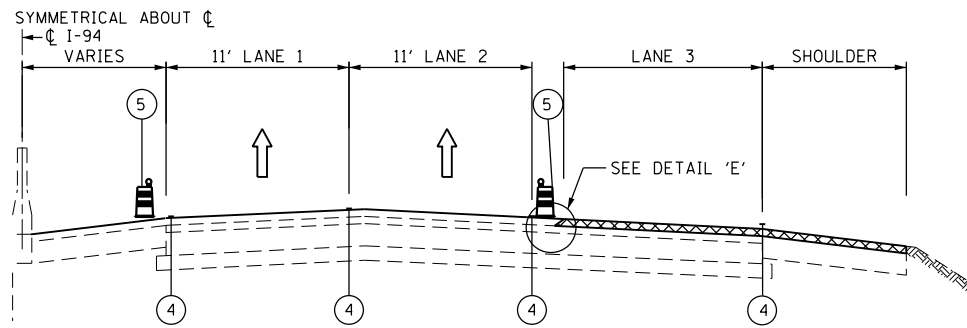


OPERATION FIVE - I-94

PLACE HOT-MIX ASPHALT SURFACE/BINDER - LANES 1 & 2 & INSIDE SHOULDER

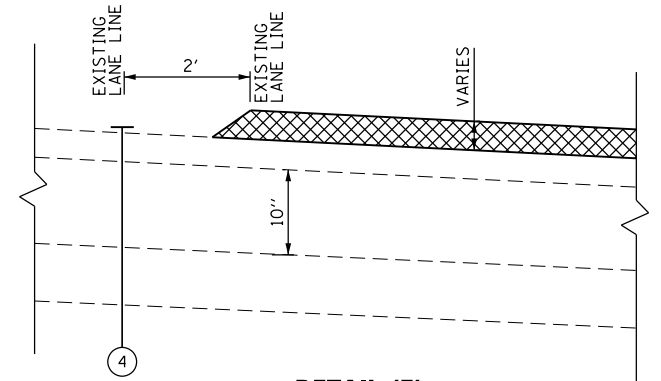


DETAIL 'D'



OPERATION SIX - I-94

PLACE HOT-MIX ASPHALT SURFACE/BINDER - LANE 3 & OUTSIDE SHOULDER



DETAIL 'E'

COMPANY NAME: HRGreen.com
PROJECT CONTACT: bhartma
DATE PLOTTED: 4/20/2016 12:23:58 PM
FILE NAME: 86150046-05-sht-mot-02.dgn
PLOT DRIVER: ILpdf.plt
PEN TABLE: plot.tbl



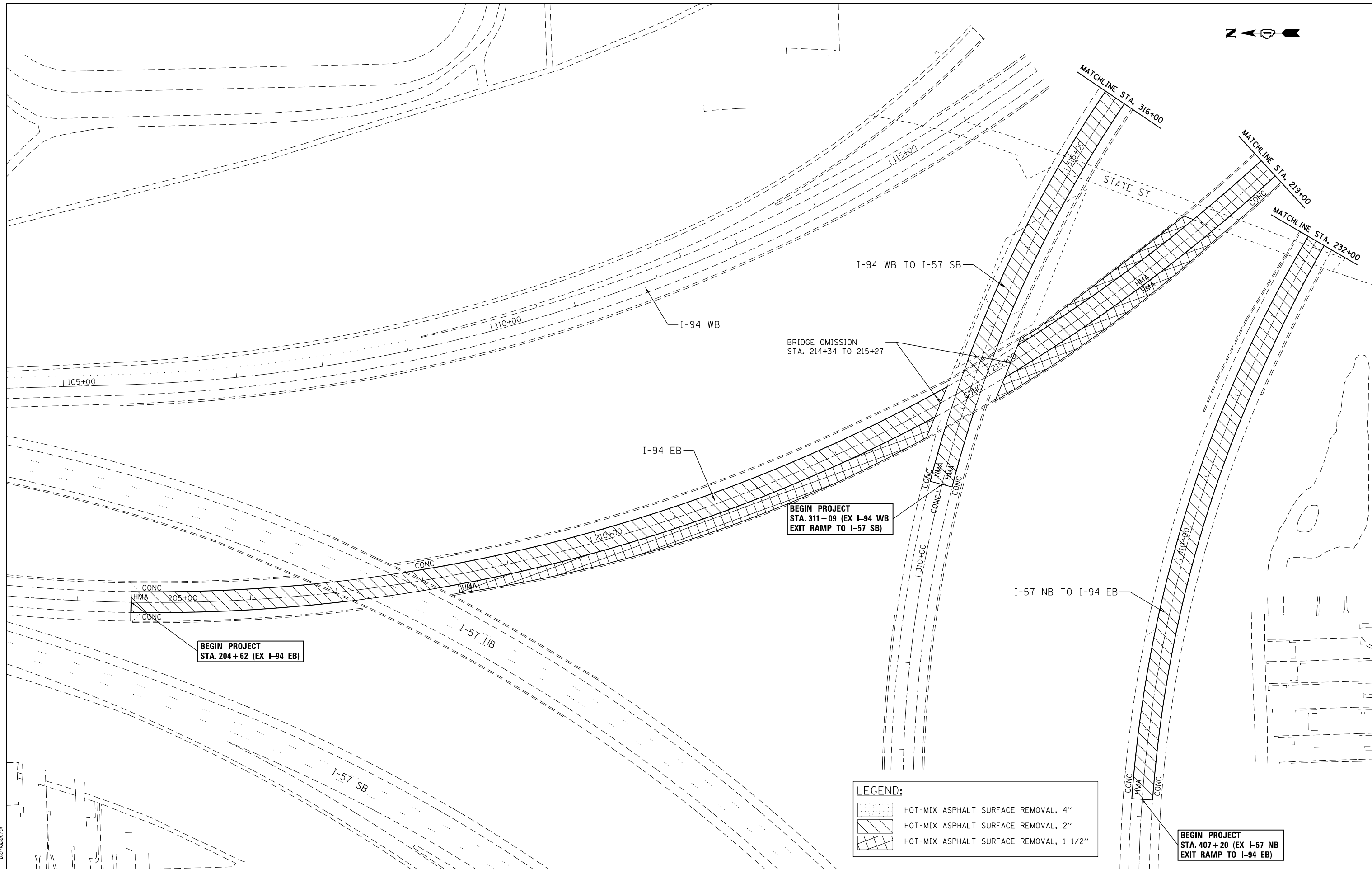
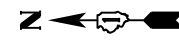
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FILE NAME = 86150046-05-sht-mot-02.dgn	DRAWN - DS	REVISED -
PLOT SCALE = N.T.S.	CHECKED - TH	REVISED -
PLOT DATE = 4/20/2016	DATE - 03/18/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED MAINTENANCE OF TRAFFIC
BISHOP FORD EXPRESSWAY (I-94)

SCALE: N.T.S. SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	8
CONTRACT NO. 62B59				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



**BEGIN PROJECT
STA. 204 + 62 (EX I-94 EB)**

**BEGIN PROJECT
STA. 311 + 09 (EX I-94 WB
EXIT RAMP TO I-57 SB)**

**BEGIN PROJECT
STA. 407 + 20 (EX I-57 NB
EXIT RAMP TO I-94 EB)**

LEGEND:

	HOT-MIX ASPHALT SURFACE REMOVAL, 4"
	HOT-MIX ASPHALT SURFACE REMOVAL, 2"
	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"

COMPANY NAME: HRGreen
 PROJECT CONTACT: [Name]
 DATE PLOTTED: 3/18/2016 12:08:42 PM
 FILE NAME: 86150046-05-sht-RemovalPlan
 PLOT DRIVER: [Name]
 PEN TABLE: [Name]



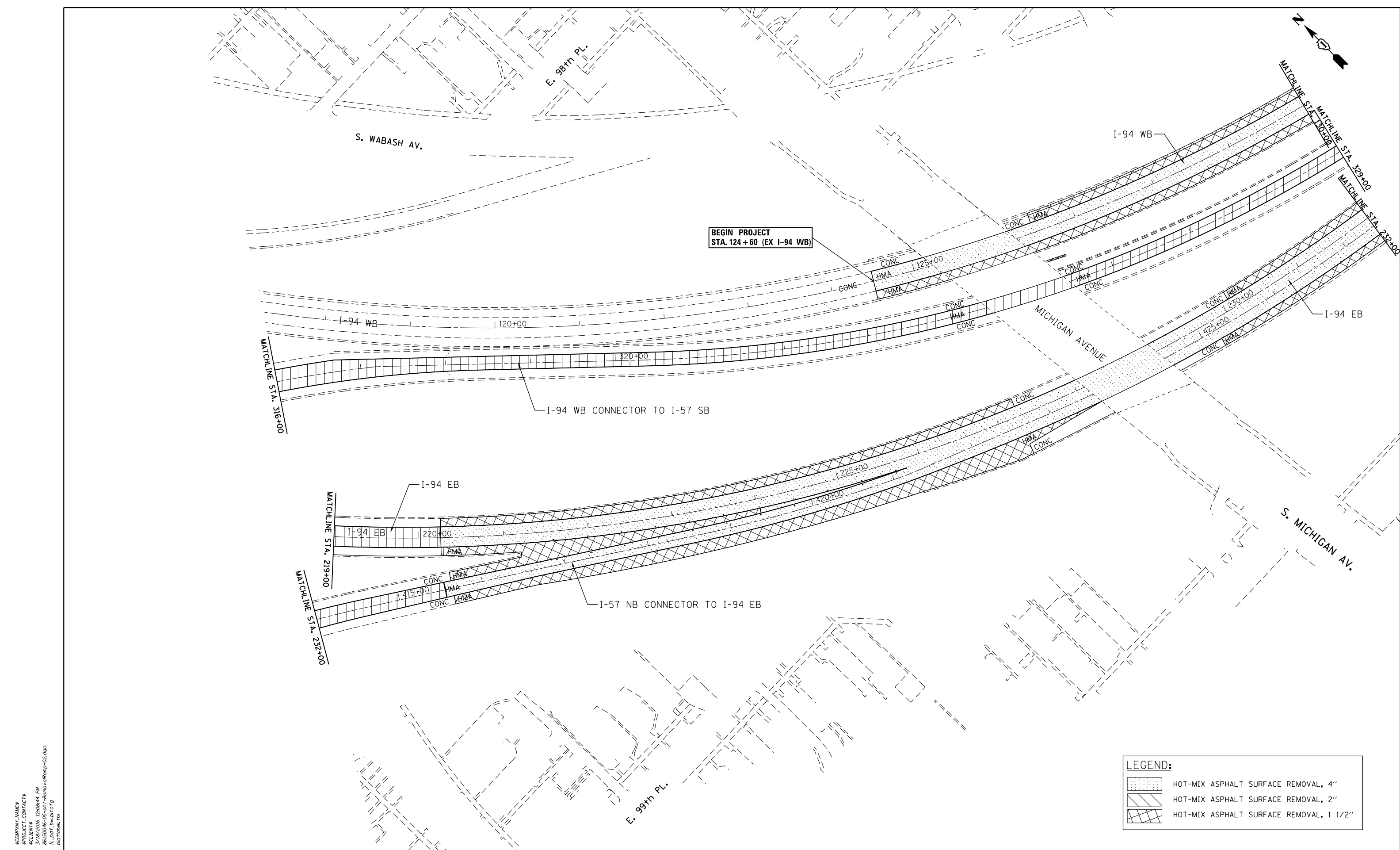
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PLOT SCALE = 1"=50'	CHECKED - TH	REVISED -
PLOT DATE = 3/18/2016	DATE - 03/18/16	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
BISHOP FORD EXPRESSWAY (I-94)**

SCALE: 1"=50' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	9
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	



**BEGIN PROJECT
STA. 124+60 (EX I-94 WB)**

LEGEND:

	HOT-MIX ASPHALT SURFACE REMOVAL, 4"
	HOT-MIX ASPHALT SURFACE REMOVAL, 2"
	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: bhartma
 DATE PLOTTED: 3/18/2016 12:08:44 PM
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 PLOT DRIVER: IL_Pdf_Plotter.fq
 PEN TABLE: plot.tbl



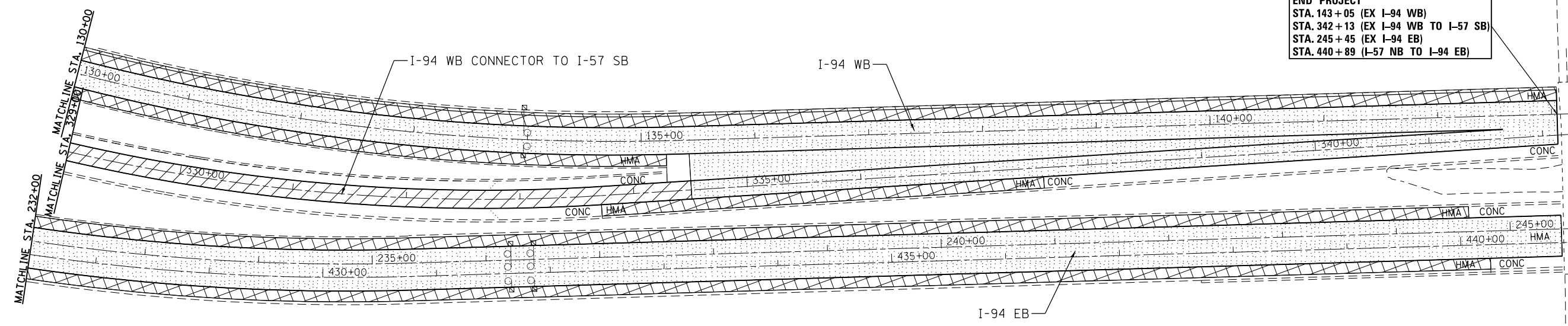
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PLOT SCALE = 1"=50'	CHECKED - TH	REVISED -
PLOT DATE = 3/18/2016	DATE - 03/18/16	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
BISHOP FORD EXPRESSWAY (I-94)**

SCALE: 1"=50' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	10
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	



END PROJECT
 STA. 143+05 (EX I-94 WB)
 STA. 342+13 (EX I-94 WB TO I-57 SB)
 STA. 245+45 (EX I-94 EB)
 STA. 440+89 (I-57 NB TO I-94 EB)

LEGEND:

	HOT-MIX ASPHALT SURFACE REMOVAL, 4"
	HOT-MIX ASPHALT SURFACE REMOVAL, 2"
	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: bhartha
 DATE PLOTTED: 3/18/2016 12:08:46 PM
 FILE NAME: 86150046-05-sht-RemovalRamp-03.dgn
 PLOT DRIVER: ILpdf.plt
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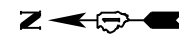
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PLOT DATE = 3/18/2016	DATE - 03/18/16	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN
 BISHOP FORD EXPRESSWAY (I-94)**

SCALE: 1"=50' SHEET NO. 3 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	11
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	



NOTE: PLAN ALIGNMENTS & SUPPORTING DATA

THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS WAS DEVELOPED FROM PREVIOUS PLANIMETRICS AND AERIAL PHOTOGRAPHY FURNISHED BY THE DEPARTMENT AND IS NOT THE RESULT OF A GROUND SURVEY. THEREFORE, THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS IS FOR REFERENCE PURPOSES ONLY. THE RELATIVE ACCURACY OF THE INFORMATION IS UNKNOWN AND CANNOT BE GUARANTEED. THE CONTRACTOR MAY BE REQUIRED TO ADJUST LAYOUT TO MATCH ACTUAL FIELD CONDITIONS AND THE INTENT OF THE PLANS.

PROPOSED MAINLINE RESURFACING
 STA. 204+62 TO STA. 219+00
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,
 STONE MATRIX ASPHALT, 12.5, N80 (IL 9.5 mm); 2"

CONC
 HMA
 205+00

**BEGIN PROJECT
 STA. 204 + 62 (EX I-94 EB)**

PROPOSED SHOULDER RESURFACING
 - HOT-MIX ASPHALT SURFACE COURSE,
 MIX "D", N70 (IL 9.5 mm); 1 1/2"

**BEGIN PROJECT
 STA. 311 + 09 (EX I-94 WB
 EXIT RAMP TO I-57 SB)**

PROPOSED MAINLINE RESURFACING
 STA. 311+09 TO STA. 316+00
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,
 STONE MATRIX ASPHALT, 12.5, N80; 2"

PROPOSED MAINLINE RESURFACING
 STA. 407+20 TO STA. 414+00
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,
 STONE MATRIX ASPHALT, 12.5, N80; 2"

**BEGIN PROJECT
 STA. 407 + 20 (EX I-57 NB
 EXIT RAMP TO I-94 EB)**

BRIDGE OMISSION
 STA. 214+34 TO 215+27

COMPANY NAME: HRGreen
 PROJECT CONTACT: bhartha
 DATE PLOTTED: 4/20/2016 12:24:03 PM
 FILE NAME: 86150046-05-sht-ResurfRamp-01.dgn
 PLOT DRIVER: ILpdf.bw.dwt
 PEN TABLE: plc-table.tbl



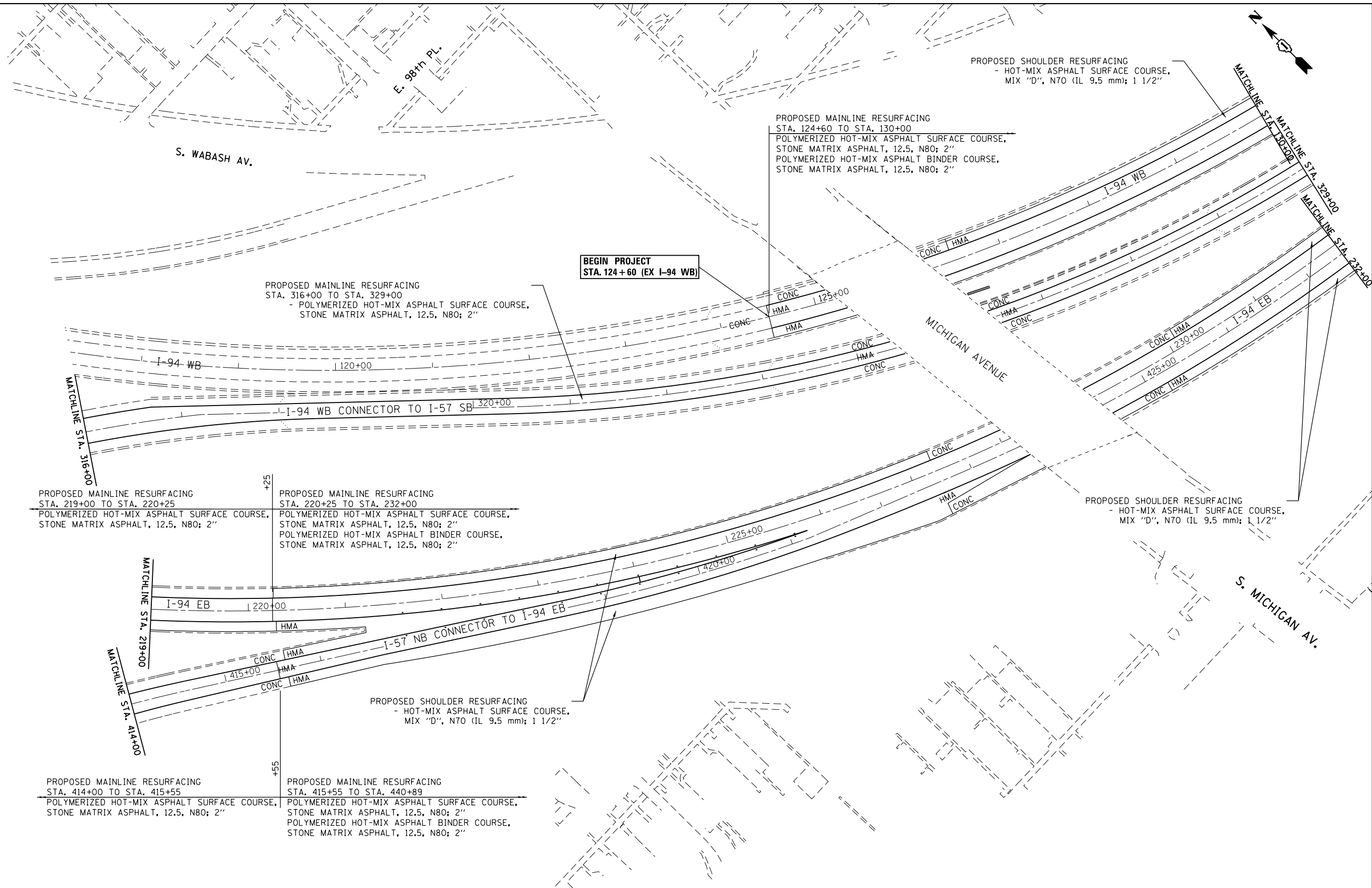
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PLOT DATE = 4/20/2016	DATE - 03/18/16	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**RESURFACING PLAN
 BISHOP FORD EXPRESSWAY (I-94)**

SCALE: 1"=50' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	12
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	



PROPOSED SHOULDER RESURFACING
- HOT-MIX ASPHALT SURFACE COURSE,
MIX "D", N70 (IL 9.5 mm); 1 1/2"

PROPOSED MAINLINE RESURFACING
STA. 124+60 TO STA. 130+00
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,
STONE MATRIX ASPHALT, 12.5, N80; 2"
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE,
STONE MATRIX ASPHALT, 12.5, N80; 2"

PROPOSED MAINLINE RESURFACING
STA. 316+00 TO STA. 329+00
- POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,
STONE MATRIX ASPHALT, 12.5, N80; 2"

BEGIN PROJECT
STA. 124 + 60 (EX I-94 WB)

PROPOSED MAINLINE RESURFACING
STA. 219+00 TO STA. 220+25
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,
STONE MATRIX ASPHALT, 12.5, N80; 2"

PROPOSED MAINLINE RESURFACING
STA. 220+25 TO STA. 232+00
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,
STONE MATRIX ASPHALT, 12.5, N80; 2"
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE,
STONE MATRIX ASPHALT, 12.5, N80; 2"

PROPOSED SHOULDER RESURFACING
- HOT-MIX ASPHALT SURFACE COURSE,
MIX "D", N70 (IL 9.5 mm); 1 1/2"

PROPOSED SHOULDER RESURFACING
- HOT-MIX ASPHALT SURFACE COURSE,
MIX "D", N70 (IL 9.5 mm); 1 1/2"

PROPOSED MAINLINE RESURFACING
STA. 414+00 TO STA. 415+55
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,
STONE MATRIX ASPHALT, 12.5, N80; 2"

PROPOSED MAINLINE RESURFACING
STA. 415+55 TO STA. 440+89
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,
STONE MATRIX ASPHALT, 12.5, N80; 2"
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE,
STONE MATRIX ASPHALT, 12.5, N80; 2"

COMPANY NAME: HRGreen.com
PROJECT CONTACT: bhartha
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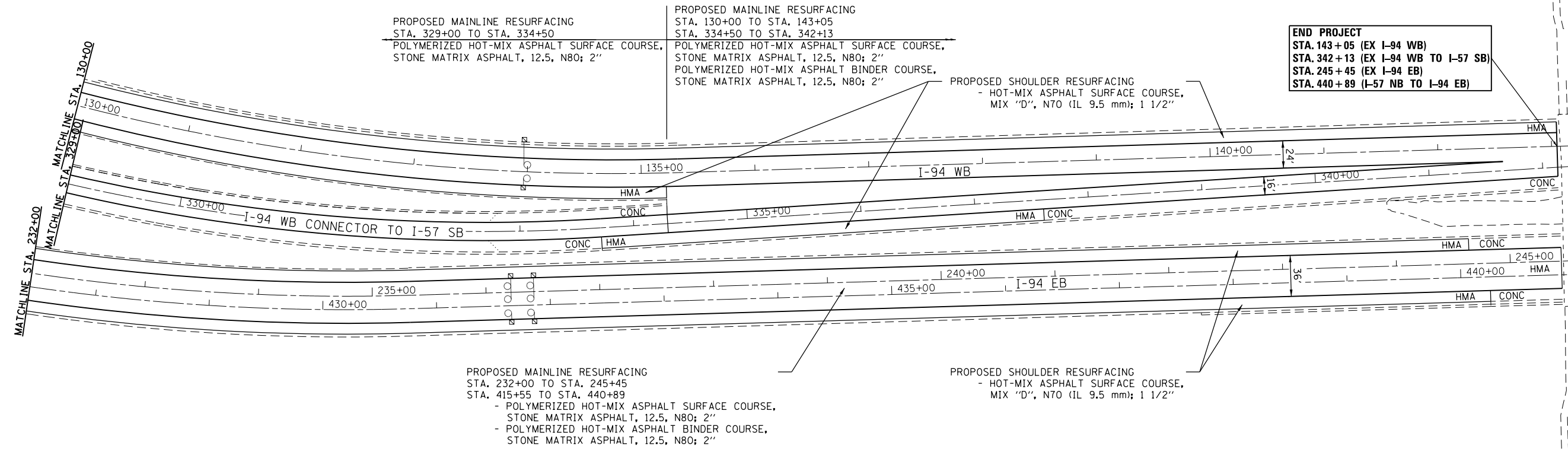
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PLOT DATE = 4/20/2016	DATE - 03/18/16	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RESURFACING PLAN
BISHOP FORD EXPRESSWAY (I-94)**

SCALE: 1"=50' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	13
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: bhartma
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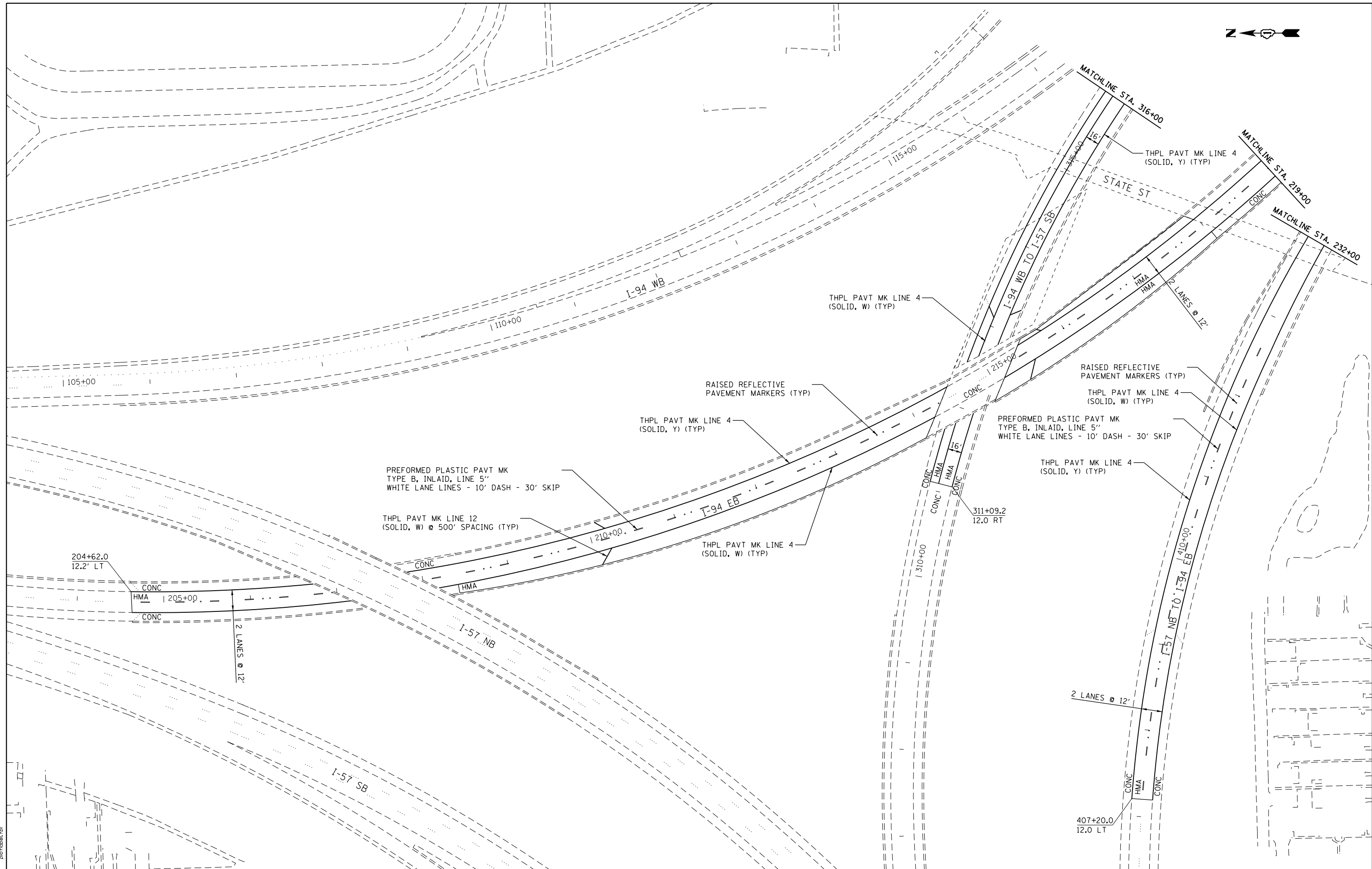
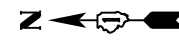


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PLOT DATE = 4/20/2016	DATE - 03/18/16	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RESURFACING PLAN		
BISHOP FORD EXPRESSWAY (I-94)		
SCALE: 1"=50'	SHEET NO. 3 OF 3 SHEETS	STA. TO STA.

F.A.I. RTE. 94	SECTION 1516.IRS-5	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 14
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	



COMPANY NAME: HRGreen.com
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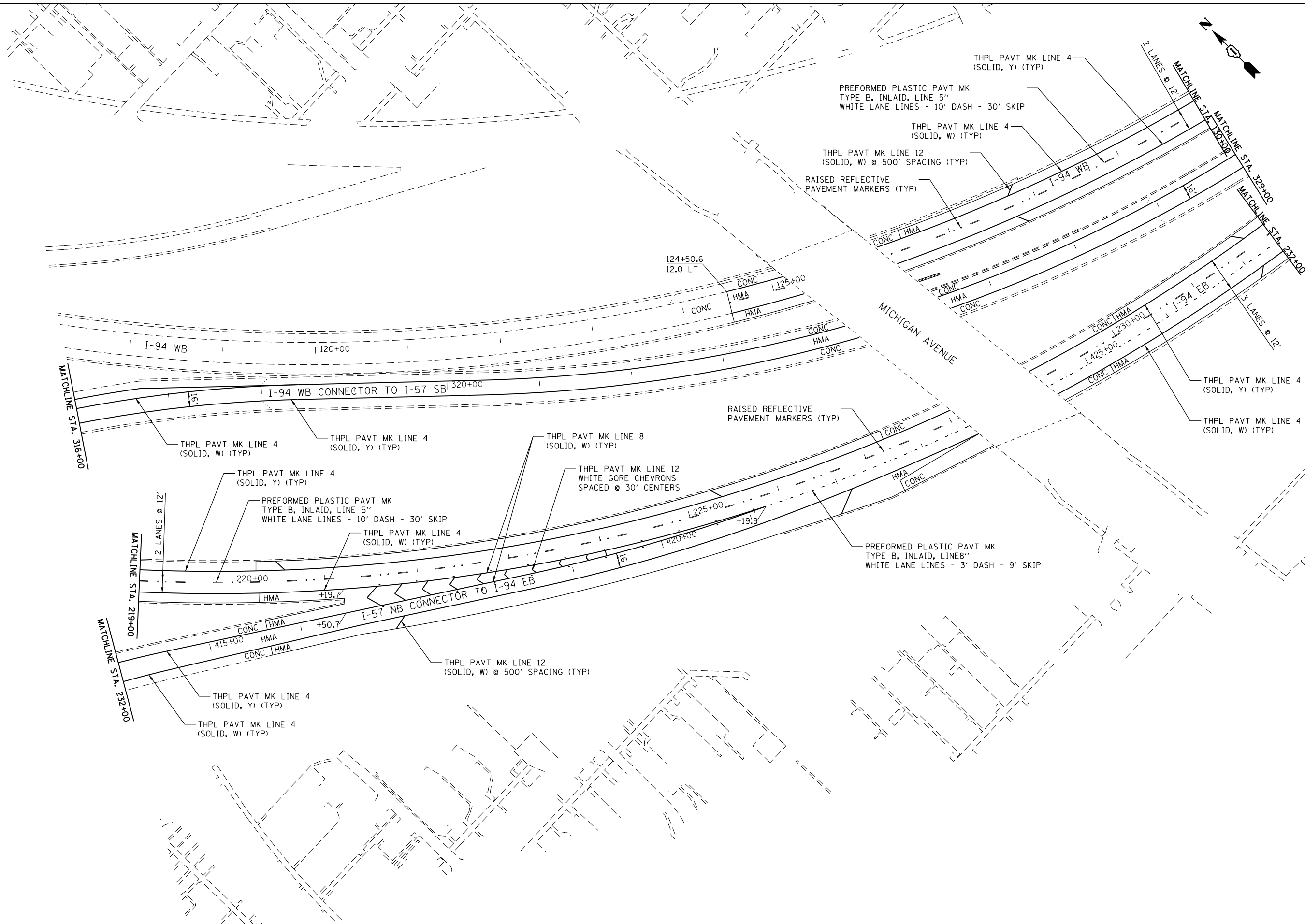
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PLOT DATE = 3/18/2016	DATE - 03/18/16	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN
 BISHOP FORD EXPRESSWAY (I-94)**

SCALE: 1"=50' SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	15
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	



COMPANY NAME: HRGreen
 PROJECT CONTACT: [Redacted]
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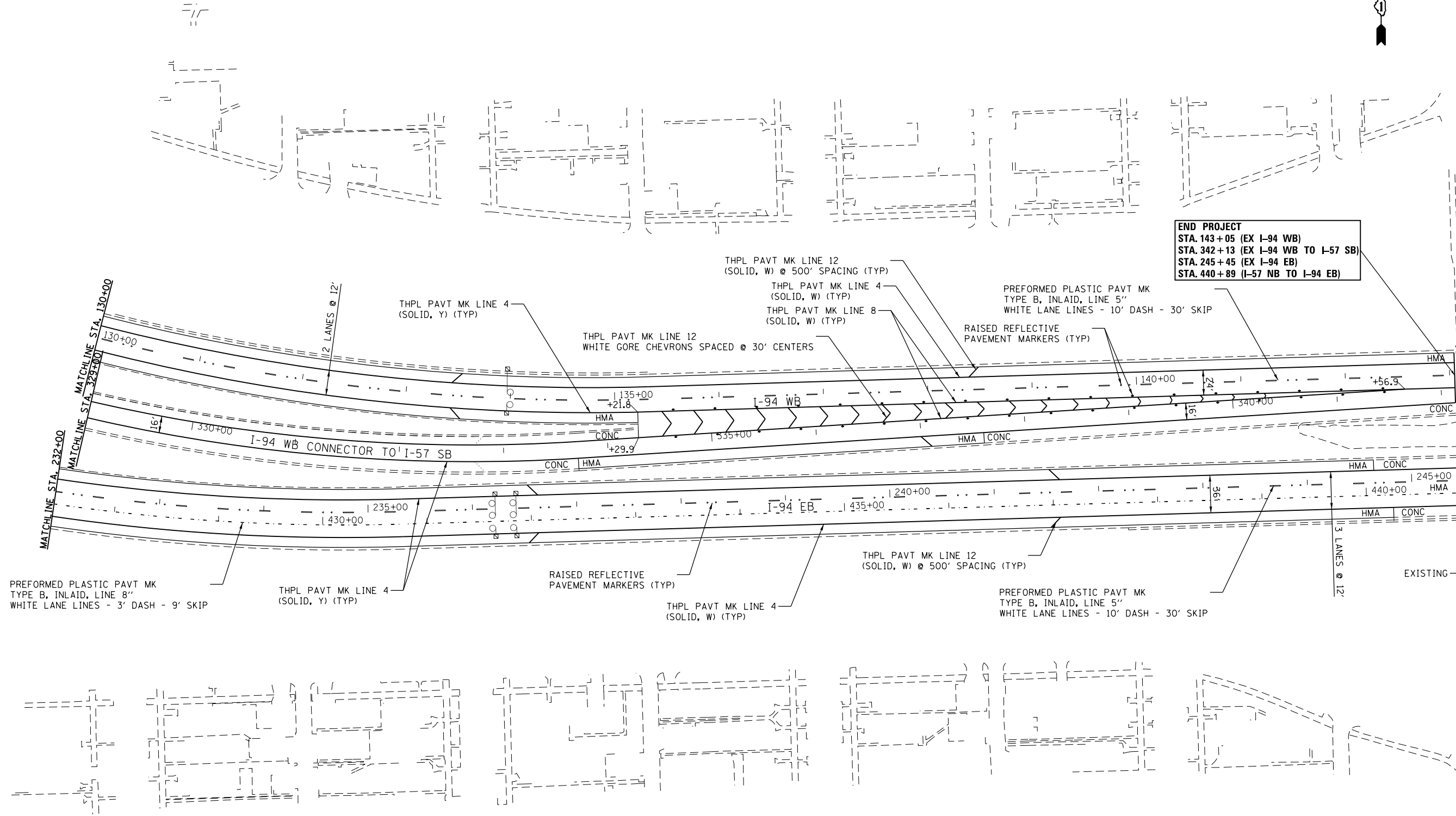


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PLOT DATE = 3/18/2016	DATE - 03/18/16	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN	
BISHOP FORD EXPRESSWAY (I-94)	
SCALE: 1"=50'	SHEET NO. 2 OF 3 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	16
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	



END PROJECT
 STA. 143+05 (EX I-94 WB)
 STA. 342+13 (EX I-94 WB TO I-57 SB)
 STA. 245+45 (EX I-94 EB)
 STA. 440+89 (I-57 NB TO I-94 EB)

PREFORMED PLASTIC PAVT MK
 TYPE B, INLAID, LINE 8"
 WHITE LANE LINES - 3' DASH - 9' SKIP

THPL PAVT MK LINE 4
 (SOLID, Y) (TYP)

RAISED REFLECTIVE
 PAVEMENT MARKERS (TYP)

THPL PAVT MK LINE 4
 (SOLID, W) (TYP)

THPL PAVT MK LINE 12
 (SOLID, W) @ 500' SPACING (TYP)

PREFORMED PLASTIC PAVT MK
 TYPE B, INLAID, LINE 5"
 WHITE LANE LINES - 10' DASH - 30' SKIP

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: Illinois Professional Design Firm
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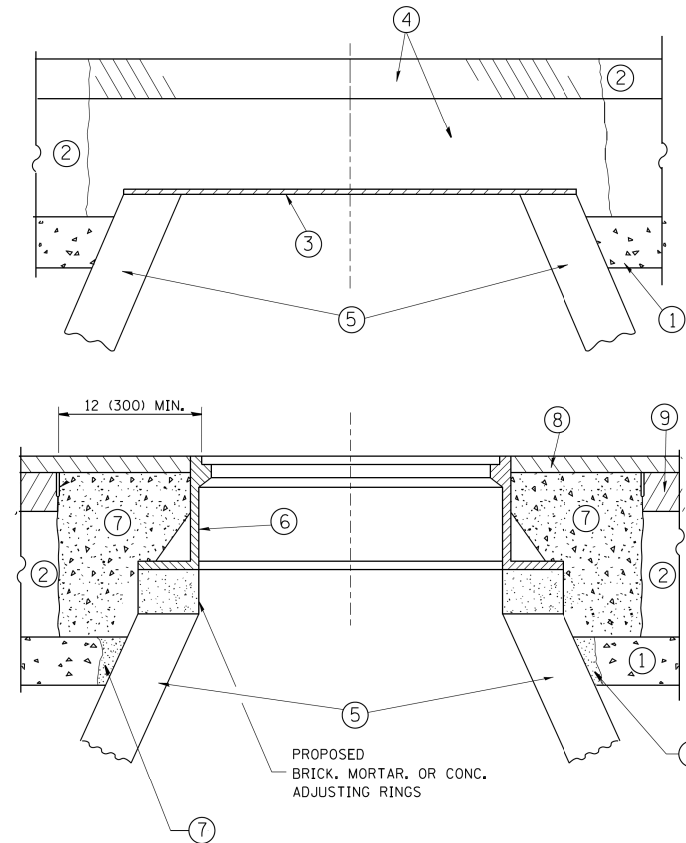
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PLOT DATE = 3/18/2016	DATE - 03/18/16	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN	
BISHOP FORD EXPRESSWAY (I-94)	
SCALE: 1"=50'	SHEET NO. 3 OF 3 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	17
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	

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 PROJECT CONTACT: #PROJECT CONTACT#
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 FILE NAME: 86150046-05-shr-det-d01-bd08.dgn
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 PEN TABLE: pla-table.tbl



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

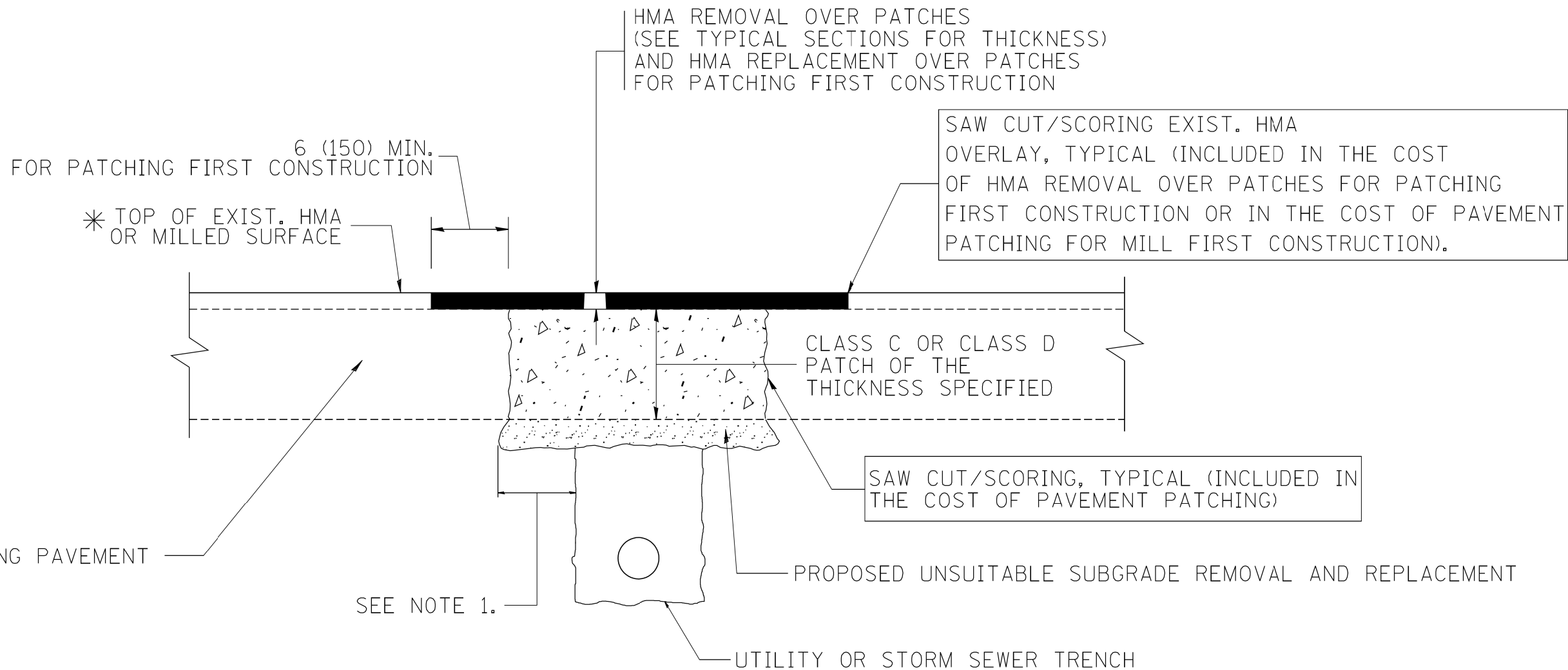
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		CHECKED -	REVISED - R. BORO 03-09-11
		DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
 FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	18
BD600-03 (BD-8)		CONTRACT NO.	62B59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		PLOT DATE = 10/27/2008	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
 HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	19
BD400-04 (BD-22)			CONTRACT NO.	62B59
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

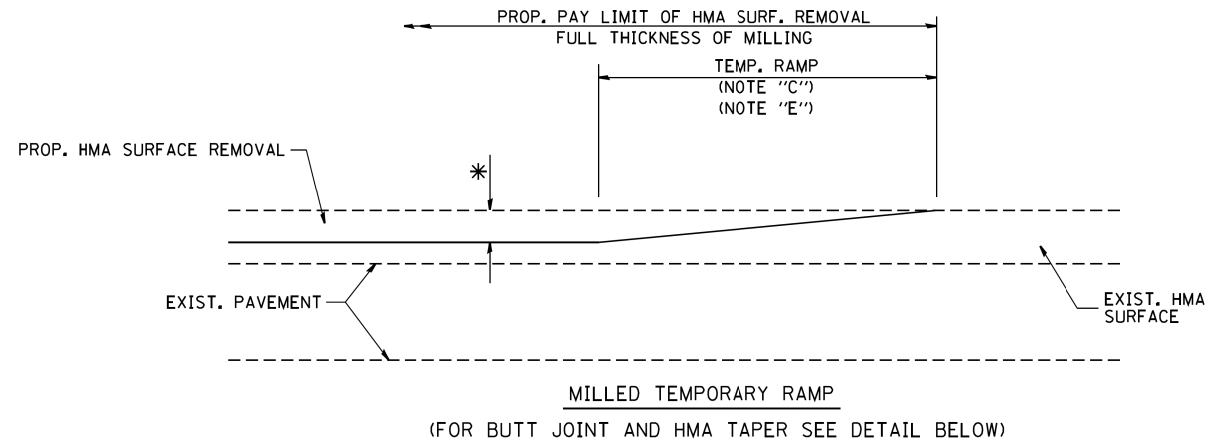
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

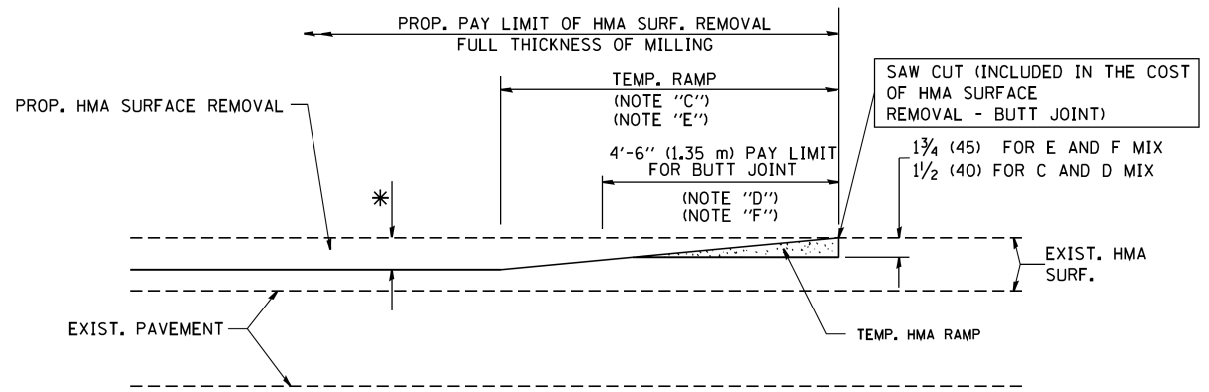
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PLOT SCALE = 50,000 ' / IN.	CHECKED -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01					94	1516.IRS-5	COOK	34	20
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										FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



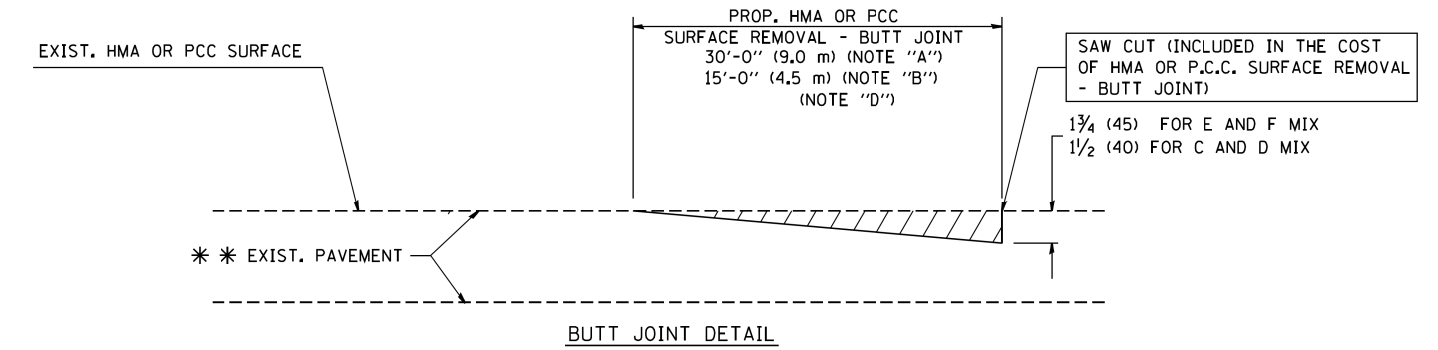
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

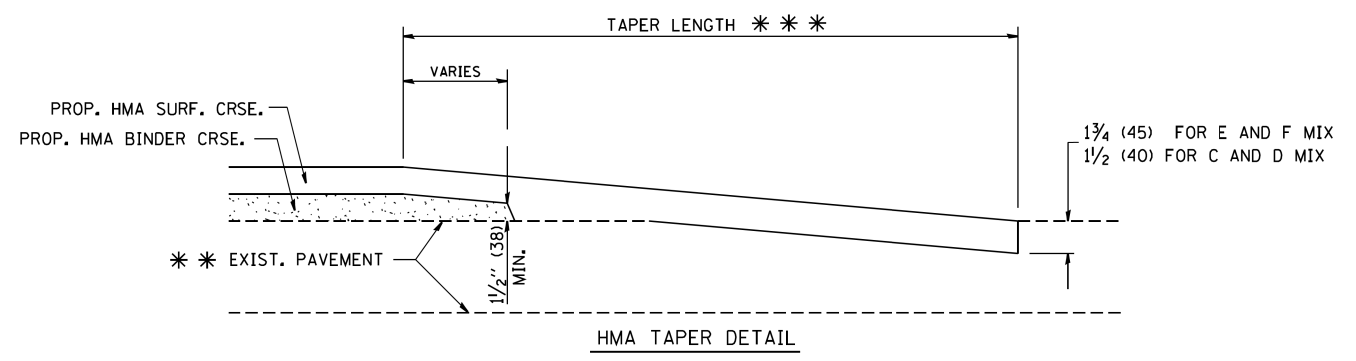


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

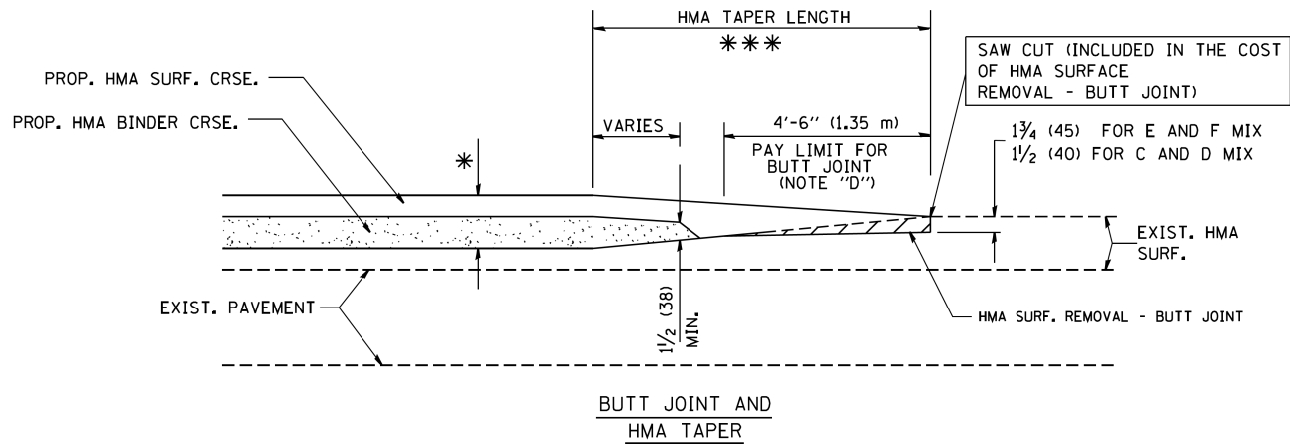
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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 PROJECT CONTACT: #PROJECT CONTACT#
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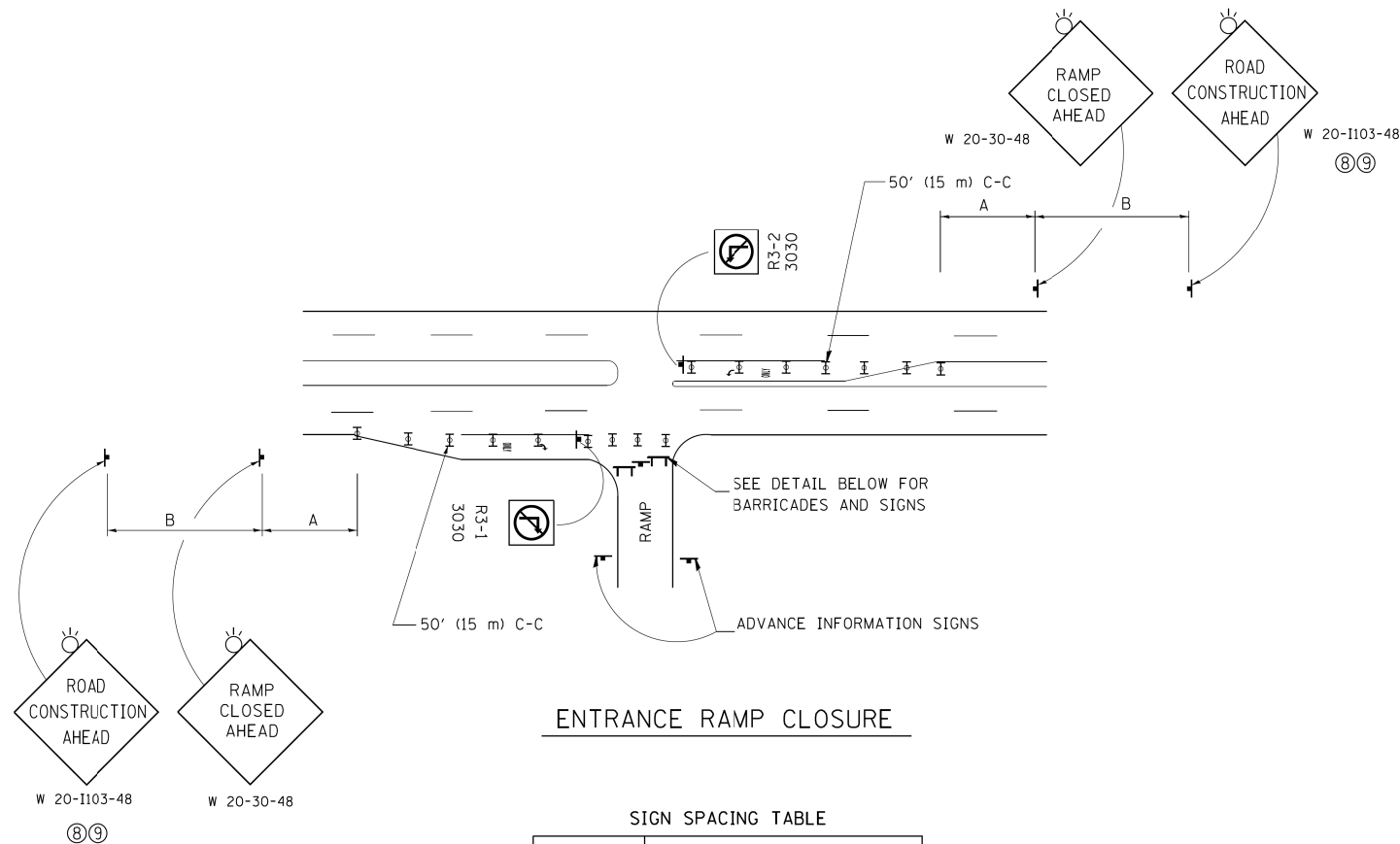
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	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	21
BD400-05 BD32		CONTRACT NO.	62B59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

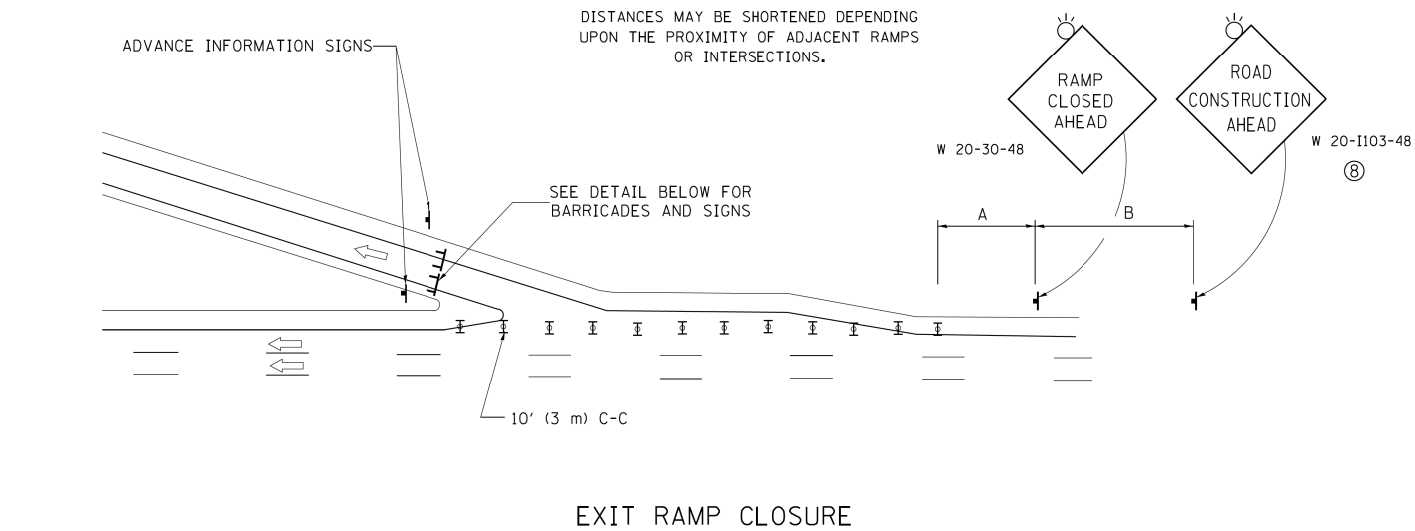


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

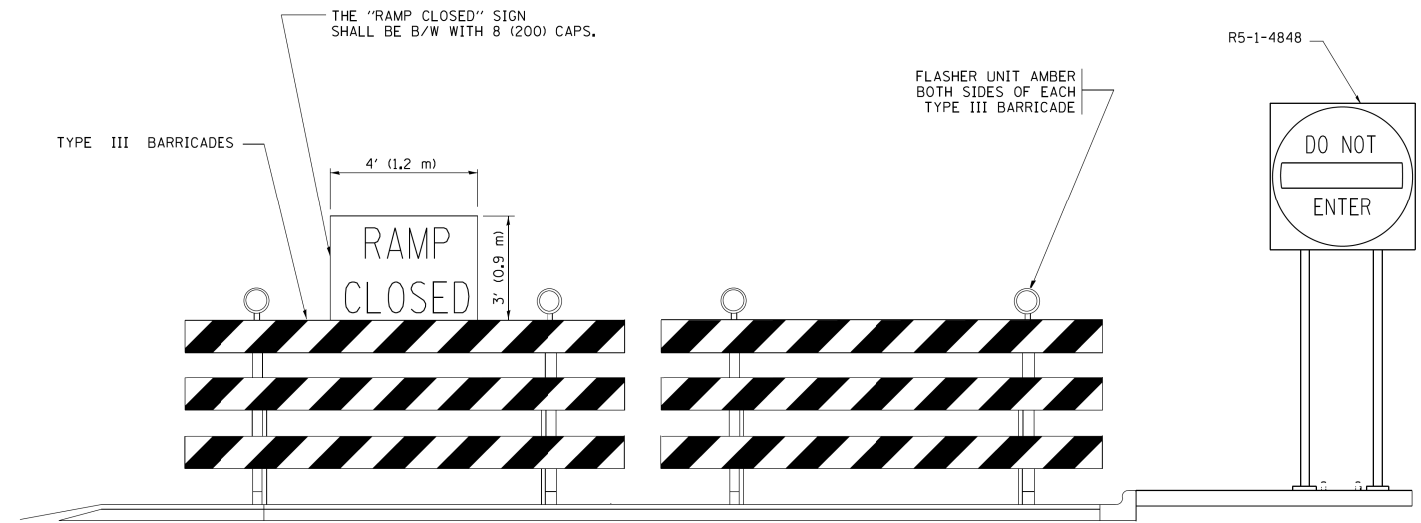
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

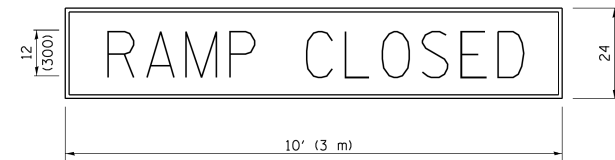
SYMBOLS

- ⊥ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊥ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

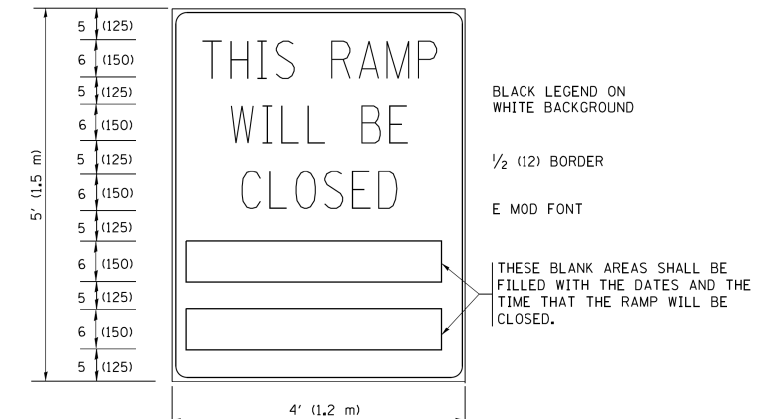
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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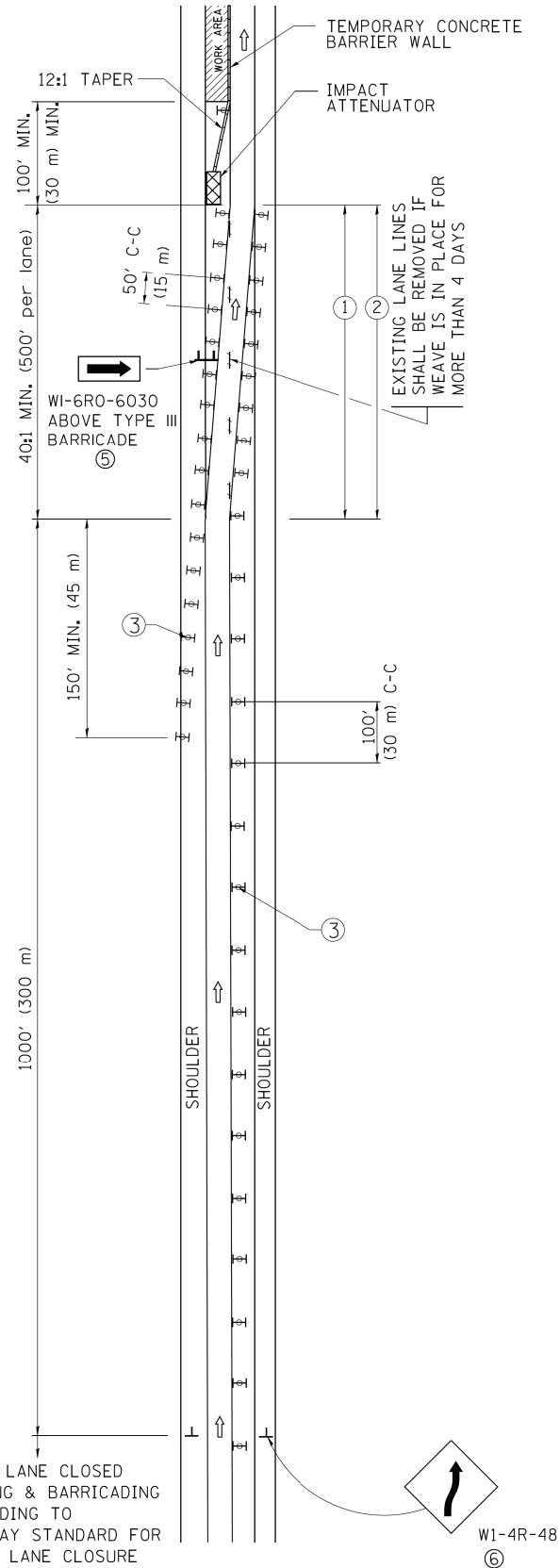
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

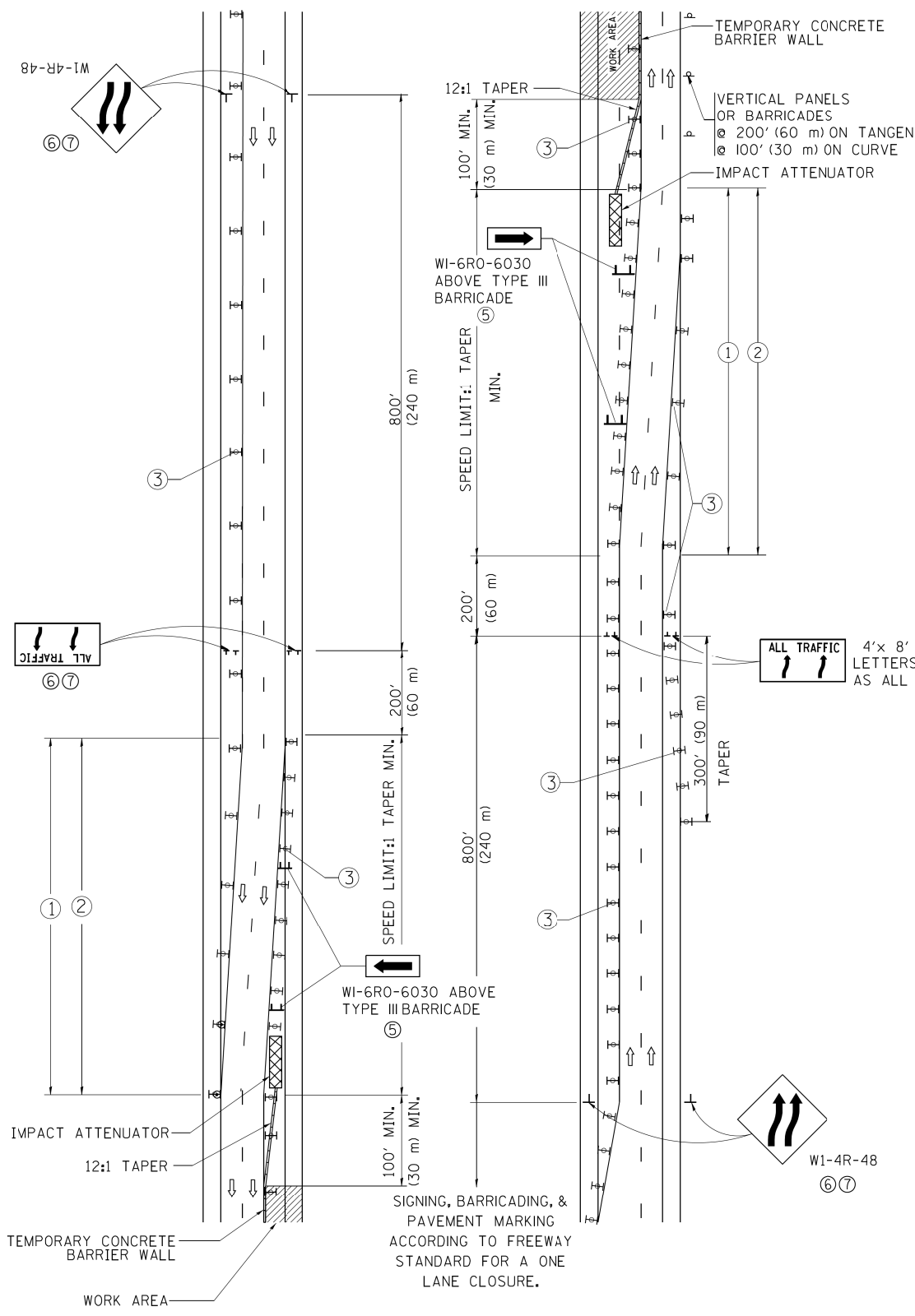
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	22
TC-08			CONTRACT NO. 62B59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

COMPANY NAME: PROJECT CONTACT: DATE PLOTTED: FILE NAME: PLOT DRIVER: PEN TABLE:

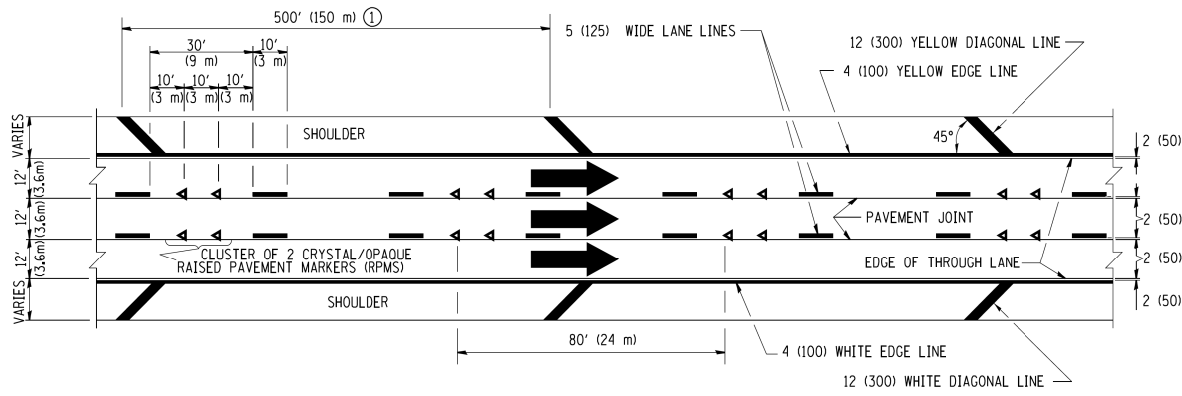
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	PLOT DATE = 7/1/2013	DATE - 02-87	REVISED - MD 06-13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR
FREEWAY SINGLE & MULTI-LANE WEAVE

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

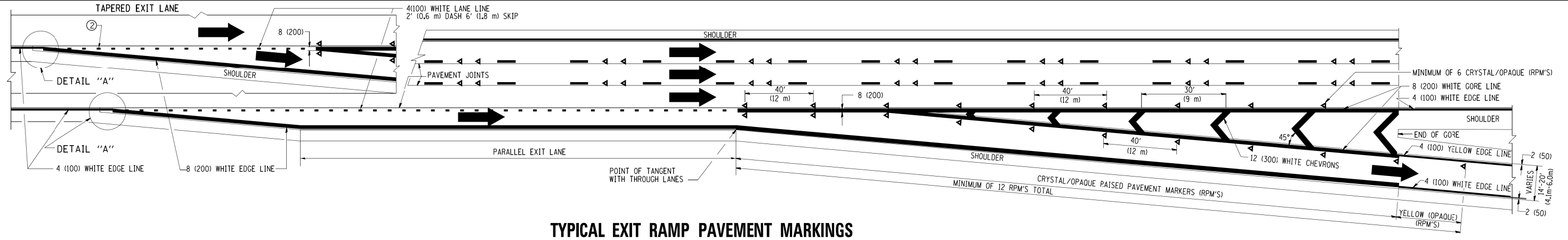
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TC-09		CONTRACT NO.	62B59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



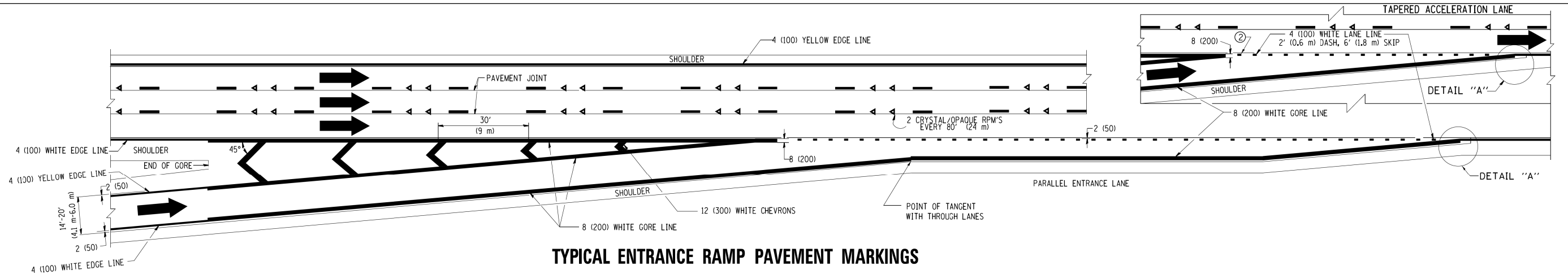
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

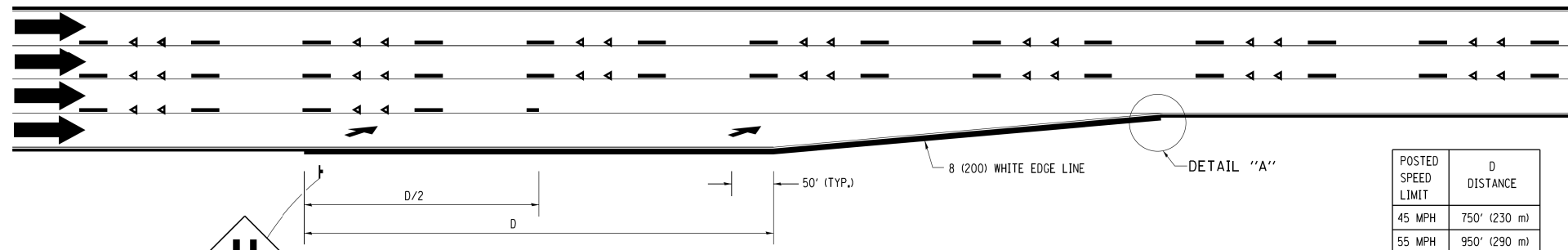
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE; INLAID OR GROOVED IN SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENT PROJECTS.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC PROJECTS.



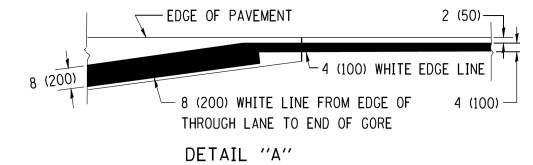
TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS



NOTES:

- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)

COMPANY NAME: #COMPANY NAME#
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 DATE PLOTTED: 3/17/2016 9:12:10 AM
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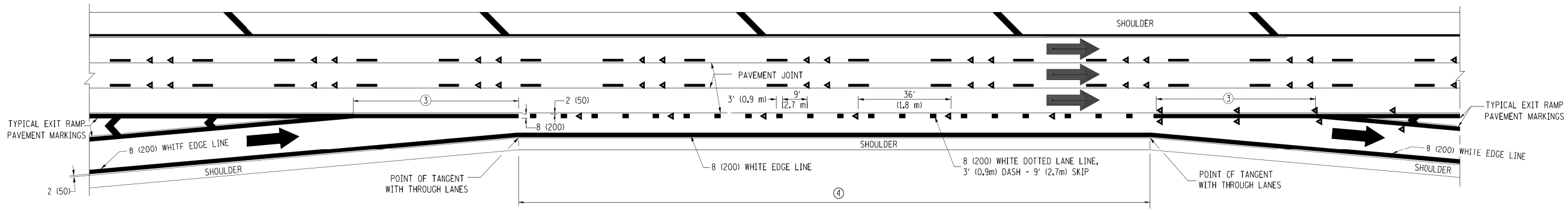
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		DATE - 01-90	REVISED - M.D. 05-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

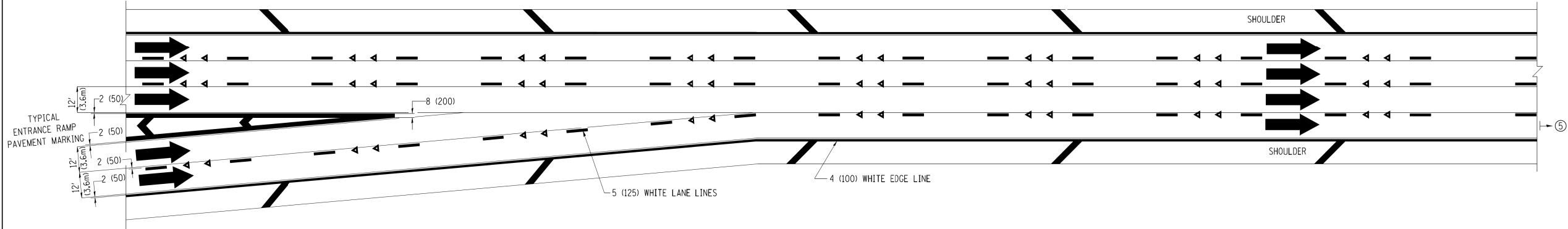
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**MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS**

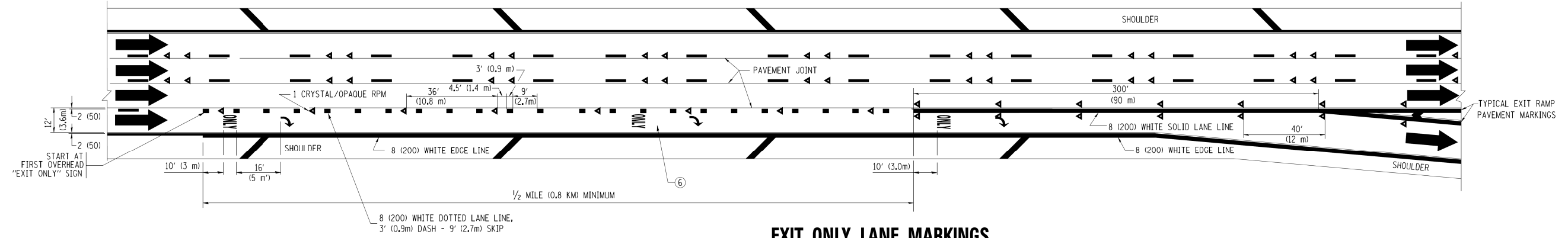
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TC-12			CONTRACT NO. 62B59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



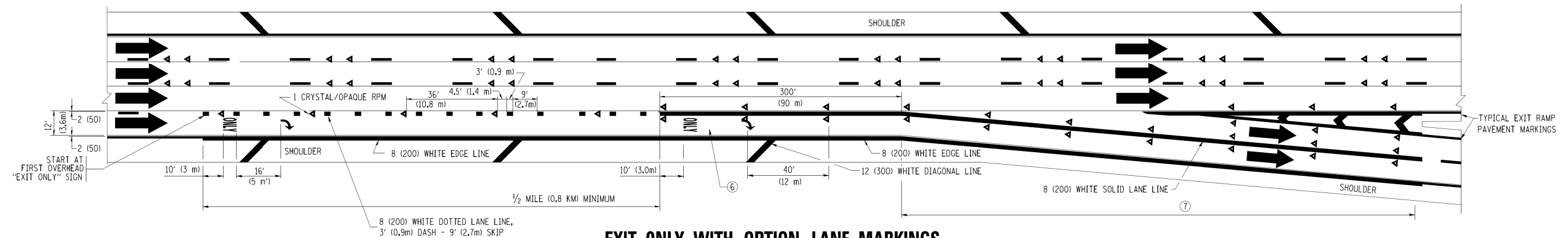
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

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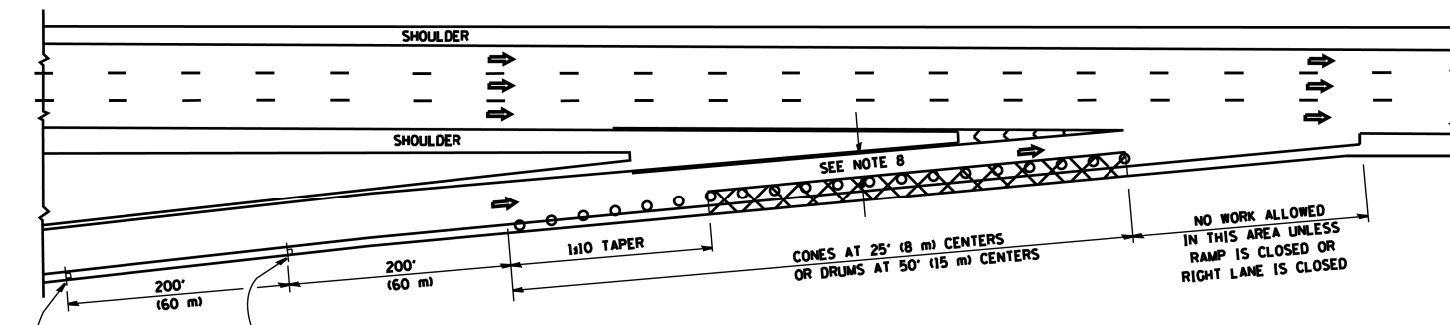
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

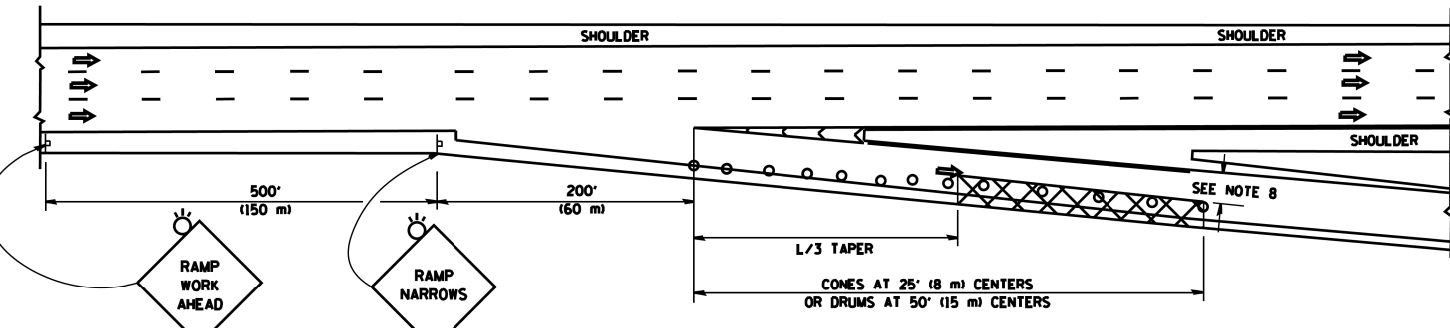
MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			
SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	25
TC-12		CONTRACT NO.	62B59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

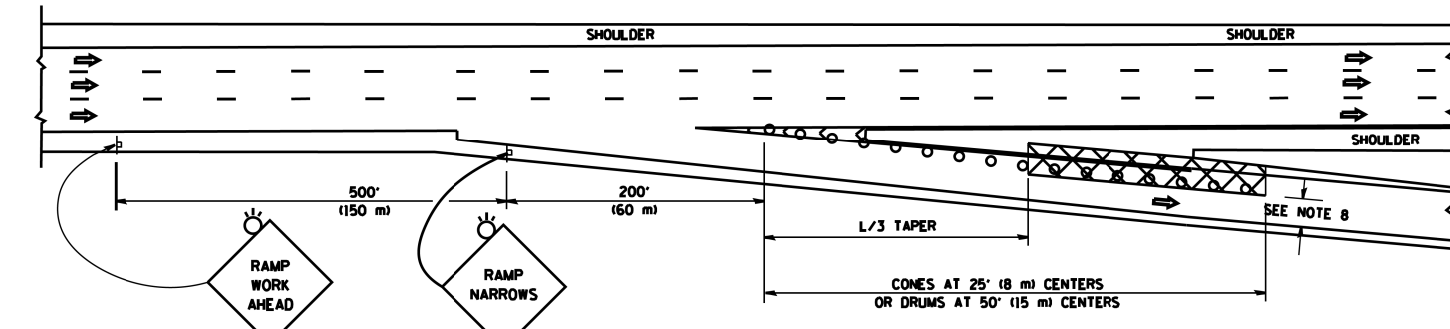
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

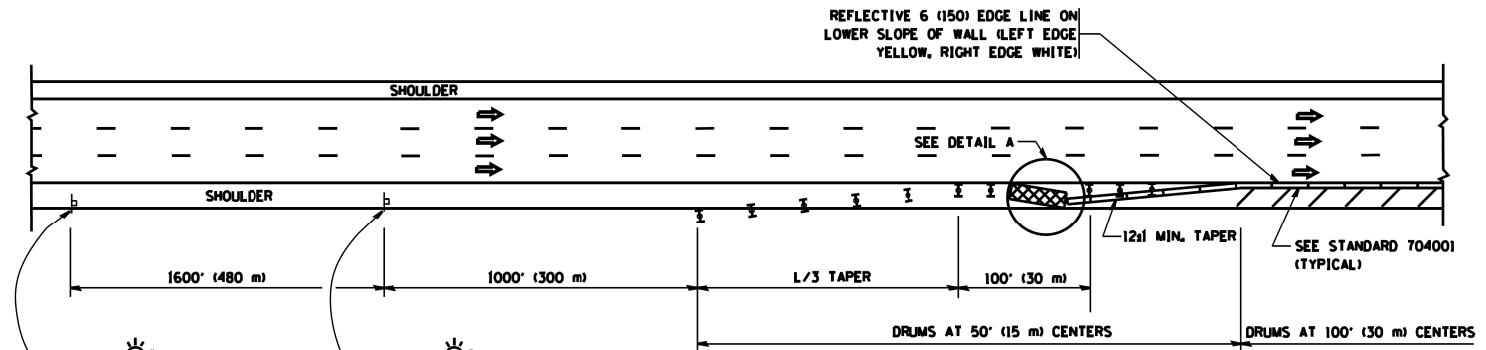
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

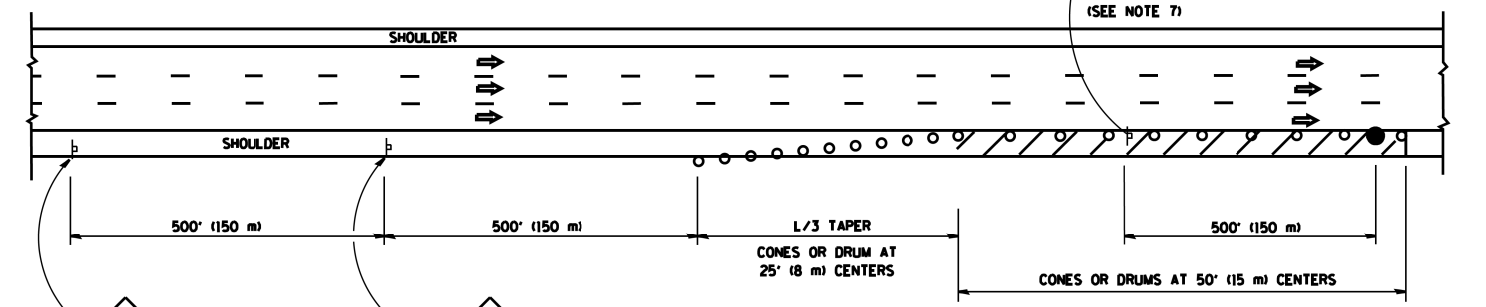
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH
	$L = 0.65(WNS)$ $L = (WNS)$
	$W =$ WIDTH OF OFFSET IN FEET (METERS)
	$S =$ NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

SHOULDER CLOSURE DETAILS

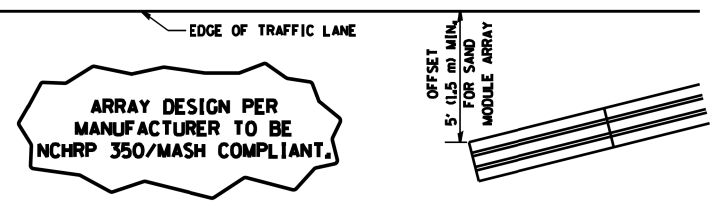


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



**DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PROJECT CONTACT: #PROJECT CONTACTS
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USER NAME = ileysa
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DESIGNED -	J.A.F. 12-06
REVISD -	S.P.B. 01-07
DRAWN -	D.W.S.
CHECKED -	S.P.B. 12-09
DATE -	11-96
REVISD -	M.D. 06-13

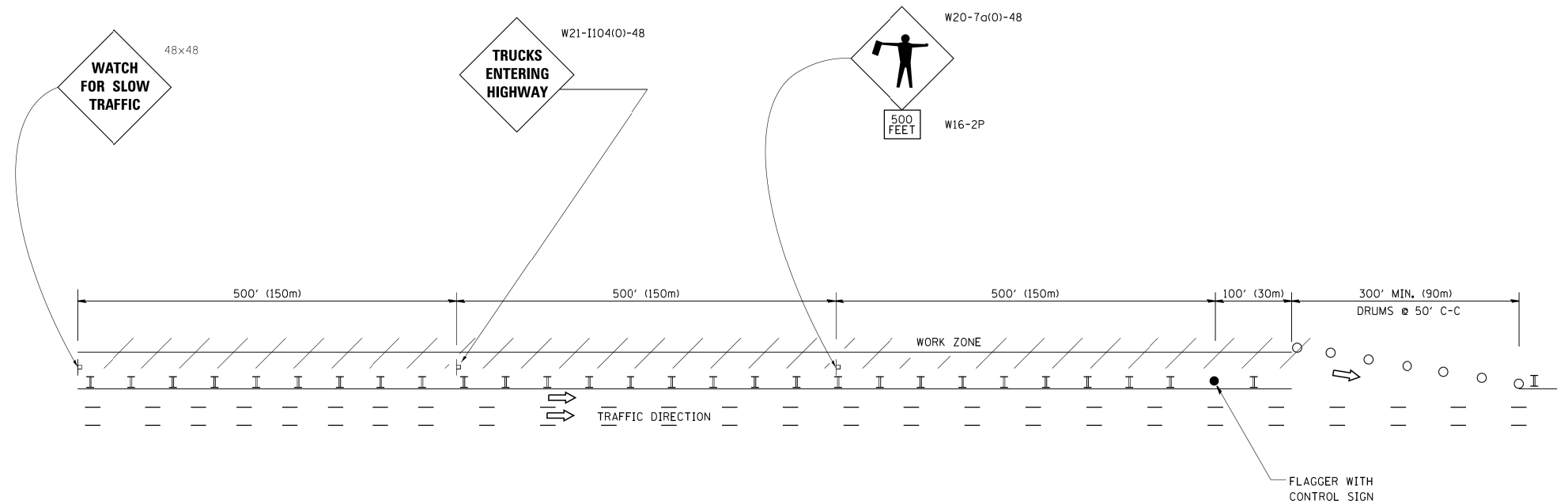
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL DETAILS FOR FREEWAY	
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA. TO STA.	

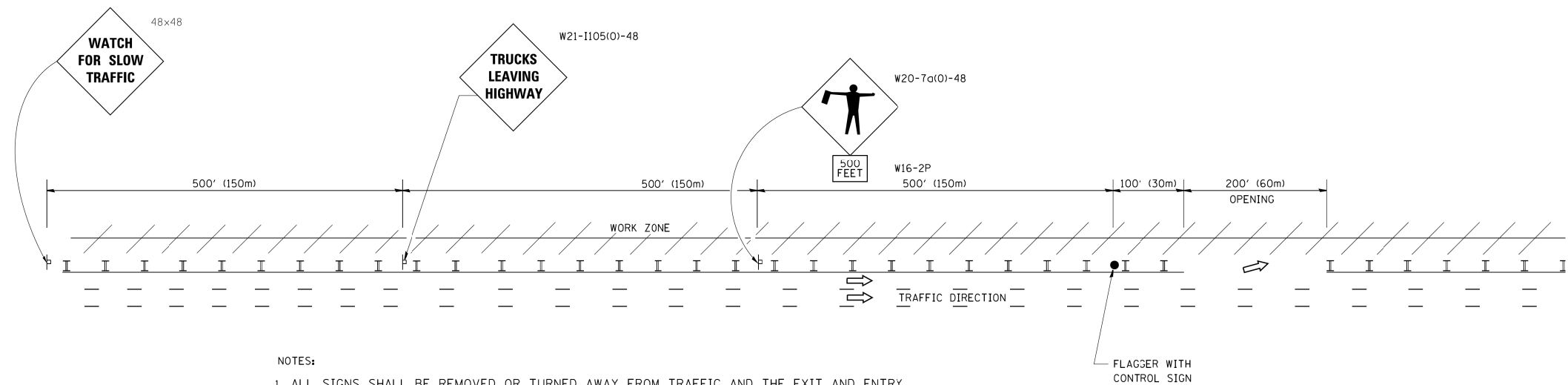
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	26
TC-17			CONTRACT NO.	62B59
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMPS.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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 PROJECT CONTACT: #PROJECT CONTACTS
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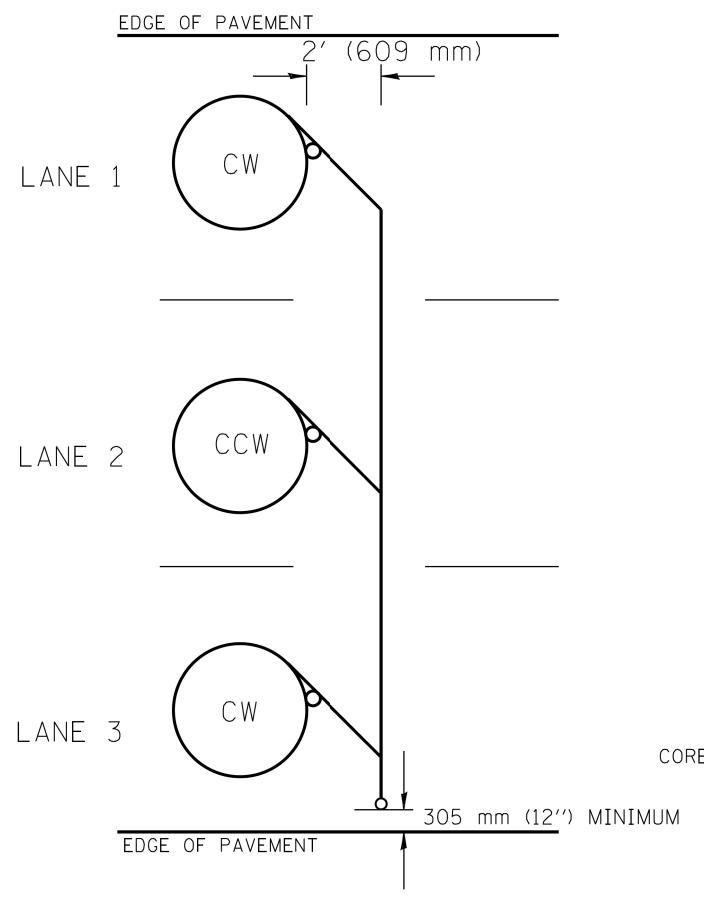
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

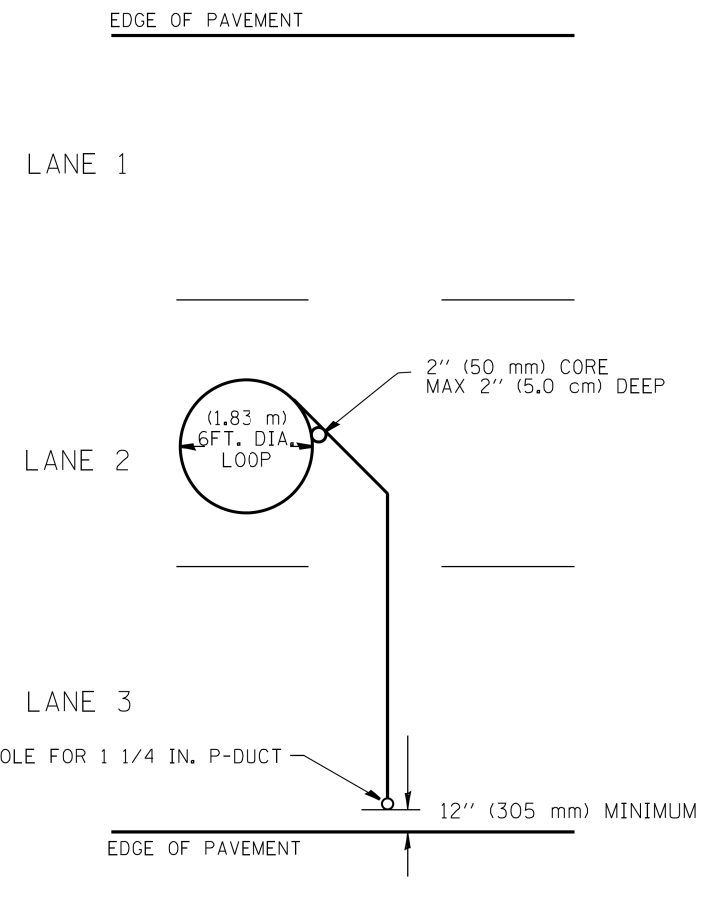
FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS
 AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

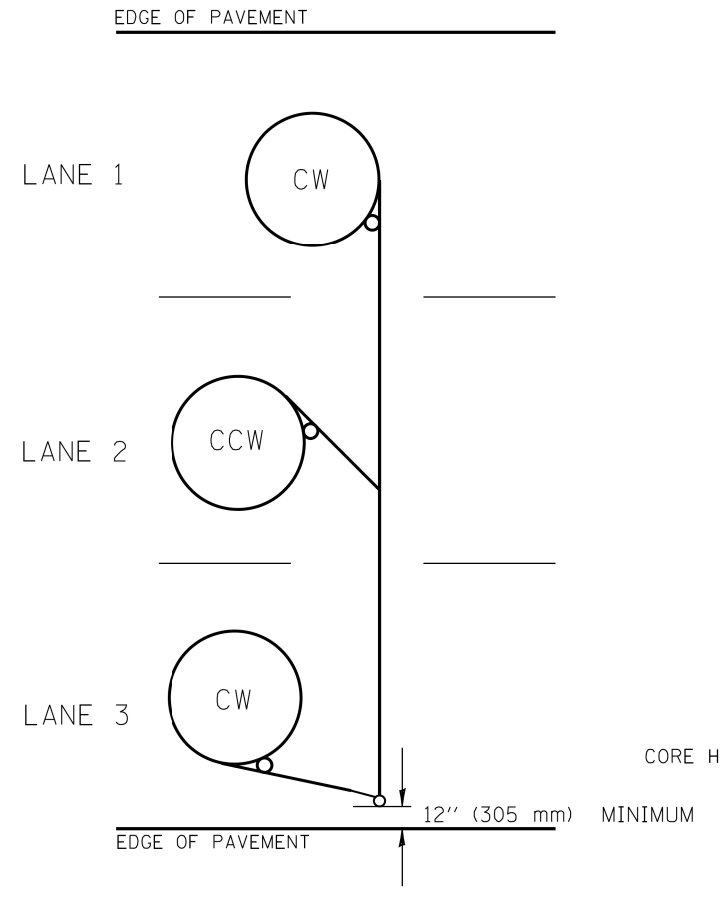
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TC-18		CONTRACT NO.	62B59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



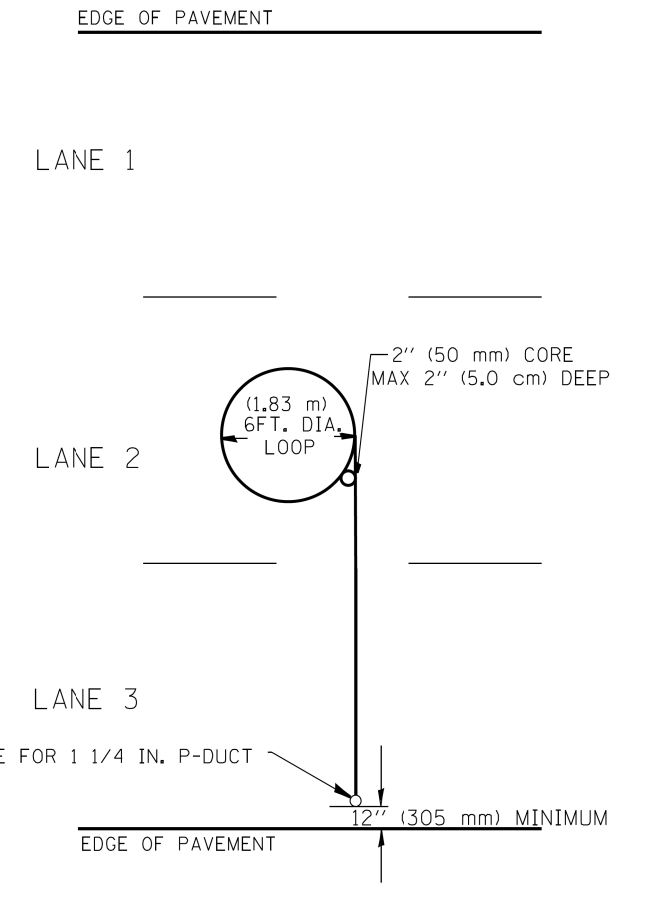
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



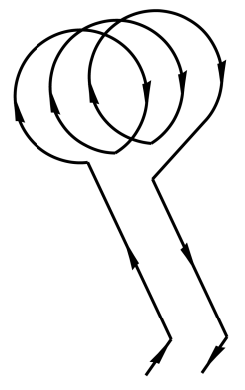
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



WIRING DETAILS

NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

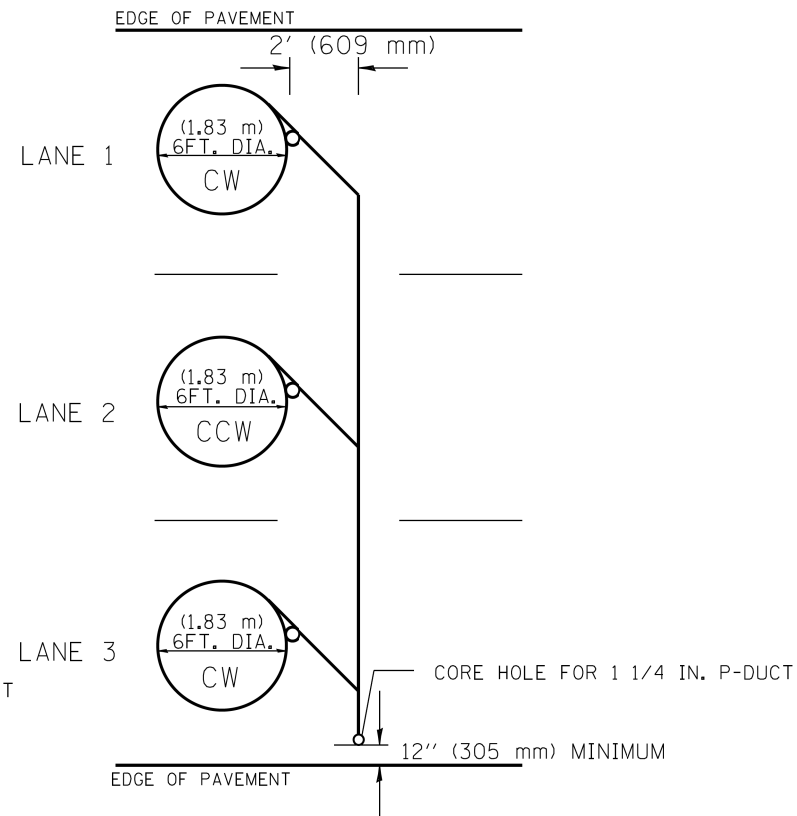
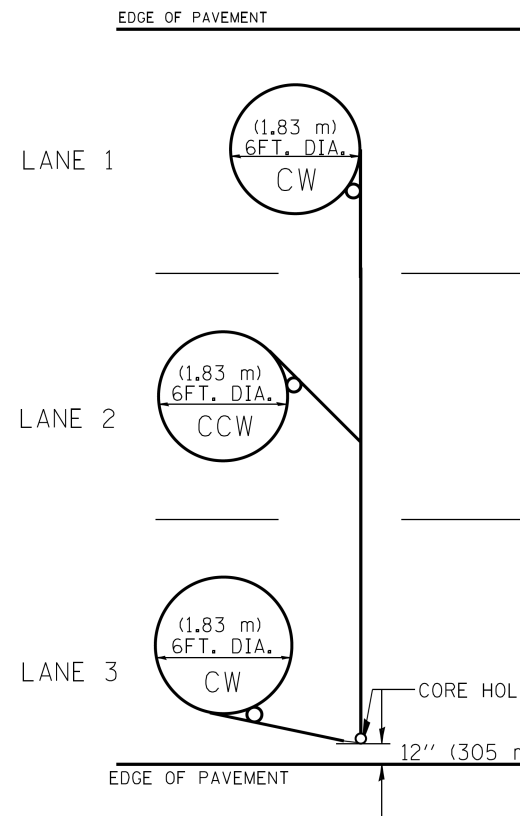
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PROJECT CONTACT: MEZAR
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PLOT DRIVER: ILpdf.plt
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	PLOT DATE = 5/18/2012	DATE - 6-22-94	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

EXISTING ROUND LOOP INSTALLATION		F.A. I. RTE. 94	SECTION 1516.1RS-5	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 28
SCALE: NONE	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 62B59	

F.A. I. RTE. 94	SECTION 1516.1RS-5	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 28
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

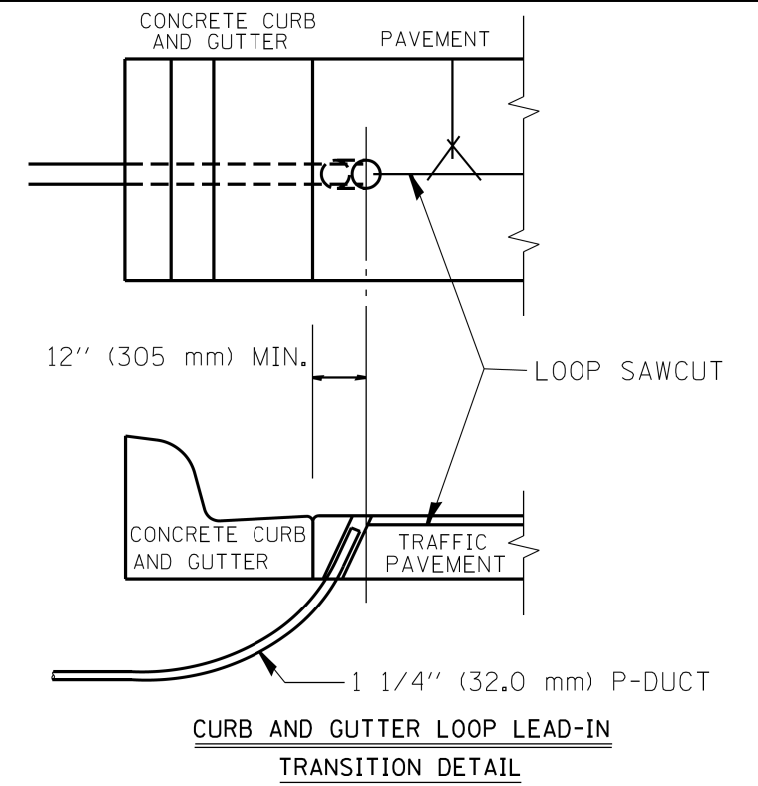


TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL

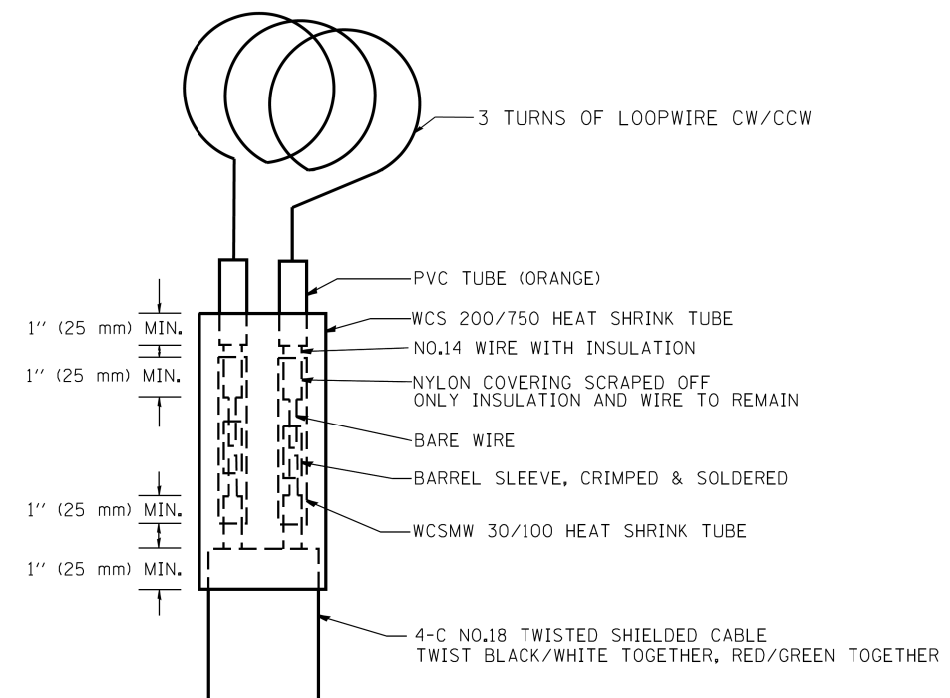
TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL

LAYOUT FOR MULTIPLE LANE ROADWAY

LAYOUT FOR MULTIPLE LANE ROADWAY



CURB AND GUTTER LOOP LEAD-IN
TRANSITION DETAIL



MINIMUM 1" (25 mm) HEAT SHRINK TUBING OVERLAP
ON WIRE, PVC & SHIELDED CABLE TO FORM WATER TIGHT SEAL

LOOP SPLICING REQUIREMENTS

NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150FT. (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

COMPANY NAME: TSC SYSTEMS
 PROJECT CONTACT: MEZAR
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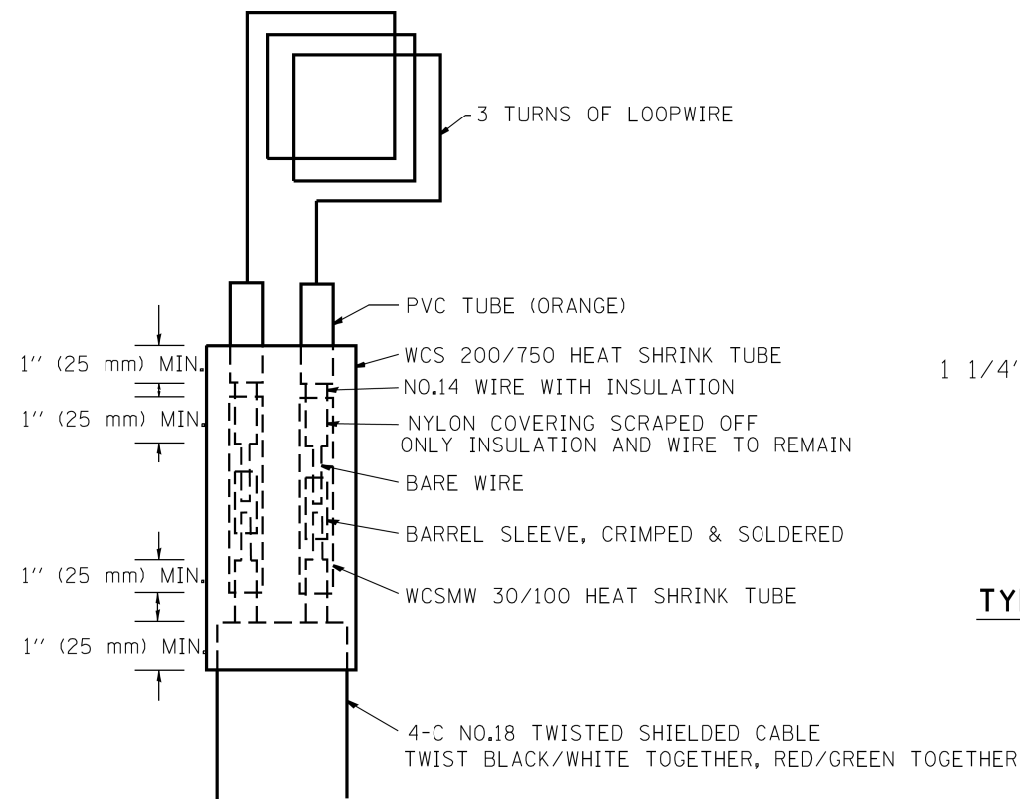
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PLOT DATE = 5/18/2012		DATE - 6-22-94	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 TRAFFIC SYSTEMS CENTER

EXISTING
 ROUND INDUCTION LOOP
 TYPICALS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

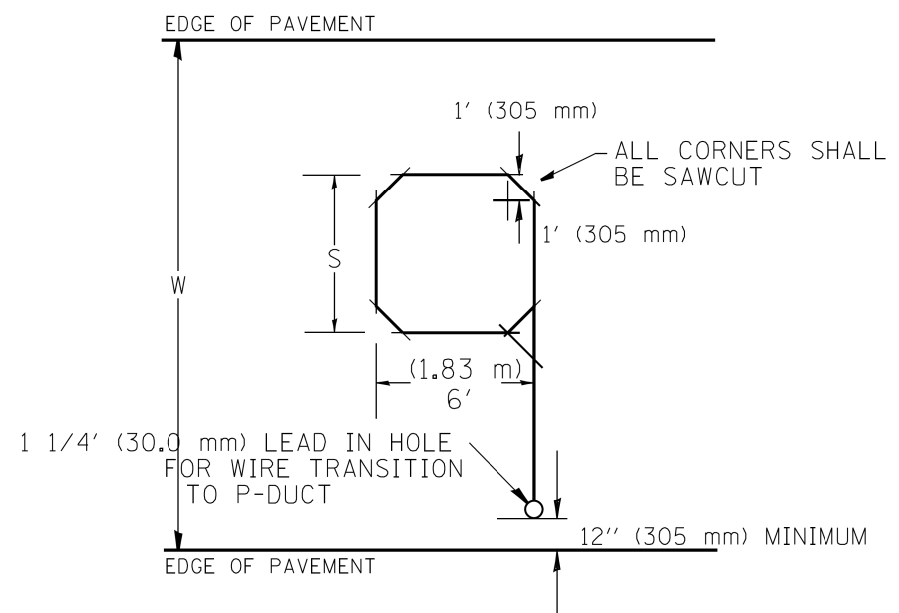
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	29
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	



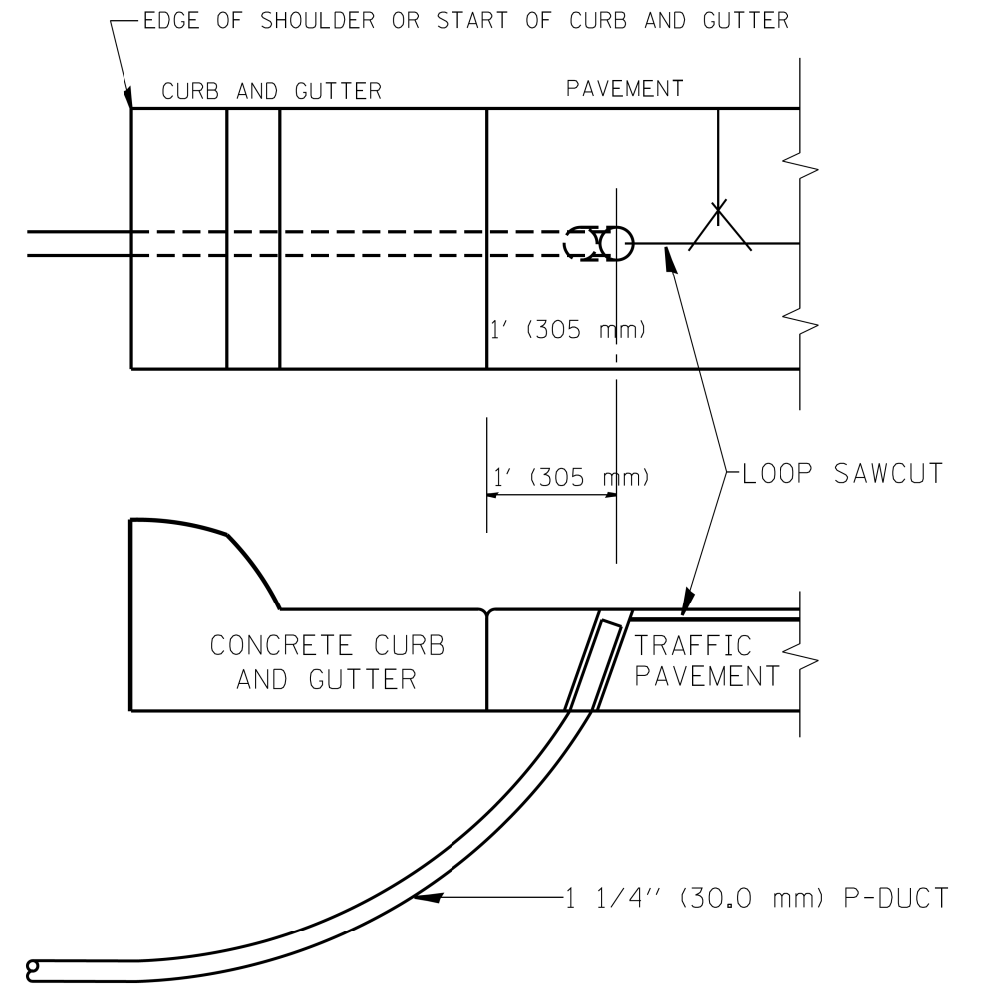
MINIMUM 1" (25 mm) HEAT SHRINK TUBING OVERLAP ON WIRE, PVC & SHIELDED CABLE TO FORM WATER TIGHT SEAL

LOOP SPLICING REQUIREMENTS

WIDTH (W)	WIDTH (S)
12' (3.7 m)	8' (2.5 m)
13' (4.0 m)	9' (2.8 m)
14' (4.3 m)	10' (3.1 m)
15' (4.6 m)	11' (3.4 m)
16' (4.9 m)	12' (3.7 m)
17' (5.2 m)	13' (4.0 m)
18' (5.5 m)	14' (4.3 m)
19' (5.8 m)	15' (4.6 m)
20' (6.1 m)	18' (4.9 m)
21' (6.4 m)	17' (5.2 m)
22' (6.7 m)	18' (5.5 m)
23' (7.0 m)	19' (5.8 m)
24' (7.3 m)	20' (6.1 m)
25' (7.6 m)	21' (6.4 m)



TYPICAL "S" FT. BY 6' (1.83 m) INDUCTION LOOP SAWCUT LAYOUT FOR RAMPS



CURB AND GUTTER LOOP LEAD-IN TRANSITION DETAIL

NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

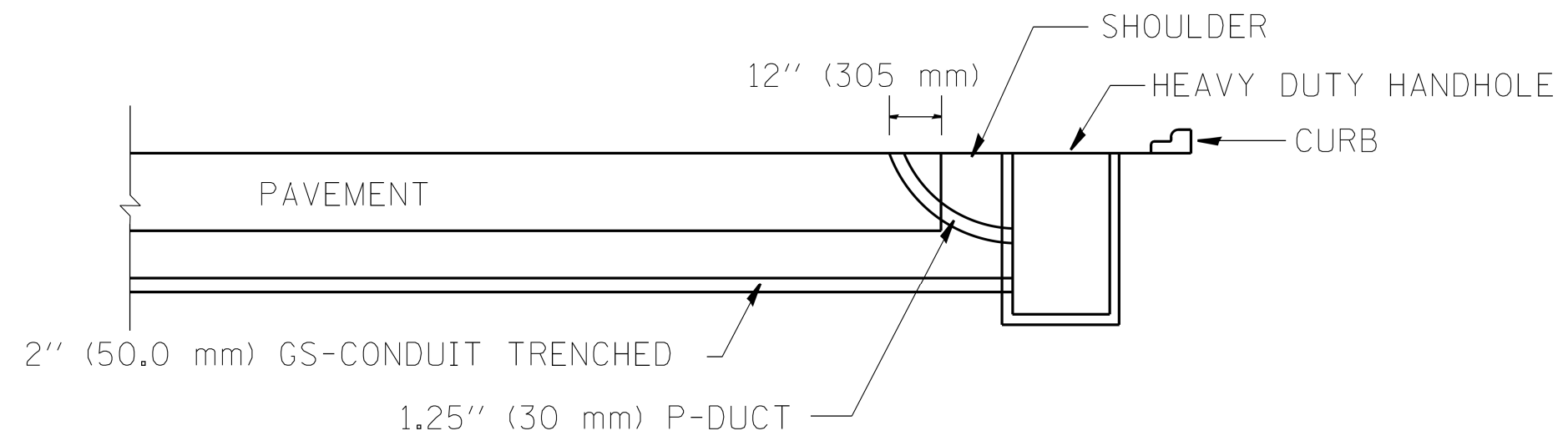
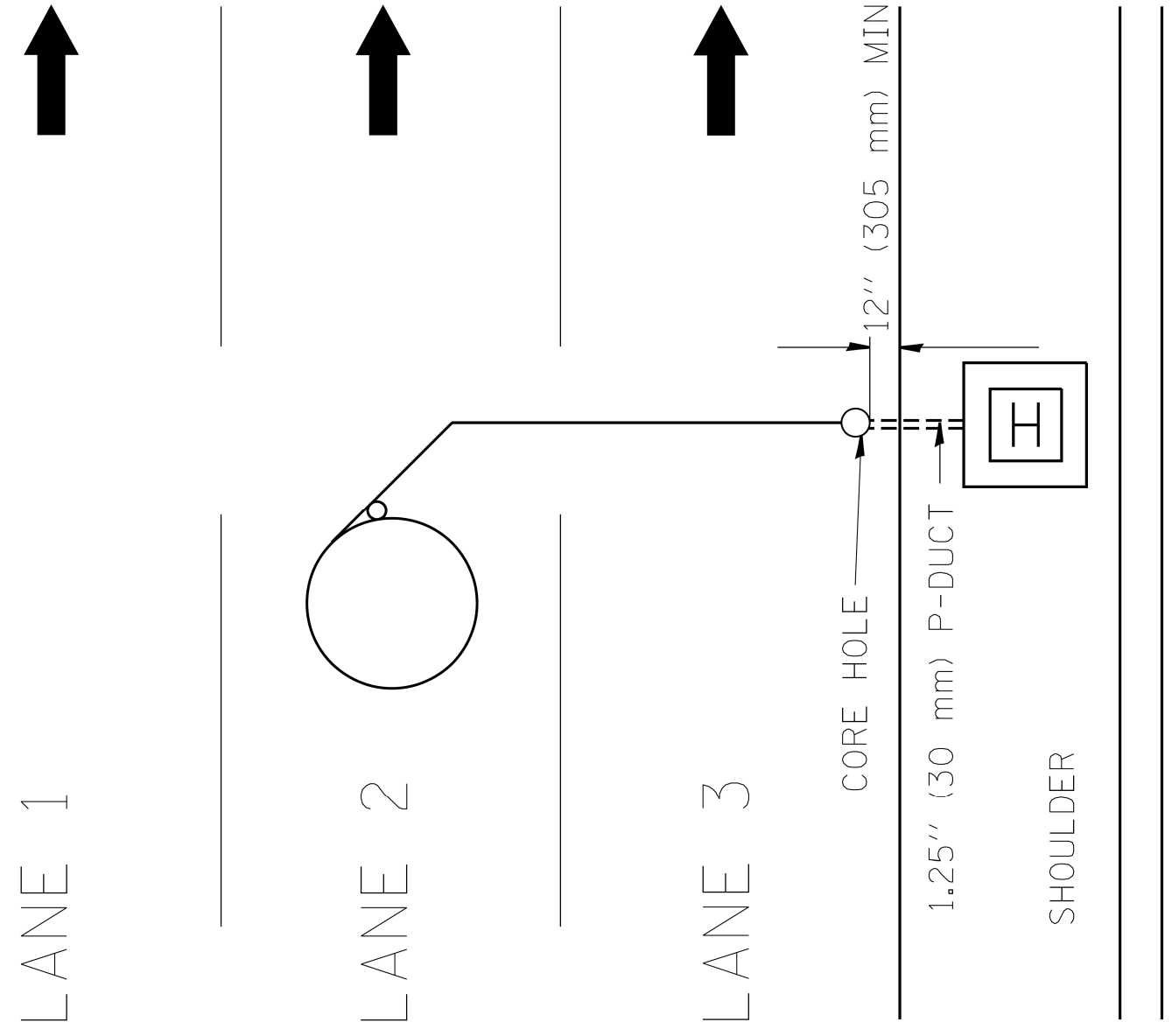
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 PROJECT CONTACT: PROJECT CONTACT
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	PLOT SCALE = 100.0000' / 1"	CHECKED - R.L.	REVISED - 05/96
	PLOT DATE = 5/17/2012	DATE - 6-22-94	REVISED - 10/96

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RECTANGULAR INDUCTION LOOP TYPICAL			
SCALE: NONE	SHEET NO. OF SHEETS	STA. TO STA.	

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	30
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62B59	



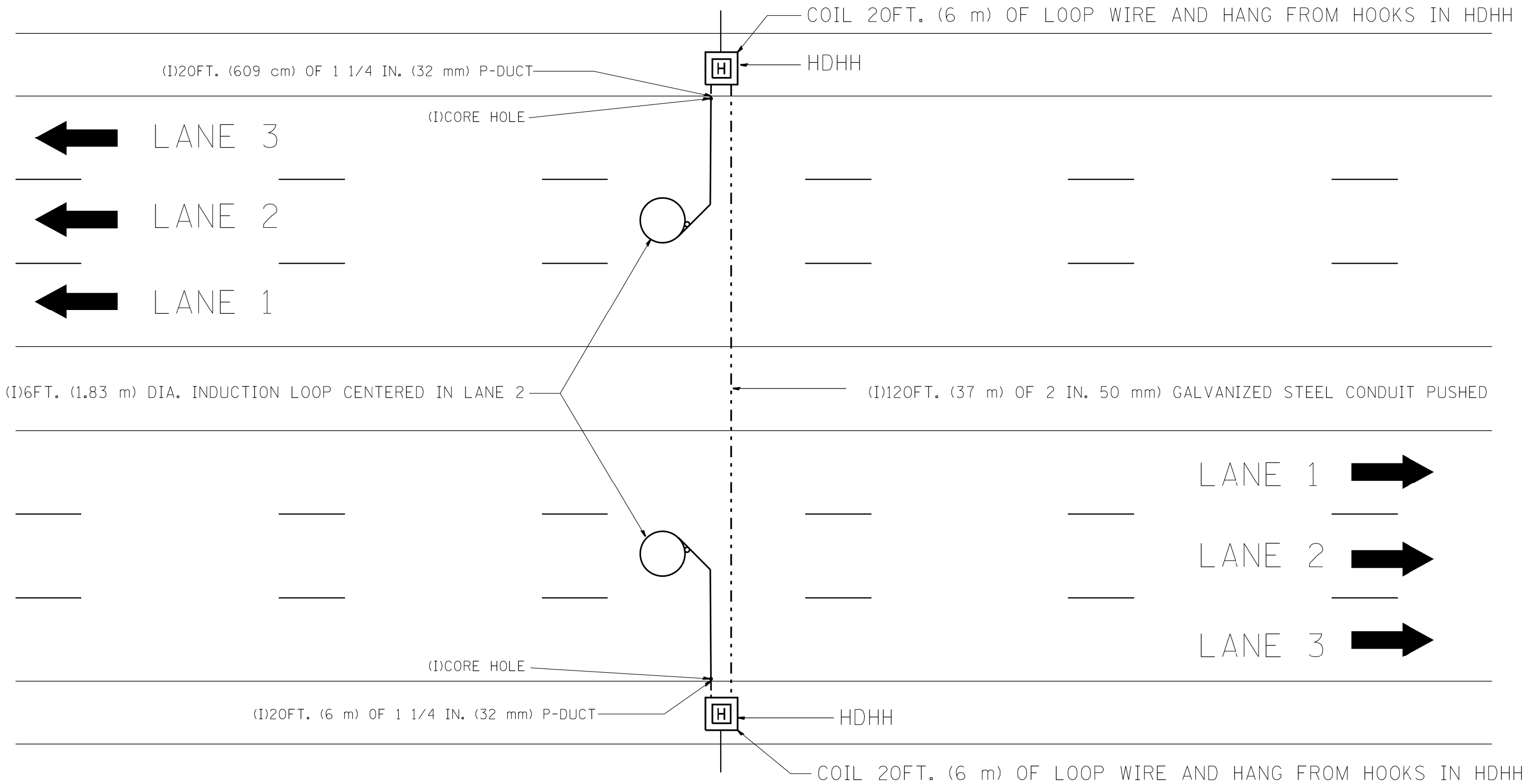
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		DRAWN - C.M.	REVISED - T.C.
		CHECKED - R.L.	REVISED -
		DATE - 11/7/95	REVISED -

STATE OF ILLINOIS
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TRAFFIC SYSTEMS CENTER

DIVE HOLE DUCT SYSTEM
 SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A. I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.1RS-5	COOK	34	32
CONTRACT NO.			62B59	



NOTE:

THE COST OF LOOP WIRE IN HDHH IS INCIDENTAL TO THE INDUCTION LOOP.
IT SHALL NOT BE MEASURE FOR PAYMENT.

COMPANY NAME: #COMPANY NAMES
PROJECT CONTACT: #PROJECT CONTACTS
DATE PLOTTED: 5/17/2012 9:26:21 AM
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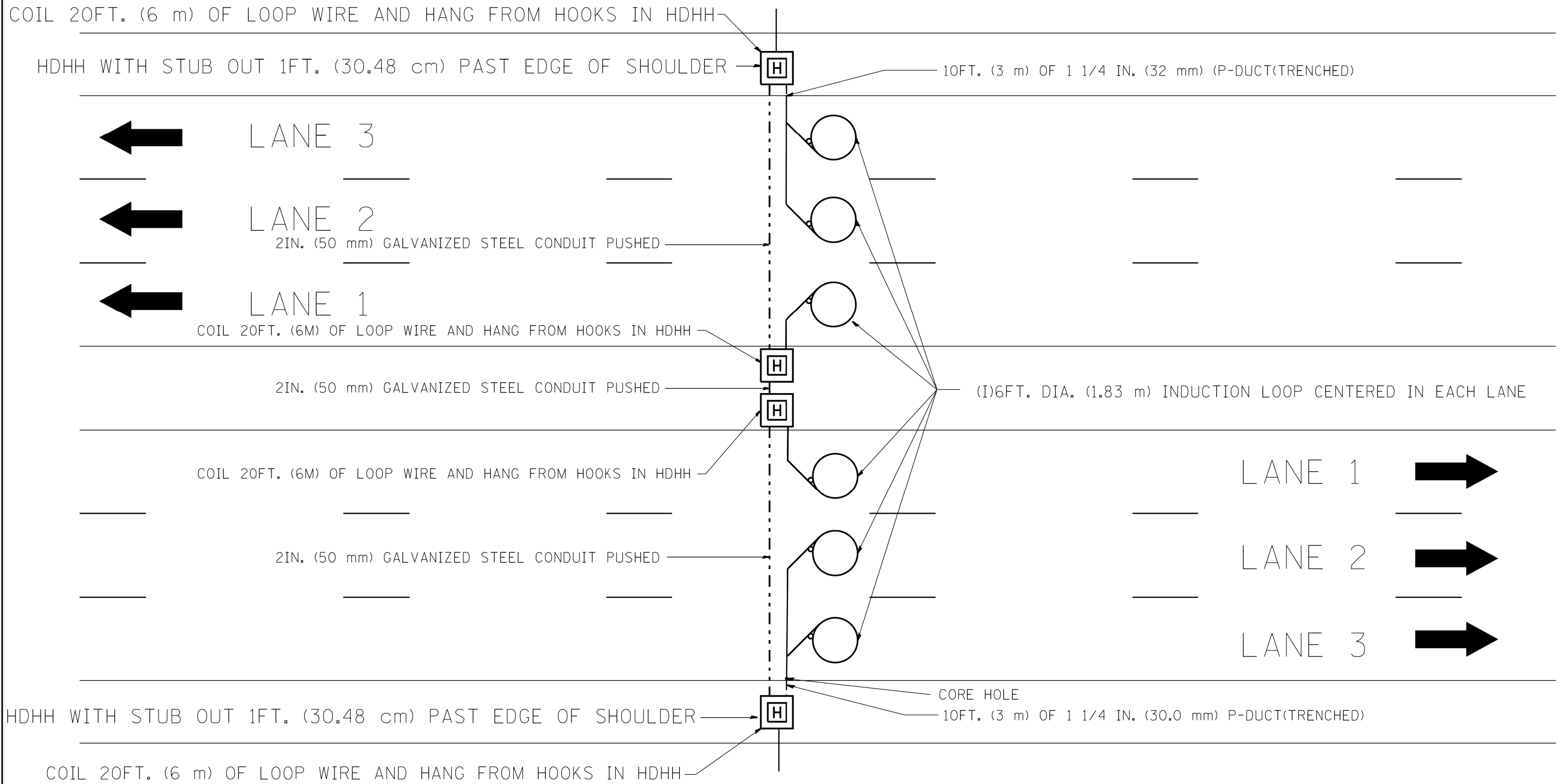
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		DATE - 01-31-07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER**

ONE LANE COUNT STATION

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A. I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	33
CONTRACT NO.			62B59	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



NOTE:

THE COST OF LOOP WIRE IN HDHH IS INCIDENTAL TO THE INDUCTION LOOP. IT SHALL NOT BE MEASURED FOR PAYMENT.

COMPANY NAME: #COMPANY NAME#
 PROJECT CONTACT: #PROJECT CONTACT#
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	PLOT DATE = 5/17/2012	DATE - 12-02-2010	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 TRAFFIC SYSTEMS CENTER

3 LANE COUNT STATION
 (DIFFERENT DIRECTIONS)

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A. I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	1516.IRS-5	COOK	34	34
CONTRACT NO.			62B59	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				