

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	1
		ILLINOIS	CONTRACT NO. 76P04	

D-98-133-20



PROPOSED HIGHWAY PLANS

FAI ROUTE 64 (I-064)
SECTION 82-9HVB-2
BRIDGE DECK, JOINT REPLACE &
APPROACH ROADWAY REPAIRS – CM
ST. CLAIR COUNTY

C-98-143-20

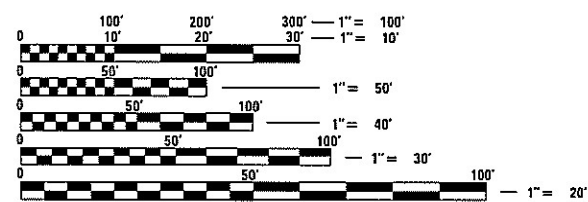
FOR INDEX OF SHEETS, SEE SHEET NO. 2

EB 64 (SN 082-0192)
2019 ADT = 17100 (ACTUAL)
2022 ADT = 17500 (ESTIMATED)
2042 ADT = 22300 (ESTIMATED)
SU = 2.2% MU = 22.2%

WB 64 (SN 082-0193)
2019 ADT = 17300 (ACTUAL)
2022 ADT = 17700 (ESTIMATED)
2042 ADT = 22600 (ESTIMATED)
SU = 2.3% MU = 23.7%

EB 64 (SN 082-0194)
2019 ADT = 14800 (ACTUAL)
2022 ADT = 15200 (ESTIMATED)
2042 ADT = 19300 (ESTIMATED)
SU = 2.5% MU = 25.7%

WB 64 (SN 082-0195)
2019 ADT = 15100 (ACTUAL)
2022 ADT = 15500 (ESTIMATED)
2042 ADT = 19700 (ESTIMATED)
SU = 2.2% MU = 27.2%

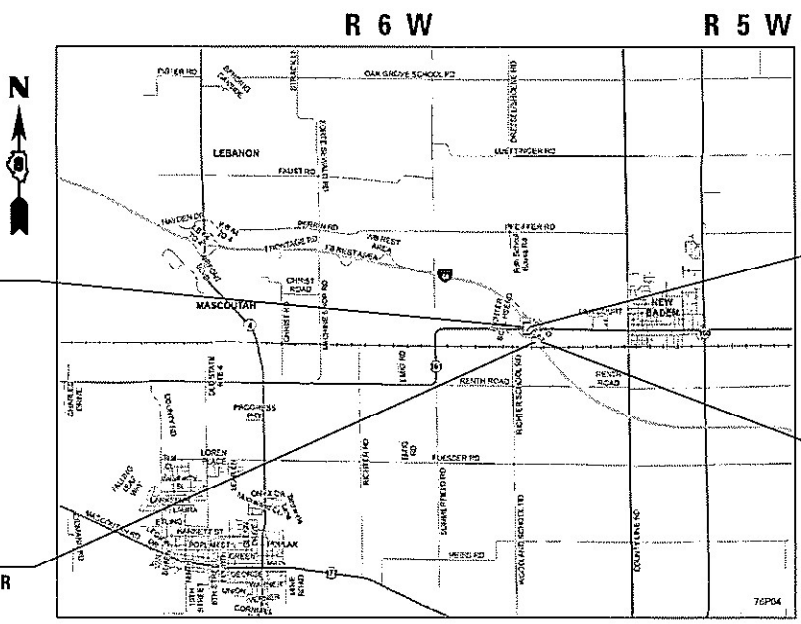


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER CHERYL KEPLAR 618-346-3123
PROJECT MANAGER J'NAYE LEE 618-346-3201

CONTRACT NO. 76P04



PROJECT LOCATION 1:
EB I-64 OVER IL 161
SN 082-0192
STA 1365 + 47.46
LATITUDE: 38.53547
LONGITUDE: -89.72903

PROJECT LOCATION 3:
EB I-64 OVER SOUTHERN RR
SN 082-0194
STA 1381 + 31.32
LATITUDE: 38.53282
LONGITUDE: -89.72699

PROJECT LOCATION 2:
WB I-64 OVER IL 161
SN 082-0193
STA 1365 + 47.46
LATITUDE: 38.53539
LONGITUDE: -89.72897

PROJECT LOCATION 4:
WB I-64 OVER NORFOLK SOUTHERN RR
SN 082-0195
STA 1381 + 32
LATITUDE: 38.53273
LONGITUDE: -89.72693

ILLINOIS DIVISION, MP 31.3

GROSS LENGTH = 1888.23 FT. = 0.358 MILE
NET LENGTH = 582.78 FT. = 0.100 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 16, 2021
Keith Roberts REGIONAL ENGINEER

May 7, 2021 Joe A. Etk
ENGINEER OF DESIGN AND ENVIRONMENT

May 7, 2021 James J. [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

- 1 COVER SHEET
- 2 INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES & COMMITMENTS
- 3-5 SUMMARY OF QUANTITIES
- 6-7 TYPICAL SECTION
- 8 SCHEDULES
- 9-17 TRAFFIC CONTROL PLAN
- 18 WIDE LOAD SIGNING
- 19-26 BRIDGE DETAILS

HIGHWAY STANDARDS

- 000001-08
- 001001-02
- 001006
- 701400-10
- 701402-12
- 701411-09
- 701901-08
- 704001-08
- 780001-05
- 781001-04
- 782006-01
- 643001-02

COMMITMENTS

NONE

GENERAL NOTES

1. UTILITIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

UTILITY	CONTACT INFORMATION	ABOVE GROUND	BELOW GROUND
*CHARTER COMMUNICATIONS, INC. (CABLE TV)	4336 EAST HIGHWAY 161; BELLEVILLE, IL 62221	X	X
*FRONTIER COMMUNICATIONS (COMMUNICATIONS)	SOUTHERN DIVISION 111 E. STATE STREET; MASCOUTAH, IL 62258	X	X
*VILLAGE OF NEW BADEN (WATER & SANITARY SEWER)	ONE EAST HANOVER STREET; NEW BADEN, IL 62265		X
*WINDSTREAM KDL, INC. (COMMUNICATIONS)	3701 COMMUNICATIONS WAY; EVANSVILLE, IN 47715	X	X

MEMBERS OF J.U.L.I.E. CALL TOLL FREE (800) 892-0123 OR 811 AND ARE INDICATED BY *. NON- J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.

2. NO SURVEY WAS PERFORMED FOR THIS PROJECT AND THE PLANS WERE CREATED USING MICROFILM AND FIELD MEASUREMENTS.
3. THE RESIDENT ENGINEER SHALL VERIFY THE EXISTENCE OF HIGHWAY LIGHTING AND/OR INTELLIGENT TRANSPORTATION SYSTEMS (I.T.S.) UTILITIES WITHIN THE PROJECT LIMITS. IF HIGHWAY LIGHTING AND/OR I.T.S. EXISTS WITHIN THE PROJECT LIMITS, AND IF THESE ITEMS REQUIRE LOCATING, THE CONTRACTOR SHALL BE DIRECTED TO DO SO ACCORDING TO SECTION 803 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
4. FOUR CHANGEABLE MESSAGE BOARDS SHALL BE REQUIRED FOR THIS PROJECT. THEY SHALL BE PLACED TWO WEEKS PRIOR TO ANY LANE CLOSURE. THE CHANGEABLE MESSAGE BOARDS SHALL BE PLACED ALONG I-64 EASTBOUND AND WESTBOUND LANES AND AT BOTH ENTRANCE RAMP WITHIN THE PROJECT LIMITS, OR AT THE DIRECTION OF THE ENGINEER. A TOTAL OF 204 CAL DAYS HAVE BEEN INCLUDED IN THE PLANS. MAINLINE: 2 SIGNS X 14 DAYS SET UP = 28 CAL DAYS. ENTRANCE RAMP: (74 CAL DAYS FOR PROJECT DURATION + 14 DAYS SET UP) X 2 SIGNS = 176 CAL DAYS.
5. THE DEPARTMENT STRONGLY ENCOURAGES THE PRIME CONTRACTOR AND THEIR APPROVED SUB-CONTRACTORS TO HIRE MINORITY, WOMEN AND DISADVANTAGED INDIVIDUALS FROM ITS FEDERALLY FUNDED HIGHWAY CONSTRUCTION CAREERS TRAINING PROGRAM (HCCTP) TO HELP MEET WORKFORCE AND TRAINEE GOALS. THIS PROGRAM IS TRAINING MINORITIES, WOMEN AND DISADVANTAGED INDIVIDUALS IN HIGHWAY CONSTRUCTION-RELATED SKILLS, E.G., MATH FOR THE TRADES, JOB READINESS, TECHNICAL SKILLS COURSEWORK (CARPENTRY, CONCRETE FLATWORK, BLUEPRINT READING, SITE PLANS, SITE WORK, TOOLS USE, ETC.) AND OSHA 10 HOUR CERTIFICATION, TO PREPARE THEM FOR A CAREER IN THE HIGHWAY CONSTRUCTION TRADES. GRADUATES ARE WELL-TRAINED AND READY TO BECOME PRODUCTIVE ENTRY-LEVEL CONSTRUCTION WORKERS. CONTACT THE DISTRICT 8 EEO OFFICE AT 618-346-3360 AND/OR THE HCCTP COORDINATOR AT 618-874-6528 TO LEARN MORE ABOUT THE PROGRAM AND FOR ASSISTANCE IN MEETING WORKFORCE AND TRAINEE GOALS.

6. THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

MIXTURE USE	POLY SURFACE
AC/PG	SBS PG 76-22
RAP % (MAX)	SEE SPECIAL PROVISION
DESIGN AIR VOIDS	4.0% @ Ndes=80
MIX COMPOSITION (Gradation)	SMA 9.5
FRICTION AGG	MIXTURE "E"
QUALITY MGMT PROGRAM	QC/QA

PLAN QUANTITIES FOR HOT-MIX ASPHALT ARE CALCULATED USING A UNIT WEIGHT OF 112 LB/SQ YD/IN.

7. THE CONTACT FOR THE NORFOLK SOUTHERN RAILROAD IS SHOWN BELOW:
 SCOTT OVERBEY
 ENGINEER PUBLIC IMPROVEMENTS
 NORFOLK SOUTHERN CORPORATION
 1200 PEACHTREE STREET, NE
 ATLANTA, GA 30309
 (404) 582 5588 [OFFICE]
 (404) 529-2589 [FAX]
 SCOTT.OVERBEY@NSCORP.COM
8. ALL WORK TO BE PERFORMED ON, OVER, UNDER, OR ADJACENT TO THE RAILROAD RIGHT-OF-WAY SHALL COMPLY WITH THE NORFOLK SOUTHERN RAILWAY COMPANY ("RAILROAD", "NSR" OR "NS") PUBLIC PROJECTS MANUAL (APPENDIX E, SPECIAL PROVISIONS FOR THE PROTECTION OF RAILWAY INTERESTS, AND APPENDIX H1, OVERHEAD GRADE SEPARATION DESIGN CRITERIA). WHEN IN CONFLICT WITH OTHER PROJECT SPECIFICATIONS, THE MOST STRINGENT ONE SHALL APPLY.
9. EXISTING VERTICAL AND HORIZONTAL CLEARANCES MUST BE VERIFIED BY CONTRACTOR PRIOR TO COMMENCEMENT OF WORK.
10. TRACK AND GROUND PROTECTION SHALL BE PROVIDED TO PROTECT THE BALLAST SECTION AND GROUND FROM CONTAMINATION BY SPENT ABRASIVES OR OTHER MATERIAL FROM FALLING ONTO THE RAILWAY'S TRACKS AND RIGHT OF WAY. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL REMOVAL, REMEDIATION, AND RESTORATION WORK.
11. THE MINIMUM HORIZONTAL AND VERTICAL TEMPORARY AND PERMANENT CLEARANCES SHOULD BE SHOWN ON THE PLANS. PER NS PUBLIC PROJECTS MANUAL, APPENDIX H1 (OVERHEAD GRADE SEPARATION DESIGN CRITERIA), SECTIONS 2.A AND 2.B, THE MINIMUM ALLOWABLE TEMPORARY CLEARANCES SHALL BE INDICATED ON THE GENERAL PLAN AND ELEVATION SHEET. THE PERMANENT CLEARANCE SHALL BE CORRELATED WITH THE METHODS OF CONSTRUCTION SO THAT TEMPORARY CONSTRUCTION CLEARANCES ARE NOT LESS THAN THE MINIMUM ALLOWED. THE MINIMUM TEMPORARY CLEARANCES OF 22'-0" VERTICALLY ABOVE TOP OF HIGHEST RAIL AND 13'-0" HORIZONTALLY FROM CENTERLINE OF TANGENT TRACK (OR 14'-0" HORIZONTALLY FROM CENTERLINE OF CURVED TRACK) SHALL BE MAINTAINED AT ALL TIMES. A MINIMUM VERTICAL PERMANENT CLEARANCE OF 23'-0" MEASURED FROM TOP OF HIGH RAIL TO THE LOWEST POINT OF STRUCTURE, MEASURED AT A POINT OFFSET 5'-6" FROM CENTERLINE OF TRACK, SHALL BE PROVIDED. MINIMUM CLEARANCES FOR CONTAINMENT SYSTEM, OPERATIONS AND REMOVAL SHALL NOT BE LESS THAN 22'-0" ABOVE TOP OF RAIL OF NORFOLK SOUTHERN TRACKS AND 13'-0" FROM CENTERLINE OF TRACK MEASURED AT RIGHT ANGLE. ALL PROPOSED TEMPORARY CLEARANCES WHICH ARE LESS THAN THOSE LISTED ABOVE MUST BE SUBMITTED TO RAILROAD ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION AND MUST ALSO BE AUTHORIZED BY THE REGULATORY BODY OF THE STATE IF LESS THAN THE LEGALLY PRESCRIBED CLEARANCES.
12. FOR PROJECTS REQUIRING MORE THAN 30 CONSECUTIVE DAYS OF FLAGGING, CONTRACTOR SHALL PROVIDE THE FLAGMAN A SMALL WORK AREA WITH A DESK/COUNTER AND CHAIR WITHIN THE FIELD/SITE TRAILER, INCLUDING THE USE OF BATHROOM FACILITIES, WHERE THE FLAGMAN CAN CHECK IN/OUT WITH THE PROJECT, AS WELL AS TO THE FLAGMAN'S HOME TERMINAL. THE WORK AREA SHOULD PROVIDE ACCESS TO TWO (2) ELECTRICAL OUTLETS FOR RECHARGING RADIO(S), AND A LAPTOP COMPUTER; AND HAVE THE ABILITY TO PRINT OFF NEEDED DOCUMENTATION AND ORDERS AS NEEDED AT THE FIELD/SITE TRAILER. THIS SHOULD AID IN MAXIMIZING THE FLAGMAN'S TIME AND EFFICIENCY ON THE PROJECT.

MODEL: MODELNAME\$
FILE NUMBER: FILE#

USER NAME = \$USERS\$	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES & COMMITMENTS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	DRAWN -	REVISED -			64	82-9HVB-2	ST. CLAIR	26	2	
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -			CONTRACT NO. 76P04					
PLOT DATE = \$DATES\$	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
				SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	

REV. - MS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				100% STATE	100% STATE	
				BRIDGE	BRIDGE	
				0047	0047	
				082-0192 & 0193	082-0194 & 0195	
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	432		216	216
40605024	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "E", N80	TON	108		54	54
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	960		480	480
50102400	CONCRETE REMOVAL	CU YD	8.2			8.2
50157300	PROTECTIVE SHIELD	SQ YD	278			278
50300255	CONCRETE SUPERSTRUCTURE	CU YD	8.4			8.4
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	760			760
50800515	BAR SPLICERS	EACH	8			8
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6		3	3
67100100	MOBILIZATION	L SUM	1		0.5	0.5
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	3		1	2
70107005	PAVEMENT MARKING BLACKOUT TAPE, 5"	FOOT	1975		900	1075
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	204		102	102
70300100	SHORT TERM PAVEMENT MARKING	FOOT	80		48	32

REV. - MS

MODEL: MODELNAME\$
FILE NAME: FILE\$

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED +
PLOT DATE = \$DATE\$	DATE -	REVISED +

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 76P04	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				100% STATE	100% STATE
				BRIDGE	BRIDGE
				0047	0047
				082-0192 & 0193	082-0194 & 0195
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	850	391	459
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	11745	5872.5	5872.5
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1775	950	825
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1775	950	825
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4	2	2
70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4	2	2
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	18	8	10
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	32	18	14
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	18	8	10
* X2700006	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 4"	FOOT	2472	1236	1236
X7010208	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402 (SPECIAL)	EACH	2	1	1
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	3915	1957.5	1957.5
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	2472	1236	1236
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	50	10	40

* SPECIALTY ITEM

REV. - MS

MODEL: MODELNAME\$
FILE NAME: FILE\$

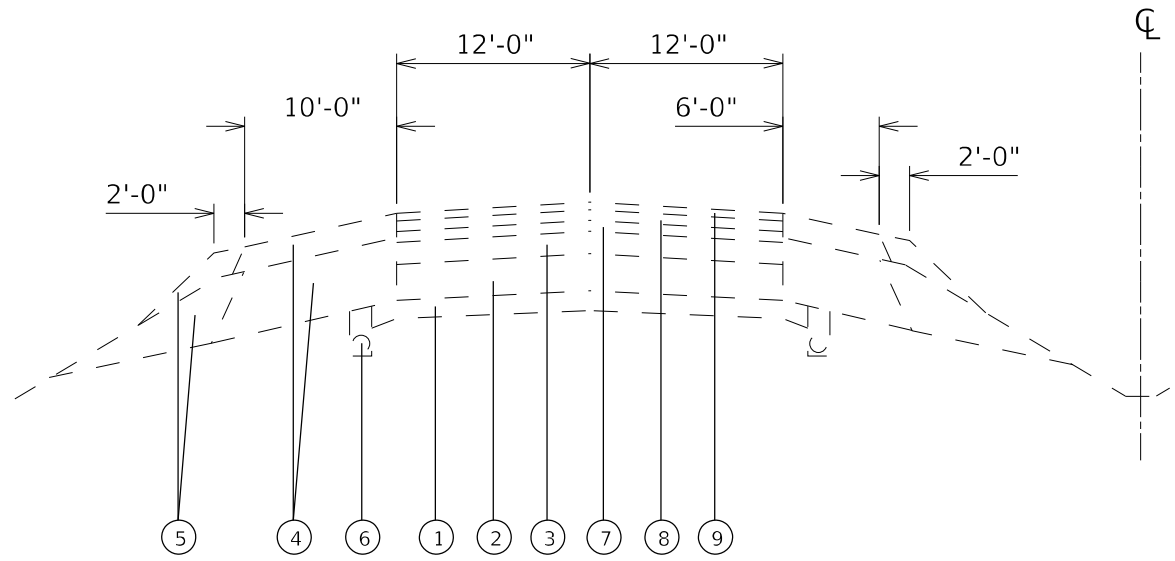
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED +
PLOT DATE = \$DATE\$	DATE -	REVISED +

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

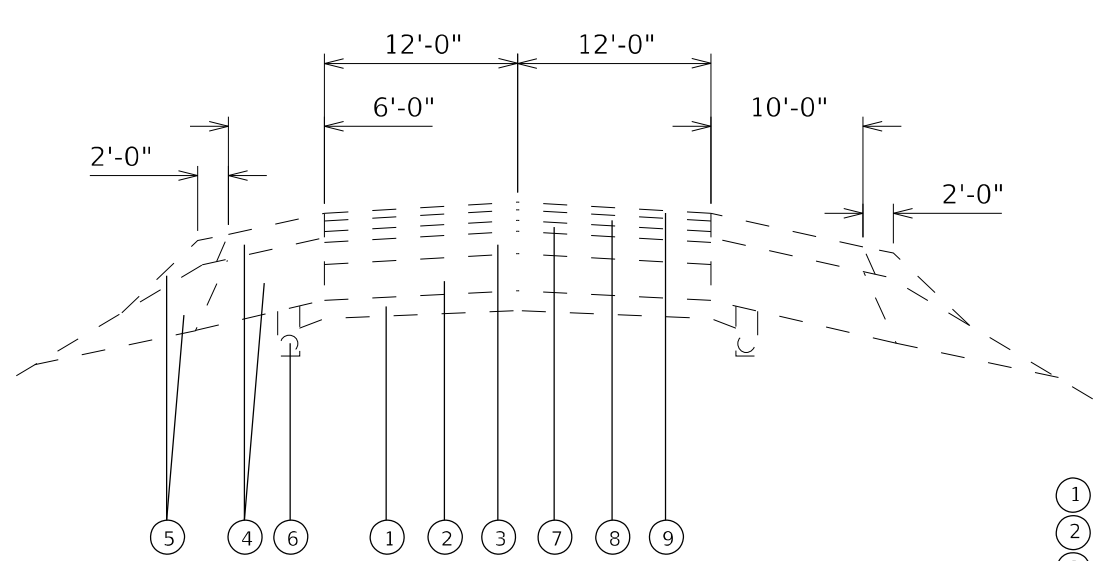
SUMMARY OF QUANTITIES

SCALE: SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	4
			CONTRACT NO. 76P04	
ILLINOIS FED. AID PROJECT				

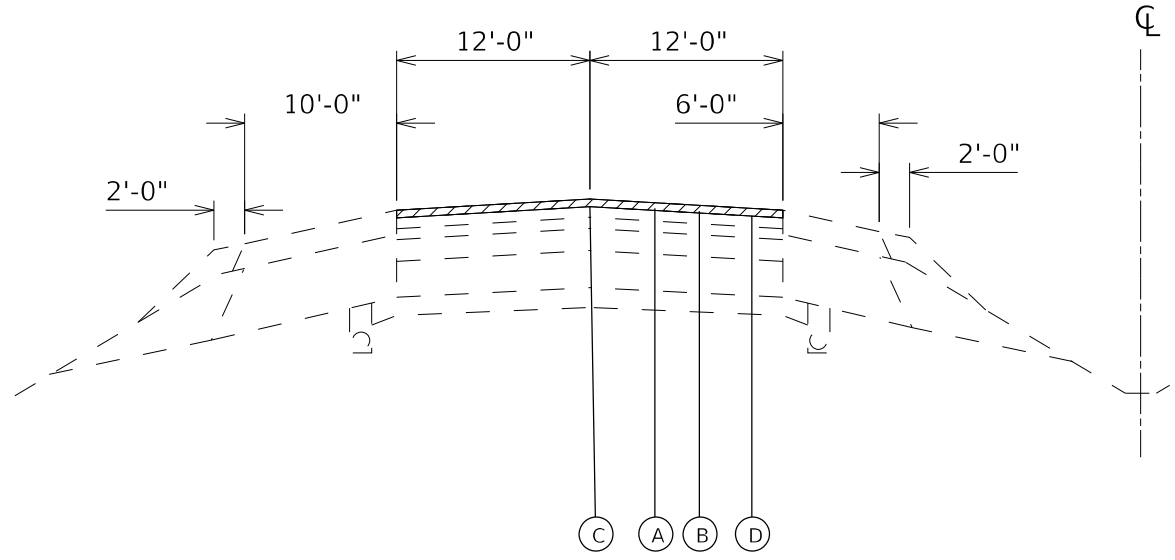


SN 082-0193
WB - EXISTING TYPICAL SECTION
 STA 1364+43.82 TO STA 1364+73.82
 STA 1366+81.44 TO STA 1367+41.44
 BRIDGE SECTION (SEE BRIDGE PLANS)

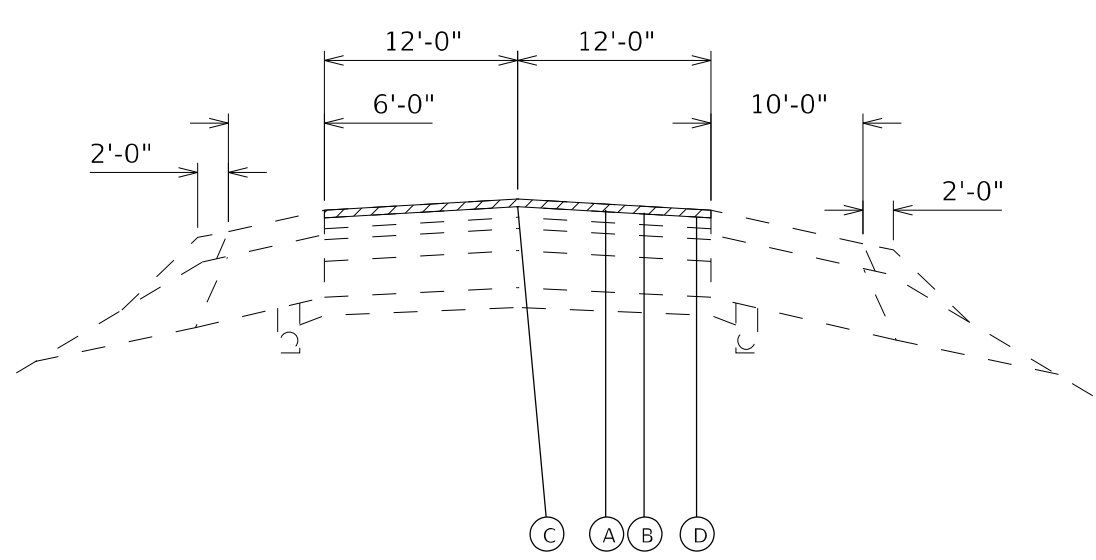


SN 082-0192
EB - EXISTING TYPICAL SECTION
 STA 1363+89.13 TO STA 1364+19.13
 STA 1366+26.75 TO STA 1366+86.75
 BRIDGE SECTION (SEE BRIDGE PLANS)

- LEGEND**
- ① EXISTING SUB-BASE 4"
 - ② EXISTING C.R.P.C.C. PAVEMENT 8"
 - ③ EXISTING RESURFACING 6½" OR 8¾"
 - ④ EXISTING BITUMINOUS SHOULDER
 - ⑤ EXISTING AGGREGATE SHOULDER, TYPE B
 - ⑥ EXISTING PIPE UNDERDRAINS
 - ⑦ EXISTING HMA BINDER COURSE, IL-19.0, N-105 2¼"
 - ⑧ EXISTING HMA BINDER COURSE N-105 2¼"
 - ⑨ EXISTING POLYMERIZED HMA SURFACE COURSE SMA N80 2"
 - Ⓐ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 - Ⓑ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "E", N80, 2"
 - Ⓒ PROPOSED LONGITUDINAL JOINT SEALANT
 - Ⓓ PROPOSED POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)



SN 082-0193
WB - PROPOSED TYPICAL SECTION
 STA 1364+43.82 TO STA 1364+73.82
 STA 1366+81.44 TO STA 1367+41.44
 BRIDGE SECTION (SEE BRIDGE PLANS)



SN 082-0192
EB - PROPOSED TYPICAL SECTION
 STA 1363+89.13 TO STA 1364+19.13
 STA 1366+26.75 TO STA 1366+86.75
 BRIDGE SECTION (SEE BRIDGE PLANS)

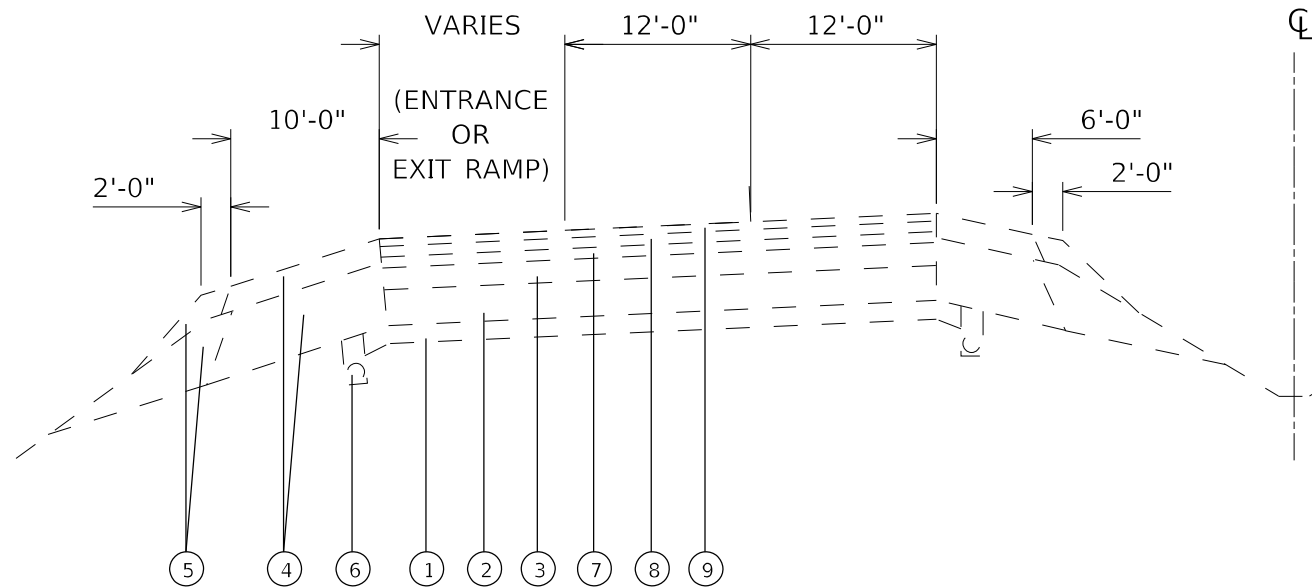
MODEL NUMBER: MMS
 FILE NAME: 76P04

USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALES	DRAWN -	REVISED -
PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE -	REVISED -

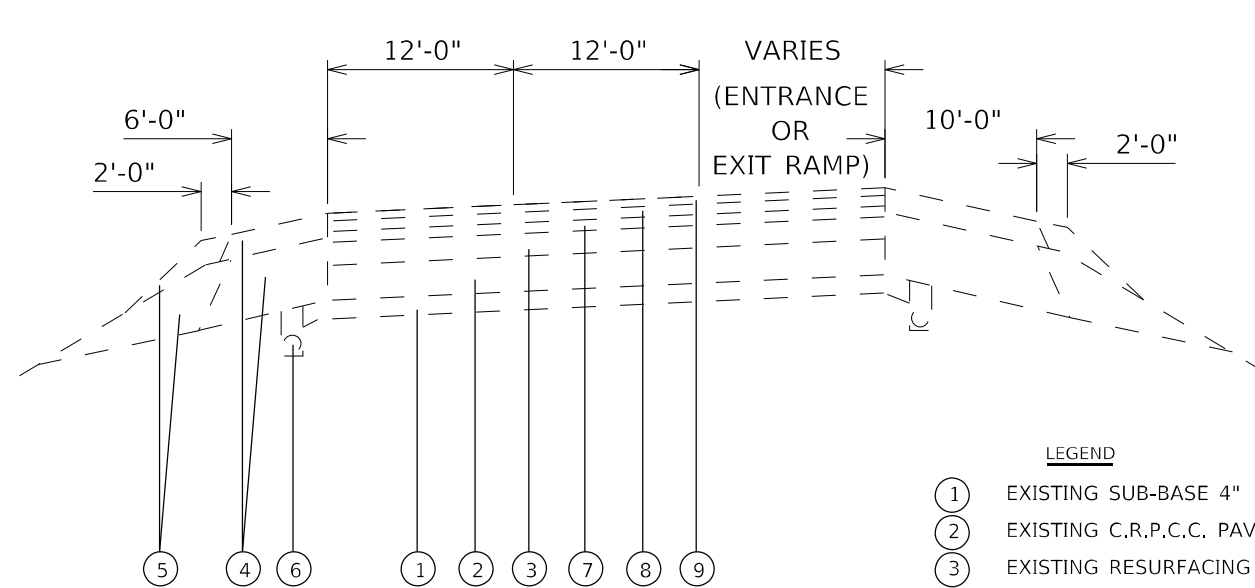
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS			
SCALE:	SHEET 1	OF 2 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	6
CONTRACT NO. 76P04				
ILLINOIS FED. AID PROJECT				

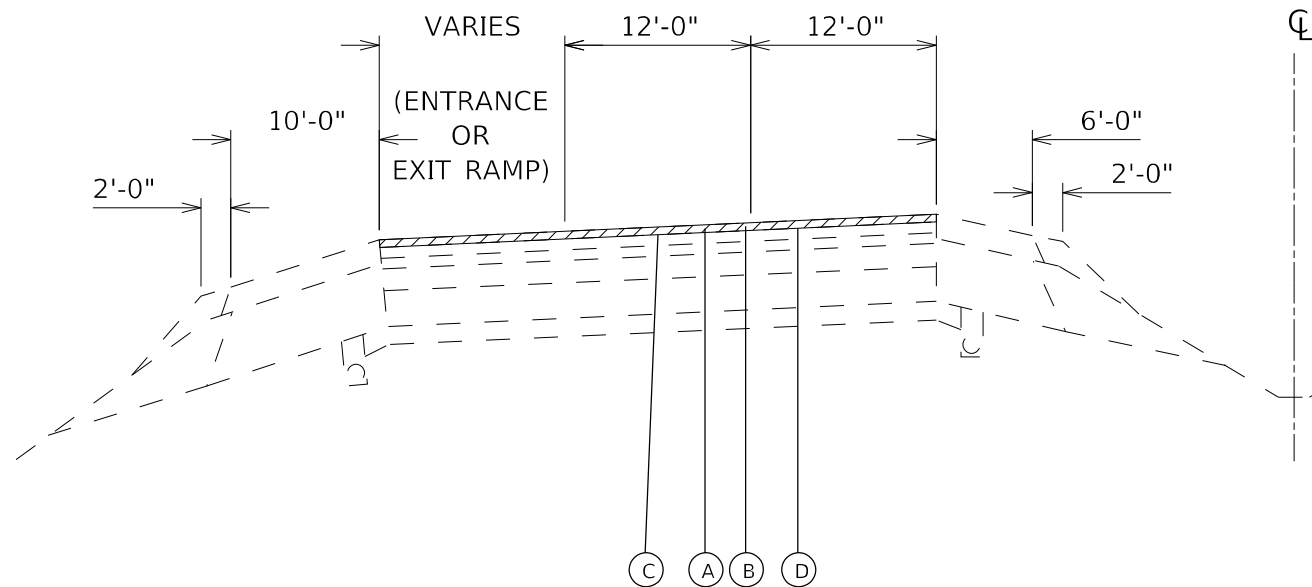


SN 082-0195
 WB - EXISTING TYPICAL SECTION
 STA 1380+46.20 TO STA 1380+76.20
 STA 1382+47.36 TO STA 1382+77.36
 BRIDGE SECTION (SEE BRIDGE PLANS)

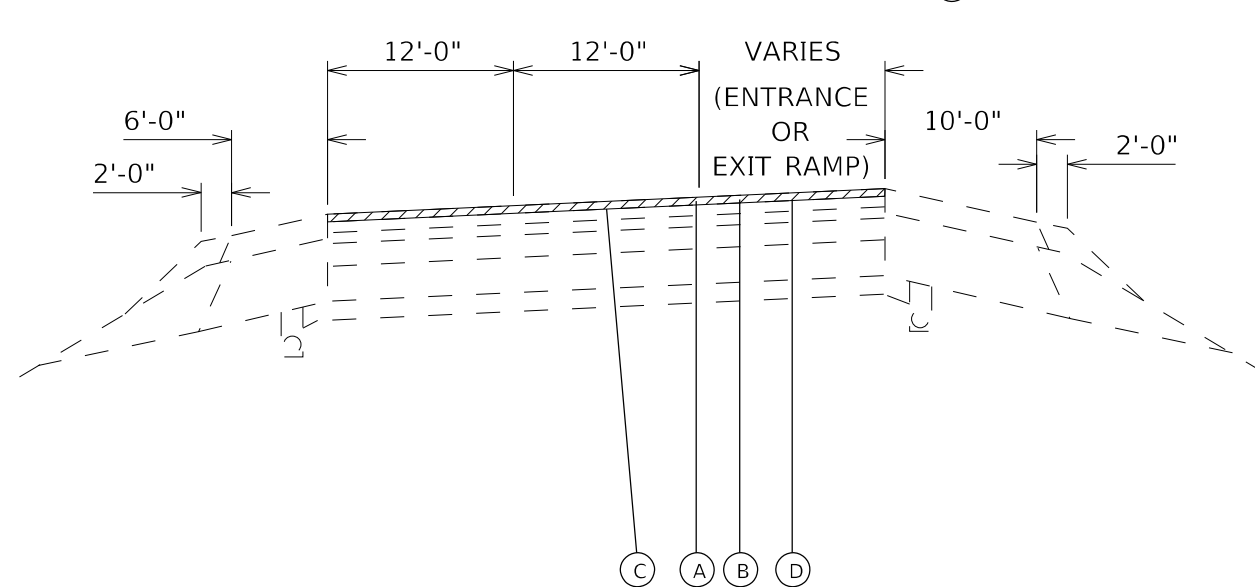


SN 082-0194
 EB - EXISTING TYPICAL SECTION
 STA 1379+86.85 TO STA 1380+16.85
 STA 1381+86.23 TO STA 1382+16.23
 BRIDGE SECTION (SEE BRIDGE PLANS)

- LEGEND**
- ① EXISTING SUB-BASE 4"
 - ② EXISTING C.R.P.C.C. PAVEMENT 8"
 - ③ EXISTING RESURFACING 6½" OR 8¾"
 - ④ EXISTING BITUMINOUS SHOULDER
 - ⑤ EXISTING AGGREGATE SHOULDER, TYPE B
 - ⑥ EXISTING PIPE UNDERDRAINS
 - ⑦ EXISTING HMA BINDER COURSE, 1L-19.0, N-105 2¾"
 - ⑧ EXISTING HMA BINDER COURSE N-105 2¾"
 - ⑨ EXISTING POLYMERIZED HMA SURFACE COURSE SMA N80 2"
 - Ⓐ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 - Ⓑ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "E", N80, 2"
 - Ⓒ PROPOSED LONGITUDINAL JOINT SEALANT
 - Ⓓ PROPOSED POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)



SN 082-0195
 WB - PROPOSED TYPICAL SECTION
 STA 1380+46.20 TO STA 1380+76.20
 STA 1382+47.36 TO STA 1382+77.36
 BRIDGE SECTION (SEE BRIDGE PLANS)



SN 082-0194
 EB - PROPOSED TYPICAL SECTION
 STA 1379+86.85 TO STA 1380+16.85
 STA 1381+86.23 TO STA 1382+16.23
 BRIDGE SECTION (SEE BRIDGE PLANS)

MODEL NUMBER: MMS
 FILE NAME: 311215

USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALES	DRAWN -	REVISED -
PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS			
SCALE:	SHEET 2	OF 2 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	7
CONTRACT NO. 76P04			ILLINOIS FED. AID PROJECT	

STAGING SCHEDULE											
	STATION	TO	STATION	PAVEMENT MARKING BLACKOUT TAPE, 5"	SHORT TERM PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING - LINE 4"	TEMPORARY PAVEMENT MARKING REMOVAL	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
				FOOT	SQFT	SOLID WHITE	FOOT	SQFT	FOOT	FOOT	EACH
STAGE I - EB LANES 082-0192 & 082-0194	1351+50.00	TO	1382+16.23			3066.23	1022.08		887.50	1.00	1.00
STAGE I - WB LANES 082-0193 & 082-0195	1364+43.82	TO	1390+00.00			2556.18	852.06		887.50	1.00	1.00
STAGE II - EB LANES 082-0192 & 082-0194	1346+50.00	TO	1382+16.23	900.00	375.00	3566.23	1188.74	887.50		1.00	1.00
STAGE II - WB LANES 082-0193 & 082-0195	1364+43.82	TO	1390+00.00	1075.00	447.92	2556.18	852.06	887.50		1.00	1.00
SUBTOTAL				1975.00	822.92	11744.82	3914.94	1775.00	1775.00	4.00	4.00
TOTAL				1975	*	11745	3915	1775.00	1775.00	4	4

PAVEMENT MARKING SCHEDULE													
	STATION	TO	STATION	LENGTH	SHORT TERM PAVEMENT MARKING	SHORT TERM PAVEMENT MARKING REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 4"			BARRIER WALL REFLECTORS, TYPE C	GROOVING FOR RECESSED PAVEMENT MARKING 5"
									SOLID YELLOW	SOLID WHITE	SKIP-DASH WHITE		
				FOOT	FOOT	SQFT	EACH	EACH	FOOT	FOOT	FOOT	EACH	FOOT
APPROACH	1363+89.13	TO	1364+19.13	30.00	8.00	2.67	2.00	2.00	30.00	30.00	7.50		67.50
SN 082-0192	1364+19.13	TO	1366+26.75	207.62					207.62	207.62	51.90	9.00	467.14
APPROACH	1366+26.75	TO	1366+86.75	60.00	16.00	5.33	2.00	2.00	60.00	60.00	15.00		135.00
APPROACH	1364+43.82	TO	1364+73.82	30.00	8.00	2.67	2.00	2.00	30.00	30.00	7.50		67.50
SN 082-0193	1364+73.82	TO	1366+81.44	207.62					207.62	207.62	51.90	9.00	467.14
APPROACH	1366+81.44	TO	1367+41.44	60.00	16.00	5.33	2.00	2.00	60.00	60.00	15.00		135.00
APPROACH	1379+86.85	TO	1380+16.85	30.00	8.00	2.67	2.00	2.00	30.00	30.00	7.50		67.50
SN 082-0194	1380+16.85	TO	1381+86.23	169.38					169.38	169.38	42.35	7.00	381.11
APPROACH	1381+86.23	TO	1382+16.23	30.00	8.00	2.67	2.00	2.00	30.00	30.00	7.50		67.50
APPROACH	1380+46.20	TO	1380+76.20	30.00	8.00	2.67	4.00	4.00	30.00	90.00	7.50		127.50
SN 082-0195	1380+76.20	TO	1382+47.36	171.16					171.16	171.16	72.79	7.00	415.11
APPROACH	1382+47.36	TO	1382+77.36	30.00	8.00	2.67	2.00	2.00	30.00	30.00	13.50		73.50
SUBTOTAL					80.00	26.67	18.00	18.00	1055.78	1115.78	299.94	32.00	2471.50
TOTAL					80	*	18	18	2472			32	2472

SHORT TERM PAVEMENT MARKING REMOVAL (TOTAL FROM SCHEDULES)	
	SHORT TERM PAVEMENT MARKING REMOVAL
	SQ FT
STAGING SCHEDULE (SUBTOTAL)	822.92
PAVEMENT MARKING SCHEDULE (SUBTOTAL)	26.67
TOTAL	850

RESURFACING SCHEDULE										
STRUCTURE	APPROACH	STATION	TO	STATION	LENGTH	WIDTH	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "E", N80	LONGITUDINAL JOINT SEALANT
082-0192	NORTH	1363+89.13	TO	1364+19.13	30.00	24.00	80.00	36.00	8.96	90.00
082-0192	SOUTH	1366+26.75	TO	1365+86.75	60.00	24.00	160.00	72.00	17.92	180.00
082-0193	NORTH	1364+43.82	TO	1364+73.82	30.00	24.00	80.00	36.00	8.96	90.00
082-0193	SOUTH	1366+81.44	TO	1367+41.44	60.00	24.00	160.00	72.00	17.92	180.00
082-0194	NORTH	1379+86.85	TO	1380+16.85	30.00	36.00	120.00	54.00	13.44	90.00
082-0194	SOUTH	1381+86.23	TO	1382+16.23	30.00	33.00	110.00	49.50	12.32	90.00
082-0195	NORTH	1380+46.20	TO	1380+76.20	30.00	43.00	143.33	64.50	16.05	90.00
082-0195	SOUTH	1382+47.36	TO	1382+77.36	30.00	32.00	106.67	48.00	11.95	90.00
TOTAL							960	432	108	900

NOTE:
AN ADDITIONAL 1730.50 FEET OF BOTH PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 4" AND GROOVING FOR RECESSED PAVEMENT MARKING 5" IS INCLUDED FOR THE STRUCTURES WHERE PAVEMENT MARKING IS REMOVED FROM PATCHING.

*SEE SHORT TERM PAVEMENT MARKING REMOVAL (TOTAL FROM SCHEDULES)

MODEL NUMBER NAMES FILE NUMBER SHEETS

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

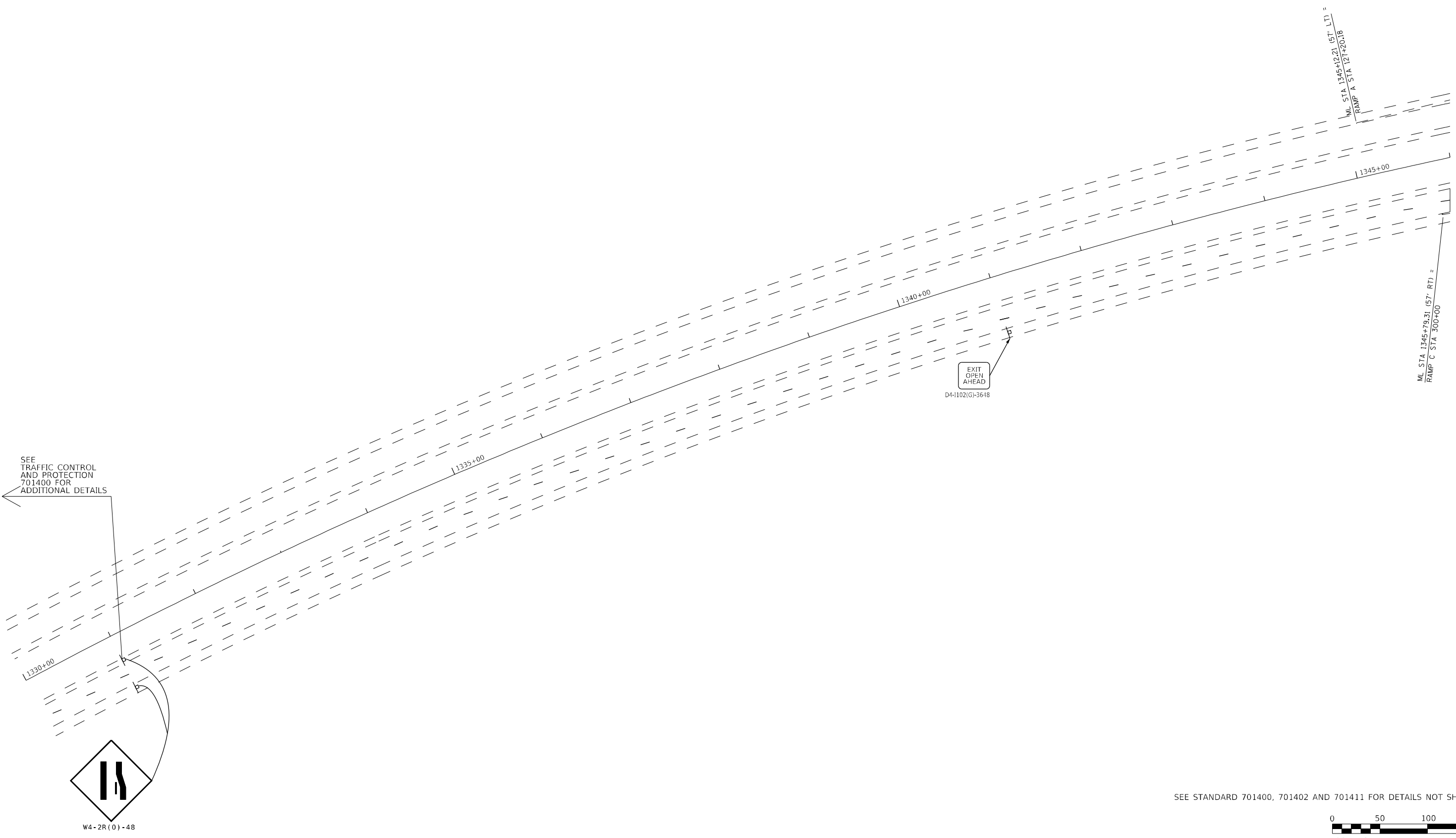
SCHEDULES			
SCALE:	SHEET 1	OF 1	SHEETS
			STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	8
CONTRACT NO. 76P04				
ILLINOIS FED. AID PROJECT				

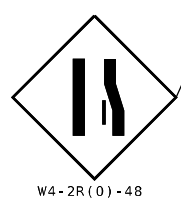
MATCH LINE STA. 1346+00

ML STA 1345+79.31 (57' RT) =
RAMP C STA 300+00

ML STA 1345+12.21 (57' LT) =
RAMP A STA 127+20.18

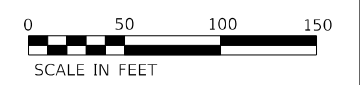


SEE TRAFFIC CONTROL AND PROTECTION 701400 FOR ADDITIONAL DETAILS



EXIT OPEN AHEAD
D4-1102(G)-3648

SEE STANDARD 701400, 701402 AND 701411 FOR DETAILS NOT SHOWN



MODEL: 1400ELEMAMES
FILE NAME: 31E15

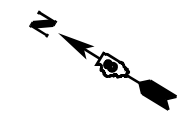
USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALES	DRAWN -	REVISED -
PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
STAGE I

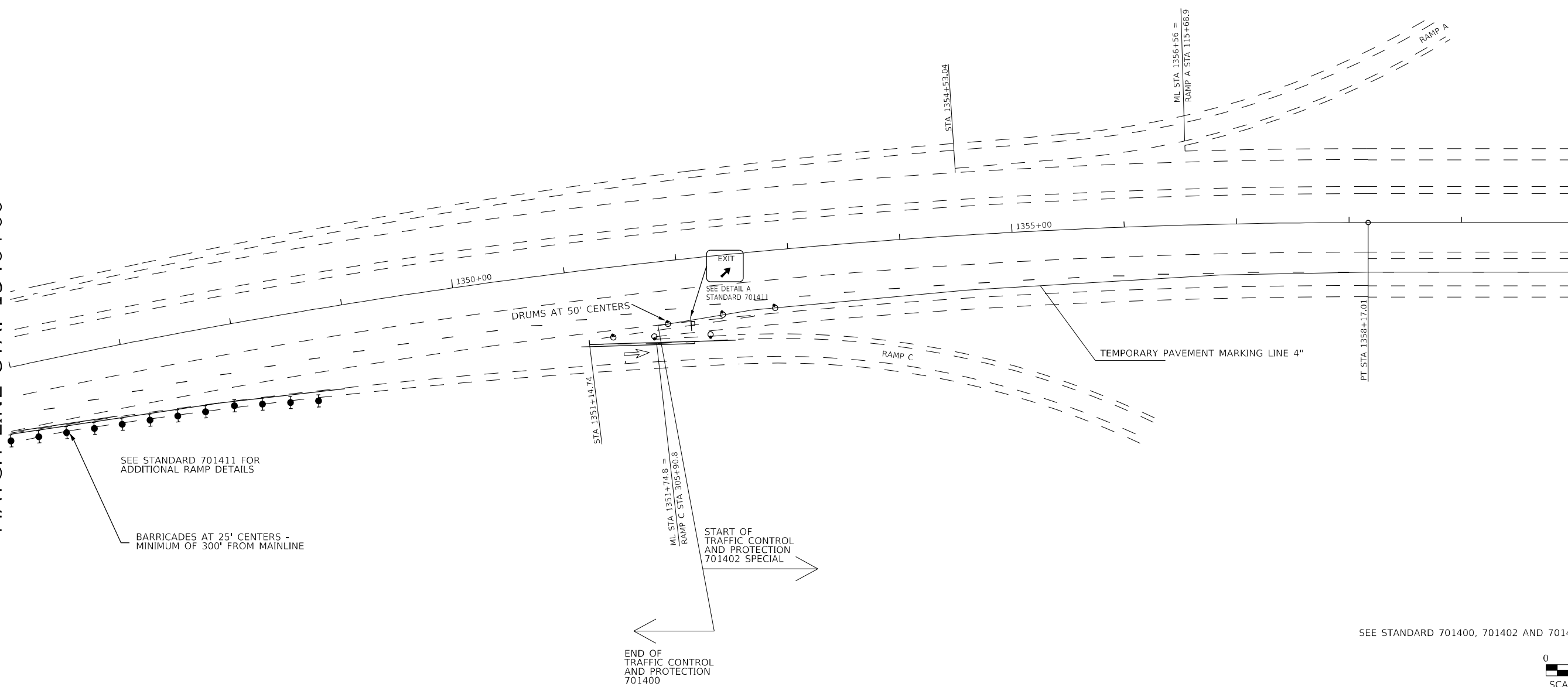
SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	9
ILLINOIS FED. AID PROJECT			CONTRACT NO. 76P04	



MATCH LINE STA. 1346+00

MATCH LINE STA. 1360+00



SEE STANDARD 701411 FOR ADDITIONAL RAMP DETAILS

BARRICADES AT 25' CENTERS - MINIMUM OF 300' FROM MAINLINE

DRUMS AT 50' CENTERS

EXIT
SEE DETAIL A STANDARD 701411

TEMPORARY PAVEMENT MARKING LINE 4"

START OF TRAFFIC CONTROL AND PROTECTION 701402 SPECIAL

END OF TRAFFIC CONTROL AND PROTECTION 701400

SEE STANDARD 701400, 701402 AND 701411 FOR DETAILS NOT SHOWN



MODEL: 1400ELEM1.MXD
FILE NAME: 311E15

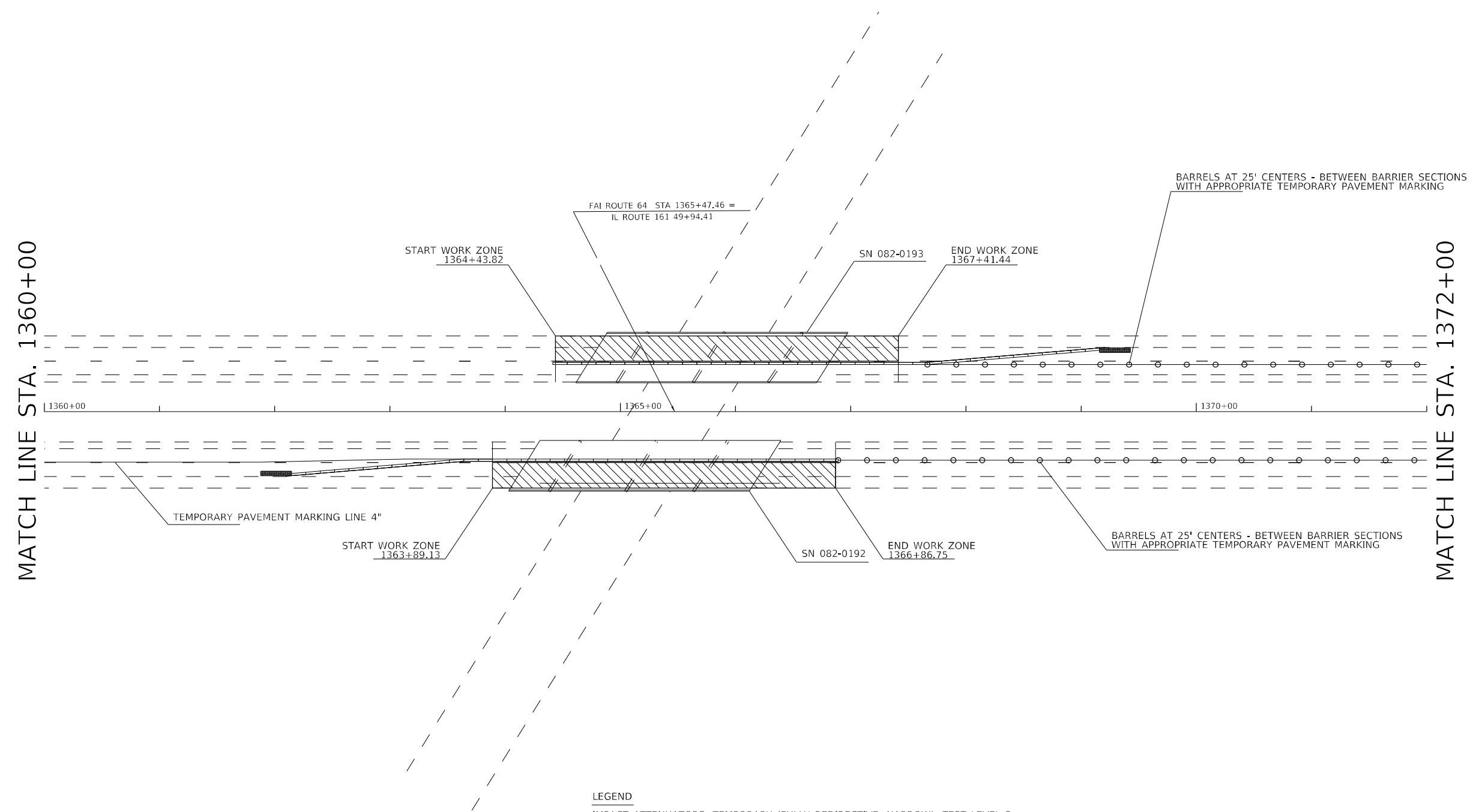
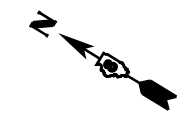
USER NAME = \$USERS	DESIGNED -	REVISED -
DRAWN -	REVISOR -	
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN
STAGE I**

SCALE: SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	10
CONTRACT NO. 76P04			ILLINOIS FED. AID PROJECT	



FAI ROUTE 64 STA 1365+47.46 =
IL ROUTE 161 49+94.41

START WORK ZONE
1364+43.82

SN 082-0193

END WORK ZONE
1367+41.44

BARRELS AT 25' CENTERS - BETWEEN BARRIER SECTIONS
WITH APPROPRIATE TEMPORARY PAVEMENT MARKING

MATCH LINE STA. 1360+00

MATCH LINE STA. 1372+00

TEMPORARY PAVEMENT MARKING LINE 4"

START WORK ZONE
1363+89.13

SN 082-0192

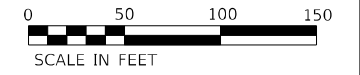
END WORK ZONE
1366+86.75

BARRELS AT 25' CENTERS - BETWEEN BARRIER SECTIONS
WITH APPROPRIATE TEMPORARY PAVEMENT MARKING

LEGEND

- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
- WORK ZONE
- RELOCATE TEMPORARY CONCRETE BARRIER (WESTBOUND-475' ; EASTBOUND-475')

SEE STANDARD 701400, 701402 AND 701411 FOR DETAILS NOT SHOWN



MODEL: 1400RELENAME
FILE NAME: 311EUS

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

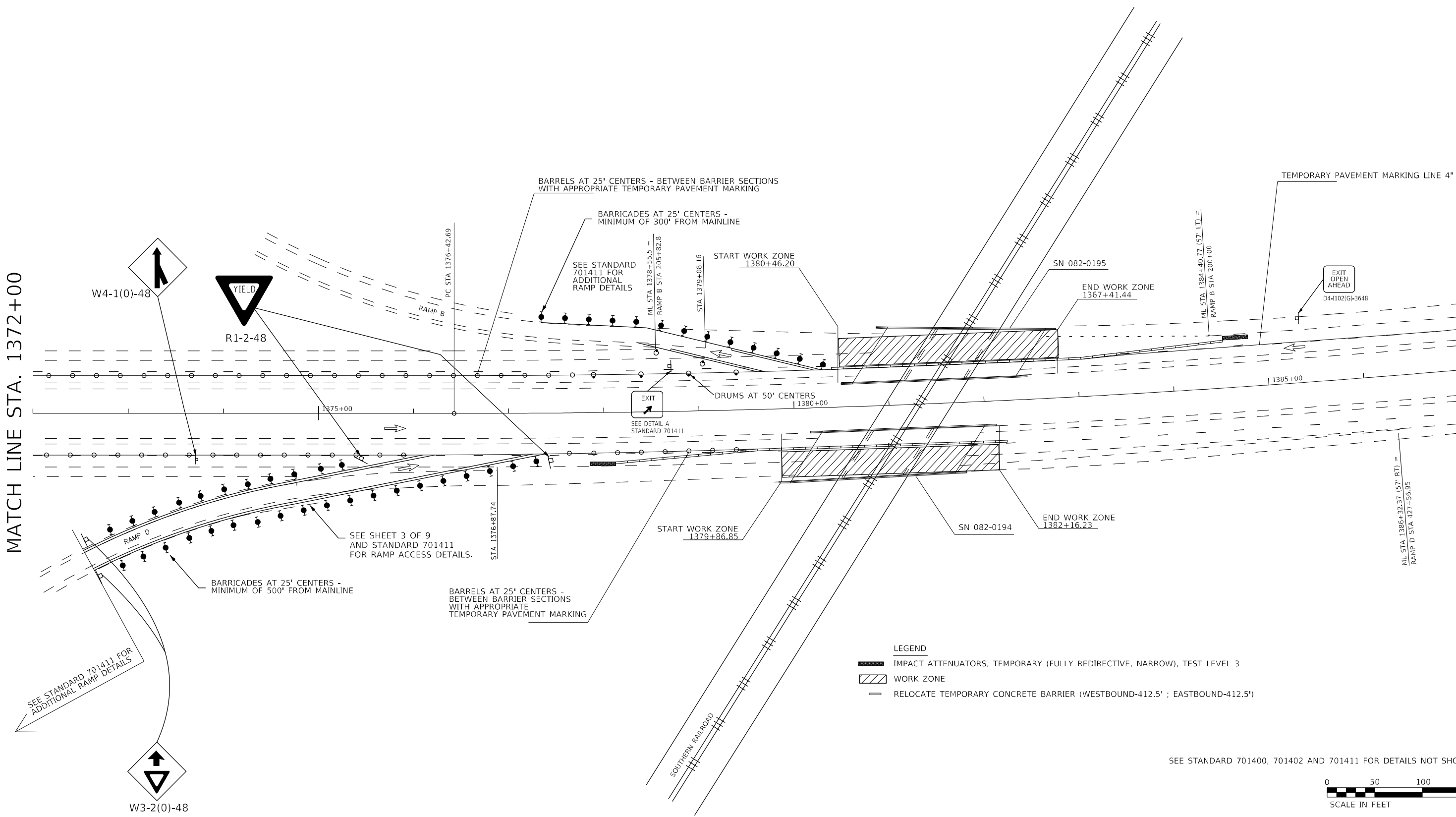
**TRAFFIC CONTROL PLAN
STAGE I**

SCALE: SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	11
ILLINOIS FED. AID PROJECT			CONTRACT NO. 76P04	

MATCH LINE STA. 1372+00

MATCH LINE STA. 1387+00



- LEGEND**
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
 - WORK ZONE
 - RELOCATE TEMPORARY CONCRETE BARRIER (WESTBOUND-412.5' ; EASTBOUND-412.5')

SEE STANDARD 701400, 701402 AND 701411 FOR DETAILS NOT SHOWN



MODEL NUMBER/NAME
FILE NUMBER/FILES

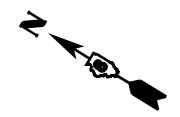
USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALES	DRAWN -	REVISED -
PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN
STAGE I**

SCALE: SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	12
CONTRACT NO. 76P04			ILLINOIS FED. AID PROJECT	

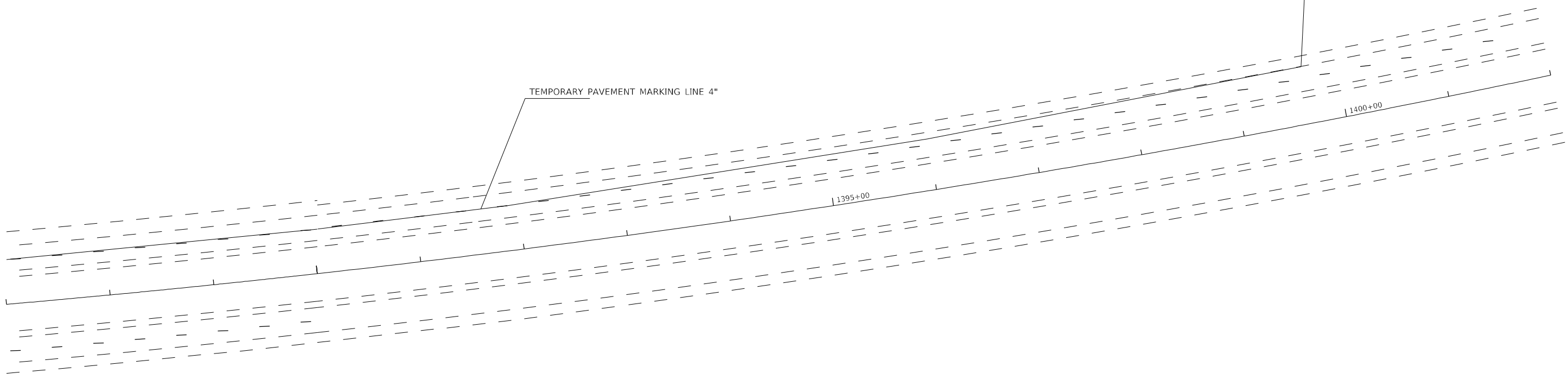


END OF
TRAFFIC CONTROL
AND PROTECTION
701400

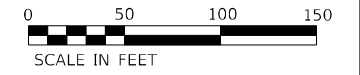
START OF
TRAFFIC CONTROL
AND PROTECTION
701402 SPECIAL

TEMPORARY PAVEMENT MARKING LINE 4"

MATCH LINE STA. 1387+00



SEE STANDARD 701400, 701402 AND 701411 FOR DETAILS NOT SHOWN



MODEL: \\MODELS\NAME\$
FILE: NAME\$.PLOT\$

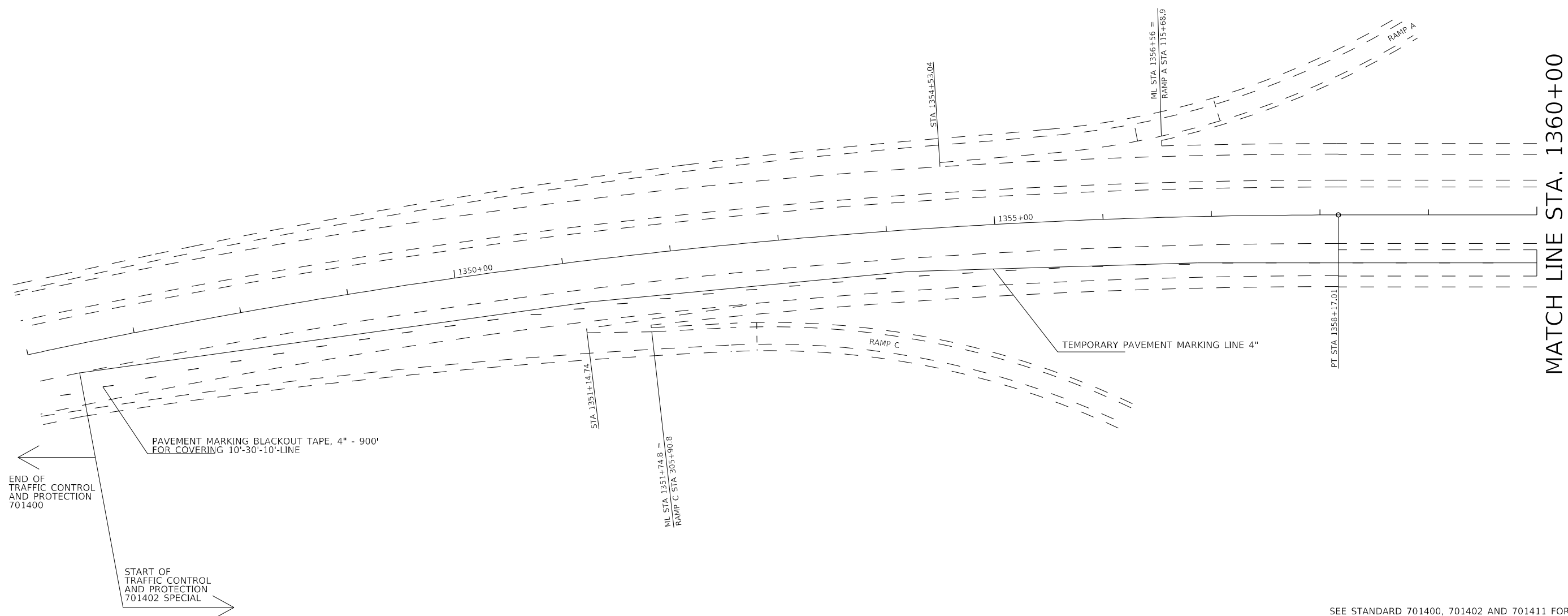
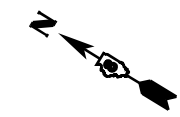
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN
STAGE I**

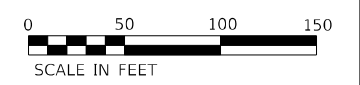
SCALE: SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	13
CONTRACT NO. 76P04			ILLINOIS FED. AID PROJECT	



MATCH LINE STA. 1360+00

SEE STANDARD 701400, 701402 AND 701411 FOR DETAILS NOT SHOWN



MODEL: 140001ENR1M1F5
FILE NAME: 311E15

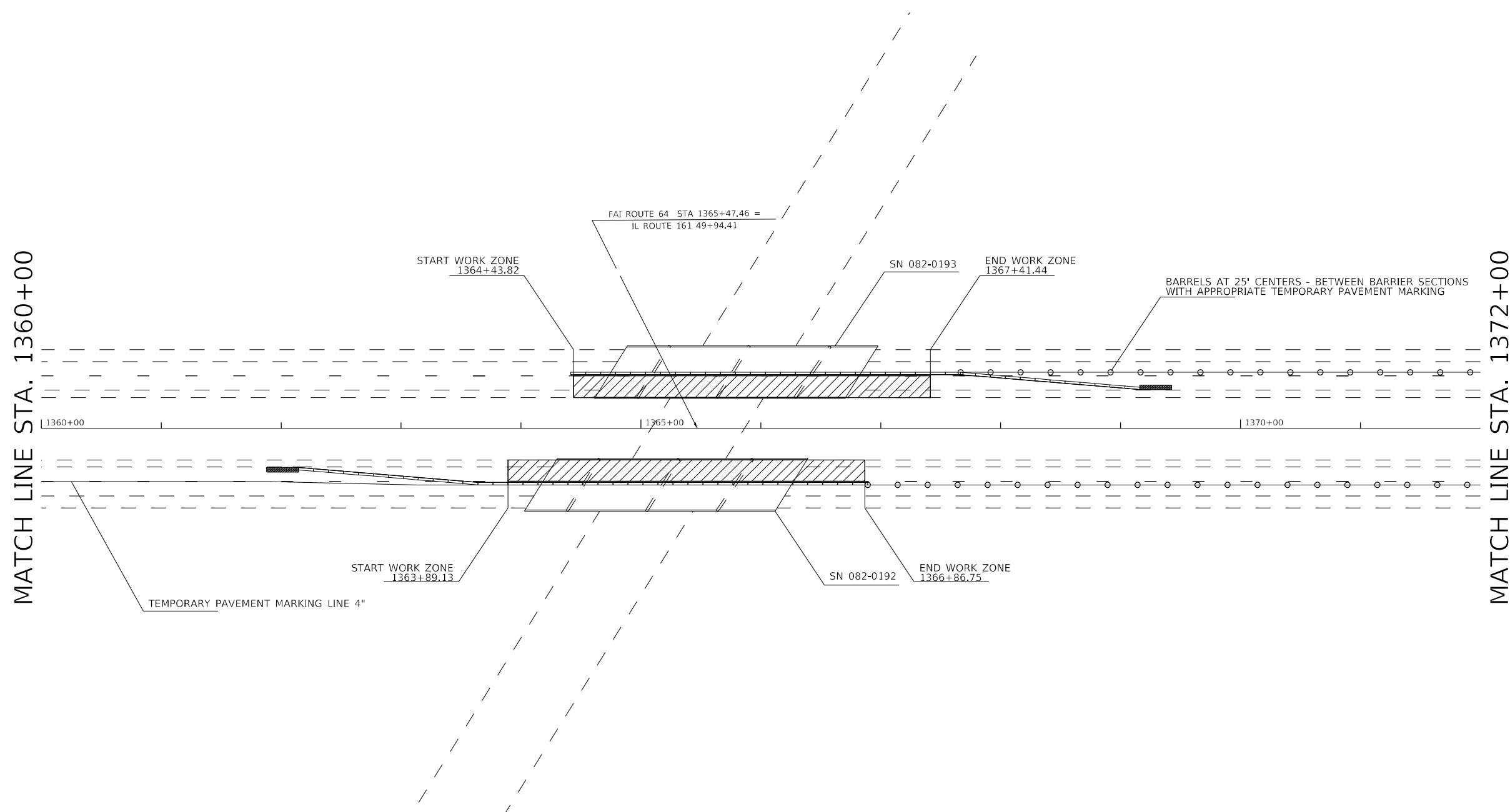
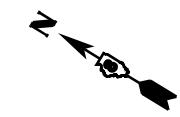
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN
STAGE II**

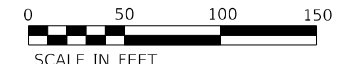
SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	14
CONTRACT NO. 76P04			ILLINOIS FED. AID PROJECT	



- LEGEND
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
 - WORK ZONE
 - TEMPORARY CONCRETE BARRIER (WESTBOUND-475' ; EASTBOUND-475')

SEE STANDARD 701400, 701402 AND 701411 FOR DETAILS NOT SHOWN



MODEL: 1400ELEM.MXS
FILE NAME: 37EUS

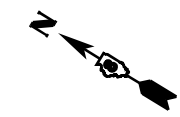
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN
STAGE II**

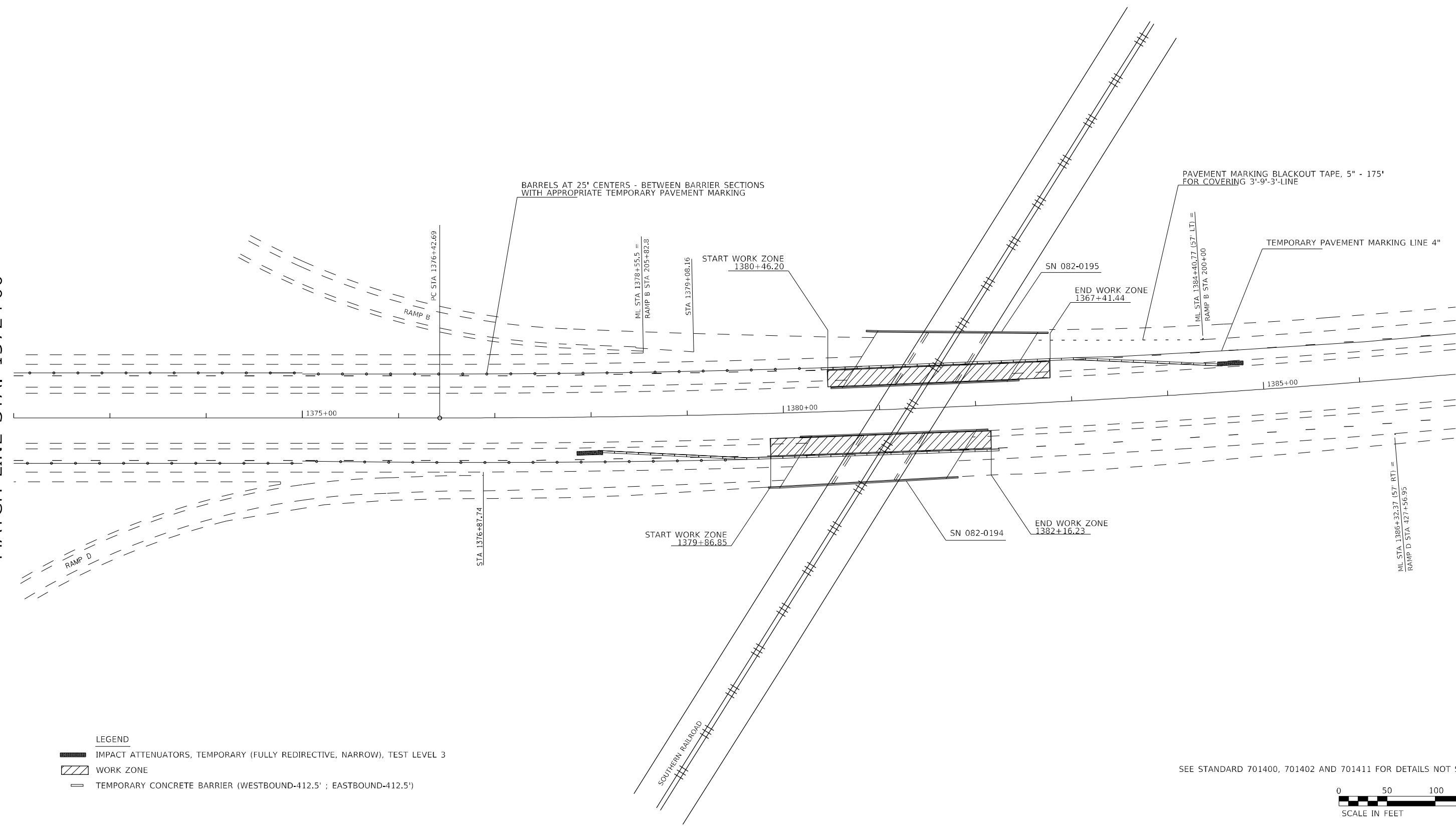
SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	15
CONTRACT NO. 76P04			ILLINOIS FED. AID PROJECT	



MATCH LINE STA. 1372+00

MATCH LINE STA. 1387+00



- LEGEND**
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
 - WORK ZONE
 - TEMPORARY CONCRETE BARRIER (WESTBOUND-412.5' ; EASTBOUND-412.5')

SEE STANDARD 701400, 701402 AND 701411 FOR DETAILS NOT SHOWN



MODEL NAME: TRAFFIC CONTROL PLAN
FILE NAME: TRAFFIC CONTROL PLAN

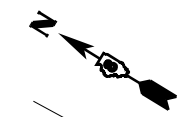
USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN
STAGE II**

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	16
ILLINOIS FED. AID PROJECT			CONTRACT NO. 76P04	



END OF
TRAFFIC CONTROL
AND PROTECTION
701400

START OF
TRAFFIC CONTROL
AND PROTECTION
701402 SPECIAL

PAVEMENT MARKING BLACKOUT TAPE, 4" - 900'
FOR COVERING 10'-30'-10'-LINE

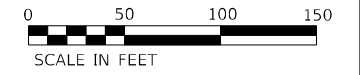
TEMPORARY PAVEMENT MARKING LINE 4"

1395+00

1400+00

MATCH LINE STA. 1387+00

SEE STANDARD 701400, 701402 AND 701411 FOR DETAILS NOT SHOWN



MODEL: 1400ELEM.MXS
FILE NAME: 37EUS

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN
STAGE II**

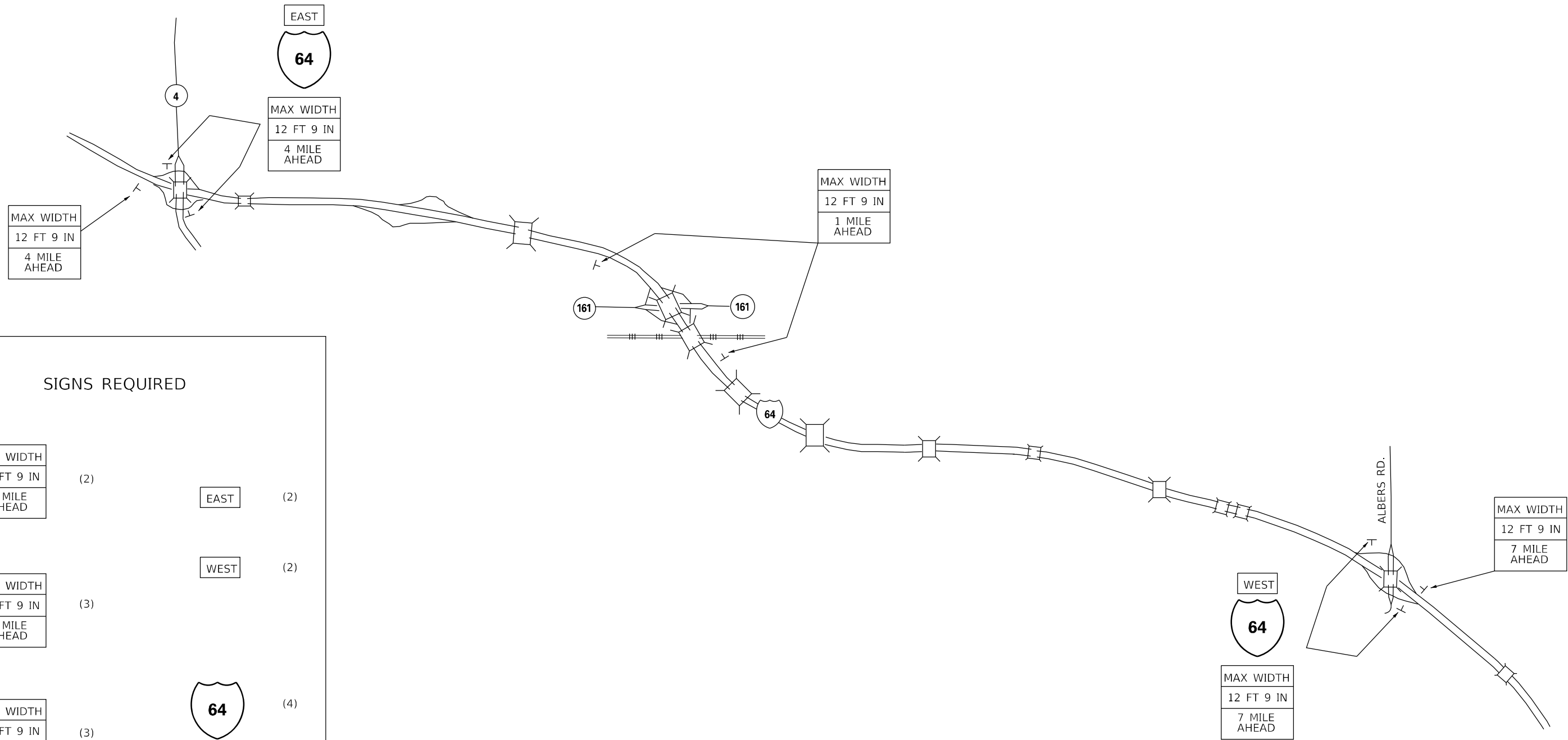
SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	17
ILLINOIS FED. AID PROJECT			CONTRACT NO. 76P04	

NOTES:

1. ALL SIGNS REQUIRED WILL BE SUPPLIED TO THE CONTRACTOR BY I.D.O.T.
2. THE CONTRACTOR SHALL FURNISH THE POSTS AND ERECT SIGNS AT THE LOCATIONS SHOWN ON THIS SHEET, AS DIRECTED BY THE R.E./R.T. THE POSTS SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
3. THE CONTRACTOR SHALL GIVE ILLINOIS DEPARTMENT OF TRANSPORTATION, BUREAU OF OPERATIONS TWO WEEKS NOTICE FOR SIGNS. THE CONTRACTOR SHALL PICK UP THE SIGNS AT THE T.M. BUILDING IN FAIRVIEW HEIGHTS AND RETURN THEM UPON COMPLETION OF THE CONTRACT. CONTACT JEAN SLAPE, AT (618) 394-2189.
4. THE ABOVE NOTED WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE, LUMP SUM, FOR WIDE LOAD SIGNING AND NO OTHER COMPENSATION WILL BE ALLOWED.
5. SIGN SPACING WILL BE 400' OR TO FIT FIELD CONDITIONS.
6. THE HEIGHT TO THE BOTTOM OF THE LOWEST SIGN SHALL NOT BE LESS THAN 6'.

WIDE LOAD SIGNING
I-64 OVER IL 161 & NSRR



SIGNS REQUIRED

MAX WIDTH 12 FT 9 IN 1 MILE AHEAD	(2)	EAST	(2)
MAX WIDTH 12 FT 9 IN 4 MILE AHEAD	(3)	WEST	(2)
MAX WIDTH 12 FT 9 IN 7 MILE AHEAD	(3)	64	(4)

MODEL NUMBER: 11/15/05
FILE NAME: 76P04

USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALE	DRAWN -	REVISED -
PLOT DATE = \$DATE	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WIDE LOAD SIGNING

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	18
CONTRACT NO. 76P04			ILLINOIS FED. AID PROJECT	

Existing Structures: 082-0192 & 0193 were built in 1972 under FAI Route 64 Sec 82-9HB-1. They are 2 span continuous wide flange supported on PPCI beam vaulted abutments and pile supported multi-column piers.
 In 1980 an HMA overlay was added and polymer nosing and silicone joints were installed on top of the existing PJS steel angles.
 In 1996 the metal railing was replaced with concrete, the abutment rocker bearings replaced with elastomeric, the HMA overlay was replaced with microsilica concrete overlay, and the polymer & silicone joints were replaced (on top of the original PJS steel angles).
 In 2000 the deck was patched along the microsilica concrete stage joint.
 In 2009 the deck was patched and sealed.

The deck shall be patched for the fourth time, and polymer & silicone joints replaced (on top of the original PJS steel angles).

GENERAL NOTES

No field welding is permitted except as specified in the contract documents.

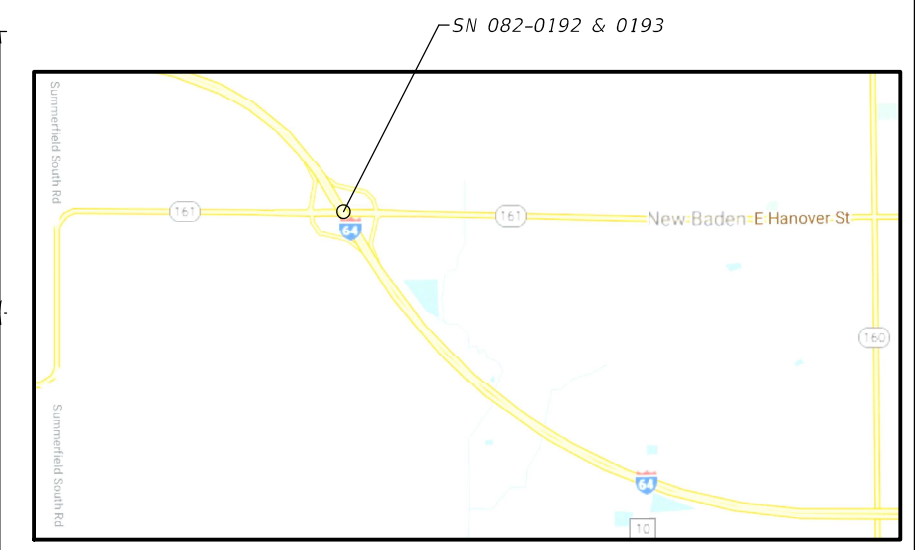
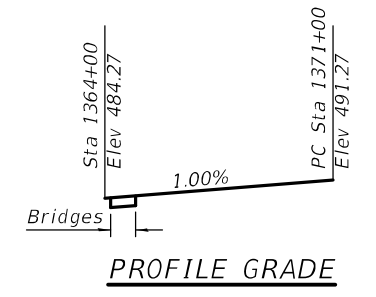
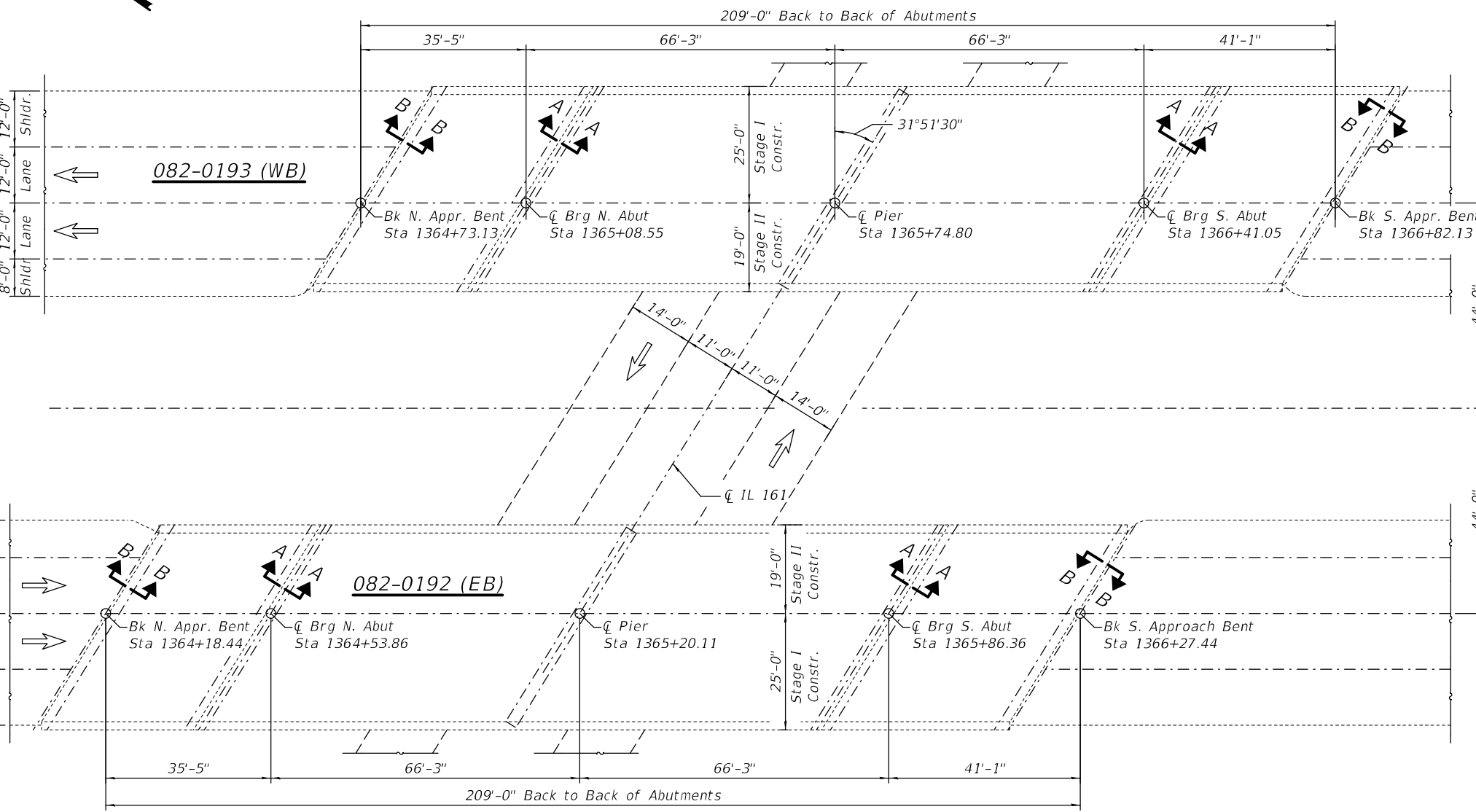
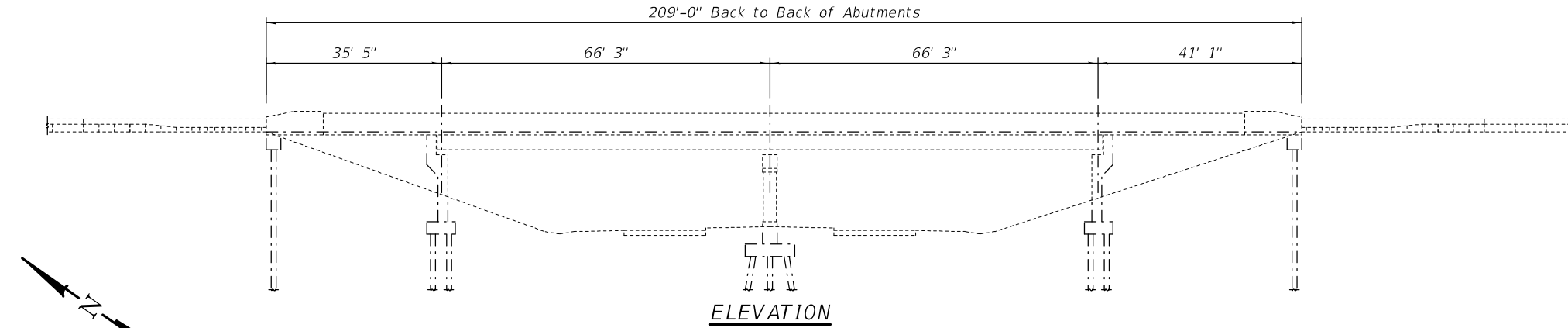
All reinforcement bars shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Joint opening shall be adjusted according to Article 520.04 of the Standard Specs. when the deck is poured at an ambient temperature other than 50°F.

The areas of deck repairs are estimated. The Engineer shall show actual locations of deck repairs on As-built plans.

Existing reinforcement extending into removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".



TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	10
Deck Slab Repair (Partial)	Sq. Yd.	150
Silicone Joint Sealer 1.5"	Foot	199.0
Polymer Concrete	Cu. Ft.	16.8

See sheet 2 for Sections A-A and B-B.

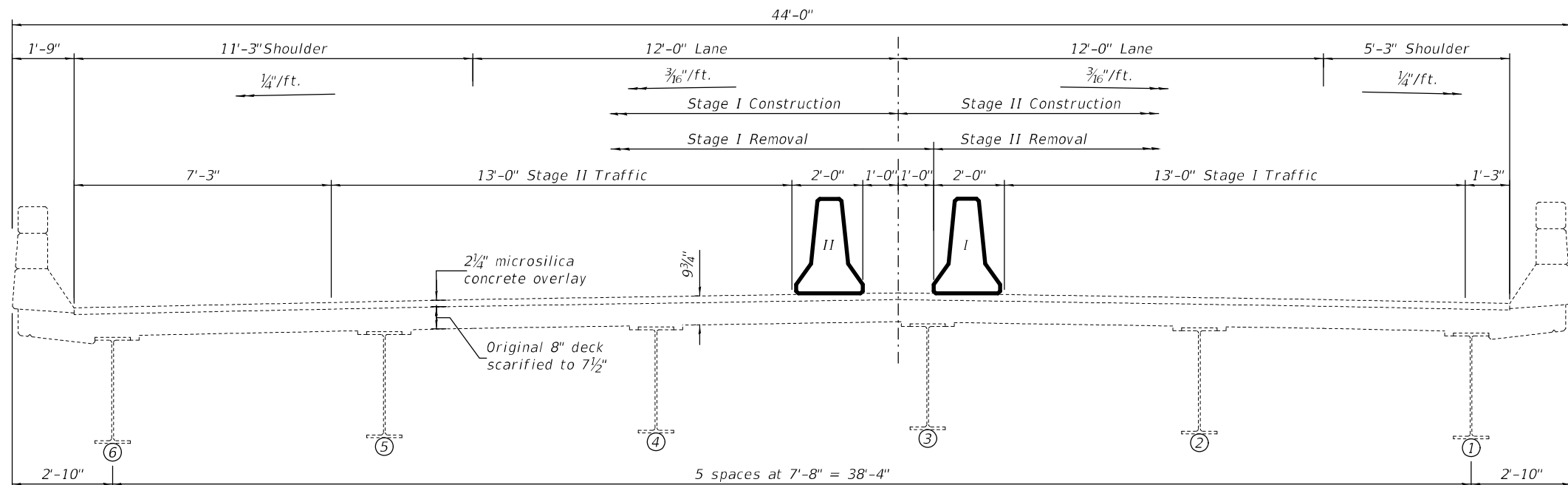
MODEL: 0820192-0820193-76P04-001
 FILE NAME: p:\v\lanroom-doi.illinois.gov\PWIDOT\Documents\DOT Offices\District 8\Project\82-9HB-1\CAD\Drawings\0820192-0820193-76P04.dgn

USER NAME =	DESIGNED - John Uehle	REVISED - MARCH 30, 2021
PLOT SCALE =	CHECKED - Anthony Vinson	REVISED -
PLOT DATE =	DRAWN - John Uehle	REVISED -
	CHECKED - Anthony Vinson	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

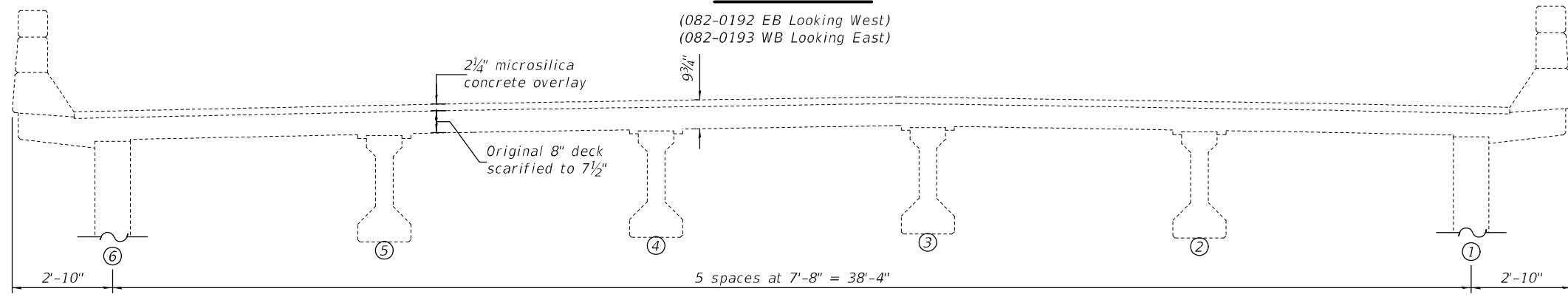
**GENERAL PLAN & ELEVATION
 FAI 64 OVER IL 161
 SN 082-0192(EB) & 0193(WB)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HB-2	ST. CLAIR	26	19
CONTRACT NO. 76P04				
ILLINOIS FED. AID PROJECT				



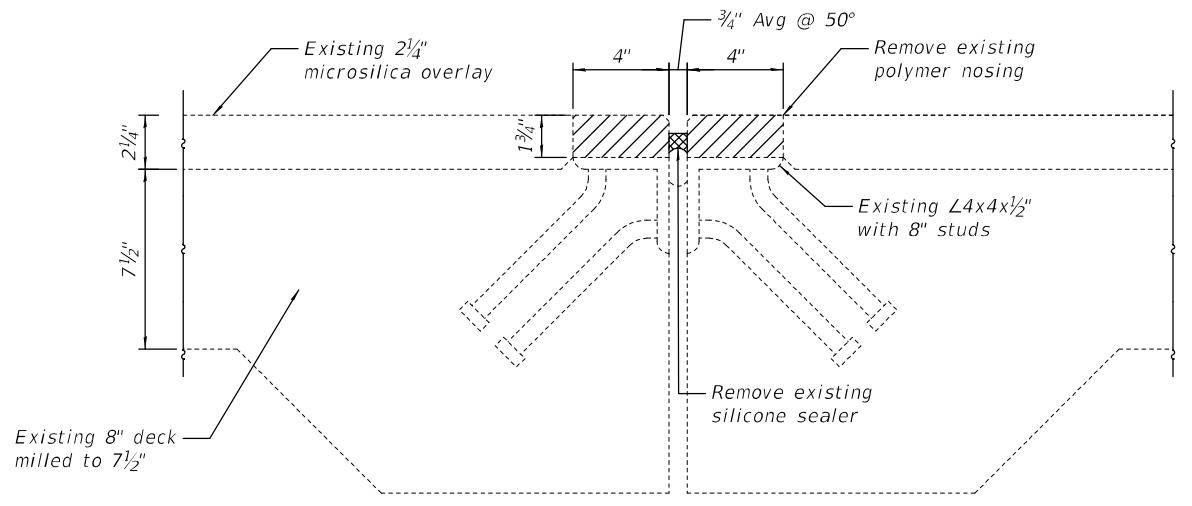
DECK SECTION

(082-0192 EB Looking West)
(082-0193 WB Looking East)

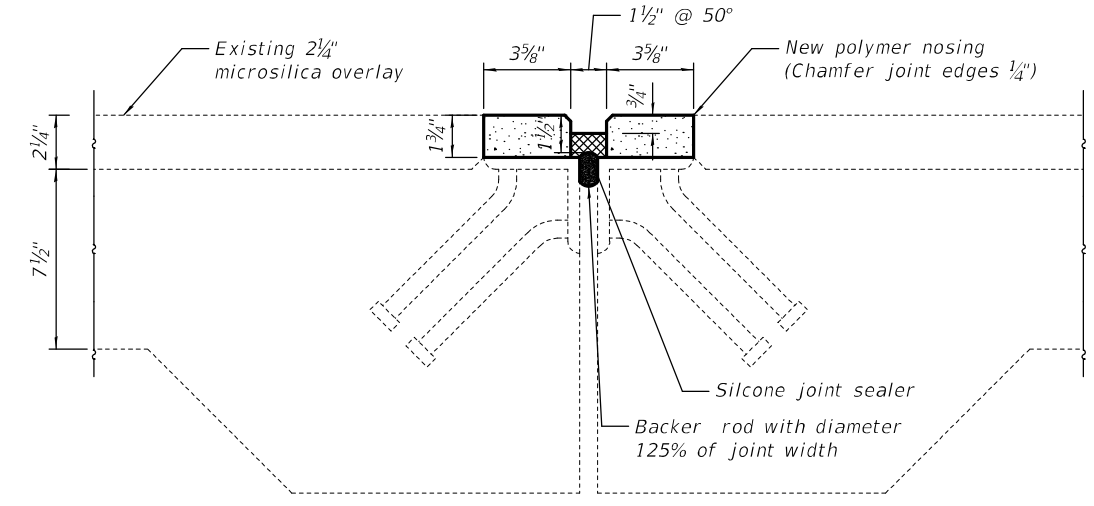


PPCI BEAM VAULTED ABUTMENT SECTION

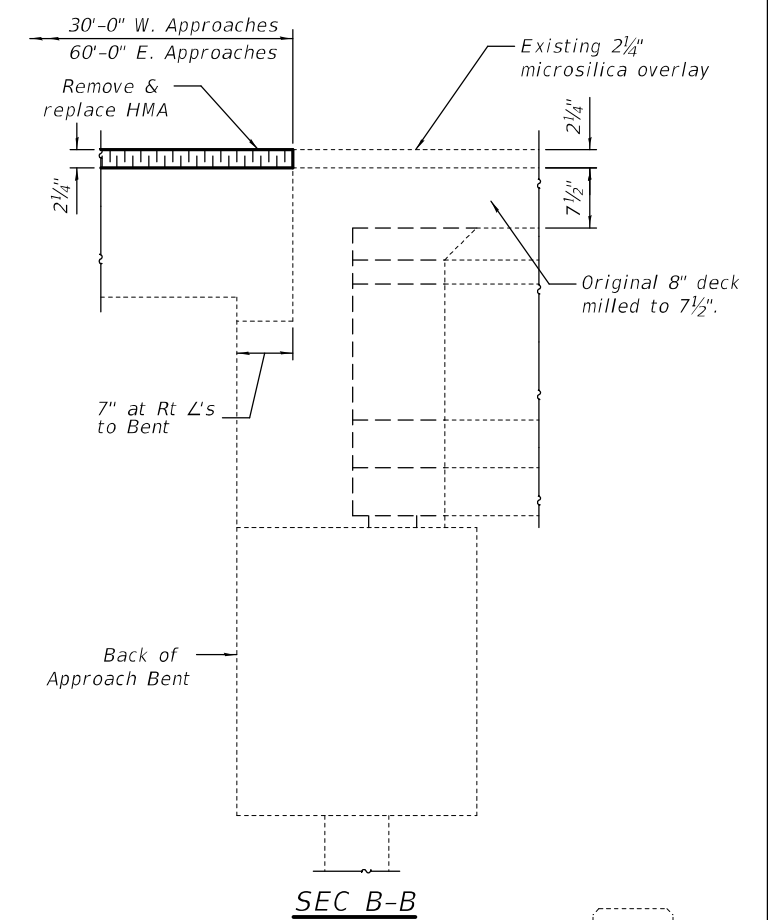
(082-0192 EB Looking West)
(082-0193 WB Looking East)
(Staging and dimensions same as above)



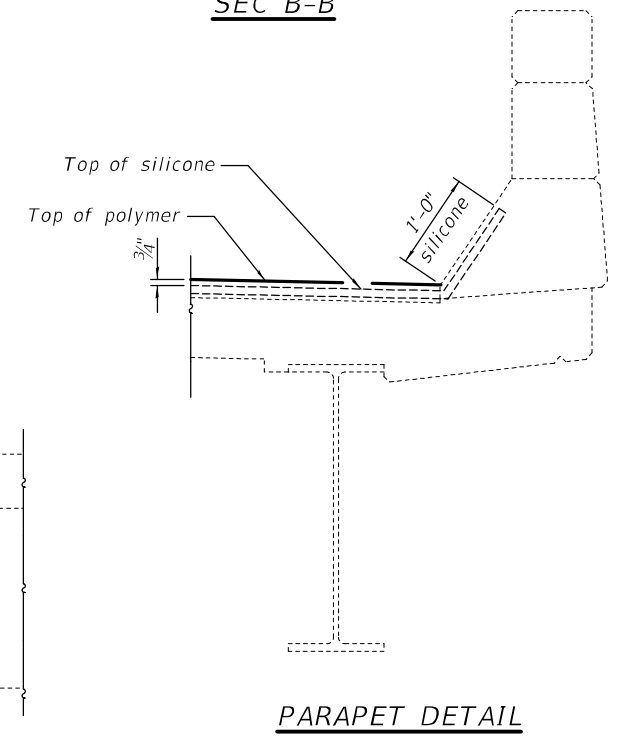
SEC A-A
(Removal)



SEC A-A
(Proposed)



SEC B-B



PARAPET DETAIL

NOTES
The areas of the deck adjacent to the new polymer shall be repaired prior to installation of the polymer. Any loose steel angles shall be removed. Cost included with "Deck Slab Repair (Partial)", and "Deck Slab Repair, Full Depth (Type II)".

MODEL: 0820192-0820193-76P04-002
FILE NAME: p:\v\lanroom-doi.illinois.gov\PWIDOT\Documents\DOT Offices\District 8\Project\IS\876P04\CADD\data\Structures\0820192-0820193-76P04.dgn

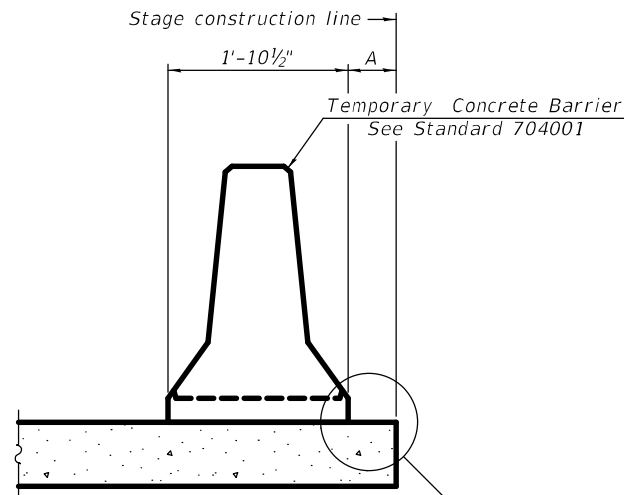
USER NAME =	DESIGNED - John Uehle	REVISED -
	CHECKED - Anthony Vinson	REVISED -
PLOT SCALE =	DRAWN - John Uehle	REVISED -
PLOT DATE =	CHECKED - Anthony Vinson	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING & JOINT DETAILS
082-0192 & 0193

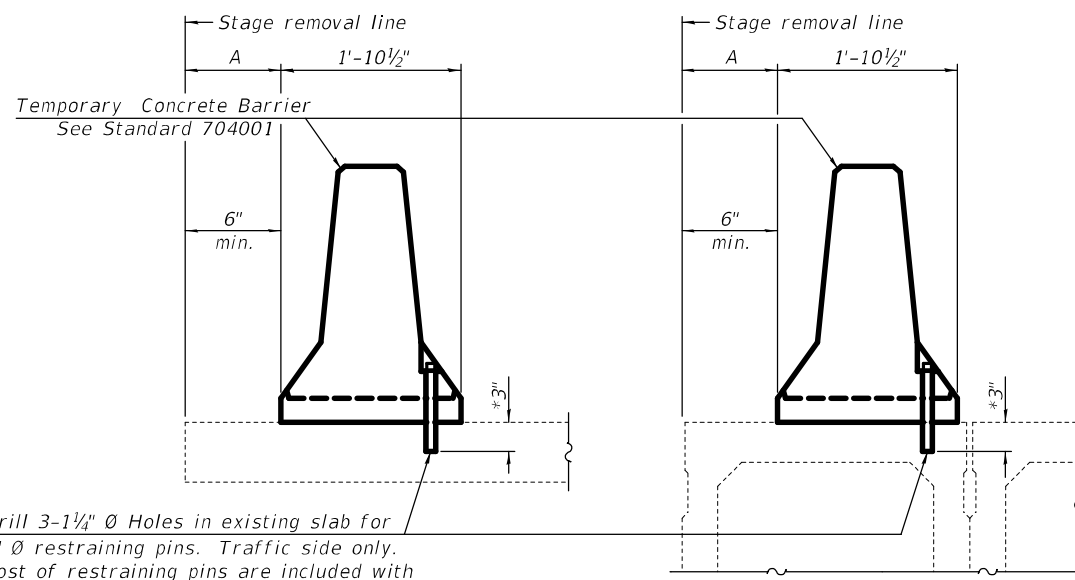
SHEET 2 OF 3 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	20
CONTRACT NO. 76P04				
ILLINOIS FED. AID PROJECT				



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



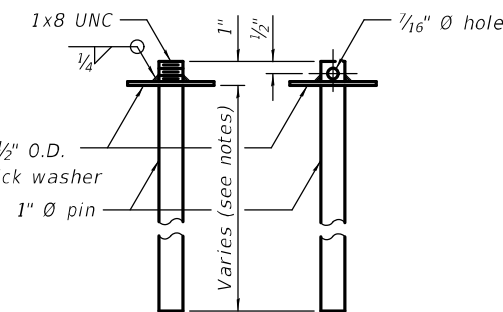
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

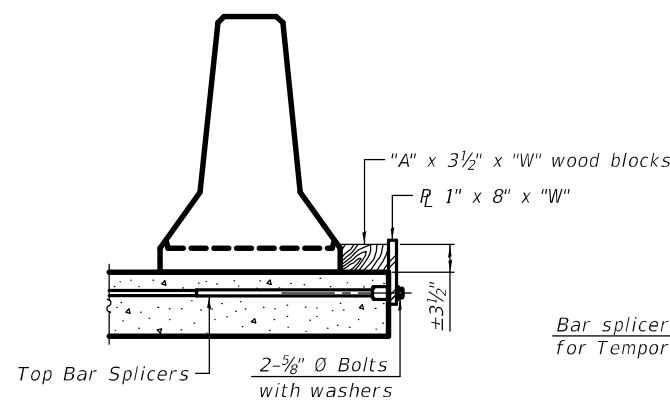
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

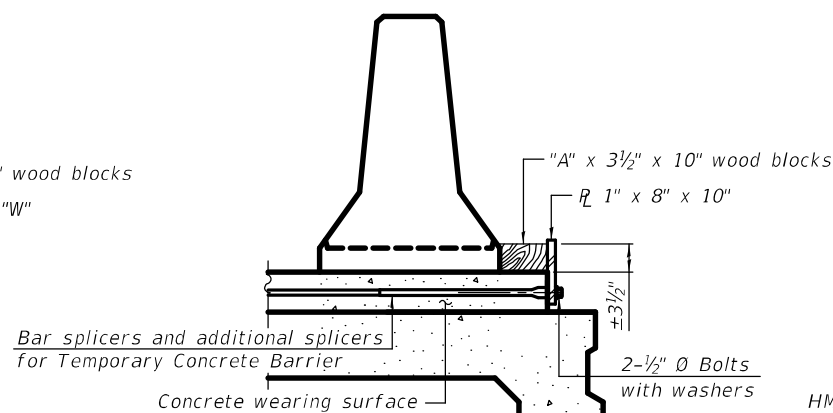
SECTIONS THRU SLAB OR DECK BEAM



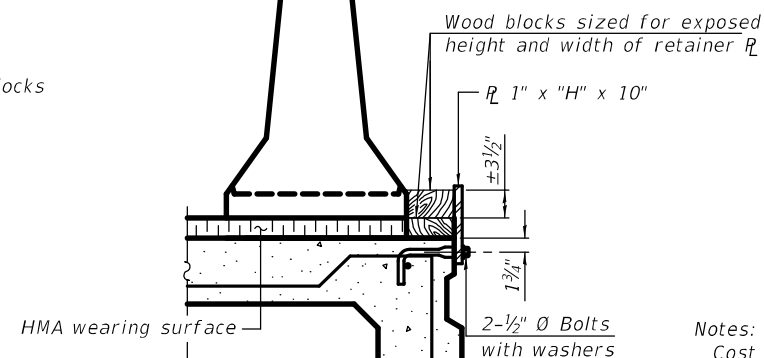
RESTRAINING PIN



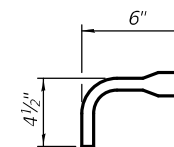
DETAIL I



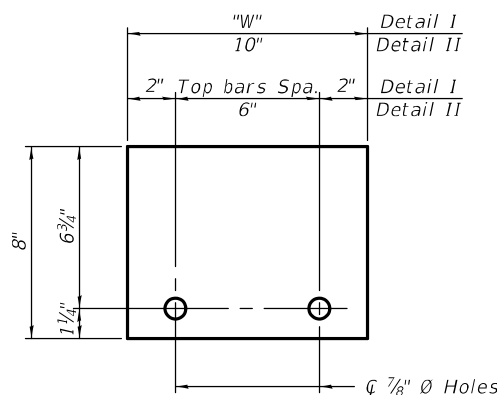
DETAIL II



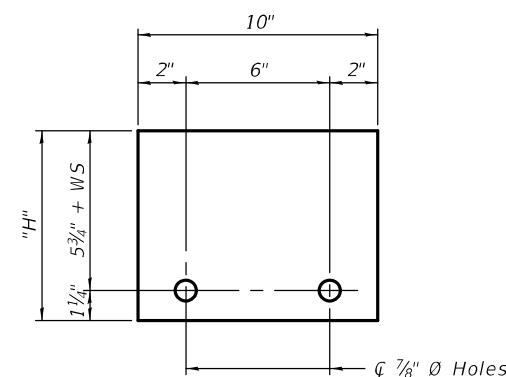
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{C} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6' to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6' apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

MODEL: 0820192-0820193-76P04-003
 FILE NAME: p:\v\lanroom-doi.illinois.gov\PWIDOT\Documents\DOT Offices\District 8\Project\IS\876P04\CAD\Detail\Structures\0820192-0820193-76P04.dgn

R-27 8-11-2017

USER NAME =	DESIGNED - John Uehle	REVISED -
PLOT SCALE =	CHECKED - Anthony Vinson	REVISED -
PLOT DATE =	DRAWN - John Uehle	REVISED -
	CHECKED - Anthony Vinson	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 082-0192 & 0193

SHEET 3 OF 3 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	21
CONTRACT NO. 76P04				

ILLINOIS FED. AID PROJECT

Existing Structures: 082-0194 & 0195 were built in 1971 under contract 25021 FAI Route 64 Sec 82-9VB. They are 3 span continuous wide flange supported on pile bent abutments and pile supported multi-column piers.
 In 1980 an HMA overlay was added and polymer nosing and silicone joints were installed on top of the existing PJS steel angles.
 In 1996 (082-0195 WB) the metal railing was replaced with concrete, the abutment rocker bearings replaced with elastomeric, the HMA overlay was replaced with microsilica concrete overlay, and the polymer & silicone joints were replaced (on top of the original PJS steel angles).
 In 2000 (082-0194 EB) the metal railing was replaced with concrete, the abutment rocker bearings replaced with elastomeric, the HMA overlay was replaced with microsilica concrete overlay, and deck ends and hatchblocks replaced with blockouts for polymer & silicone joints.
 In 2004 (082-0194 EB) was painted.
 In 2009 (082-0195 WB) was patched and the polymer & silicone joints were replaced (on top of the original PJS steel angles). Both bridges were sealed.

The deck shall be patched for the 3rd time, and polymer & silicone joints replaced.

GENERAL NOTES

No field welding is permitted except as specified in the contract documents.

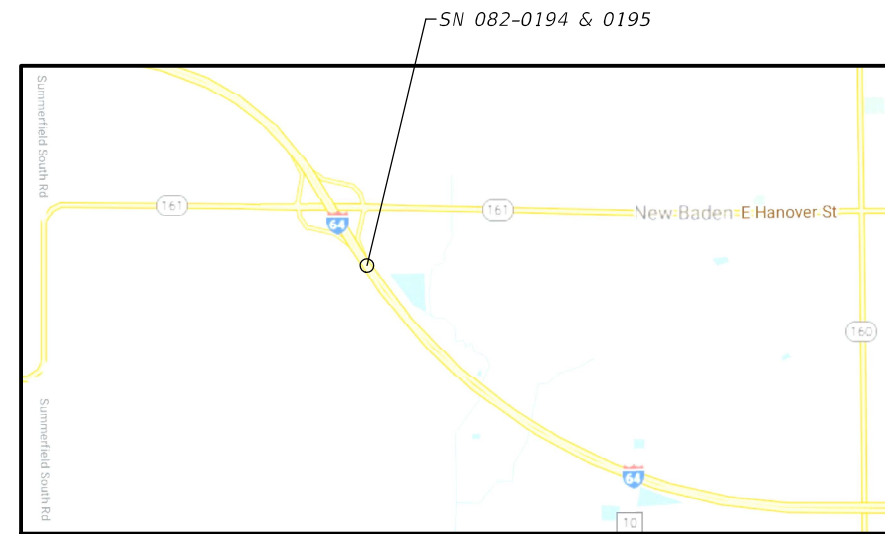
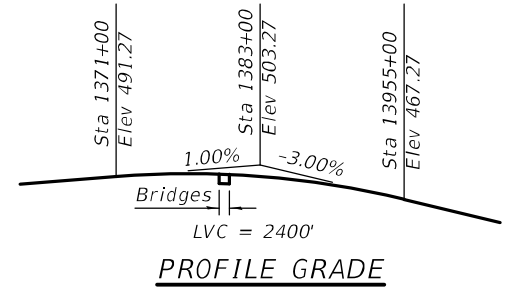
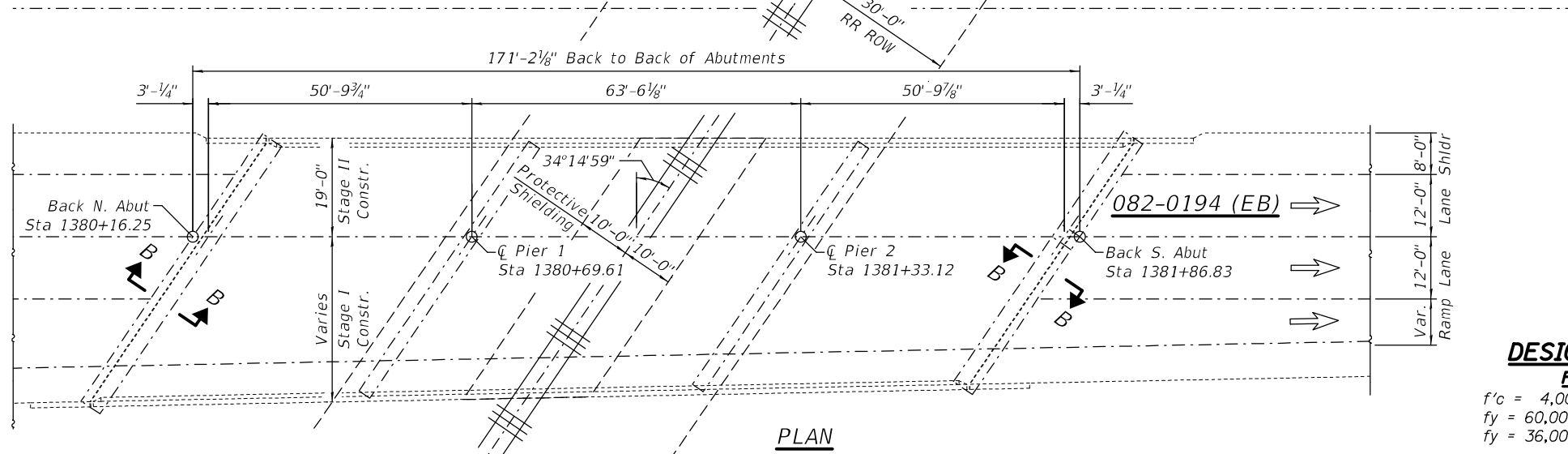
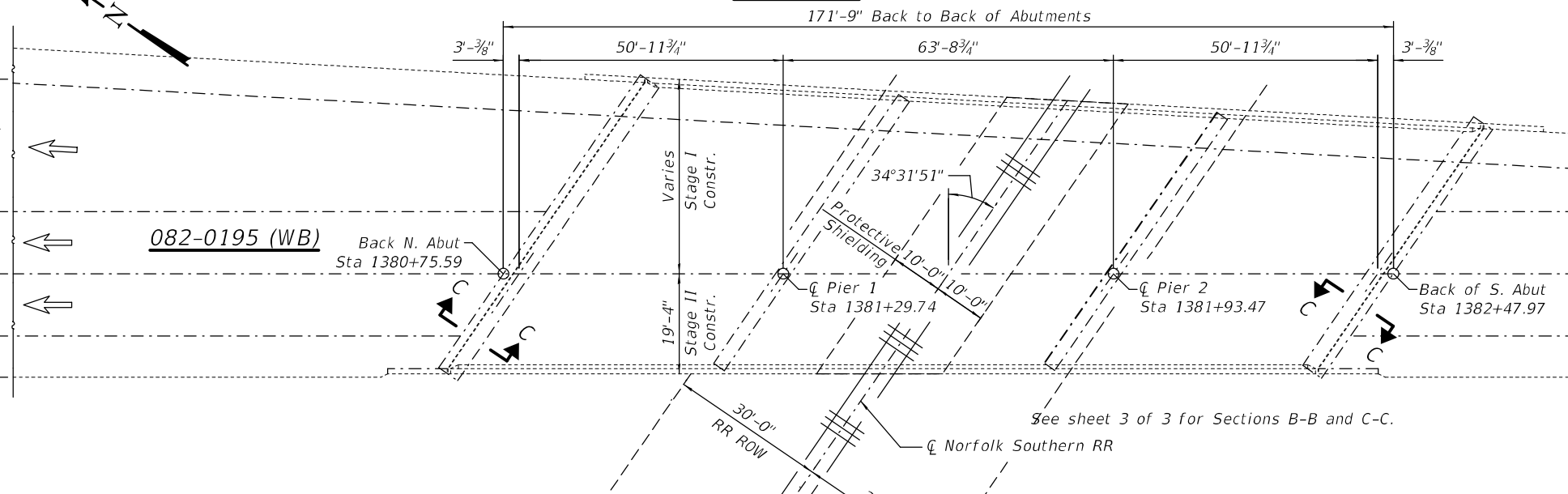
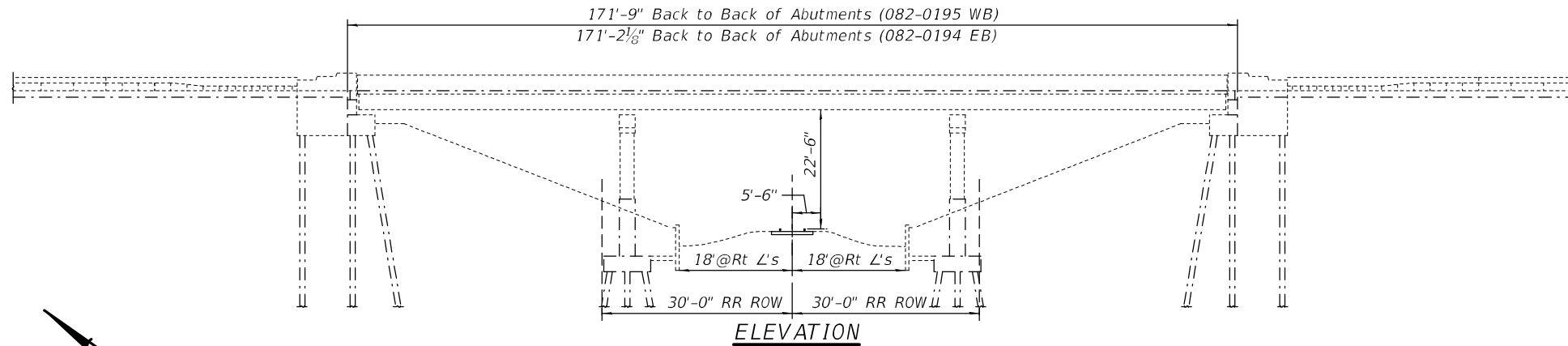
All reinforcement bars shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Joint opening shall be adjusted according to Article 520.04 of the Standard Specs. when the deck is poured at an ambient temperature other than 50°F.

The areas of deck repairs are estimated. The Engineer shall show actual locations of deck repairs on As-built plans.

Existing reinforcement extending into removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".



LOCATION SKETCH

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu. Yd.	8.2
Protective Shield	Sq. Yd.	278
Concrete Superstructure	Cu. Yd.	8.4
Reinforcement Bars, Epoxy Coated	Pound	760
Bar Splicers	Each	8
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	40
Deck Slab Repair (Partial)	Sq. Yd.	350
Silicone Joint Sealer 1.5"	Foot	117
Silicone Joint Sealer 2"	Foot	131
Polymer Concrete	Cu. Ft.	18.5
Railroad Protective Liability Insurance	L Sum	1

DESIGN STRESSES

FIELD UNITS

f'c = 4,000 psi
 fy = 60,000 psi (Reinforcement)
 fy = 36,000 psi (M270 Grade 36)

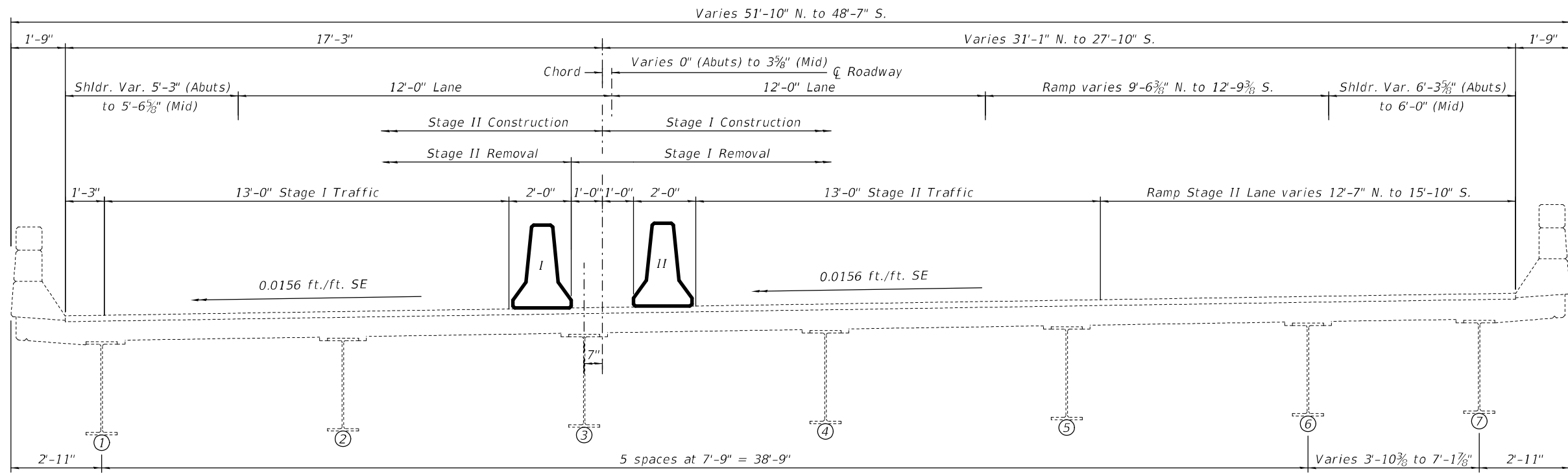
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN & ELEVATION
 FAI 64 OVER SOUTHERN RR
 SN 082-0194(EB) & 0195(WB)**

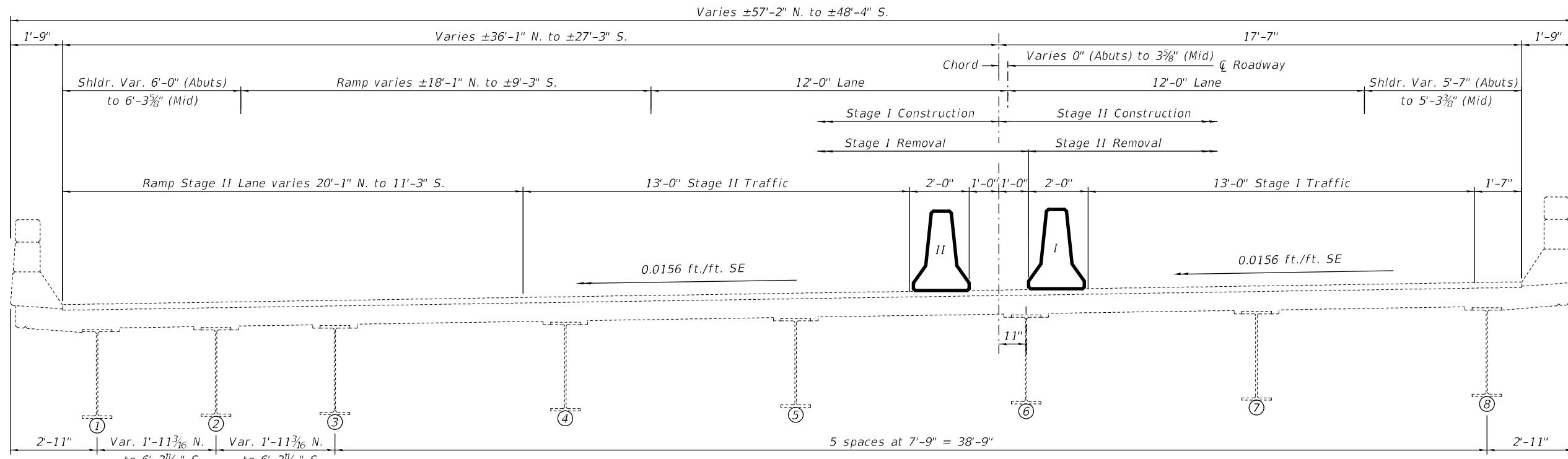
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	22
CONTRACT NO. 76P04				
ILLINOIS FED. AID PROJECT				

MODEL: 0820194-0820195-76P04-001
 FILE NAME: p:\v\lanroom\dol.illinois.gov\PWDOT\Documents\DOT Offices\District 8\Projects\876P04\CADD\876P04.dgn
 3/30/2021 12:30:58 PM

USER NAME =	DESIGNED - John Uehle	REVISED - MARCH 30, 2021
CHECKED = Anthony Vinson	REVISIONS -	
PLOT SCALE =	DRAWN - John Uehle	REVISIONS -
PLOT DATE =	CHECKED - Anthony Vinson	REVISIONS -



DECK SECTION 082-0194 (EB)
(Looking East)



DECK SECTION 082-0195 (WB)
(Looking East)

MODEL: 0820194-0820195-76P04-002
 FILE NAME: p:\v\lanroom.doi.illinois.gov\PWIDOT\Documents\DOT Offices\District 8\Project\0820194\CAD\Structures\0820194-0820195-76P04.dgn

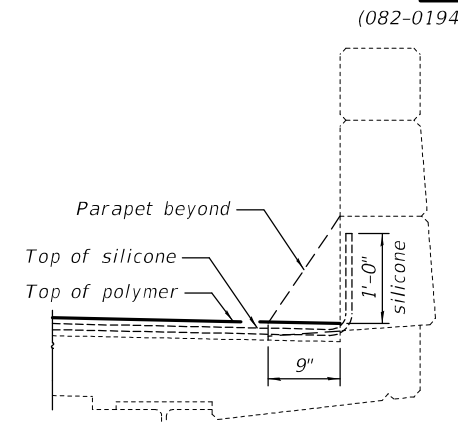
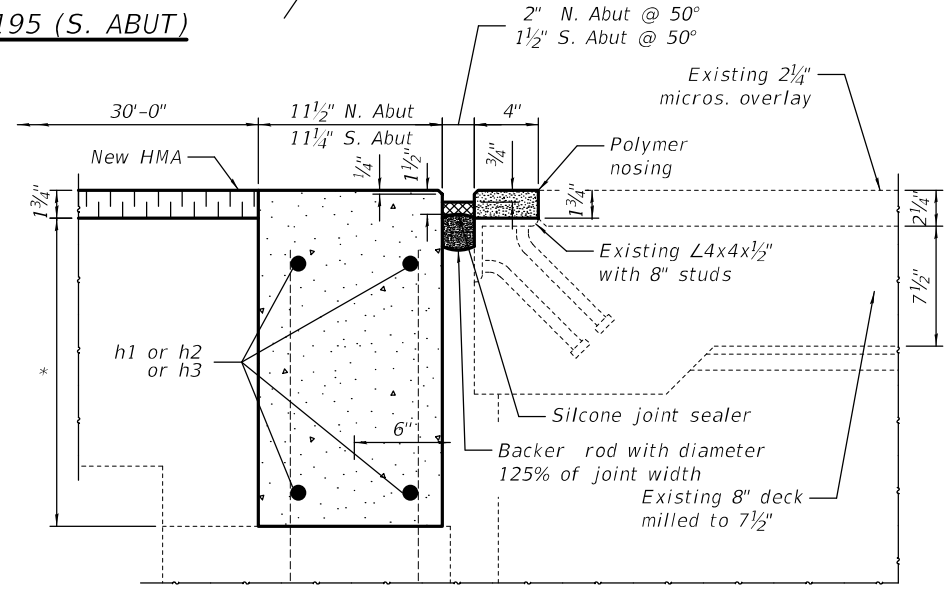
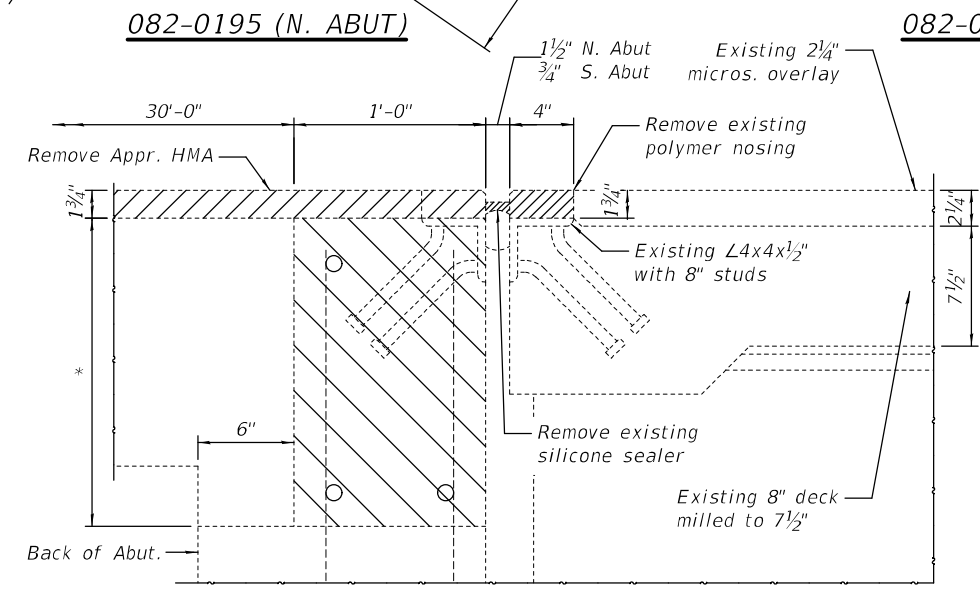
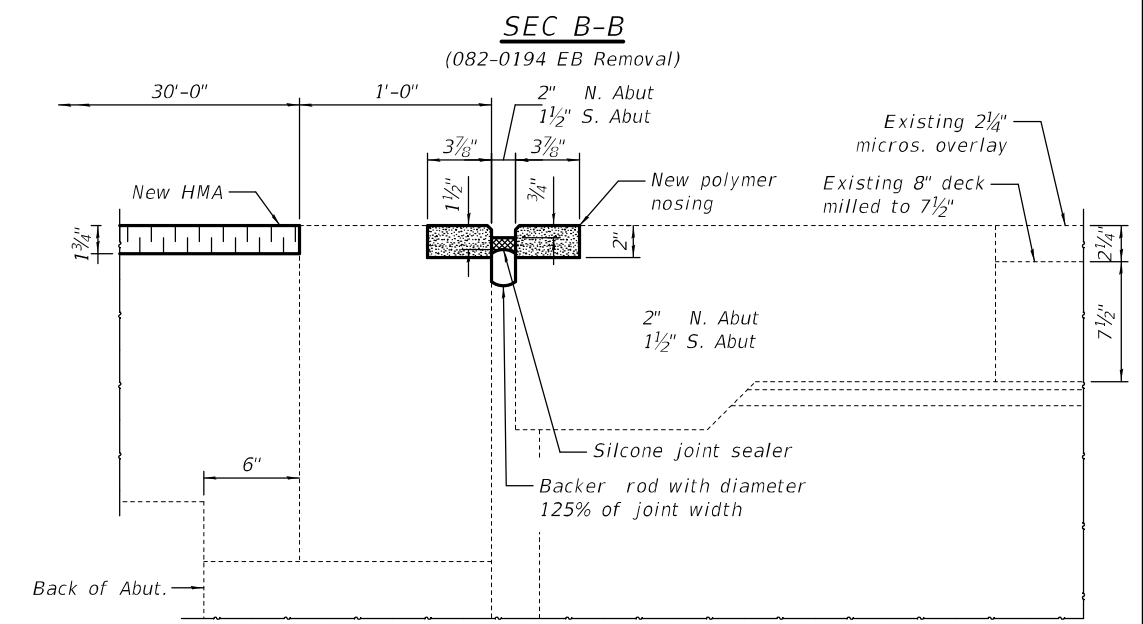
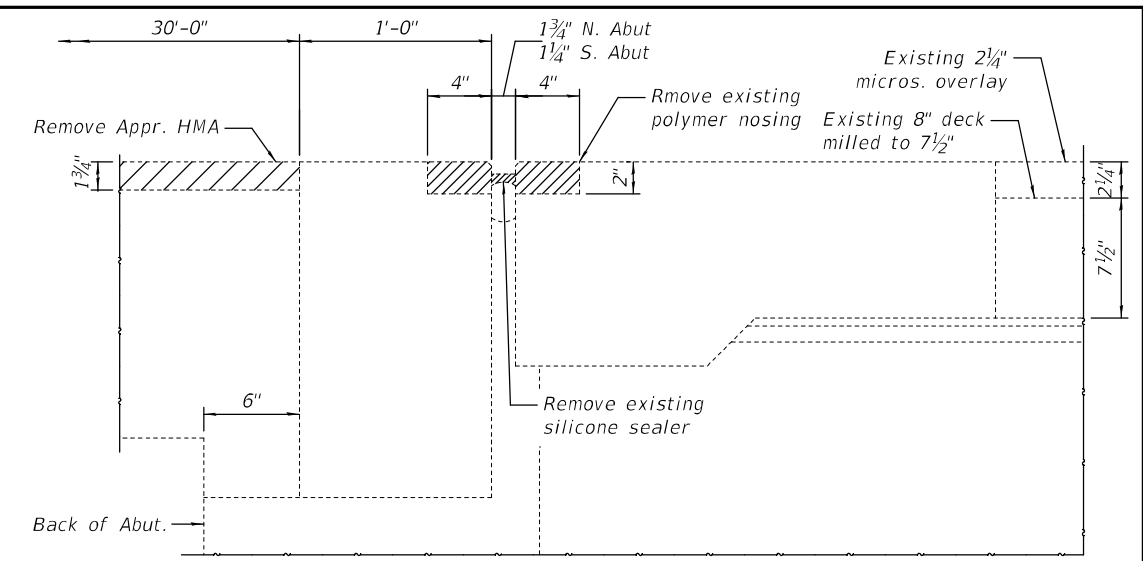
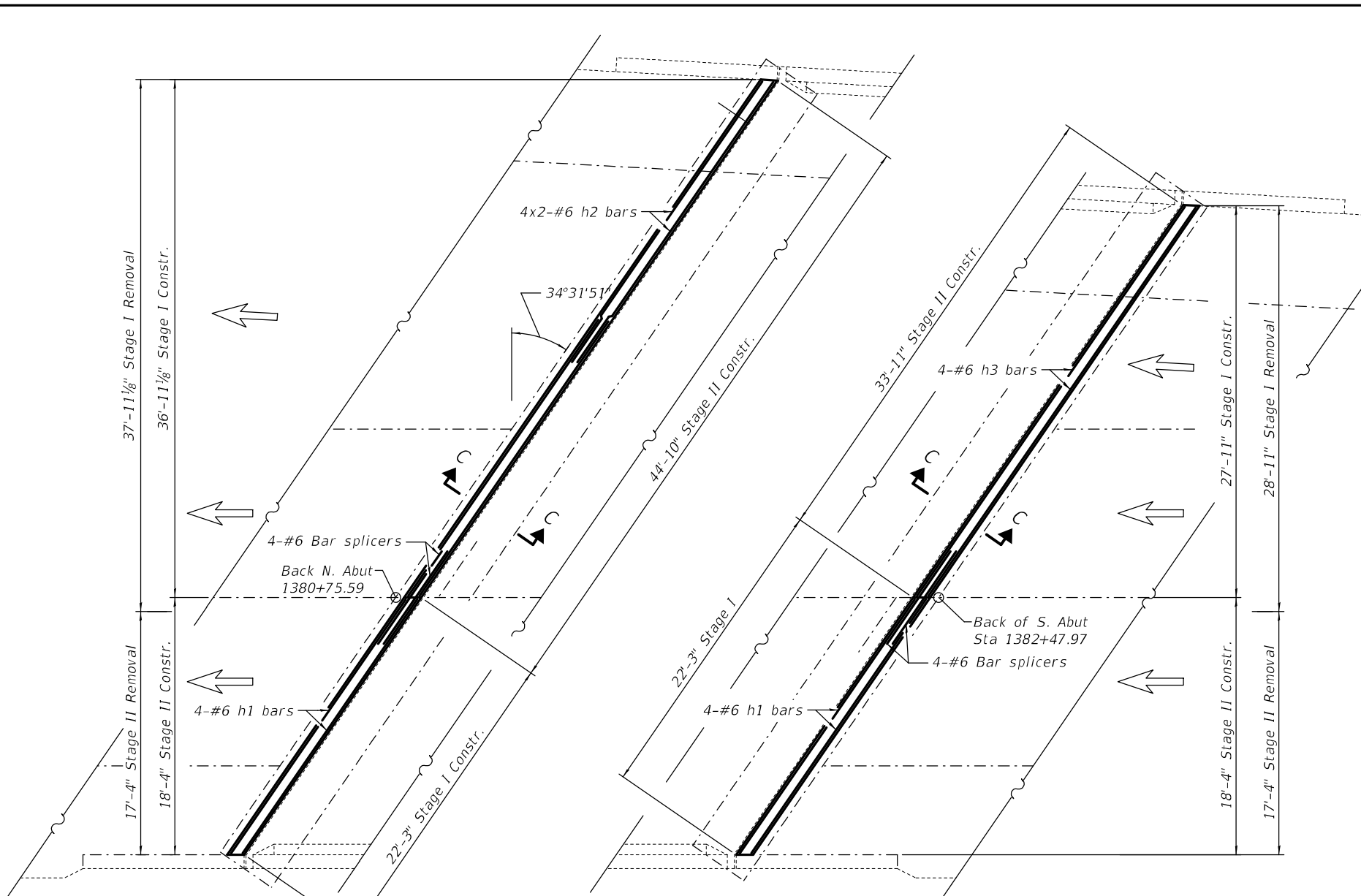
USER NAME =	DESIGNED - John Uehle	REVISED -
CHECKED - Anthony Vinson	REVISED -	
PLOT SCALE =	DRAWN - John Uehle	REVISED -
PLOT DATE =	CHECKED - Anthony Vinson	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING
082-0194 & 0195
 SHEET 2 OF 5 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	23
CONTRACT NO. 76P04				

MODEL: 0820194-0820195-76P04-003
 FILE NAME: p:\v\lanroom-dot.illinois.gov-PWDOT\Documents\DOT Offices\District 8\Project\820194-0820195-76P04\CADD\Structures\0820194-0820195-76P04.dgn



NOTES
 The areas of the deck adjacent to the new polymer shall be repaired prior to installation of the polymer. Any loose steel angles shall be removed. Cost included with "Deck Slab Repair (Partial)", and "Deck Slab Repair, Full Depth (Type II)". Areas where the polymer is intact and tightly bonded to 082-0194 Abutments may be left in place as directed by the Engineer.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1(E)	8	#6	22'-0"	—
h2(E)	8	#6	24'-3"	—
h3(E)	4	#6	33'-7"	—
Concrete Removal			Cu. Yd.	8.2
Concrete Structures			Cu. Yd.	8.4
Reinforcement Bars, Epoxy Coated			Pound	760

Bars indicated thus 4x2-#6 indicate 4 lines of bars with 2 lengths per line

USER NAME =	DESIGNED - John Uehle	REVISED -
CHECKED - Anthony Vinson	REVISED -	
PLOT SCALE =	DRAWN - John Uehle	REVISED -
PLOT DATE =	CHECKED - Anthony Vinson	REVISED -

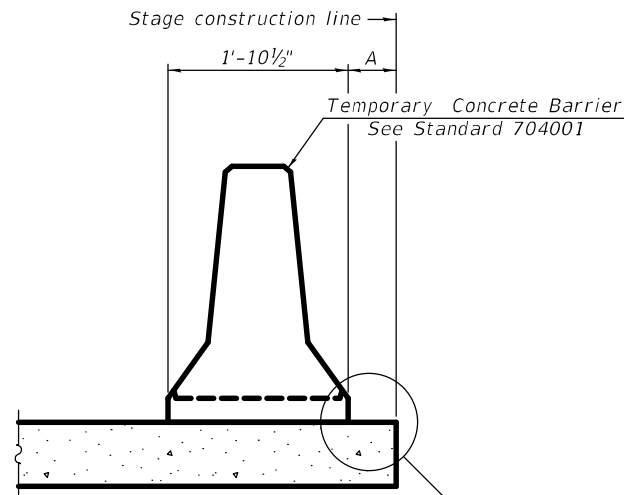
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

JOINT DETAILS
 082-0194 & 0195

SHEET 3 OF 5 SHEETS

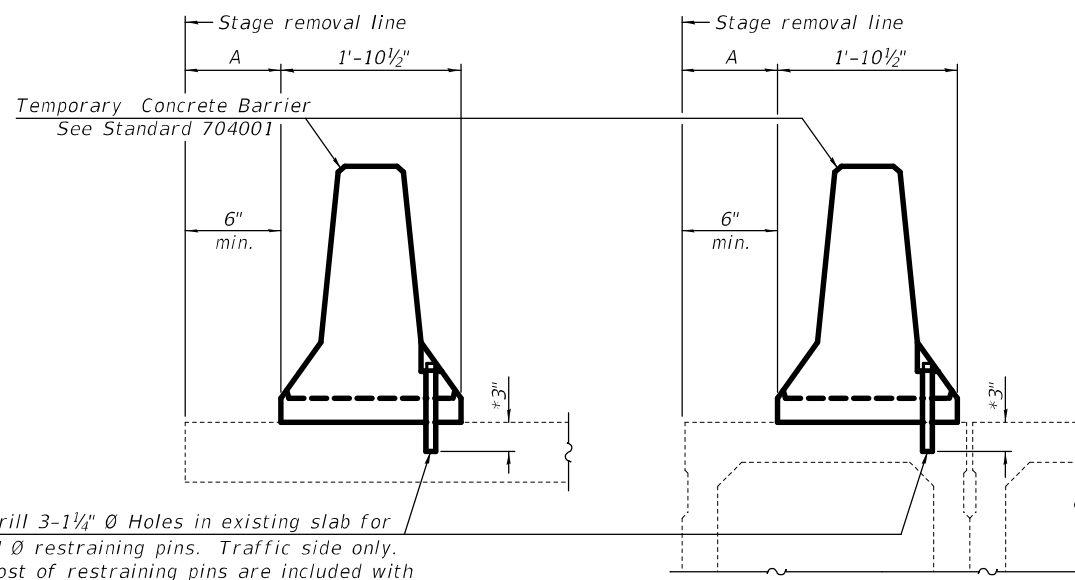
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HV-B-2	ST. CLAIR	26	24
				76P04

ILLINOIS FED. AID PROJECT



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



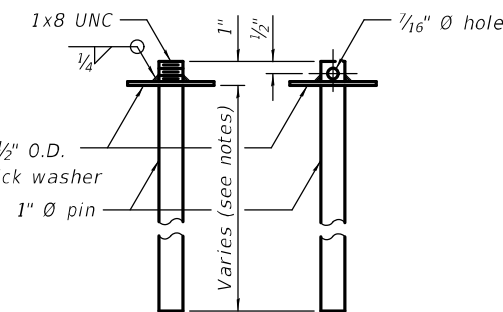
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

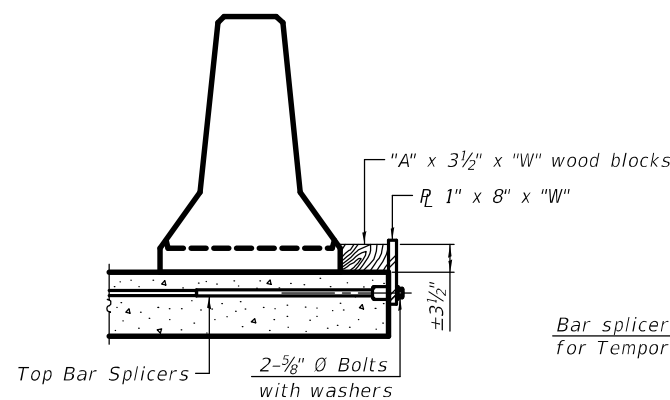
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

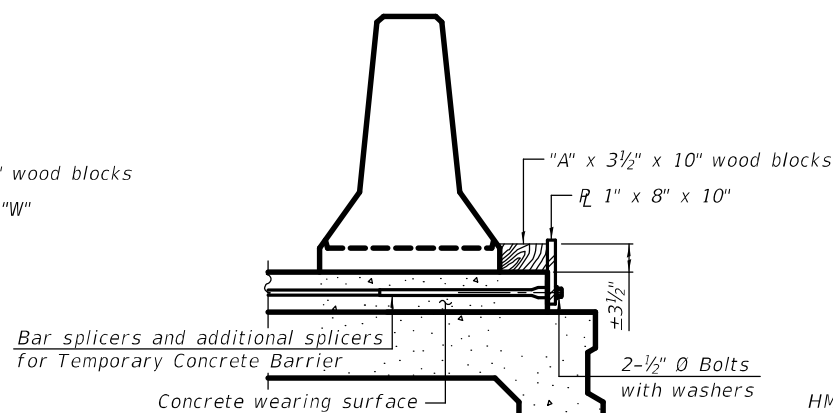
SECTIONS THRU SLAB OR DECK BEAM



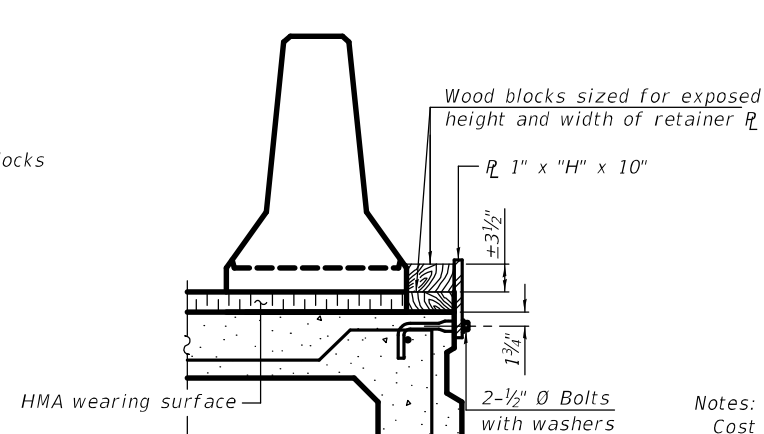
RESTRAINING PIN



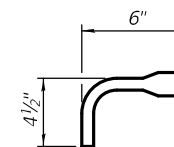
DETAIL I



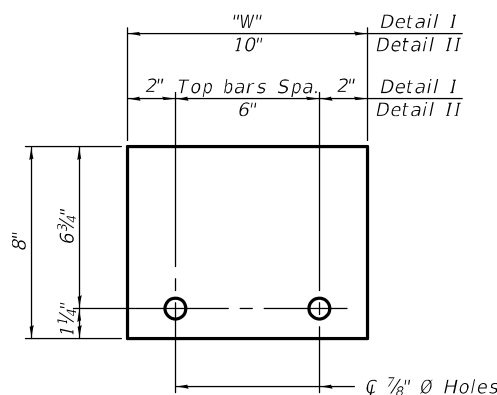
DETAIL II



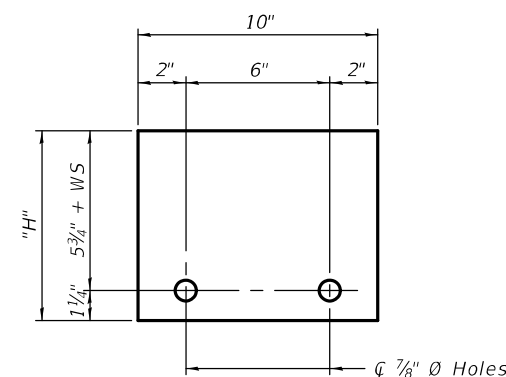
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{C} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6' to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6' apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27 8-11-2017

USER NAME =	DESIGNED - John Uehle	REVISED -
PLOT SCALE =	CHECKED - Anthony Vinson	REVISED -
PLOT DATE =	DRAWN - John Uehle	REVISED -
	CHECKED - Anthony Vinson	REVISED -

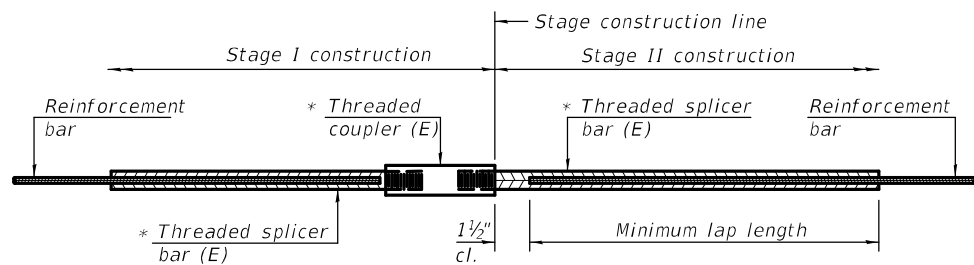
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
082-0194 & 0195

SHEET 4 OF 5 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	25
CONTRACT NO. 76P04				

ILLINOIS FED. AID PROJECT

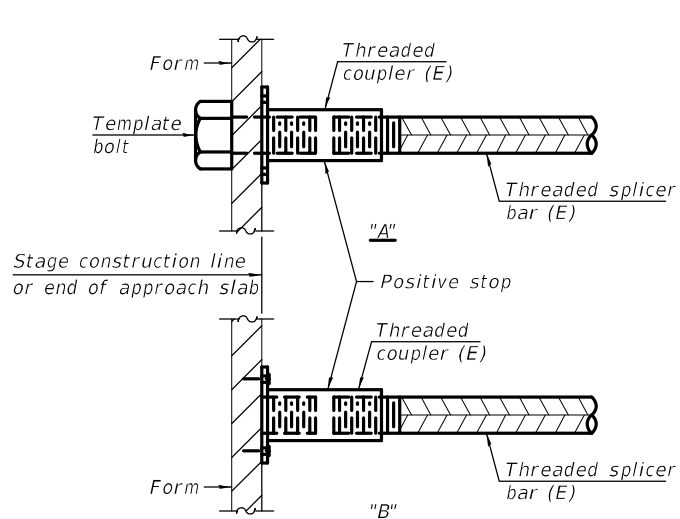


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

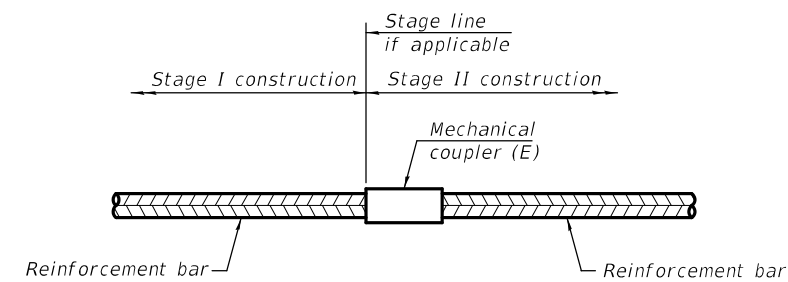
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
082-0195 Abuts	#6	8	3'-4"



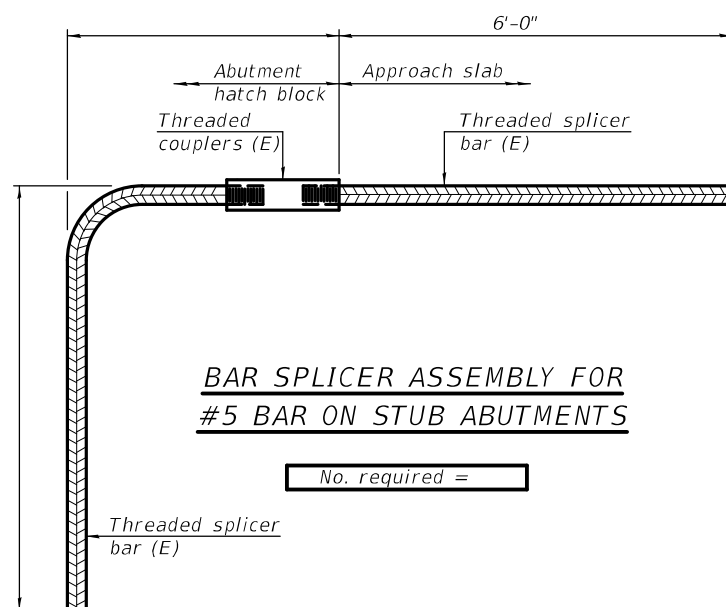
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: 0820194-0820195-76P04-005
 FILE NAME: p:\v\lanroom-doi.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 8\Project\0820194\CADD\0820194-0820195-76P04.dgn

BSD-1 2-17-2017

USER NAME =	DESIGNED - John Uehle	REVISED -
PLOT SCALE =	CHECKED - Anthony Vinson	REVISED -
PLOT DATE =	DRAWN - John Uehle	REVISED -
	CHECKED - Anthony Vinson	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 082-0194 & 0195

SHEET 5 OF 5 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
64	82-9HVB-2	ST. CLAIR	26	26
CONTRACT NO. 76P04				
ILLINOIS FED. AID PROJECT				