

06-12-2015 LETTING ITEM 019

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAU 1289 (WILLOW ROAD)
RUSSET WOOD DRIVE TO SCHOENBECK ROAD
SIDEWALK IMPROVEMENTS
SECTION: 12-00051-00-SW
PROJECT: TE-00D1(971)
CITY OF PROSPECT HEIGHTS
COOK COUNTY
C-91-425-14

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	1
		ILLINOIS	CONTRACT NO. 61B34	



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED February 10 2015
John J. Cantor
CITY OF PROSPECT HEIGHTS

PASSED February 18 2015
Christopher Holt
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW February 18 2015
John Fortman
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

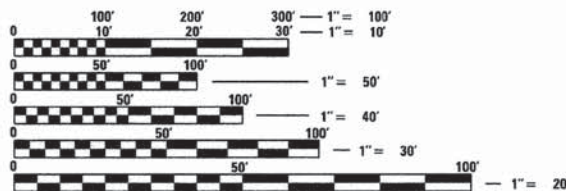
SIGNED: Kevin L. Belgrave
KEVIN L. BELGRAVE, EXP. 11/30/2015

DATE: FEBRUARY 2, 2015

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. (847) 705-4021 SCHAUMBURG, IL

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

ROADWAY DATA
WILLOW ROAD - MAJOR COLLECTOR
2010 ADT = 4,200
SPEED LIMIT = 30 MPH

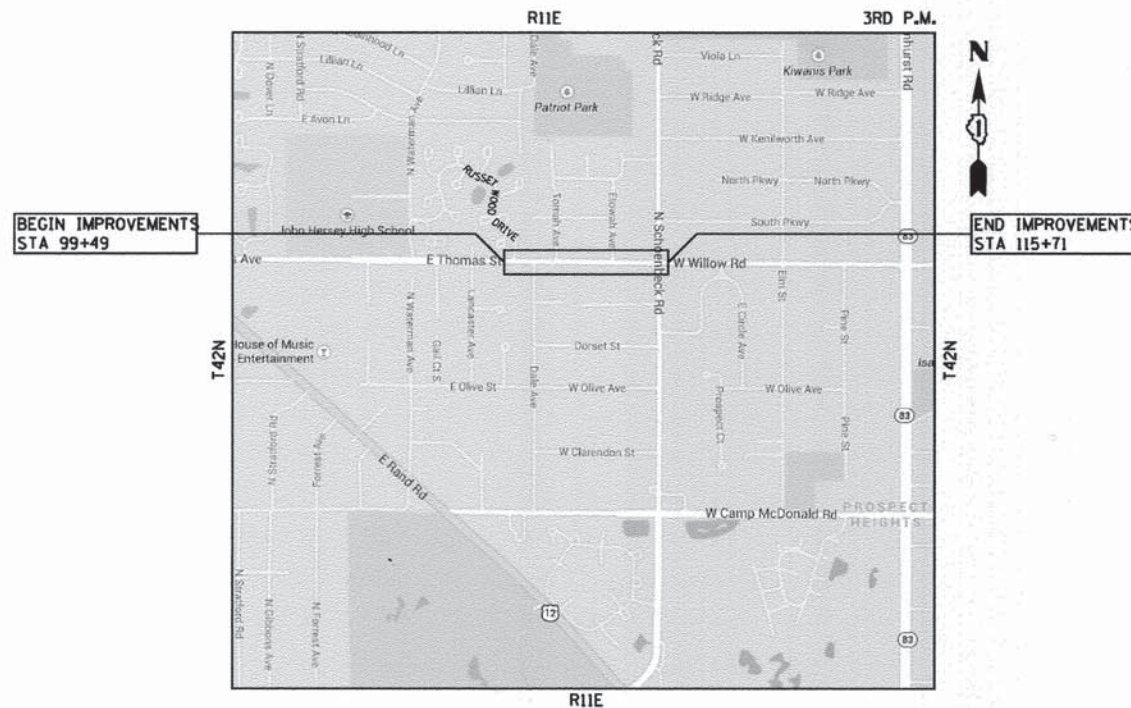


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER
PROJECT MANAGER

CONTRACT NO. 61B34



WHEELING TOWNSHIP
GROSS LENGTH = 1,622 FT. = 0.307 MILE
NET LENGTH = 1,622 FT. = 0.307 MILE

GHA GEWALT HAMILTON ASSOCIATES, INC.
850 Forest Edge Drive ■ Vernon Hills, IL. 60061
Consulting Engineers & Surveyors
847-478-9700
FAX: 847-478-9701

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OF THE STATE OF ILLINOIS

BENCHMARKS

BM #1 RRS IN POWER POLE ON SOUTH SIDE OF WILLOW AT RUSSET WOOD DRIVE
EL 680.40 (NAVD 88)

BM #2 RRS IN POWER POLE ON SOUTH SIDE OF WILLOW AT HOUSE ADDRESS #413
EL 677.38 (NAVD 88)

INDEX OF SHEETS

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HIGHWAY STANDARDS

000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS

280001-07 TEMPORARY EROSION CONTROL SYSTEMS

424001-08 PERPENDICULAR CURB RAMPS FOR SIDEWALKS

424006-02 DIAGONAL CURB RAMPS FOR SIDEWALKS

542301-03 PRECAST REINFORCED CONCRETE FLARED END SECTION

602001-02 CATCH BASIN TYPE A

602011-02 CATCH BASIN TYPE C

602601-03 PRECAST REINFORCED CONCRETE FLAT SLAB TOP

604001-04 FRAMES AND LIDS TYPE 1

604036-03 GRATE TYPE B

606001-06 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER

701001-02 OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5m) AWAY

701006-05 OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5m) to 24" (600 mm) FROM PAVEMENT EDGE AWAY

701301-04 LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS

701311-03 LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY

701501-06 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED

701701-09 URBAN LANE CLOSURE, MULTILANE INTERSECTION

701801-05 SIDEWALK, CORNER, OR CROSSWALK CLOSURE

701901-04 TRAFFIC CONTROL DEVICES

720001-01 SIGN PANEL MOUNTING DETAILS

720006-04 SIGN PANEL ERECTION DETAILS

780001-05 TYPICAL PAVEMENT MARKINGS

GHA #4755.018

FILE NAME =	USER NAME = MCoeman	DESIGNED - MAC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HIGHWAY STANDARDS, DISTRICT ONE DETAILS, BENCHMARKS, AND INDEX OF SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
G:\4700\4755 Prospect Heights Projects\755.018 Willow Road Sidewalk\DDNs\Plans\Folder\DRAWN\Sheets\4755\Z01.02 - GenNote-Index	PLDT SCALE = 20.0000' / 1" =	CHECKED - KLB	REVISED -			1289	12-00051-00-SW	COOK	21	2	
Default	PLDT DATE = 1/29/2015	DATE -	REVISED -			SCALE: NONE	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 61B34		
						ILLINOIS FED. AID PROJECT					

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS") ADOPTED JANUARY 1, 2012, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2015, THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", AND THE "MANUAL OF TEST PROCEDURES FOR MATERIALS".
2. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE DEPARTMENT DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY OR SPRINKLER SYSTEM THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER. ANY SPRINKLER SYSTEM IN CONFLICT WITH THE IMPROVEMENTS SHALL BE REMOVED AND RETURNED TO THE OWNER.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE OR DESTRUCTION OF PUBLIC OR PRIVATE UTILITIES, AND SHALL REPAIR ANY UTILITIES AT HIS OR HER OWN EXPENSE. COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT A PRECONSTRUCTION CONFERENCE. THE CONTRACTOR IS RESPONSIBLE FOR THE COORDINATION OF ANY UTILITY RELOCATIONS REQUIRED.
5. THE CONTRACTOR SHALL NOTIFY THE CITY PUBLIC WORKS DIRECTOR / CITY ENGINEER AT 847-398-6070 AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN CITY UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
6. DURING CONSTRUCTION STAGING OPERATIONS, THE PROSPECT HEIGHTS CITY POLICE AT 847-398-5511 AND FIRE DEPARTMENTS AT 847-253-8060 SHALL BE NOTIFIED IN WRITING 24 HOURS PRIOR TO LANE CLOSURES. EMERGENCY ACCESS SHALL BE MAINTAINED AND ALLOWED AT ALL TIMES. NO OVERNIGHT LANE CLOSURES WILL BE ALLOWED.
7. MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, SIDEWALK, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE.
8. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE SCHOOLS, RESIDENTS AND THE CITY WHEN ACCESS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER, AND/OR DRIVEWAY, AND/OR SIDEWALK INSTALLATION REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE CITY TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAY/SIDEWALK IS ABOUT TO BE CLOSED.

SCHOOLS -
DWIGHT D. EISENHOWER ELEMENTARY SCHOOL - 847-870-3875
ANNE SULLIVAN SCHOOL - 847-870-3865
BETSY ROSS SCHOOL - 847-870-3868
MAC ARTHUR MIDDLE SCHOOL - 847-870-3879
JOHN HERSEY HIGH SCHOOL - 847-718-4800
9. ACCESS TO SIDEWALK SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL ADJACENT CONSTRUCTION. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS, UTILIZING CRUSHED STONE OR CRUSHED GRAVEL. THIS WORK SHALL BE INCLUDED IN THE PRICE OF PCC SIDEWALK.
10. EXISTING PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
11. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
12. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES RESIDENTIAL DRIVEWAYS AND TO 8-INCHES WHERE THE SIDEWALK CROSSES COMMERCIAL DRIVEWAYS. THIS SHALL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH.
13. TRANSVERSE EXPANSION JOINTS 3/4" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET. THIS WORK WILL BE INCLUDED IN THE COST OF THE APPLICABLE PORTLAND CEMENT CONCRETE SIDEWALK PAY ITEM.
14. A 1/2-INCH THICK EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK AND AT THE JUNCTION OF THE SIDEWALK AND CURB. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT OR PORTLAND CEMENT CONCRETE SIDEWALK PAY ITEMS.
15. DETECTABLE WARNINGS SHALL BE CAST IRON AND COMPLY WITH ADA REQUIREMENTS. THE DOMES LOCATED ON THE PANEL SHALL PARALLEL THE PAVEMENT CROSS WALK WITH THE CLOSEST EDGE LOCATED AT THE BACK OF CURB. THE PANEL COLOR SHALL BE - SELECTED BY THE ENGINEER. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS.
16. THE CONTRACTOR SHALL CONTACT THE LOCAL AGENCY MATERIAL INSPECTOR AT LEAST 48 HOURS PRIOR TO ANY CONCRETE OR HOT-MIX ASPHALT MATERIAL DELIVERIES.
17. A PORTABLE BATHROOM(S) SHALL BE PLACED ON THE JOB SITE(S) AND RELOCATED WHEN NECESSARY SO IT IS ACCESSIBLE TO WORKERS. IF WORK IS OCCURRING AT SEVERAL LOCATIONS, ONE PORTABLE BATHROOM SHALL BE PLACED AT EACH LOCATION WITHIN A REASONABLE DISTANCE FROM THE WORK AS DETERMINED BY THE ENGINEER. THIS SHALL BE INCLUDED IN THE PAY ITEM FOR MOBILIZATION.
18. FOR SIDEWALKS, AND CURB AND GUTTER CONSTRUCTION, AFTER THE CONCRETE HAS OBTAINED THE SPECIFIED STRENGTH OR WHEN DETERMINED BY THE ENGINEER, THE SPACE IN BACK OF CONSTRUCTION SHALL BE BACKFILLED TO THE TOP OF THE PROPOSED SIDEWALK WITH SAND OR OTHER MATERIAL APPROVED BY THE ENGINEER, AND NEATLY GRADED TO THE SATISFACTION OF THE ENGINEER. EXCESS SAND SHALL BE REMOVED JUST PRIOR TO COMPLETING PARKWAY RESTORATION WORK.
19. ANY SIGNS OR MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH IDOT AND U.S.P.S. STANDARDS AND INCLUDED IN THE APPLICABLE PAY ITEM. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.
20. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
21. INLET FILTERS SHALL BE CLEANED OF ALL SEDIMENT AND DEBRIS OR FILTER BAGS SHALL BE REPLACED AFTER EVERY 1/2" OR GREATER RAINFALL OR AS REQUIRED BY THE ENGINEER.
22. THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY THE CONTRACTOR'S OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE PER WEEK AND ADDITIONALLY AS DETERMINED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE MOBILIZATION PAY ITEM.
23. ALL POSTS, RAILROAD TIES, FENCES, IRRIGATION SYSTEMS, INVISIBLE FENCES AND DECORATIVE LANDSCAPE IN CONFLICT WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED AND PLACED AT THE ROW AS DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION AND SHALL BE INCLUDED IN THE COST OF THE EARTH EXCAVATION PAY ITEM. EVERY EFFORT SHALL BE MADE BY THE CONTRACTOR WHEN REMOVING THESE ITEMS TO PRESERVE THEM FROM HARM. ITEMS NOT RELOCATED SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR. CONTRACTOR IS NOT RESPONSIBLE FOR REINSTALLING LANDSCAPE ITEMS.
24. CONNECTION OF PROPOSED STORM SEWERS INTO EXISTING PIPE CULVERTS, STORM SEWER OR STORM SEWER STRUCTURES SHALL BE INCLUDED IN THE COST OF STORM SEWER PAY ITEM.
25. IN ALL TRENCHES CROSSING DRIVEWAYS AND ALL PROPOSED AND EXISTING ROADWAYS, THE MATERIAL FOR THE TOP 12 INCHES SHALL BE CA-6 CRUSHED GRAVEL OR CRUSHED STONE AND BE INCLUDED IN THE PAY ITEM FOR TRENCH BACKFILL. THE BACKFILL SHALL EXTEND TO AND BE MEASURED FOR PAYMENT TO THE EXISTING GROUND OR SURFACE ELEVATION.
26. FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF COST OF THE STRUCTURE.
27. EARTH EXCAVATION NECESSARY TO GRADE PROPOSED DITCHES TO PROPERLY DRAIN SHALL BE INCLUDED IN THE COST OF THE EARTH EXCAVATION PAY ITEM. SUITABLE EXCESS EARTH EXCAVATION SHALL BE REUSED ON THE PROJECT AT LOCATIONS REQUIRING FILL AT NO ADDITIONAL COST TO THE CONTRACT.
28. PIPE CULVERT REMOVAL SHALL APPLY TO ALL EXISTING CULVERT PIPES TO BE REMOVED, REGARDLESS OF SIZE, TYPE OR MATERIAL. REMOVAL OF CONCRETE FLARED END SECTIONS ALSO INCLUDES REMOVING AND DISPOSING EXISTING CONCRETE OR MASONRY HEADWALLS AT THE ENDS OF CULVERTS.
29. IN AREAS WHERE THE EXISTING DRIVEWAY, SIDEWALK, OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY, SIDEWALK, OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE REMOVAL PAY ITEMS.
30. THE CONTRACTOR SHALL SUBMIT PARTIAL WAIVERS OF LIEN FROM ALL SUBCONTRACTORS AND SUPPLIERS WITH EACH PARTIAL PAYMENT ESTIMATE AND CONTRACTOR'S AFFIDAVIT FOR SUBCONTRACTORS AND SUPPLIERS WITH SECOND PAYMENT REQUEST FOR THE PREVIOUS PAYMENT ESTIMATES AND THEN WITH ALL SUBSEQUENT PAYMENT ESTIMATES.
31. THE ENGINEER WILL BE THE MUNICIPALITY'S REPRESENTATIVE DURING THE CONSTRUCTION PERIOD. THE ENGINEER WILL FURNISH A RESIDENT PROJECT REPRESENTATIVE (RPR) TO ASSIST THE ENGINEER IN PROVIDING JOB-SITE OBSERVATION OF THE CONTRACTOR'S WORK. THE RPR WILL PROVIDE BASE LINES, BENCHMARKS AND REFERENCE POINTS, ASSIST THE CONTRACTOR WITH INTERPRETATION OF THE PLANS AND SPECIFICATIONS, OBSERVE IN GENERAL IF THE CONTRACTOR'S WORK IS IN CONFORMITY WITH THE CONTRACT DOCUMENTS, AND MONITOR THE CONTRACTOR'S PROGRESS AS RELATED TO THE DATE OF COMPLETION. THE ENGINEER WILL NOT SUPERVISE, DIRECT, CONTROL OR HAVE AUTHORITY OVER OR BE RESPONSIBLE FOR THE CONTRACTOR'S MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES OF CONSTRUCTION, OR THE SAFETY PRECAUTIONS AND PROGRAMS INCIDENT THERETO, OR FOR ANY FAILURE OF THE CONTRACTOR TO COMPLY WITH LAWS AND REGULATIONS APPLICABLE TO THE FURNISHING OR PERFORMANCE OF THE WORK. THE ENGINEER WILL NOT BE RESPONSIBLE FOR THE CONTRACTOR'S FAILURE TO PERFORM OR FURNISH THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. THE ENGINEER WILL NOT BE RESPONSIBLE FOR THE ACTS OR OMISSIONS OF THE CONTRACTOR OR ANY SUBCONTRACTOR, ANY SUPPLIER, OR OF ANY OTHER PERSON OR ORGANIZATION PERFORMING OR FURNISHING ANY OF THE WORK. THESE LIMITATIONS ON AUTHORITY AND RESPONSIBILITY SET FORTH HEREIN SHALL ALSO APPLY TO THE ENGINEER'S CONSULTANTS, RESIDENT PROJECT REPRESENTATIVE AND ASSISTANTS.
32. STORM STRUCTURE OFFSET LOCATIONS ARE TO THE EDGE OF PAVEMENT IF THE STRUCTURE IS IN THE CURB LINE OR TO THE CENTER OF STRUCTURE IF THE STRUCTURE IS NOT IN THE CURB LINE.
33. ALL EXISTING FRAME AND GRATES OR FRAME AND LIDS SCHEDULED TO BE REMOVED OR REPLACED SHALL BE DELIVERED TO THE CITY PUBLIC WORKS FACILITY. CARE SHALL BE TAKEN TO AVOID DAMAGE TO FRAMES. THIS WORK WILL BE INCLUDED IN THE APPLICABLE REMOVAL OR REPLACEMENT PAY ITEM.
34. CONTRACTOR SHALL PROVIDE AS-BUILT RIM AND INVERT ELEVATIONS FOR ALL PROPOSED STRUCTURES. THIS WORK SHALL BE INCLUDED IN THE COST OF THE PROPOSED STRUCTURE.
35. BEDDING MATERIAL FOR STORM SEWERS AND ALL UTILITY STRUCTURES SHALL BE CA-11 CRUSHED AGGREGATE.
36. NO GAPS WILL BE ALLOWED BETWEEN FRAMES AND NEW STRUCTURES. THE CONTRACTOR SHALL SET THE FRAME IN CONCRETE TO CLOSE ANY GAPS PRIOR TO CURB BEING POURED.
37. HYDRAULIC CEMENT SHALL BE USED FOR ALL INTERIOR JOINTS OF STORM, SANITARY AND WATER STRUCTURES TO MAKE THEM WATER TIGHT.
38. TEMPORARY EROSION CONTROL MEASURES SHALL BE INSTALLED AS REQUIRED. WHERE PARKWAYS ARE DISTURBED, TEMPORARY EROSION CONTROL MEASURES SHALL BE ESTABLISHED WITHIN 14 DAYS OF THE COMPLETION OF DISTURBANCE AND MAINTAINED TO THE SATISFACTION OF THE CITY. THIS WORK, INCLUDING MAINTENANCE, SHALL BE INCLUDED IN THE COST OF THE CONTRACT UNLESS OTHERWISE NOTED.
39. PORTIONS OF THE EXISTING COMBINATION CONCRETE CURB AND GUTTER WITHIN THE PROJECT LIMITS ARE PAINTED. IF PAINTED COMBINATION CONCRETE CURB AND GUTTER REQUIRES REMOVAL AND REPLACEMENT, THE REPLACEMENT COMBINATION CONCRETE CURB AND GUTTER SHALL BE PAINTED TO MATCH THE COLOR OF THE COMBINATION CONCRETE CURB AND GUTTER REMOVED. SAMPLES OF THE PAINT SHALL BE PROVIDED TO THE ENGINEER PRIOR TO APPLICATION TO ENSURE COMPLIANCE WITH THE MANUFACTURERS RECOMMENDATIONS. NO PAINTING SHALL OCCUR UNTIL THE PAINT AND APPLICATION IS APPROVED BY THE ENGINEER. PAINTING SHALL BE INCLUDED IN THE COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT PAY ITEM. IF NEW CURB AND GUTTER IS DEEMED NECESSARY TO BE PAINTED, PAINTING SHALL BE INCLUDED IN THE NEW CURB AND GUTTER PAY ITEM.
40. ANY ANTI-STRIPPING ADDITIVE REQUIRED SHALL BE INCLUDED IN THE COST OF THE SURFACE COURSE.
41. "TYPE I FRAME AND LIDS" SHALL BE HEAVY DUTY (4") WHEN LOCATED IN THE PARKWAY.
42. ALL OPEN LIDS SHALL BE BICYCLE SAFE.

C.C.D.O.T.H. CONSTRUCTION NOTES

1. IF ANY PAVEMENT MARKINGS OR SIGNING ALONG WILLOW ROAD ARE DISTURBED DUE TO THE PROPOSED IMPROVEMENTS, IT SHALL BE REINSTALLED BY THE CONTRACTOR.
2. DURING CONSTRUCTION OF SIDEWALKS AND OTHER RELATED ITEMS, IF LANE CLOSURE ALONG WILLOW ROAD FOR WESTBOUND TRAFFIC IS REQUIRED, IT SHALL BE ONLY DURING NON-PEAK HOURS I.E. BETWEEN 8:30 AM TO 3:30 PM. AND IT SHALL FOLLOW APPLICABLE IDOT TRAFFIC CONTROL STANDARDS. ANY OVERNIGHT LANE CLOSURES ALONG WILLOW ROAD WILL NOT BE ALLOWED.

GHA #4755.018

FILE NAME =	USER NAME = MCoImen	DESIGNED - MAC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
G:\4700\4755 Prospect Heights Projects\755.018 Willow Road Sidewalk\Drawings\Drawings\02 - G in Note Index	PLLOT SCALE = 20.0000 ' / in.	CHECKED - KLB	REVISED -						1289	12-00051-00-SW	COOK	21	3
Default	PLLOT DATE = 2/17/2015	DATE -	REVISED -		SCALE: NONE SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 61B34				
												ILLINOIS FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE	
				80% FED	20% LOCAL
				BIKE PATH	
				28	
20101100	TREE TRUNK PROTECTION	EACH	15	15	
20101200	TREE ROOT PRUNING	EACH	15	15	
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	15	15	
20101700	SUPPLEMENTAL WATERING	UNIT	1	1	
20200100	EARTH EXCAVATION	CU YD	335	335	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	25	25	
20800150	TRENCH BACKFILL	CU YD	37	37	
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	3040	3040	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	60	60	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	60	60	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	60	60	
25100630	EROSION CONTROL BLANKET	SQ YD	3040	3040	
25200100	SODDING	SQ YD	3040	3040	
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	65	65	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE	
				80% FED	20% LOCAL
				BIKE PATH	
				28	
28000305	TEMPORARY DITCH CHECKS	FOOT	74	74	
28000400	PERIMETER EROSION BARRIER	FOOT	1235	1235	
28000510	INLET FILTERS	EACH	7	7	
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	3040	3040	
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	25	25	
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	808	808	
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	424	424	
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	1908	1908	
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	7	7	
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	54	54	
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	36	36	
42001300	PROTECTIVE COAT	SQ YD	76	76	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	7267	7267	
42400800	DETECTABLE WARNINGS	SQ FT	86	86	

• SPECIALTY ITEM

GHA #4755.018

FILE NAME =	USER NAME = MColeson	DESIGNED - MAC	REVISED -
G:\4780\4755 Prospect Heights Projects\755.018 Willow Road Sidewalk\Drawings\Plans\Folder\Drawings\4755\CON.05 - S00.dgn			
Default	PLOT SCALE = 28.0000' / 1"	CHECKED - KLB	REVISED -
	PLOT DATE = 2/16/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	4
CONTRACT NO. 61B34				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				80% FED 20% LOCAL
				BIKE PATH
				28
42400800	DETECTABLE WARNINGS	SQ FT	86	86
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	424	424
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	21	21
44000600	SIDEWALK REMOVAL	SQ FT	165	165
44002206	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 1 1/2"	SQ YD	76	76
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	76	76
50105220	PIPE CULVERT REMOVAL	FOOT	216	216
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	11	11
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	3	3
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	563	563
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	184	184
60100905	PIPE DRAINS 4"	FOOT	0	
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	4	4
60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	3	3

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				80% FED 20% LOCAL
				BIKE PATH
				28
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	1	1
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	21	21
67100100	MOBILIZATION	L SUM	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
72000100	SIGN PANEL - TYPE 1	SQ FT	124	124
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	370	370
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	414	414
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	53	53
78300100	PAVEMENT MARKING REMOVAL	SQ FT	45	45
X0322936	REMOVE EXISTING FLARED END SECTION	EACH	1	1
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	9	9
X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	3	3
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52

* SPECIALTY ITEM

FILE NAME =	USER NAME = MCoeman	DESIGNED - KLB	REVISED -
G:\4755 Prospect Heights Projects\755.018 Willow Road Sidewalk\Drawings\Plans\Folder\DRAIN\Sheets\4755.018.05 - S00.dgn		CHECKED - KLB	REVISED -
	PLOT SCALE = 28.0000' / in.	DATE -	REVISED -
S00 SHEET 2	PLOT DATE = 1/29/2015		

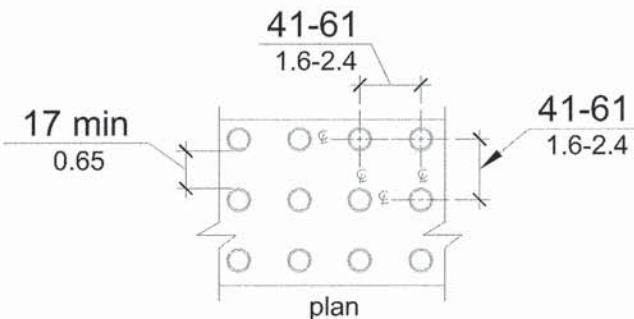
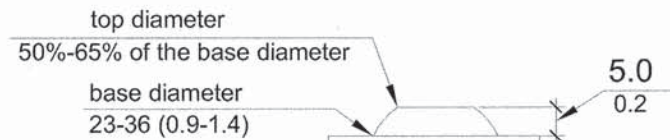
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 1289	SECTION 12-00051-00-SW	COUNTY COOK	TOTAL SHEETS 21	SHEET NO. 5
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

GHA #4755.018



DETECTABLE WARNING TILE DETAILS

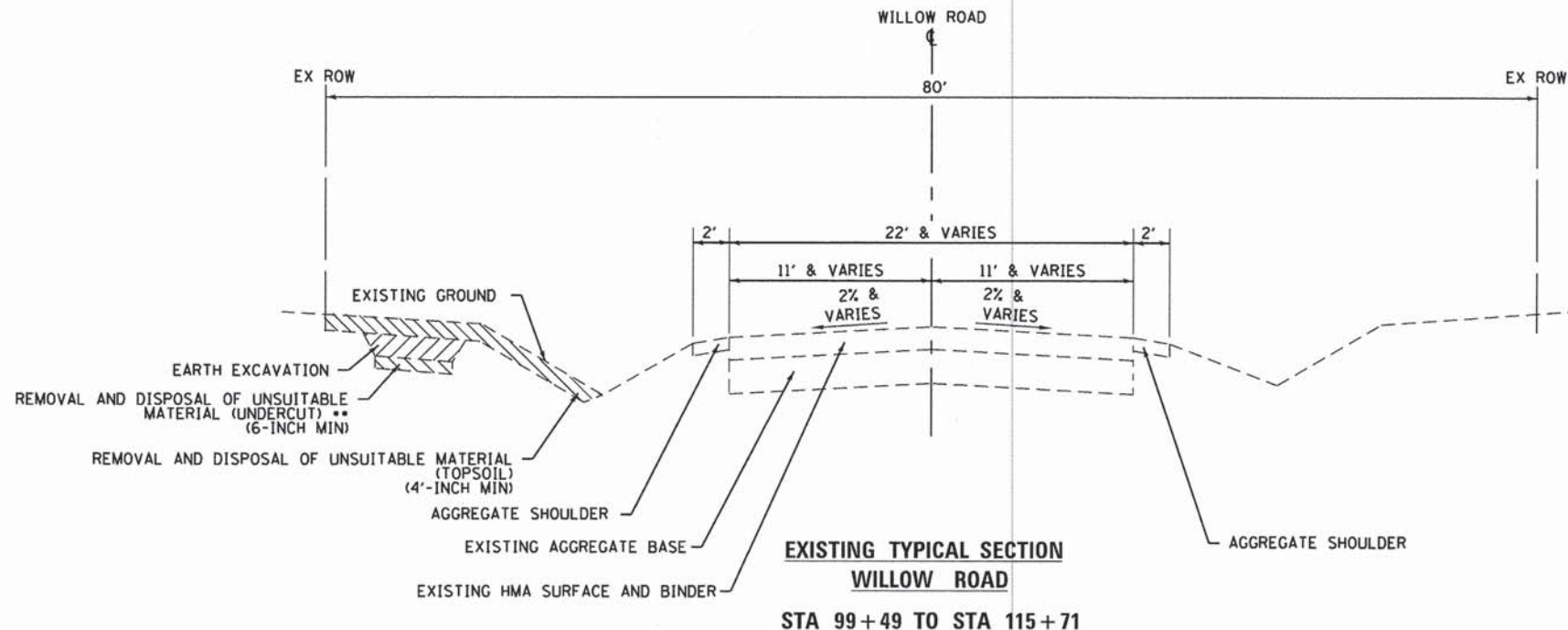
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
DRIVEWAYS	
HMA SURFACE COURSE, MIX "D", N50, (IL-9.5mm), 1 1/2"	4% @ 50 GYR.
HMA BINDER COURSE, (IL-19.0, N50), 2 1/4"	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES, TYPE IV	4% @ 70 GYR.

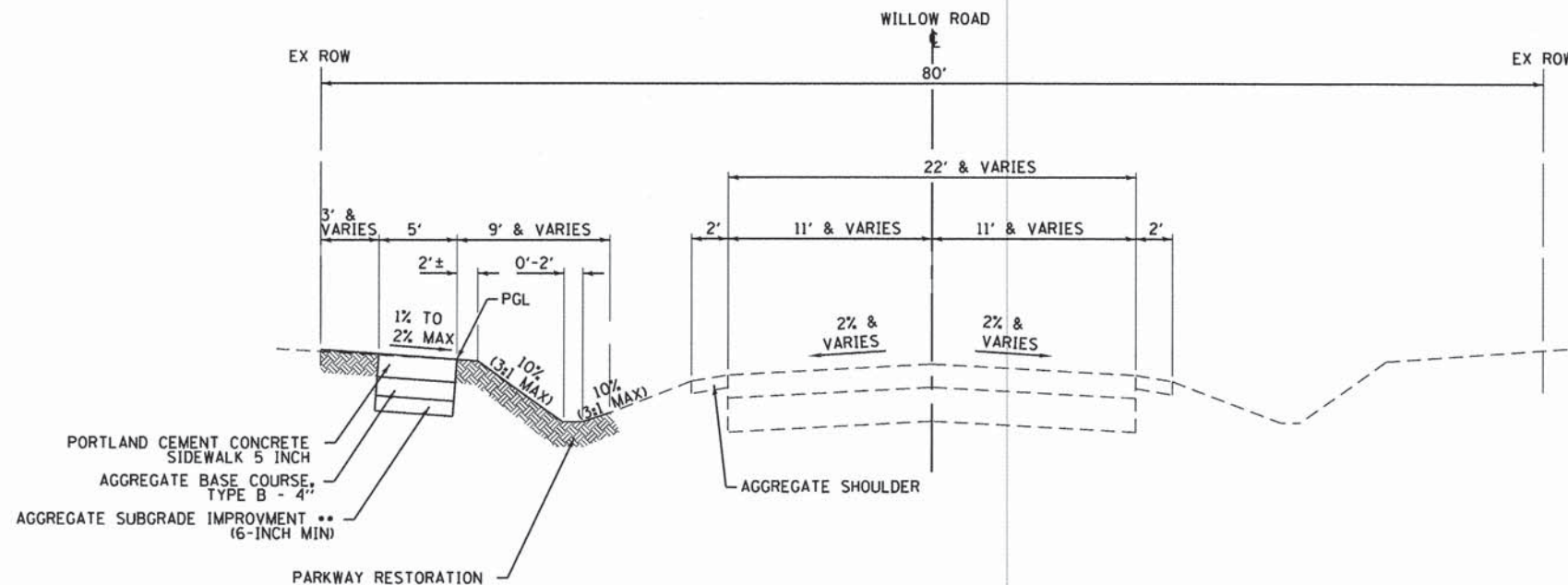
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SO YD/IN.

THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

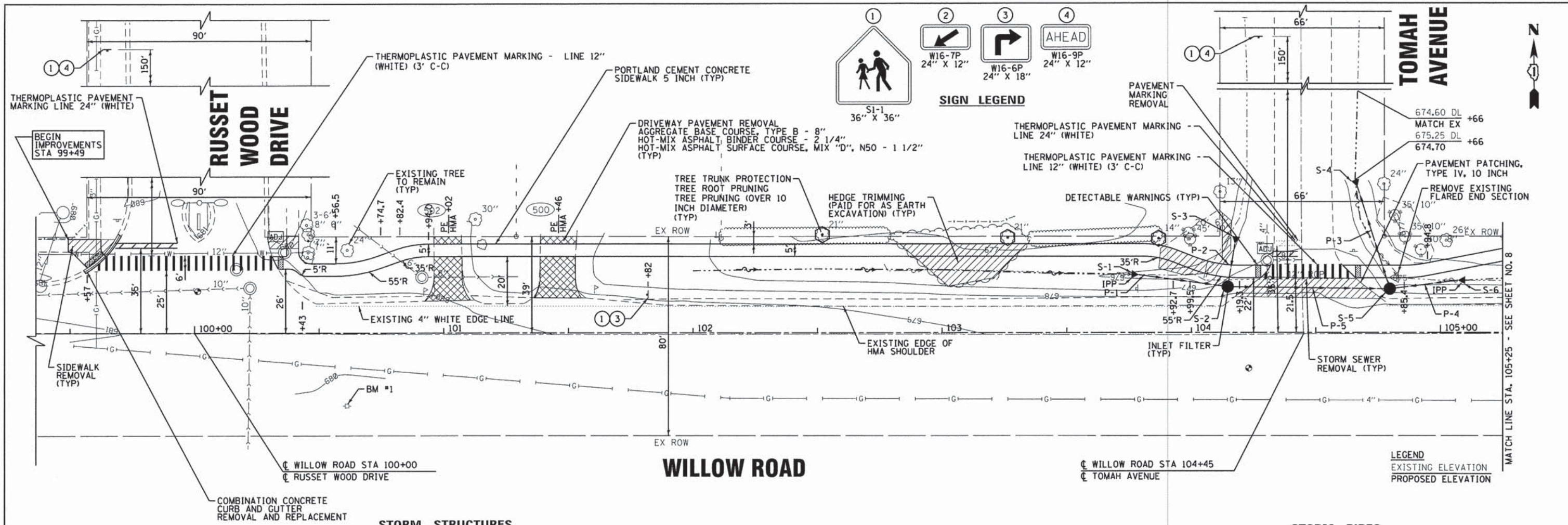
** AGGREGATE SUBGRADE IMPROVEMENT HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE THE CONTRACTOR.



**EXISTING TYPICAL SECTION
WILLOW ROAD
STA 99+49 TO STA 115+71**



**PROPOSED TYPICAL SECTION
WILLOW ROAD
STA 99+49 TO STA 115+71**

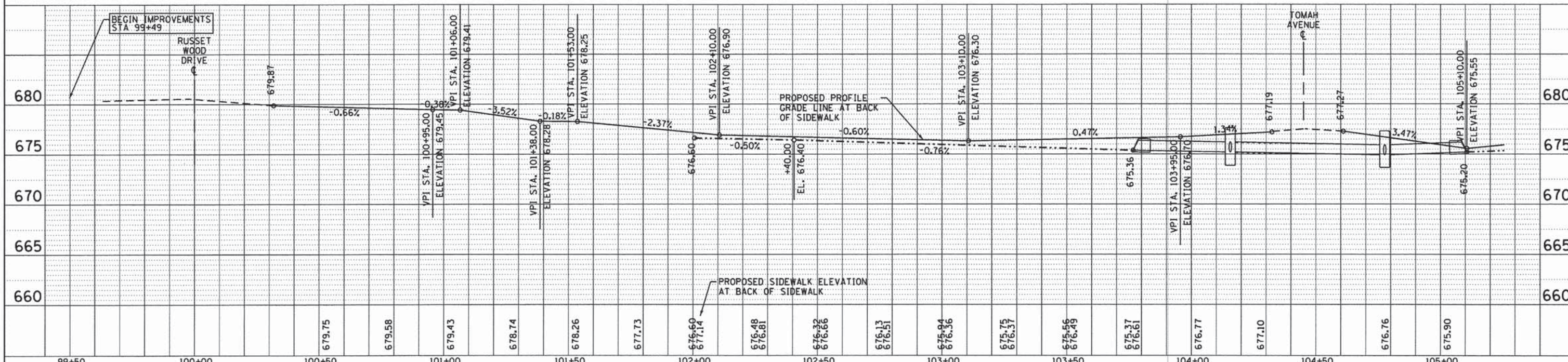


STORM STRUCTURES

STRUCTURE NUMBER	STATION	OFFSET	STRUCTURE	FRAME AND LID	RIM	INVERTS		
S-1	103+76	23.4' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"		675.36 E			
S-2	104+15	18.5' LT	CATCH BASIN, TYPE A	TYPE 1 FRAME, OPEN LID	677.61	675.19 E	675.19 N	675.19 W
S-3	104+18	38.5' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"			675.25 S		
S-4	104+66	61.2' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"			674.70 SE		
S-5	104+80	17.6' LT	CATCH BASIN, TYPE A	TYPE 1 FRAME, OPEN LID	677.30	674.90 W	674.90 NW	674.90 E
S-6	105+10	21.0' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"			675.20 W		

STORM PIPES

PIPE NUMBER	CLASS/TYPE	DIAMETER	LENGTH (FT)	SLOPE (%)	TBF (CU YD)
P-1	STORM SEWER, CLASS A, TYPE 1	12"	39	0.41	0.0
P-2	STORM SEWER, CLASS A, TYPE 1	12"	20	0.29	1.2
P-3	STORM SEWER, CLASS A, TYPE 1	12"	46	0.45	1.5
P-4	STORM SEWER, CLASS A, TYPE 1	12"	31	0.98	0.0
P-5	STORM SEWER, CLASS A, TYPE 1	12"	65	0.45	9.2



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 850 Forest Edge Drive • Vernon Hills, IL 60061
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DATE - 12-5-2014	FILE - 120607_PPL.sh+

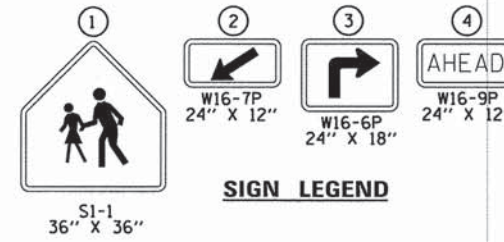
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE

SCALE: H: 1"=20' V: 1"=5'
 STA. 99+49 TO STA. 105+25
 F.A.U. RTE. 1289 SECTION 12-00051-00-SW COUNTY COOK TOTAL SHEETS 21 SHEET NO. 7
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT CONTRACT NO. 61B34

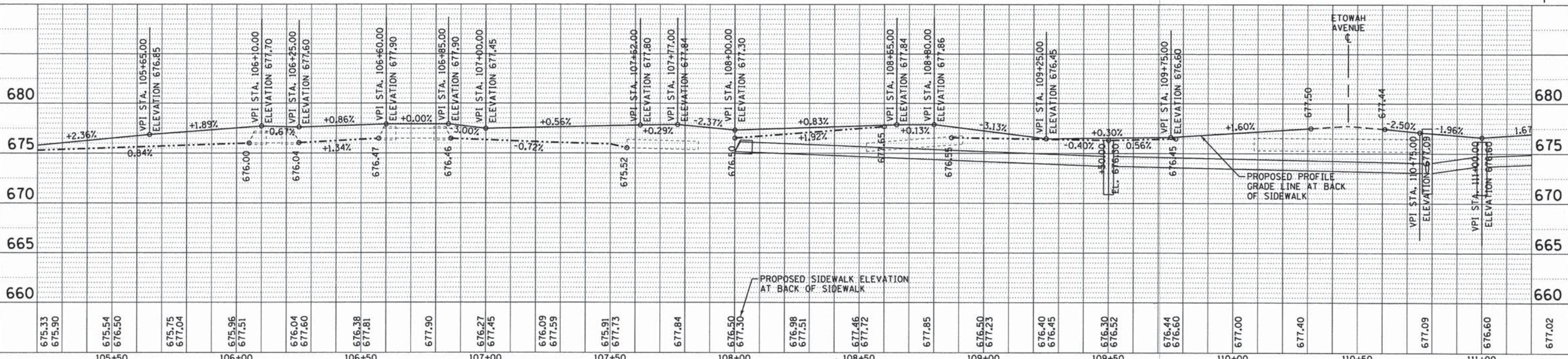
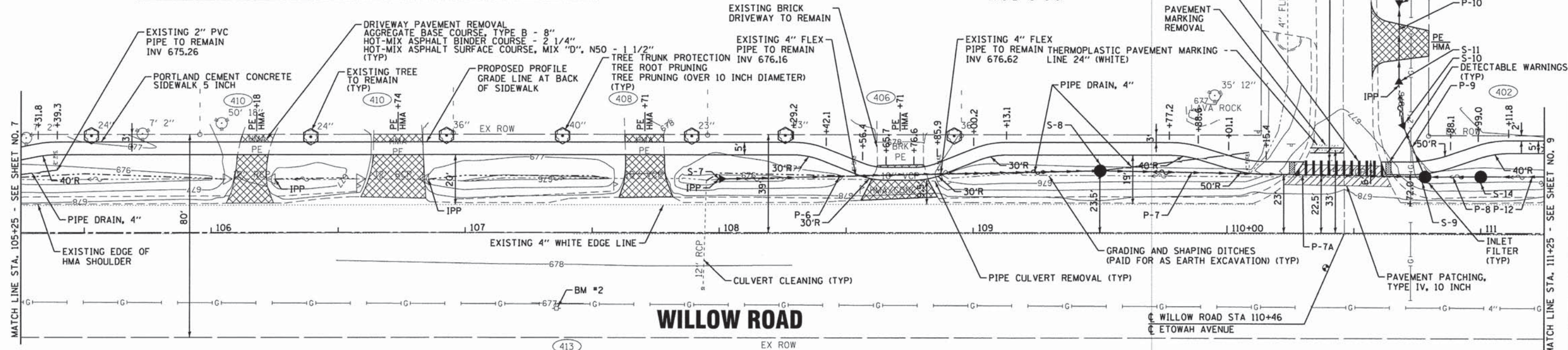
STORM STRUCTURES

STRUCTURE NUMBER	STATION	OFFSET	STRUCTURE	FRAME AND LID	RIM	INVERTS
S-7	108+00	21.1' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"		675.15 E	
S-8	109+50	24.4' LT	CATCH BASIN, TYPE A	TYPE 8 GRATE	676.30	673.70 W 673.70 E
S-9	110+78	22.0' LT	CATCH BASIN, TYPE A	TYPE 1 FRAME, OPEN LID	677.50	673.05 W 673.05 NW 675.05 E
S-10	110+69	43.5' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 15"		672.90 SE	
S-11	110+68	59.0' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 15"		672.80 N	
S-12	110+68	91.0' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 15"		672.16 S	
S-13	110+69	118.6' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"		671.65 N	
S-14	111+00	22.3' LT	CATCH BASIN, TYPE A	TYPE 8 GRATE	676.20	673.70



STORM PIPES

PIPE NUMBER	CLASS/TYPE	DIAMETER	LENGTH (FT)	SLOPE (%)	TBF (CU YD)
P-6	STORM SEWER, CLASS A, TYPE 1	12"	150	0.97	5.7
P-7	STORM SEWER, CLASS A, TYPE 1	15"	128	1.05	13.1
P-8	STORM SEWER, CLASS A, TYPE 1	12"	22	0.00	0.4
P-9	STORM SEWER, CLASS A, TYPE 1	15"	23	0.50	1.1
P-10	STORM SEWER, CLASS A, TYPE 1	15"	33	5.30	3.2
P-11	STORM SEWER, CLASS A, TYPE 1	12"	10	1.00	0.0
P-12	STORM SEWER, CLASS A, TYPE 1	12"	58	0.96	0.4



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DATE - 12-5-2014	FILE - 120607_PP2.sht

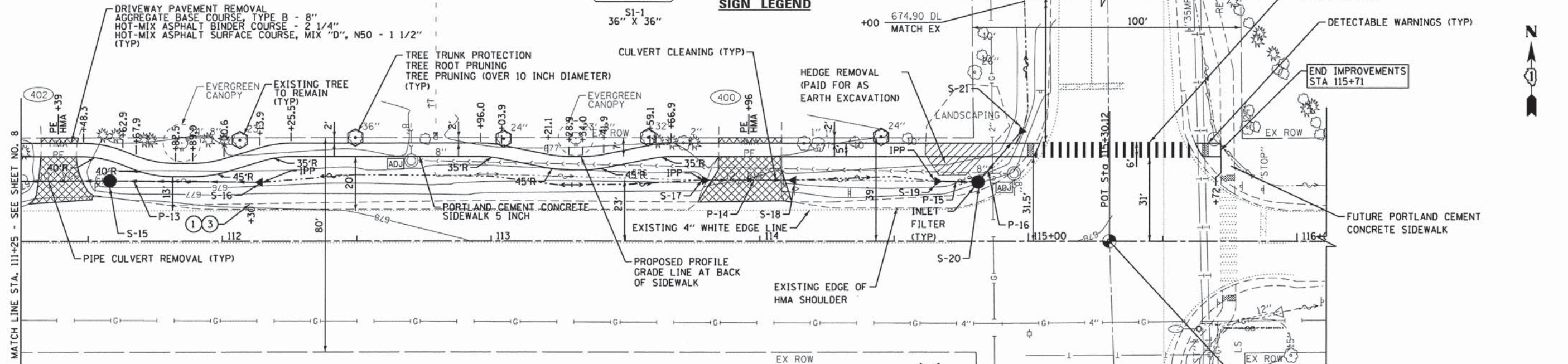
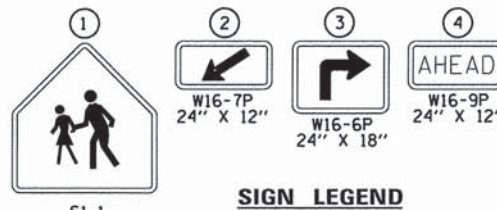
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE

SCALE: H: 1"=20' V: 1"=5' STA. 105+25 TO STA. 111+25
 SECTION 12-00051-00-SW COUNTY COOK TOTAL SHEETS 21 SHEET NO. 8
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT CONTRACT NO. 61B34

STORM PIPES

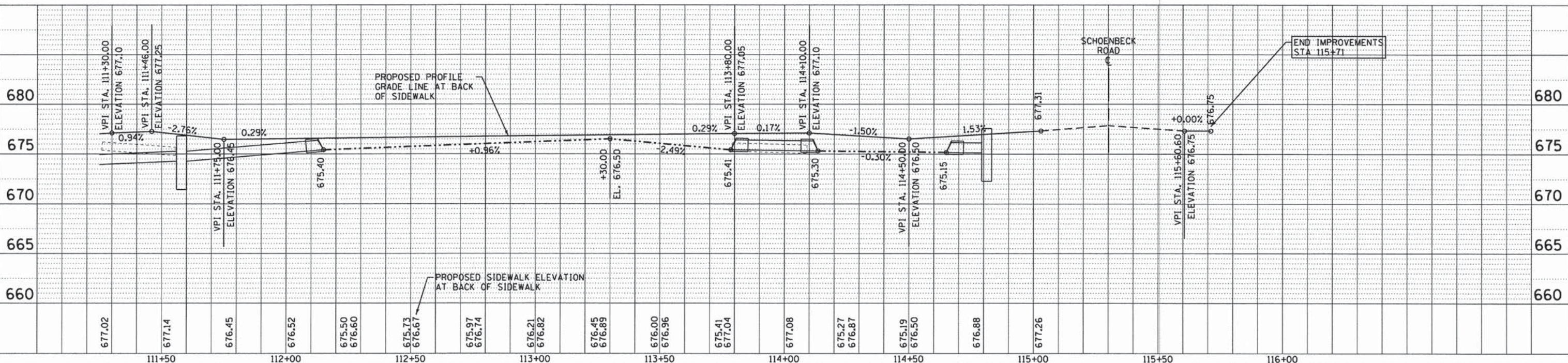
PIPE NUMBER	CLASS/TYPE	DIAMETER	LENGTH (FT)	SLOPE (%)	TBF (CU YD)
P-13	STORM SEWER, CLASS A, TYPE 1	12"	57	2.00	0.3
P-14	STORM SEWER, CLASS A, TYPE 1	12"	20	0.30	0.3
P-15	STORM SEWER, CLASS A, TYPE 1	12"	20	0.30	0.0
P-16	STORM SEWER, CLASS A, TYPE 1	12"	10	0.30	0.2



STORM STRUCTURES

STRUCTURE NUMBER	STATION	OFFSET	STRUCTURE	FRAME AND LID	RIM	INVERTS
S-15	111+58	22.0' LT	CATCH BASIN, TYPE A	TYPE 8 GRATE	676.80	674.26 E 674.26 W
S-16	112+15	22.0' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"			675.40 E
S-17	113+79	22.7' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"			675.41 E
S-18	114+14	22.7' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"			675.30 W
S-19	114+65	22.3' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"			675.15 W
S-20	114+81	22.0' LT	CATCH BASIN, TYPE A	TYPE 1 FRAME, OPEN LID	677.55	675.11 E 675.11 NE
S-21	114+95	36.5' LT	PRECAST REINFORCED CONCRETE FLARED END SECTION, 12"			675.05 SW

WILLOW ROAD



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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE

SCALE: H: 1"=20' V: 1"=5'

STA. 111+25 TO STA. 115+71

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	9
CONTRACT NO. 61B34				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT 61B34

CONSTRUCTION STAGING

1. THE CONTRACTOR SHALL SUBMIT A PRE-PLANNED SEQUENCE OF WORK (CONSTRUCTION SCHEDULE) AT THE PRECONSTRUCTION CONFERENCE FOR REVIEW AND APPROVAL. WORK SHALL BE SCHEDULED TO MINIMIZE INCONVENIENCE TO RESIDENTS AND TO MAINTAIN A REASONABLE LEVEL OF CONSTRUCTION EFFICIENCY. THE CITY AND/OR ENGINEER RESERVES THE RIGHT TO RESTRICT WORK ON ANY ROADWAY SEGMENT IF CONSTRUCTION OPERATIONS ON A PREVIOUS SEGMENT ARE UNACCEPTABLE; TRAFFIC CONTROL OPERATIONS BECOME UNACCEPTABLE; OR AN EROSION CONTROL DEFICIENCY EXISTS.
2. CONSTRUCTION OPERATIONS INVOLVING THE TEMPORARY CLOSING OF DRIVEWAYS OR THE ROAD SHALL NOT COMMENCE WITHOUT THE CONTRACTOR NOTIFYING THE CITY AT LEAST THREE DAYS PRIOR. THE CONTRACTOR WILL ALSO BE REQUIRED TO PROVIDE 48-HOUR NOTICE TO AFFECTED PARTIES BY DISTRIBUTING NOTICES AND/OR POSTING SIGNS.
3. ALL STREETS MUST BE OPEN TO TWO-WAY TRAFFIC AT THE END OF EACH DAY.
4. ACCESS FOR LOCAL TRAFFIC, MAIL SERVICE, GARBAGE SERVICE AND EMERGENCY VEHICLES MUST BE MAINTAINED AT ALL TIMES.
5. ALL OPEN HOLES AND TRENCHES MUST BE FILLED AT THE END OF EACH DAY.
6. RESIDENTS SHALL HAVE ACCESS TO THEIR DRIVEWAYS AT THE END OF EACH DAY (EXCEPT WITHIN 3 DAYS OF A CONCRETE POUR).
7. A COOK COUNTY HIGHWAY DEPARTMENT PERMIT IS REQUIRED FOR WORK WITHIN THE COUNTY ROW AS PART OF THE IMPROVEMENTS. THIS PERMIT HAS BEEN SUBMITTED BUT NOT YET APPROVED. THE CONTRACTOR SHALL NOT PERFORM THIS WORK UNTIL THE PERMIT IS APPROVED.

THE FOLLOWING IS A LIST OF STAGES FOR EACH TYPE OF CONSTRUCTION EXPECTED ON THIS PROJECT.

- STAGE I: PRIOR TO CONSTRUCTION, ESTABLISH EROSION CONTROL AND NECESSARY TRAFFIC CONTROL.
 STAGE II: SIDEWALK CONSTRUCTION

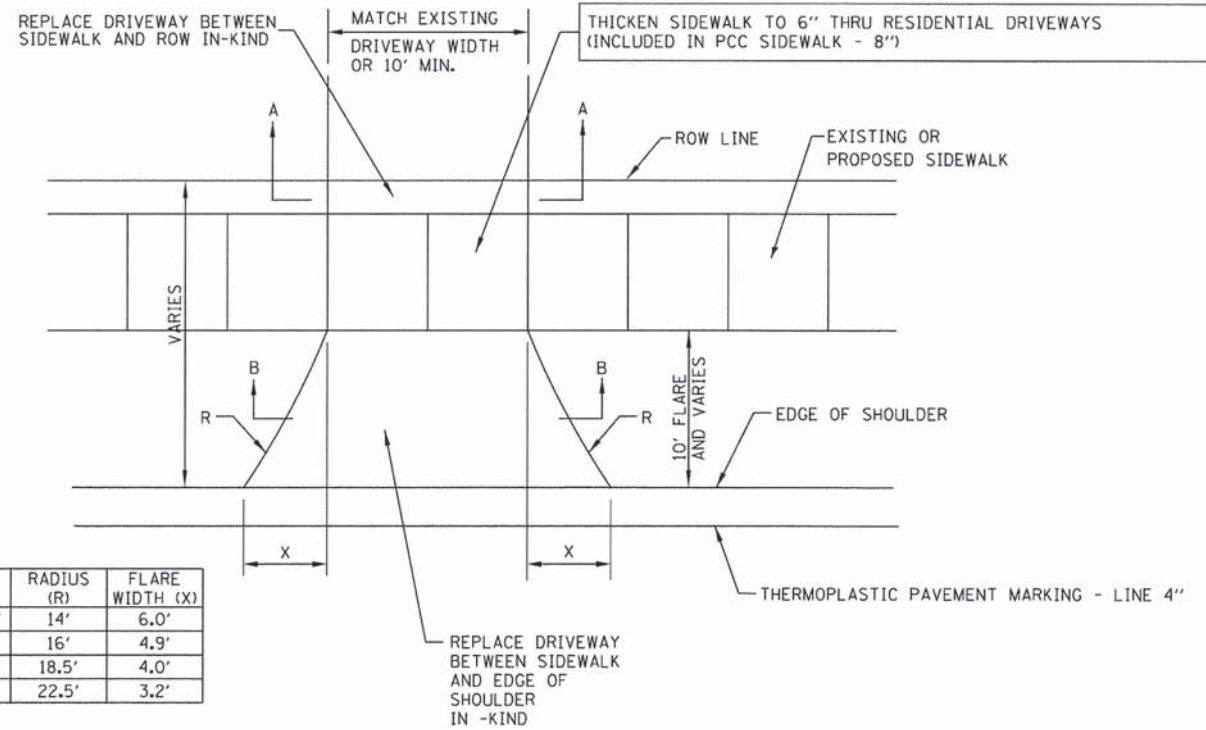
1. INSTALL NECESSARY STORM SEWER, END SECTIONS AND DITCHES.
2. COMPLETE SIDEWALK INSTALLATION.
3. CONSTRUCT DRIVEWAYS. ALLOW 72 HOURS CURE TIME FOR CONCRETE DRIVEWAYS.
4. BEGIN PARKWAY RESTORATION.
5. INSTALL REQUIRED PAVEMENT MARKING.
6. COMPLETE PARKWAY RESTORATION AND ANY REMAINING DRIVEWAYS.
7. COMPLETE PUNCH LIST ITEMS.
8. REMOVE TEMPORARY EROSION CONTROL ITEMS ONCE SEED ESTABLISHES.

SOIL EROSION AND SEDIMENT CONTROL NOTES

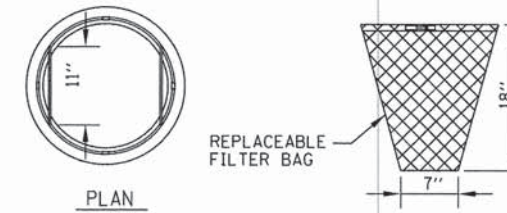
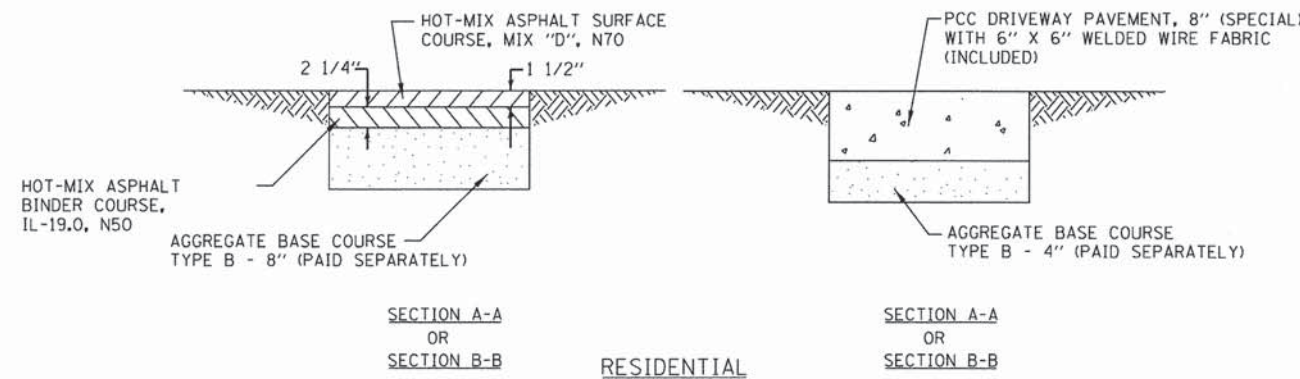
1. EROSION AND SEDIMENT CONTROL PRACTICES SHALL MEET ALL FEDERAL AND STATE LAWS AND COOK COUNTY AND CITY OF PROSPECT HEIGHTS ORDINANCES FOR SEDIMENT AND EROSION CONTROL, INCLUDING ALL REQUIREMENTS OF THE CITY'S NPDES PERMIT.
2. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS, AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
3. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED DOWNSTREAM PRIOR TO OR CONCURRENTLY WITH THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE UPSTREAM.
4. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR FINAL MEASURES WITHIN 7 CALENDAR DAYS OF THE END OF HYDROLOGIC DISTURBANCE OR REDISTURBANCE.
5. ALL STORM SEWERS AND CULVERTS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE EROSION CONTROL MEASURE.
6. ALL TEMPORARY EROSION CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
7. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. EROSION CONTROL MEASURES SHALL BE INSPECTED AND CLEANED OR REPAIRED EVERY 2 WEEKS OR AFTER A RAINFALL EVENT OF 1/2 INCH OR MORE.
8. EROSION CONTROL MEASURES LISTED IN THE SWPPP ARE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DETERMINED BY THE ENGINEER OR OUTSIDE INSPECTION AGENCY.
9. ALL SHEET FLOW AREAS THAT CAN POTENTIALLY FLOW OFFSITE OR INTO UNDISTURBED AREAS SHALL BE PROTECTED WITH PERIMETER EROSION BARRIER (SILT FENCE) PLACED AT THE LIMITS OF THE DISTURBANCE.
10. ALL UPSTREAM ENDS OF EXISTING AND PROPOSED CULVERTS LOCATED DOWNSTREAM OF A DISTURBED AREA SHALL HAVE INLET AND PIPE PROTECTION INSTALLED AROUND THE END SECTION OR PIPE OPENING.
11. ALL EXISTING AND PROPOSED OPEN-LID STRUCTURES LOCATED WITHIN OR DOWNSTREAM OF A DISTURBED AREA SHALL BE PROTECTED BY INSTALLING INLET FILTERS (FILTER BASKETS) UNDER THE FRAME OR LID.
12. TEMPORARY DITCH CHECKS SHALL BE INSTALLED ON ALL DISTURBED DITCH LINES AT EVERY ONE (1) FOOT IN ELEVATION CHANGE AND AT THE LIMITS OF DITCH DISTURBANCE WHERE DITCH FLOW CONTINUES DOWNSTREAM. STRAW BALES SHALL NOT BE USED FOR TEMPORARY DITCH CHECKS.
13. IF EROSION CONTROL MEASURES DO NOT MEET THE REQUIREMENTS OF THE NPDES PERMIT, THE SWPPP, OR THESE EROSION CONTROL NOTES TO THE SATISFACTION OF THE ENGINEER, AN EROSION CONTROL DEFICIENCY DEDUCTION SHALL BE CHARGED TO THE CONTRACTOR IF THE DEFICIENCIES ARE NOT CORRECTED WITHIN THE SPECIFIED TIME OF RECEIVING WRITTEN NOTIFICATION FROM THE ENGINEER.

GHA #4755.018

FILE NAME =	USER NAME = M Coleman	DESIGNED - KLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONSTRUCTION STAGING AND EROSION CONTROL NOTES			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
G:\4755\Prospect Heights Projects\755.018 Willow Road Sidewalk\DGNs\Plans\Fold	DRAWN	DATE	REVISED					1289	12-00051-00-SW	COOK	21	10
Default	PLOT SCALE = 20.0000' / 1" =	CHECKED - KLB	REVISED -		CONTRACT NO. 61B34			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 1/29/2015	DATE -	REVISED -		SCALE: NONE	SHEET	OF	SHEETS	STA.	TO STA.		

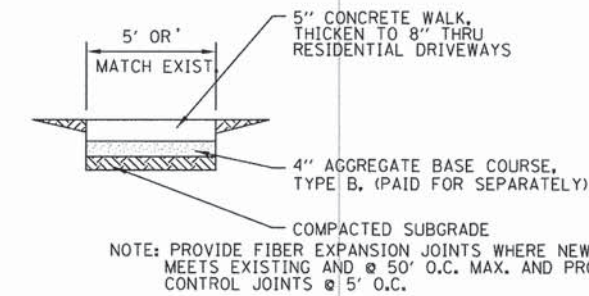


FOR DRIVEWAY WIDTH	RADIUS (R)	FLARE WIDTH (X)
LESS THAN 14'	14'	6.0'
14' TO 16'	16'	4.9'
17' TO 18'	18.5'	4.0'
OVER 18'	22.5'	3.2'

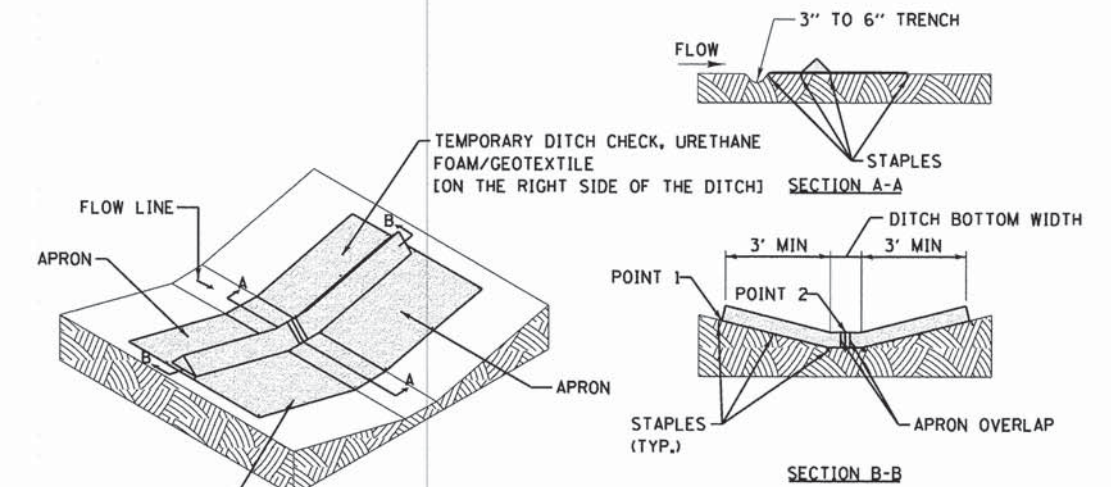


GENERAL NOTES:
FRAME: TOP RING CONSTRUCTED FROM 1 1/4" x 1 1/4" x 1/8" ANGLE. BASE RING CONSTRUCTED OF 1 1/2" x 1/2" x 1/8" CHANNEL. HANDLES & SUSPENSION BRACKETS CONSTRUCTED FROM 1/4" x 1 1/4" FLAT. ALL STEEL CONFORMING TO ASTM-A36.
REPLACEABLE BAG: CONSTRUCTED FROM 4 OZ./SQ. YD. NON-WOVEN POLYPROPYLENE GEOTEXTILE REINFORCED WITH POLYESTER MESH. CONNECTED TO BASE RING WITH STAINLESS STEEL STRAP & LOCK.

INLET FILTER



PORTLAND CEMENT CONCRETE SIDEWALK - 5 INCH



TEMPORARY DITCH CHECK, URETHANE FOAM/GEOTEXTILE [ON THE LEFT SIDE OF THE DITCH]

TEMPORARY DTICH CHECKS

NOTES:
 1. THE INSTALLATION SHOWN ABOVE WILL BE MEASURED FOR PAYMENT IN FEET PERPENDICULAR TO THE FLOW LINE.

STAPLES SHALL BE PLACED WHERE THE UNITS OVERLAP AND IN THE CENTER OF THE DITCH AS SHOWN ON THE DIAGRAM.
 POINT 1 MUST BE HIGHER THAN POINT 2 TO INSURE THAT WATER FLOWS OVER THE DIKE AND NOT AROUND THE ENDS.

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CHECKED - KLB	REVISED -
DATE - 12-5-2014	FILE - 120607.MISC-DETAILS.sht

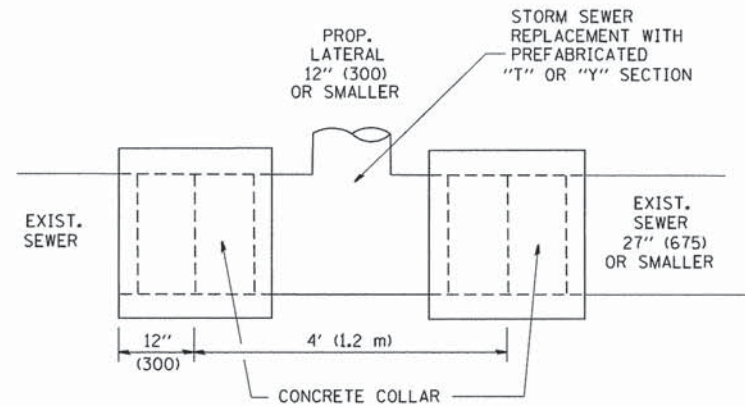
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS DETAILS

SCALE: NONE

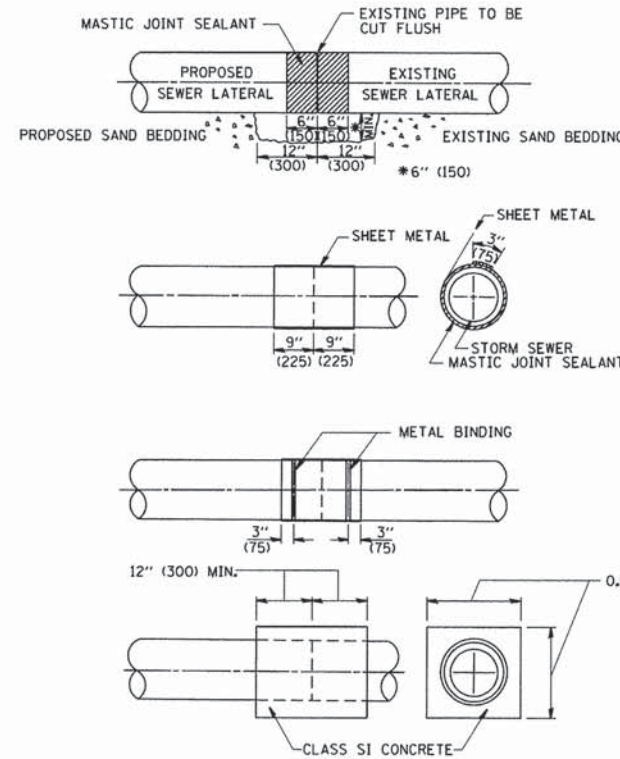
STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	11
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61B34	



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

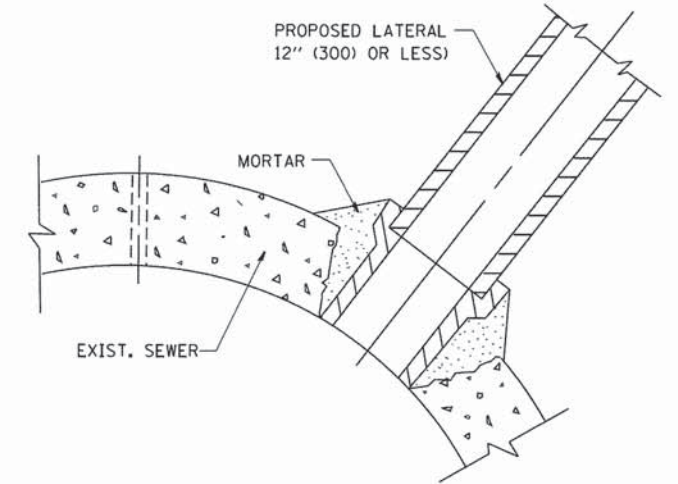


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" X 6" (300 X 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 I.I (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN. GHA #4755.018

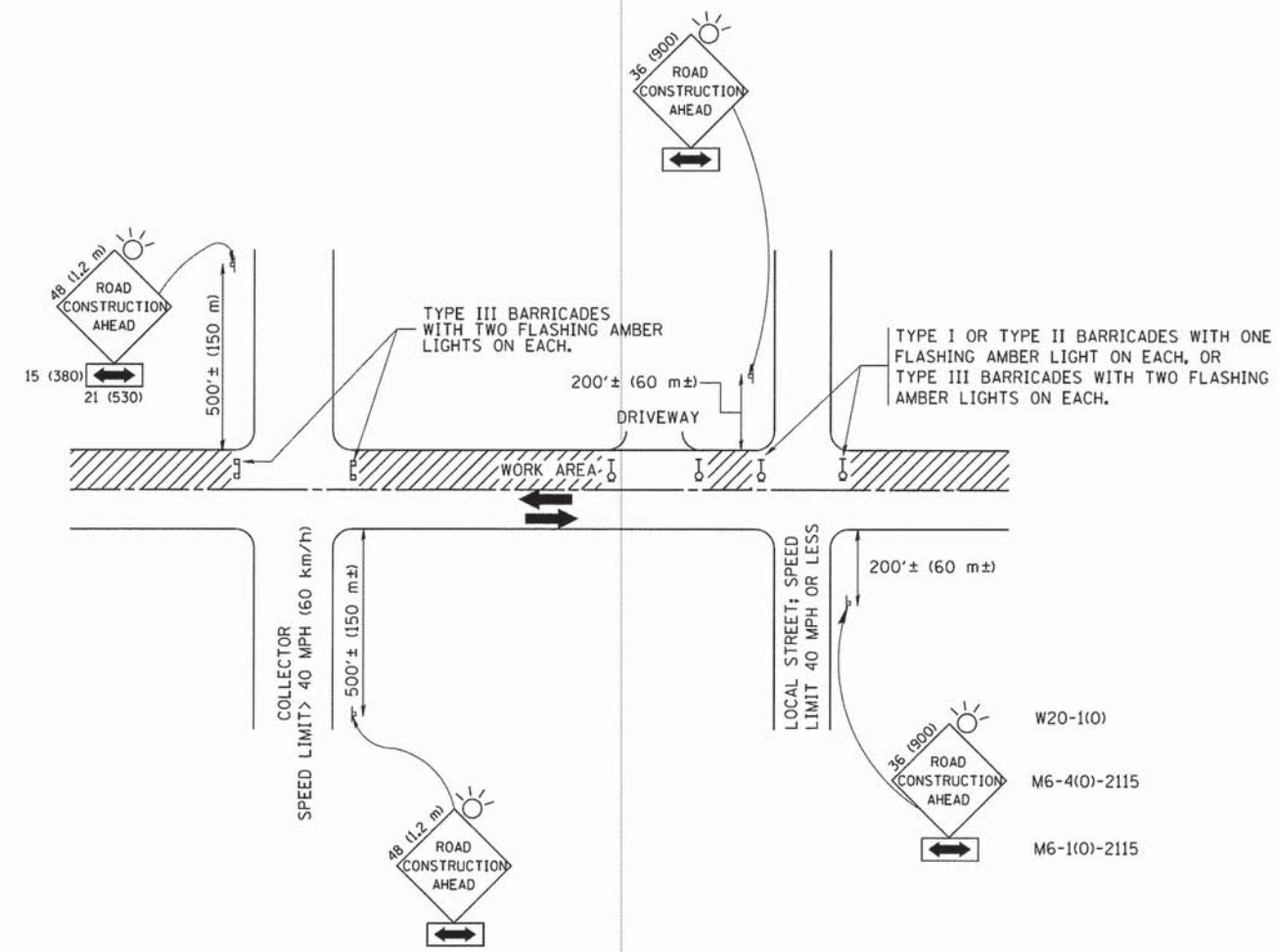
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Default	PLOT SCALE = 20.0000' / 1"	CHECKED -	REVISED - R. SHAH 10-25-94
	PLOT DATE = 1/29/2015	DATE - 07-25-90	REVISED - R. SHAH 06-12-96

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	12
BD500-01 (BD-7)			CONTRACT NO. 61B34	
ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown. GHA #4755.018

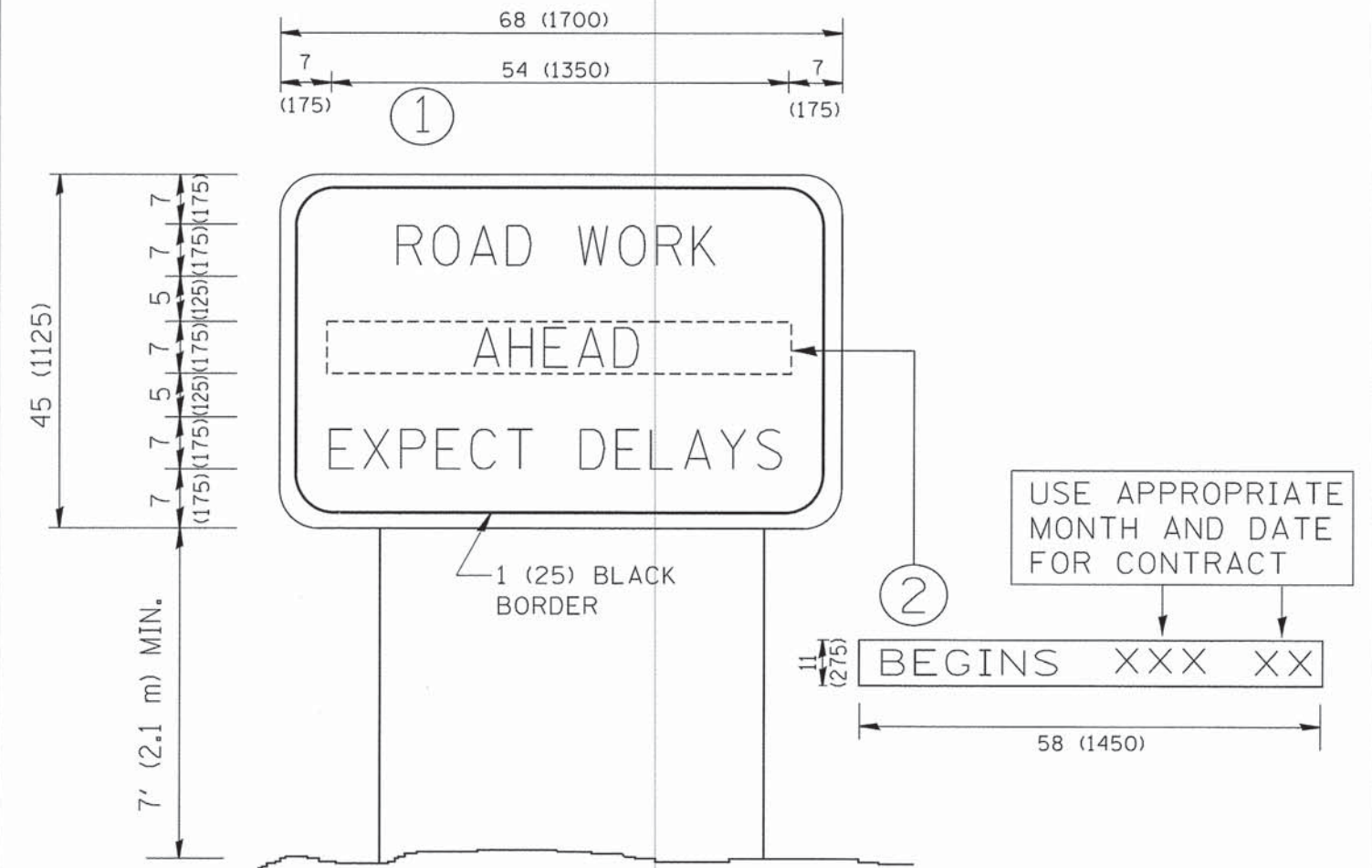
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	PLOT DATE = 1/29/2015	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	14
TC-10			CONTRACT NO. 61B34	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = MCoeman	DESIGNED -	REVISED - R. MIRS 09-15-97
Gr\4700\4755 Prospect Heights Projects\755.018 Willow Road Sidewalk\DDNs\Plans\Folder\DRAWN\Sheets\4755.018_01-Details.dgn		CHECKED -	REVISED - R. MIRS 12-11-97
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	PLOT DATE = 1/29/2015		REVISED - C. JUCIUS 01-31-07

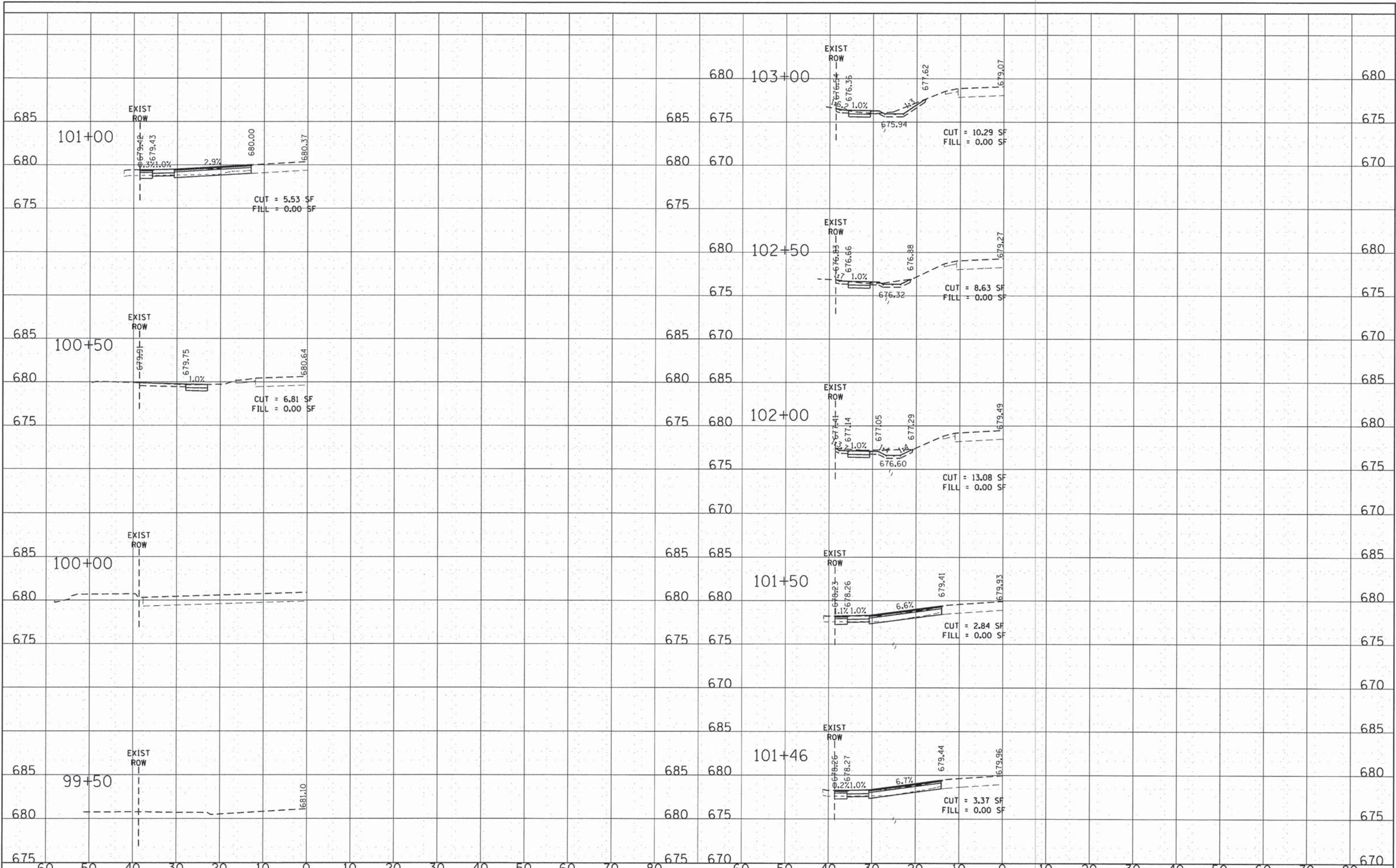
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	16
TC-22			CONTRACT NO. 61B34	
ILLINOIS FED. AID PROJECT				

GHA #4755.018



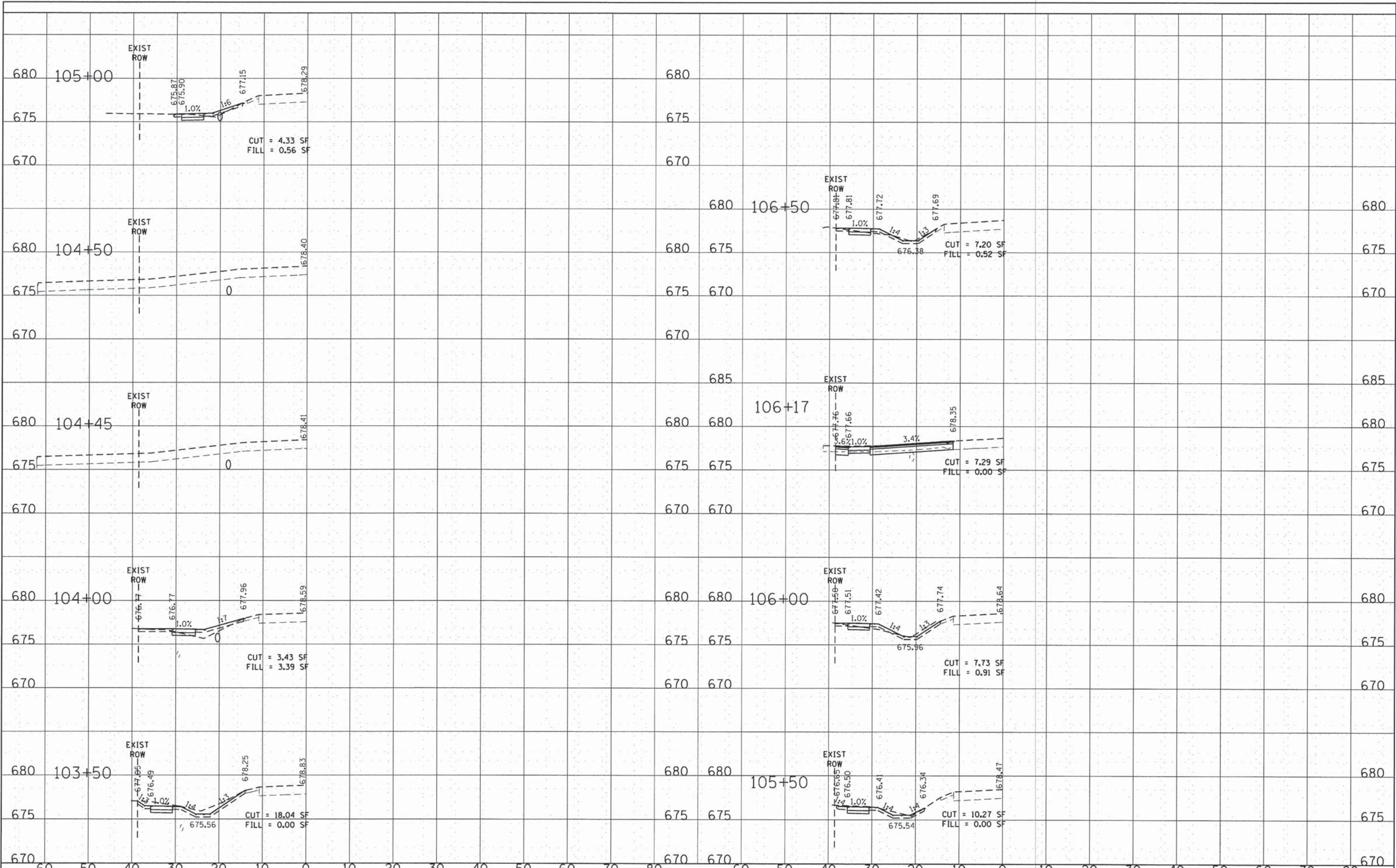
GHA GENWALT HAMILTON ASSOCIATES, INC.
 850 Forest Edge Drive • Vernon Hills, IL 60061
 TEL 847.478.9700 • FAX 847.478.9701

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DRAWN - MAC	REVISED -
CHECKED - KLB	REVISED -
DATE - 12/5/2014	REVISED -

CITY OF PROSPECT HEIGHTS, ILLINOIS
WILLOW ROAD SIDEWALK IMPROVEMENTS
RUSSET WOOD DRIVE TO SCHOENBECK ROAD

CROSS SECTIONS
WILLOW ROAD
 SCALE: H: 1"=10' V: 1"=5'
 STA. 99+50 TO STA. 103+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	17
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61B34	



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CHECKED - KLB	REVISED -
DATE - 12/5/2014	REVISED -

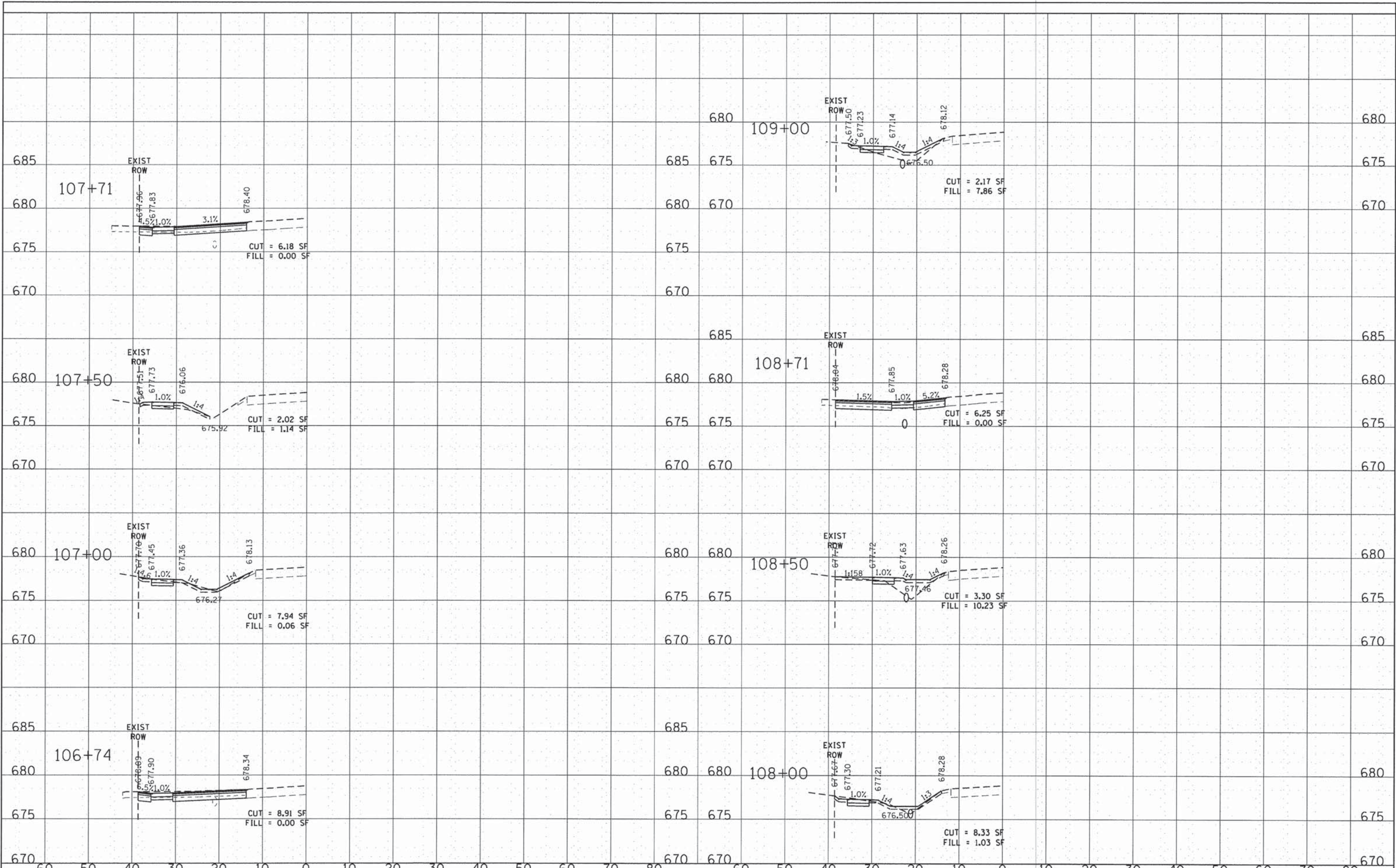
**CITY OF PROSPECT HEIGHTS, ILLINOIS
WILLOW ROAD SIDEWALK IMPROVEMENTS
RUSSET WOOD DRIVE TO SCHOENBECK ROAD**

**CROSS SECTIONS
WILLOW ROAD**

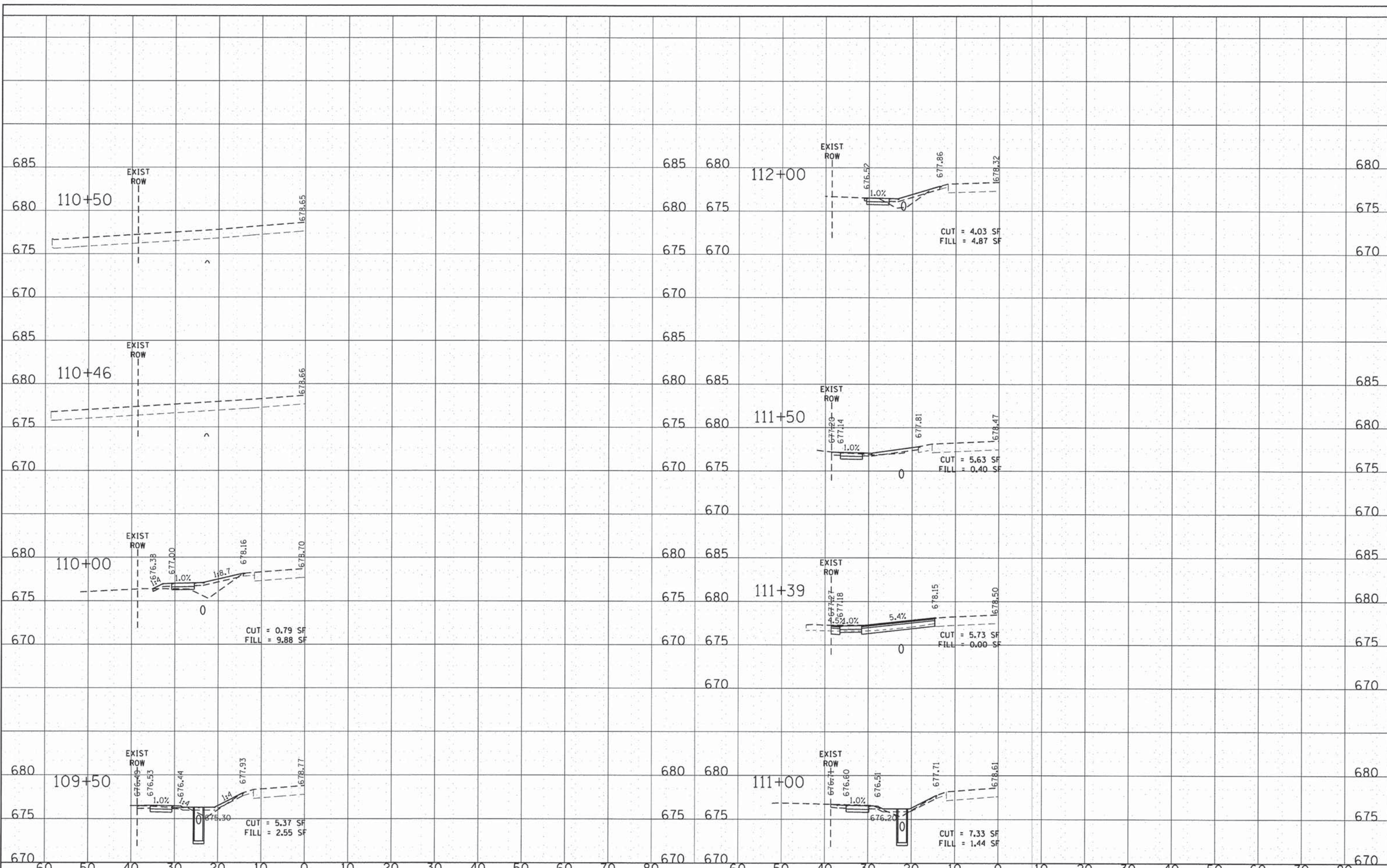
SCALE: H: 1"=10' V: 1"=5'

STA. 103+50 TO STA. 106+50

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	18
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 61B34	



<p>850 Forest Edge Drive • Vernon Hills, IL 60061 Tel: 847-478-9700 • Fax: 847-478-9701</p>	DESIGNED - MAC	REVISED -	CITY OF PROSPECT HEIGHTS, ILLINOIS WILLOW ROAD SIDEWALK IMPROVEMENTS RUSSET WOOD DRIVE TO SCHOENBECK ROAD	CROSS SECTIONS		F.A.U. RTE. 1289 SECTION 12-00051-00-SW COUNTY COOK TOTAL SHEETS 21 SHEET NO. 19 CONTRACT NO. 61B34 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
	DRAWN - MAC	REVISED -		WILLOW ROAD		
	CHECKED - KLB	REVISED -		SCALE: H: 1"=10' V: 1"=5'		
	DATE - 12/5/2015	REVISED -		STA. 106+74 TO STA. 109+00		



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DRAWN - MAC	REVISED -
CHECKED - KLB	REVISED -
DATE - 12/5/2014	REVISED -

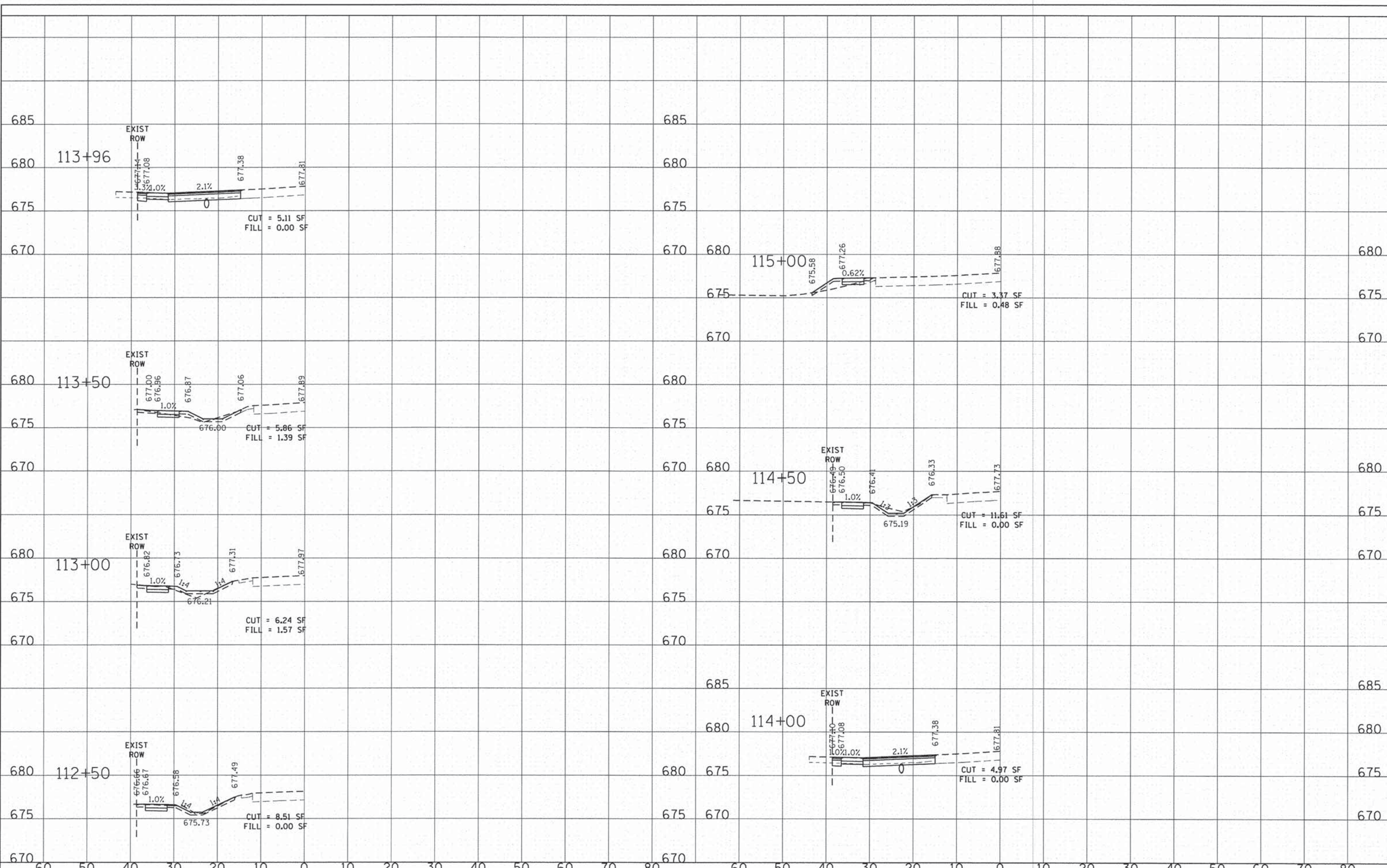
**CITY OF PROSPECT HEIGHTS, ILLINOIS
 WILLOW ROAD SIDEWALK IMPROVEMENTS
 RUSSET WOOD DRIVE TO SCHOENBECK ROAD**

**CROSS SECTIONS
 WILLOW ROAD**

SCALE: H: 1"=10' V: 1"=5'

STA. 109+50 TO STA. 112+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	20
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61B34	



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DATE - 12/5/2014	REVISED -

**CITY OF PROSPECT HEIGHTS, ILLINOIS
 WILLOW ROAD SIDEWALK IMPROVEMENTS
 RUSSET WOOD DRIVE TO SCHOENBECK ROAD**

**CROSS SECTIONS
 WILLOW ROAD**

SCALE: H: 1"=10' V: 1"=5'

STA. 112+50 TO STA. 115+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1289	12-00051-00-SW	COOK	21	21
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 61B34	