

SECTION 21, TOWNSHIP 36, RANGE 14
SECTION 28, TOWNSHIP 36, RANGE 14

06-12-2020 LETTING ITEM 163

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PLANS FOR PROPOSED FEDERAL AID HIGHWAY

F.A.H. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	1
STA.	TO STA.			
FED. ROAD DIST. NO. 1	MARKS	FED. AID PROJECT JHVC (074)		

CONTRACT #61G50

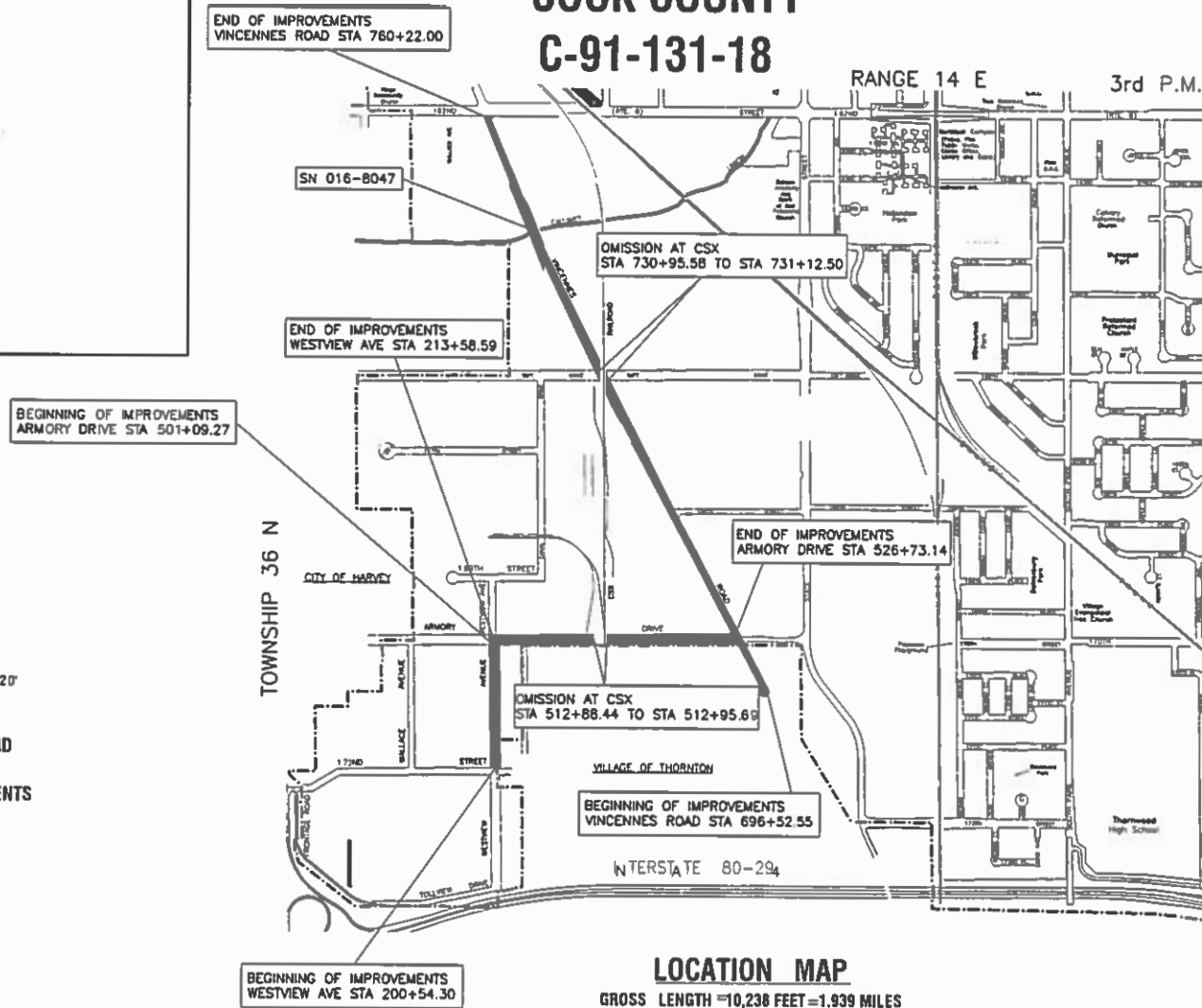
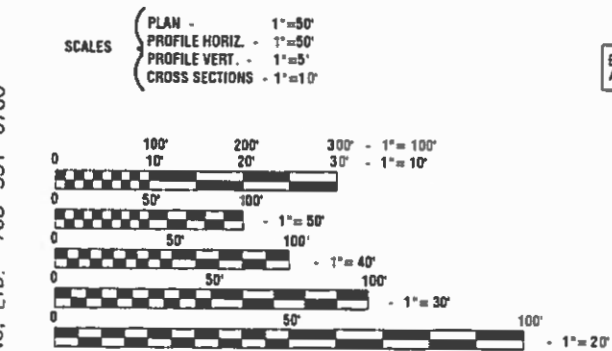
INDEX OF SHEETS
SEE SHEET NO. 2

HIGHWAY STANDARDS
SEE SHEET NO. 2

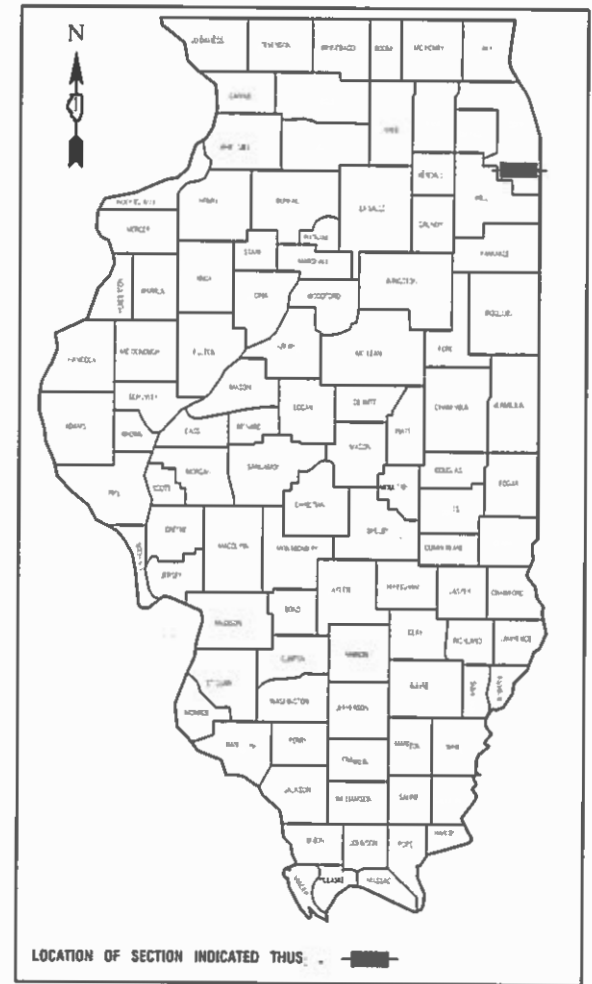
VINCENNES ROAD (FAU 3598) - US ROUTE 6 (FAP 0351) TO SOUTH OF ARMORY DRIVE (FAU 1010)
ARMORY DRIVE (FAU 1010) - WESTVIEW AVENUE (FAU 1010) TO VINCENNES ROAD (FAU 3598)
WESTVIEW AVENUE (FAU 1010) - 172 ND STREET (FAU 1010) TO ARMORY DRIVE (FAU 1010)
RESURFACING

PROJECT NO.: JHVC (074)
SECTION NO.: 17-00110-00-RS
VILLAGE of SOUTH HOLLAND
COOK COUNTY
C-91-131-18

VINCENNES ROAD	
2018 ADT -	3,000
2040 ADT -	3,700
POSTED SPEED LIMIT -	40 mph
DESIGN PERIOD -	20 YEARS
DESIGN SPEED LIMIT -	45 mph
STREET CLASSIFICATION -	MINOR COLLECTOR
ARMORY DRIVE	
2018 ADT -	7,700
2040 ADT -	9,500
POSTED SPEED LIMIT -	30 mph
DESIGN PERIOD -	20 YEARS
DESIGN SPEED LIMIT -	35 mph
STREET CLASSIFICATION -	MAJOR COLLECTOR
WESTVIEW AVENUE	
2018 ADT -	7,700
2040 ADT -	9,500
POSTED SPEED LIMIT -	30 mph
DESIGN PERIOD -	20 YEARS
DESIGN SPEED LIMIT -	35 mph
STREET CLASSIFICATION -	MAJOR COLLECTOR



LOCATION MAP
GROSS LENGTH = 10,238 FEET = 1.939 MILES
NET LENGTH = 10,213 FEET = 1.934 MILES



STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

Approved: *[Signature]* 2-27-2020
Village of South Holland

Passed: 4-7-2020
[Signature]
District Engineer of Local Roads & Streets

Released for Bid Based on Limited Review: APRIL 13, 2020
[Signature]
Regional Engineer

PRINTED BY THE AUTHORITY OF
THE STATE OF ILLINOIS

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:
[Signature]
04/01/2020

JOHN D. HILSON
REGISTERED PROFESSIONAL ENGINEER
STATE OF ILLINOIS
LICENSE EXPIRES 11/30/21

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E. SCHAUMBURG, IL
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 or 811

CONTRACT NO. 61G50

INDEX OF SHEETS

- 1 COVER SHEET
- 2 INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES, AND COMMITMENTS
- 3 SUMMARY OF QUANTITIES
- 4-5 TYPICAL CROSS SECTIONS
- 6-10 IMPROVEMENT PLAN
- 11-15 PAVEMENT MARKING PLAN
- 16-27 IDOT DISTRICT 1 STANDARD DETAILS

HIGHWAY STANDARDS

- 000001-07 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 442201-03 CLASS C AND D PATCHES
- 604001-05 FRAMES AND LIDS TYPE 1
- 606001-07 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 701006-05 OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
- 701501-06 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701502-09 URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
- 701606-10 URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701611-01 URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701701-10 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701901-08 TRAFFIC CONTROL DEVICES
- 780001-05 TYPICAL PAVEMENT MARKINGS
- 781001-04 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
- 886001-01 DETECTOR LOOP INSTALLATIONS

DISTRICT ONE DETAILS

- BD-01 DRIVEWAY DETAIL
- BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
- BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
- BD-24 CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
- BD-32 BUTT JOINT AND HMA TAPER DETAILS
- TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
- TC-11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
- TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- TC-16 SHORT TERM PAVEMENT MARKINGS LETTERS AND SYMBOLS
- TC-26 DRIVEWAY ENTRANCE SIGNING
- TS-05 STANDARD TRAFFIC SIGNAL DESIGN DETAILS
- TS-07 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

GENERAL NOTES

1. THE ROBINSON ENGINEERING, LTD. OFFICE (708-331-6700), AND THE PUBLIC WORKS DIRECTOR, AT THE VILLAGE OF SOUTH HOLLAND (GEORGE GUNKEL, 708-339-2323), SHALL BE NOTIFIED TWO (2) WORKING DAYS BEFORE CONSTRUCTION BEGINS.
2. BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 AND (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED)
3. UTILITIES INDICATED ON THE PLANS ARE PROVIDED FOR THE CONTRACTOR'S USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR CONTRACT BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF UTILITY INFORMATION.
4. THE THICKNESS OF HMA MIXTURE STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA SURFACE IS PLACED.
5. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SHALL BE REMOVED BY THE END OF EACH DAY BY THE CONTRACTOR AT THEIR EXPENSE.
6. CLASS D PATCHING QUANTITIES FOR THIS CONTRACT SHALL BE PERFORMED AT THE DIRECTION OF THE ENGINEER AFTER PAVEMENT MILLING.
7. EXISTING TREES ARE NOT AFFECTED BY THE LIMITS OF CONSTRUCTION. CONTRACTOR SHALL BE CAUTIOUS ON TREE PROTECTION DURING THE CONSTRUCTION. IF ANY DAMAGE OCCURS, CONTRACTOR SHALL REPLACE ANY TREES AT NO ADDITIONAL COST TO THE DEPARTMENT.
8. CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR FOR ARTERIALS AT KALPANA.KANNA-HOSADURGA@ILLINOIS.GOV AT LEAST 72 HOURS BEFORE BEGINNING WORK.
9. LONGITUDINAL JOINT SEALANT SHALL BE APPLIED UNDER THE HMA SURFACE LIFT ON VINNENNES ROAD, ARMORY DRIVE AND WESTVIEW AVE MAINLINE PAVEMENT.
10. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE CSX RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE CSX RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT-OF-WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05

COMMITMENTS

1. NO PAVEMENT PATCHING SHALL BE PERMITTED AFTER FRIDAY AT 3:00PM OF EACH AND EVERY WEEK AND NO HOLES WILL BE ALLOWED TO REMAIN OPEN OVERNIGHT OR OVER THE WEEKEND.
2. ALL EQUIPMENT SHALL BE REMOVED OFF THE VILLAGE STREETS DURING ALL HOLIDAY WEEKENDS AS COORDINATED WITH THE VILLAGE.

INFORMATION OF RAILROAD		
RAILROAD COMPANY	CONTACT	TELEPHONE NUMBER
CSX TRANSPORTATION, INC.	AMANDA J. DECESARE	513-853-1221

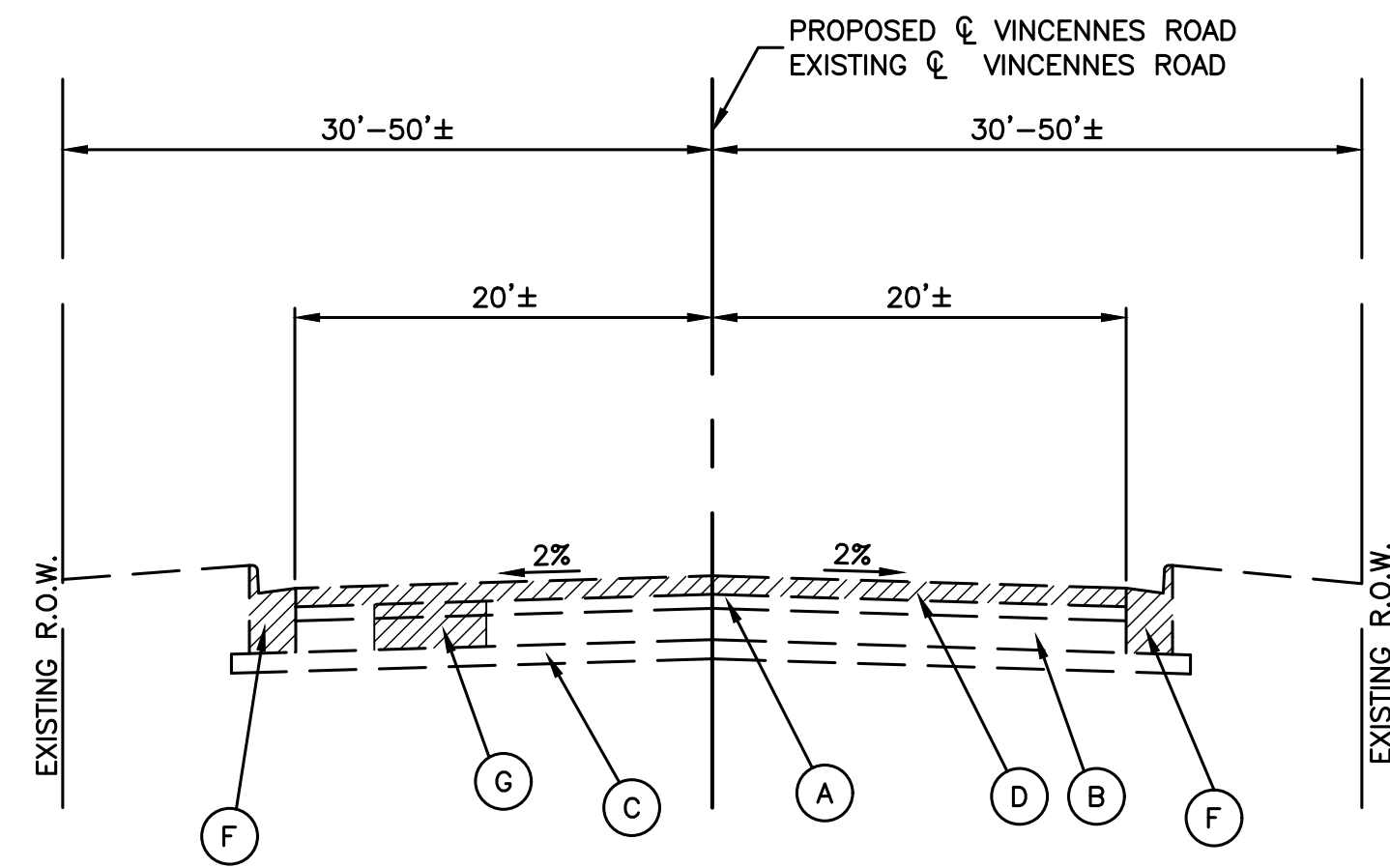
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		CHECKED -- WD	REVISED --		INDEX OF SHEETS & STATE STANDARDS & GENERAL NOTES					
	PLOT SCALE =	DRAWN -- RG/JJB	REVISED --		SCALE: NONE	SHEET NO. 2 OF 27 SHEETS	STA. TO STA.	CONTRACT NO. 61G50		
	PLOT DATE = 4/1/2020	CHECKED -- AG	REVISED --					FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT JHVC (074)

SUMMARY OF QUANTITIES					ROAD	SAFETY	SUMMARY OF QUANTITIES					ROAD	SAFETY		
S.I.	S.P.	CODE NO.	PAY ITEM	UNIT	QUANTITY	CONSTRUCTION	TYPE CODE	S.I.	S.P.	CODE NO.	PAY ITEM	UNIT	QUANTITY	CONSTRUCTION	TYPE CODE
						005	021							005	021
		21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	472	472				70300100	SHORT TERM PAVEMENT MARKING	FOOT	3,776		3,776
**		25200110	SODDING, SALT TOLERANT	SQ YD	472	472				70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1,247		1,247
**		40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	30,744	30,744		*		78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	527		527
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	146	146		*		78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	18,580		18,580
**		40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	5,740	5,740		*		78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	194		194
**		40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	5,103	5,103		*		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	519		519
**		42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	209	209		*		78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	316		316
		44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SQ YD	45,545	45,545		*		78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	220		220
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1,591	1,591				78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	366		366
**		44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	3,718	3,718		*	**	88600600	DETECTOR LOOP REPLACEMENT	FOOT	398	398	
**		44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	3	3			**	Z0004530	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8"	SQ YD	1,382	1,382	
**		44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	158	158			**	Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	154	154	
**		44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	167	167			**	Z0017700	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED	EACH	1	1	
**		44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	142	142			**	Z0030850	TEMPOARARY INFORMATION SIGNING	SQ FT	344	344	
**		60266600	VALVE BOXES TO BE ADJUSTED	EACH	7	7			**	Z0033700	LONGINTUDINAL JOINT SEALANT	FOOT	19,556	19,556	
		60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2	2			**	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1		1
**		60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	2,510	2,510									
**		60608562	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12	FOOT	1,208	1,208									
		67100100	MOBILIZATION	LSUM	1	1									
**		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	1									
**		70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	LSUM	1	1									
**		70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	LSUM	1	1									
**		70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	LSUM	1	1									
**		70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1									

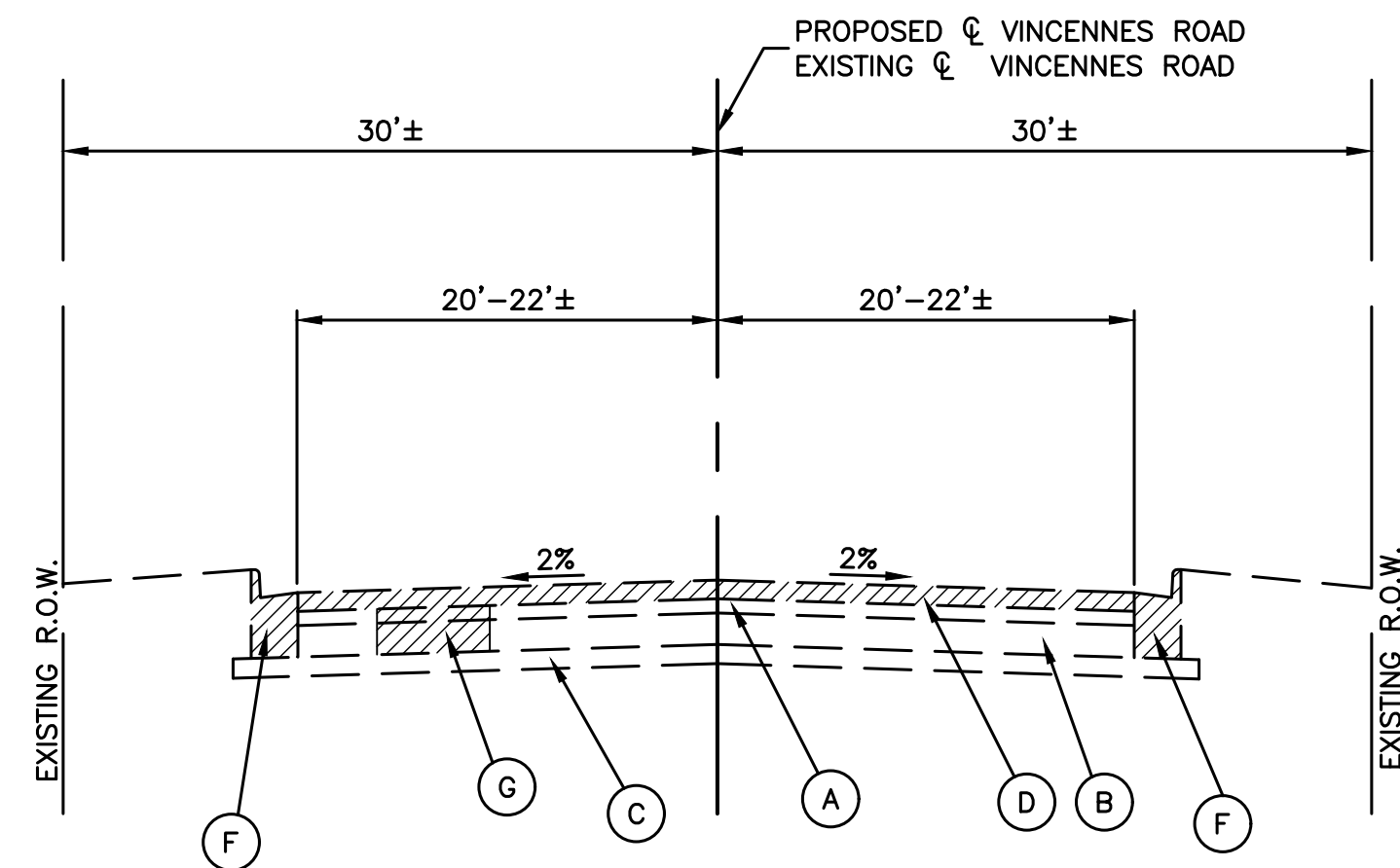
* - INDICATES SPECIALTY ITEMS
 ** - INDICATES SPECIAL PROVISION

* - INDICATES SPECIALTY ITEMS

FILE NAME = 18R0708-NOTE-01 - QTY01	USER NAME =	DESIGNED - JDH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING	F.A.U. RTE. 3598	SECTION 17-00110-00-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 3	
	PLOT SCALE =	CHECKED - WD	REVISED -		SCALE: NONE	SHEET NO. 3 OF 27 SHEETS	STA.	TO STA.	CONTRACT NO. 61G50		
	PLOT DATE = 4/8/2020	DRAWN - RG/JJB	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT JHVC (074)			
		CHECKED - AG	REVISED -								



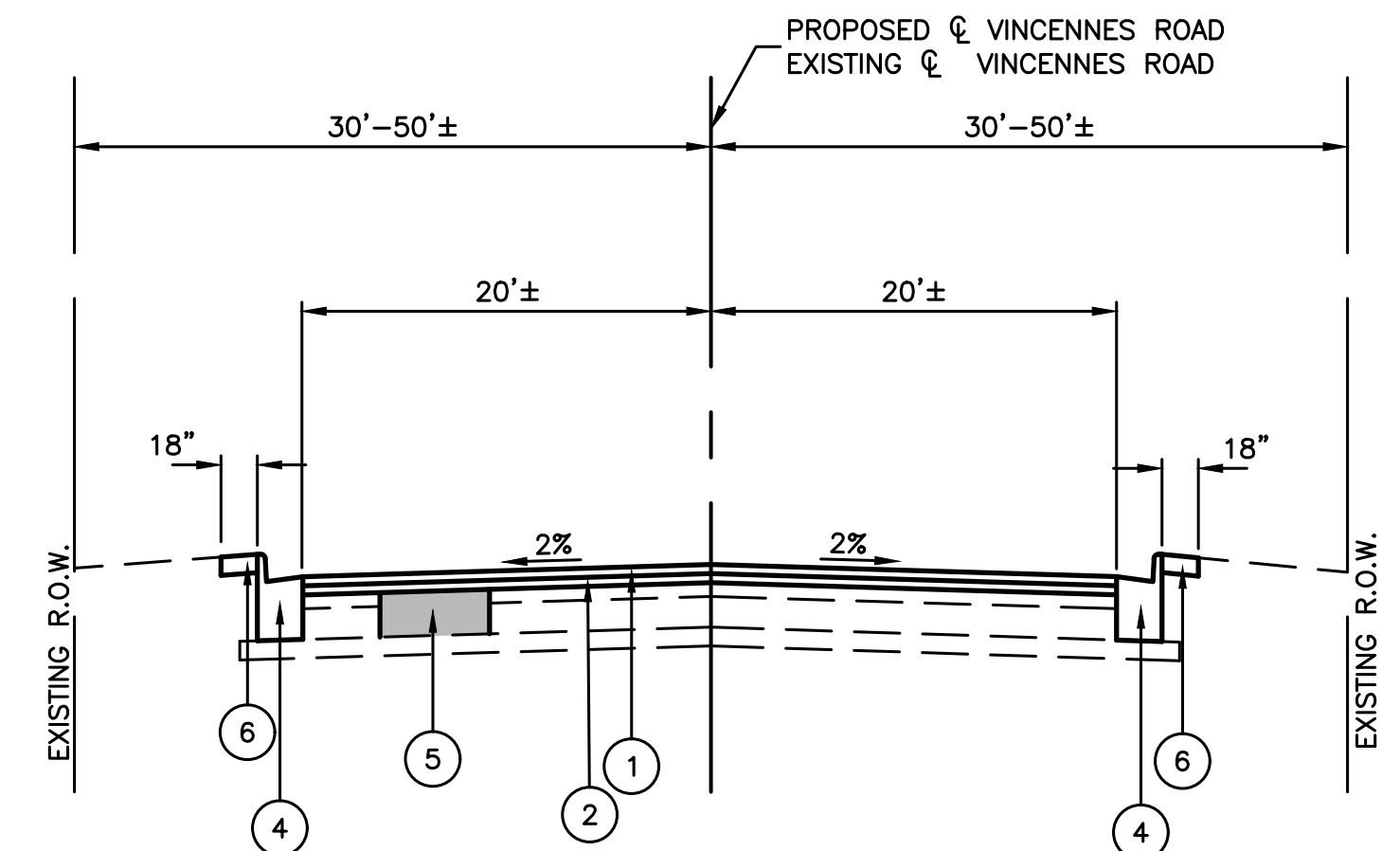
EXISTING TYPICAL SECTION
VINCENNES ROAD
STA 696+52.55 TO STA 756+00
OMISSION AT CSX RAILROAD
STA 730+95.58 TO STA 731+12.50



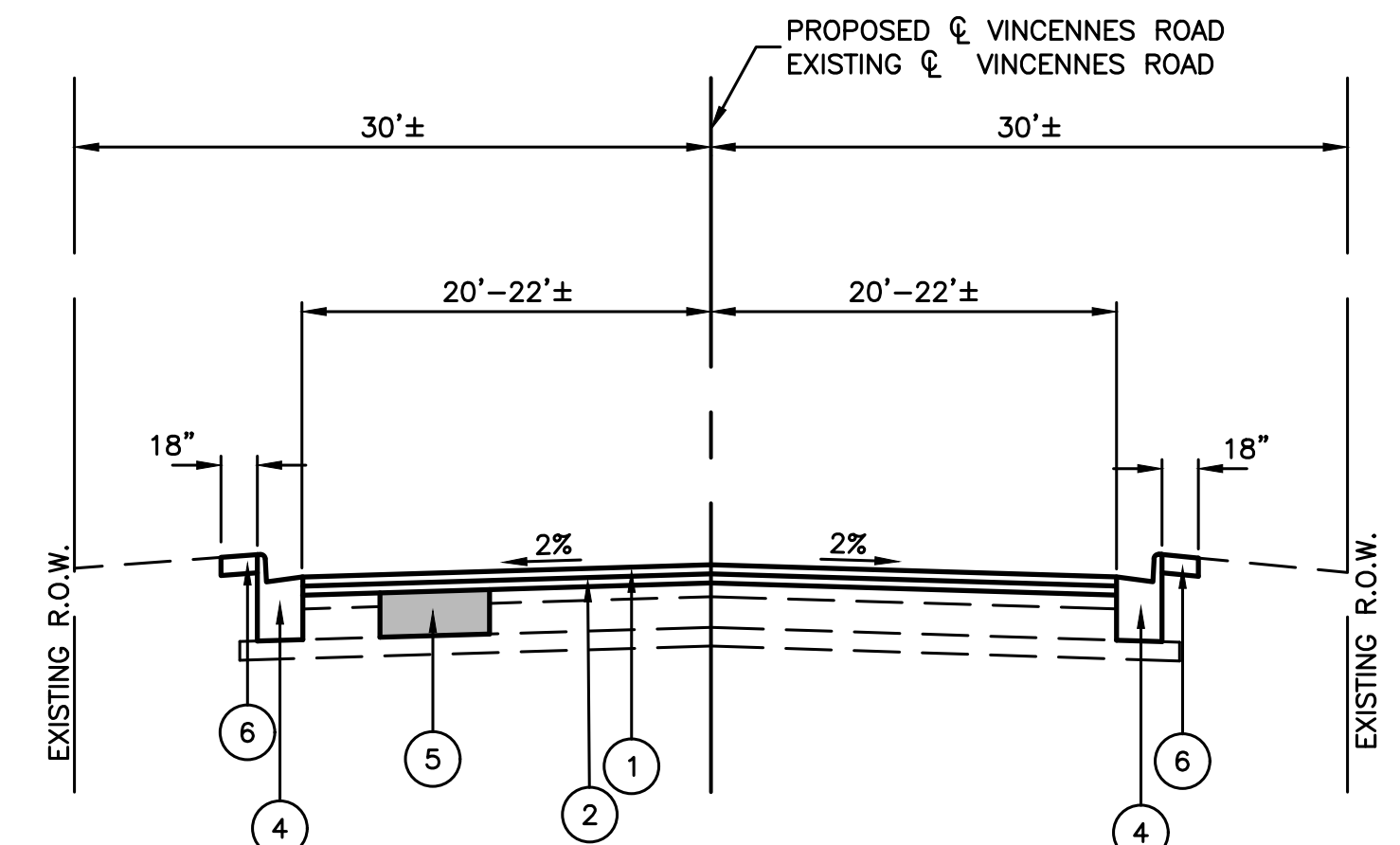
EXISTING TYPICAL SECTION
VINCENNES ROAD
STA 756+00 TO STA 758+04.48

LEGEND

- (A) EXISTING PAVEMENT, 5" TO 7"±
 - (B) EXISTING POZZOLANIC BASE, 10" ±
 - (C) EXISTING SUBBASE, 4" TO 8"±
 - (D) HOT-MIX ASPHALT SURFACE REMOVAL, 4"
 - (E) EXISTING CURB AND GUTTER, TYPE M-4.12
 - (F) EXISTING CURB AND GUTTER, TYPE B-6.24
 - (G) PAVEMENT REMOVAL FOR CLASS "D" PATCHES, 8 INCH
- ▨ ITEM TO BE REMOVED (AS DIRECTED BY ENGINEER)
- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 2"
 - (2) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4"
 - (3) COMBINATION CONCRETE CURB & GUTTER, TYPE M-4.14 (AS DIRECTED BY THE ENGINEER)
 - (4) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 (AS DIRECTED BY THE ENGINEER)
 - (5) CLASS D PATCH, 8 INCH (AS DIRECTED BY ENGINEER)
 - (6) TOPSOIL, FURNISH AND PLACE, 4" WITH SODDING, SALT TOLERANT



PROPOSED TYPICAL SECTION
VINCENNES ROAD
STA 696+52.55 TO STA 756+00
OMISSION AT CSX RAILROAD
STA 730+95.58 TO STA 731+12.50



PROPOSED TYPICAL SECTION
VINCENNES ROAD
STA 756+00 TO STA 758+04.48

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS
ROADWAY RESURFACING	
HOT MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 2"	4% @ 50 Gyr.
HOT MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4"	4% @ 50 Gyr.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19.0 MM): 8"	4% @ 70 Gyr.
HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8"	
HOT MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 2"	4% @ 50 Gyr.
HOT MIX ASPHALT BINDER COURSE, IL-19.0, N50, 6"	4% @ 50 Gyr.

NOTE:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.
2. FOR USE OF RECYCLED MATERIALS, SEE SPECIAL PROVISIONS.
3. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PC 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE PG 64-22 UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
4. THE EXISTING PAVEMENT SHALL BE MILLED PRIOR TO PATCHING OPERATIONS.

FILE NAME = 18R0706-TYPX-01 - IDOT P01

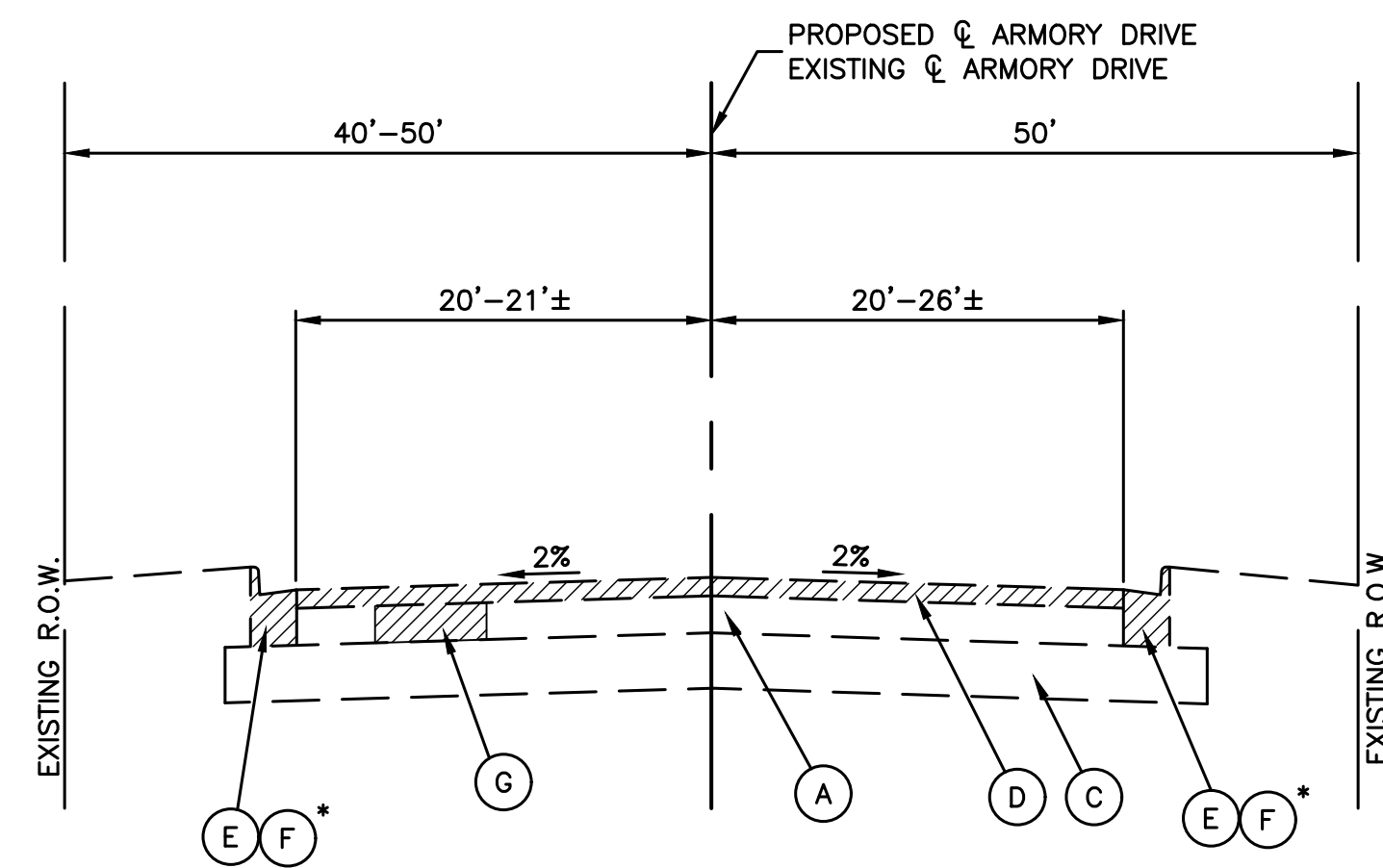
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	CHECKED -- WD	REVISED --
PLOT SCALE =	DRAWN -- MED	REVISED --
PLOT DATE = 4/8/2020	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

VINCENNES ROAD
VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING
TYPICAL CROSS SECTIONS

SCALE: SHEET NO. 4 OF 27 SHEETS STA. TO STA.

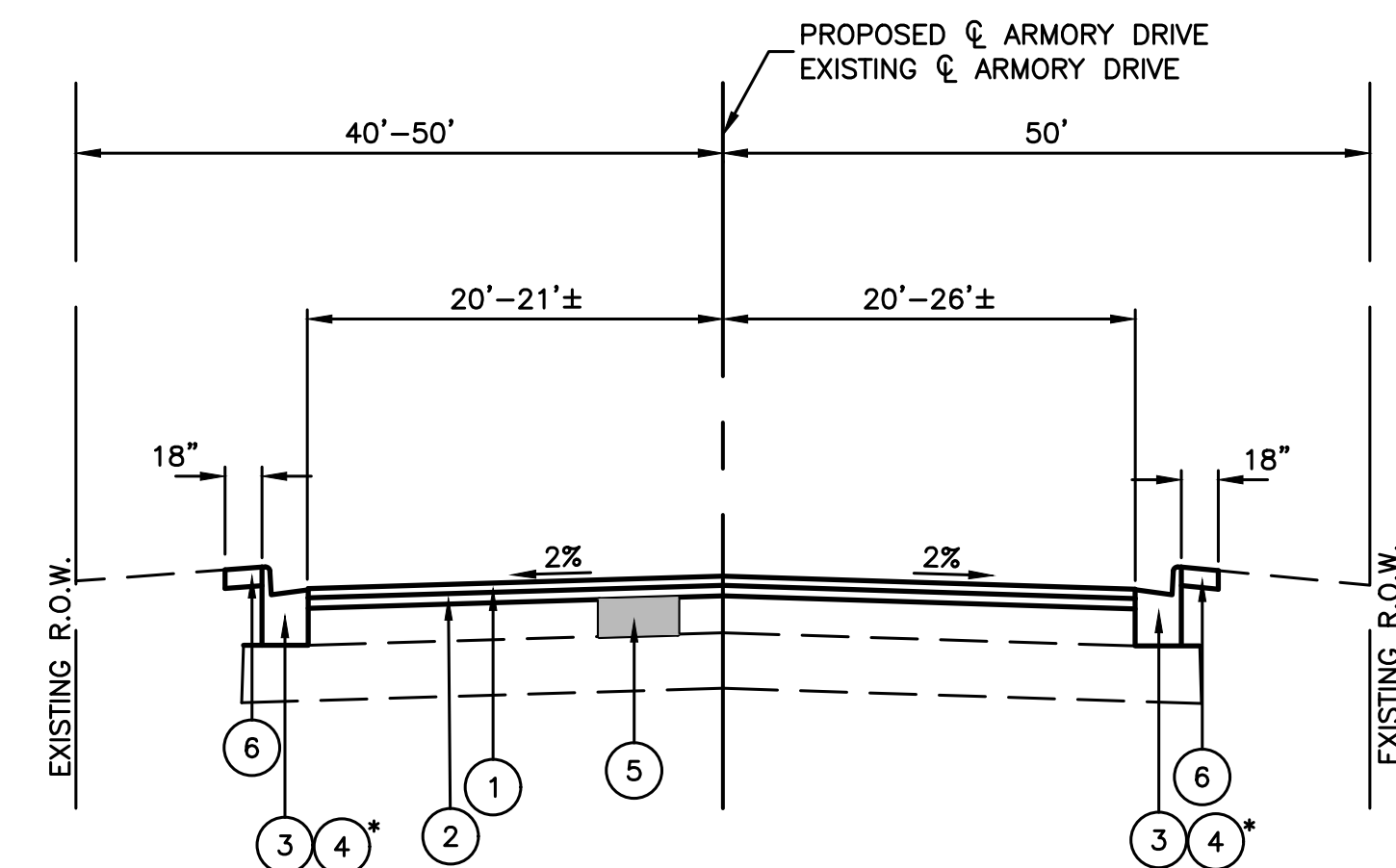
F.A.U. RTE. 3598	SECTION 17-00110-00-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 4
CONTRACT NO. 61G50			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)	



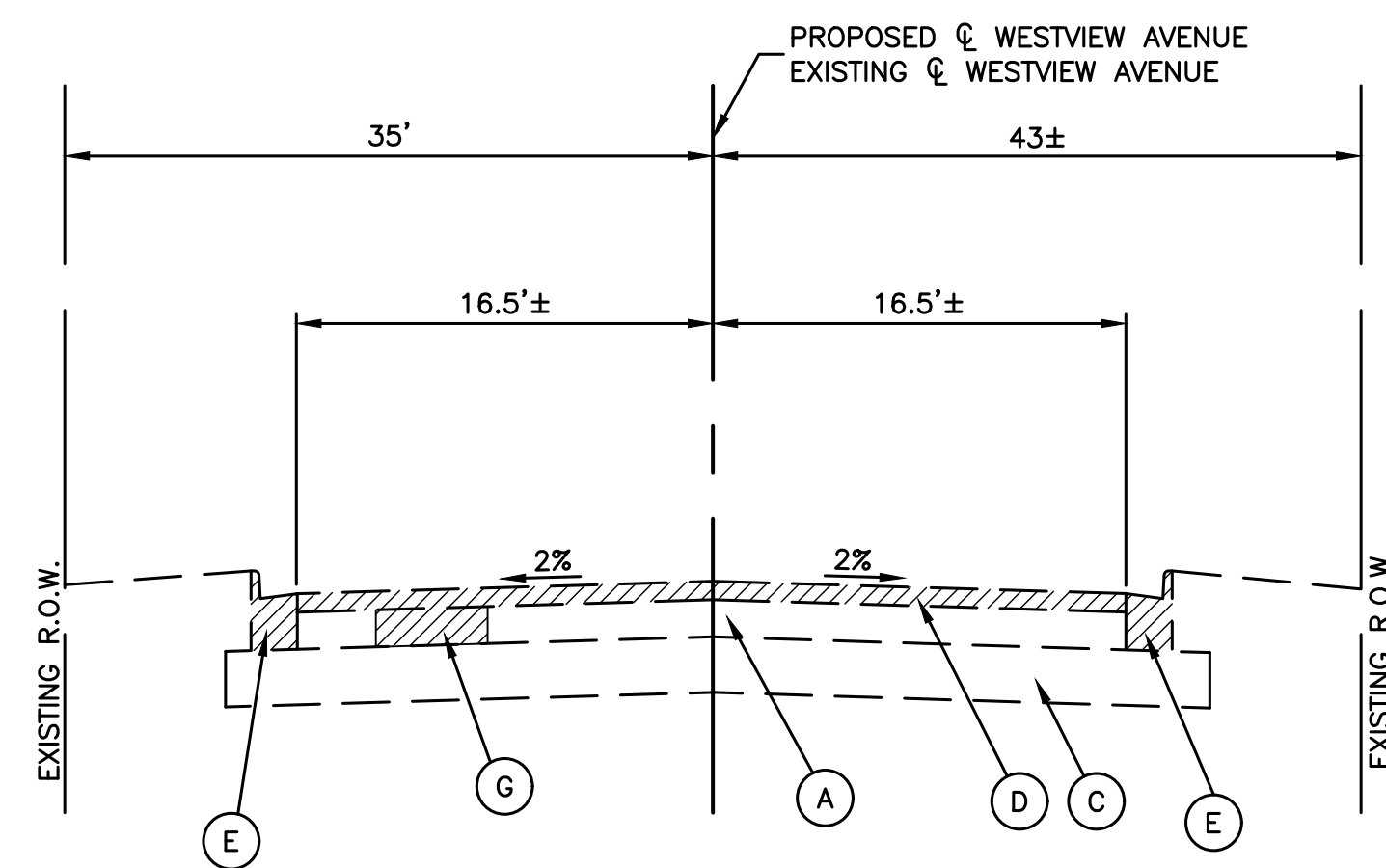
**EXISTING TYPICAL SECTION
ARMORY DRIVE**
STA 501+09.27 TO STA 525+42.85
OMISSION AT CSX RAILROAD
STA 512+88.44 TO STA 512+95.69
*TRANSITIONS FROM (E) TO (F) BETWEEN STATIONS
STATIONS 522+64 TO 522+79

LEGEND

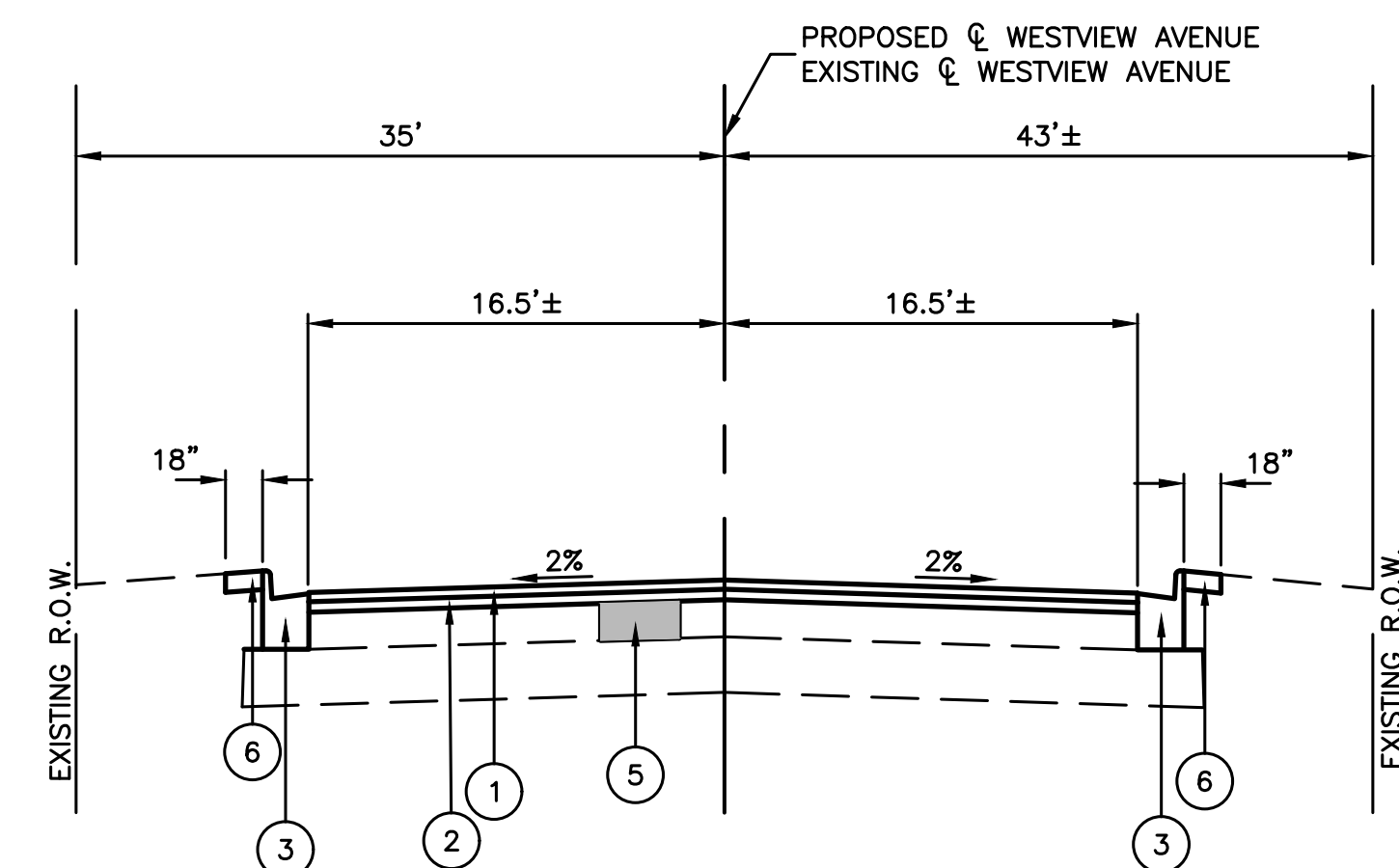
- (A) EXISTING PAVEMENT, 10" TO 12"±
- (B) EXISTING POZZOLANIC BASE, 10" ±
- (C) EXISTING SUBBASE, 0" TO 5"±
- (D) HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- (E) EXISTING CURB AND GUTTER, TYPE M-4.12
- (F) EXISTING CURB AND GUTTER, TYPE B-6.24
- (G) PAVEMENT REMOVAL FOR CLASS "D" PATCHES, 8 INCH
- ▨ ITEM TO BE REMOVED (AS DIRECTED BY ENGINEER)
- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 2"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4"
- (3) COMBINATION CONCRETE CURB & GUTTER, TYPE M-4.14 (AS DIRECTED BY THE ENGINEER)
- (4) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 (AS DIRECTED BY THE ENGINEER)
- (5) CLASS D PATCH, 8 INCH (AS DIRECTED BY ENGINEER)
- (6) TOPSOIL, FURNISH AND PLACE, 4" WITH SODDING, SALT TOLERANT



**PROPOSED TYPICAL SECTION
ARMORY DRIVE**
STA 501+09.27 TO STA 525+42.85
OMISSION AT CSX RAILROAD
STA 512+88.44 TO STA 512+95.69
*TRANSITIONS FROM (3) TO (4) BETWEEN STATIONS
STATIONS 522+64 TO 522+79



**EXISTING TYPICAL SECTION
WESTVIEW AVENUE**
STA 200+54.30 TO STA 213+58.59



**PROPOSED TYPICAL SECTION
WESTVIEW AVENUE**
STA 200+54.30 TO STA 213+58.59

FILE NAME = 18R0708-TYPX-01 - IDOT P01

USER NAME =	DESIGNED -- JDH	REVISED --
CHECKED -- WD	CHECKED -- AG	REVISED --
PLOT SCALE =	DRAWN -- MED	REVISED --
PLOT DATE = 4/8/2020	CHECKED -- AG	REVISED --

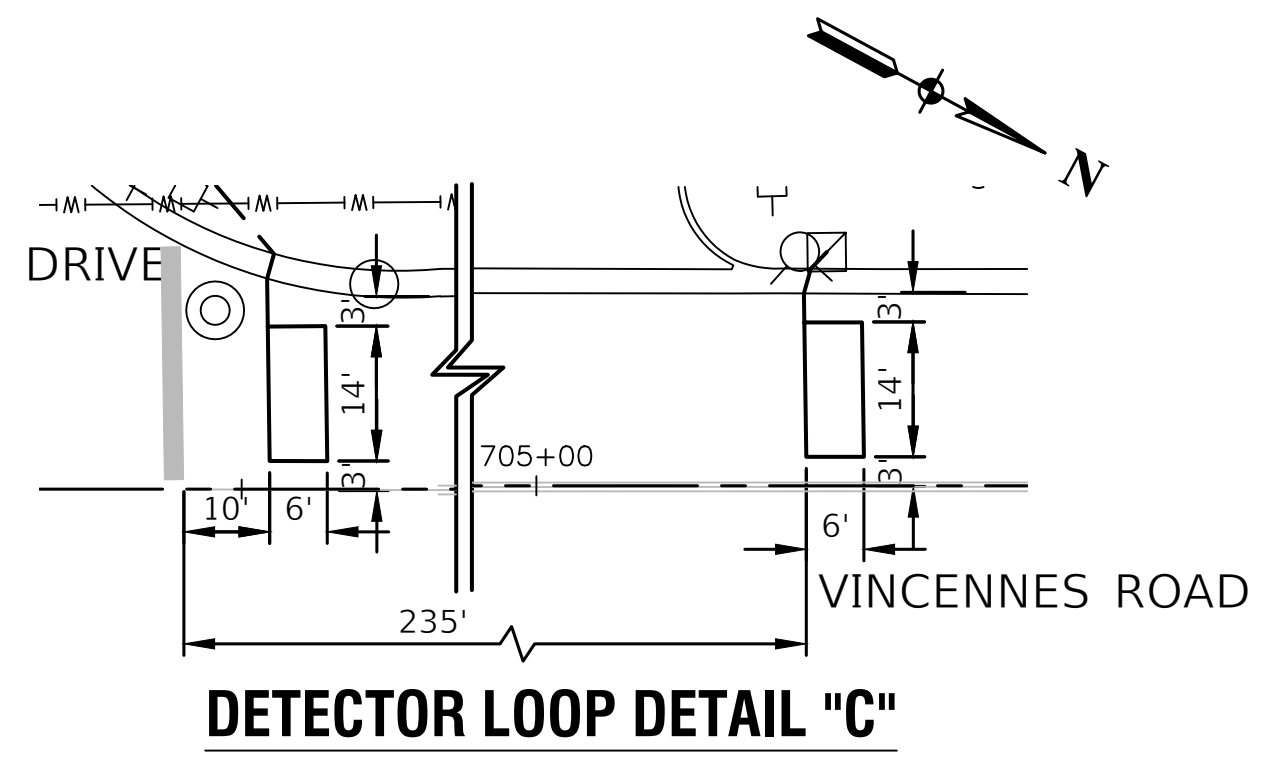
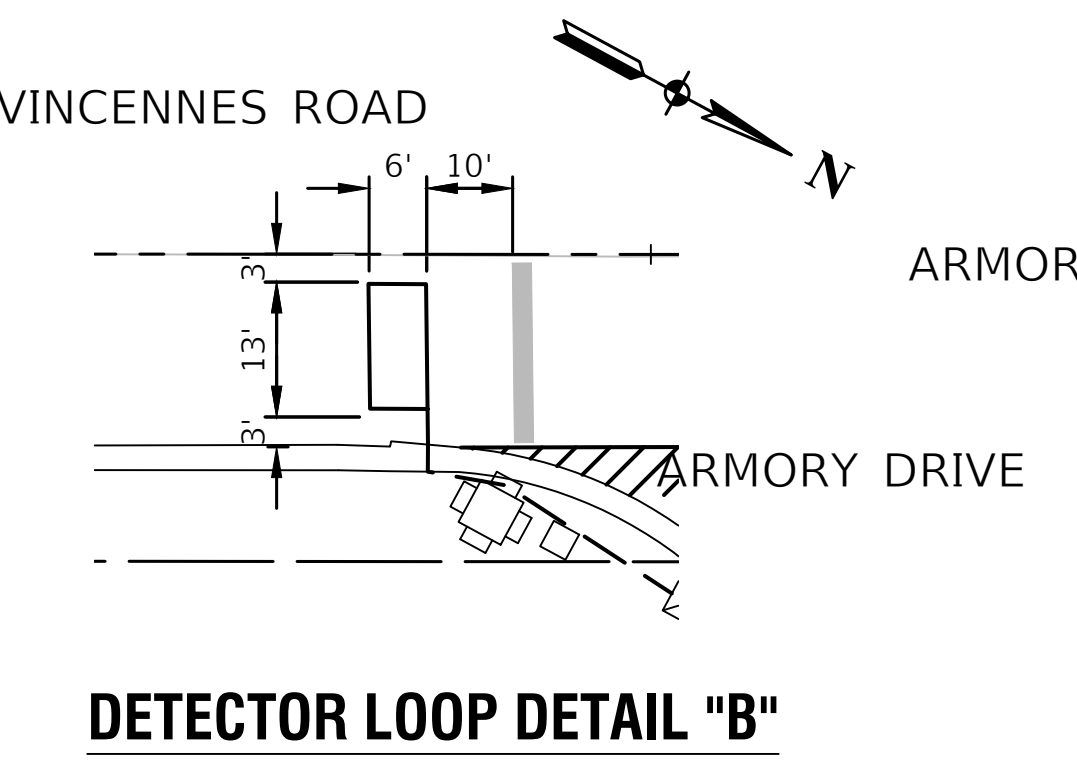
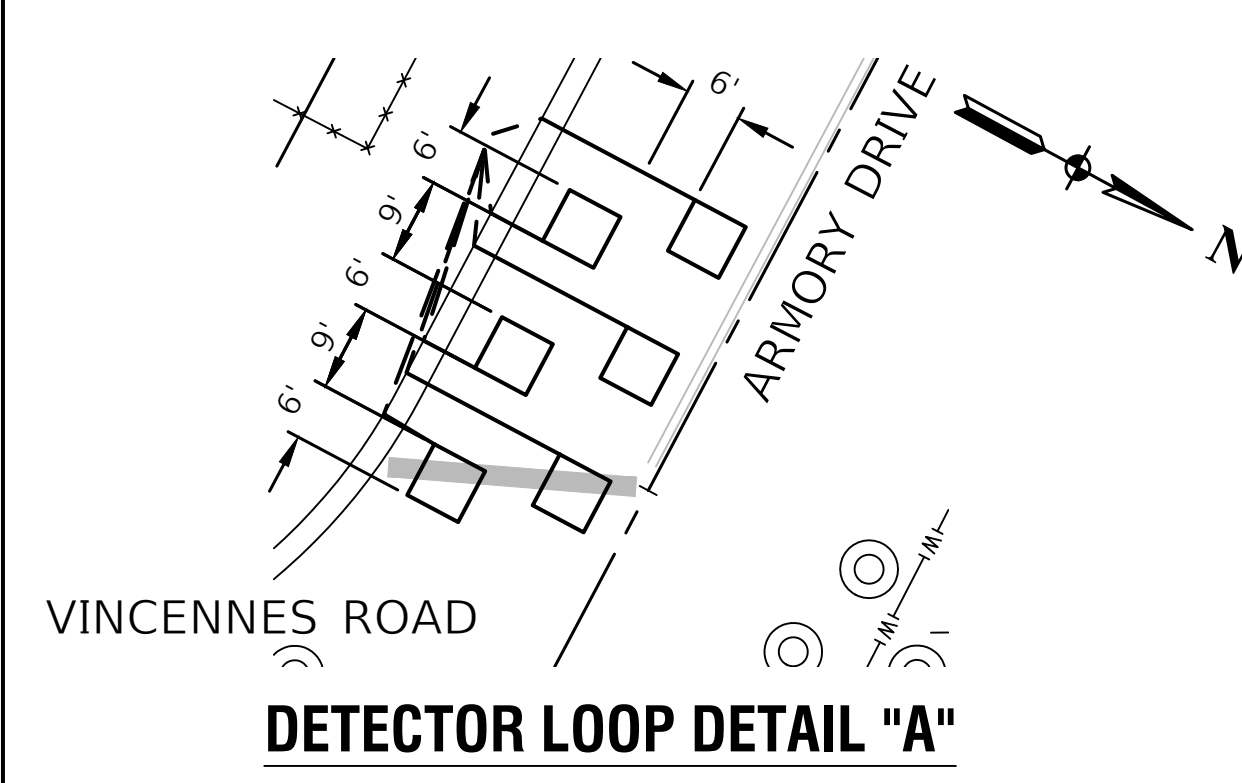
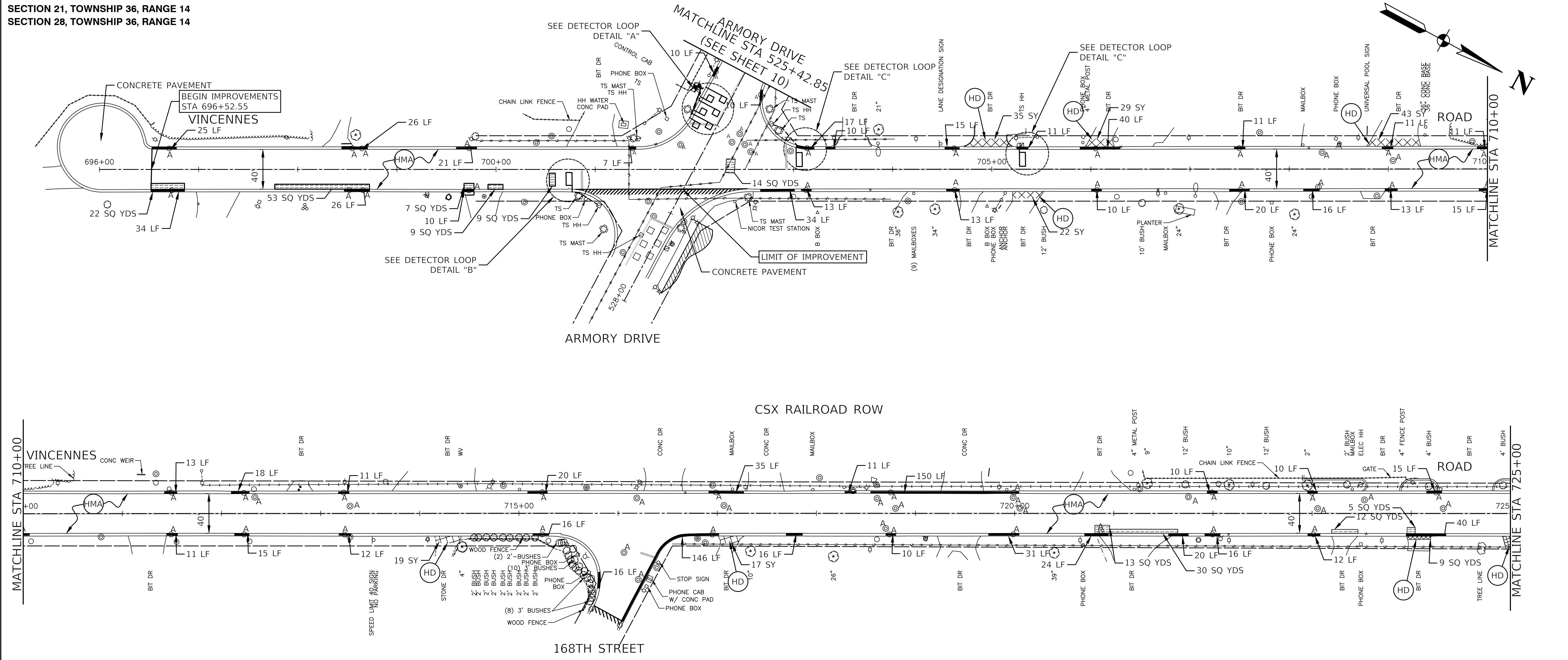
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARMORY DRIVE AND WESTVIEW AVENUE
VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING
TYPICAL CROSS SECTIONS**

SCALE: SHEET NO. 5 OF 27 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	5
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT JHVC (074)	

CONTRACT NO. 61G50



LEGEND

- CLASS D PATCHES, 8 INCH
- DRIVEWAY PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- VALVE BOXES TO BE ADJUSTED
- DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED
- DRAINAGE AND UTILITY STRUCTURES TO BE RECONSTRUCTED

- HOT-MIX ASPHALT SURFACE REMOVAL, 4" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50 - 2" HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 2 1/4"
- HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8" ON AGGREGATE BASE COURSE, TYPE B 4"
- PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH ON AGGREGATE BASE COURSE, TYPE B 4"

NOTES

1. CLASS D PATCHES, 8 INCH SHALL BE AS SHOWN ON THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
2. DRIVEWAY REMOVAL AND REPLACEMENT SHALL BE COMPLETED AS DIRECTED BY THE ENGINEER.
3. ANY TOPSOIL EXCAVATED DURING CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE STOCK PILED ON-SITE AND RE-USED AS FILL IN THE RESTORATION AREA. THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONTRACT PAY ITEMS.
4. THE TYPE OF COMBINATION CURB AND GUTTER ON THIS SHEET IS B-6.24.

FILE NAME = 18R0706-PLAN-01 - IDOT P01

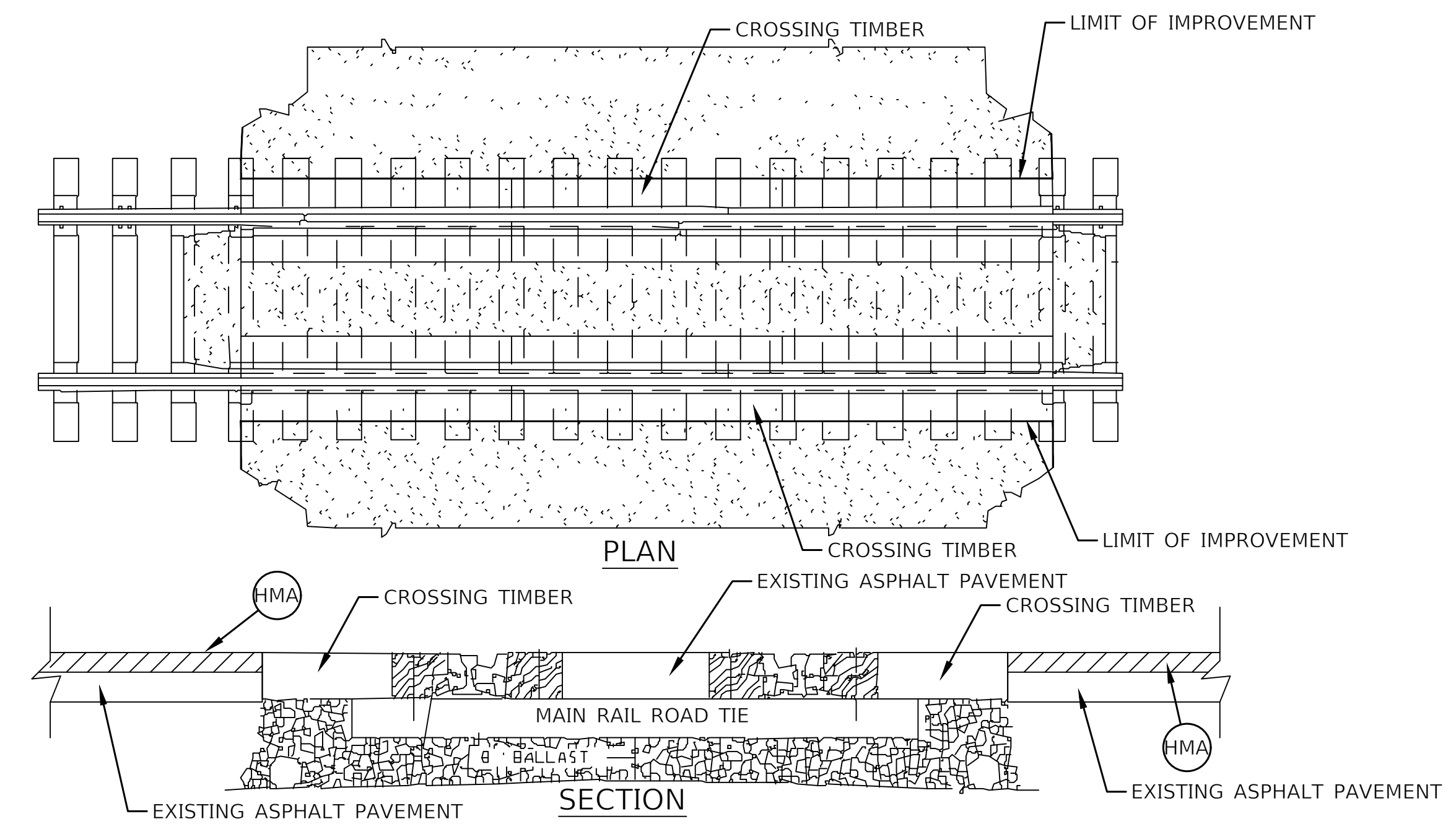
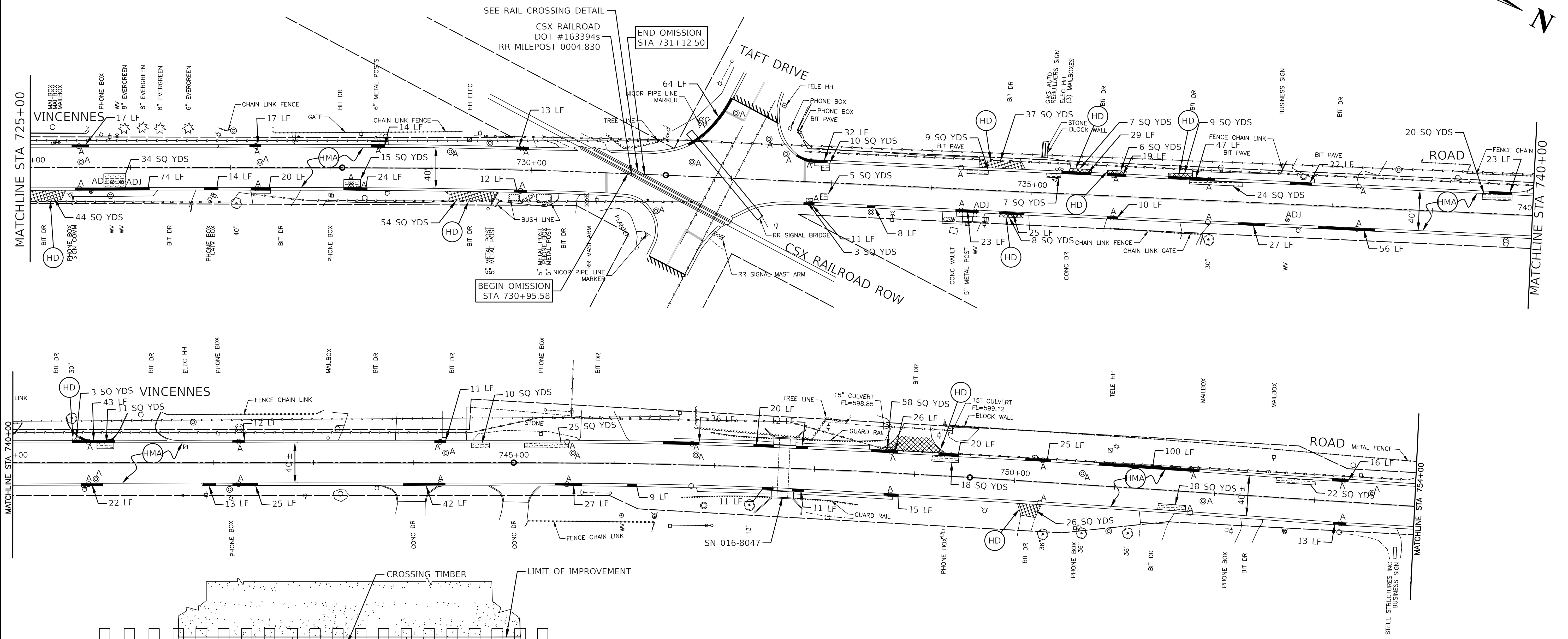
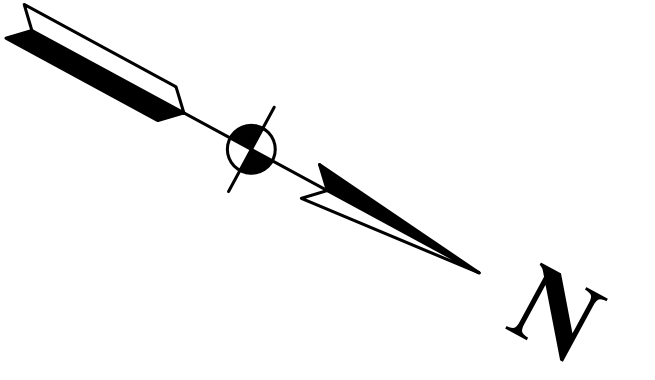
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CHECKED -- WD	REVISED --	
PLOT SCALE =	DRAWN -- MED	REVISED --
PLOT DATE = 4/1/2020	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING
IMPROVEMENT PLAN

SCALE: 1"=50' SHEET NO. 6 OF 27 SHEETS STA. TO STA.

F.A.U. RTE. 3598	SECTION 17-00110-00-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 6
CONTRACT NO. 61G50			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)	



RAIL ROAD CROSSING DETAIL

LEGEND

- CLASS D PATCHES, 8 INCH
- DRIVEWAY PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- VALVE BOXES TO BE ADJUSTED
- DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED
- DRAINAGE AND UTILITY STRUCTURES TO BE RECONSTRUCTED

- HOT-MIX ASPHALT SURFACE REMOVAL, 4" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50 - 2" HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 2 1/4"
- HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8" ON AGGREGATE BASE COURSE, TYPE B 4"
- PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH ON AGGREGATE BASE COURSE, TYPE B 4"

NOTES

1. CLASS D PATCHES, 8 INCH SHALL BE AS SHOWN ON THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
2. DRIVEWAY REMOVAL AND REPLACEMENT SHALL BE COMPLETED AS DIRECTED BY THE ENGINEER.
3. ANY TOPSOIL EXCAVATED DURING CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE STOCK PILED ON-SITE AND RE-USED AS FILL IN THE RESTORATION AREAS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONTRACT PAY ITEMS.
4. THE TYPE OF COMBINATION CURB AND GUTTER ON THIS SHEET IS B-6.24.

FILE NAME = 18R0706-PLAN-01 - IDOT P01 (2)

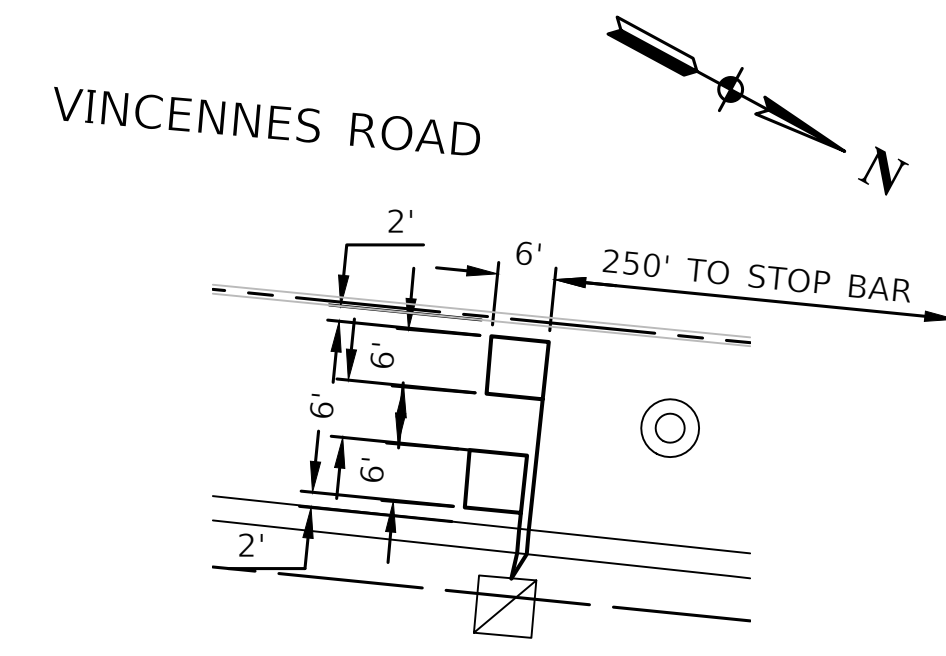
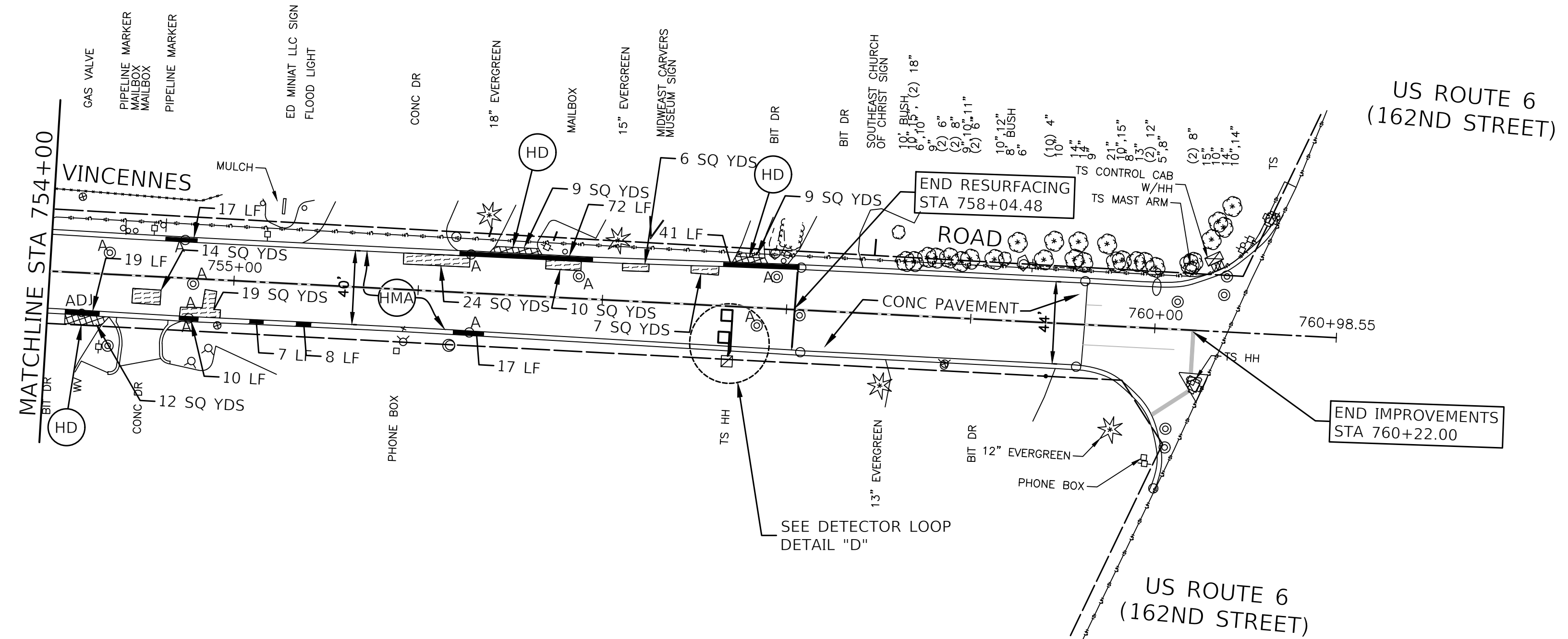
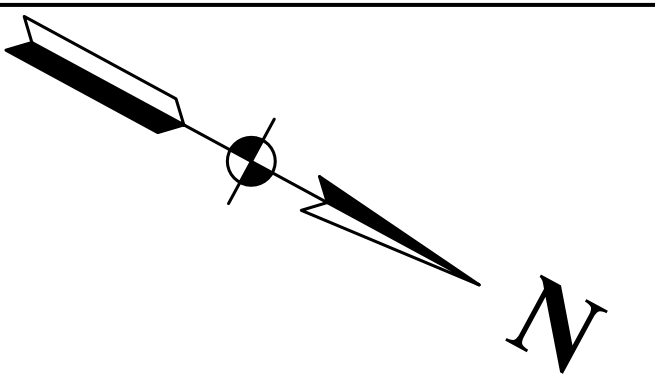
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PLOT DATE = 4/1/2020	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

VINCENNES ROAD
VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING
IMPROVEMENT PLAN

F.A.U. RTE. 3598	SECTION 17-00110-00-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 7
CONTRACT NO. 61G50				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)				

SCALE: 1"=50' SHEET NO. 7 OF 27 SHEETS STA. TO STA.



DETECTOR LOOP DETAIL "D"

LEGEND

- CLASS D PATCHES, 8 INCH
- DRIVEWAY PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- ADJ VALVE BOXES TO BE ADJUSTED
- A DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED
- REC DRAINAGE AND UTILITY STRUCTURES TO BE RECONSTRUCTED

- HMA HOT-MIX ASPHALT SURFACE REMOVAL, 4" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50 - 2" HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 2 1/4"
- HD HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8" ON AGGREGATE BASE COURSE, TYPE B 4"
- CD PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH ON AGGREGATE BASE COURSE, TYPE B 4"

NOTES

1. CLASS D PATCHES, 8 INCH SHALL BE AS SHOWN ON THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
2. DRIVEWAY REMOVAL AND REPLACEMENT SHALL BE COMPLETED A DIRECTED BY THE ENGINEER.
3. ANY TOPSOIL EXCAVATED DURING CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE STOCK PILED ON-SITE AND RE-USED AS FILL IN THE RESTORATION AREAS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONTRACT PAY ITEMS.
4. THE TYPE OF COMBINATION CURB AND GUTTER ON THIS SHEET IS B-6.24.

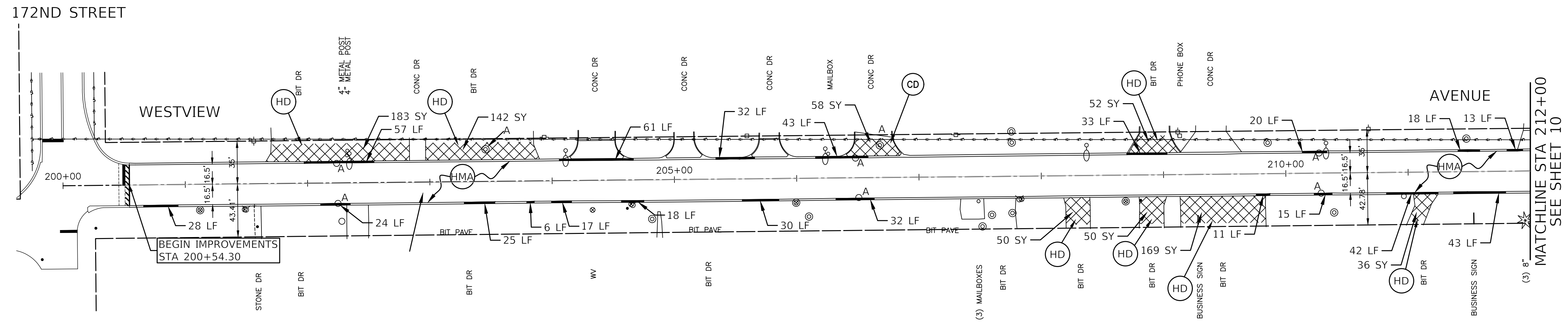
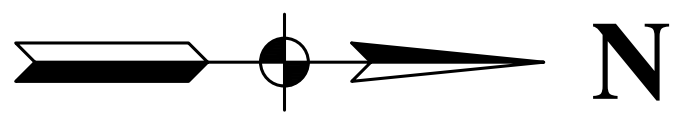
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	PLOT SCALE =	DRAWN -- MED	REVISED --
	PLOT DATE = 4/1/2020	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**VINCENNES ROAD
VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING
IMPROVEMENT PLAN**

F.A.U. RTE. 3598	SECTION 17-00110-00-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 8
CONTRACT NO. 61G50				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)				

SCALE: 1"=50' SHEET NO. 8 OF 27 SHEETS STA. TO STA.



LEGEND

- CLASS D PATCHES, 8 INCH
- DRIVEWAY PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- ADJ VALVE BOXES TO BE ADJUSTED
- A DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED
- REC DRAINAGE AND UTILITY STRUCTURES TO BE RECONSTRUCTED
- HMA HOT-MIX ASPHALT SURFACE REMOVAL, 4" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50 - 2" HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 2 1/4"
- HD HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8" ON AGGREGATE BASE COURSE, TYPE B 4"
- CD PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH ON AGGREGATE BASE COURSE, TYPE B 4"

NOTES

1. CLASS D PATCHES, 8 INCH SHALL BE AS SHOWN ON THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
2. DRIVEWAY REMOVAL AND REPLACEMENT SHALL BE COMPLETED AS DIRECTED BY THE ENGINEER.
3. ANY TOPSOIL EXCAVATED DURING CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE STOCK PILED ON-SITE AND RE-USED AS FILL IN THE RESTORATION AREAS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONTRACT PAY ITEMS.
4. THE TYPE OF COMBINATION CURB AND GUTTER ON THIS SHEET IS M-4.12.

FILE NAME = 18R0708-PLAN-01 - IDOT P01 (2)

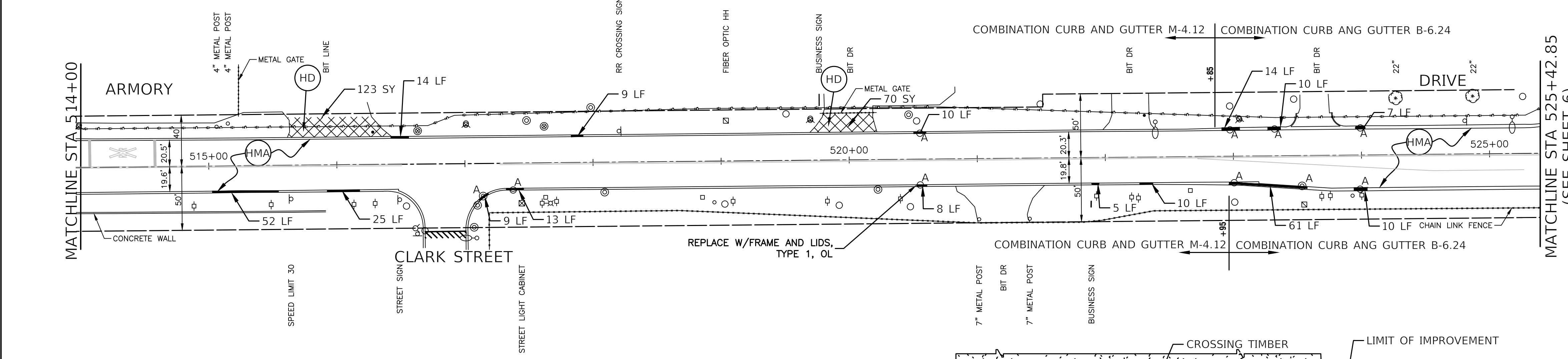
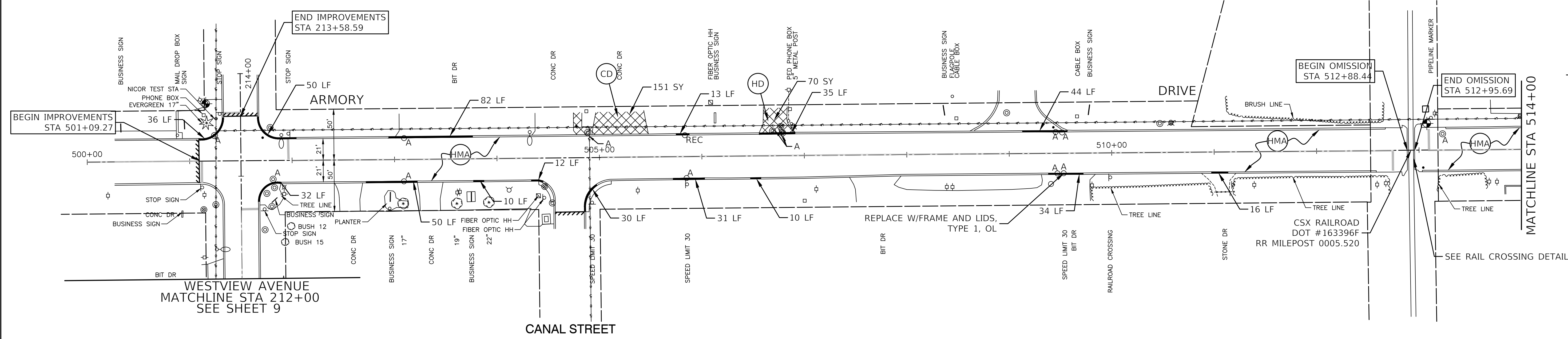
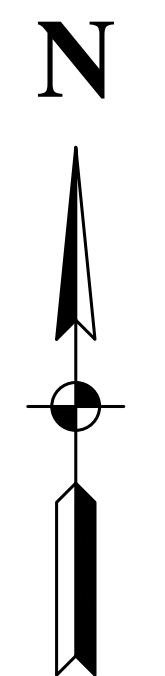
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CHECKED -- WD	REVISED --	
PLOT SCALE =	DRAWN -- MED	REVISED --
PLOT DATE = 4/1/2020	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WESTVIEW AVENUE
VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING
IMPROVEMENT PLAN**

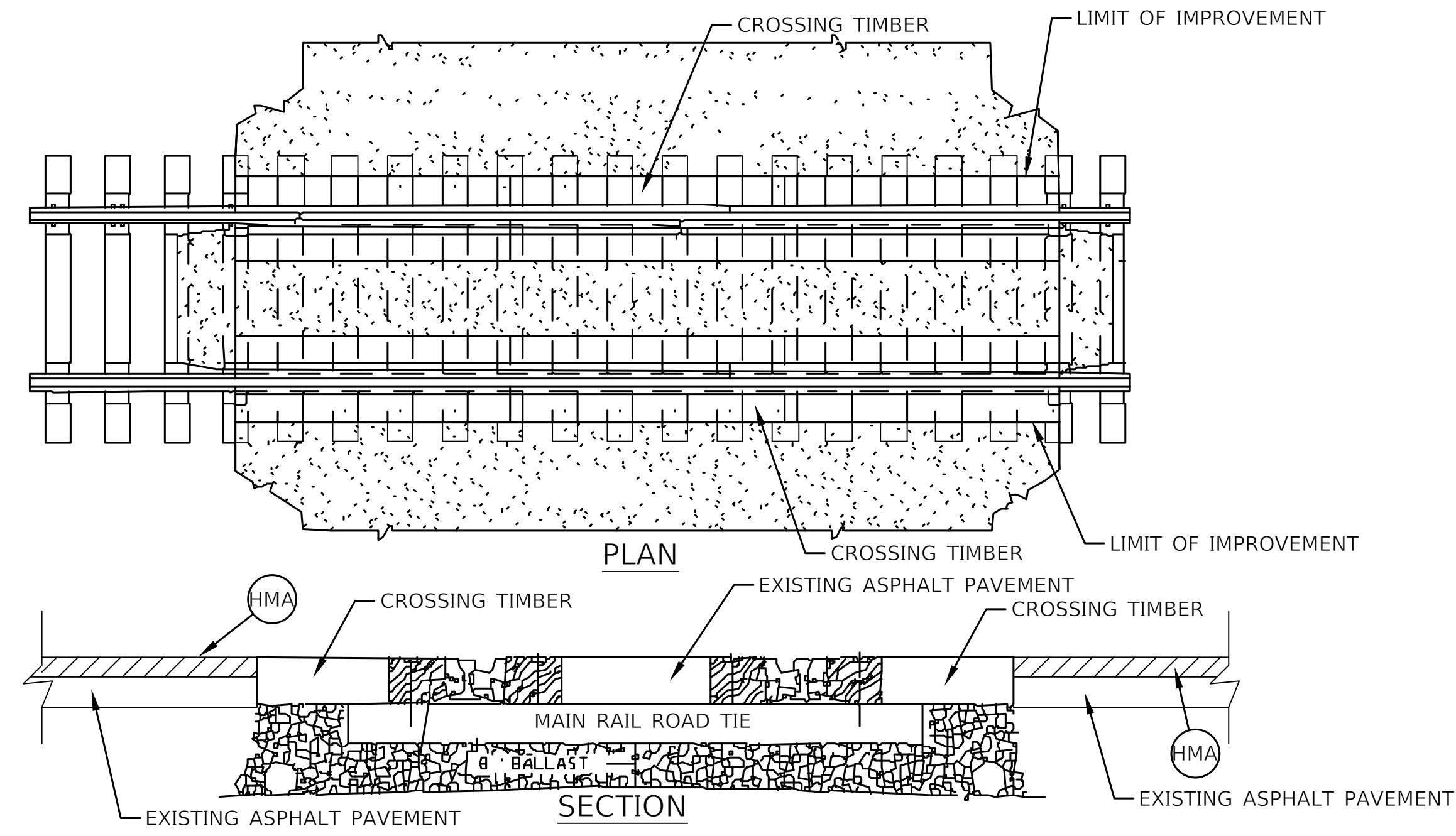
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	9
CONTRACT NO. 61G50				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)				

SCALE: 1"=50' SHEET NO. 9 OF 27 SHEETS STA. TO STA.



- LEGEND**
- CLASS D PATCHES, 8 INCH
 - DRIVEWAY PAVEMENT REMOVAL
 - HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
 - COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
 - VALVE BOXES TO BE ADJUSTED
 - DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED
 - DRAINAGE AND UTILITY STRUCTURES TO BE RECONSTRUCTED
 - HOT-MIX ASPHALT SURFACE REMOVAL, 4"
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50 - 2"
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 2 1/4"
 - HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8"
ON AGGREGATE BASE COURSE, TYPE B 4"
 - PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH
ON AGGREGATE BASE COURSE, TYPE B 4"

- NOTES**
1. CLASS D PATCHES, 8 INCH SHALL BE AS SHOWN ON THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
 2. DRIVEWAY REMOVAL AND REPLACEMENT SHALL BE COMPLETED AS DIRECTED BY THE ENGINEER.
 3. ANY TOPSOIL EXCAVATED DURING CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE STOCK PILED ON-SITE AND RE-USED AS FILL IN THE RESTORATION AREA. THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONTRACT PAY ITEMS.
 4. THE TYPE OF COMBINATION CURB AND GUTTER ON THIS SHEET IS M-4.12 AND B-6.24 SEE PLAN FOR LOCATION.



RAIL ROAD CROSSING DETAIL

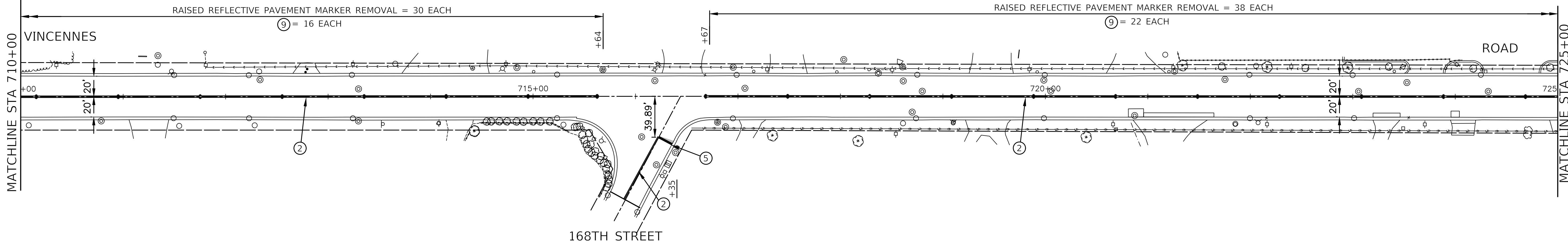
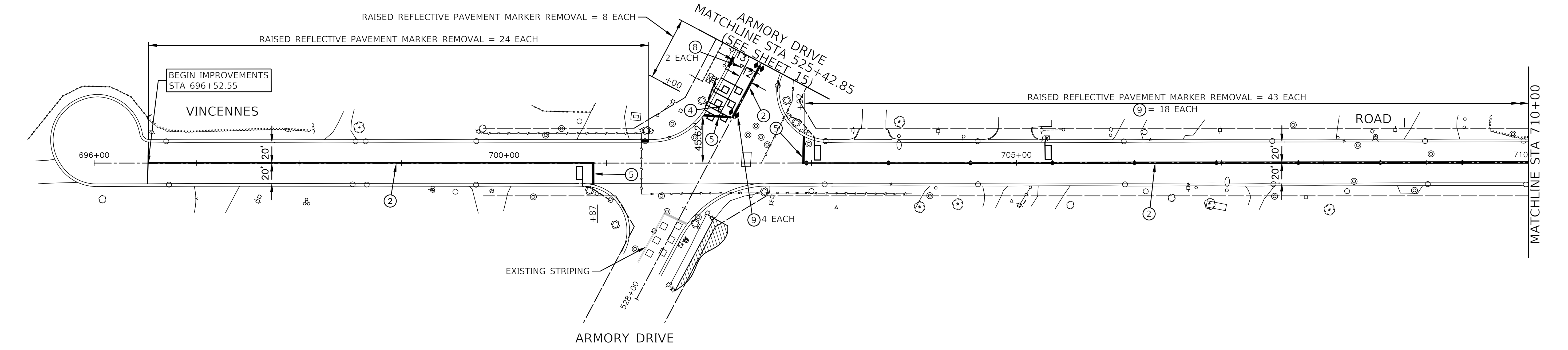
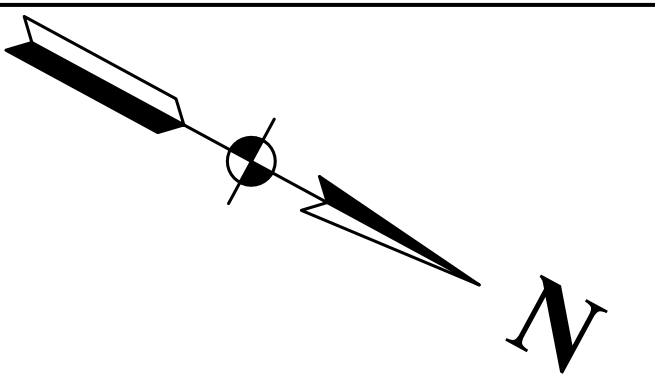
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	PLOT DATE = 4/1/2020	CHECKED -- APG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARMORY DRIVE
VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING
IMPROVEMENT PLAN

SCALE: 1"=50' SHEET NO. 10 OF 27 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	10
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT JHVC (074)	
			CONTRACT NO. 61G50	



PAVEMENT MARKING LEGEND

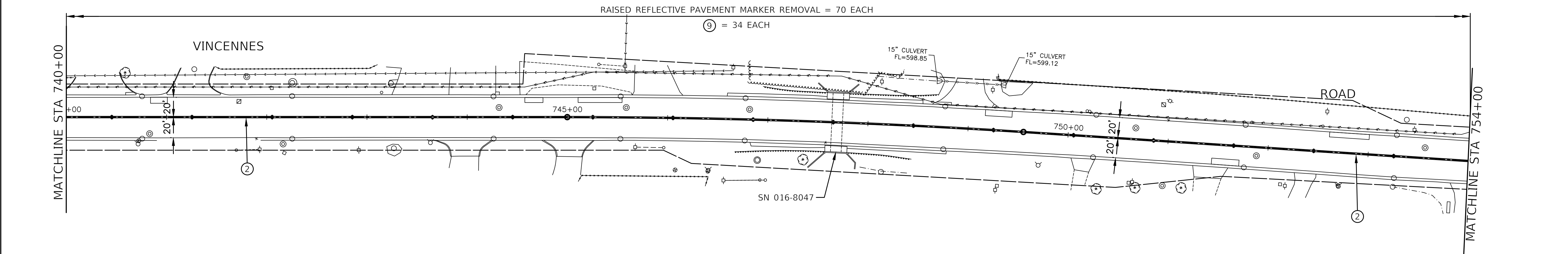
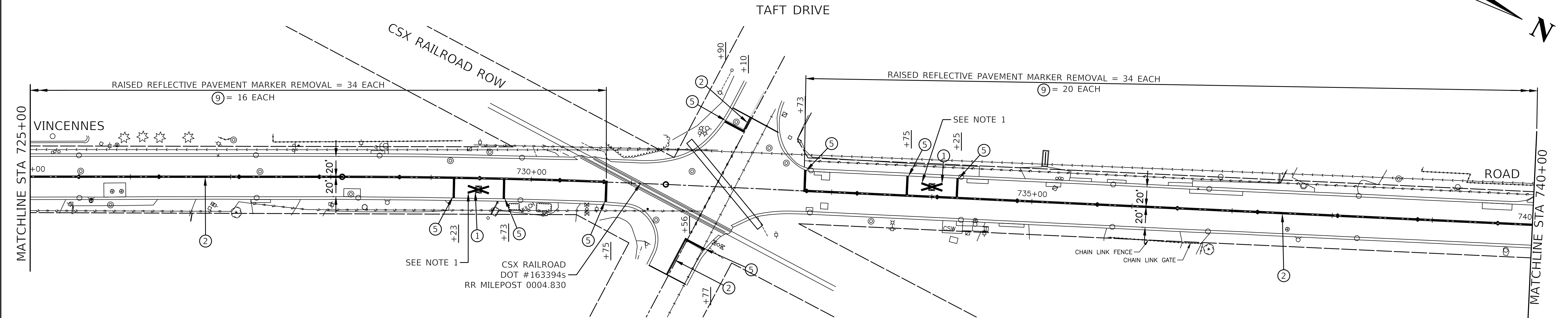
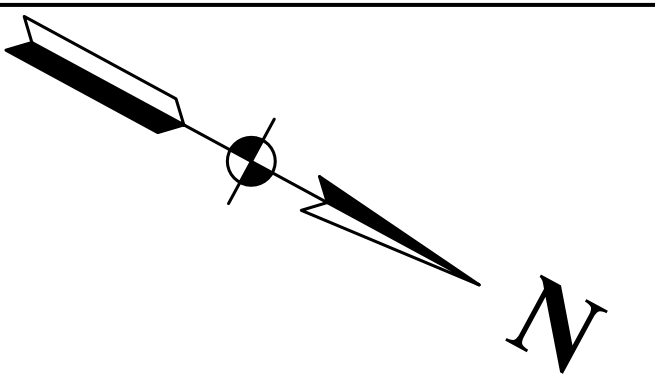
- ① WHITE - THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ② DOUBLE YELLOW - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (11" C/C)
- ③ WHITE SKIP-DASH - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (10' LINE - 30' SPACE)
- ④ WHITE - THERMOPLASTIC PAVEMENT MARKING - LINE 6"
- ⑤ WHITE STOP BAR - THERMOPLASTIC PAVEMENT MARKING - LINE 24"
- ⑥ DOUBLE YELLOW - PAINT PAVEMENT MARKING - LINE 4"
- ⑦ WHITE SKIP-DASH - THERMOPLASTIC PAVEMENT MARKING - LINE 6" (2' LINE - 6' SPACE)
- ⑧ ▶ ONE-WAY CRYSTAL RAISED REFLECTIVE PAVEMENT MARKER
80' C/C UNLESS OTHERWISE INDICATED
- ⑨ ◆ TWO-WAY AMBER RAISED REFLECTIVE PAVEMENT MARKER
80' C/C UNLESS OTHERWISE INDICATED

FILE NAME = 18R0706-PMKG-01 - IDOT P01	USER NAME =	DESIGNED -- JDH	REVISED --
	CHECKED -- WD	REVISIONS --	
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	PLOT DATE = 4/1/2020	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING	
PAVEMENT MARKING PLAN	
SCALE: 1"=50'	SHEET NO. 11 OF 27 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	11
CONTRACT NO. 61G50				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	JHVC (074)	



PAVEMENT MARKING LEGEND

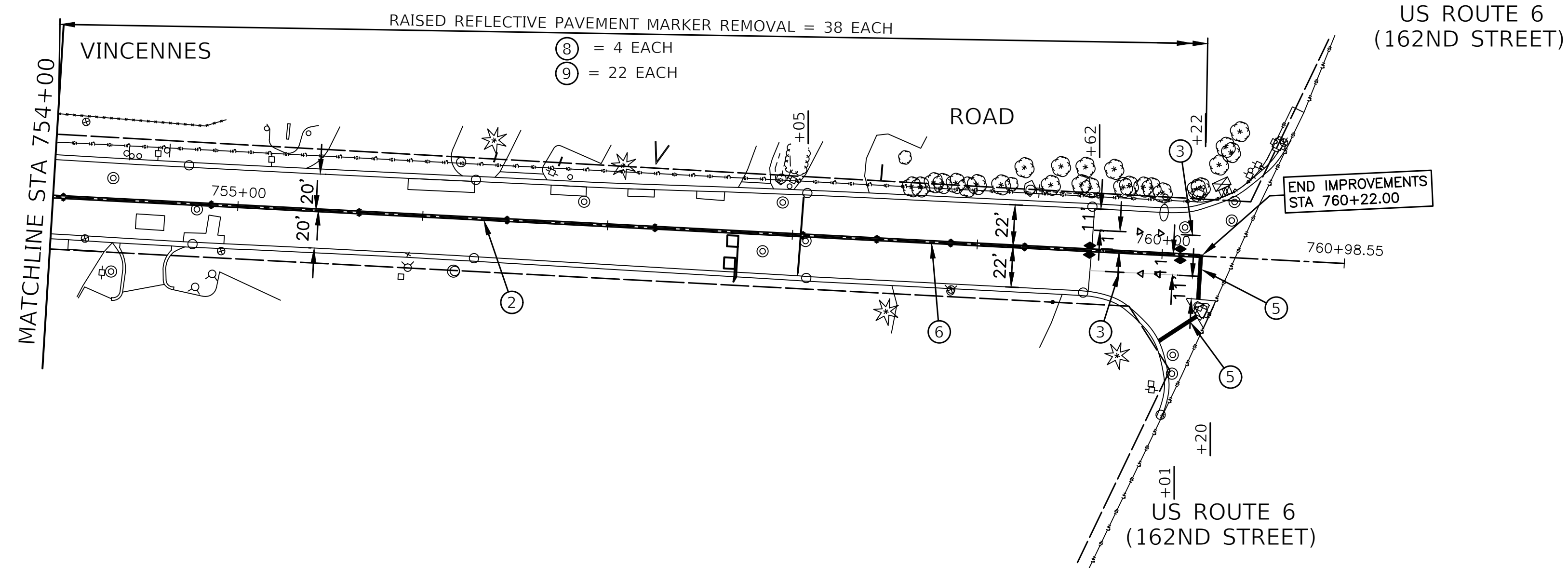
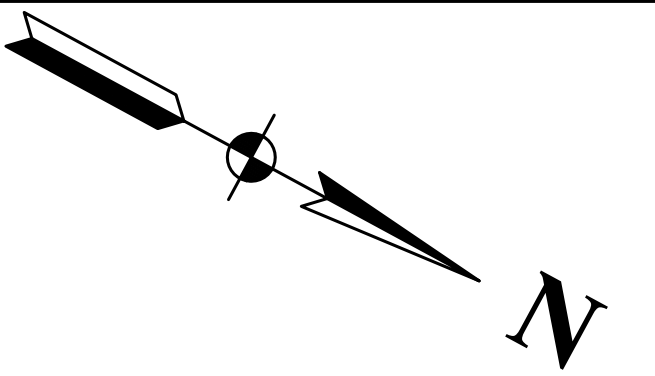
- ① WHITE - THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ② DOUBLE YELLOW - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (11" C/C)
- ③ WHITE SKIP-DASH - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (10' LINE - 30' SPACE)
- ④ WHITE - THERMOPLASTIC PAVEMENT MARKING - LINE 6"
- ⑤ WHITE STOP BAR - THERMOPLASTIC PAVEMENT MARKING - LINE 24"
- ⑥ DOUBLE YELLOW - PAINT PAVEMENT MARKING - LINE 4"
- ⑦ WHITE SKIP-DASH - THERMOPLASTIC PAVEMENT MARKING - LINE 6" (2' LINE - 6' SPACE)
- ⑧ ▶ ONE-WAY CRYSTAL RAISED REFLECTIVE PAVEMENT MARKER
80' C/C UNLESS OTHERWISE INDICATED
- ⑨ ◆ TWO-WAY AMBER RAISED REFLECTIVE PAVEMENT MARKER
80' C/C UNLESS OTHERWISE INDICATED

FILE NAME = 18R0706-PMKG-01 - IDOT P01 (2)	USER NAME =	DESIGNED -- JDH	REVISED --
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	PLOT SCALE =	DRAWN -- MED	REVISED --
	PLOT DATE = 4/1/2020	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING	
PAVEMENT MARKING PLAN	
SCALE: 1"=50'	SHEET NO. 12 OF 27 SHEETS
STA.	TO STA.

F.A.U. RTE. 3598	SECTION 17-00110-00-RS	COUNTY COOK	TOTAL SHEETS 27	SHEET NO. 12
CONTRACT NO. 61G50			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)	



PAVEMENT MARKING LEGEND

- ① WHITE - THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ② DOUBLE YELLOW - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (11" C/C)
- ③ WHITE SKIP-DASH - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (10' LINE - 30' SPACE)
- ④ WHITE - THERMOPLASTIC PAVEMENT MARKING - LINE 6"
- ⑤ WHITE STOP BAR - THERMOPLASTIC PAVEMENT MARKING - LINE 24"
- ⑥ DOUBLE YELLOW - PAINT PAVEMENT MARKING - LINE 4"
- ⑦ WHITE SKIP-DASH - THERMOPLASTIC PAVEMENT MARKING - LINE 6" (2' LINE - 6' SPACE)
- ⑧ ▶ ONE-WAY CRYSTAL RAISED REFLECTIVE PAVEMENT MARKER
80' C/C UNLESS OTHERWISE INDICATED
- ⑨ ◆ TWO-WAY AMBER RAISED REFLECTIVE PAVEMENT MARKER
80' C/C UNLESS OTHERWISE INDICATED

NOTES

1. SEE STANDARD 78001-05 FOR PAVEMENT MARKING DETAIL.

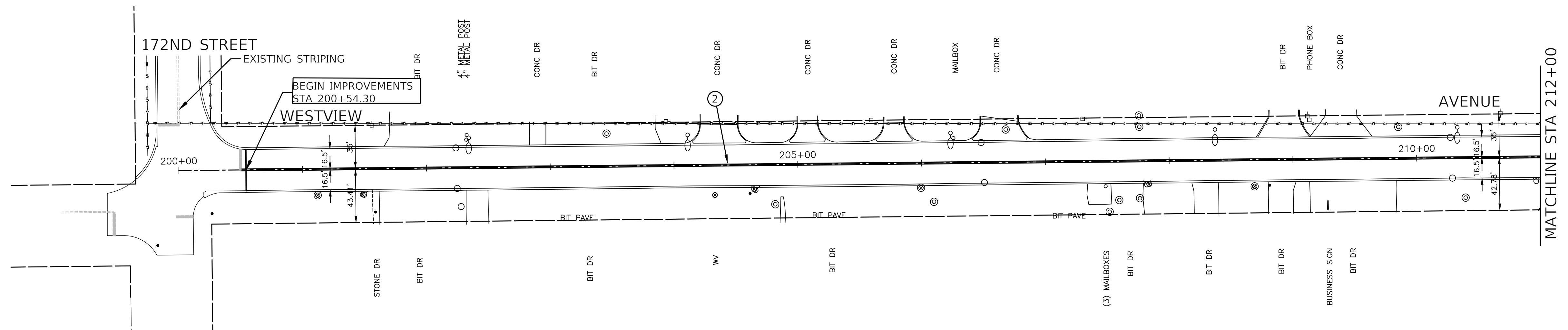
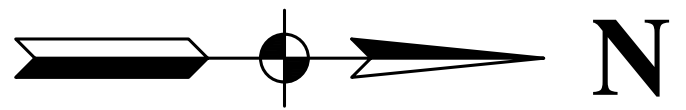
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	PLOT DATE = 4/1/2020	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

VINCENNES ROAD
VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING
PAVEMENT MARKING PLAN

SCALE: 1"=50' SHEET NO. 13 OF 27 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	13
CONTRACT NO. 61G50				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	JHVC (074)	



PAVEMENT MARKING LEGEND

- ① WHITE - THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ② DOUBLE YELLOW - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (11" C/C)
- ③ WHITE SKIP-DASH - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (10' LINE - 30' SPACE)
- ④ WHITE - THERMOPLASTIC PAVEMENT MARKING - LINE 6"
- ⑤ WHITE STOP BAR - THERMOPLASTIC PAVEMENT MARKING - LINE 24"
- ⑥ DOUBLE YELLOW - PAINT PAVEMENT MARKING - LINE 4"
- ⑦ WHITE SKIP-DASH - THERMOPLASTIC PAVEMENT MARKING - LINE 6" (2' LINE - 6' SPACE)
- ⑧ ▶ ONE-WAY CRYSTAL RAISED REFLECTIVE PAVEMENT MARKER
80' C/C UNLESS OTHERWISE INDICATED
- ⑨ ◆ TWO-WAY AMBER RAISED REFLECTIVE PAVEMENT MARKER
80' C/C UNLESS OTHERWISE INDICATED

NOTES

- 1. SEE STANDARD 78001-05 FOR PAVEMENT MARKING DETAIL.

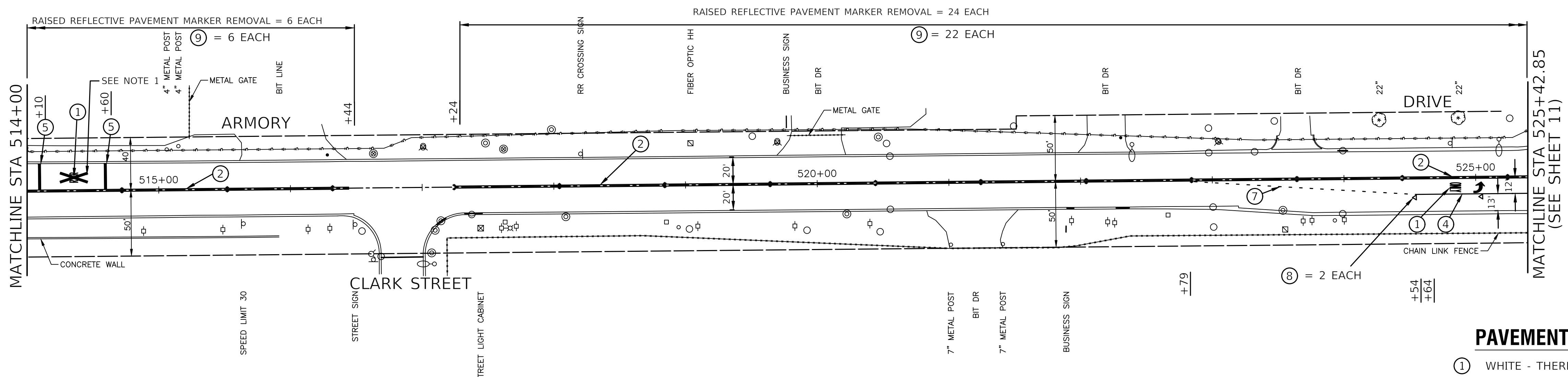
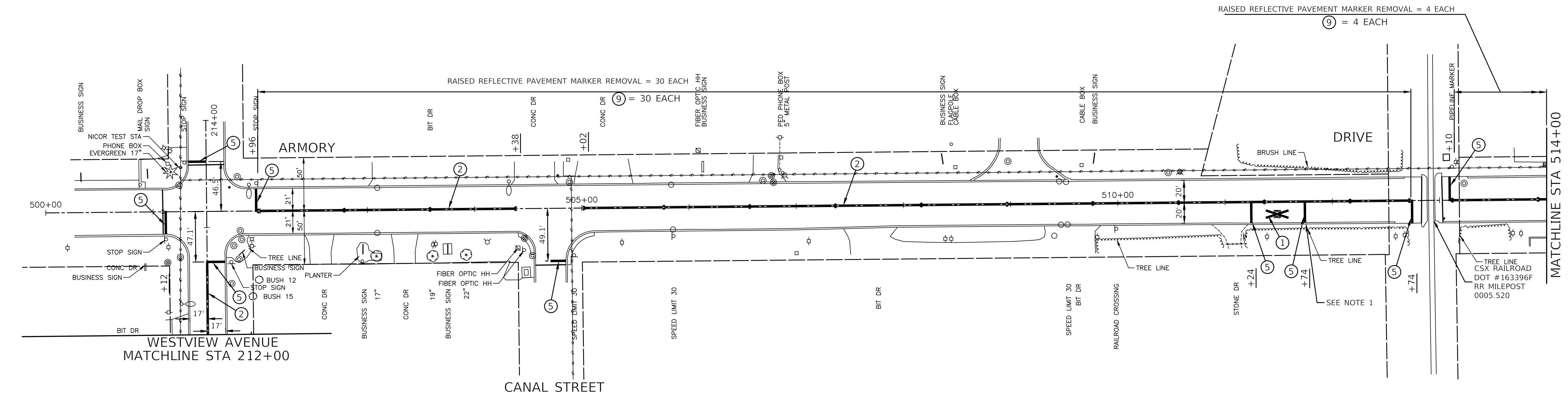
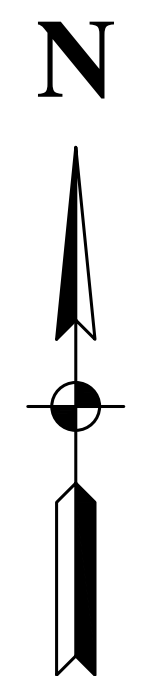
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	PLOT SCALE =	DRAWN -- MED	REVISED --
	PLOT DATE = 4/1/2020	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WESTVIEW AVENUE
VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING
PAVEMENT MARKING PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	14
CONTRACT NO. 61G50				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)				

SCALE: 1"=50' SHEET NO. 14 OF 27 SHEETS STA. TO STA.



PAVEMENT MARKING LEGEND

- ① WHITE - THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ② DOUBLE YELLOW - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (11" C/C)
- ③ WHITE SKIP-DASH - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (10' LINE - 30' SPACE)
- ④ WHITE - THERMOPLASTIC PAVEMENT MARKING - LINE 6"
- ⑤ WHITE STOP BAR - THERMOPLASTIC PAVEMENT MARKING - LINE 24"
- ⑥ DOUBLE YELLOW - PAINT PAVEMENT MARKING - LINE 4"
- ⑦ WHITE SKIP-DASH - THERMOPLASTIC PAVEMENT MARKING - LINE 6" (2' LINE - 6' SPACE)
- ⑧ ▶ ONE-WAY CRYSTAL RAISED REFLECTIVE PAVEMENT MARKER 80" C/C UNLESS OTHERWISE INDICATED
- ⑨ ◆ TWO-WAY AMBER RAISED REFLECTIVE PAVEMENT MARKER 80" C/C UNLESS OTHERWISE INDICATED

NOTES

1. SEE STANDARD 78001-05 FOR PAVEMENT MARKING DETAIL.

FILE NAME = 18R0708-PMKG-01 - IDOT P01

USER NAME =	DESIGNED — JDH	REVISED —
	CHECKED — WD	REVISED —
PLOT SCALE =	DRAWN — MED	REVISED —
PLOT DATE = 4/1/2020	CHECKED — APG	REVISED —

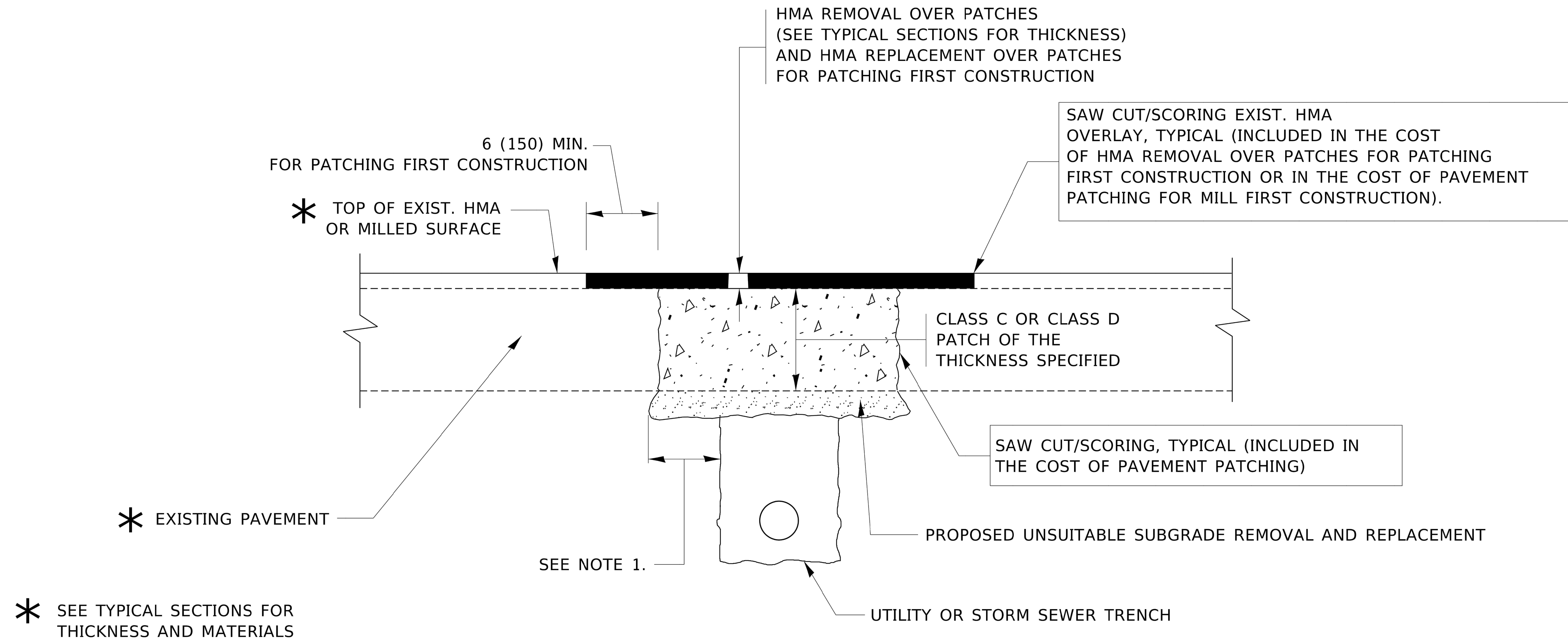
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARMORY DRIVE
VINCENNES ROAD, ARMORY DRIVE, AND WESTVIEW AVENUE ROAD RESURFACING
PAVEMENT MARKING PLAN

SCALE: 1"=50'

SHEET NO. 15 OF 27 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	15
CONTRACT NO. 61G50				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	JHVC (074)	



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

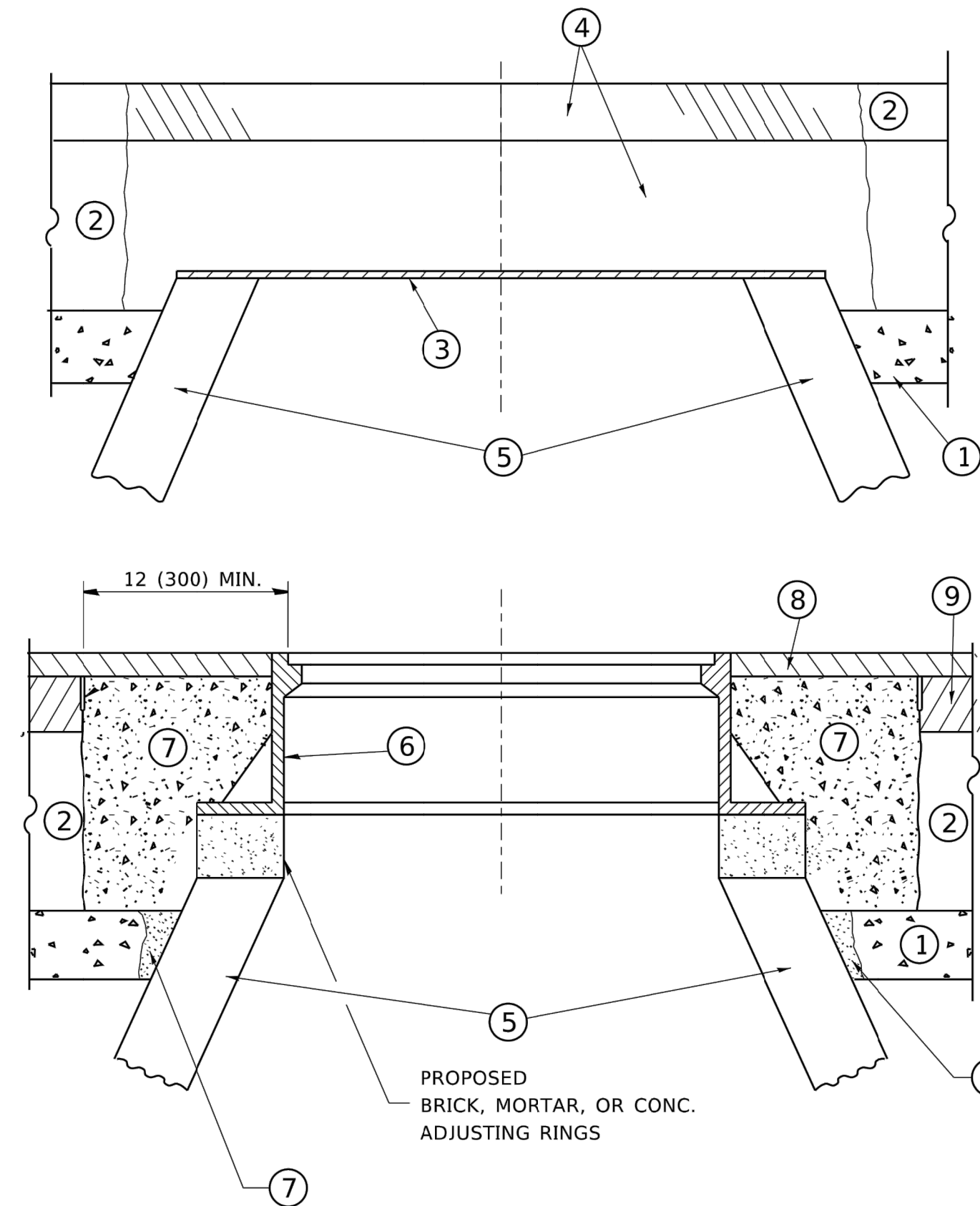
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 18R0708-DTLS-01 - IDOT P01 (12)	USER NAME = footemj	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - R. BORO 01-01-07			3598	17-00110-00-RS	COOK	27	16	
	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - R. BORO 09-04-07			BD400-04 (BD-22)		CONTRACT NO. 61G50			
	PLOT DATE = 3/27/2019	DATE - 10-25-94	REVISED - K. ENG 10-27-08			SCALE:	SHEET NO. 16 OF 27 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-1 *CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

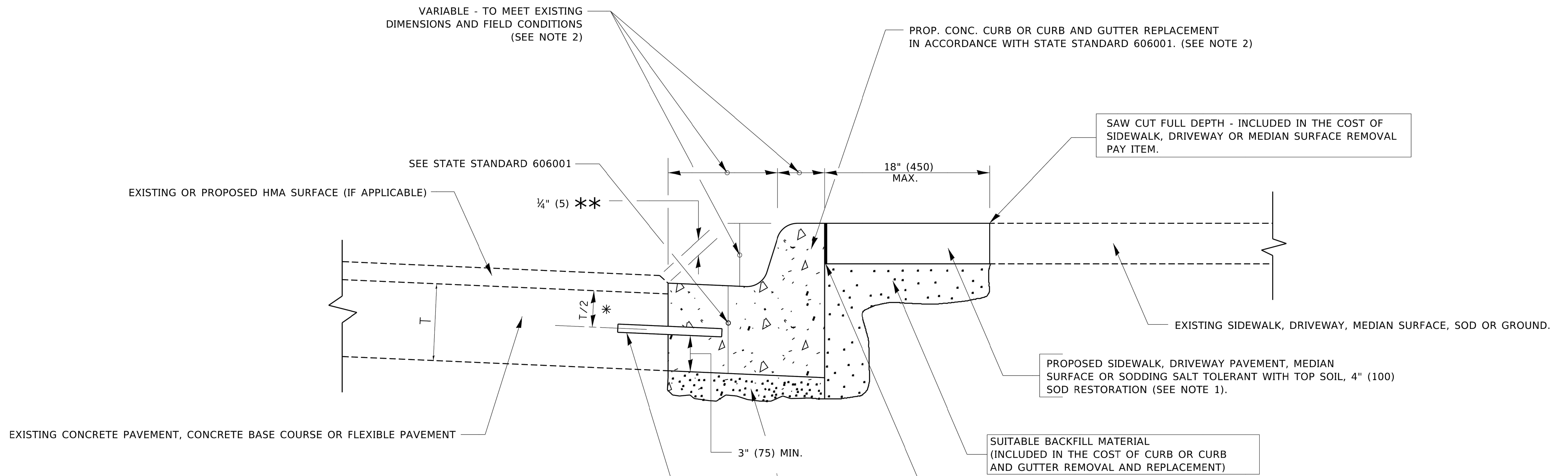
FILE NAME = 18R0708-DTLS-01 - IDOT P01	USER NAME = footemj	DESIGNED - R. SHAH	REVISED - R. WEDEMAN 05-14-04
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 3/27/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: SHEET NO. 17 OF 27 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	17
BD600-03 (BD-8)		CONTRACT NO. 61G50		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT JHVC (074)		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE:

- SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED.
- CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE. REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

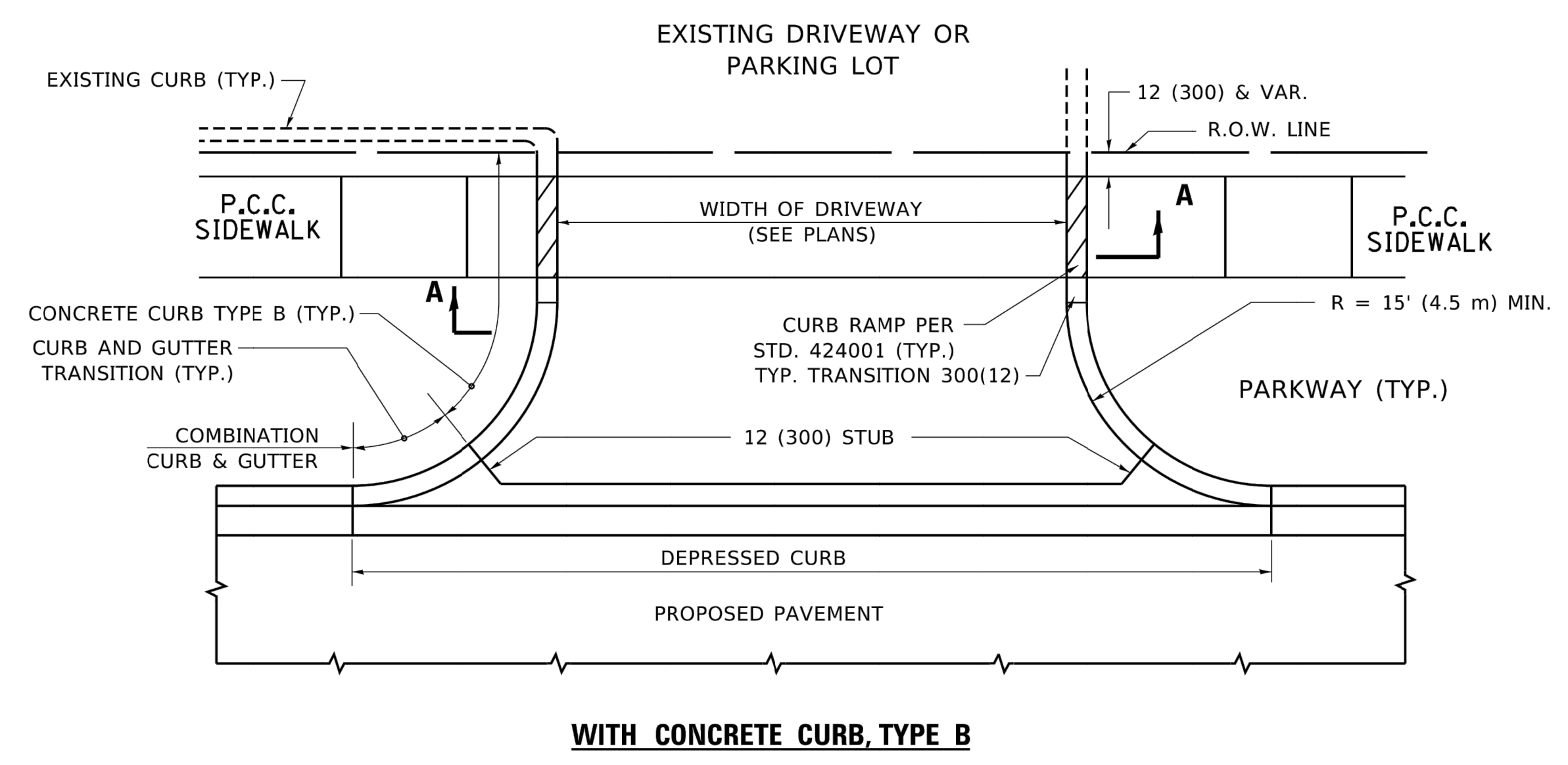
BASIS OF PAYMENT

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

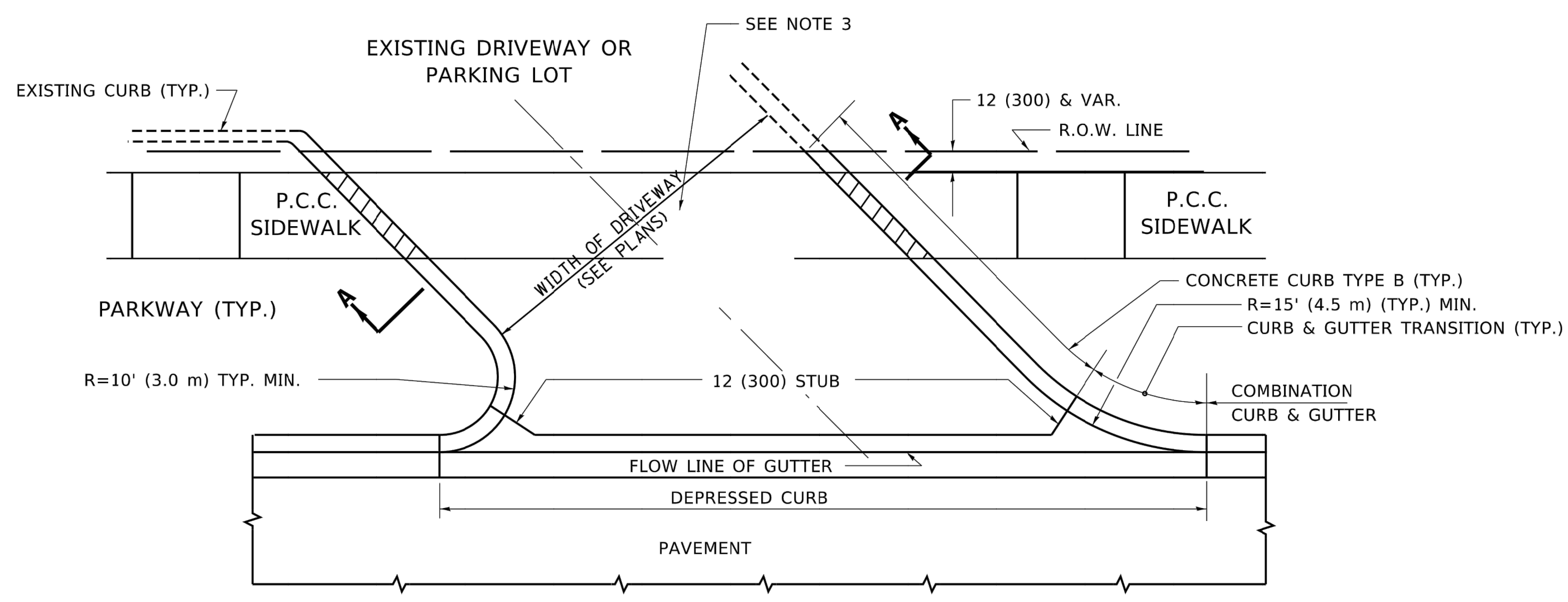
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

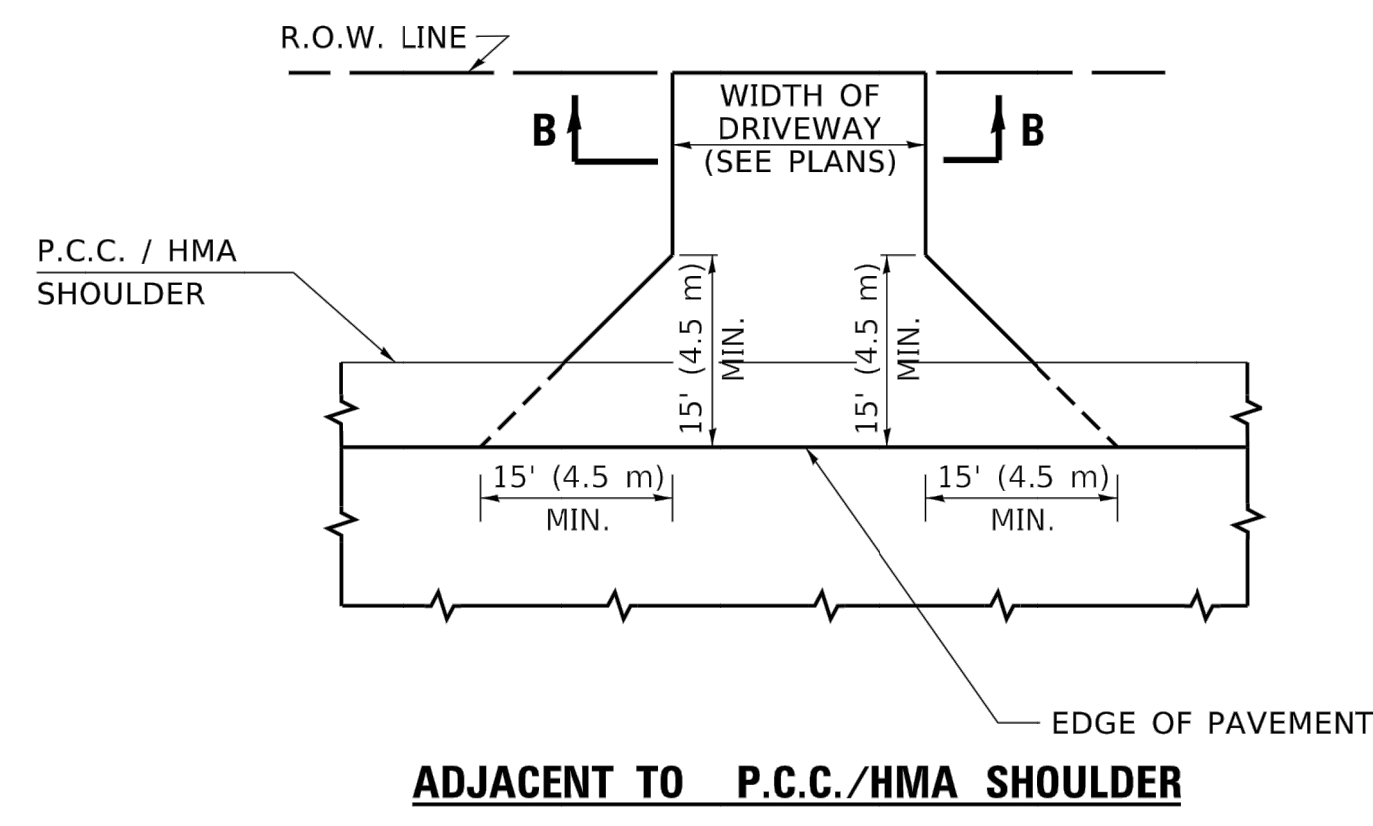
FILE NAME = 18R0708-DTLS-01 - IDOT P01 (2)	USER NAME = footemj	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. ABBAS 03-21-97			3598	17-00110-00-RS	COOK	27	18
	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 61G50		
	PLOT DATE = 3/27/2019	DATE - 03-11-94	REVISED - R. BORO 12-15-09			FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT JHVC (074)		
SCALE:						SHEET NO. 18 OF 27 SHEETS	STA.	TO STA.		



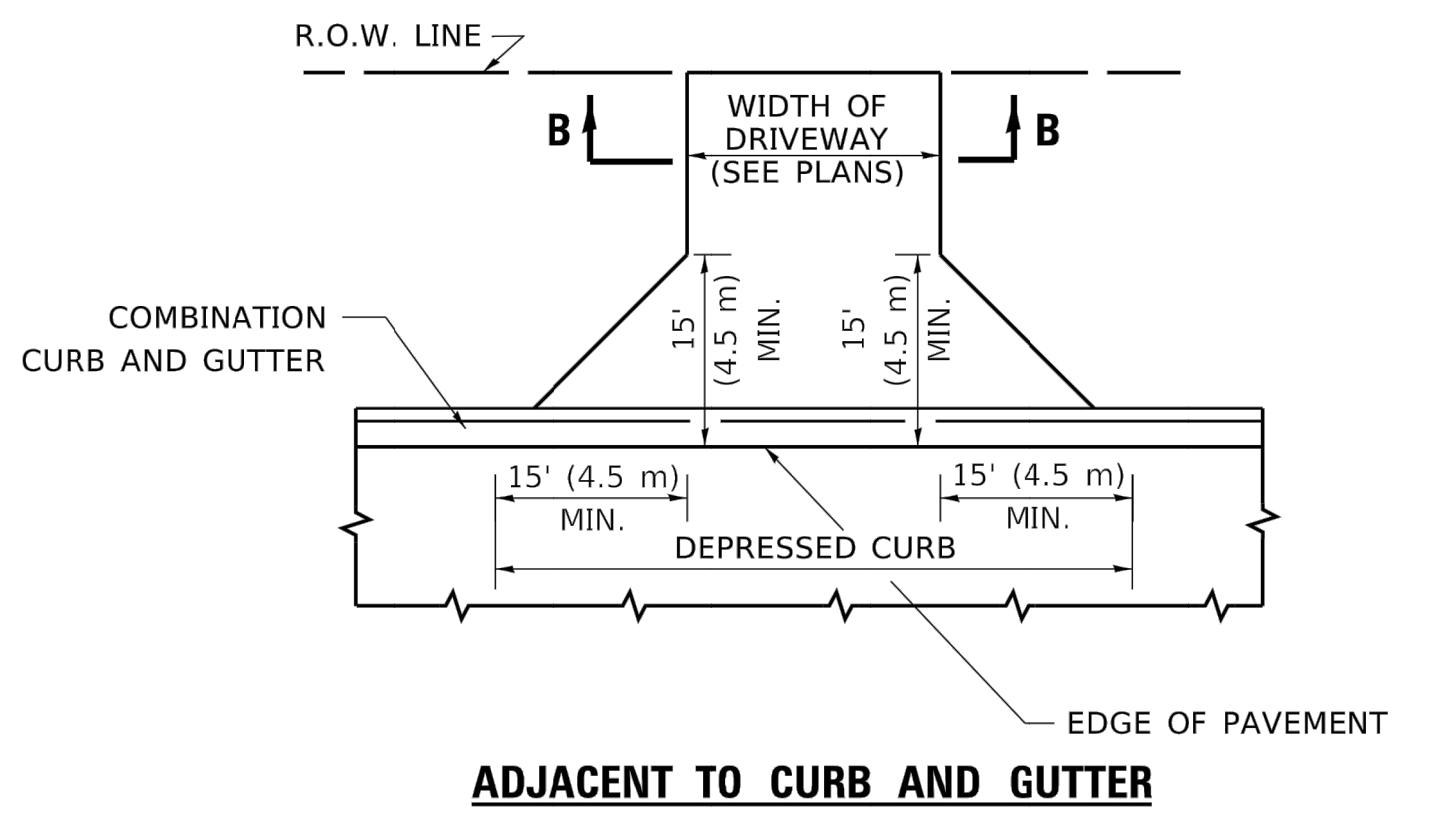
WITH CONCRETE CURB, TYPE B



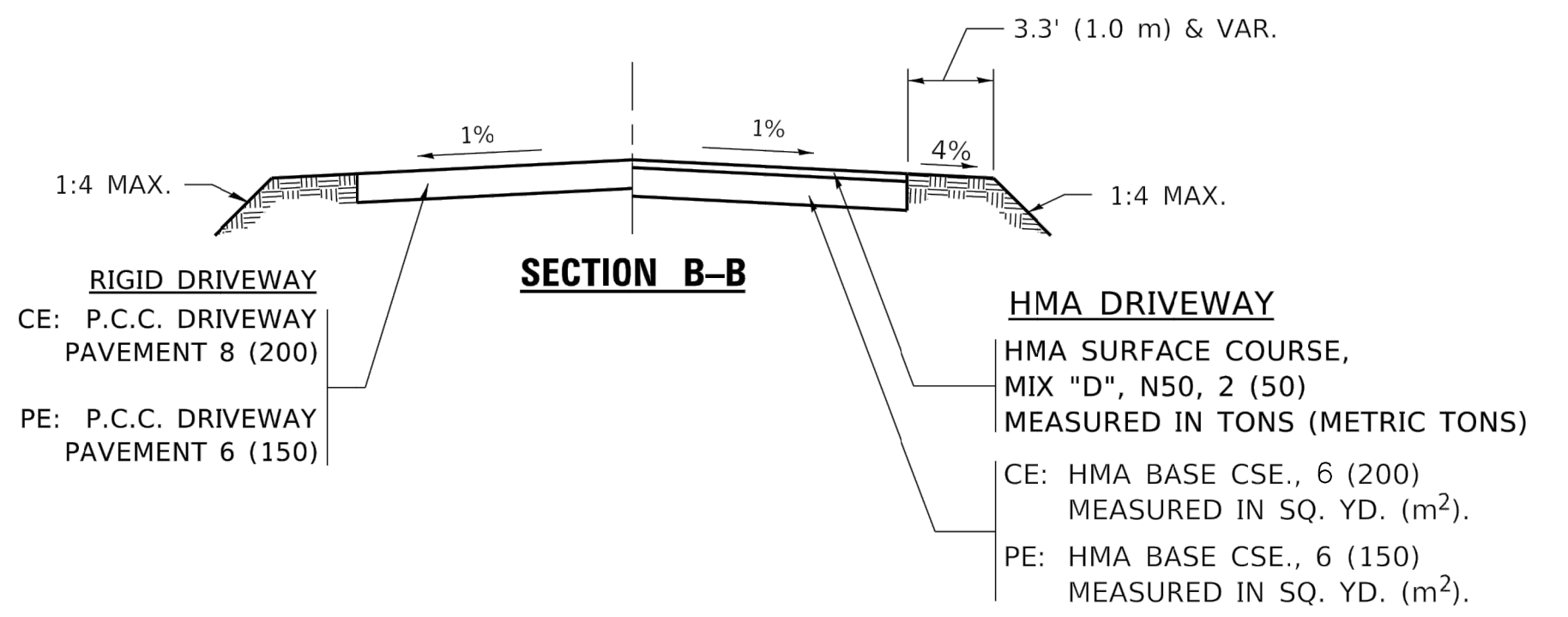
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C./HMA SHOULDER



ADJACENT TO CURB AND GUTTER



SECTION B-B

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

RIGID DRIVEWAY
 COMMERCIAL ENTRANCE (CE):
 P.C.C. DRIVEWAY PAVEMENT 8 (200)
 MEASURED IN SQ. YD. (m²)
 NON-COMMERCIAL ENTRANCE (PE):
 P.C.C. DRIVEWAY PAVEMENT 6 (150)
 MEASURED IN SQ. YD. (m²)

SECTION A-A

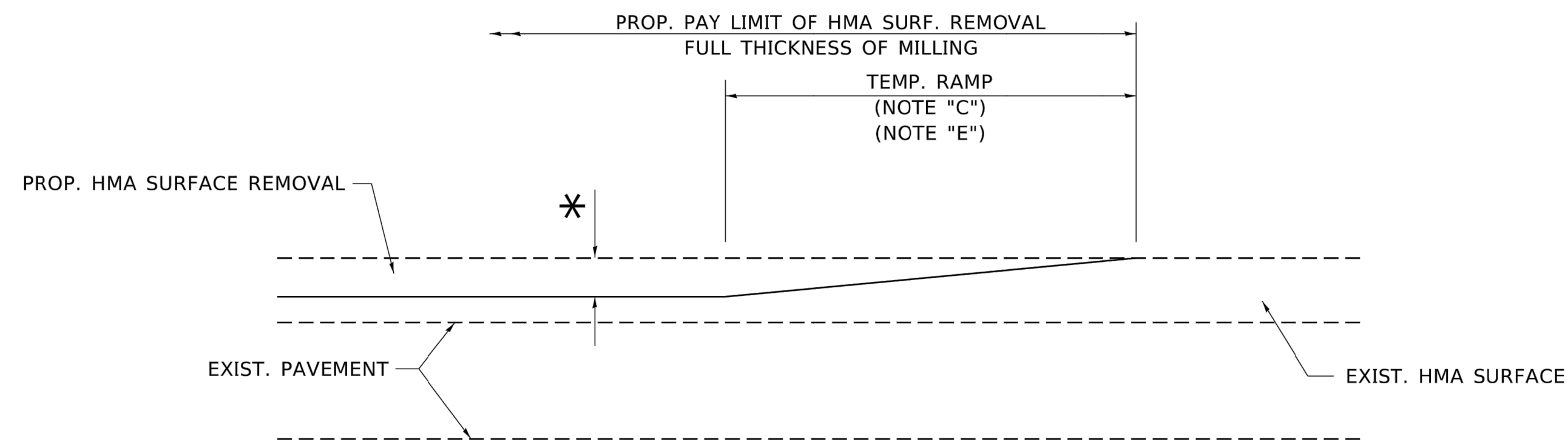
HMA DRIVEWAY
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE COURSE, 6 (200)
 MEASURED IN SQ. YD. (m²).
 PE: HMA BASE COURSE, 6 (150)
 MEASURED IN SQ. YD. (m²).

FILE NAME = 18R0708-DTLS-01 - IDOT P01 (11)	USER NAME = footemj	DESIGNED - R. SHAH	REVISED - P. LofLUER 04-15-03
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 3/27/2019	DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER ≥ 15'(4.5m)			
SCALE:	SHEET NO. 19 OF 27 SHEETS	STA. TO STA.	

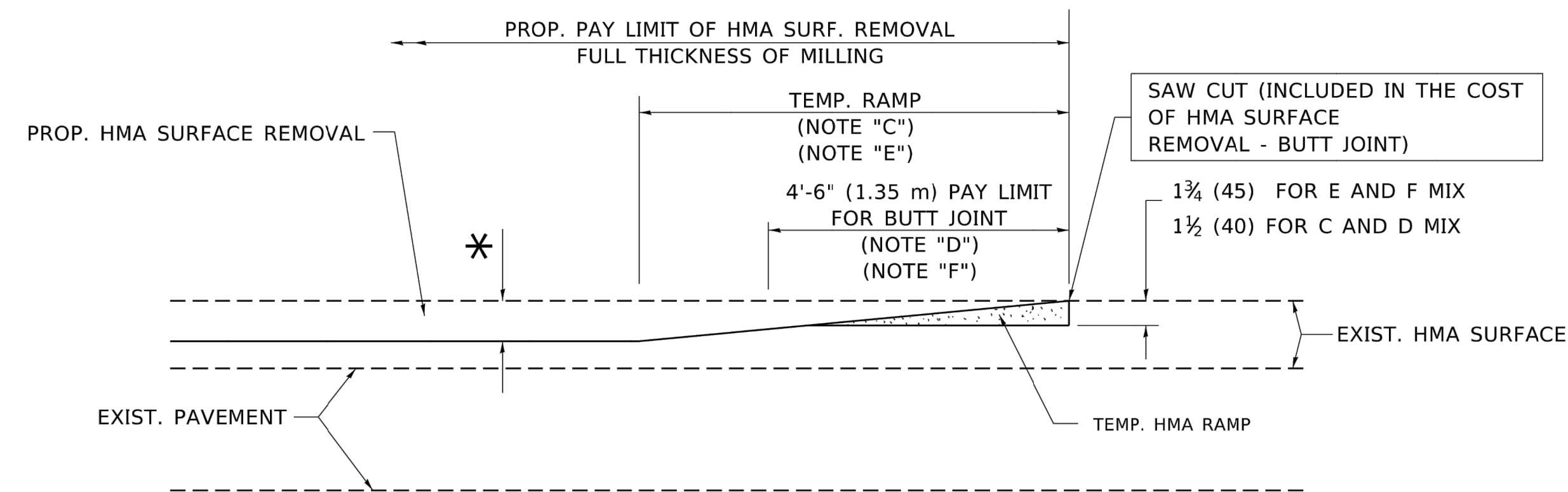
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	19
BD400-01 (BD-01)		CONTRACT NO. 61G50		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT JHVC (074)		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

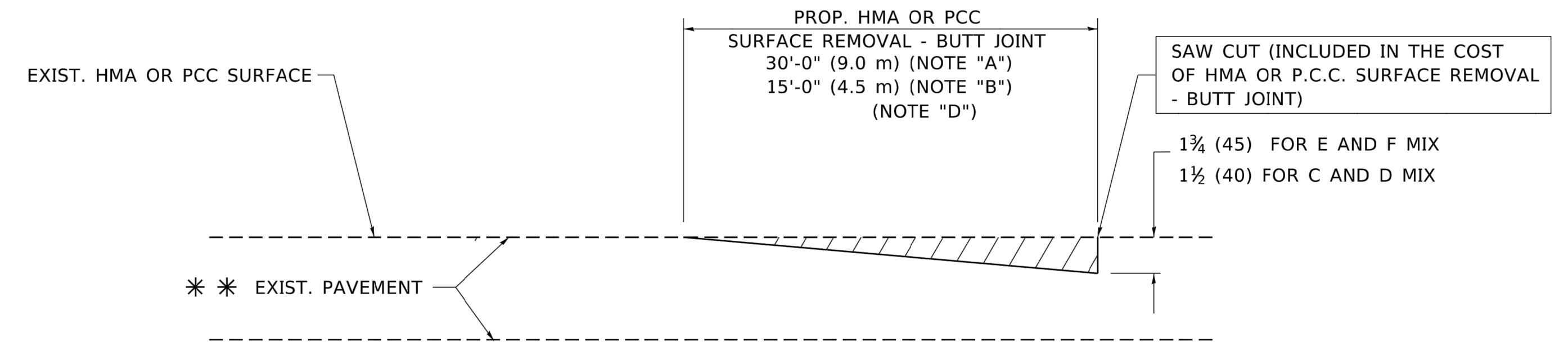


HMA CONSTRUCTED TEMPORARY RAMP

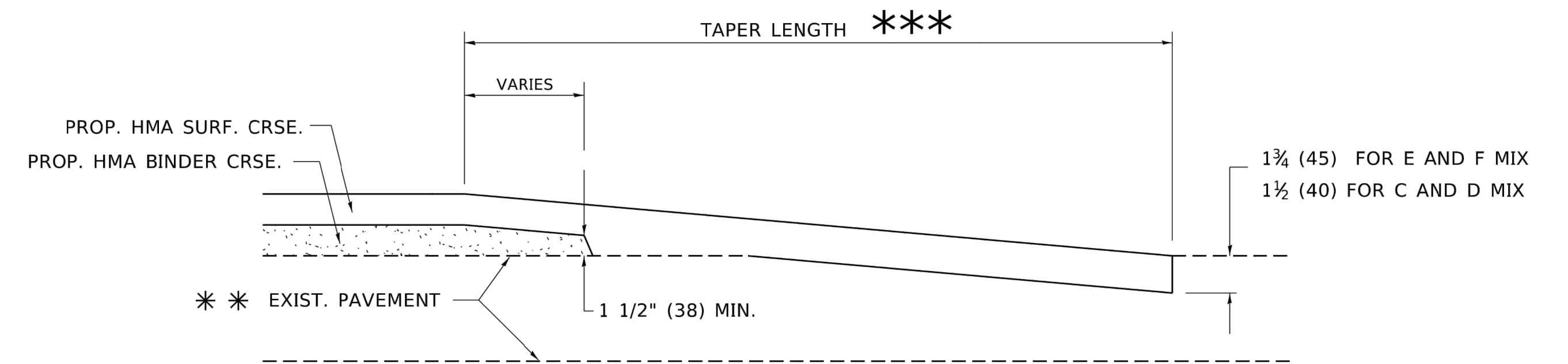
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

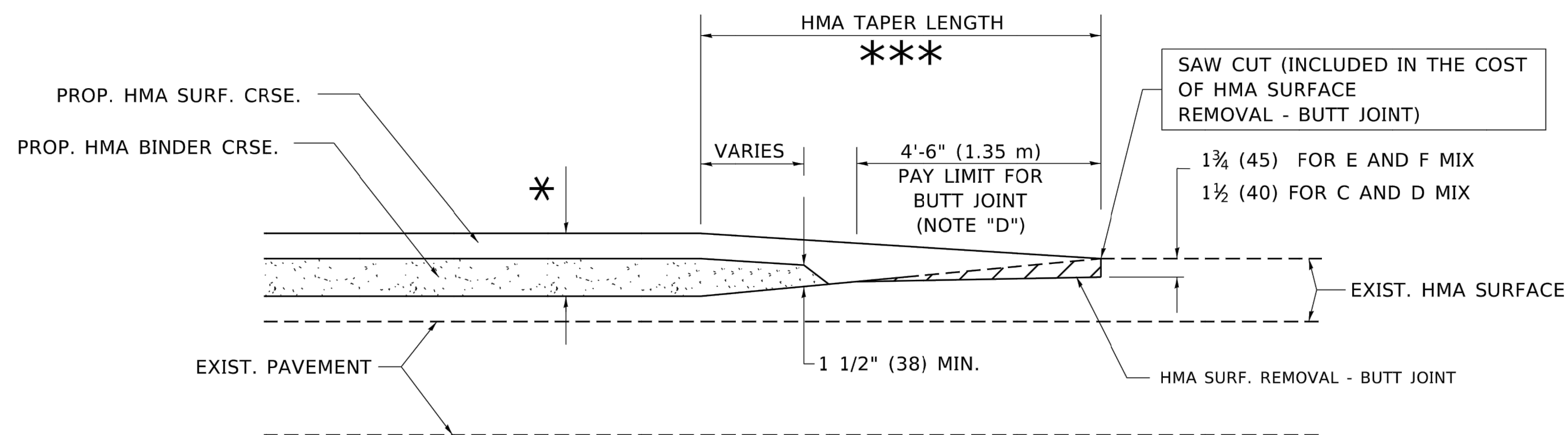
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

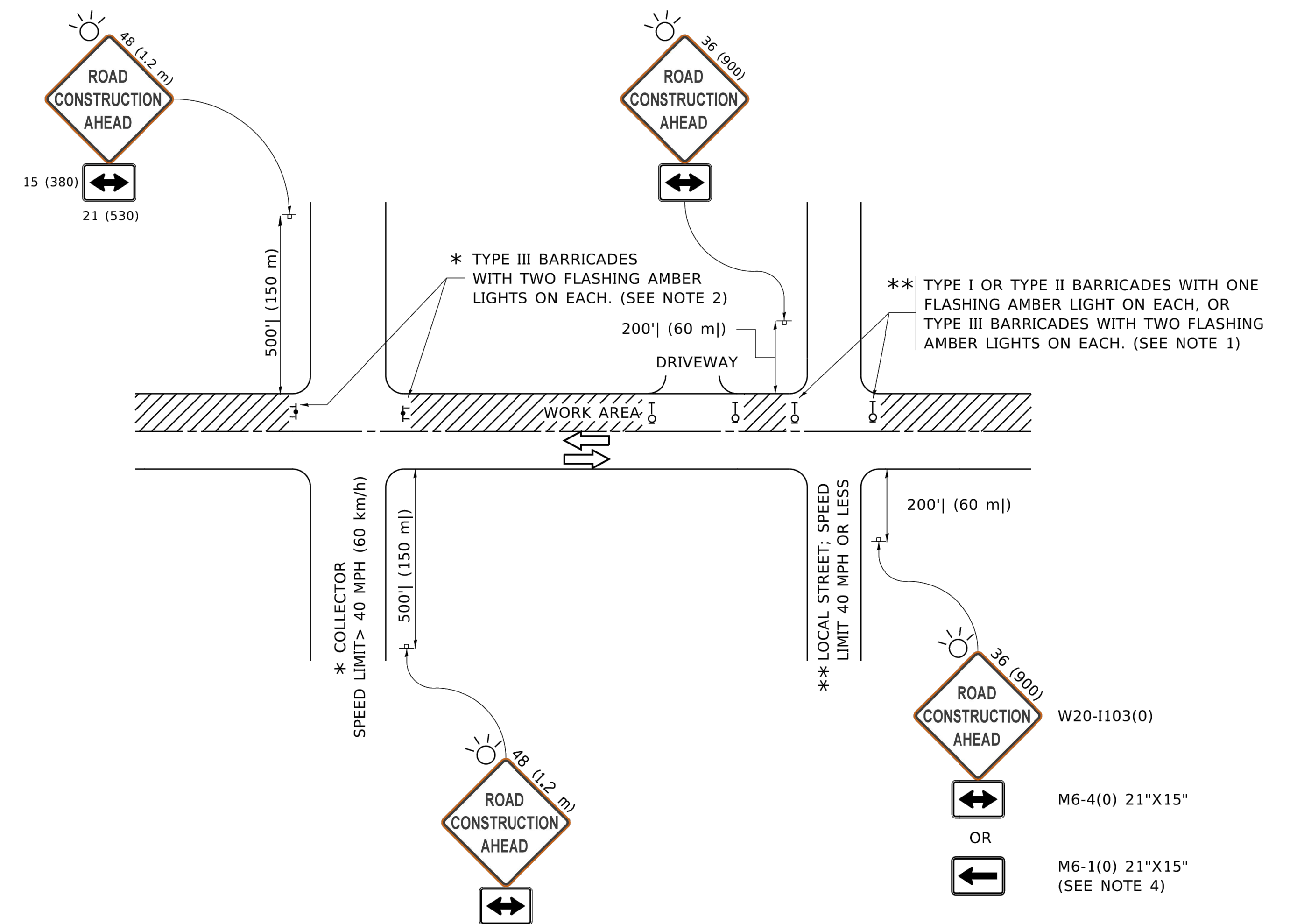
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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 3/27/2019	DATE - 06-13-90	REVISED - R.BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: SHEET NO. 20 OF 27 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	20
BD400-05 BD32		CONTRACT NO. 61G50		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT JHVC (074)		



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

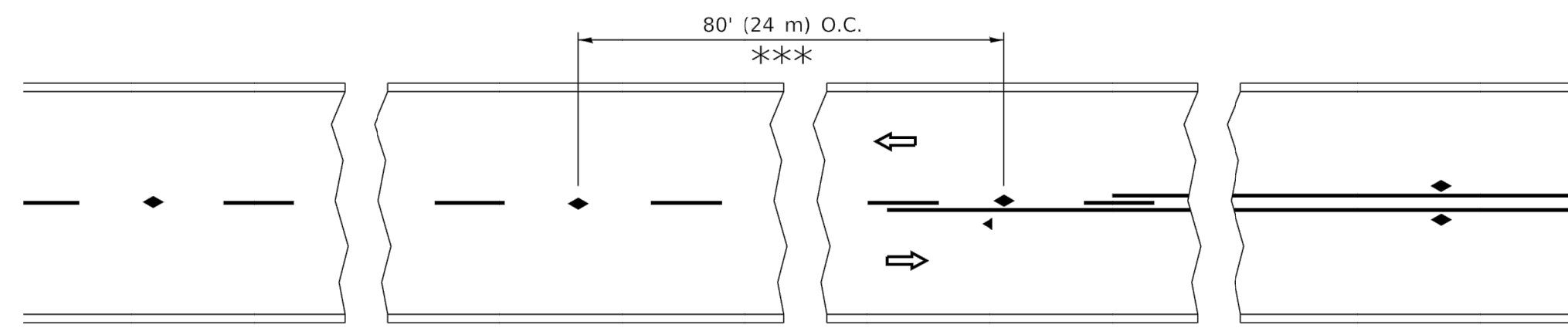
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		DRAWN -	REVISED - T. RAMMACHER 01-06-00
	PLOT SCALE = 50.0000" / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

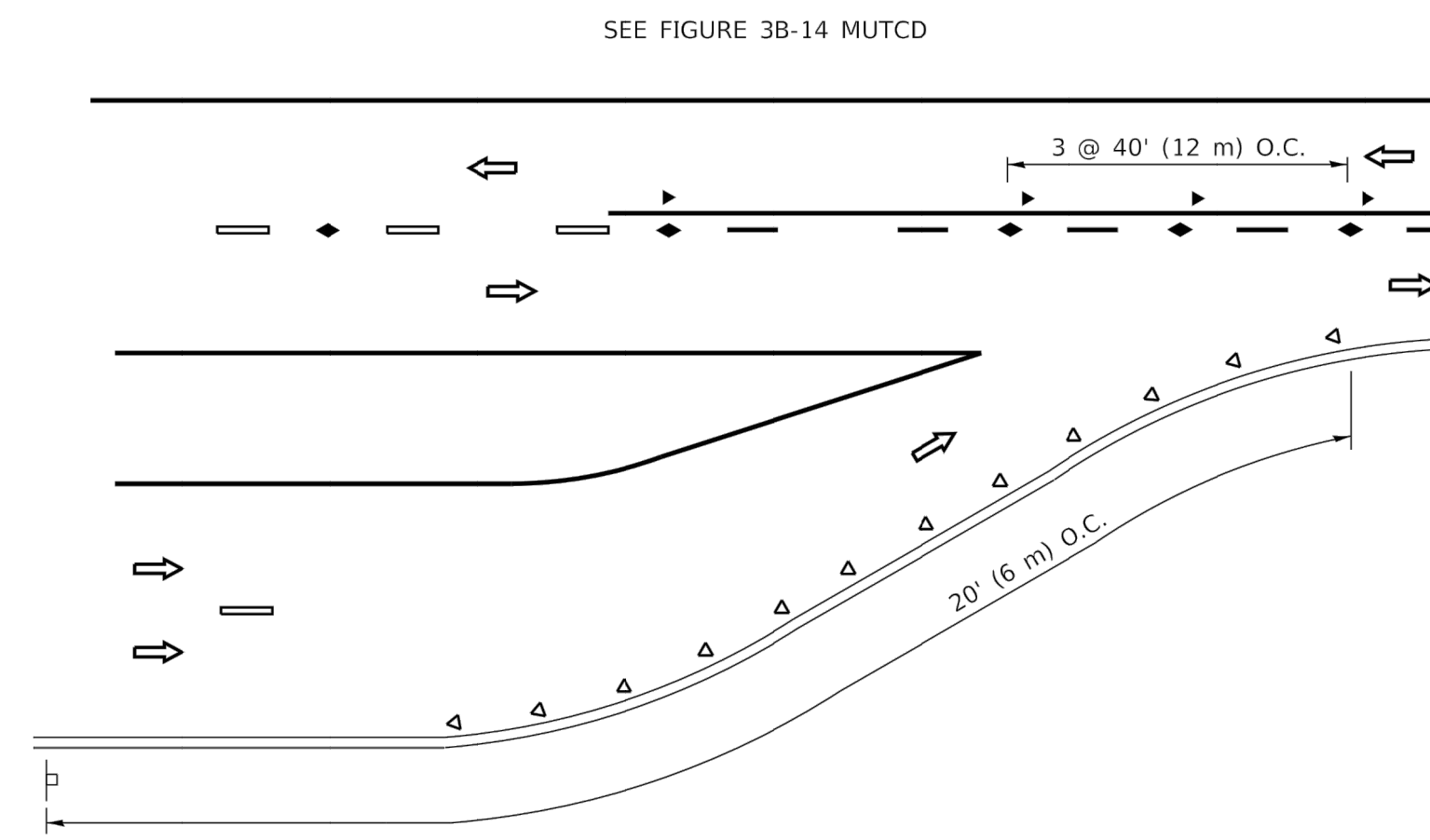
SCALE: SHEET NO. 21 OF 27 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	21
TC-10		CONTRACT NO. 61G50		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)				

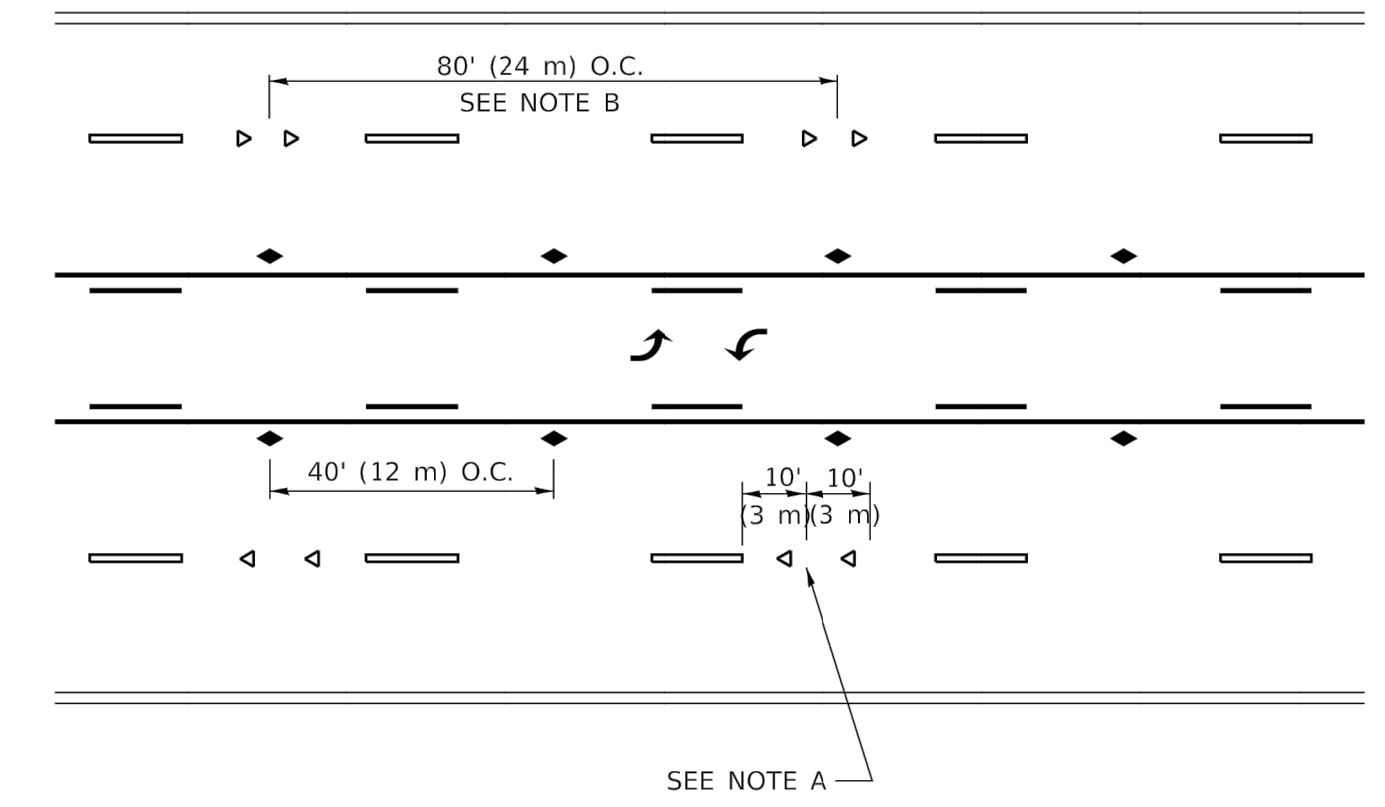


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

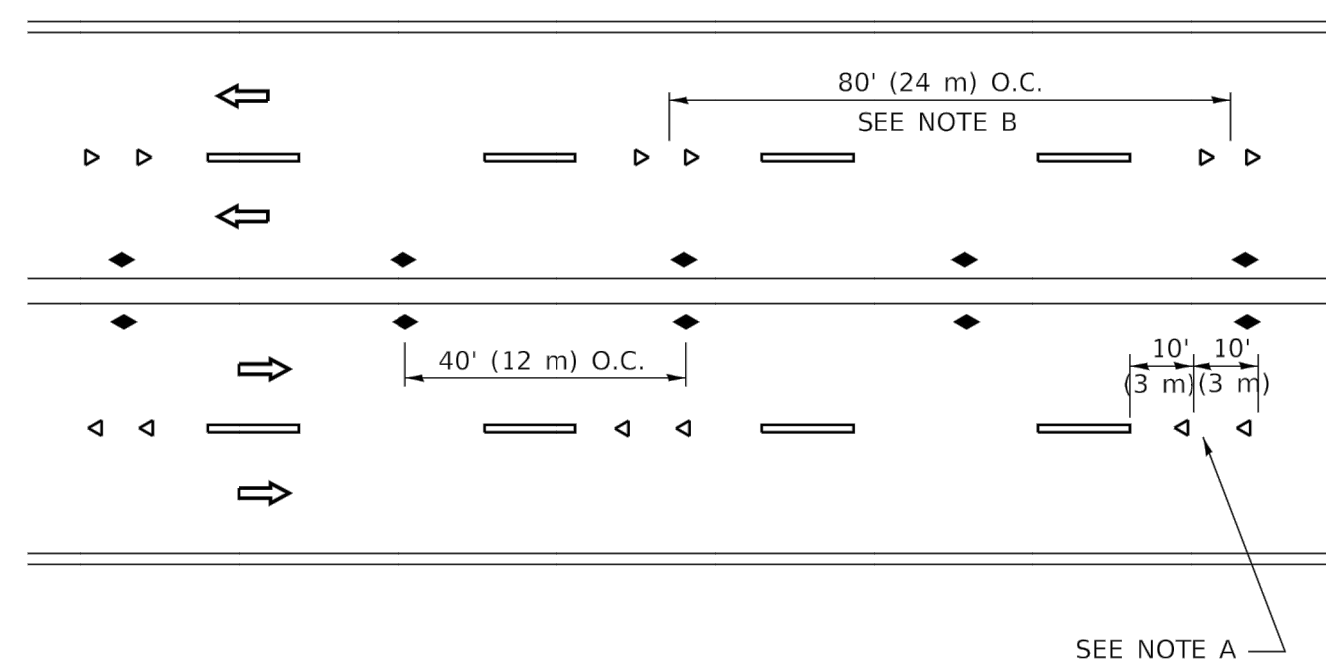
TWO-LANE/TWO-WAY



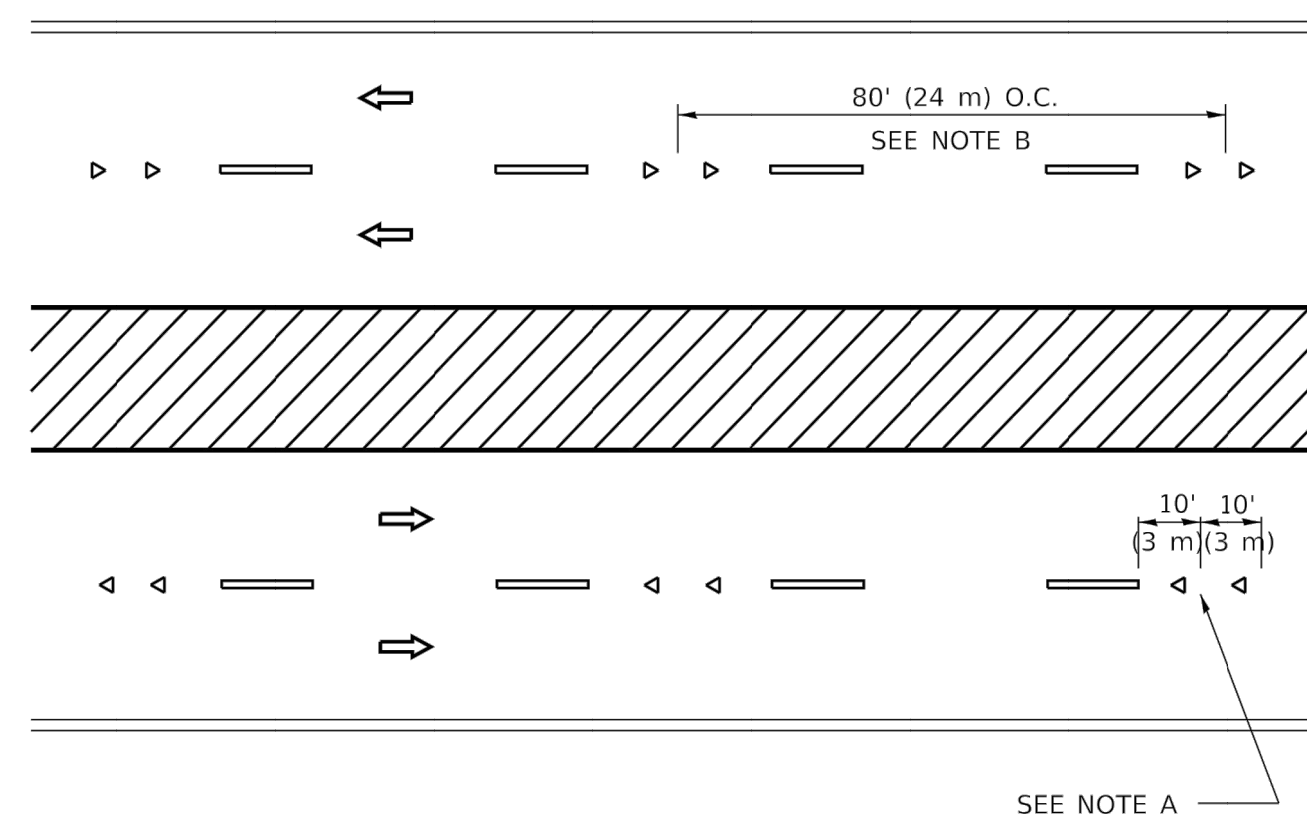
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

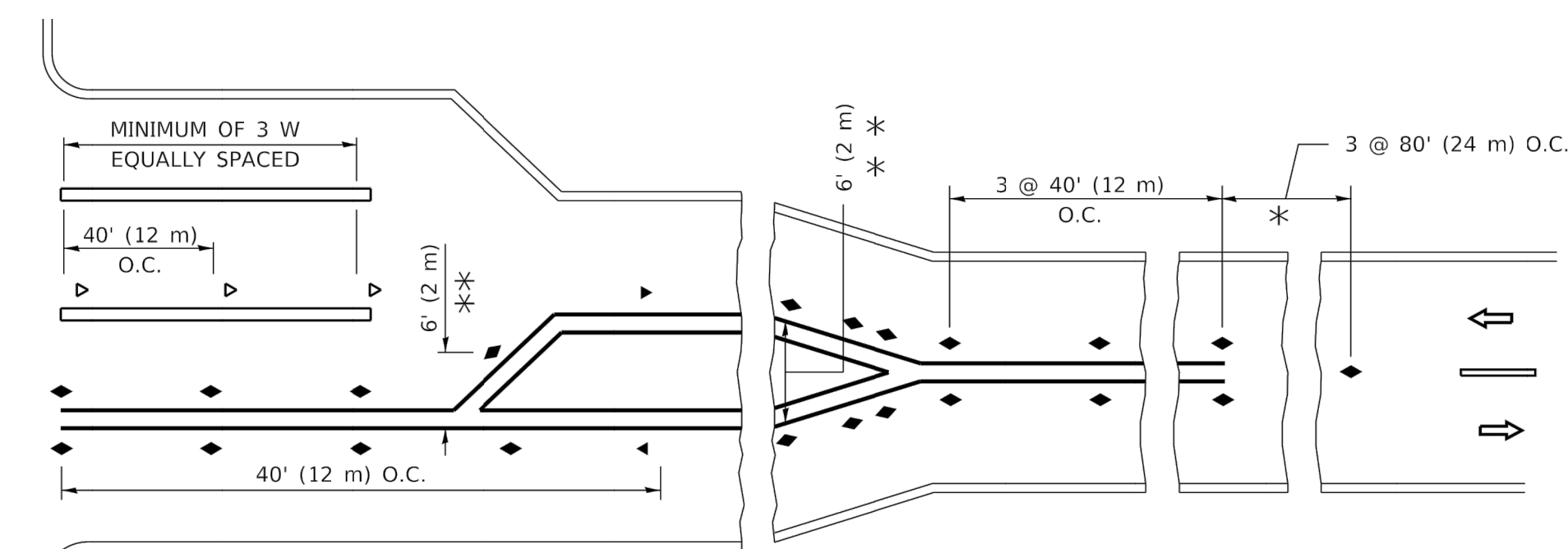
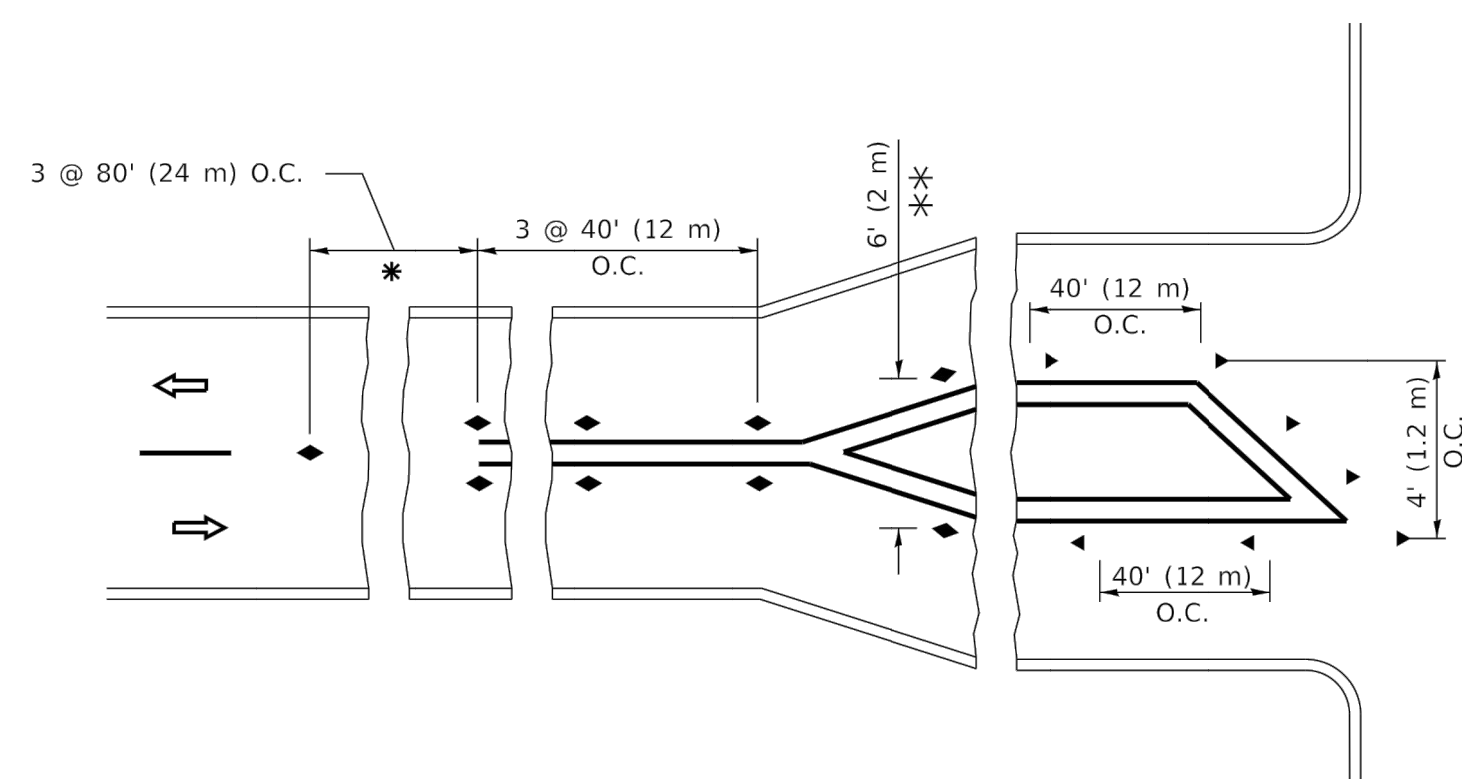
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

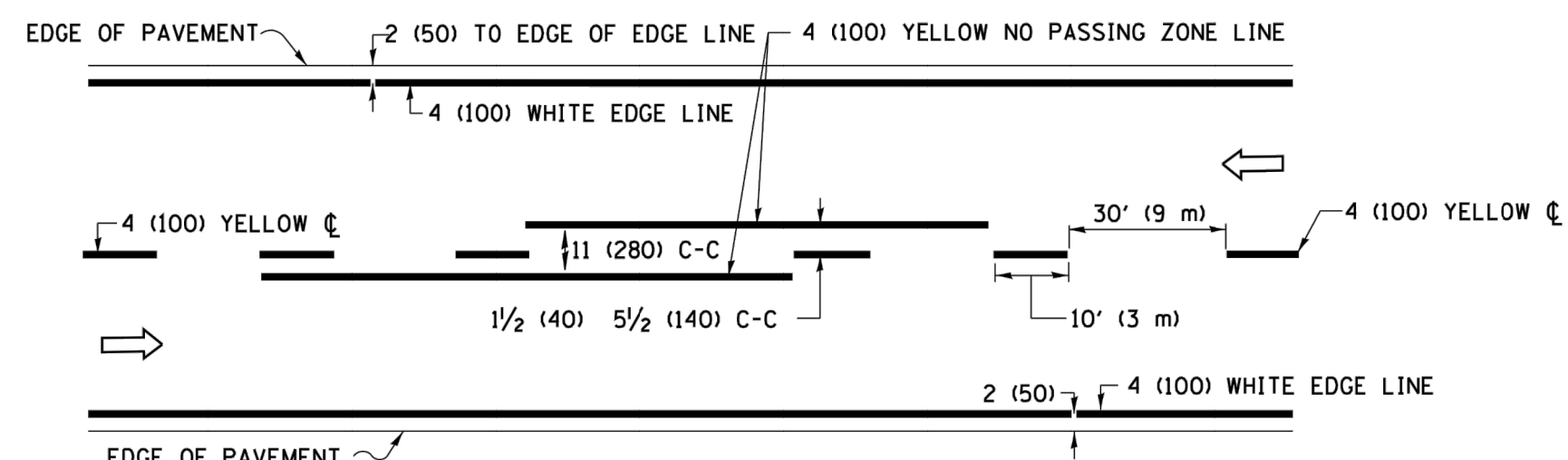
FILE NAME = 18R0708-DTLS-01 - IDOT P01 (5)	USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
		DRAWN -	REVISED - T. RAMMACHER 01-06-00
	PLOT SCALE = 50.0000" / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09
	PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13

STATE OF ILLINOIS
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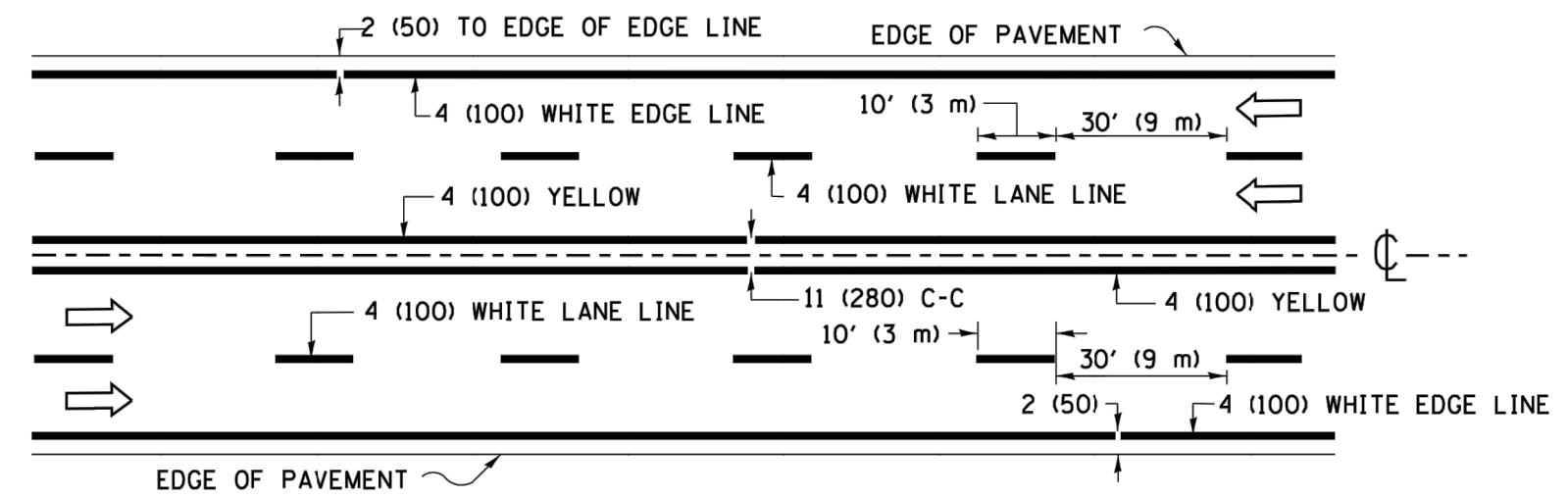
TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: SHEET NO. 22 OF 27 SHEETS STA. TO STA.

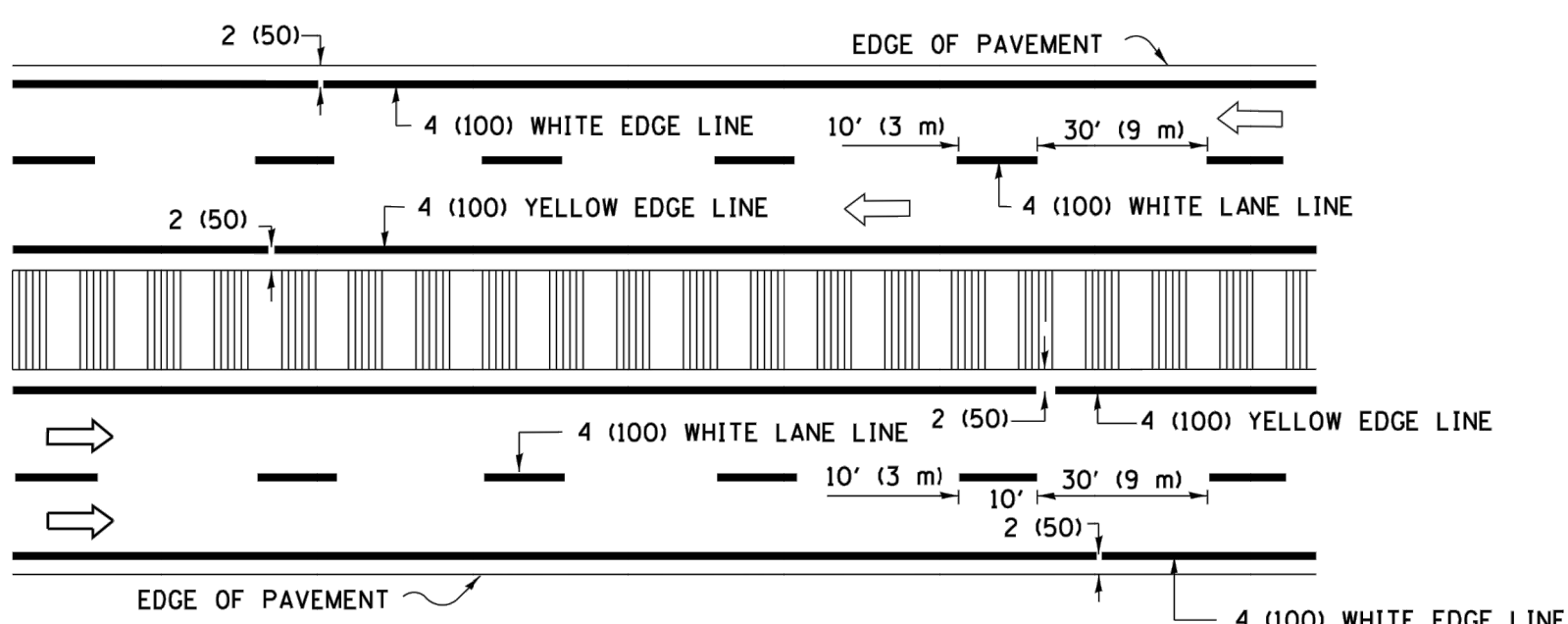
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	22
TC-11		CONTRACT NO. 61G50		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT JHVC (074)		



2-LANE ROADWAY

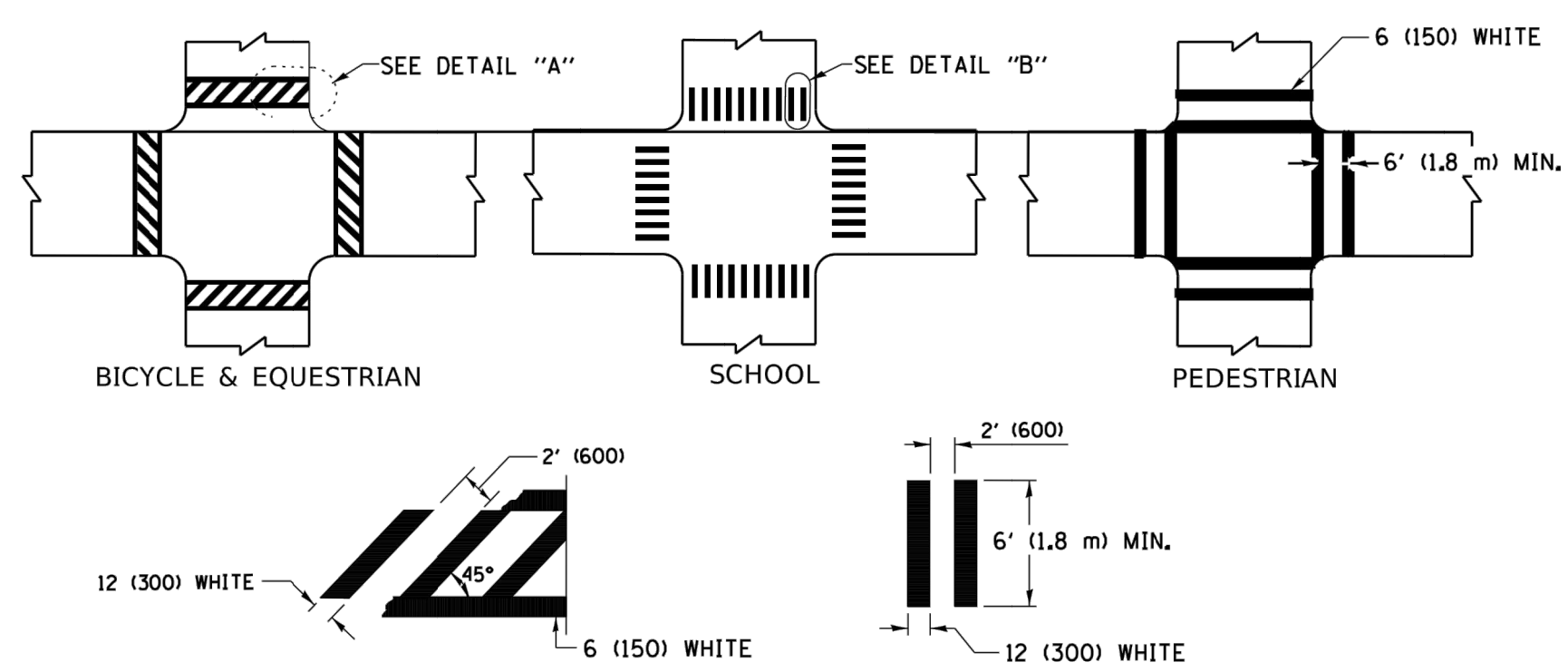


MULTI-LANE UNDIVIDED



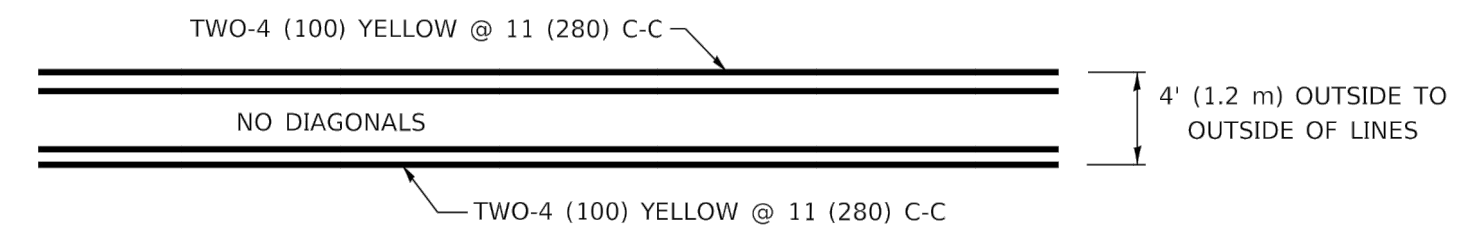
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

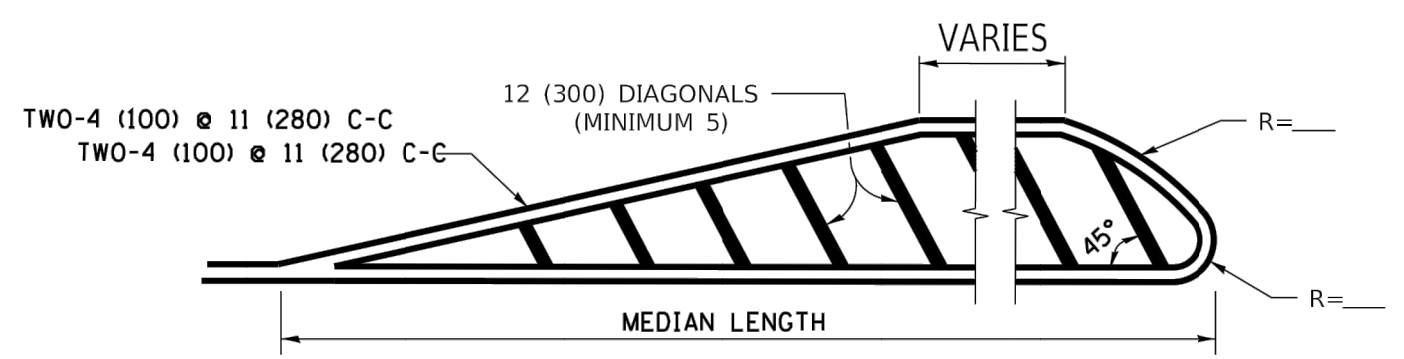


TYPICAL CROSSWALK MARKING

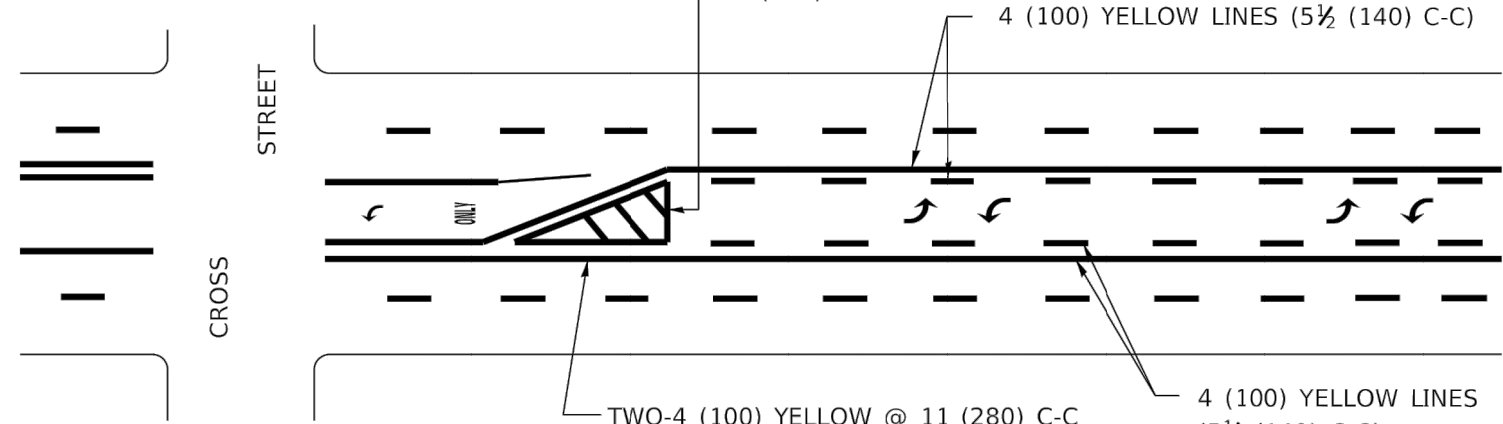
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



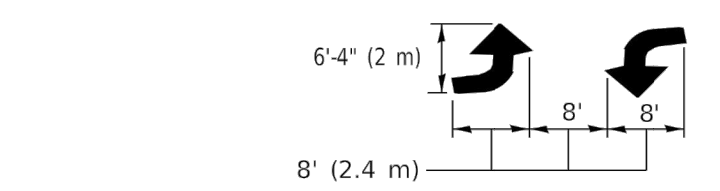
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



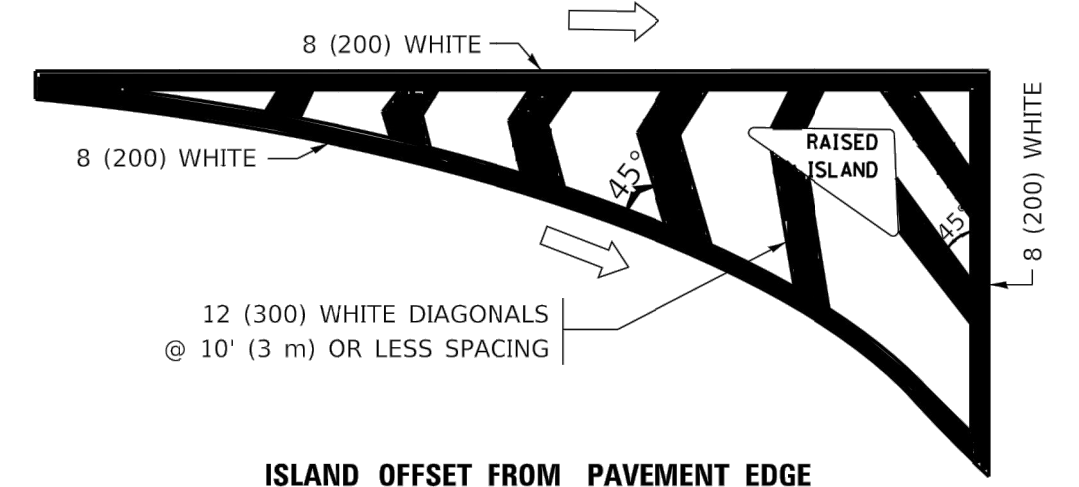
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING



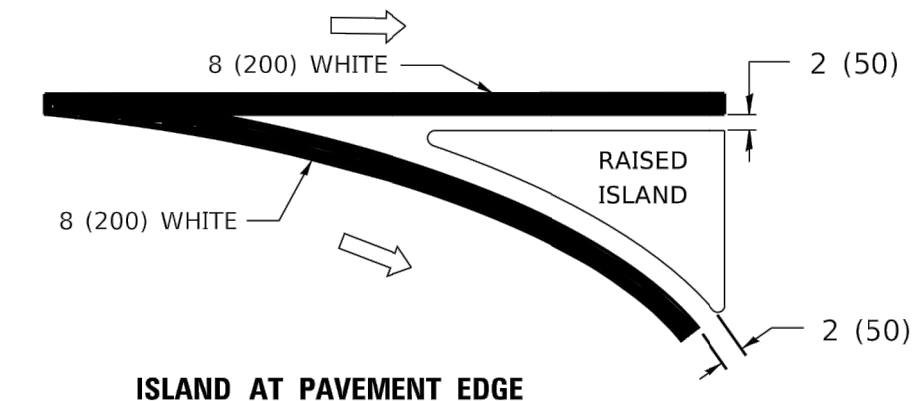
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

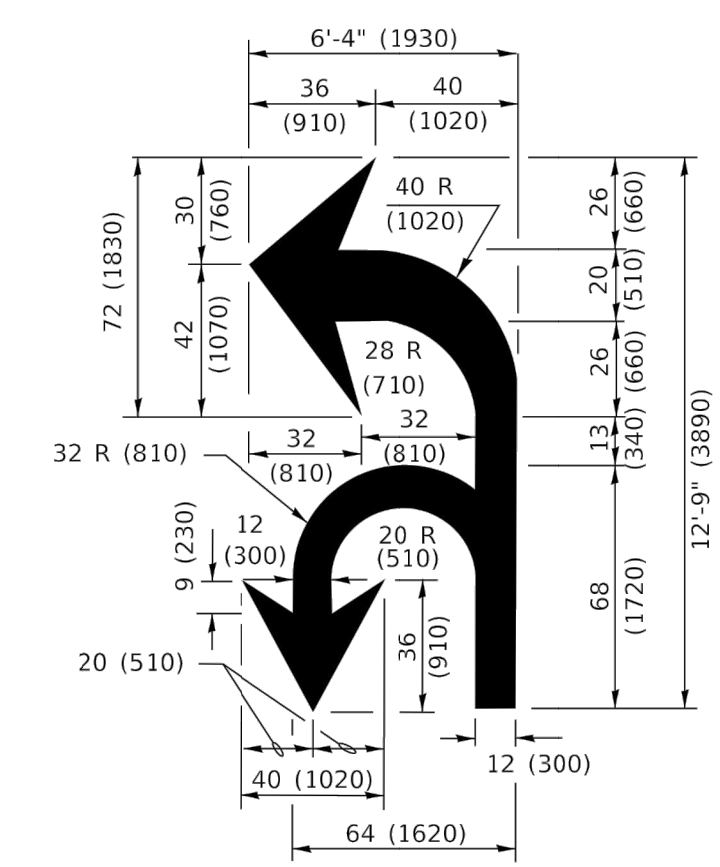


ISLAND OFFSET FROM PAVEMENT EDGE

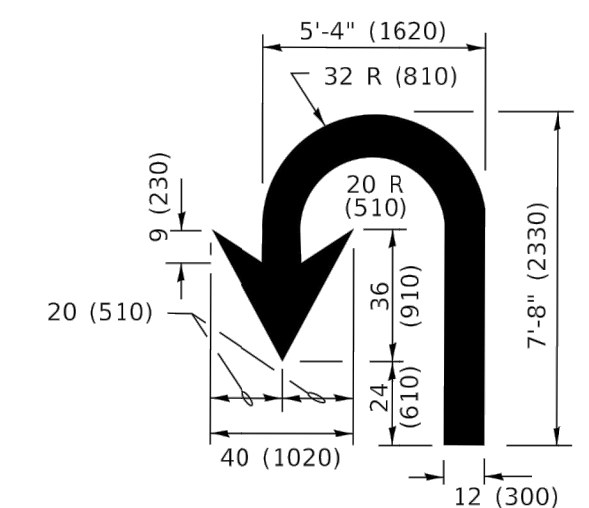


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
 * LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

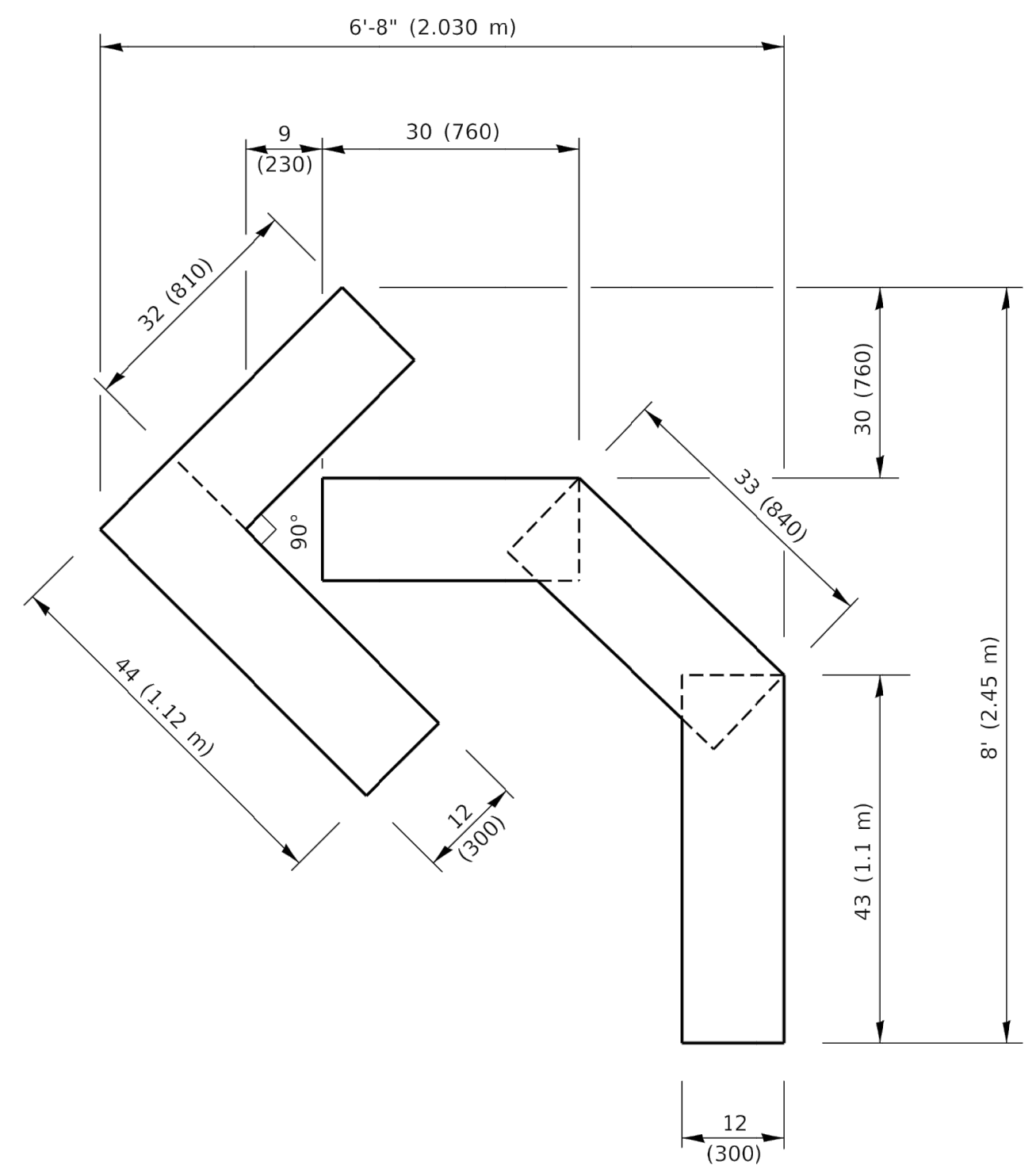
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = 18R0708-DTLS-01 - IDOT P01 (8)	USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
		DRAWN -	REVISED - C. JUCIUS 07-01-13
	PLOT SCALE = 50.0000" / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

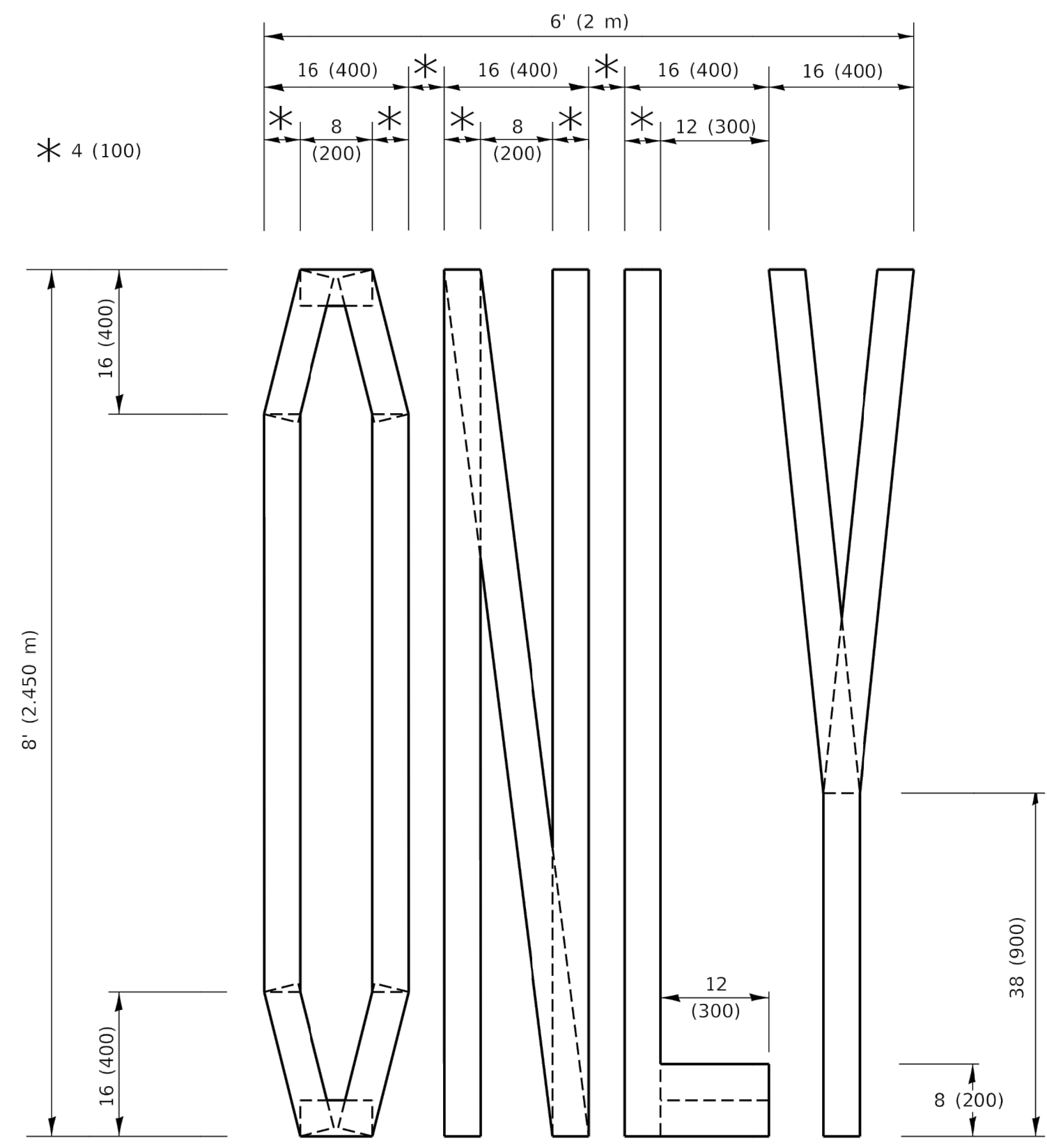
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		3598	17-00110-00-RS	COOK	27	23
SCALE:		SHEET NO. 23 OF 27 SHEETS		STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)	

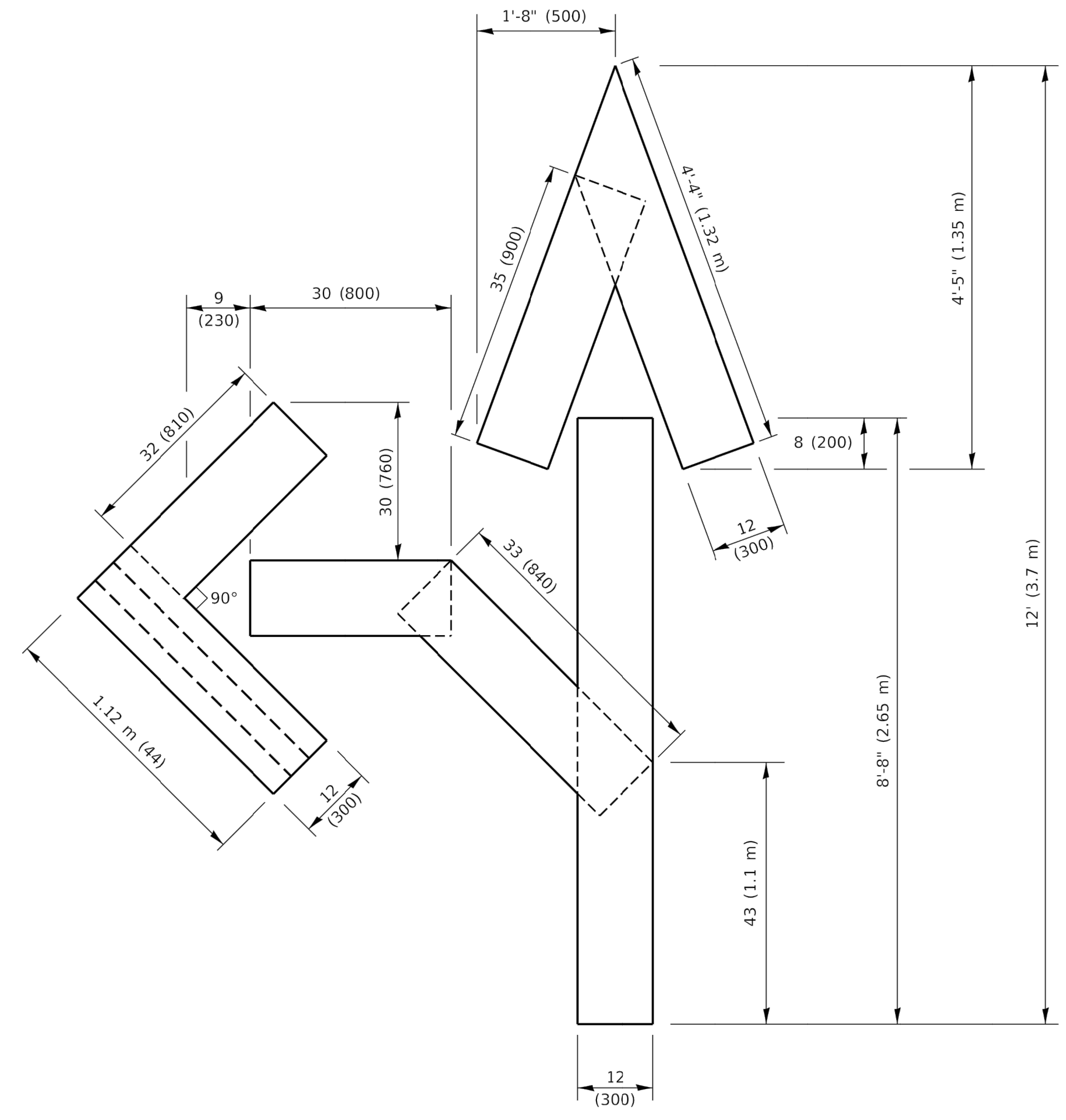
DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		3598	17-00110-00-RS	COOK	27	23
SCALE:		SHEET NO. 23 OF 27 SHEETS		STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)	



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

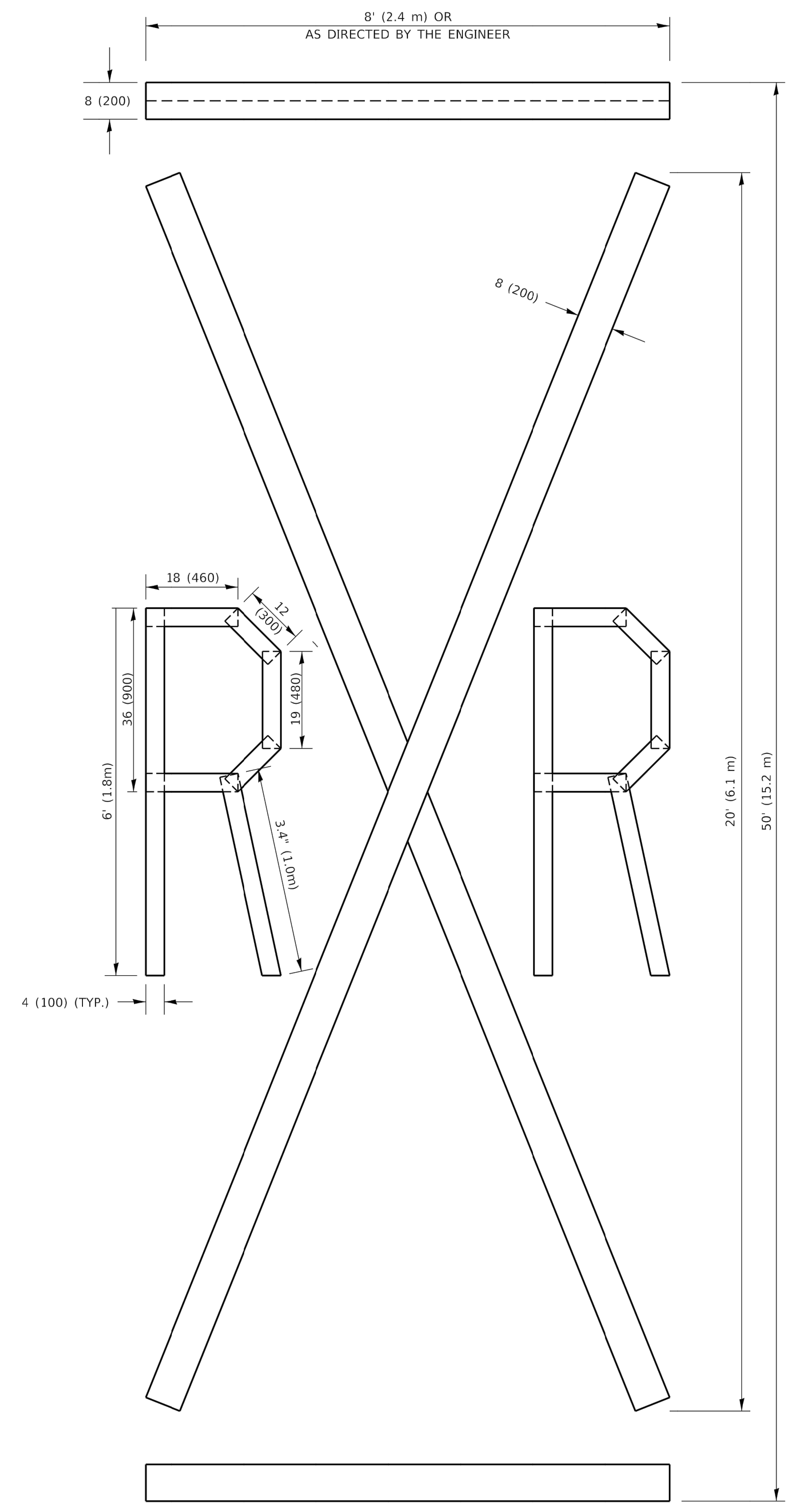


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = 18R0708-DTLS-01 - IDOT P01 (7)

USER NAME = footem
 PLOT SCALE = 50.0068 "/in.
 PLOT DATE = 3/4/2019

DESIGNED -
 DRAWN -
 CHECKED -
 DATE - 09-18-94

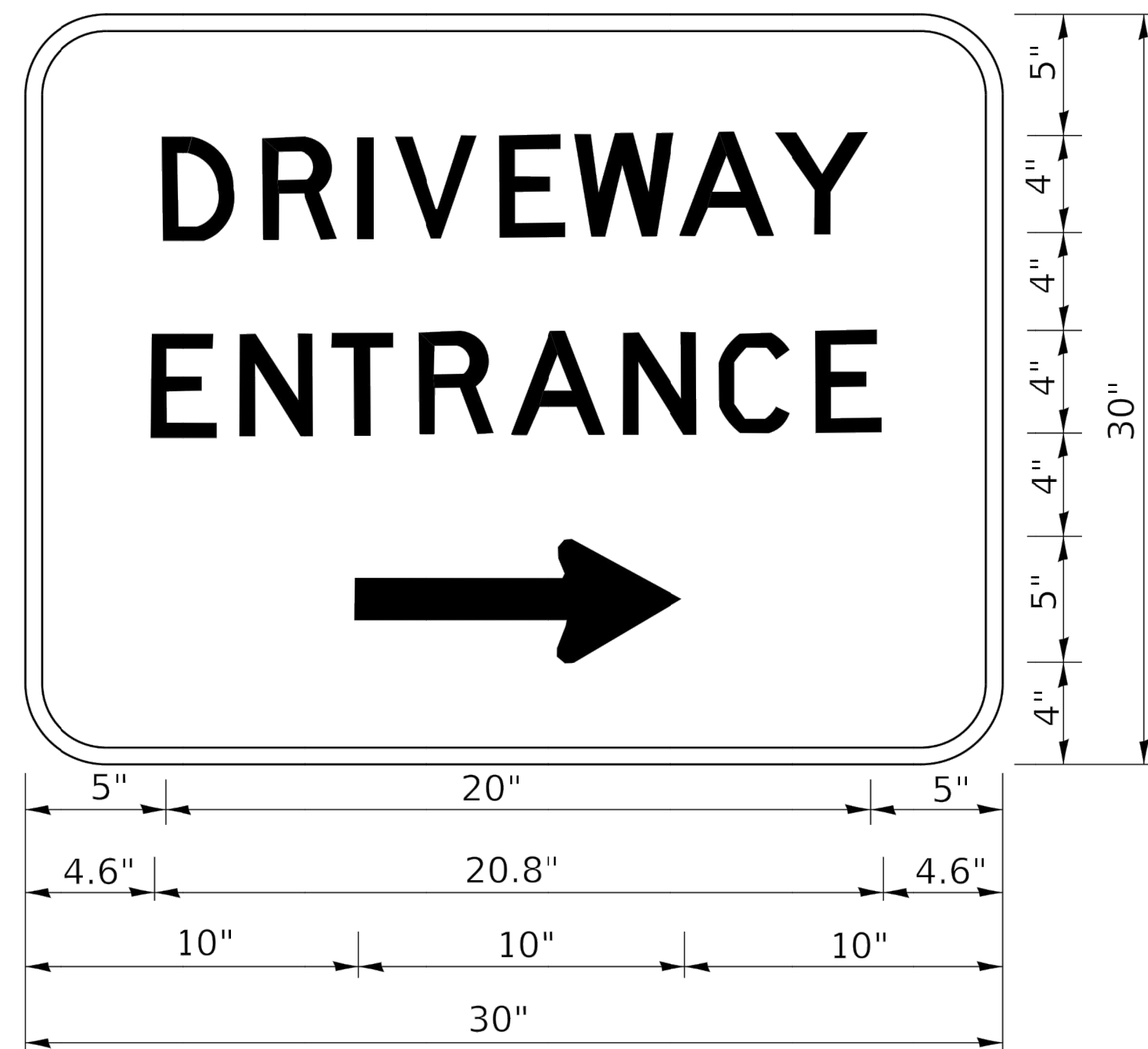
REVISED - T. RAMMACHER 03-02-98
 REVISED - E. GOMEZ 08-28-00
 REVISED - E. GOMEZ 08-28-00
 REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: SHEET NO. 24 OF 27 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	24
TC-16		CONTRACT NO. 61G50		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT JHVC (074)				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = 18R0708-DTLS-01 - IDOT P01 (10)	USER NAME = footemj	DESIGNED -	REVISED - C. JUCIUS 02-15-07
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

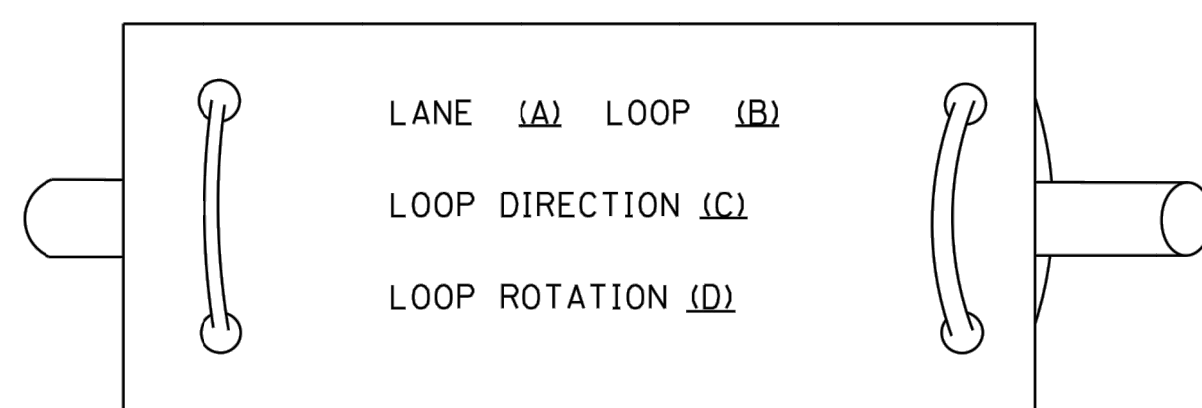
SCALE: SHEET NO. 25 OF 27 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	25
TC-26			CONTRACT NO. 61G50	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT JHVC (074)		

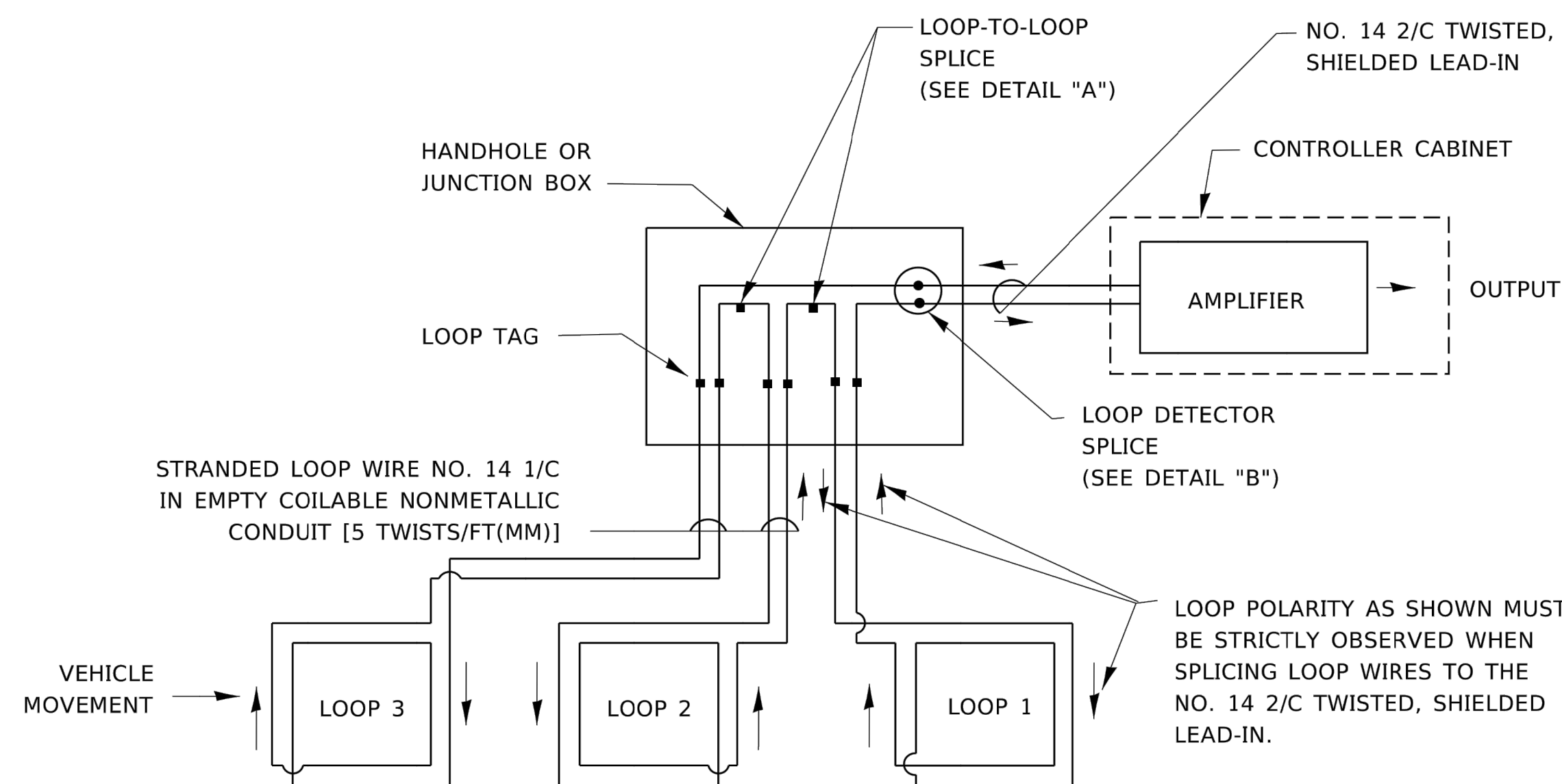
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

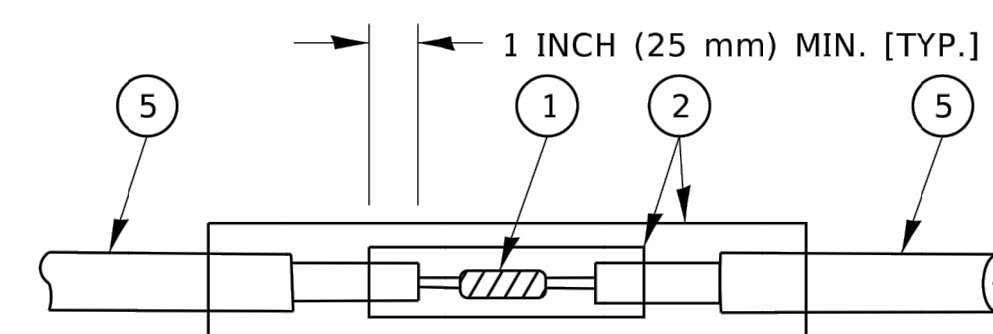


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

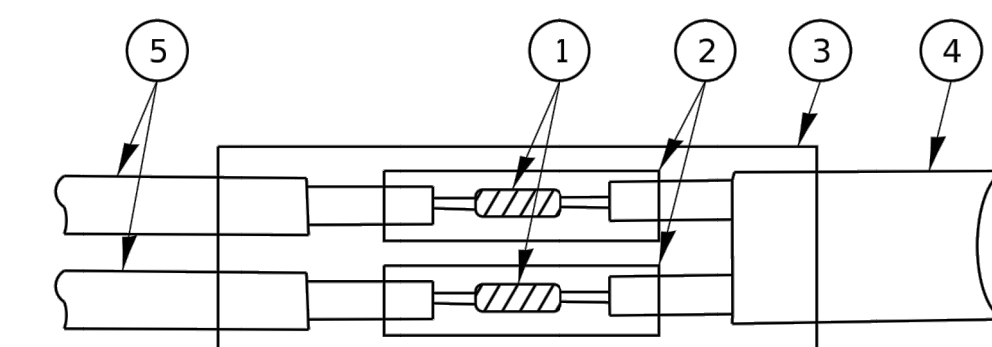


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

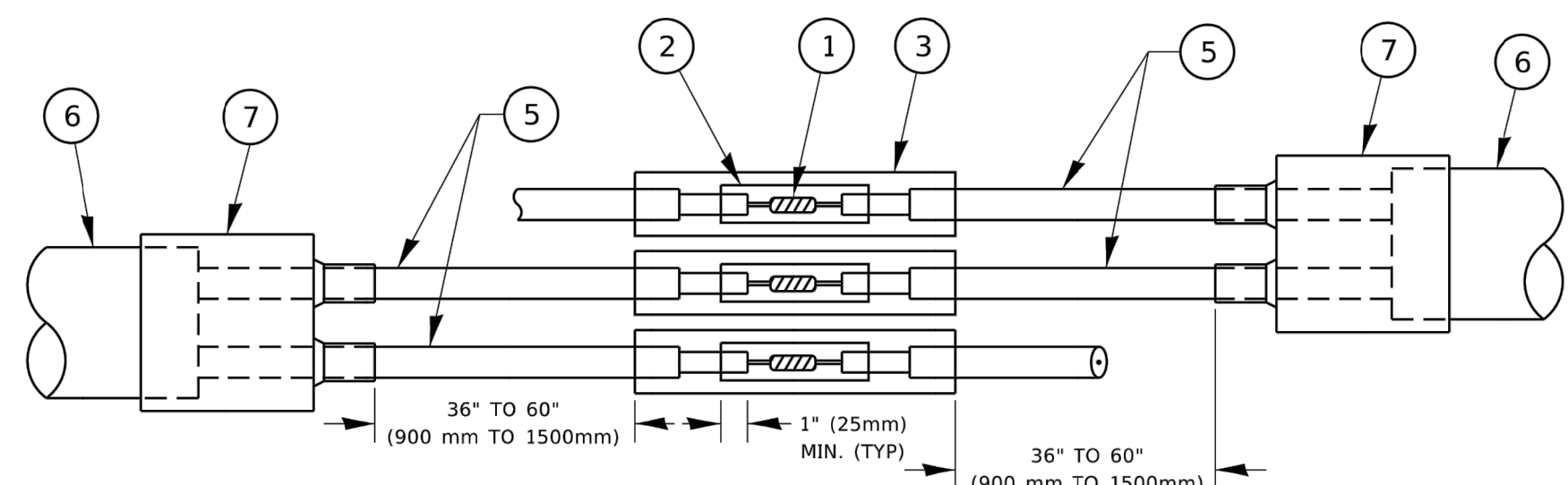


DETAIL "A"
LOOP-TO-LOOP SPLICE

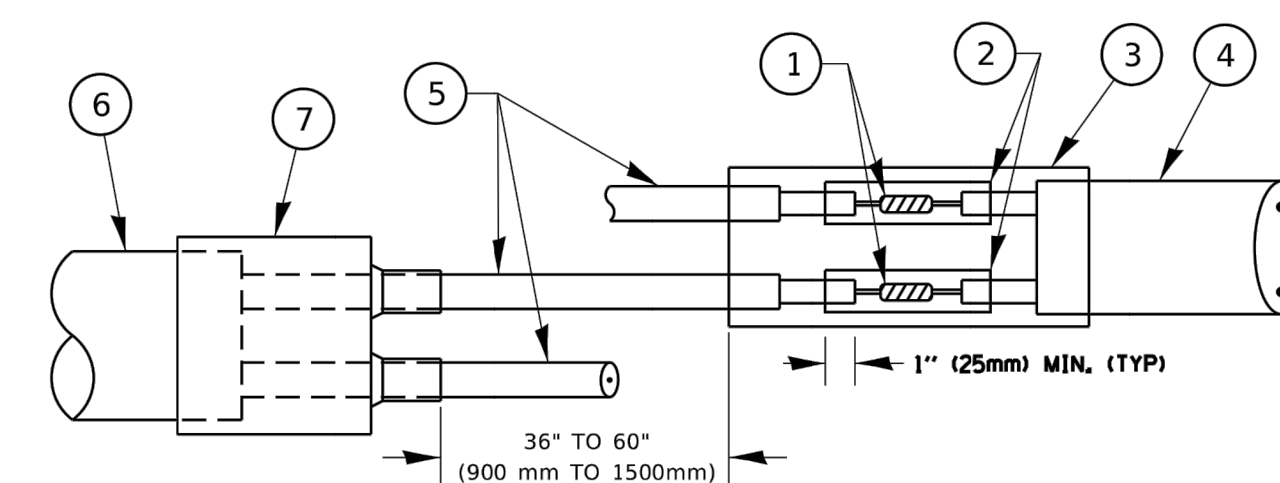


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- ⑥ XL POLYOLEFIN 2 CONDUCTOR
- ⑦ BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME = 18R0708-DTLS-01 - IDOT P01 (8)
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USER NAME = footemj
PLOT SCALE = 50.0000 ' / in.
PLOT DATE = 3/4/2019

DESIGNED -	REVISD -
DRAWN -	REVISD -
CHECKED -	REVISD -
DATE -	REVISD -

DESIGNED -	REVISD -
DRAWN -	REVISD -
CHECKED -	REVISD -
DATE -	REVISD -

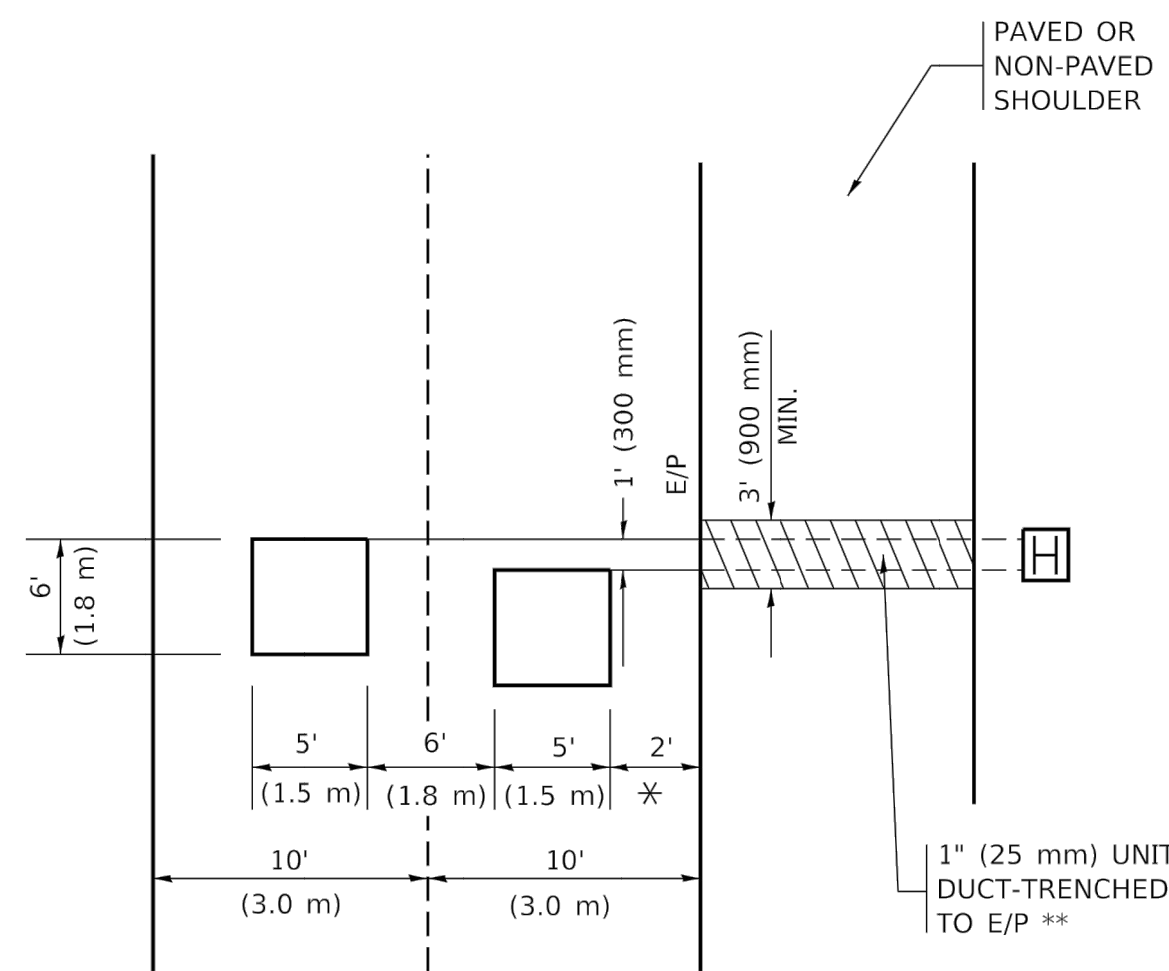
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE:	SHEET NO. 26 OF 27 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3598	17-00110-00-RS	COOK	27	26
TS-05		CONTRACT NO. 61G50		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT JHVC (074)		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

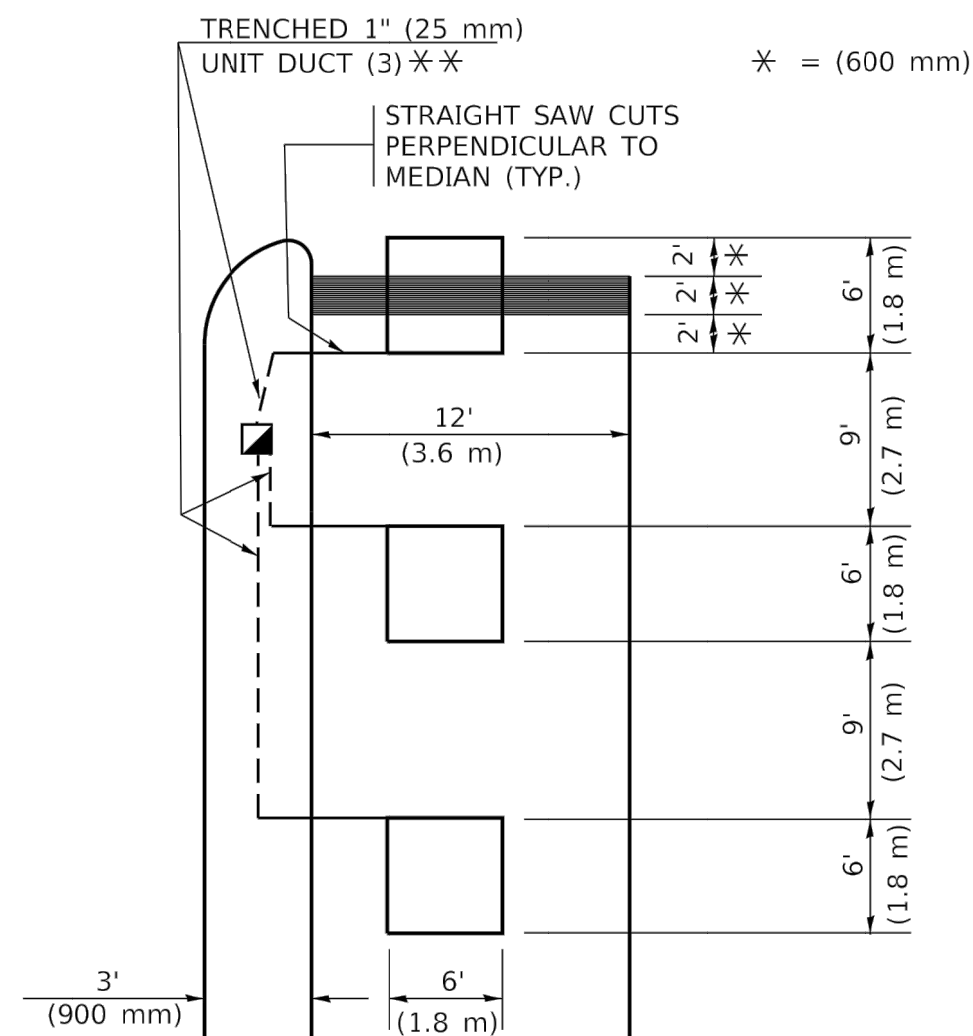


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

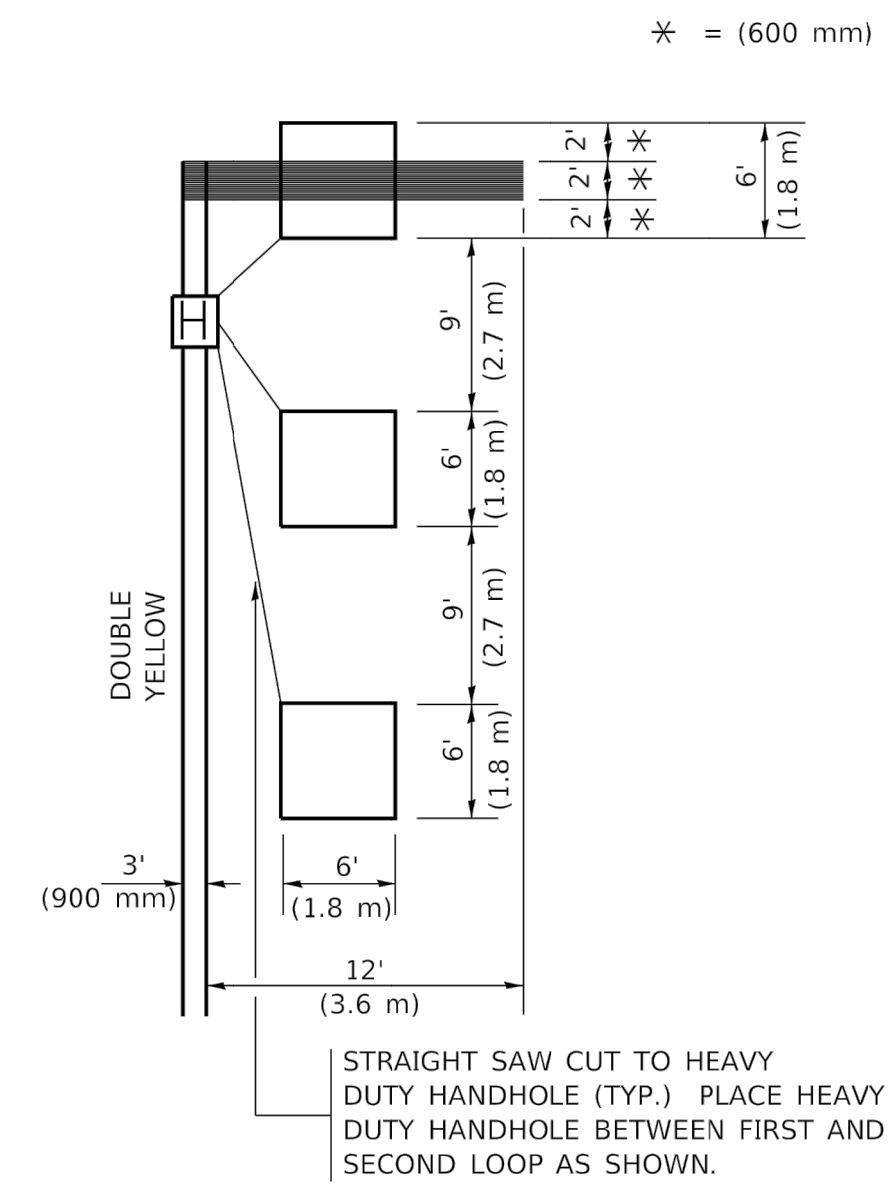


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

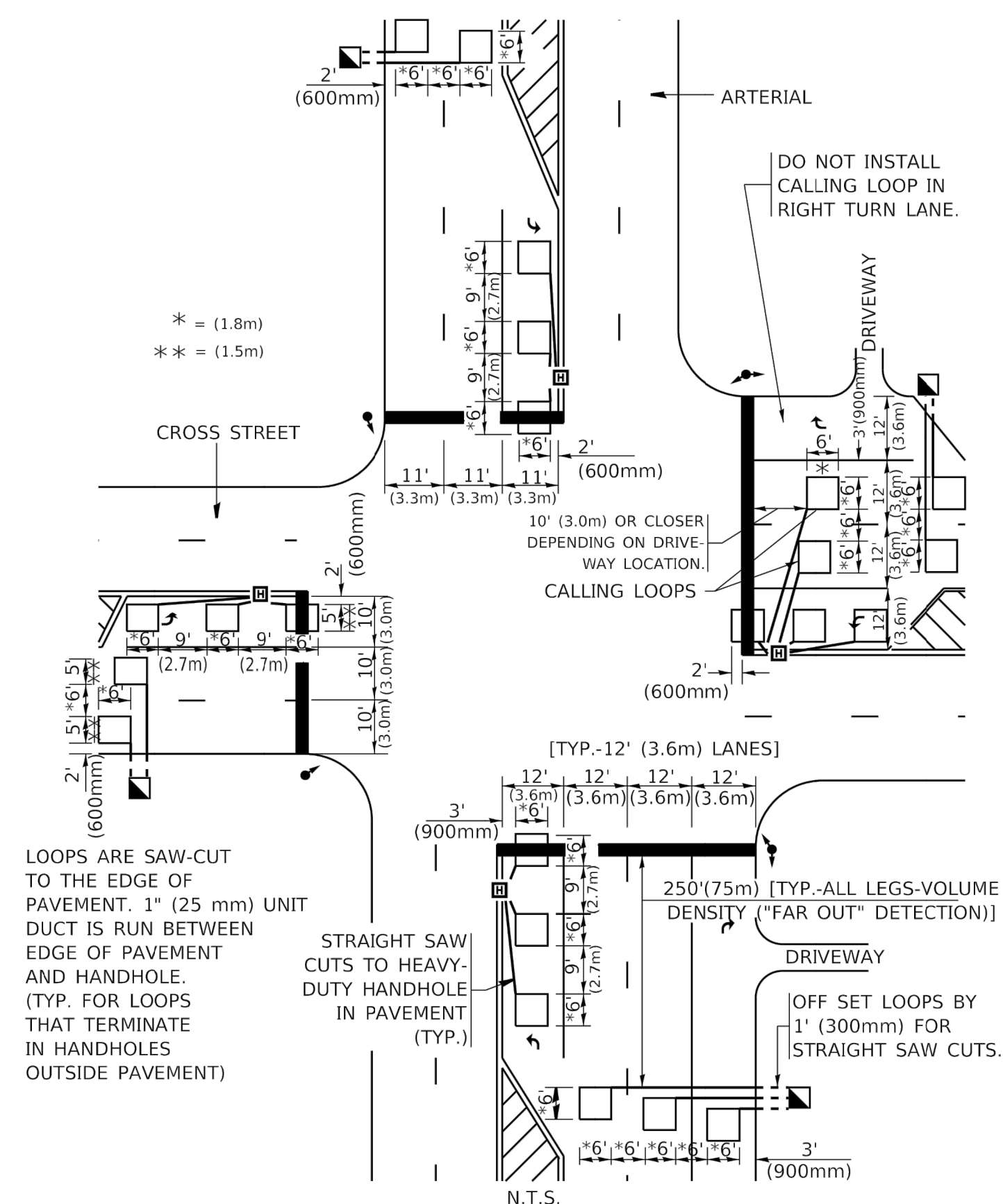
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



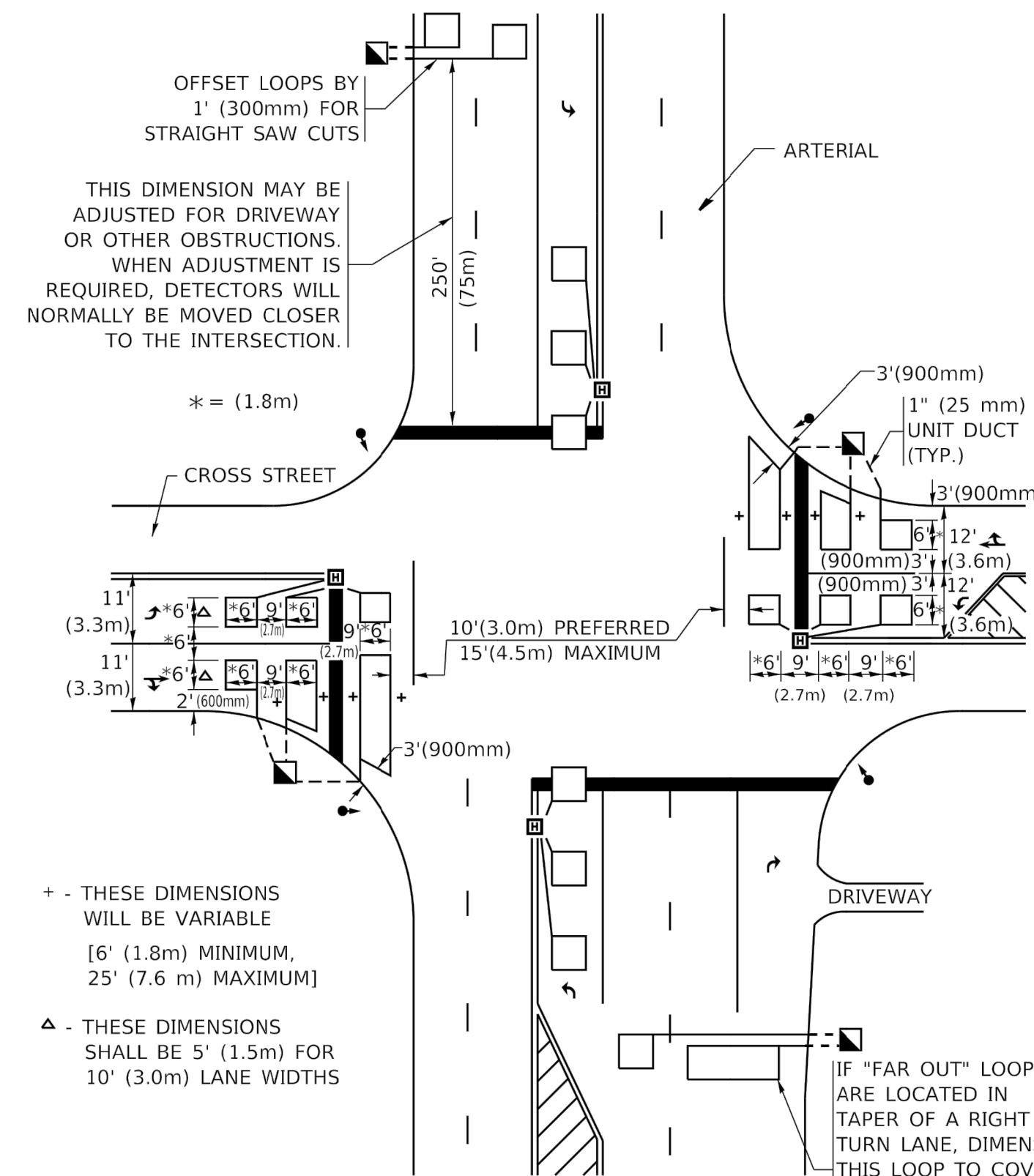
* = (1.8m)
 ** = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
 N.T.S.

FILE NAME = 18R0708-DTLS-01 - IDOT P01 (9)	USER NAME = footem	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 50.0000' / in.	CHECKED - R.K.F.	REVISED -	REVISED -			3598	17-00110-00-RS	COOK	27	27	
PLOT DATE = 3/4/2019	DATE -	REVISED -	REVISED -			TS-07		CONTRACT NO. 61G50			
						FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	JHVC (074)		
					SCALE:	SHEET NO. 27	OF 27 SHEETS	STA.	TO STA.		