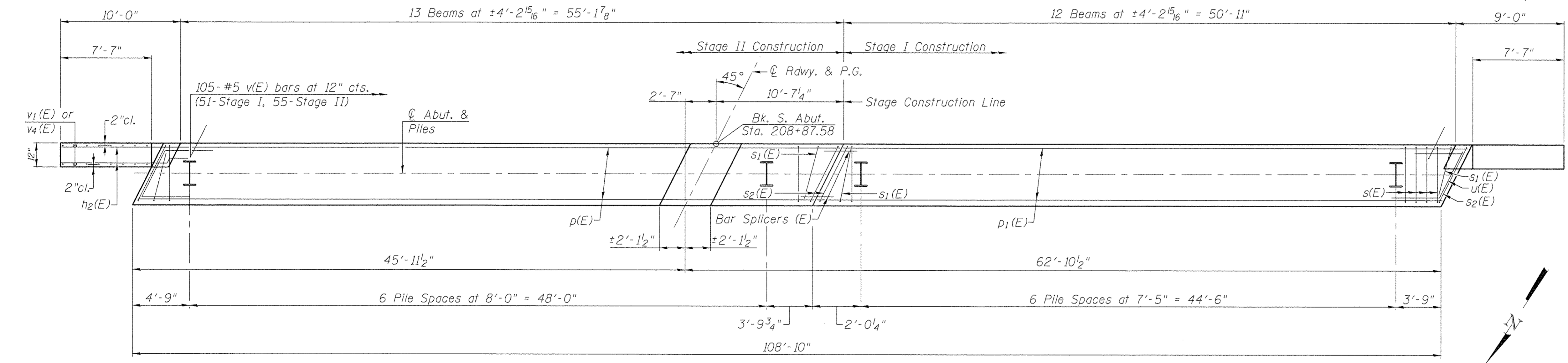
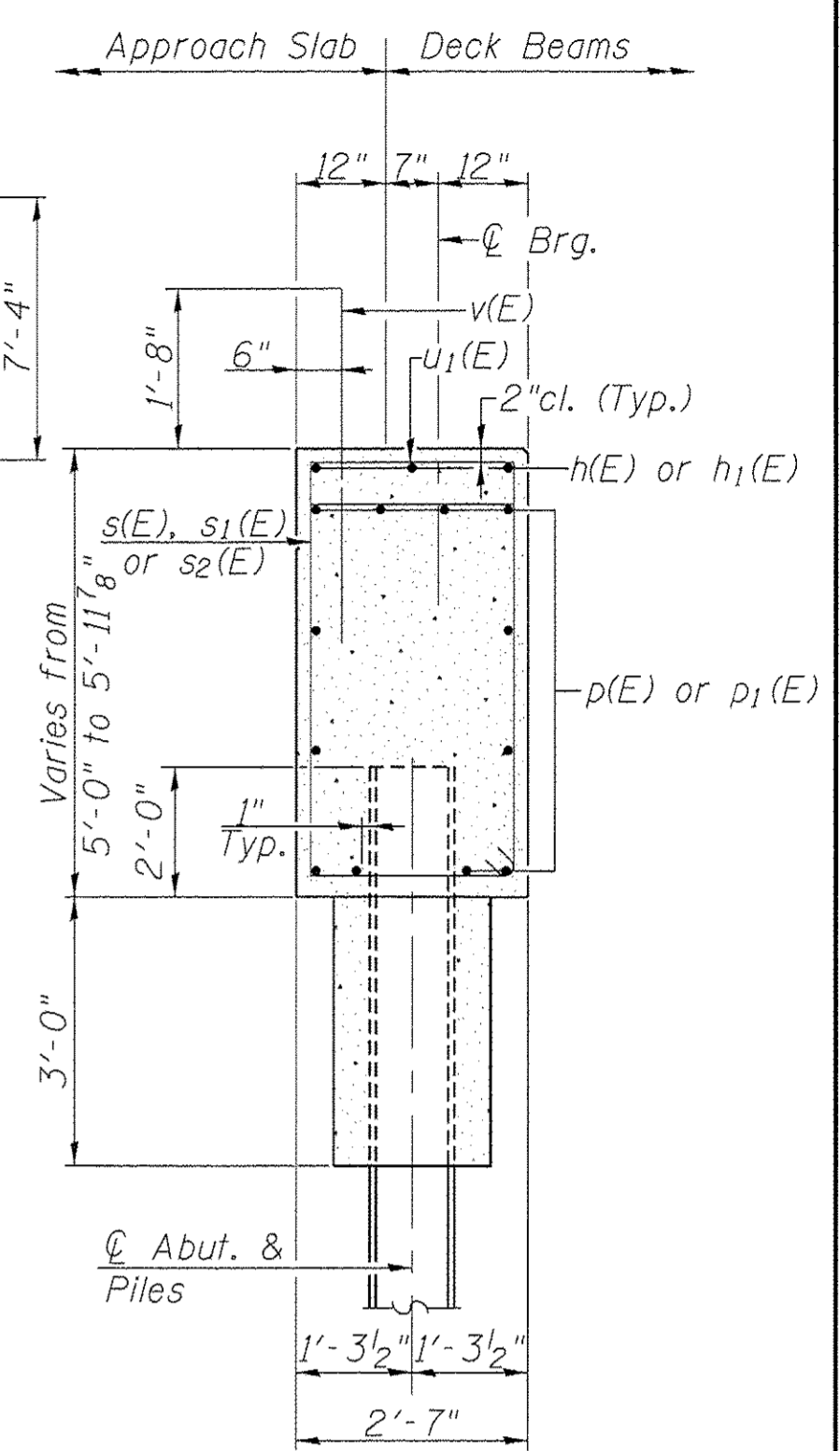
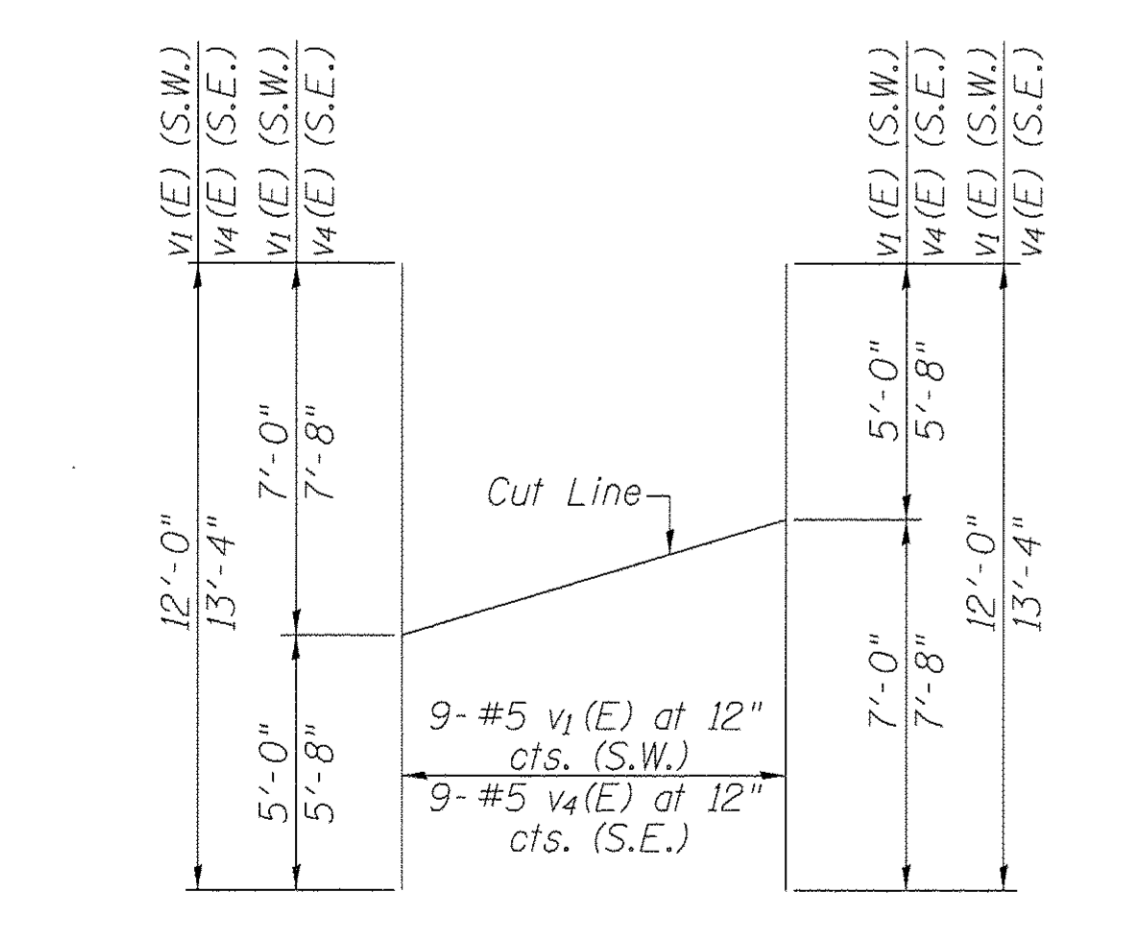


Piles located within the Stage I Construction limits shall not be driven prior to the de-energizing of the aerial electrical lines (hereafter known as "utility adjustment") located along the west side of Chatham Road. The electrical lines are owned and maintained by City, Water, Light and Power (CWLP). CWLP shall be responsible for all work required for the utility adjustment. The Contractor shall coordinate and accommodate the utility adjustment with CWLP. Rob Cunningham of CWLP shall be contacted a minimum of two months prior to driving piles at 217-757-8520. All costs associated with the utility adjustment shall not be paid for separately but shall be included in the cost of Driving Piles.

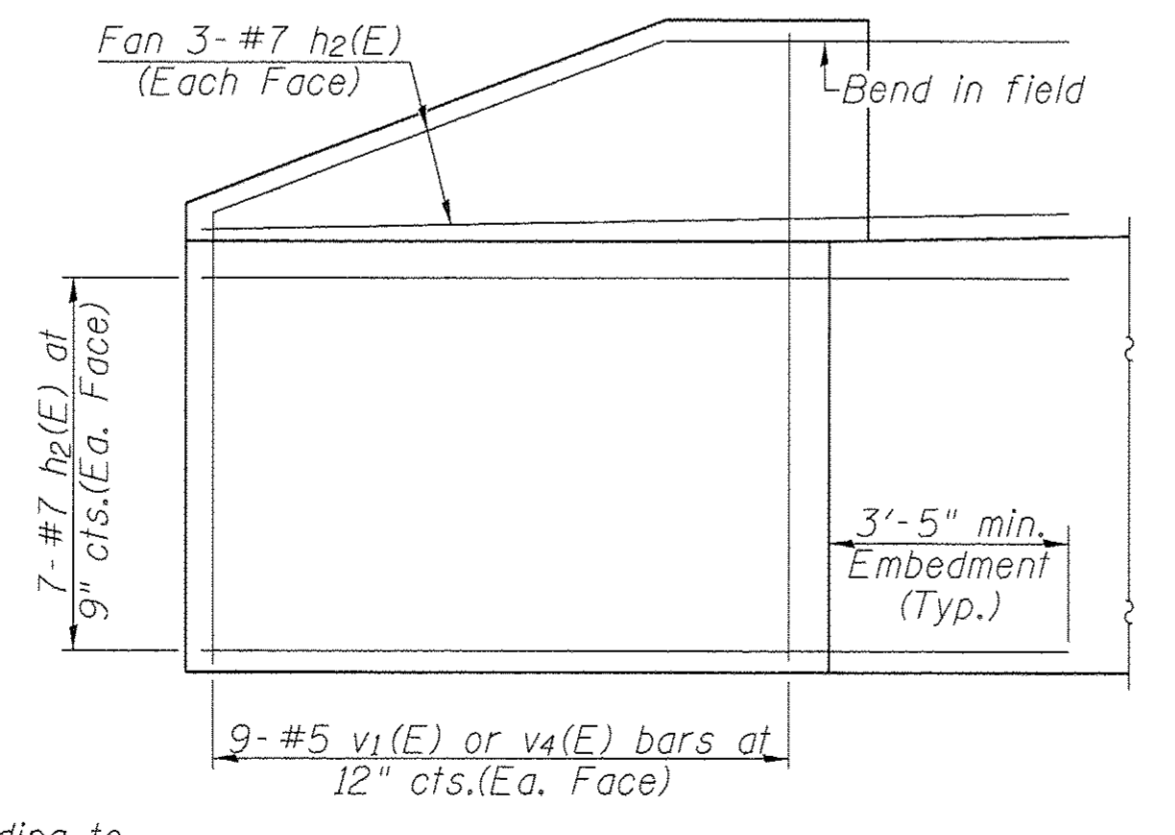
**PILE DATA**  
 Type: Steel HP 12x63  
 Nominal Required Bearing: 497 Kips  
 Factored Resistance Available: 260 Kips  
 Estimated Pile Length: 66'  
 Number of Production: 13  
 Number of Test Piles: 1\*\*\*  
 \*\*\* Driven to 110% of the Nominal Required Bearing



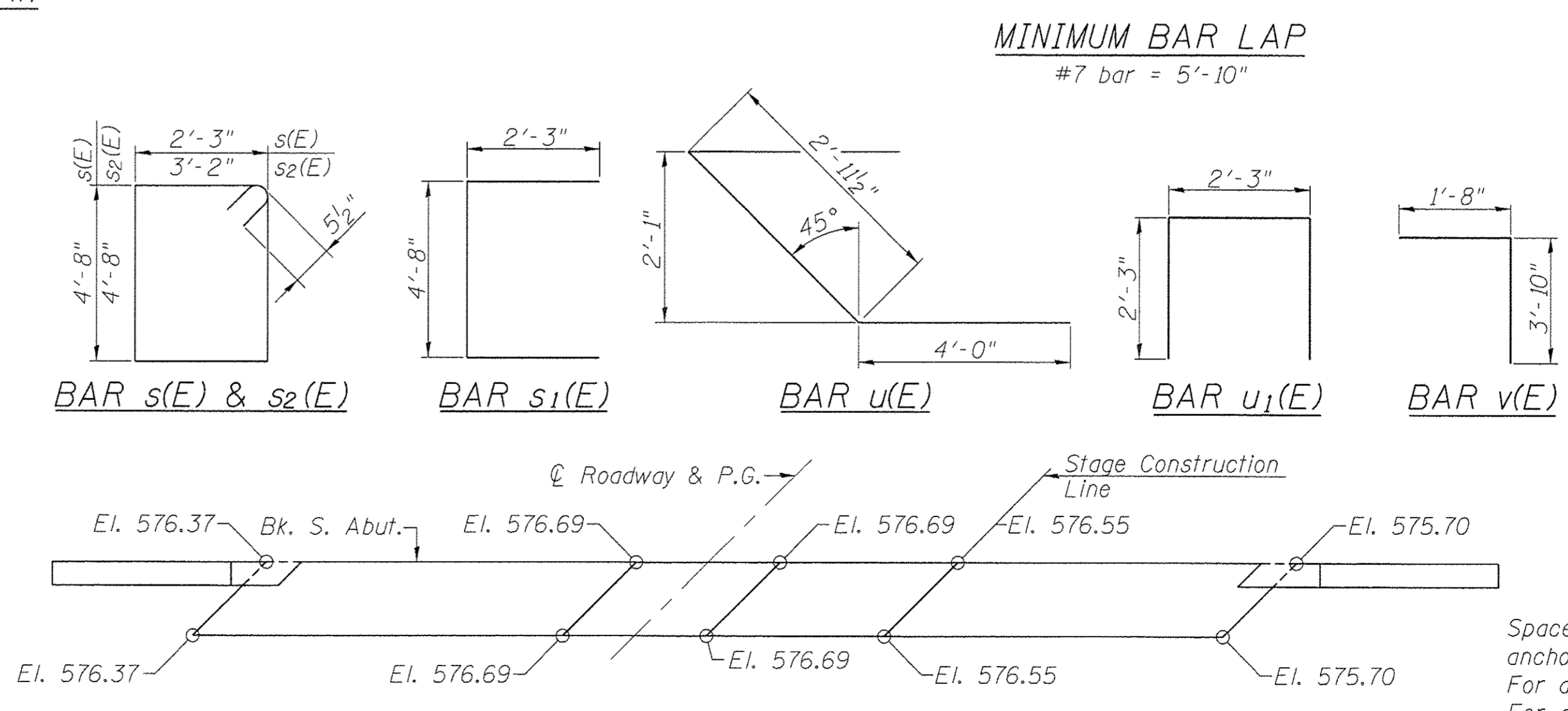
**SECTION THRU ABUT.**  
 (Dimensions are at Rt. L's)  
**SOUTH ABUTMENT**  
**BILL OF MATERIAL**



**v1(E) & v2(E) - BAR CUT DIAGRAM**  
 Order v1(E) & v4(E) bars full length. Layout in field according to diagram. Cut v1(E) & v4(E) bars along cut line. Use remainder of each bar in opposite face.



**WINGWALL ELEVATION**  
 (Showing Reinforcement)  
 See v1(E) & v4(E) bar cut diagram



**\* BEARING SEAT DIAGRAM**

Space reinforcement in cap to miss beam anchor dowels.  
 For details of Bar Splicers, see sheet 25 of 28.  
 For details of piles and Concrete Encasement, see sheet 24 of 28.