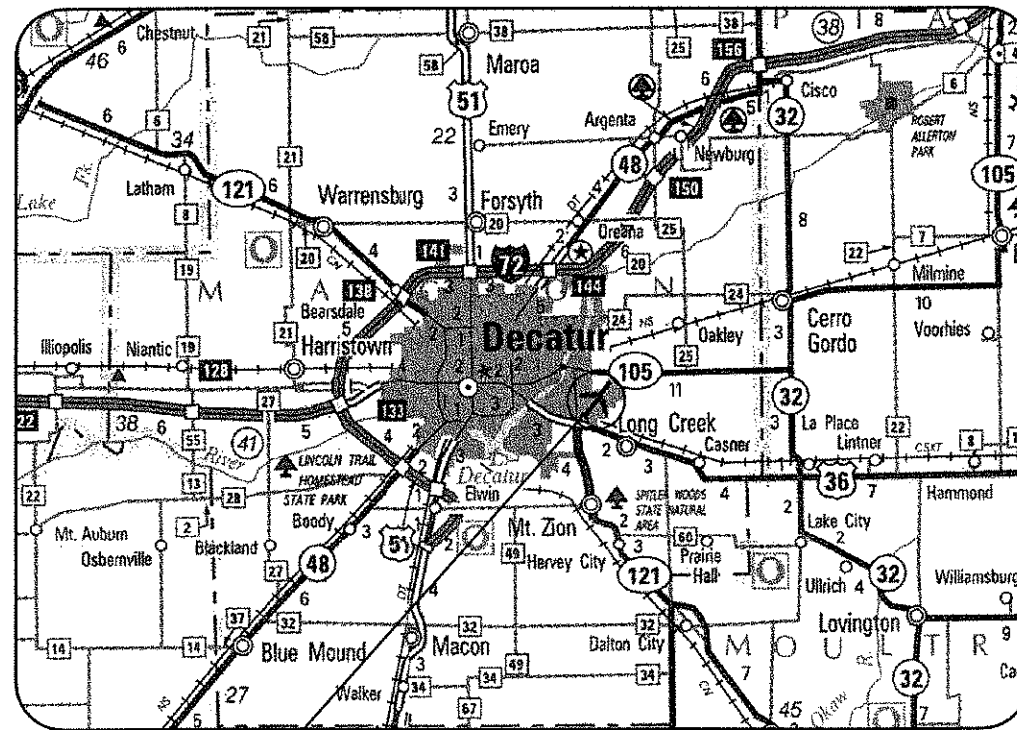


# CONSTRUCTION PLANS FOR DECATUR AIRPORT DECATUR, MACON COUNTY, ILLINOIS REHABILITATE TERMINAL BUILDING AUTO PARKING LOT

### SCOPE OF WORK

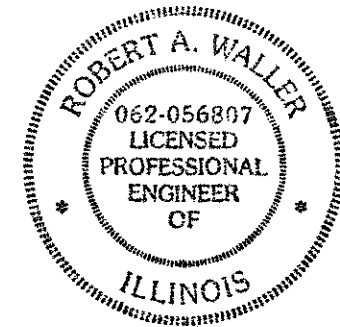
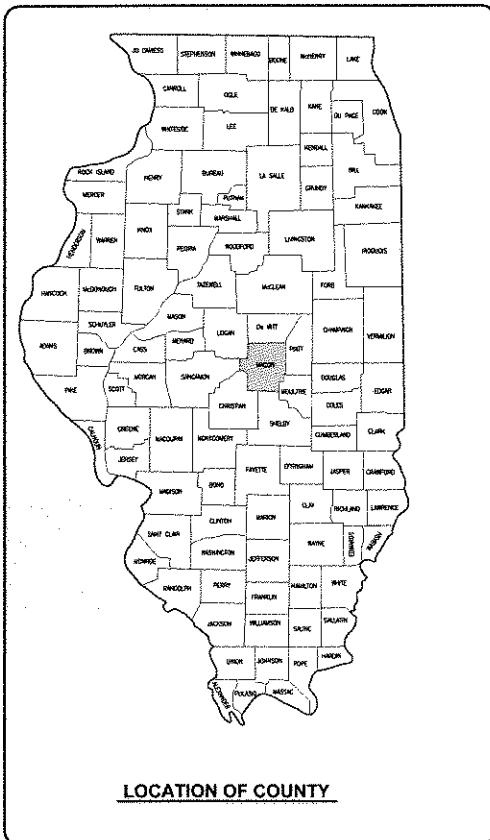
THIS PROJECT CONSISTS OF THE REHABILITATION OF APPROXIMATELY 15,300 SQ. YARDS OF BITUMINOUS PAVEMENT IN THE TERMINAL BUILDING AUTO PARKING LOT. THE REHABILITATION WILL CONSIST OF 2" BITUMINOUS PAVEMENT MILLING AND THE CONSTRUCTION OF A 2" BITUMINOUS SURFACE COURSE LAYER. ASSOCIATED WORK SHALL INCLUDE CRACK CLEANING AND SEALING; BITUMINOUS TACK COAT, REMOVAL, REPAIR AND/OR REPLACEMENT OF CONCRETE CURB, SHOULDER ADJUSTMENT AND PAVEMENT MARKING.



### LOCATION

DECATUR  
AIRPORT

ILL. PROJ.: DEC-4271  
  
LATITUDE: 39° 50' 05"  
LONGITUDE: 88° 51' 59"  
ELEVATION: 682.0' M.S.L.  
DATE: APRIL 4, 2013



Hanson Professional Services Inc.

Submitted by: *[Signature]* ENG'R  
Date Submitted: *5/10/13*  
Lic. Exp. Date: *11/30/2013*

DECATUR PARK DISTRICT

Approved: *[Signature]* AIRPORT DIRECTOR  
Date: *6 May 2013*



DECATUR, ILLINOIS


Revision	
Date	
Drawn	RAW
Checked	RAW
Reviewed	RAW
Scale	NOT TO SCALE
Date	05/03/13

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REHABILITATE  
PARKING LOT  
  
COVER SHEET

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REVISION	DATE



**DECATUR, ILLINOIS**

IL PROJ.: DEC-4271

Hanson Proj. No. 13A0013	File Name C-002-FLP.dwg	Scale NOT TO SCALE	Date 05/03/13
LAYOUT	BRB	03/27/13	
DRAWN	RAD	03/27/13	
REVIEWED	RAW	05/03/13	



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**REHABILITATE  
PARKING LOT**

**SUMMARY OF QUANTITIES  
AND INDEX TO SHEETS**

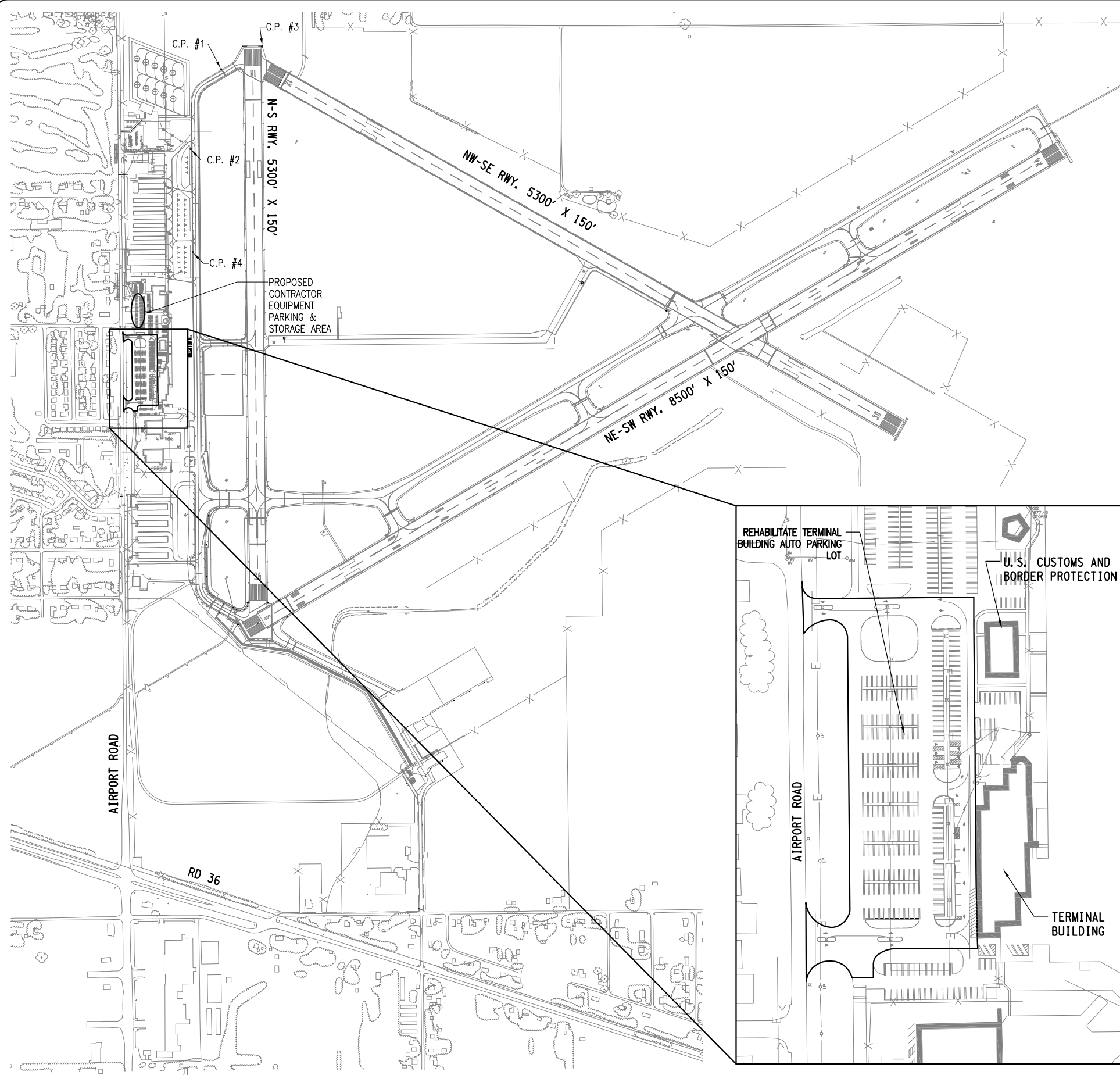
**SUMMARY OF QUANTITIES (BASE BID)**

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150520	MOBILIZATION	L.S.	1	
AR152480	SHOULDER ADJUSTMENT	S.Y.	578	
AR401613	BIT. SURF. CSE. - METHOD I, SUPERPAVE	TON	1,798	
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	15,297	
AR603510	BITUMINOUS TACK COAT	GAL.	2,295	
AR605541	CLEAN & SEAL CRACKS	L.F.	800	
AR620520	PAVEMENT MARKING - WATERBORNE	S.F.	3,365	
AR800571	PAVEMENT MARKING - SPECIAL, HANDICAP SYMBOL	EA.	7	
AR800572	PAVEMENT MARKING - SPECIAL, ARROW - STRAIGHT	EA.	28	
AR910230	HANDICAP SIGN	EA.	7	
SUMMARY OF QUANTITIES (ADDITIVE ALTERNATE NO. 1)				
AS800419	MODIFY TRAFFIC ISLAND	L.S.	1	
SUMMARY OF QUANTITIES (ADDITIVE ALTERNATE NO. 2)				
AT754900	REMOVE CONCRETE CURB	L.F.	367	
AT754920	REPLACE CONCRETE CURB	L.F.	367	
AT800433	REMOVE & REPLACE COMB CURB & GUTTER	L.F.	29	

**INDEX TO SHEETS**

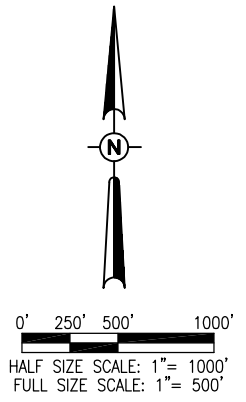
SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	PROJECT LAYOUT
4	PROPOSED SAFETY AND PHASING PLAN
5	PROPOSED PAVEMENT PREPARATION PLAN
6	TYPICAL SECTIONS AND DETAILS
7	PROPOSED CONSTRUCTION PLAN
8	PROPOSED MARKING PLAN
9	PROPOSED MARKING DETAILS

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**LEGEND**

- EXISTING BUILDING
- PROPOSED IMPROVEMENTS
- EXISTING FENCE
- EXISTING STORM SEWER
- EXISTING WATERLINE
- EXISTING SANITARY
- EXISTING TELEPHONE
- EXISTING FIBER OPTIC
- EXISTING GAS
- EXISTING ELECTRIC
- EXISTING STORM INLET/MAHOLE
- EXISTING LIGHT POLE/POLE-MOUNTED LIGHT



HORIZONTAL AND VERTICAL CONTROL DATA				
NO.	DESCRIPTION	NORTHING	EASTING	ELEVATION
1	BM #6 CHISELED SQUARE ON SOUTHEAST CORNER OF CONCRETE FOUNDATION OF TAXIWAY SIGN	---	---	676.13
2	CB-51 CHISELED SQUARE ON SOUTH SIDE OF CONCRETE LID OF DROP INLET, NORTH END OF GRASS MEDIAN	---	---	672.91
3	TP #41 USGS BRASS DISC STAMPED "AP STA A 1965, STA. 171+57, 111' RT. RUNWAY 18-36"	---	---	676.19
4	CB-53 CHISELED ON NORTHEAST CORNER OF CONCRETE LID OF DROP IN APRON	---	---	676.22

**UTILITY NOTE**

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

**PROPOSED SAFETY PLAN**

GENERAL - THE DECATUR AIRPORT IS COMPRISED OF THREE RUNWAYS. THE PROPOSED CONSTRUCTION WILL NOT NECESSITATE CLOSING ANY RUNWAYS.

ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR 150/5370-2F, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE DECATUR AIRPORT AT THE PRE-CONSTRUCTION CONFERENCE.

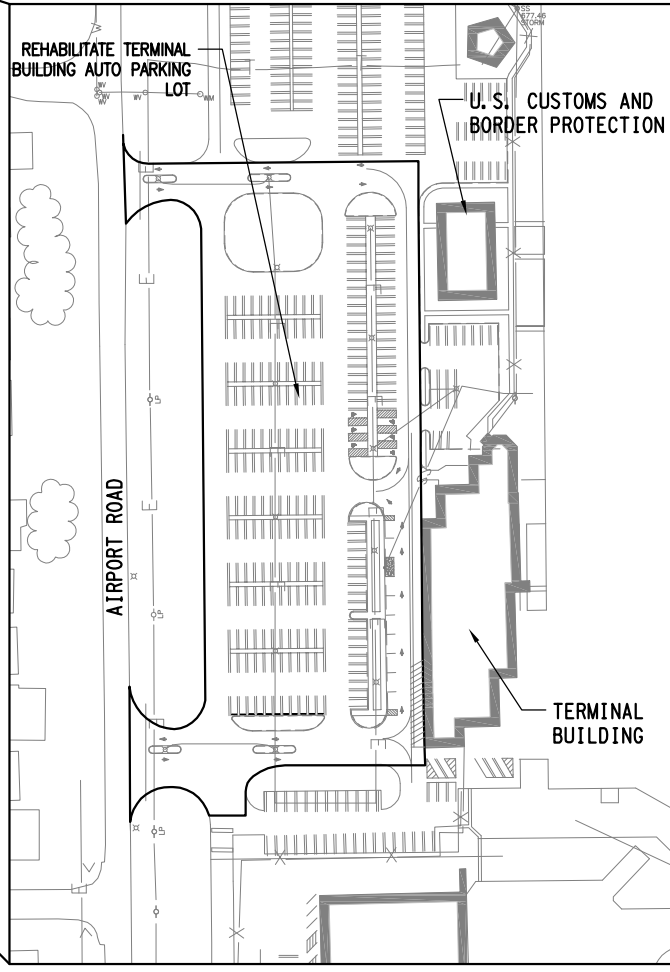
THE CONTRACTOR WILL BE REQUIRED TO SUBMIT TO THE RESIDENT ENGINEER, FOR APPROVAL BY THE AIRPORT DIRECTOR, A SCHEDULE OF CONSTRUCTION ACTIVITIES PRIOR TO THE START OF CONSTRUCTION. CLOSURE OF ANY PORTION OF AIRPORT ROAD BEGINNING AND ENDING TIMES WILL BE DETERMINED BY THE AIRPORT DIRECTOR. THE CONTRACTOR IS REQUIRED TO ALLOW SUFFICIENT TIME PRIOR TO THE ENDING OF THE CLOSURE TIME FOR CURING OF PLACED MATERIALS AND CLEAN UP OF THE WORK AREA.

THE CONTRACTOR IS REQUIRED TO NOTIFY THE AIRPORT DIRECTOR THROUGH THE RESIDENT ENGINEER, A MINIMUM OF 7 DAYS PRIOR TO THE START OF CONSTRUCTION.

THE CONTRACTOR IS REQUIRED TO IMPLEMENT A PRACTICAL AND EFFECTIVE STAGING PLAN THAT WILL MINIMIZE DISRUPTION TO NORMAL AIRPORT ACTIVITY WHILE NOT COMPROMISING SAFETY OF PERSONNEL OR THE QUALITY OF THE PROJECT.

**NOTE:**

THE AIRPORT WILL NOTIFY THE PUBLIC OF ANTICIPATED CONSTRUCTION IN AN EFFORT TO REDUCE OR ELIMINATE THE NEED FOR TOWING, HOWEVER THE CONTRACTOR SHALL BE REQUIRED TO PROCURE TOWING SERVICES FOR ANY VEHICLE THAT REMAINS IN THE PARKING AREA AT THE TIME OF CONSTRUCTION. THE COST OF THE TOWING SERVICES SHALL BE INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CONTRACTOR AND/OR THE TOWING SERVICE SHALL ACCEPT THE LIABILITY FOR THE VEHICLE RELOCATION, AND THE AIRPORT (PARK DISTRICT), CONSULTANT, STATE OF ILLINOIS, AND THE FEDERAL AVIATION ADMINISTRATION SHALL BE INDEMNIFIED. THE VEHICLES SHALL BE RELOCATED TO THE CONTRACTOR EQUIPMENT PARKING & MATERIAL STORAGE AREA, AS SHOWN ON THIS SHEET.



**PROJECT DETAIL**

SCALE: 1"= 100'

DE075

REVISION	DATE

**DECATUR AIRPORT**  
**DECATUR, ILLINOIS**

Hanson Proj. No. 13A0013	
Filename C-003-PL.dwg	
Scale 1"= 500'	
Date 05/03/13	
LAYOUT BKB	03/27/13
DRAWN RAD	04/27/13
REVIEWED RAW	05/03/13

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**REHABILITATE PARKING LOT**

**PROJECT LAYOUT**

**3**  
 3 of 9 sheets

IL PROJ.: DEC-4271

LEGEND

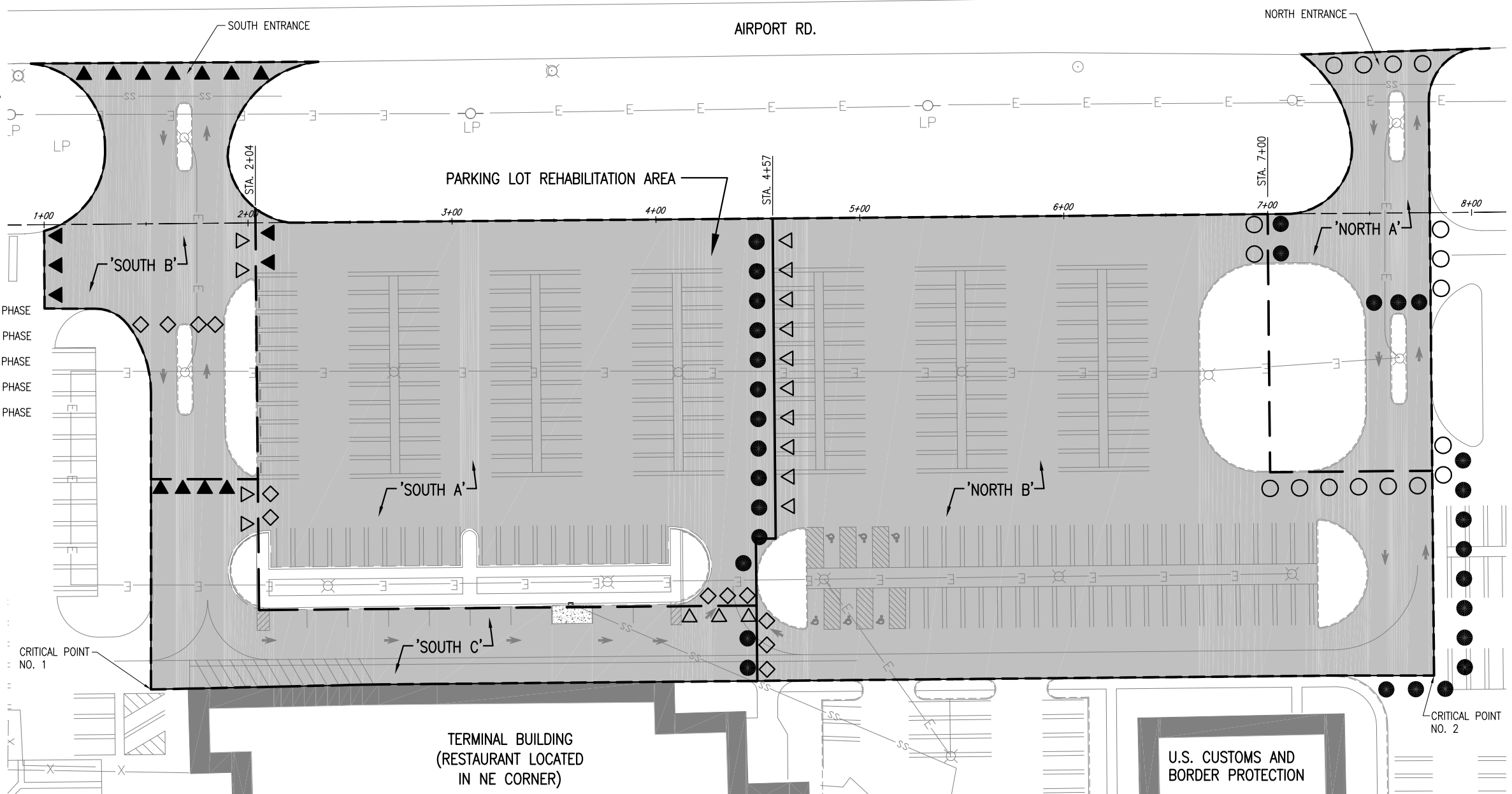
- EXISTING BUILDINGS
- PROPOSED IMPROVEMENTS
- PROPOSED HAUL ROUTE
- PROPOSED CONTRACTOR STAGING AREA
- PROPOSED BARRICADES
- EXISTING FENCE
- EXISTING ELECTRIC
- EXISTING STORM SEWER
- AIRPORT PROPERTY LINE
- EXISTING STORM INLET/MANHOLE
- EXISTING LIGHT POLE/POLE-MOUNTED LIGHT
- PROPOSED BARRICADES - NORTH "A" PHASE
- PROPOSED BARRICADES - NORTH "B" PHASE
- PROPOSED BARRICADES - SOUTH "A" PHASE
- PROPOSED BARRICADES - SOUTH "B" PHASE
- PROPOSED BARRICADES - SOUTH "C" PHASE

J.U.L.I.E. INFORMATION

COUNTY \_\_\_\_\_ MACON  
 CITY \_\_\_\_\_ DECATUR  
 TOWNSHIP \_\_\_\_\_ LONG CREEK  
 SECTION NO. \_\_\_\_\_ 20 & 21  
 ADDRESS \_\_\_\_\_ DECATUR AIRPORT  
 AIRPORT ROAD  
 DECATUR, ILLINOIS 62524



0' 15' 30' 60'  
 HALF SIZE SCALE: 1" = 60'  
 FULL SIZE SCALE: 1" = 30'



**NOTE TO CONTRACTOR**  
 FOR PROPOSED PHASING SEQUENCE SEE THE PROPOSED PHASING NOTES LOCATED IN THE SPECIAL PROVISIONS.

150-ENGINEER'S FIELD OFFICE NOTES

A FIELD OFFICE WILL BE PROVIDED IN ACCORDANCE WITH ITEM AR150510 OF THE STANDARD SPECIFICATIONS. IN ADDITION, THE CONTRACTOR WILL FURNISH A CELL PHONE TO THE RESIDENT ENGINEER FOR HIS EXCLUSIVE USE FOR THE DURATION OF THIS PROJECT. THE RESIDENT ENGINEER WILL USE THIS PHONE FOR PROJECT BUSINESS ONLY. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL CHARGES ASSOCIATED WITH THIS CELL PHONE. THE PROPOSED ENGINEER'S FIELD OFFICE, INCLUDING THE CELL PHONE, WILL BE PAID FOR UNDER ITEM:

AR150510 ENGINEER'S FIELD OFFICE \_\_\_\_\_ 1 L.S.

EROSION CONTROL

THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND, THEREFORE NO N.P.D.E.S. PERMIT WILL BE REQUIRED.

CERTIFIED PAYROLLS

THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 20 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI-TRUCK AND TRAILER.

NOTE

ALL CONSTRUCTION/OPERATIONS ARE TO BE PERFORMED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2F "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" AND AC 150/5300-13A "AIRPORT DESIGN".

ALL CONSTRUCTION EQUIPMENT ON THE AIRPORT SHALL BE MARKED, LIGHTED AND/OR FLAGGED IN ACCORDANCE WITH AC 150/5210-5D AND 70/7460-1.

SCOPE OF WORK

HIS PROJECT CONSISTS OF THE REHABILITATION OF APPROXIMATELY 15,300 SQ. YARDS OF BITUMINOUS PAVEMENT IN THE TERMINAL BUILDING AUTO PARKING LOT. THE REHABILITATION WILL CONSIST OF 2" BITUMINOUS PAVEMENT MILLING AND THE CONSTRUCTION OF A 2" BITUMINOUS SURFACE COURSE LAYER. ASSOCIATED WORK SHALL INCLUDE CRACK CLEANING AND SEALING; BITUMINOUS TACK COAT, REMOVAL, REPAIR AND/OR REPLACEMENT OF CONCRETE CURB, SHOULDER ADJUSTMENT AND PAVEMENT MARKING.

CRITICAL POINT DATA

**CRITICAL POINT #1**  
 LATITUDE: 39° 50' 03.73"  
 LONGITUDE: 88° 52' 38.52"  
 ELEVATION: 676.3' M.S.L.

**CRITICAL POINT #2**  
 LATITUDE: 39° 49' 57.50"  
 LONGITUDE: 88° 52' 38.37"  
 ELEVATION: 678.3' M.S.L.

TERMINAL BUILDING  
 (RESTAURANT LOCATED  
 IN NE CORNER)

U.S. CUSTOMS AND  
 BORDER PROTECTION

HAUL ROUTE

THE CONTRACTOR WILL USE AIRPORT ROAD AS HIS ACCESS TO THE PROPOSED CONSTRUCTION SITE. THE CONTRACTOR WILL COORDINATE WITH THE APPROPRIATE AUTHORITIES IN ORDER TO USE AIRPORT ROAD.

EMPLOYEE PARKING

THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR PERSONAL VEHICLES IN THE NORTH PARKING LOT. THE CONTRACTOR WILL TRANSPORT HIS EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED ON THE PROPOSED CONSTRUCTION SITE.

EQUIPMENT PARKING

THE CONTRACTOR WILL BE ALLOWED TO PARK HIS EQUIPMENT WITHIN THE LIMITS OF THE PROPOSED CONSTRUCTION. IF THERE IS NO AVAILABLE SPACE WITHIN THE PROPOSED CONSTRUCTION, THEN HE WILL PARK IN THE NORTH PARKING LOT AT A LOCATION DESIGNATED BY THE AIRPORT DIRECTOR.

TOWING OF PARKED CARS

THE AIRPORT WILL BE RESPONSIBLE FOR THE COORDINATION AND TOWING OF ANY VEHICLES THAT ARE PARKED IN A WORK AREA THAT HAS BEEN CLOSE/BARRICADED BY THE CONTRACTOR.

CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADMINISTRATIVE AND PARKING AREAS AT ALL TIMES. FULL USE AND ACCESS OF AND THE PARKING AREAS.

TRENCHES AND/OR HOLES THAT REMAIN OPEN OVERNIGHT WILL REQUIRE BARRICADES AND/OR CONES TO INDICATE THEIR LOCATION AND PREVENT ACCIDENTAL ENTRANCE, WITHIN THE PROJECT AREA AND OUTSIDE ANY ACTIVE SAFETY AREAS.

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

MATERIAL CERTIFICATION

COMPLETED WORK CANNOT BE PLACED ON A CONSTRUCTION REPORT UNTIL ALL MATERIAL CERTIFICATIONS FOR THAT PAY ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER. ALL BUY AMERICAN DOCUMENTATION SHALL BE PRESENTED BEFORE REVIEW OF SHOP DRAWINGS SHALL OCCUR.

REVISION	DATE



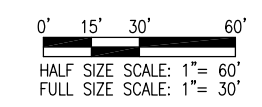
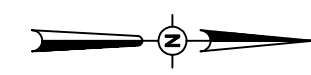
PROJECT	DATE
13A0013	05/03/13
C-004-SFY.dwg	03/27/13
Scale 1" = 30'	03/27/13
DATE	RAW
LAYOUT	RAW
DRAWN	RAW
REVIEWED	RAW

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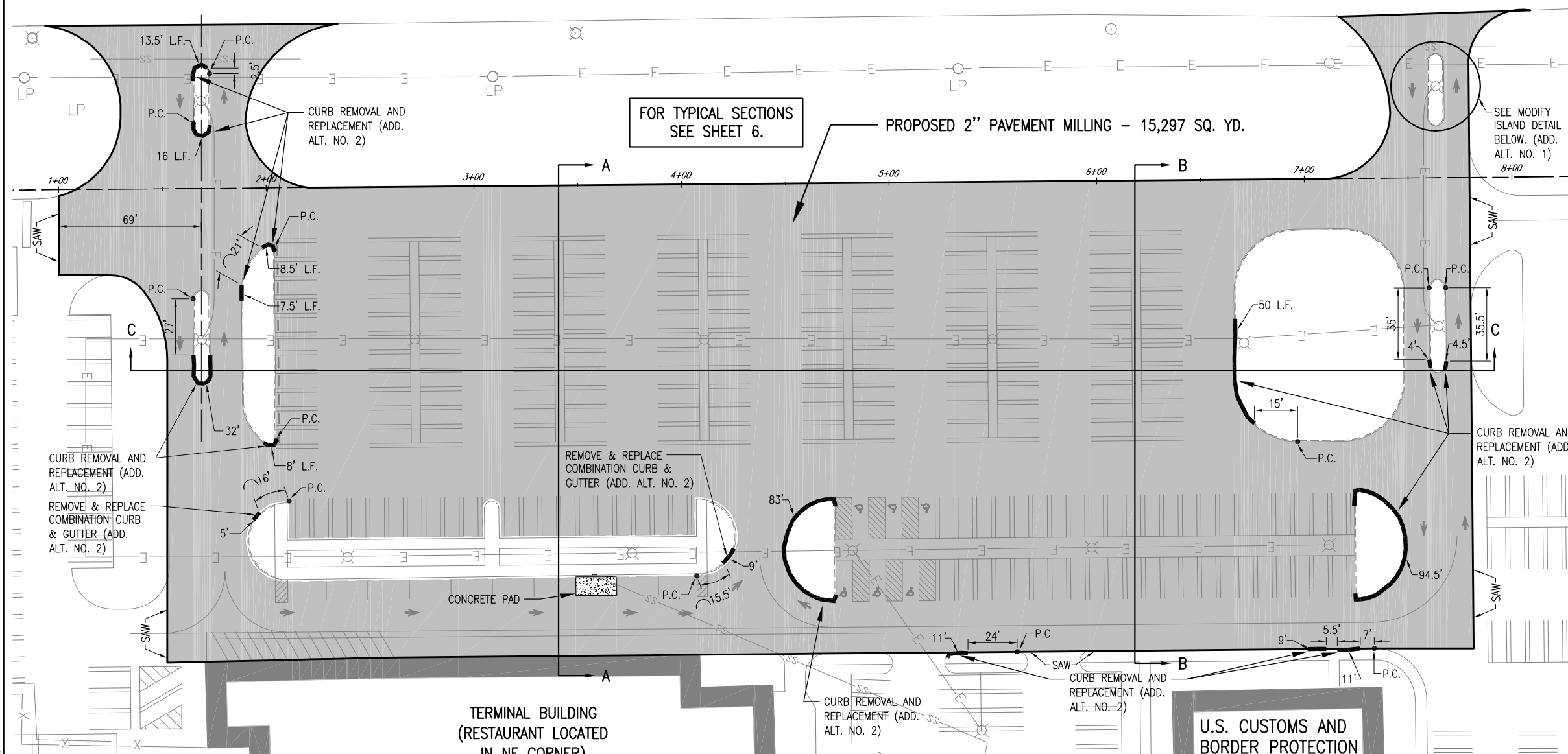
REHABILITATE  
 PARKING LOT  
 PROPOSED SAFETY AND  
 PHASING PLAN

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- EXISTING BUILDING
- PROPOSED MILLING (2.0" NOMINAL DEPTH)
- CURB REMOVAL AND REPLACEMENT
- X EXISTING FENCE
- SS EXISTING STORM SEWER
- w EXISTING WATERLINE
- S EXISTING SANITARY
- T EXISTING TELEPHONE
- FO EXISTING FIBER OPTIC
- G EXISTING GAS
- E EXISTING ELECTRIC
- /○ EXISTING STORM INLET/MAHOLE
- /X EXISTING LIGHT POLE/POLE-MOUNTED LIGHT
- P.C. POINT OF CURVATURE (BEGINNING OF RADIUS)



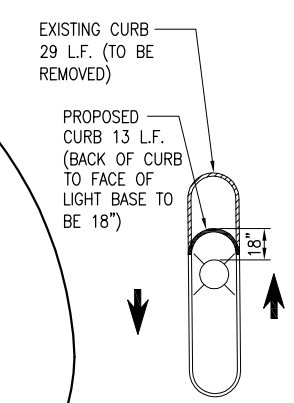
AIRPORT RD.



**MODIFY TRAFFIC ISLAND NOTES (ADD. ALT. NO. 1)**  
 THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING TRAFFIC ISLAND AT THE NORTH ENTRANCE AS DETAILED ON THIS SHEET AND FILLING IN THE HOLE WITH CRUSHED AGGREGATE BASE COURSE (209) AND BITUMINOUS SURFACE COURSE (401).  
 THE EXISTING TRAFFIC ISLAND AT THE NORTH ENTRANCE WILL HAVE THE WEST END REMOVED AND A NEW CONCRETE CURB INSTALLED 18" FROM THE EXISTING LIGHT POLE AS SHOWN ON THIS SHEET.  
 THE CONTRACTOR WILL REMOVE THE CONCRETE CURB AND THE EARTH MATERIAL LOCATED WITHIN THE SECTION OF TRAFFIC ISLAND BEING REMOVED. THE EARTH MATERIAL WILL BE REMOVED TO A DEPTH OF 13" BELOW THE TOP OF THE SURROUNDING BITUMINOUS PAVEMENT (BEFORE IT IS MILLED).  
 THE CONTRACTOR WILL COMPACT THE BOTTOM OF THE HOLE AND THEN PLACE 6" OF A CRUSHED AGGREGATE BASE COURSE MATERIAL (209) IN THE HOLE. THE CONTRACTOR WILL GRADE AND COMPACT THIS AGGREGATE MATERIAL, TO THE SATISFACTION OF THE RESIDENT ENGINEER.  
 THE CONTRACTOR WILL THEN APPLY A BITUMINOUS PRIME COAT AND THEN PLACE 5" OF A BITUMINOUS SURFACE COURSE, SUPERPAVE MATERIAL (401) ON TOP OF THE CRUSHED AGGREGATE BASE COURSE. THE BITUMINOUS SURFACE COURSE WILL BE PLACED IN A MAXIMUM 3" LIFT.  
 THIS WORK WILL BE PAID FOR UNDER ITEM:  
 AS800419 "MODIFY TRAFFIC ISLAND" \_\_\_\_\_ PER LUMP SUM.

**CURB/CURB AND GUTTER REMOVAL NOTES (ADD. ALT. NO. 2)**  
 THE EXISTING CONCRETE CURB/CURB AND GUTTER SHOWN FOR REMOVAL WILL BE REMOVED FULL DEPTH. THE ACTUAL AMOUNT OF EXISTING CURB/CURB AND GUTTER TO BE REMOVED WILL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION. THE RESIDENT ENGINEER WILL MARK CURB THAT IS TO BE REMOVED AND REPLACED. IN ADDITION, A NOMINAL QUANTITY OF 15 LF OF CURB AND GUTTER HAS BEEN INCLUDED TO REMOVE AND REPLACE ANY CURB AND GUTTER IN POOR CONDITION AND AS DIRECTED BY THE RESIDENT ENGINEER. THE EXISTING PAVEMENT ADJOINING THE CURB TO BE REMOVED WILL BE SAWED TO ALLOW THE REMOVAL OF THE CURB WITHOUT DISPLACEMENT OF THE REMAINING PAVEMENT. ANY DISPLACEMENT OR DAMAGE TO THE ADJACENT PAVEMENT WILL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.  
 THE REMOVED CURBS AND CURB & GUTTER WILL BE DISPOSED OF OFF THE AIRPORT SITE AT THE CONTRACTOR'S OWN EXPENSE.  
 THESE ITEMS OF WORK SHALL BE PAID FOR UNDER ITEM:  
 AT754900 "REMOVE CONCRETE CURB" \_\_\_\_\_ PER L.F.  
 AT800433 "REMOVE & REPLACE COMB. CURB & GUTTER" \_\_\_\_\_ PER L.F.  
**CLEAN AND SEAL BITUMINOUS CRACKS**  
 THE AMOUNT OF CRACKS TO BE CLEANED AND SEALED WILL BE THE NUMBER OF LINEAR FEET OF CRACKS MARKED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.  
 ALL CRACKS DESIGNATED BY THE RESIDENT ENGINEER FOR CLEANING AND SEALING WILL BE DONE SO AS STATED IN THE SPECIFICATIONS AND SPECIAL PROVISIONS.  
 ANY PROPOSED PAVEMENT MILLING WILL BE ACCOMPLISHED BEFORE THE CRACKS ARE CLEANED AND SEALED. THE RESIDENT ENGINEER WILL DETERMINE IF THE CRACKS ARE LARGE ENOUGH TO WARRANT CLEANING AND SEALING.  
 THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:  
 AR605541 "CLEAN & SEAL CRACKS" - PER L.F.

**BITUMINOUS PAVEMENT MILLING (2")**  
 THE AREA(S) DESIGNATED ON THIS SHEET WILL BE CUT OR TRIMMED. THE CUTTING OR TRIMMING WILL BE DONE BY ROTO-MILLING. THE TOLERANCE OF THE MILLING WILL BE AS STATED IN THE STANDARD SPECIFICATIONS.  
 IF A VERTICAL FACE IS NOT OBTAINED FROM THE MILLING OPERATIONS, OR IF THE VERTICAL FACE IS DAMAGED PRIOR TO PAVING, THE EXISTING PAVEMENT WILL BE SAW-CUT IN ORDER TO ACHIEVE A VERTICAL FACE FOR PAVING AGAINST. SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT MILLING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.  
 ALL MILLED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.  
 PRIOR TO APPLYING THE BITUMINOUS OVERLAY ALL MILLED AREAS WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.  
 ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.  
 THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:  
 AR401650 "BITUMINOUS PAVEMENT MILLING" - PER S.Y.



MODIFY ISLAND DETAIL  
 "NOT TO SCALE"

REVISION	DATE

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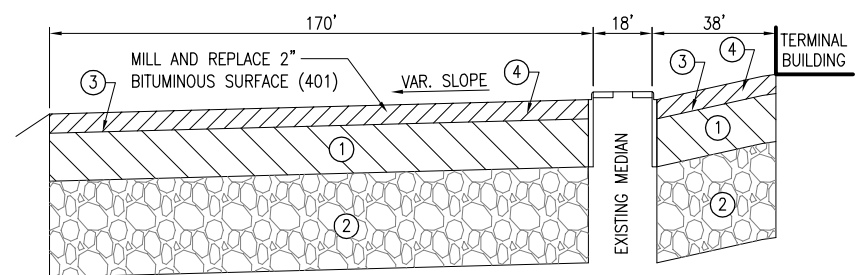
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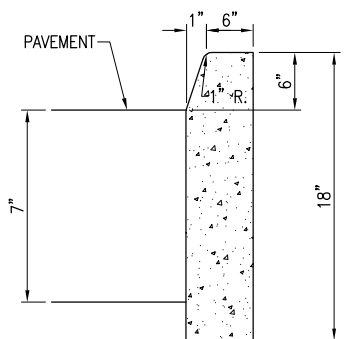
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REHABILITATE PARKING LOT  
 PROPOSED PAVEMENT PREPARATION PLAN

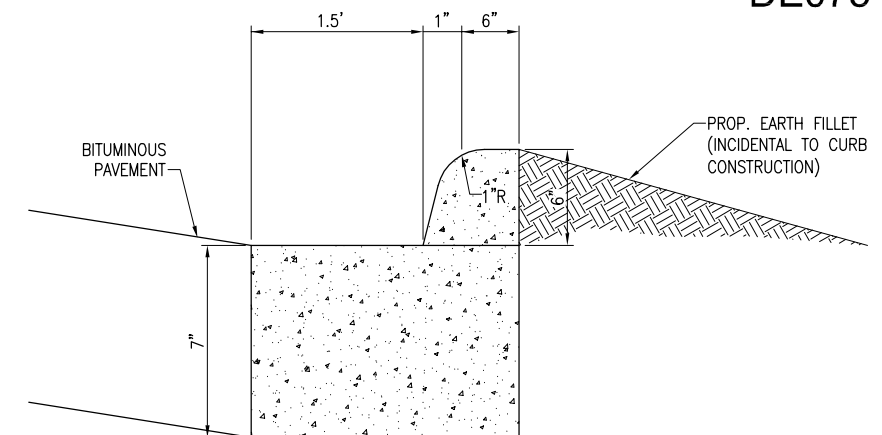
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**TYPICAL SECTION "A-A"**  
"NOT TO SCALE"



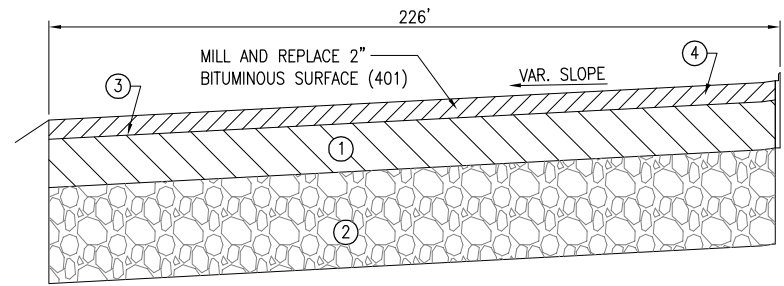
**BARRIER CURB DETAIL**  
NOT TO SCALE



**CURB REMOVING AND REPLACEMENT DETAIL**  
ADD. ALT. NO. 2)

NOT TO SCALE

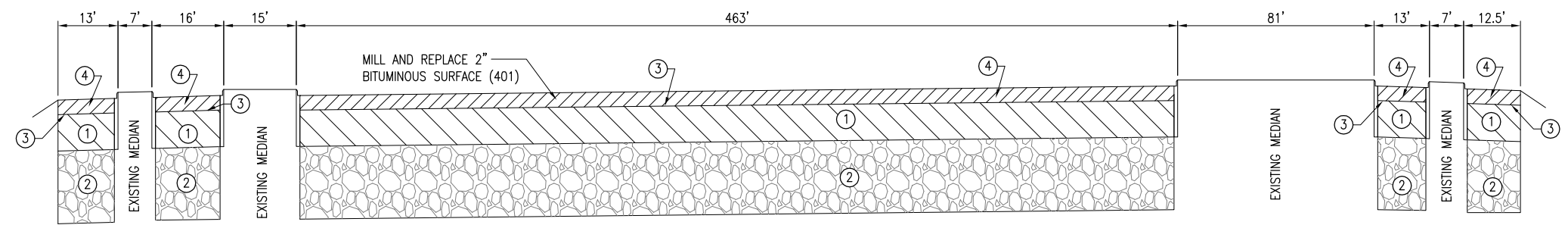
NOTE: AT LOCATIONS WHERE STD CONC CURB AND GUTTER IS SHOWN ON THE PLANS, THE CURB AND GUTTER SHALL BE TYPE "B" BARRIER CURB IN CONFORMANCE WITH IDOT STANDARD DRAWING 606001 AND SECTION 606 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2012. CONSTRUCTION OF THE CURB AND GUTTER WILL BE PAID FOR UNDER ITEM AT800433.



**TYPICAL SECTION "B-B"**  
"NOT TO SCALE"

**ITEM 610**

ALL CONCRETE UTILIZED SHALL BE IN ACCORDANCE WITH ITEM 610 OF THE STANDARD SPECIFICATIONS.



**TYPICAL SECTION "C-C"**  
"NOT TO SCALE"

**LEGEND FOR TYPICAL SECTIONS "A-A", "B-B", & "C-C"**

- ① EXISTING BITUMINOUS SURFACE COURSE (7" DEPTH)
- ② EXISTING AGGREGATE BASE COURSE (6" DEPTH)
- ③ PROPOSED BITUMINOUS TACK COAT (0.15 GAL/S.Y.)
- ④ PROPOSED BITUMINOUS SURFACE COURSE (2" DEPTH)
- ▨ PROPOSED BITUMINOUS MILLING (2" DEPTH)

REVISION	DATE

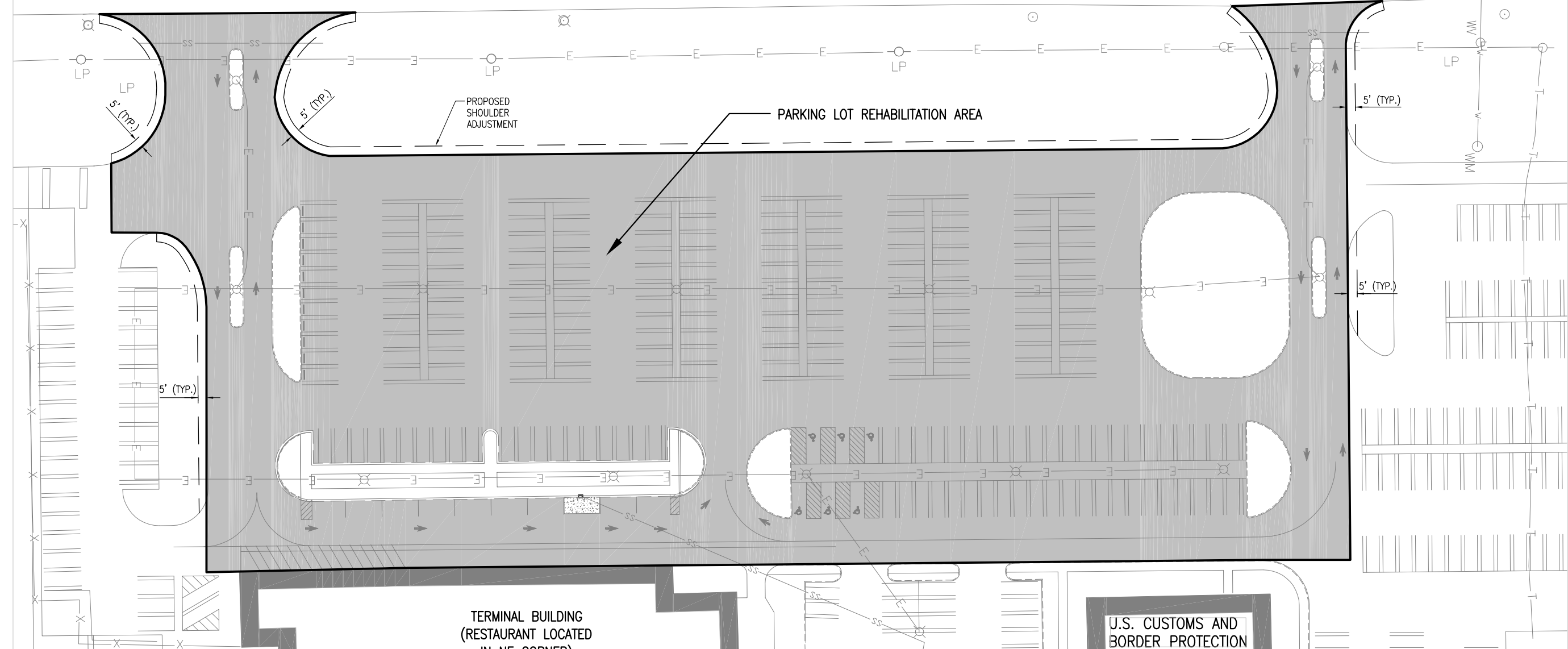
**DECATUR AIRPORT**  
DECATUR, ILLINOIS

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REHABILITATE PARKING LOT  
TYPICAL SECTIONS AND DETAILS

AIRPORT RD.



**AR401613 BITUMINOUS SURFACE COURSE—METHOD 1, SUPERPAVE**

THE BITUMINOUS SURFACE COURSE (401) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR401003 "BITUMINOUS SURFACE COURSE—METHOD 1, SUPERPAVE" AS STATED ON PAGE 127 OF THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED APRIL 1, 2012.

THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING: A 2 INCH SURFACE LIFT OF BITUMINOUS SURFACE COURSE—METHOD 1, SUPERPAVE ON THE EXISTING BITUMINOUS BASE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE QUALITY CONTROL IN THE PRODUCTION AND CONSTRUCTION OF THE BITUMINOUS SURFACE COURSE METHOD 1, SUPERPAVE.

PRIOR TO STARTING THE BITUMINOUS SURFACE COURSE—METHOD 1, SUPERPAVE OPERATION, THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER A DETAILED OUTLINE SHOWING AREAS AND ORDER OF PAVING WIDTHS OF PAVING LANES.

THE PROPOSED BITUMINOUS SURFACE COURSE METHOD 1, SUPERPAVE WILL BE DESIGNED TO A SUPERPAVE DESIGN OF LESS THAN 60,000 POUNDS.

401-4.9 ADD THE FOLLOWING TO THIS SECTION:

WHEN HAND SPREADING IS PERMITTED, THE MIXTURE WILL BE DISTRIBUTED AND SPREAD USING HAND TOOLS. WHEN THE WORK IS COMPLETED, THE LAYER WILL HAVE THE REQUIRED THICKNESS AND CONFORM TO THE GRADE AND SURFACE CONTOUR SHOWN ON THE PLANS.

401-4.12 ADD THE FOLLOWING TO THIS PARAGRAPH:

ALL PAVEMENT EDGES (LONGITUDINAL, RADIUS, AND PAVEMENT ENDS) MUST BE LEFT IN PROPER ALIGNMENT AS SHOWN ON THE PLANS. THIS MAY BE ACCOMPLISHED BY THE TRIMMING METHOD OUTLINED ABOVE OR AT THE CONTRACTOR'S OPTION BY SAWING AFTER THE PAVING HAS BEEN COMPLETED. NO ADDITIONAL COMPENSATION WILL BE MADE IF THE SAWING METHOD IS USED.

401-6.1 ADD THE FOLLOWING TO THIS SECTION  
AR401613 BIT. SURF. CSE—METHOD 1, SUPERPAVE \_\_\_\_\_ PER TON

**603-BITUMINOUS TACK COAT NOTES:**

THE BITUMINOUS TACK COAT (603) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR603 "BITUMINOUS TACK COAT" AS STATED ON PAGE 250 OF THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED APRIL 1, 2012.

THE PROPOSED BITUMINOUS TACK COAT SHALL BE PLACED ON THE PROPOSED BITUMINOUS PAVEMENT PRIOR TO THE PLACEMENT OF THE NEXT LIFT OF PROPOSED BITUMINOUS SURFACE COURSE. THE PROPOSED BITUMINOUS PAVEMENT SHALL HAVE A TACK COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS.

THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM:  
AR603510 BITUMINOUS TACK COAT \_\_\_\_\_ PER GAL.

**SHOULDER ADJUSTMENT NOTES:**

SHOULDER ADJUSTMENT WILL BE PERFORMED ADJACENT TO THE PAVEMENT OVERLAY AREAS AS SHOWN TO PROVIDE A 1-1/2" DROP-OFF. PRIOR TO PLACING THE SHOULDER ADJUSTMENT MATERIAL, THE LIMITS WILL BE MOWED AND PULVERIZED, DISKED OR TILLED TO THE SATISFACTION OF THE RESIDENT ENGINEER. THE PROPOSED GRADING, AND SEEDING LIMITS ARE SHOWN ON THE CONSTRUCTION PLANS AS PROPOSED SHOULDER ADJUSTMENT. THE EARTH FILLETS (SHOULDER ADJUSTMENT) WILL NOT REQUIRE COMPACTION OR GRADING, OTHER THAN LIGHT ROLLING AND SHAPING. THE MATERIAL FOR THE PROPOSED EARTH FILLETS (SHOULDER ADJUSTMENT) WILL BE OBTAINED FROM AN OFF-SITE LOCATION.

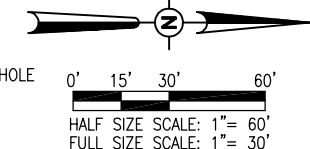
THE SHOULDER ADJUSTMENT AREA WILL BE LIMED, FERTILIZED, SEEDED AND MULCHED IN ACCORDANCE WITH ITEMS 901 AND 908.

THE PROPOSED EARTH MATERIAL, LIME, FERTILIZER, SEED AND MULCH USED FOR THE PROPOSED SHOULDER ADJUSTMENT WILL BE PAID FOR UNDER:

ITEM AR152480 "SHOULDER ADJUSTMENT" \_\_\_\_\_ PER S.Y.

**LEGEND**

- EXISTING BUILDING
- PROPOSED IMPROVEMENTS
- EXISTING FENCE
- EXISTING STORM SEWER
- EXISTING WATERLINE
- EXISTING SANITARY
- EXISTING TELEPHONE
- EXISTING FIBER OPTIC
- EXISTING GAS
- EXISTING ELECTRIC
- EXISTING STORM INLET/MAHOLE
- EXISTING LIGHT POLE/POLE-MOUNTED LIGHT

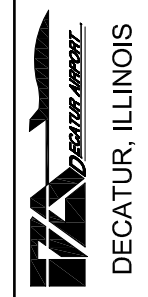


**SUPERPAVE DESIGN CRITERIA**

BITUMINOUS SURFACE COURSE AND BITUMINOUS BASE COURSE MATERIAL SHALL CONFORM TO THE REQUIREMENTS OF ITEM 401/ITEM 403, TABLE 1, "AUTOMOBILE" CATEGORY WITH RESPECT TO THE DESIGN MIX.

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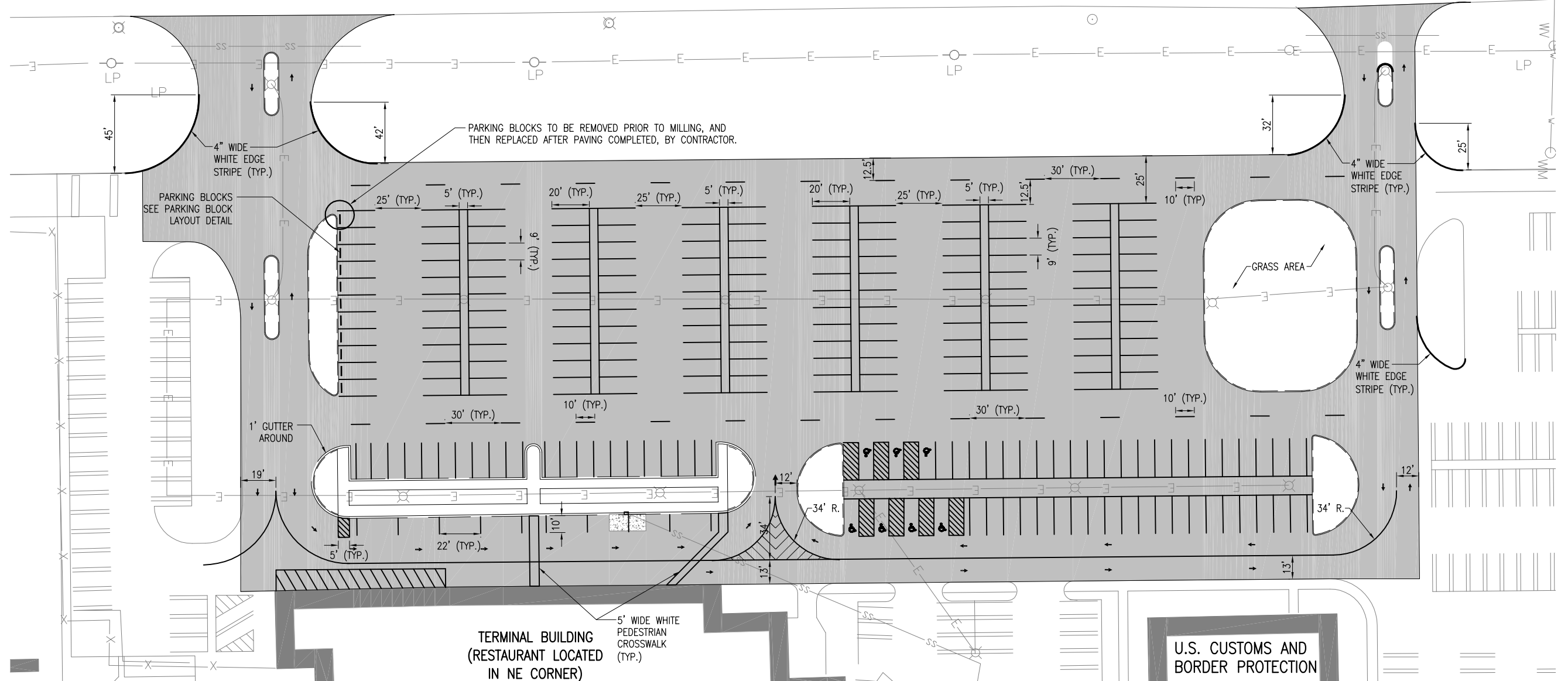
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REHABILITATE  
PARKING LOT

CONSTRUCTION PLAN



AIRPORT RD.



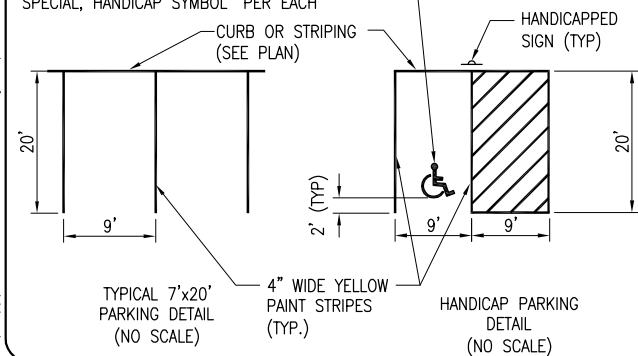
**HANDICAPPED PARKING SIGNS**

THE SIGNS SHALL BE A R7-8 AND, WHERE APPLICABLE, R7-8P STANDARD SIGNS ADOPTED BY THE U.S. DEPARTMENT OF TRANSPORTATION, AS NEEDED.

THE SIGNS SHALL BE VERTICALLY MOUNTED ON A POST AT THE FRONT CENTER OF THE PARKING SPACE AND SET A MINIMUM OF 3 FT. BEHIND THE BACK OF THE PARKING STALL MARKING AND 7 FT. FROM THE PAVEMENT TO THE BOTTOM OF THE SIGN.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:  
AR910230 "HANDICAP SIGN" - 1 EACH

INTERNATIONAL SYMBOL OF ACCESSIBILITY; PAID UNDER ITEM AR800571 "PAVEMENT MARKING - SPECIAL, HANDICAP SYMBOL" PER EACH



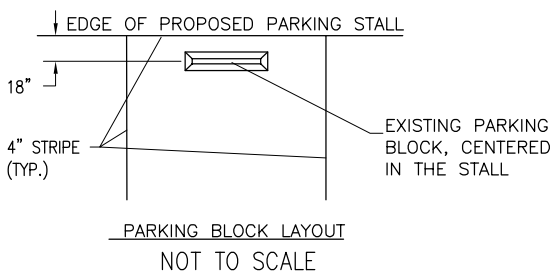
**PAVEMENT MARKINGS - DIRECTIONAL ARROWS**

THE PROPOSED DIRECTIONAL ARROW PAVEMENT MARKINGS SHALL BE LOCATED AS SHOWN ON THIS SHEET. ARROWS SHALL CONFORM TO IDOT STANDARD DRAWING NO. 780001. THIS ITEM OF WORK SHALL BE PAID FOR UNDER THE FOLLOWING ITEMS:

AR800572 "PAVEMENT MARKING - SPECIAL, ARROW-STRAIGHT" - 28 EA

**NOTE**

ALL PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH ITEM 620.



**620-PAVEMENT MARKING-WATERBORNE NOTES**

THE PAVEMENT MARKING-WATERBORNE (620) SHALL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED ON PAGE 272 OF THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED APRIL 1, 2012.

THIS ITEM SHALL CONSIST OF TAXILANE CENTERLINE IN ACCORDANCE WITH THESE SPECIFICATIONS AND AT THE LOCATIONS SHOWN ON THE CONSTRUCTION PLANS. THE PROPOSED PAVEMENT MARKING WILL BE APPLIED IN TWO APPLICATIONS.

ANY MATERIAL DELIVERED THAT FAILS TO MEET THE SPECIFICATIONS SHALL BE DISPOSED OF BY THE VENDOR AND IMMEDIATELY REPLACED WITH ACCEPTABLE MATERIAL ENTIRELY AT THE VENDOR'S EXPENSE, INCLUDING HANDLING AND TRANSPORTATION CHARGES.

ALL PROPOSED MARKING WILL BE COMPLETED IN ACCORDANCE WITH THE DETAILS SHOWN ON THE CONSTRUCTION PLANS.

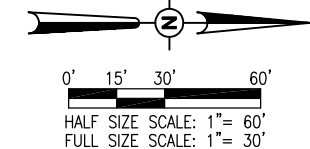
GLASS BEADS SHALL BE REQUIRED ONLY ON THE SECOND APPLICATION OF THE MARKING.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

THE PROPOSED MARKING WILL BE PAID FOR UNDER ITEM:  
AR620520 PAVEMENT MARKING-WATERBORNE \_\_\_\_ PER S.F.

**MARKING QUANTITIES**

DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)
CENTERLINE STRIPE	600	1	600
CENTERLINE SKIP - DASH	3.33	28	93
PARKING STALLS STRIPES (20')	6.67	222	1,481
PARKING STALL CURB STRIPE (9')	32.33	12	388
PARKING STALL CURB STRIPE (250')	83.33	2	167
PARKING STALL STRIPES(10')	3.33	10	33
HANDICAP STALL	43	7	301
SIDEWALK EXIT/NO PARKING	21	1	21
NO PARKING (ADJACENT BUILDING) AND MEDIAN	140	1	140
TOTAL YELLOW			3,224
EDGE STRIPE	83	1	83
PEDESTRIAN CROSSWALK	58	1	58
TOTAL WHITE			141
TOTAL MARKING			3,365



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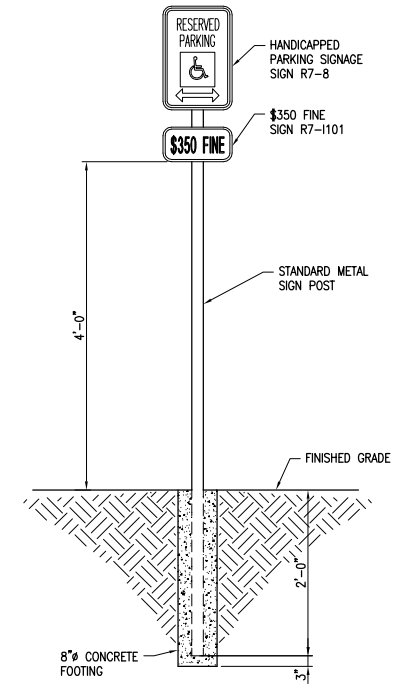
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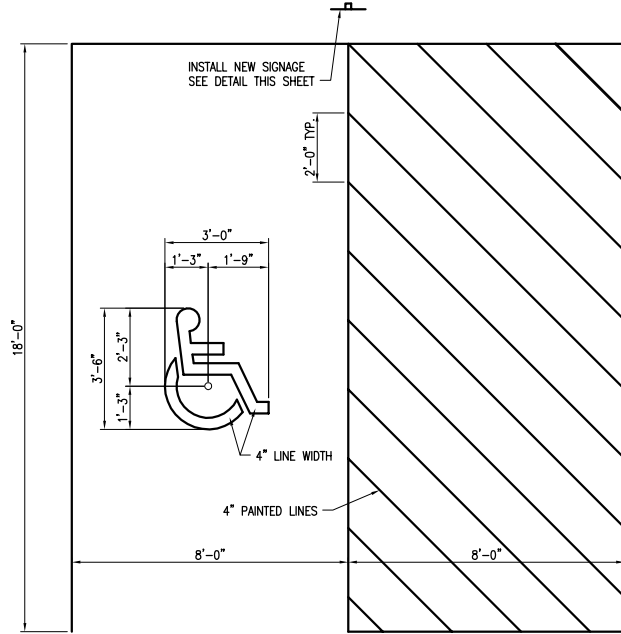
MARKING PLAN

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 8 of 9 sheets

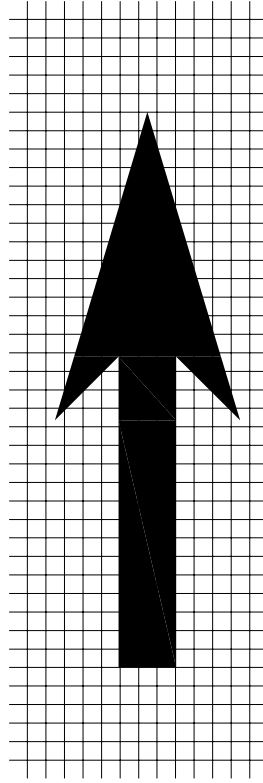




**TYPICAL SIGNAGE DETAIL**  
SCALE: NONE



**TYPICAL HANDICAPPED PARKING SPACE PAINT STRIPING**  
SCALE: NONE  
NOTE: CENTER SYMBOL IN STALL.



Legend Height	Arrow Size	a
6' (1.8 m)	Small	2.9 (74)
8' (2.4 m)	Large	3.8 (96)

IDOT STANDARD NO. 780001-03

**STRAIGHT ARROW**  
"NOT TO SCALE"

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REHABILITATE  
PARKING LOT

PROPOSED  
MARKING DETAILS

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