

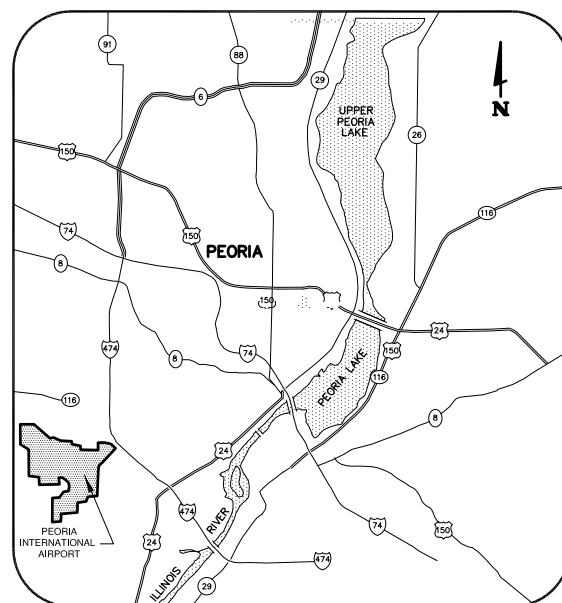
**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**

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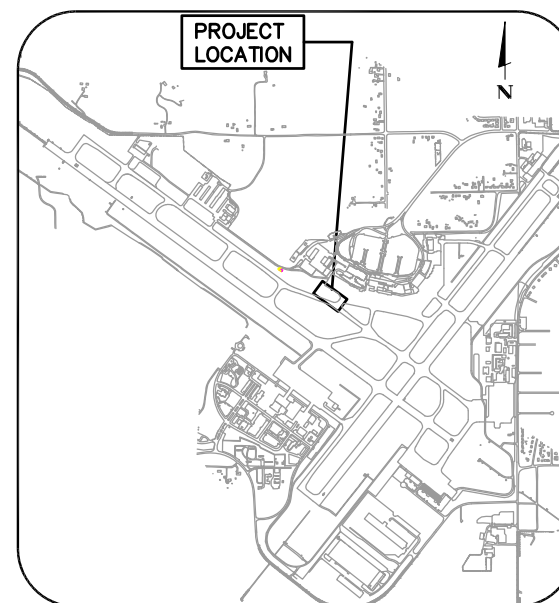
A.I.P. PROJECT NO.: 3-17-0080-XX
ILLINOIS PROJECT NO.: PIA-4180
EXPAND TERMINAL APRON

SUMMARY OF QUANTITIES			
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AR108108	1/C #8 5 KV UG CABLE	L.F.	1,156
AR108706	1/C #6 COUNTERPOISE	L.F.	484
AR110215	2" PVC CONDUIT, DIRECT BURIAL	L.F.	539
AR110946	ADJUST ELECTRICAL HANDHOLE	EACH	2
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	10
AR125964	RELOCATE TAXI GUIDANCE SIGN	EACH	2
AR150250	MOBILIZATION	LS	1
AR152455	EMBANKMENT IN PLACE	C.Y.	6,621
AR156500	TEMPORARY EROSION CONTROL	LS	1
AR156540	RIPRAP	S.Y.	139
AR208540	OVERSIZE AGGREGATE	TON	1,886
AR209608	CRUSHED AGGREGATE BASE COURSE - 8"	S.Y.	2,857
AR501518	18" PCC PAVEMENT	S.Y.	2,831
AR501530	PCC TEST BATCH	EACH	1
AR501900	REMOVE PCC PAVEMENT	S.Y.	263
AR620510	PAVEMENT MARKING	S.F.	7,750
AR620595	TEMPORARY MARKING & REMOVAL	S.F.	216
AR620900	PAVEMENT MARKING REMOVAL	S.F.	6,900
AR701524	24" RCP, CLASS IV	L.F.	72
AR701536	36" RCP, CLASS IV	L.F.	72
AR705506	6" PERFORATED UNDERDRAIN	L.F.	473
AR751415	INLET - SPECIAL	EACH	1
AR751940	ADJUST INLET	EACH	2
AR752436	PRECAST REINFORCED CONC. FES 36"	EACH	1
AR752960	RELOCATE END SECTION	EACH	1
AR901510	SEEDING	ACRE	2.6
AR908520	EXCELSIOR BLANKET	S.Y.	12,551

APRIL 26, 2013



LOCATION MAP



SITE PLAN

MAXIMUM EQUIPMENT HEIGHT = 25'
 GROUND FREQUENCY 121.85

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

TOWNSHIP: 8 NORTH
 RANGE: 7 EAST
 COUNTY: PEORIA
 TOWNSHIP: LIMESTONE



Know what's below.
 Call before you dig.

COMMON GROUND ALLIANCE
 www.call811.com or
 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE
 RESULTS IN THE RELEASE OF NATURAL GAS.



GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

APPROVED *Heavens*

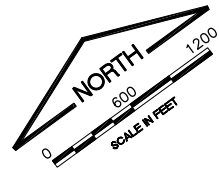
DATE April 26, 2013

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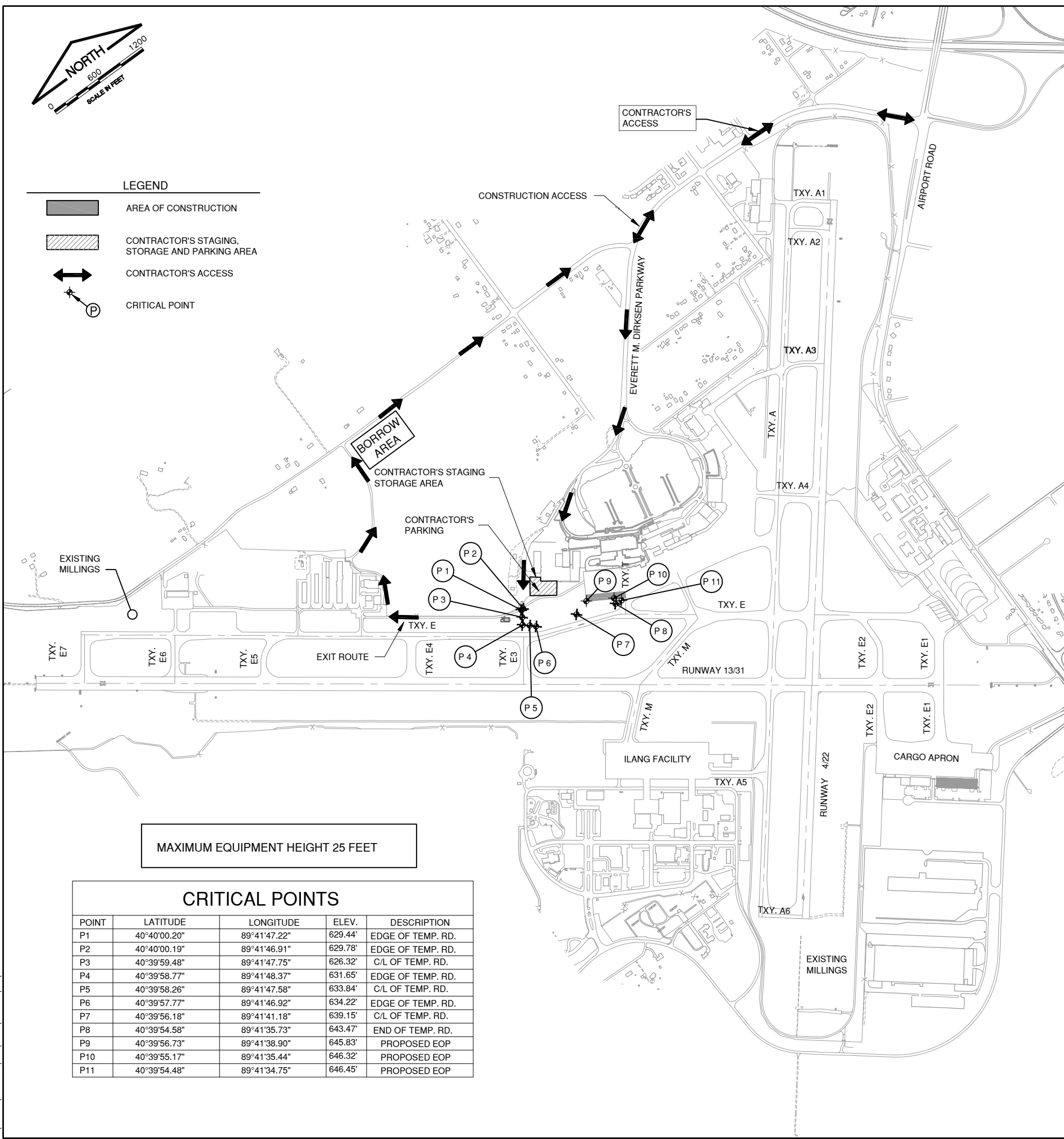
DATE: April 26, 2013

CM&T JOB NUMBER 12061-03-00



LEGEND

- AREA OF CONSTRUCTION
- CONTRACTOR'S STAGING, STORAGE AND PARKING AREA
- CONTRACTOR'S ACCESS
- CRITICAL POINT



MAXIMUM EQUIPMENT HEIGHT 25 FEET

CRITICAL POINTS

POINT	LATITUDE	LONGITUDE	ELEV.	DESCRIPTION
P1	40°40'00.20"	89°41'47.22"	629.44'	EDGE OF TEMP. RD.
P2	40°40'00.19"	89°41'46.91"	629.78'	EDGE OF TEMP. RD.
P3	40°39'59.48"	89°41'47.75"	626.32'	C/L OF TEMP. RD.
P4	40°39'58.77"	89°41'48.37"	631.65'	EDGE OF TEMP. RD.
P5	40°39'58.26"	89°41'47.58"	633.84'	C/L OF TEMP. RD.
P6	40°39'57.77"	89°41'46.92"	634.22'	EDGE OF TEMP. RD.
P7	40°39'56.18"	89°41'41.18"	639.15'	C/L OF TEMP. RD.
P8	40°39'54.58"	89°41'35.73"	643.47'	END OF TEMP. RD.
P9	40°39'56.73"	89°41'38.90"	645.83'	PROPOSED EOP
P10	40°39'55.17"	89°41'35.44"	646.32'	PROPOSED EOP
P11	40°39'54.48"	89°41'34.75"	646.45'	PROPOSED EOP

GENERAL NOTES

1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF AN ESCORT OR A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED MAAP/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT OPERATIONS DIRECTOR.
4. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
5. EXCESS EARTH, BROKEN ASPHALT AND CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 107' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
7. ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
9. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
10. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
11. SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS SHEET FOR NOTES REGARDING THE CONTRACTOR'S ACCESS.
12. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE LIGHTED, MARKED, AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
13. COST OF TEMPORARY EDGE LIGHTING AND CABLING IN ORDER TO MAINTAIN AIRFIELD CIRCUITS WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES PRIOR TO COMPLETING EXCAVATION OR TRENCHING ITEMS. ANY UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR AT HIS/HER OWN COST.

FILE: SITE PLAN.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 11:19 AM

Pia-Base_Updated
 BASE_PROP_GEO
 PIA-Base-Existing

PE094

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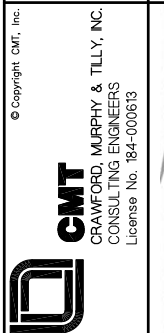
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 SITE PLAN

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DESIGN BY:	CET/TJH
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	APRIL 26, 2013
JOB No:	12061-03
IL. PROJ. NO. PIA-4180 AIP PROJ. NO. 3-17-0080-XX	
SHEET 02 OF 35 SHEETS	

GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP)...

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA)...

2. PHASING

- 1. TOTAL CONTRACT TIME SHALL BE 40 CALENDAR DAYS.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- 1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.

6. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- 1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- 1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.

Table with 3 columns: Role, Name, Phone Number. Includes Airport Operations, Airport Maintenance, and Engineer roles.

10. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP.

11. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS.

12. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. NO RUNWAY OR TAXIWAY CLOSURES ARE REQUIRED FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2F.

15. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.

16. PROTECTION

- 1. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY SAFETY AREA, WHICH EXTENDS 107' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED.

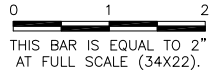
17. OTHER LIMITATIONS ON CONSTRUCTION

- 1. IF DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.

PE094

REVISIONS

Table with 3 columns: NUMBER, BY, DATE. Revision tracking table.



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

EXPAND TERMINAL APRON
CONSTRUCTION SAFETY PHASING PLAN
NOTES

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DRAWN BY: CMT

CHECKED BY: TJH

APPROVED BY: CET

DATE: APRIL 26, 2013

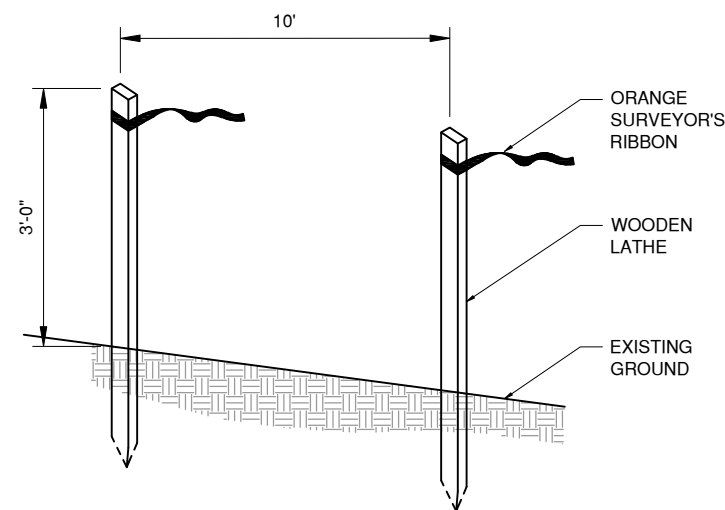
JOB No: 12061-03

IL. PROJ. NO. PIA-4180
AIP PROJ. NO. 3-17-0080-XX

SHEET 03 OF 35 SHEETS

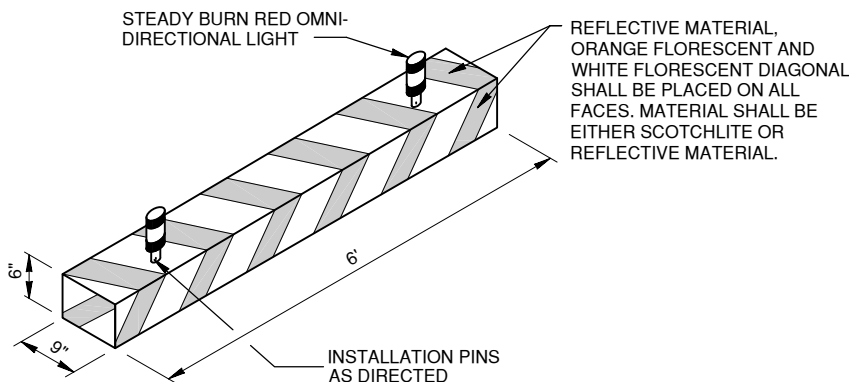
BEAM BARRICADE NOTES

- 1.) BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- 2.) BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- 3.) PLACE AT 10' INTERVALS.
- 4.) NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

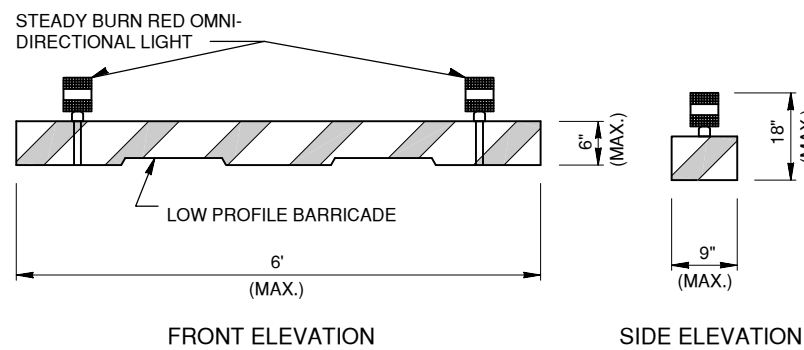


CONSTRUCTION SETBACK LINE DETAIL

N.T.S.



ISOMETRIC



LOW PROFILE - BEAM BARRICADE DETAILS

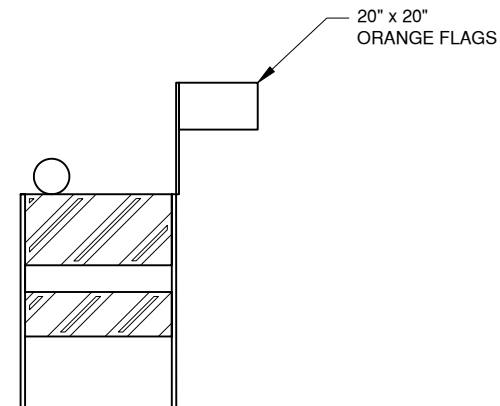
N.T.S.

SECURITY NOTES

1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. AT NO TIME ARE UNSUPERVISED GATE OPENINGS PERMITTED AT THE AIRPORT. IF THE PROPOSED WORK REQUIRES GATE OPENING, THE CONTRACTOR SHALL PROVIDE A GUARD TO PREVENT UNAUTHORIZED ACCESS. THE CONTRACTOR SHALL SECURE THE GATE AT THE COMPLETION OF THE WORK DAY.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES DURING THE COURSE OF THE WORK.
4. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

CONSTRUCTION ACTIVITY PLAN GENERAL NOTES

1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT AND THE PROJECT.
5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. VEHICLE ACCESS ON PERIMETER ROADS SHALL BE MAINTAINED THROUGH OUT THE PROJECT.
7. THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENANT AND CONTRACTOR OPERATIONS.
8. CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
9. THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
10. CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE ALLOWED TO CLOSE BOTH RUNWAYS AT THE SAME TIME.
11. CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS SHALL REQUIRE A MINIMUM OF 72 HOURS NOTICE TO COORDINATION WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION.
12. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT OPERATIONS BY THE CONTRACTOR.
 - C. THE CONTRACTOR SHALL USE AN EXISTING GATE(S), OR NEW GATES AS CALLED OUT IN THE PLANS, FOR ACCESS TO THE AIRFIELD.
 - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - G. THE CONTRACTOR SHALL CLOSE AND SECURE THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.



FLASHER BARRICADE DETAIL-IDOT TYPE 1

N.T.S.

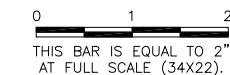
FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 10' INTERVALS.

PE094

REVISIONS

NUMBER	BY	DATE



GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 CONSTRUCTION ACTIVITY PLAN NOTES AND
 DETAILS

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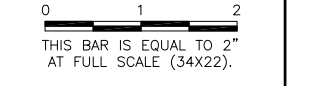


GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

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IL PROJ. NO. PIA-4180	
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SHEET	04 OF 35 SHEETS

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GENERAL WAYNE A. DOWNING
INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

EXPAND TERMINAL APRON
CONSTRUCTION ACTIVITY PLAN 1

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INTERNATIONAL AIRPORT

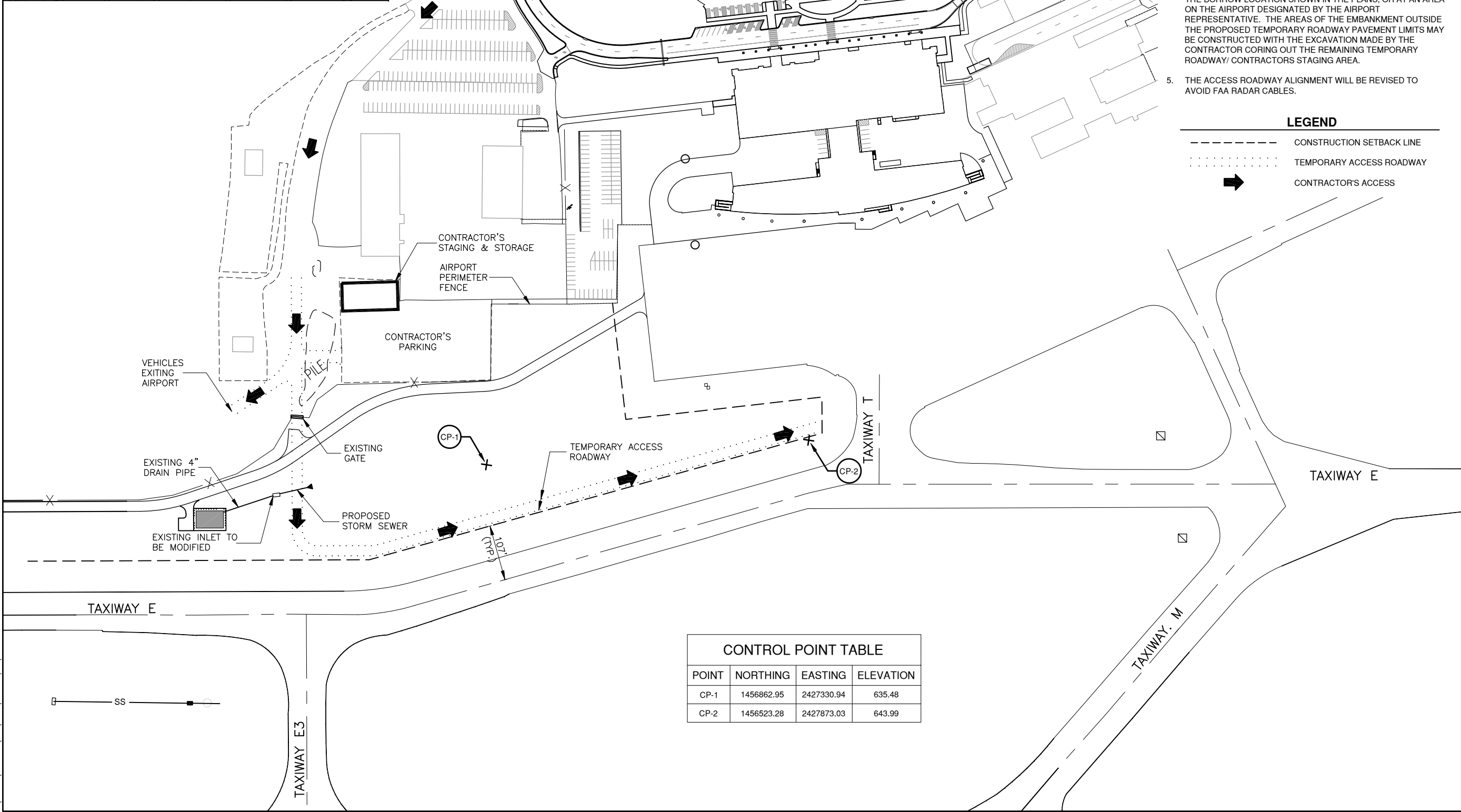
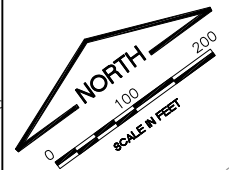
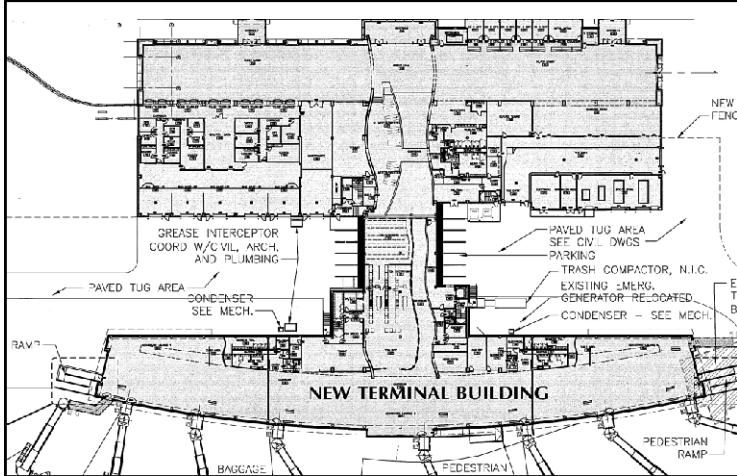
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JOB No:	12061-03
IL. PROJ. NO. PIA-4180	
AIP PROJ. NO. 3-17-0080-XX	
SHEET	05 OF 35 SHEETS

PHASE 1 NOTES

1. PHASE 1 SHALL CONSIST OF PLACEMENT OF CONSTRUCTION SETBACK LINE AS SHOWN ON THIS SHEET AND CONSTRUCTION OF THE CONTRACTOR'S TEMPORARY ACCESS ROADWAY.
2. PRIOR TO BEGINNING ANY WORK ON THE AIRFIELD, THE CONTRACTOR SHALL INSTALL CONSTRUCTION LATHES AT THE LOCATION SHOWN ON THIS SHEET TO DELINEATE THE WORK AREA. NO EQUIPMENT OR PERSONNEL SHALL BE ALLOWED OUTSIDE OF THE WORK AREA.
3. THE CONTRACTOR'S ACCESS SHALL BE USING THE AIRPORT ENTRANCE ROADWAY TO ACCESS THE SITE. VEHICLES DEPARTING THE SITE SHALL BE ROUTED WEST ALONG THE OUTSIDE PERIMETER ROADWAY AND TO THE PUBLIC ROADWAY USING THE T-HANGAR ACCESS ROADWAY. NO CONSTRUCTION TRAFFIC SHALL USE THE TERMINAL FRONTAL DRIVE.
4. THE CONTRACTOR SHALL MODIFY THE EXISTING INLET STRUCTURE AS DETAILED IN THE PLANS AND INSTALL THE NEW STORM SEWER PIPE. THE CONSTRUCTION OF THE EMBANKMENT SHALL UTILIZE EARTH MATERIAL EXCAVATED AT THE BORROW LOCATION SHOWN IN THE PLANS, OR AT AN AREA ON THE AIRPORT DESIGNATED BY THE AIRPORT REPRESENTATIVE. THE AREAS OF THE EMBANKMENT OUTSIDE THE PROPOSED TEMPORARY ROADWAY PAVEMENT LIMITS MAY BE CONSTRUCTED WITH THE EXCAVATION MADE BY THE CONTRACTOR CORING OUT THE REMAINING TEMPORARY ROADWAY/ CONTRACTORS STAGING AREA.
5. THE ACCESS ROADWAY ALIGNMENT WILL BE REVISED TO AVOID FAA RADAR CABLES.

LEGEND

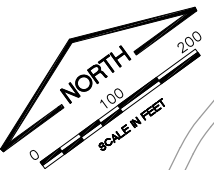
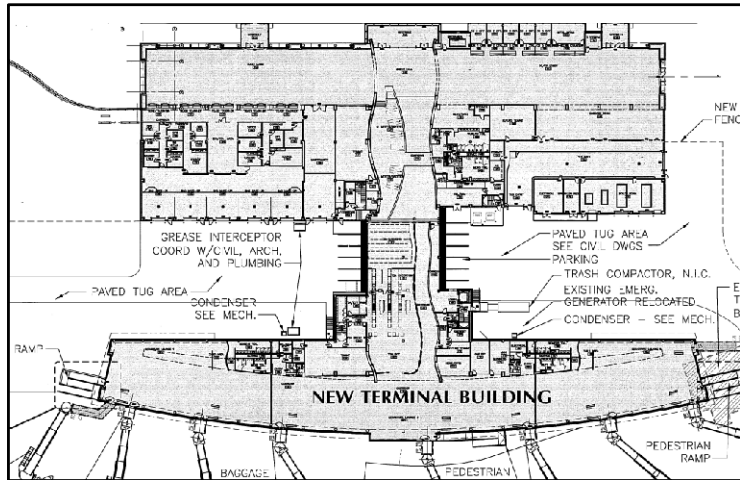
- CONSTRUCTION SETBACK LINE
- TEMPORARY ACCESS ROADWAY
- ➔ CONTRACTOR'S ACCESS



CONTROL POINT TABLE

POINT	NORTHING	EASTING	ELEVATION
CP-1	1456862.95	2427330.94	635.48
CP-2	1456523.28	2427873.03	643.99

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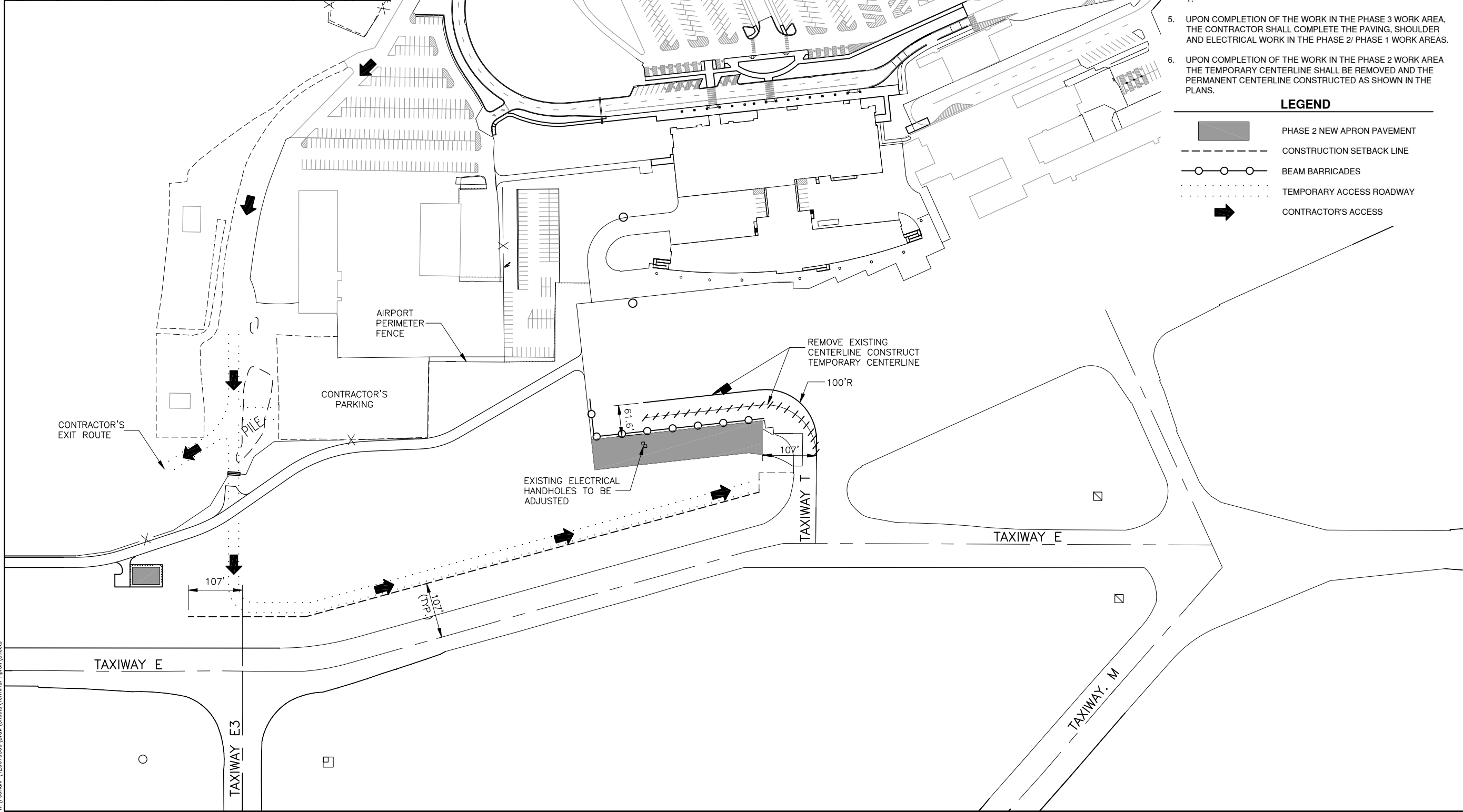


PHASE 2 NOTES

1. TAXIWAY T WILL REMAIN OPEN DURING PHASE 2.
2. PRIOR TO BEGINNING WORK IN THE PHASE 2 WORK AREA, THE CONTRACTOR SHALL REMOVE THE EXISTING TAXILANE CENTERLINE AND MARK A NEW 6" CENTERLINE AS SHOWN. THIS NEW CENTERLINE WILL MAINTAIN THE FAA REQUIRED WINGTIP CLEARANCE TO A LINE 5' INSIDE THE EDGE OF THE EXISTING PAVEMENT. THE CONTRACTOR'S EQUIPMENT WILL NOT BE ALLOWED BEYOND THIS LINE WHEN AIRCRAFT ARE ENTERING AND EXITING THE GATES.
3. THE PHASE 2 WORK SHALL CONSIST OF EMBANKMENT, BASE COURSE AND PREPARATION FOR PAVEMENT CONSTRUCTION FOR THE NEW WORK OUTSIDE OF THE LIMIT WHICH IS 107' FROM THE CENTERLINE OF TAXIWAYS T AND E.
4. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE TAXIWAY T AS SHOWN ON CONSTRUCTION ACTIVITY PLAN 3 FOR A PERIOD OF NOT MORE THAN TEN CONSECUTIVE CALENDAR DAYS TO COMPLETE THE PAVEMENT REMOVAL, BASE COURSE AND PAVING LOCATED WITHIN 107' OF THE CENTERLINE OF TAXIWAY T.
5. UPON COMPLETION OF THE WORK IN THE PHASE 3 WORK AREA, THE CONTRACTOR SHALL COMPLETE THE PAVING, SHOULDER AND ELECTRICAL WORK IN THE PHASE 2/ PHASE 1 WORK AREAS.
6. UPON COMPLETION OF THE WORK IN THE PHASE 2 WORK AREA THE TEMPORARY CENTERLINE SHALL BE REMOVED AND THE PERMANENT CENTERLINE CONSTRUCTED AS SHOWN IN THE PLANS.

LEGEND

- PHASE 2 NEW APRON PAVEMENT
- CONSTRUCTION SETBACK LINE
- BEAM BARRICADES
- TEMPORARY ACCESS ROADWAY
- CONTRACTOR'S ACCESS

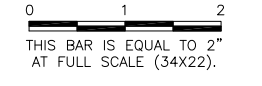


FILE: CONSTRUCTION ACTIVITY PLAN 5.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 11:20 AM

PIA-Base-Existing
 BASE_PROP_JOINT
 BASE_PROP_GEO

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PEORIA, ILLINOIS

EXPAND TERMINAL APRON
CONSTRUCTION ACTIVITY PLAN 2

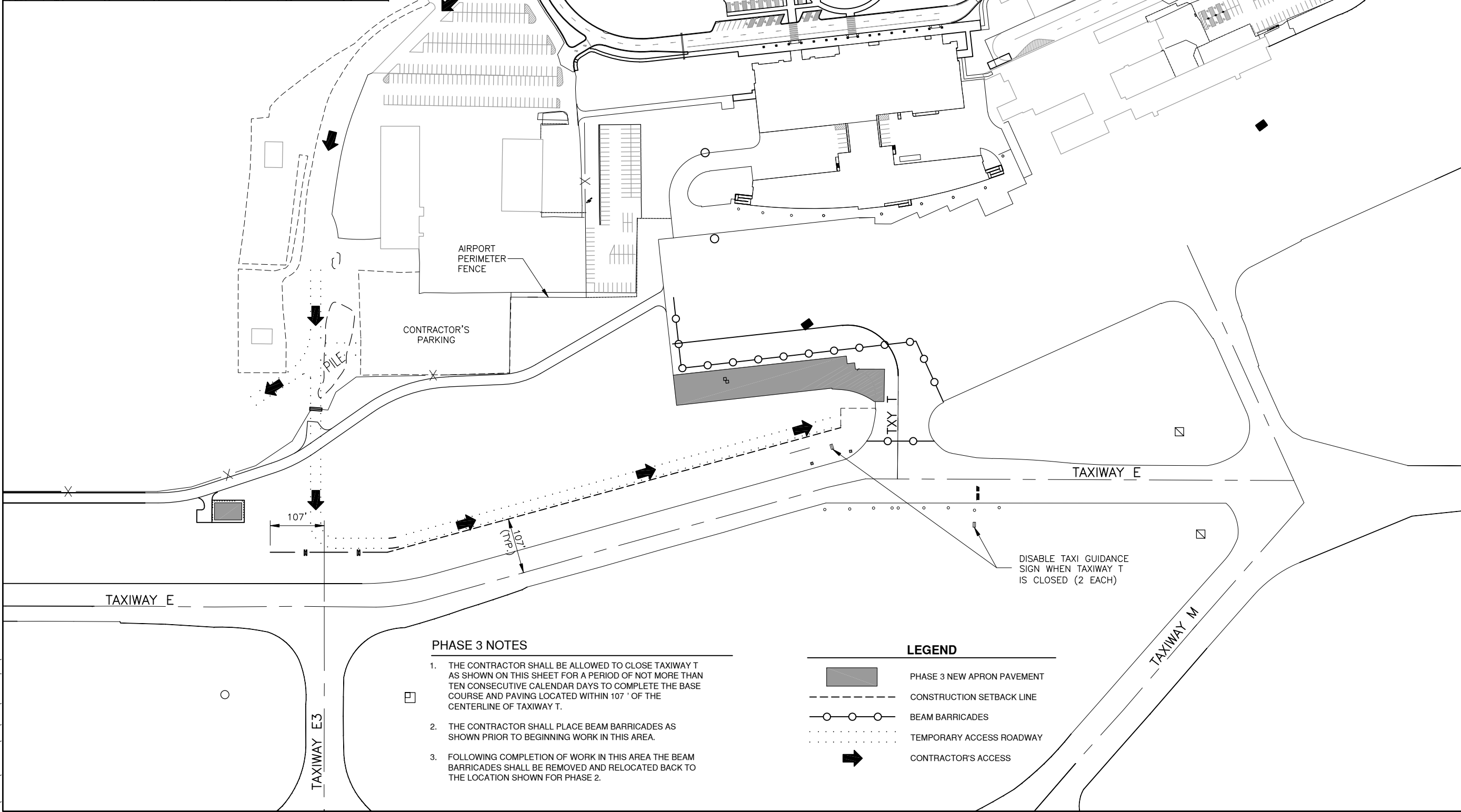
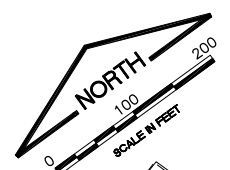
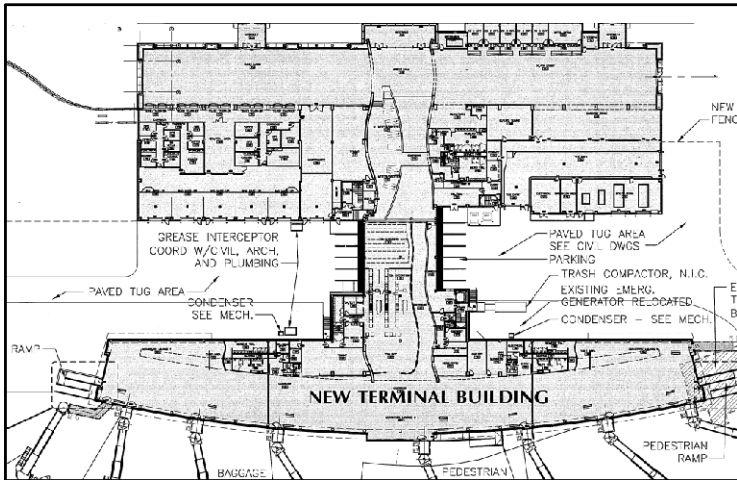
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SHEET 06 OF 35 SHEETS	

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PHASE 3 NOTES

1. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE TAXIWAY T AS SHOWN ON THIS SHEET FOR A PERIOD OF NOT MORE THAN TEN CONSECUTIVE CALENDAR DAYS TO COMPLETE THE BASE COURSE AND PAVING LOCATED WITHIN 107' OF THE CENTERLINE OF TAXIWAY T.
2. THE CONTRACTOR SHALL PLACE BEAM BARRICADES AS SHOWN PRIOR TO BEGINNING WORK IN THIS AREA.
3. FOLLOWING COMPLETION OF WORK IN THIS AREA THE BEAM BARRICADES SHALL BE REMOVED AND RELOCATED BACK TO THE LOCATION SHOWN FOR PHASE 2.

LEGEND

- PHASE 3 NEW APRON PAVEMENT
- CONSTRUCTION SETBACK LINE
- BEAM BARRICADES
- TEMPORARY ACCESS ROADWAY
- CONTRACTOR'S ACCESS

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EXPAND TERMINAL APRON

CONSTRUCTION ACTIVITY PLAN 3

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SHEET 07 OF 35 SHEETS	

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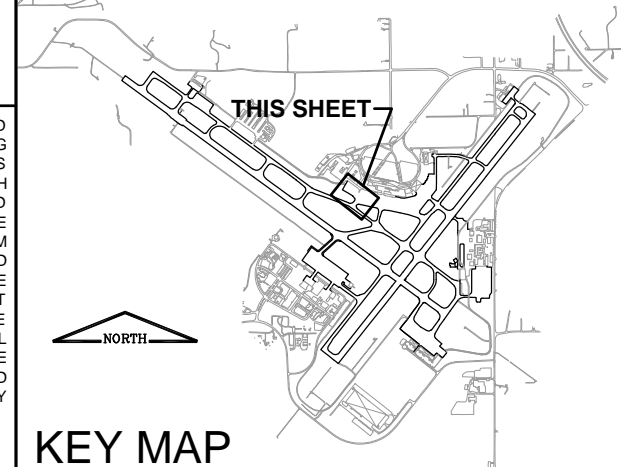


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Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.



KEY MAP

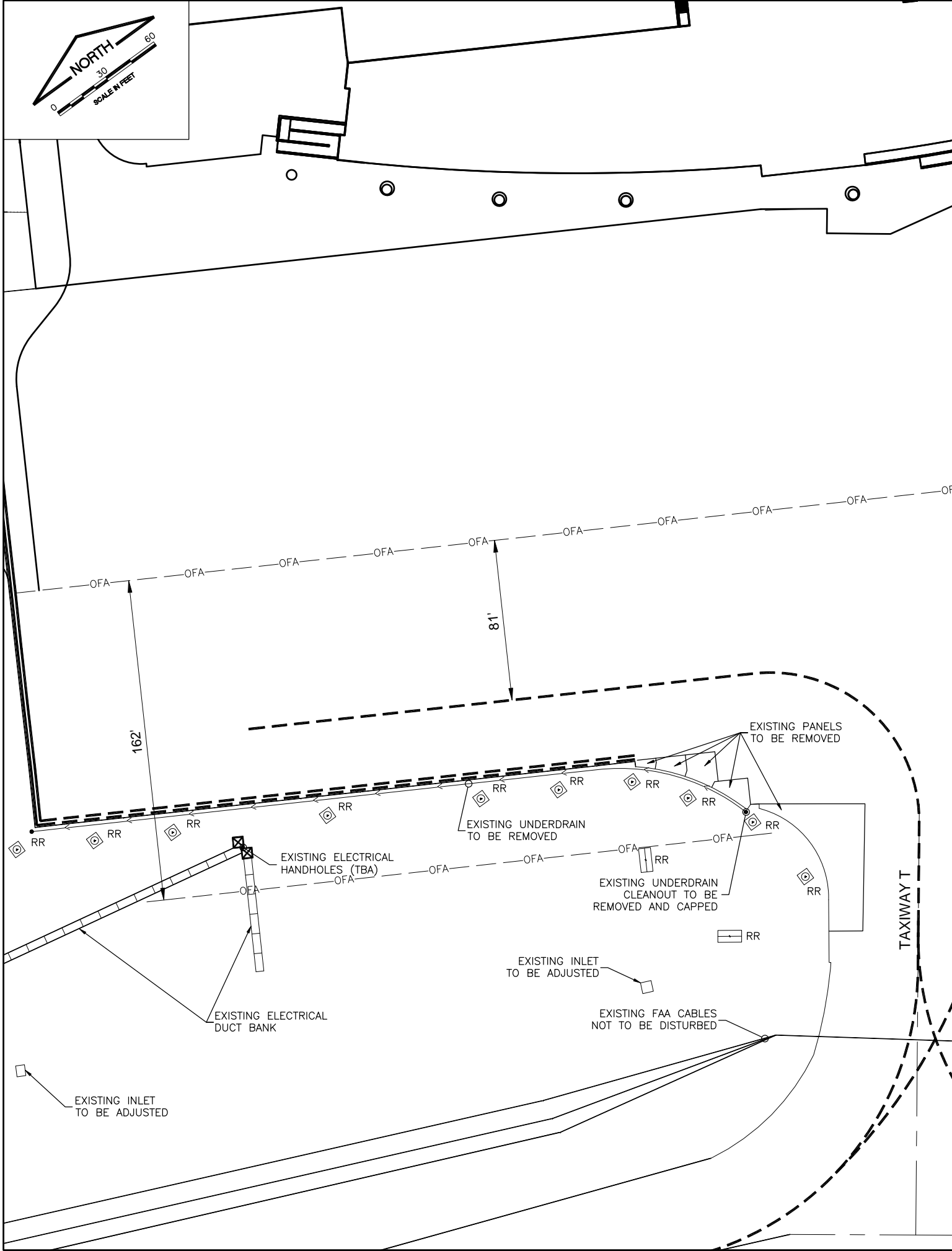
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FILE: EXCON02.dwg
UPDATE BY: TJ Heavisesides
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KEYMAP

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NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
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NOTES:

- SEE THE TERMINAL GATE MARKING REMOVAL PLAN FOR REMOVAL OF THE TERMINAL GATE MARKINGS.

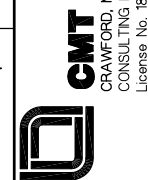
LEGEND

---	EXISTING PAVEMENT MARKING TO BE REMOVED
⊗	EXISTING BASE MOUNTED TAXIWAY LIGHT
—	FULL DEPTH SAW CUT ALONG EXISTING JOINTS (TYP.)
←	EXISTING UNDER DRAIN
RR	TO BE REMOVED AND RELOCATED
TBA	TO BE ADJUSTED
+	EXISTING TAXIWAY GUIDANCE SIGN TO BE RELOCATED
□	EXISTING INLET

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EXPAND TERMINAL APRON
EXISTING CONDITIONS AND REMOVALS

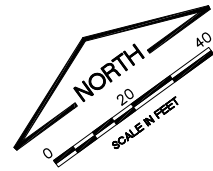
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SHEET	08 OF 35 SHEETS

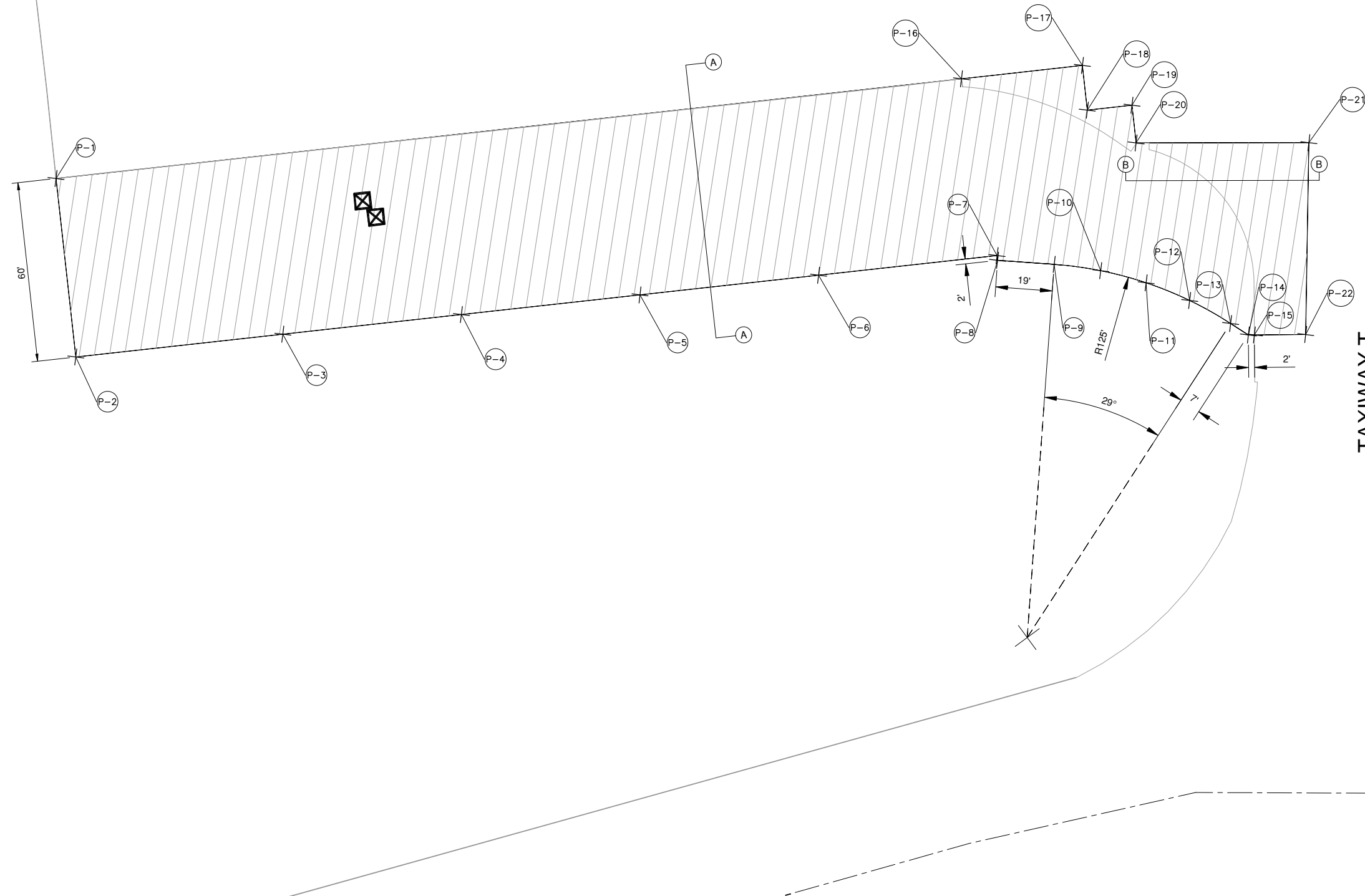


LEGEND

- EXISTING PAVEMENT
- ▨ PROPOSED 18" PCC PAVEMENT (501)
- ▨ PROPOSED 8" CRUSHED AGGREGATE BASE COURSE (209)
- ▨ PROPOSED 12" OVERSIZE AGGREGATE WORKING PLATFORM (209)
- ⊠ EXISTING ELECTRICAL HANDHOLES (TO BE ADJUSTED)

NOTES:

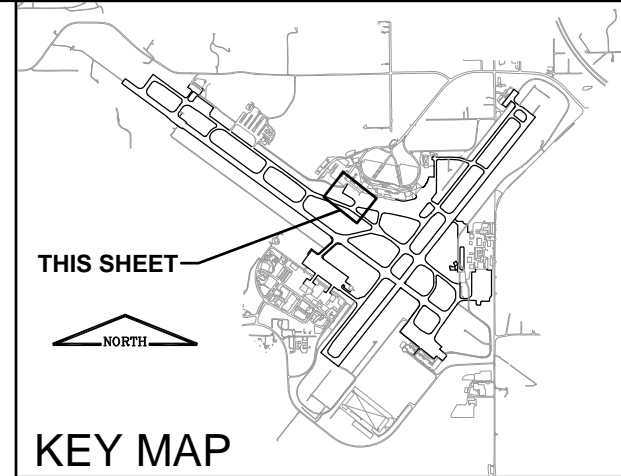
1. PANELS TO BE REMOVED AND REPLACED SHALL BE REINFORCED AND DOWELED TO EXISTING PAVEMENT AND ALL NEW JOINTS SHALL BE DOWELED. JOINT SPACING SHALL MATCH THE EXISTING.



THIS SHEET



KEY MAP



FILE: PROPOSED IMPROVEMENTS 3.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 11:20 AM

PIA-Base-Existing
 KEYMAP

PE094

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 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 PROPOSED IMPROVEMENTS

EDGE OF PAVEMENT

POINT	NORTHING	EASTING
P-1	1456798.496	2427696.113
P-2	1456746.467	2427666.230
P-3	1456711.793	2427726.520
P-4	1456681.880	2427778.531
P-5	1456651.967	2427830.543
P-6	1456622.054	2427882.555
P-7	1456592.141	2427934.566
P-8	1456590.755	2427933.770
P-9	1456578.499	2427948.211
P-10	1456567.616	2427959.513
P-11	1456555.402	2427969.363
P-12	1456542.052	2427977.605
P-13	1456527.769	2427984.111
P-14	1456521.251	2427986.628
P-15	1456520.004	2427988.251
P-16	1456646.817	2427959.846
P-17	1456626.588	2427995.085
P-18	1456613.561	2427987.583
P-19	1456606.083	2428000.586
P-20	1456595.100	2427994.261
P-21	1456561.098	2428040.869
P-22	1456509.990	2428002.152

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SHEET	09 OF 35 SHEETS

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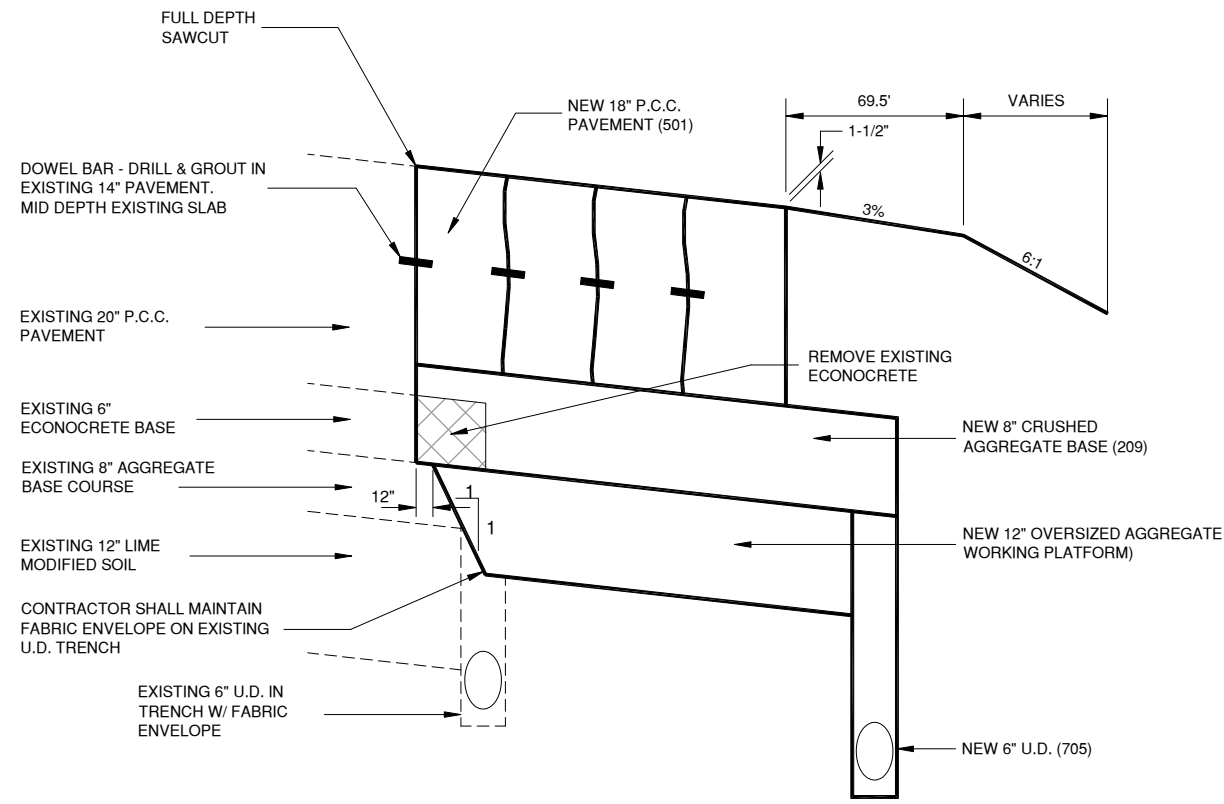
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EXPAND TERMINAL APRON
 TYPICAL SECTIONS

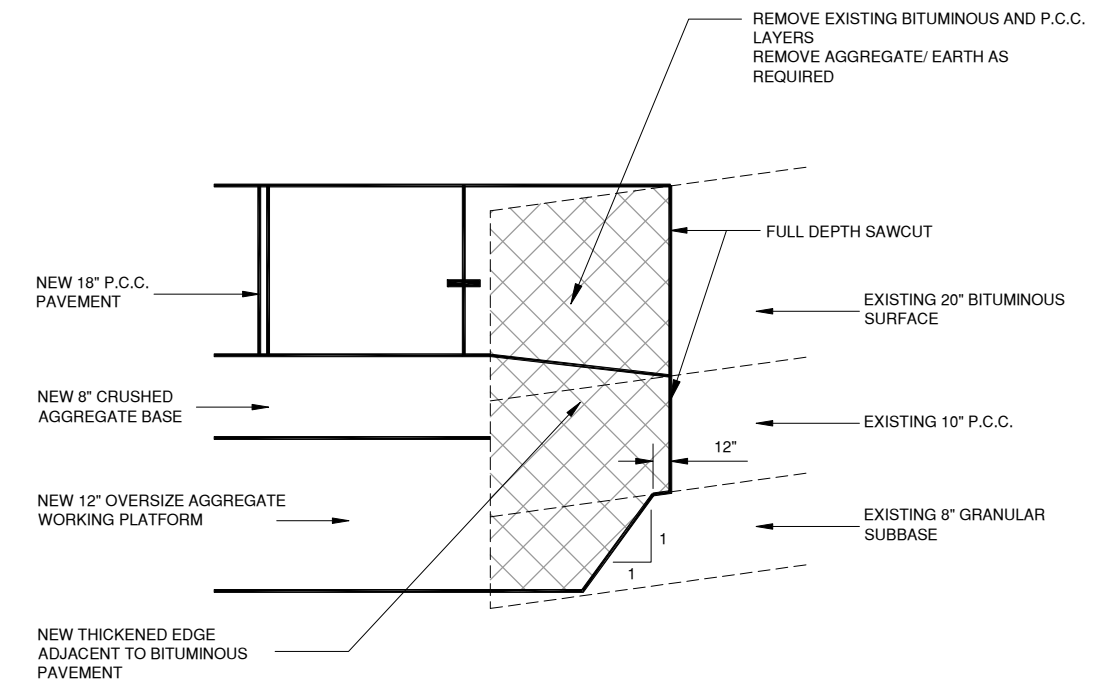
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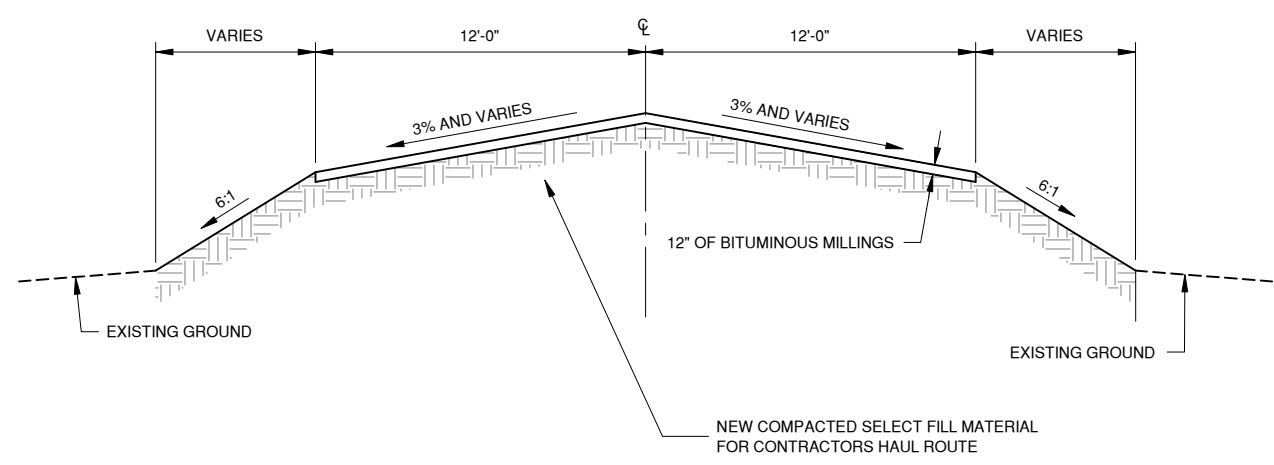
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SHEET 10 OF 35 SHEETS	



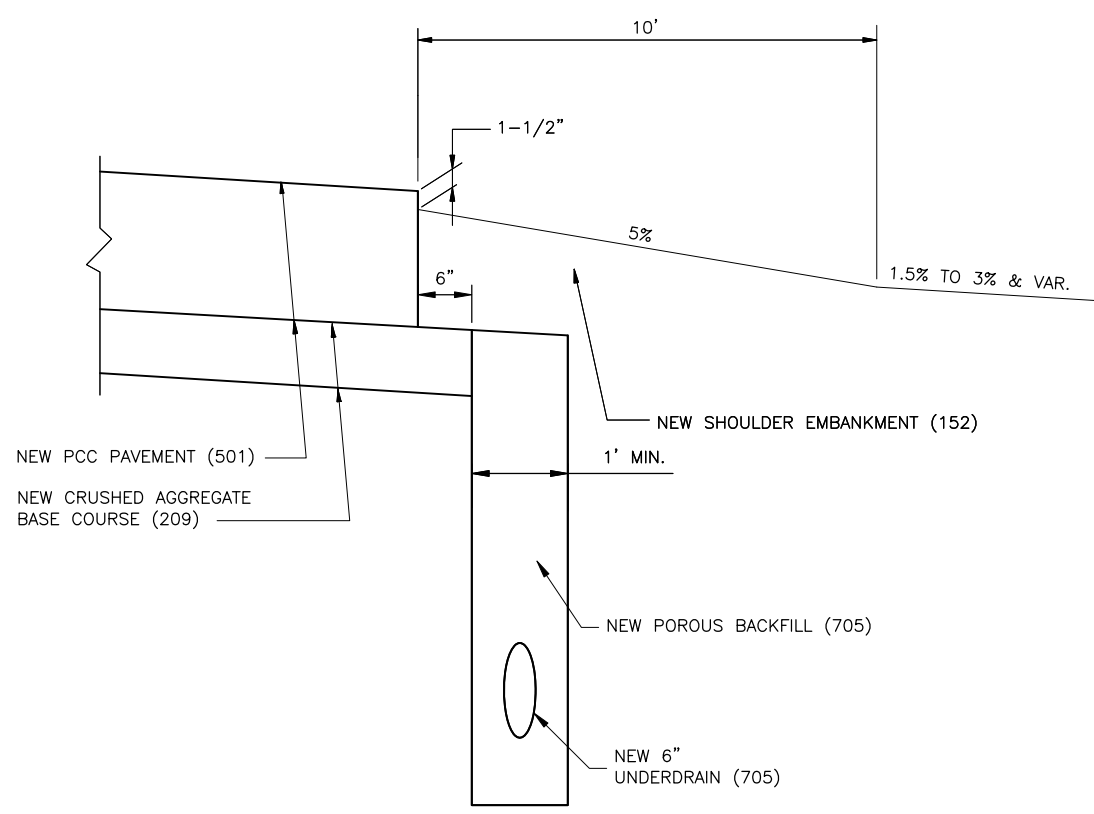
A PROPOSED TYPICAL SECTION A
 N.T.S.



B PROPOSED TYPICAL SECTION B
 N.T.S.

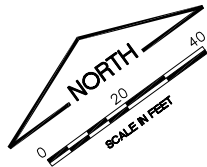


**PROPOSED TERMINAL APRON
 HAUL ROUTE SECTION**
 N.T.S.







NEW SHOULDER DETAIL "A"
 N.T.S.

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LEGEND

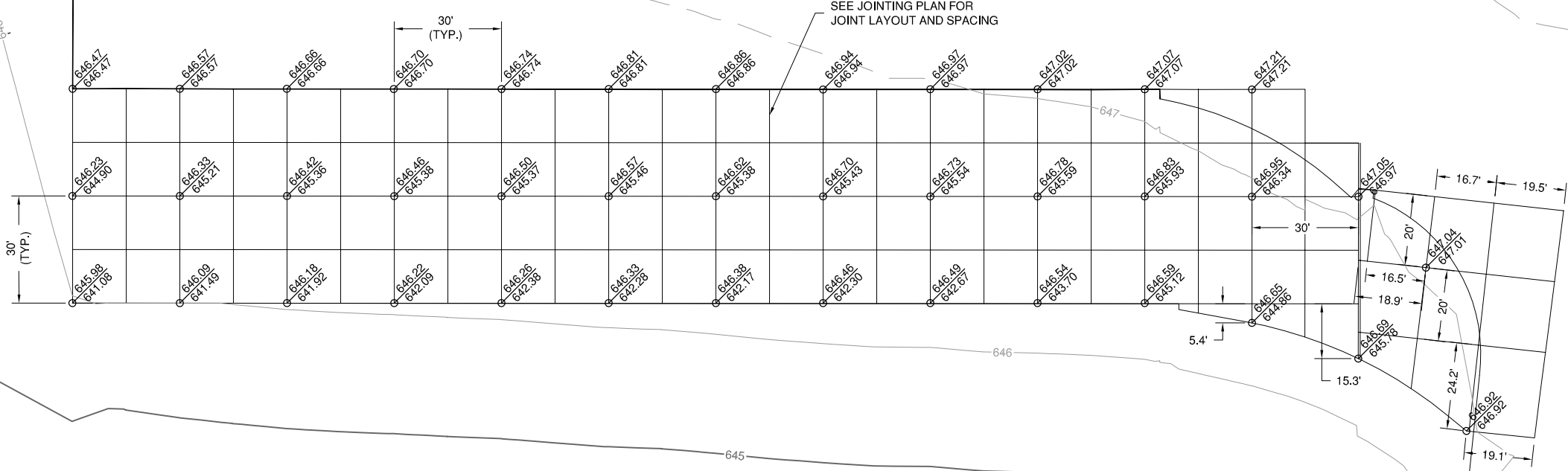
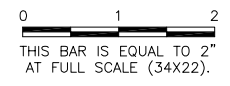
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-  EXISTING ELEVATION
-  NEW CONTOUR
-  EXISTING CONTOUR

FILE: STAKING PLAN 4.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 2:39 PM

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**EXPAND TERMINAL APRON
 STAKING PLAN**

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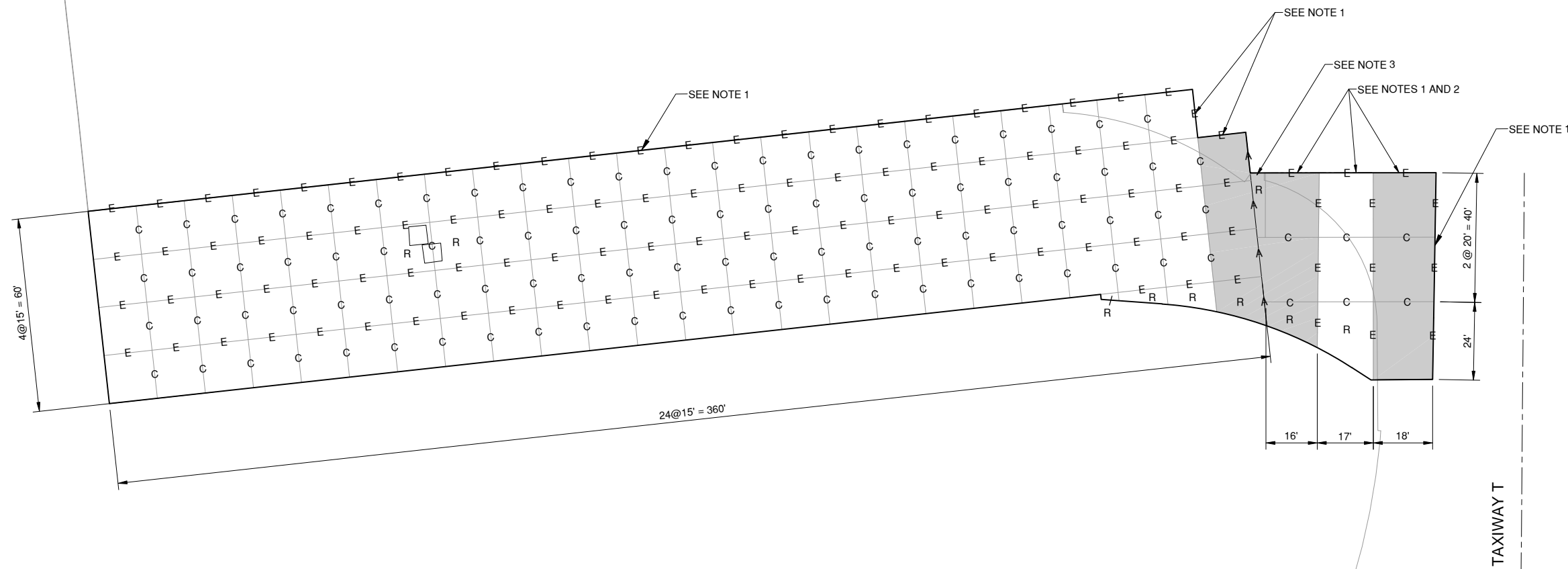
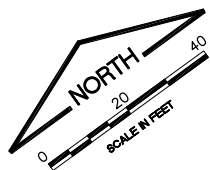
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SHEET	11 OF 35 SHEETS

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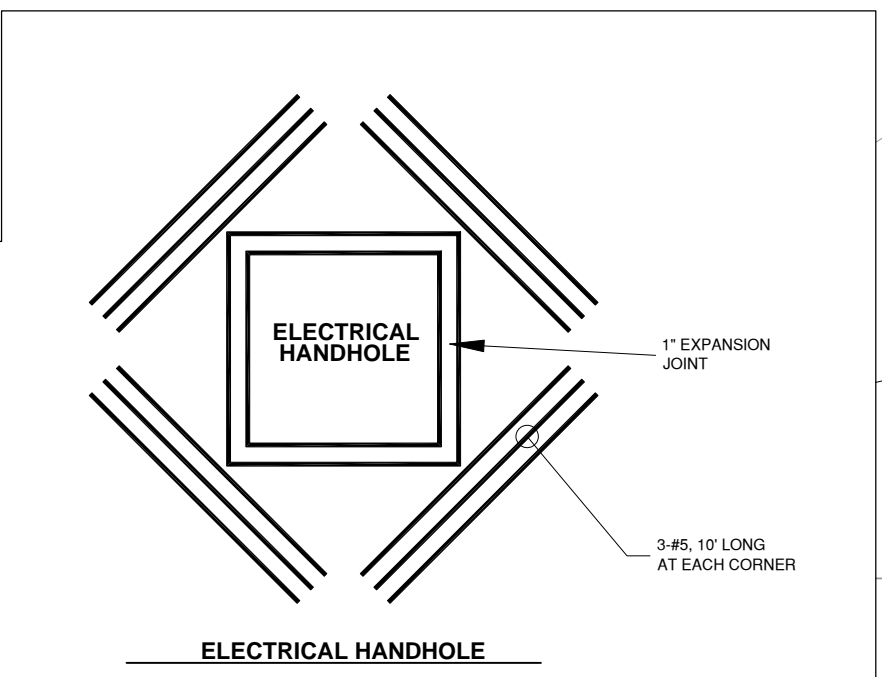
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0 1 2
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NOTE:
 THE CONTRACTOR MAY SUBMIT AN ALTERNATE JOINTING/PAVING PLAN FOR REVIEW AND CONSIDERATION PRIOR TO INITIATING THE PAVING OPERATIONS. THE PAVING PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO, THE CONTRACTOR'S PROPOSED CHANGES TO PAVING DIRECTION, PROPOSED CHANGES TO PAVING LANE WIDTHS, PROPOSED CHANGES TO THE JOINTING TYPE, CONTRACTOR'S PLAN FOR "BOXING OUT" AROUND INLETS, LIGHTS, HAND-HOLES, OR OTHER PAVEMENT PENETRATIONS, PLANS TO PREVENT PREMATURE CRACKING IN "BOXED OUT" PANELS, CONTRACTOR'S PLAN FOR PAVING AGAINST ADJACENT STRUCTURES, AND ANY OTHER REVISION OR ALTERATION THAT THE CONTRACTOR SHOULD PROPOSE THAT DIFFERS FROM THE PLAN SHEETS.




LEGEND

— A —	TYPE A ISOLATION JOINT (1")
— B —	TYPE B HINGED (TIED) CONTRACTION JOINT
— C —	TYPE C DOWELED CONTRACTION JOINT
— D —	TYPE D DUMMY CONTRACTION JOINT
— E —	TYPE E DOWELED CONSTRUCTION JOINT
R	REINFORCED PANEL
■	THICKENED EDGE

- NOTES**
1. THE CONTRACTOR SHALL DRILL AND GROUT DOWEL BARS INTO THE EXISTING PAVEMENT.
 2. EXTEND EXISTING JOINT PATTERN.
 3. NO DOWELS TO BE DRILLED IN THIS SECTION OF JOINT.
 4. IF THE CONTRACTOR CHOOSES TO PAVE IN 30' LANES, THE CENTER JOINT SHALL BE A DOWELED CONTRACTION JOINT.

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PEORIA, ILLINOIS
EXPAND TERMINAL APRON
JOINTING PLAN

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AIP PROJ. NO. 3-17-0080-XX	
SHEET 12 OF 35 SHEETS	

TABLE 1	
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I=(T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"

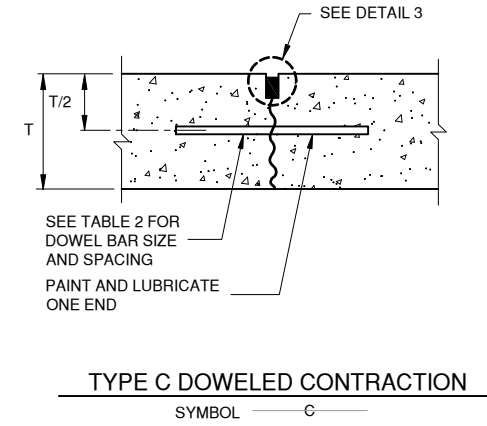
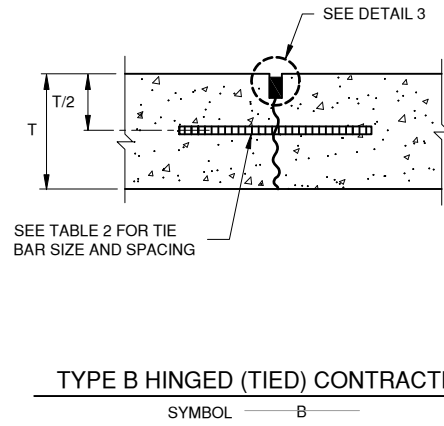
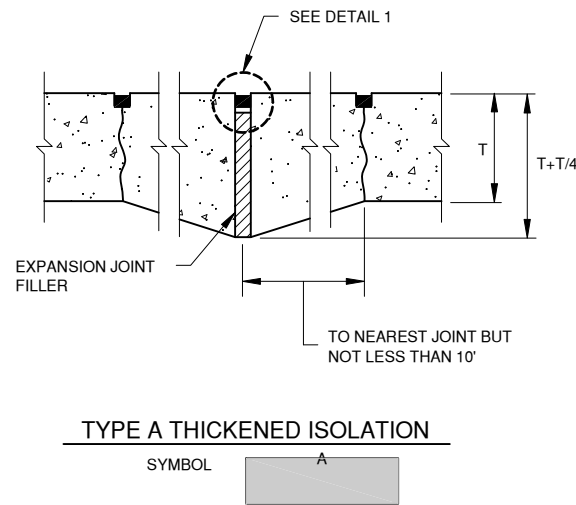
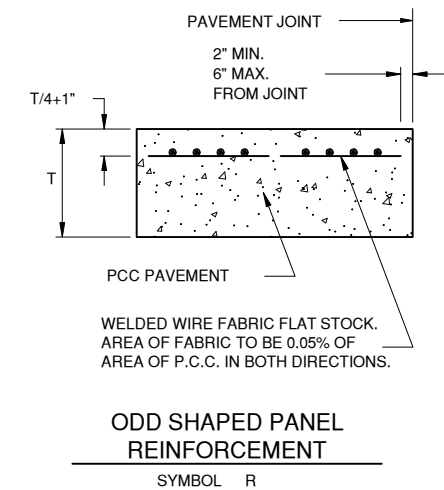
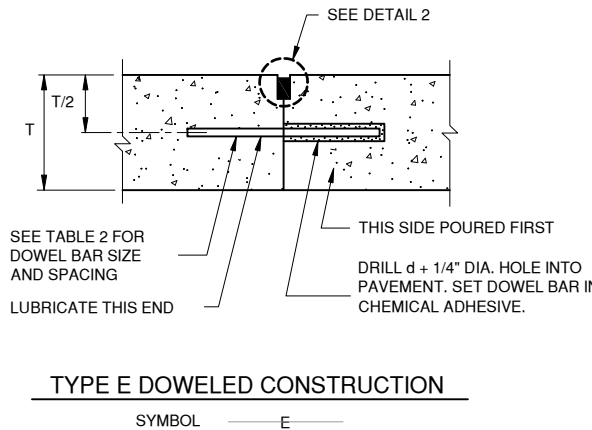
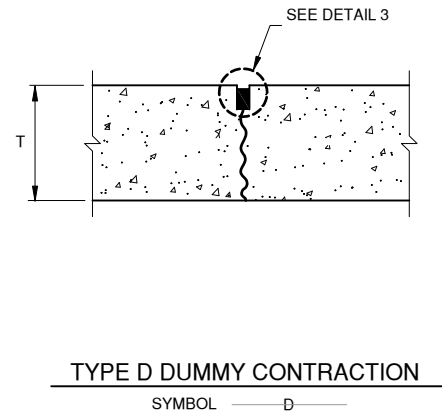


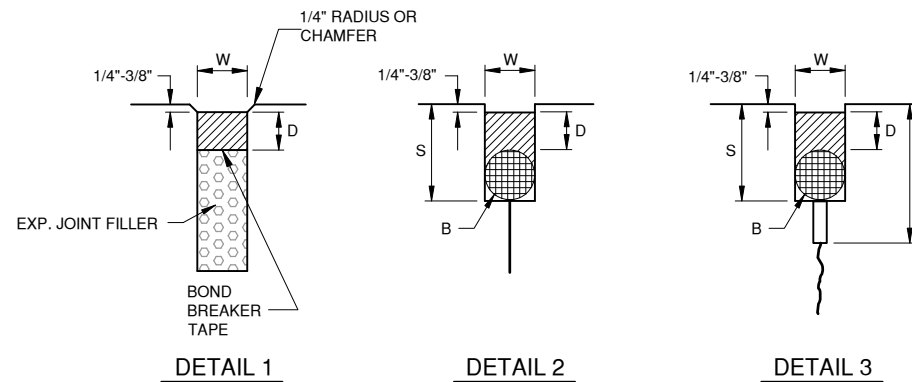
TABLE 2						
PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"



JOINT SEALING DIMENSIONS			
	DETAIL 1	DETAIL 2	DETAIL 3
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/8	1/4	1/4
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-1/8	1-1/8

JOINT NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

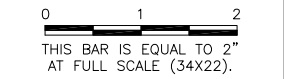


JOINT SEALING DETAILS

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GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 JOINTING DETAILS



DESIGN BY:	CET/TJH
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	APRIL 26, 2013
JOB No:	12061-03
IL PROJ. NO. PIA-4180	
AIP PROJ. NO. 3-17-0080-XX	
SHEET 13 OF 35 SHEETS	

PE094

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

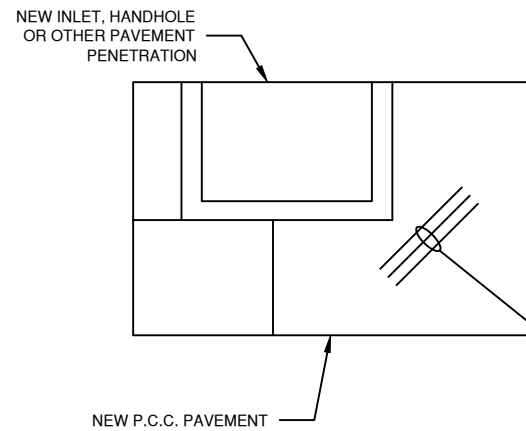
GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 PAVING AND MISCELLANEOUS DETAILS

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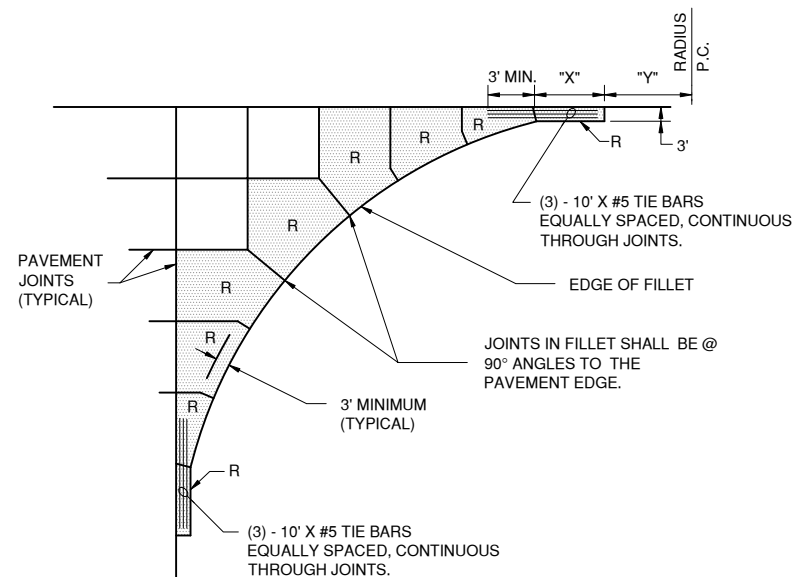


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SHEET	14 OF 35 SHEETS



PAVEMENT PENETRATION DETAIL
 N.T.S.

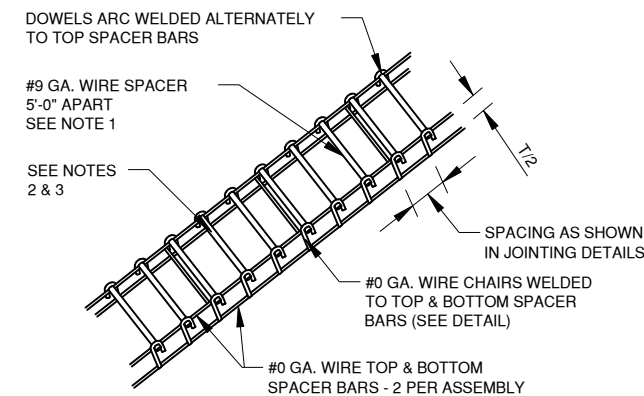
PLACE (3)- #5 TIE BARS IN NEW P.C.C. PAVEMENT ADJACENT TO CORNER OF PAVEMENT PENETRATION (TYPICAL OF ALL CORNERS)



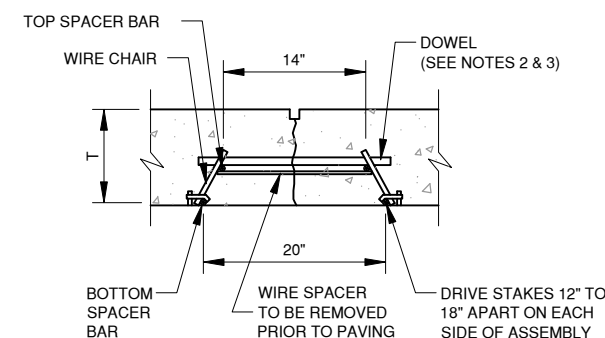
Ⓜ DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WELDED WIRE FABRIC AS SHOWN ON JOINTING DETAILS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)

FILLET RADIUS	"X" (IN FEET)	"Y" (IN FEET)
20	4.30	6.24
25	4.88	7.00
30	5.40	7.68
50	7.11	9.95
75	8.79	12.21
85	9.38	13.00
100	10.21	14.11
125	11.44	15.78
150	12.56	17.29
175	13.58	18.68
200	14.53	19.98

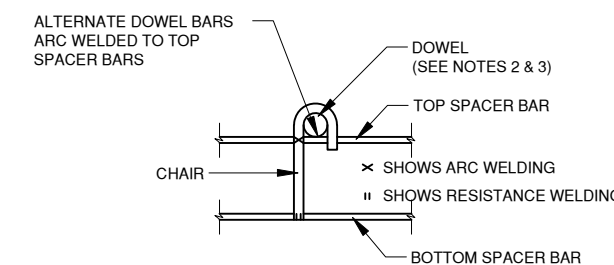
FILLET DETAIL & FILLET REINFORCING LAYOUT
 N.T.S.



DOWEL BASKET ASSEMBLY DETAIL



DOWEL BAR INSTALLATION DETAIL

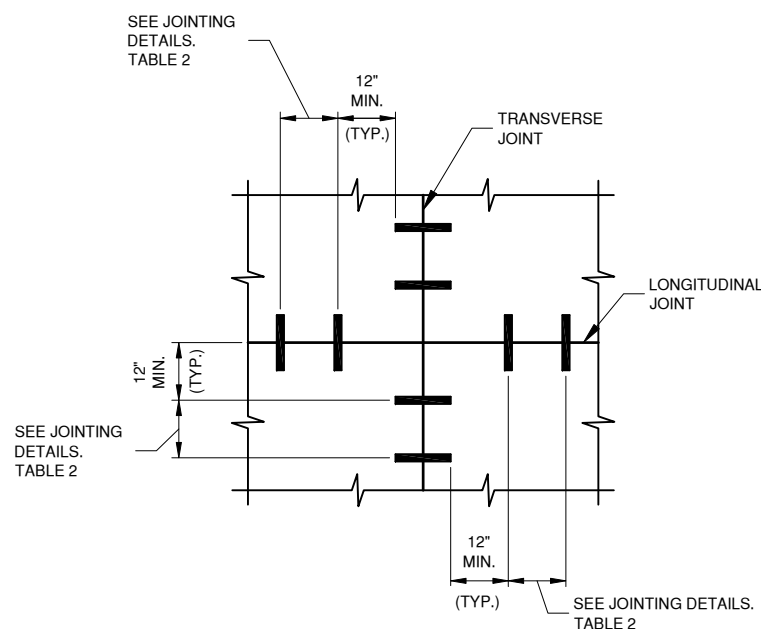


TYPICAL DOWEL BASKET ELEVATION DETAIL SHOWING CHAIR

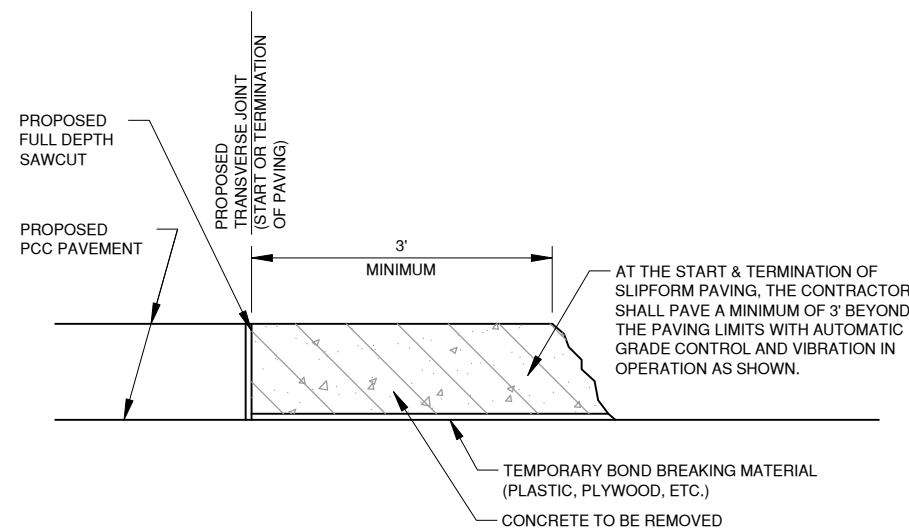
DOWEL BASKET DETAILS
 N.T.S.

DOWEL BASKET NOTES

- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.



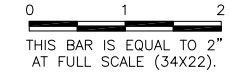
DOWEL PLACEMENT DETAIL
 N.T.S.



REQUIREMENTS AT START & TERMINATION OF SLIPFORM PAVING
 N.T.S.

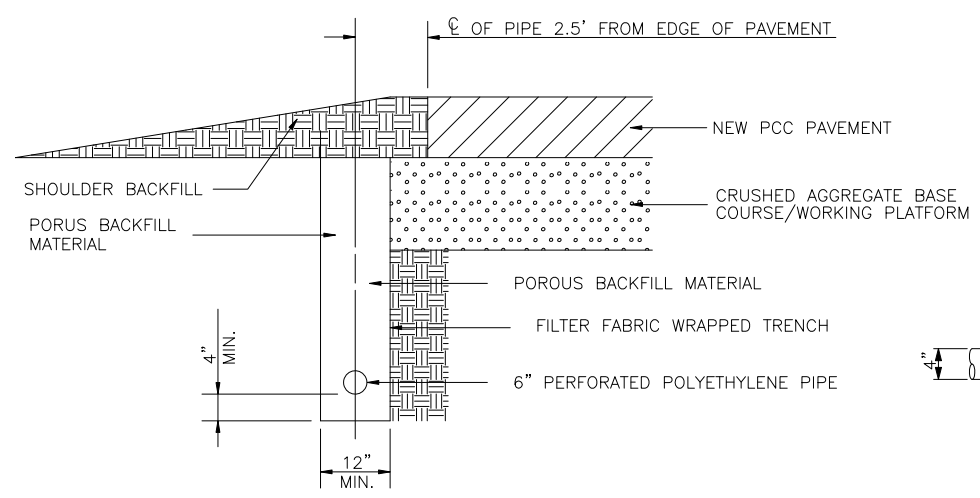
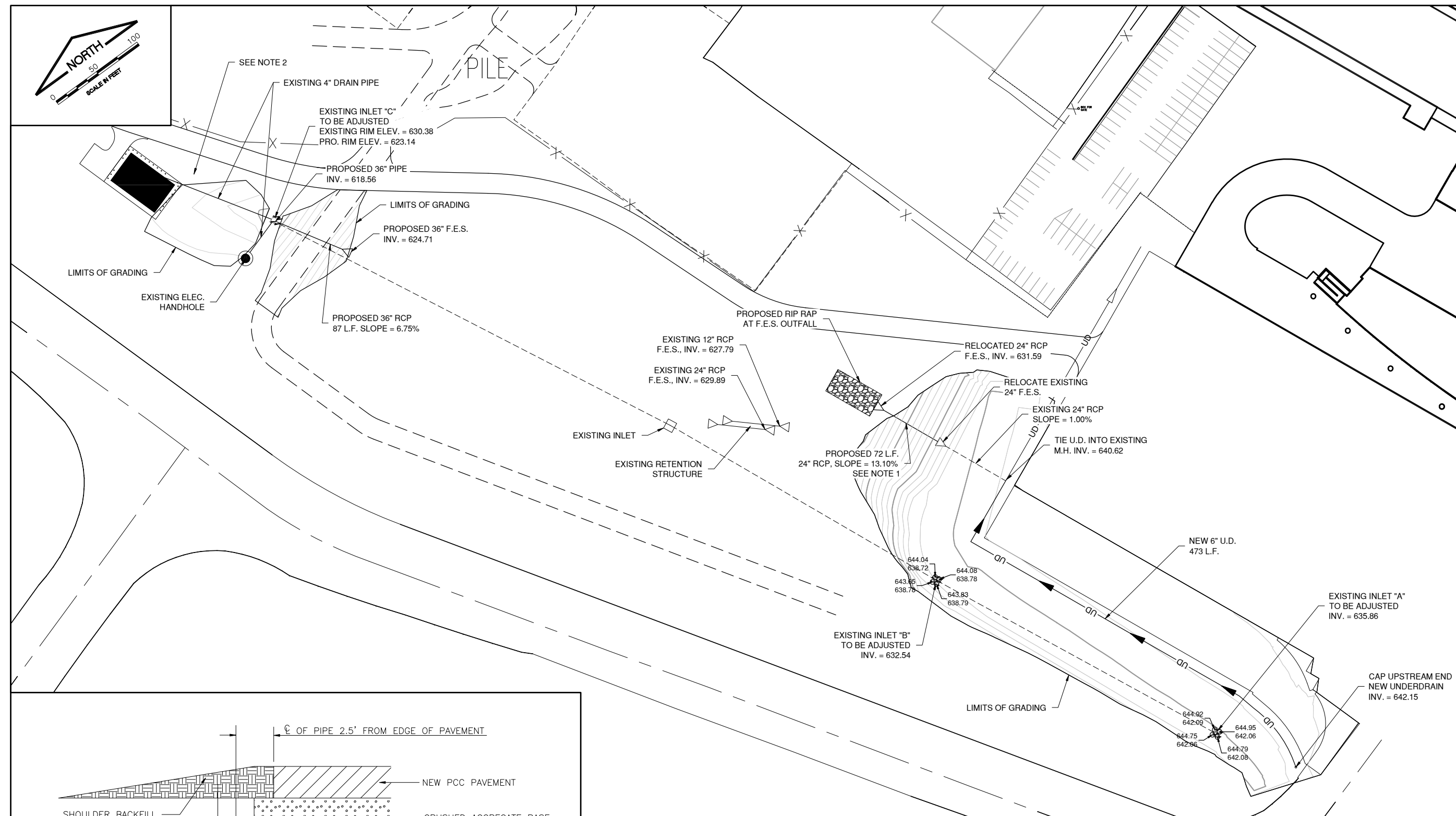
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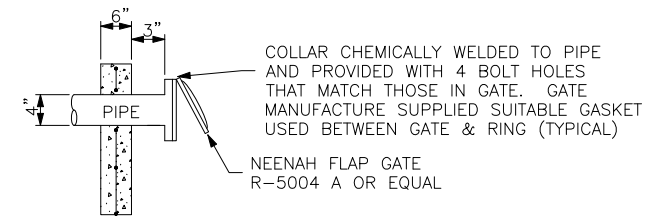


**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**EXPAND TERMINAL APRON
 DRAINAGE PLAN**



UNDERDRAIN DETAIL
 N.T.S.



UNDERDRAIN END SECTION
 N.T.S.

NOTES

1. THE PIPE LENGTH SHOWN ON THE PLANS IS SOLELY THE LENGTH OF PIPE. IT DOES NOT INCLUDE THE LENGTH OF THE FLARED END SECTION.
2. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF EXISTING FIBER OPTIC CABLE, GAS LINE, AND STORM SEWER IN THIS VICINITY BEFORE COMMENCING EXCAVATION.
3. THE CONTRACTOR SHALL INSTALL "BACKFLOW PREVENTERS" AS SHOWN IN THE UNDERDRAIN END SECTION DETAIL ON THIS SHEET FOR THE EXISTING 4" DRAIN PIPES CONNECTING TO INLET C. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE ADJUSTMENT OF INLET C.

LEGEND

- UD —> EXISTING UNDERDRAIN
- UD —> PROPOSED UNDERDRAIN
- △ FLARED END SECTION
- EXISTING INLET
- 590 — NEW CONTOUR
- ↖ 645.00 PROPOSED ELEVATION
- ↘ 645.00 EXISTING ELEVATION

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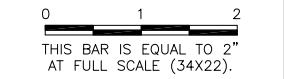
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DATE:	APRIL 26, 2013
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SHEET 15 OF 35 SHEETS	

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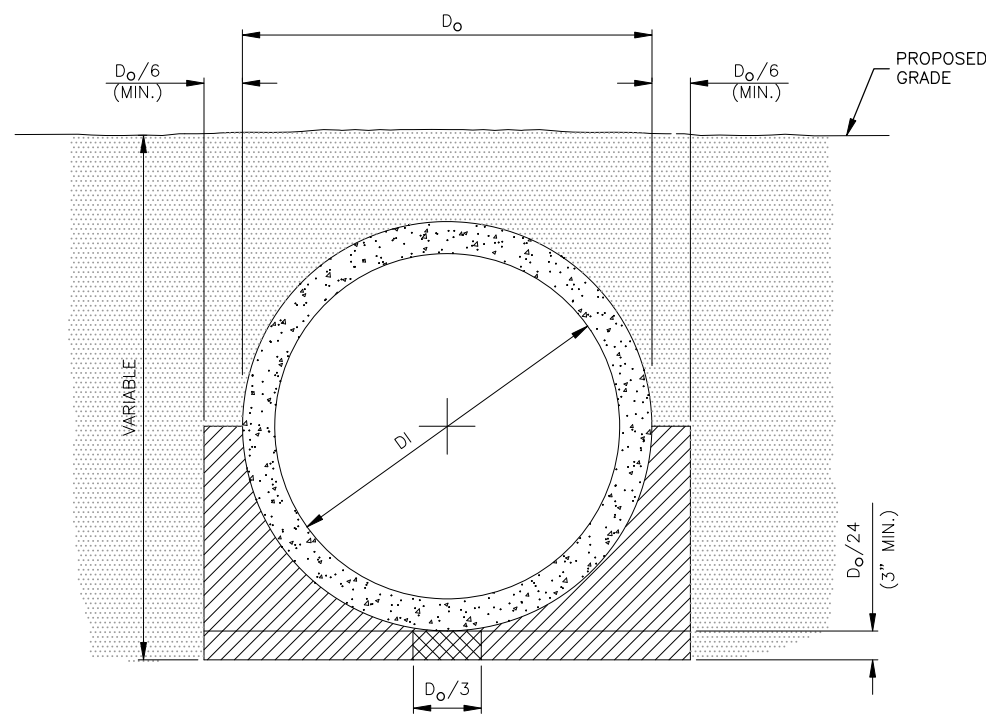


TRENCH INSTALLATION LEGEND

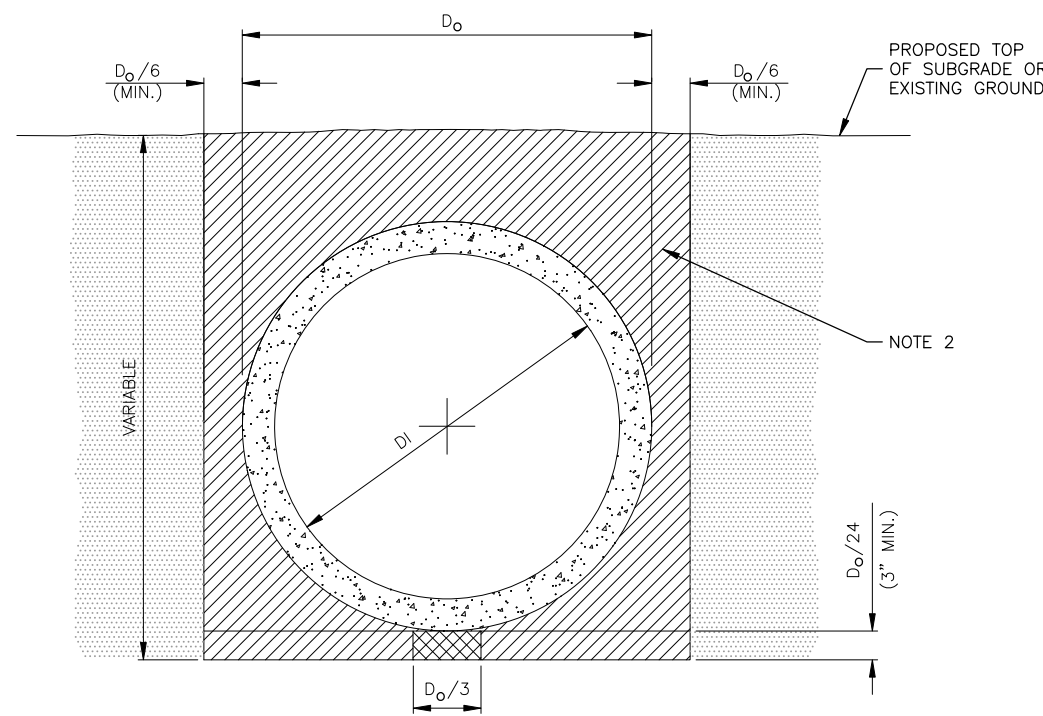
- DRAINAGE CONDUIT MATERIAL—CONCRETE
- MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
- HAUNCH AND OUTER BEDDING COMPACTION— TO ENGINEER'S SATISFACTION OR 95% STANDARD PROCTOR
- LOWER SIDE AND OVERFILL COMPACTION— SAME AS EMBANKMENT REQUIREMENTS
- D_o PIPE OUTSIDE DIAMETER
- D_i PIPE INSIDE DIAMETER

TRENCH INSTALLATION NOTES

- BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS", STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
- BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.



**STANDARD TRENCH INSTALLATION
 NON-PAVED AREA**
 N.T.S.



**STANDARD TRENCH INSTALLATION
 PROPOSED PAVED AREA**
 N.T.S.

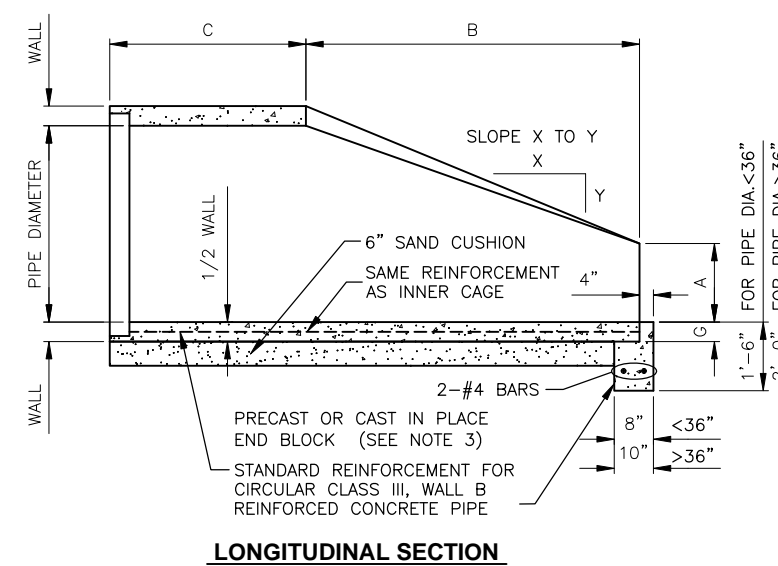
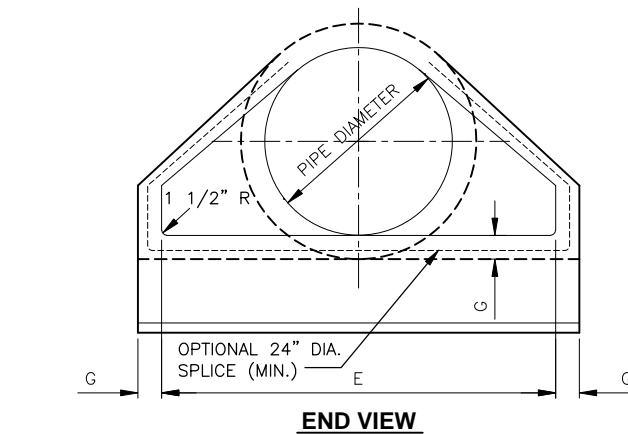
DIMENSIONS - TABLE 1

PIPE DIA.	APPROX WT.(lbs.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4"	2'-0"	4'-0 7/8"	6'-0 7/8"	2'-0"	2"	9"	3:1
15"	740	2 1/4"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2 1/4"	11"	3:1
18"	990	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2 1/2"	12"	3:1
21"	1280	2 3/4"	9"	2'-11"	3'-2"	6'-1"	3'-6"	2 3/4"	13"	3:1
24"	1520	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3"	14"	3:1
27"	1930	3 1/4"	10 1/2"	4'-0"	2'-1 1/2"	6'-1 1/2"	4'-6"	3 1/4"	14 1/2"	3:1
30"	2190	3 1/2"	1'-0"	4'-6"	1'-7 3/4"	6'-1 3/4"	5'-0"	3 1/2"	15"	3:1
33"	3200	3 3/4"	1'-1 1/2"	4'-10 1/2"	3'-3 1/4"	8'-1 3/4"	5'-6"	3 3/4"	17 1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	4"	20"	3:1
42"	5380	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4 1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5 1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5 1/2"	24"	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6 1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5 1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.86:1
78"	14770	7 1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6 1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6 1/2"	1'-9"	9'-3 1/2"	10'-0"	6 1/2"	*	1.5:1

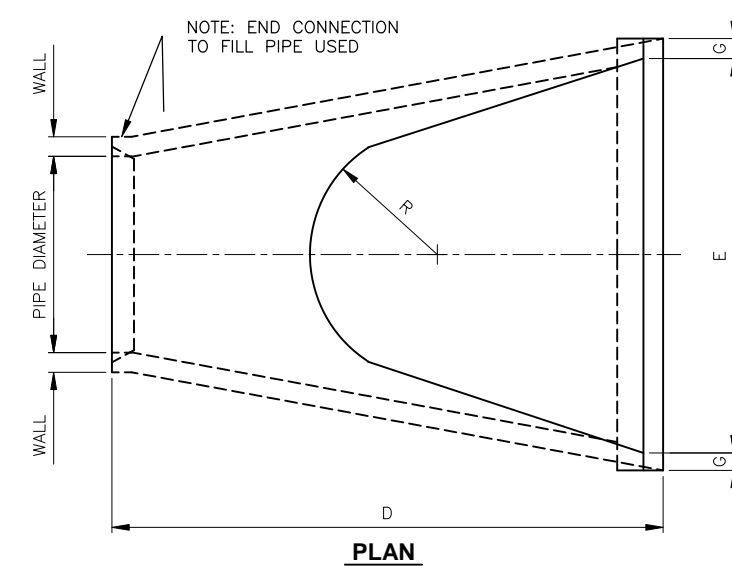
* RADIUS AS FURNISHED BY MANUFACTURER.

PRECAST FLARE SECTION NOTES

- PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
- PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
- THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 701.



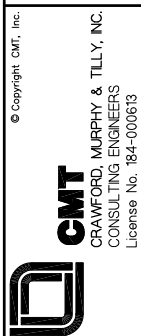
PRECAST REINFORCED CONCRETE FLARED END SECTION
 (I.D.O.T. STD. NO. 542301)
 N.T.S.



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EXPAND TERMINAL APRON
 DRAINAGE DETAILS 1

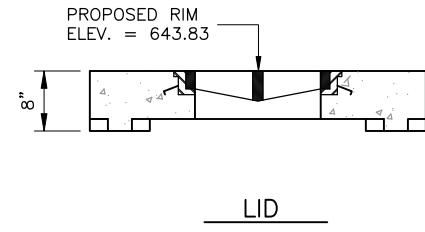
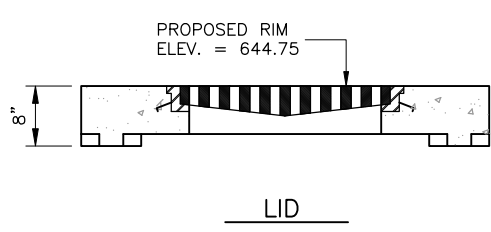
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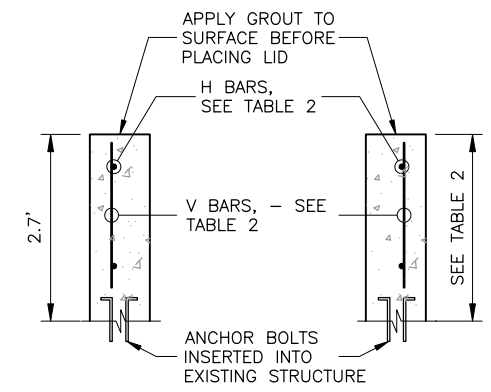
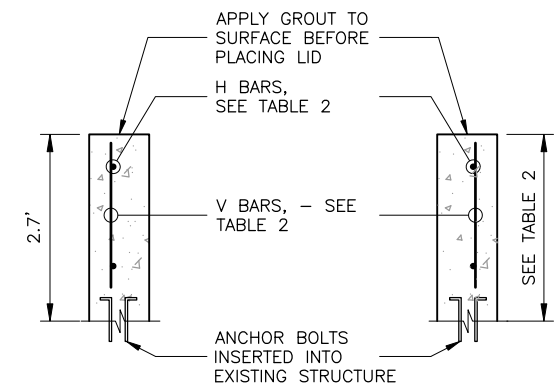


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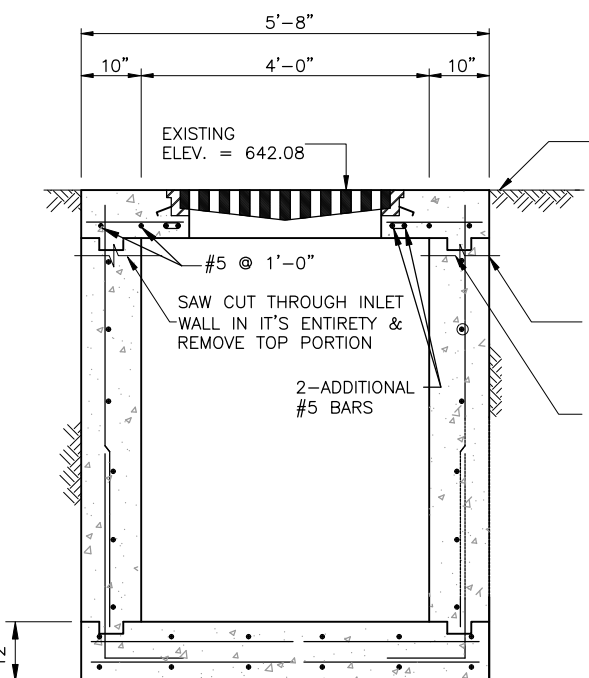
NOTE:
 UPON COMPLETION OF INLET EXTENSION, CONTRACTOR SHALL REINSTALL THE LID BY GROUTING.

NOTE:
 UPON COMPLETION OF INLET EXTENSION, CONTRACTOR SHALL REINSTALL THE LID BY GROUTING.



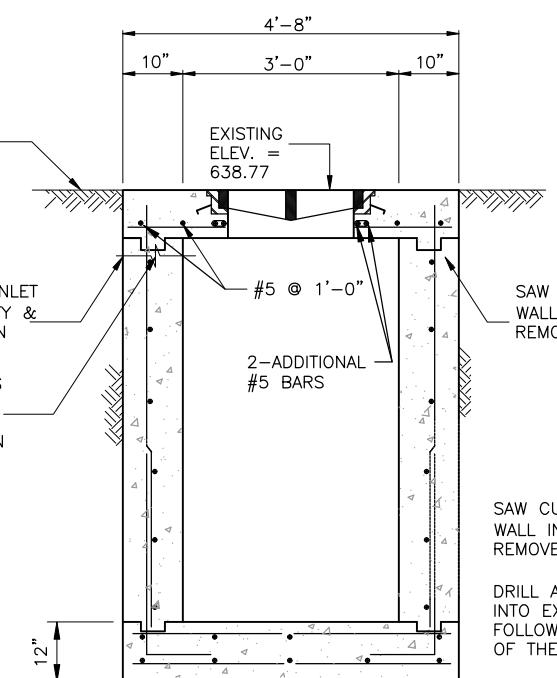
PROP. ELEV.

PROP. ELEV.



SECTION
 N.T.S.

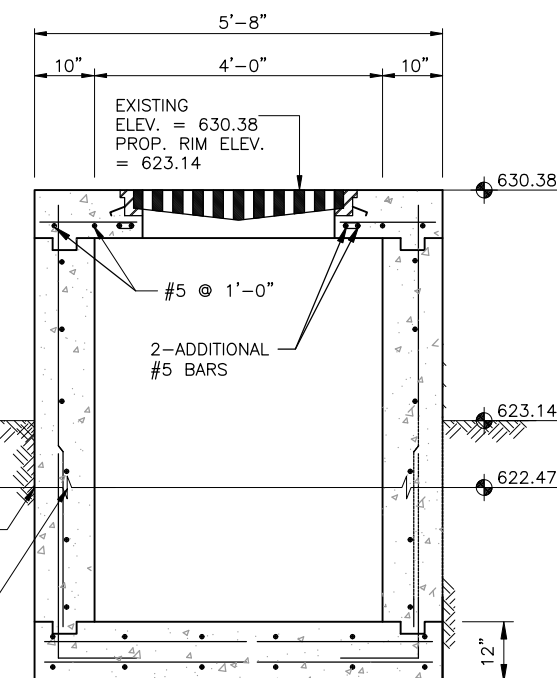
INLET A



SECTION
 N.T.S.

INLET B

NOTE:
 DIMENSIONS OF INLETS MAY VARY SLIGHTLY WITH RESPECT TO SURVEYED DATA. CONTRACTOR SHALL VERIFY DIMENSIONS OF ALL INLETS PRIOR TO COMMENCING THE WORK ASSOCIATED WITH EACH INLET.



SECTION
 N.T.S.

INLET C

NOTE:
 ONCE CONTRACTOR HAS REMOVED TOP PORTION OF THE INLET, HE/SHE SHALL RECONSTRUCT 8" LID USING THE FRAME AND GRATE REMOVED PROVIDED THAT IT IS IN GOOD CONDITION AS DEFINED BY THE RESIDENT ENGINEER.

ANCHOR BOLTS SHALL BE DRILLED INTO EXISTING WALLS TO ANCHOR THE LID ONCE PLACED.

TABLE 1

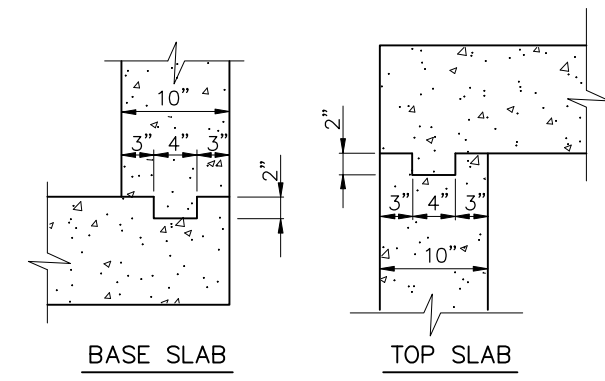
STRUCTURE	I-1	I-2	I-3
RIM ELEVATION	729.40	732.40	727.75
RCP "A" DIA. & INV.	(18") 726.70	(12") 729.77	(18") 724.80
RCP "B" DIA. & INV.			(6") 726.73
INLET INVERT	726.40	729.40	724.50
INLET HEIGHT (H)	3'-0"	3'-0"	3'-3"

TABLE 2

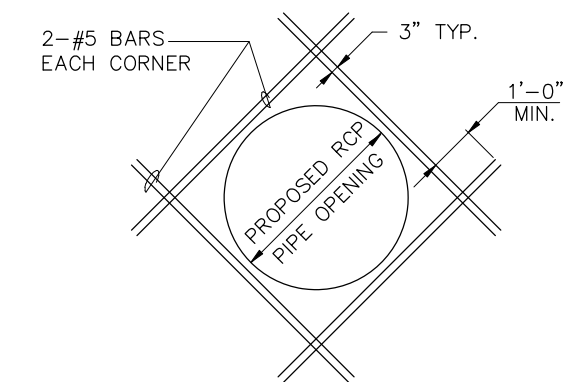
INLET HEIGHT (H)	H BARS	V AND V ₁ BARS
H ≤ 5'	#5 @ 1'-0"	#5 @ 9"
5' < H ≤ 10'	#5 @ 1'-0"	#5 @ 9"
10' < H ≤ 15' (MAX)	#6 @ 1'-0"	#6 @ 1'-0"

GENERAL NOTES

- SEE SPECIFICATIONS FOR MATERIAL REQUIREMENTS, CONSTRUCTION METHODS, BASIS OF PAYMENT, AND OTHER REQUIREMENTS.
- PIPE PENETRATION OPENINGS FOR PRECAST STRUCTURES SHALL BE CIRCULAR. DIAMETER OF OPENING SHALL BE OUTSIDE PIPE DIAMETER PLUS 6 INCHES. GROUT OPENING SOLID WITH NON-SHRINK GROUT.



KEYWAY DETAILS
 N.T.S.

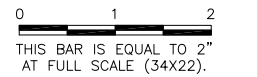


ADDITIONAL REINFORCEMENT AROUND PIPE PENETRATIONS
 N.T.S.

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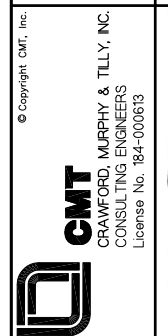
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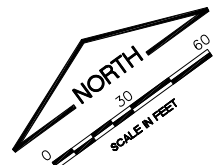


GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 DRAINAGE DETAILS 2



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CHECKED BY:	TJH
APPROVED BY:	CET
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JOB No:	12061-03
IL. PROJ. NO. PIA-4180	
AIP PROJ. NO. 3-17-0080-XX	
SHEET 17 OF 35 SHEETS	



TERMINAL BUILDING

AIR CARRIER APRON

TAXILANE

TAXILANE P

TAXIWAY T

N1456869.18
E2427837.88

N1456822.74
E2427895.17

N1456792.43
E2427958.41

N1456793.83
E2428031.23

N1456727.66
E2428050.63

N1456750.04
E2428119.95

N1456680.35
E2428135.90

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N1456526.40
E2428436.91

N1456562.13
E2428557.31

N1456639.12
E2428560.41

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0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

EXPAND TERMINAL APRON

TERMINAL GATE MARKING REMOVAL PLAN

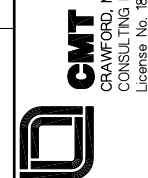
NOTES:

1. THE EXISTING MARKINGS TO BE REMOVED HAVE BEEN PAINTED OVER WITH GRAY MARKINGS BY THE AIRPORT TO TEMPORARILY HIDE THE MARKINGS. THE MARKING REMOVAL SHALL INCLUDE THE COMPLETE REMOVAL OF MARKINGS INCLUDING ALL PAINT BENEATH THE GRAY MARKINGS.

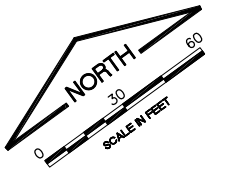
LEGEND

----- EXISTING PAVEMENT
- - - - - MARKING TO BE REMOVED

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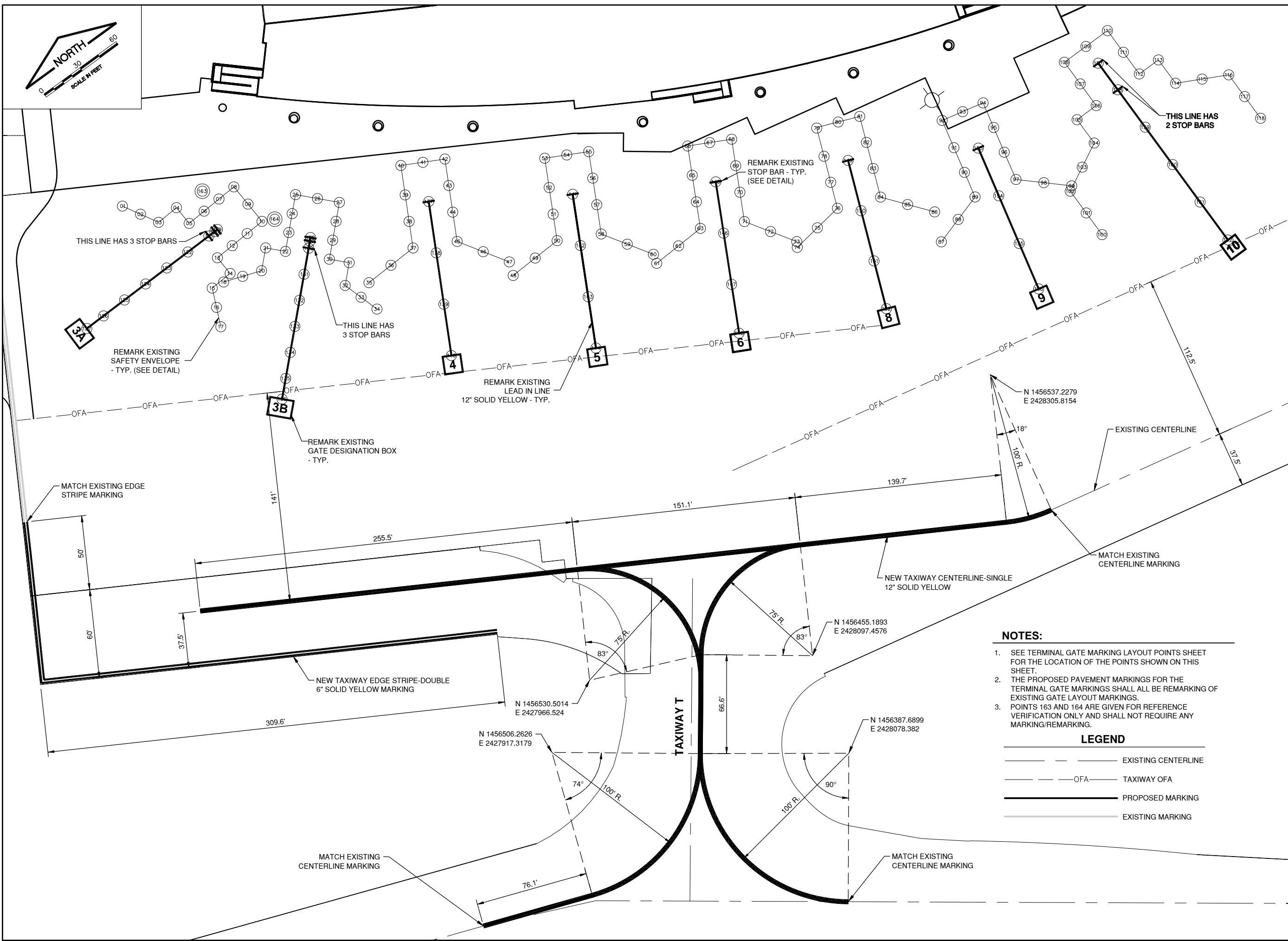
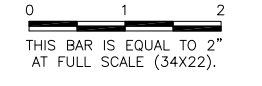
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DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	APRIL 26, 2013
JOB No:	12061-03
IL. PROJ. NO. PIA-4180	
AIP PROJ. NO. 3-17-0080-XX	
SHEET	18 OF 35 SHEETS



PE094

REVISIONS

NUMBER	BY	DATE



- NOTES:**
- SEE TERMINAL GATE MARKING LAYOUT POINTS SHEET FOR THE LOCATION OF THE POINTS SHOWN ON THIS SHEET.
 - THE PROPOSED PAVEMENT MARKINGS FOR THE TERMINAL GATE MARKINGS SHALL ALL BE REMARKING OF EXISTING GATE LAYOUT MARKINGS.
 - POINTS 163 AND 164 ARE GIVEN FOR REFERENCE VERIFICATION ONLY AND SHALL NOT REQUIRE ANY MARKING/REMARKING.

LEGEND

	EXISTING CENTERLINE
	TAXIWAY OFA
	PROPOSED MARKING
	EXISTING MARKING

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**EXPAND TERMINAL APRON
 MARKING PLAN**

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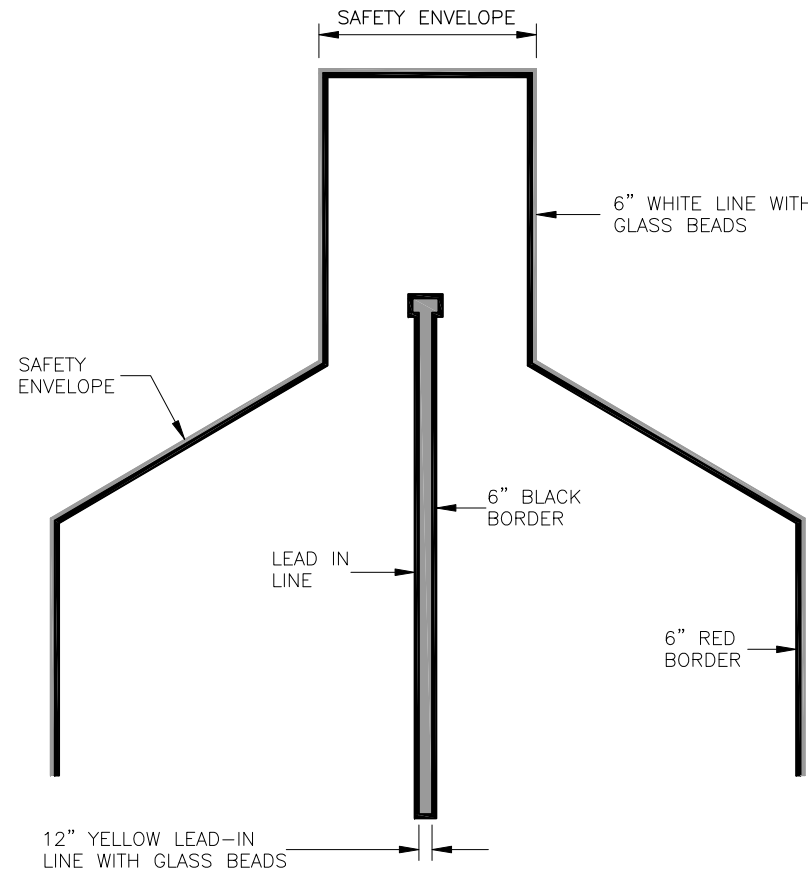
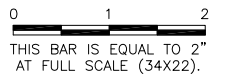
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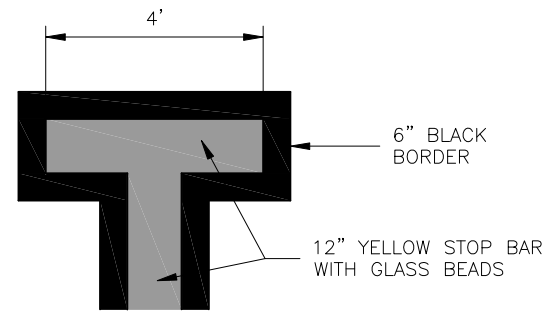
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DATE:	APRIL 26, 2013
JOB No:	12061-03
IL. PROJ. NO. PIA-4180 AIP PROJ. NO. 3-17-0080-XX	
SHEET 19 OF 35 SHEETS	

PE094

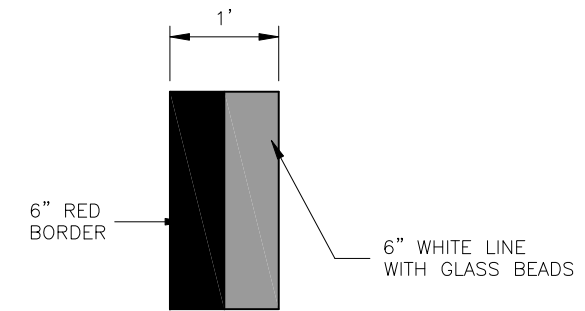
REVISIONS		
NUMBER	BY	DATE



TYPICAL LEAD-IN AND SAFETY ENVELOPE FOR CONCRETE SURFACE
 N.T.S.



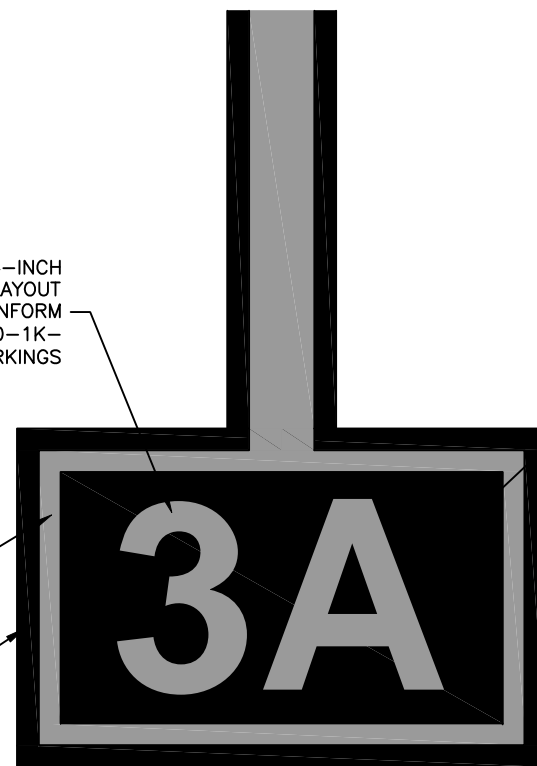
TYPICAL STOP BAR FOR CONCRETE SURFACE
 N.T.S.



SAFETY ENVELOPE ZOOMED VIEW
 N.T.S.

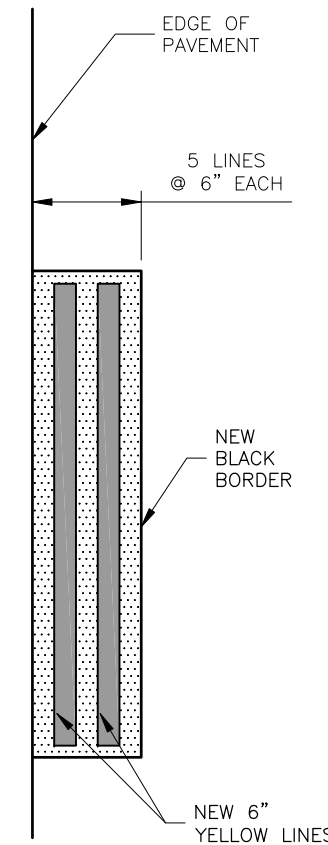
4-FOOT TALL YELLOW LETTERS 4-INCH WIDE ON BLACK BACKGROUND. LAYOUT OF NUMBERS AND LETTERS SHALL CONFORM TO APPENDIX A OF FAA AC150/5340-1K- STANDARDS FOR AIRPORT MARKINGS

6-INCH WIDE YELLOW BORDER
 6-INCH WIDE BLACK OUTLINE

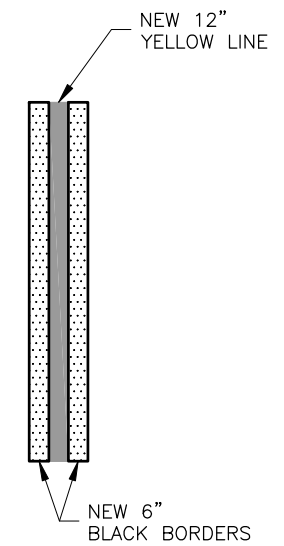


GATE DESIGNATION BOX DETAIL
 N.T.S.

NOTE:
 1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE MEDIA & 6" BLACK BORDER.
 2. BLACK BORDER SHALL NOT REQUIRE REFLECTIVE MEDIA.



TAXIWAY EDGE MARKING CONTINUOUS (YELLOW)
 N.T.S.





TAXIWAY CENTERLINE CONTINUOUS (YELLOW)
 N.T.S.

NOTE:
 1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE MEDIA & 6" BLACK BORDER WITH THE EXCEPTION OF THE SAFETY ENVELOPES.
 2. BLACK AND RED BORDER SHALL NOT REQUIRE REFLECTIVE MEDIA.
 3. ALL PCC SURFACES TO BE PAINTED SHALL HAVE THE CURE COMPOUND REMOVED PRIOR TO PAINTING

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 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 MARKING DETAILS

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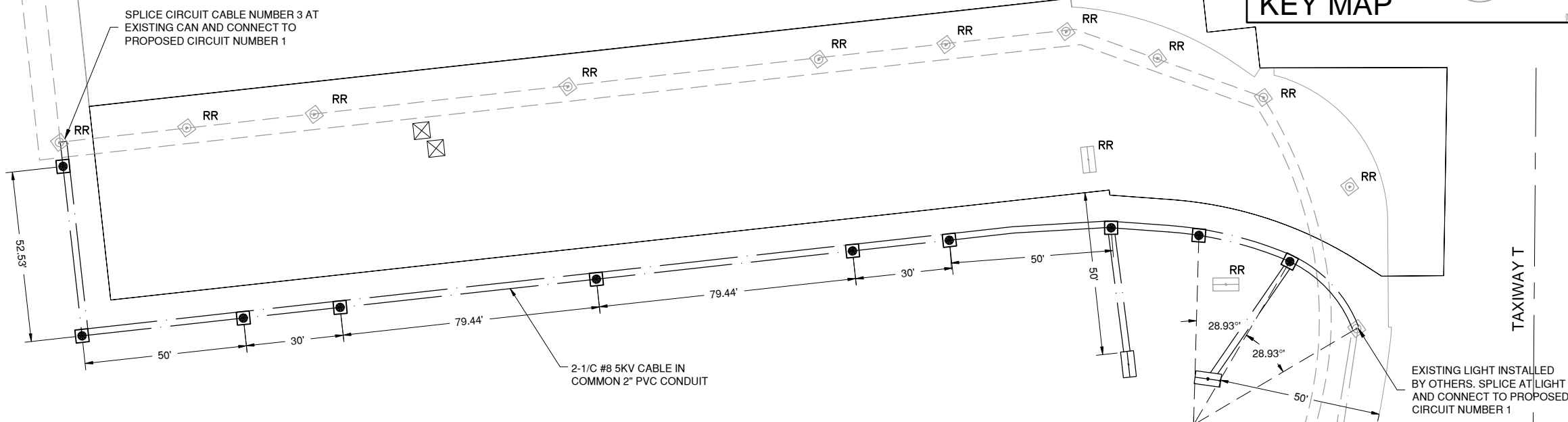
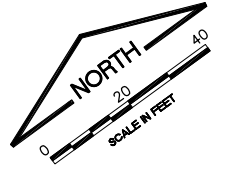
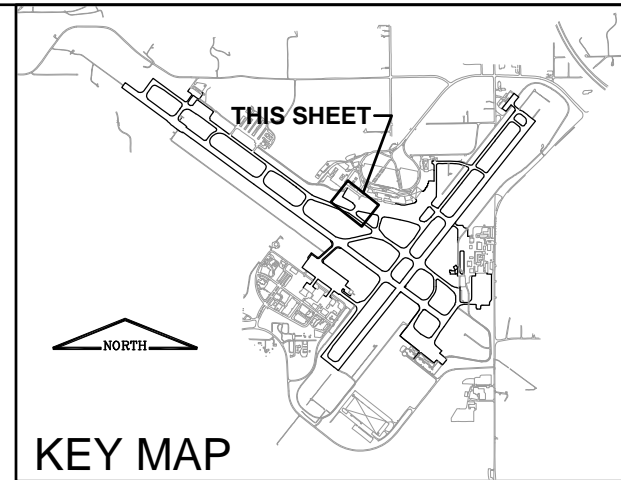
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SHEET	21 OF 35 SHEETS

PE094

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**EXPAND TERMINAL APRON
 LIGHTING PLAN**

LEGEND

- EXISTING TAXIWAY LIGHTING CIRCUIT NUMBER 1
- - - PROPOSED TAXIWAY LIGHTING CIRCUIT NUMBER 1
- - - EXISTING TAXIWAY LIGHTING CIRCUIT NUMBER 3
- RR (with arrow) EXISTING TAXIWAY GUIDANCE SIGN TO BE REMOVED AND RELOCATED
- RR (with arrow) PROPOSED RELOCATED TAXIWAY GUIDANCE SIGN PLACEMENT
- RR (in circle) EXISTING BASE MOUNTED TAXIWAY LIGHT TO BE REMOVED AND RELOCATED
- RR (in circle) EXISTING BASE MOUNTED TAXIWAY LIGHT
- ⊠ EXISTING 4.0'X4.0' ELECTRICAL HANDHOLE TO BE ADJUSTED
- PROPOSED RELOCATED BASE MOUNTED TAXIWAY LIGHT PLACEMENT

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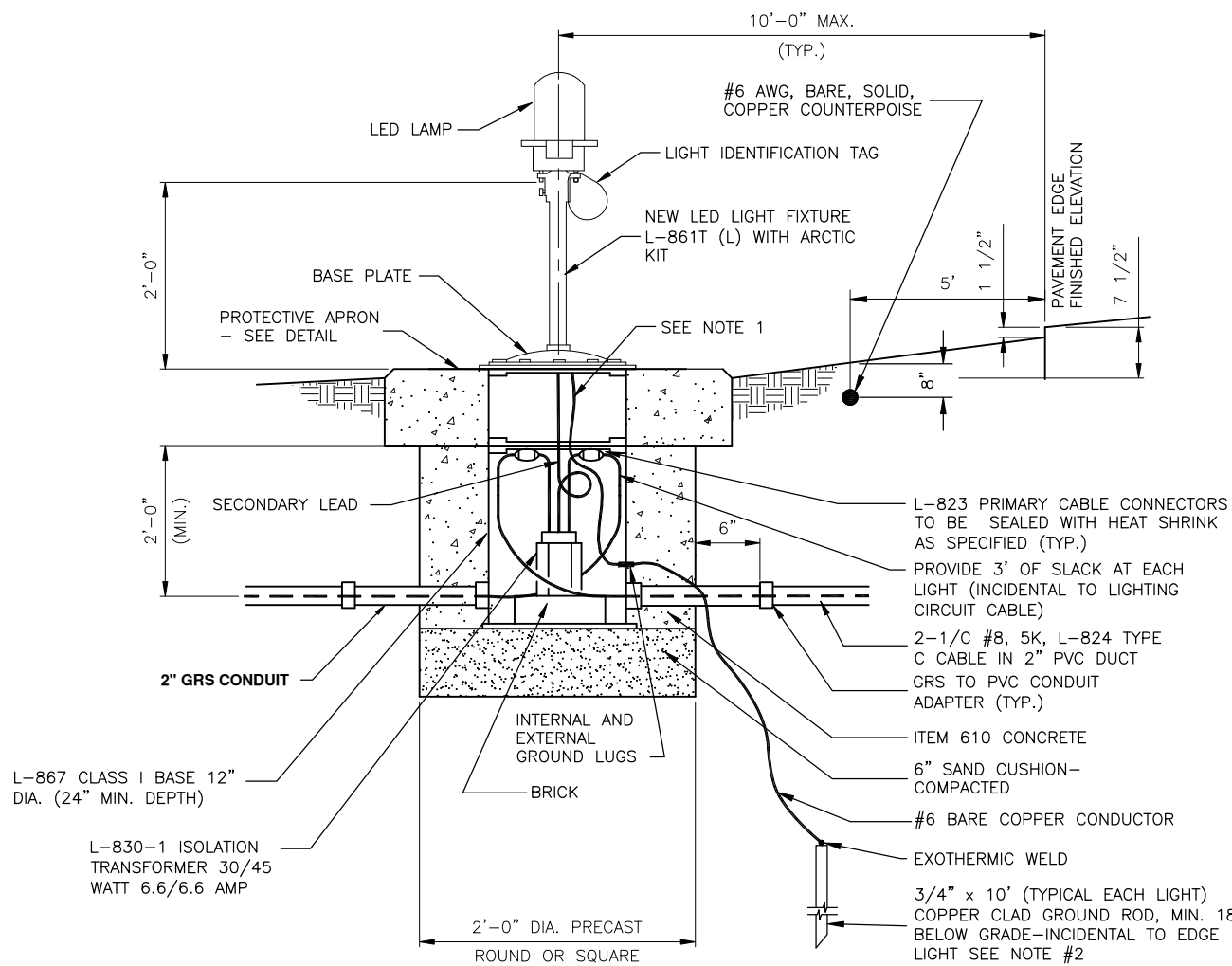
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AIP PROJ. NO.	3-17-0080-XX
SHEET	22 OF 35 SHEETS

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NUMBER	BY	DATE

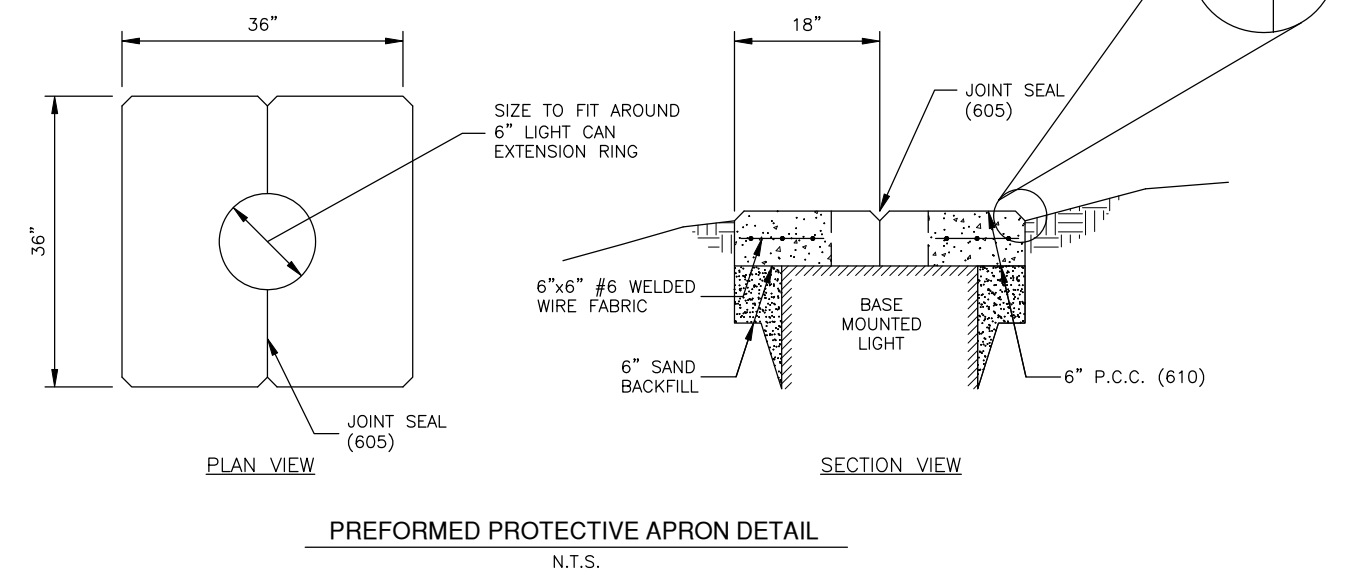
0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).



BASE MOUNTED EDGE LIGHTS
 N.T.S.

L-867 CLASS I BASE 12" DIA. (24" MIN. DEPTH)
 L-830-1 ISOLATION TRANSFORMER 30/45 WATT 6.6/6.6 AMP

NOTE: THIS SHEET IS SHOWN FOR INFORMATIONAL PURPOSES ONLY. ALL LIGHTS TO BE INSTALLED SHALL BE EXISTING LIGHTS RELOCATED.



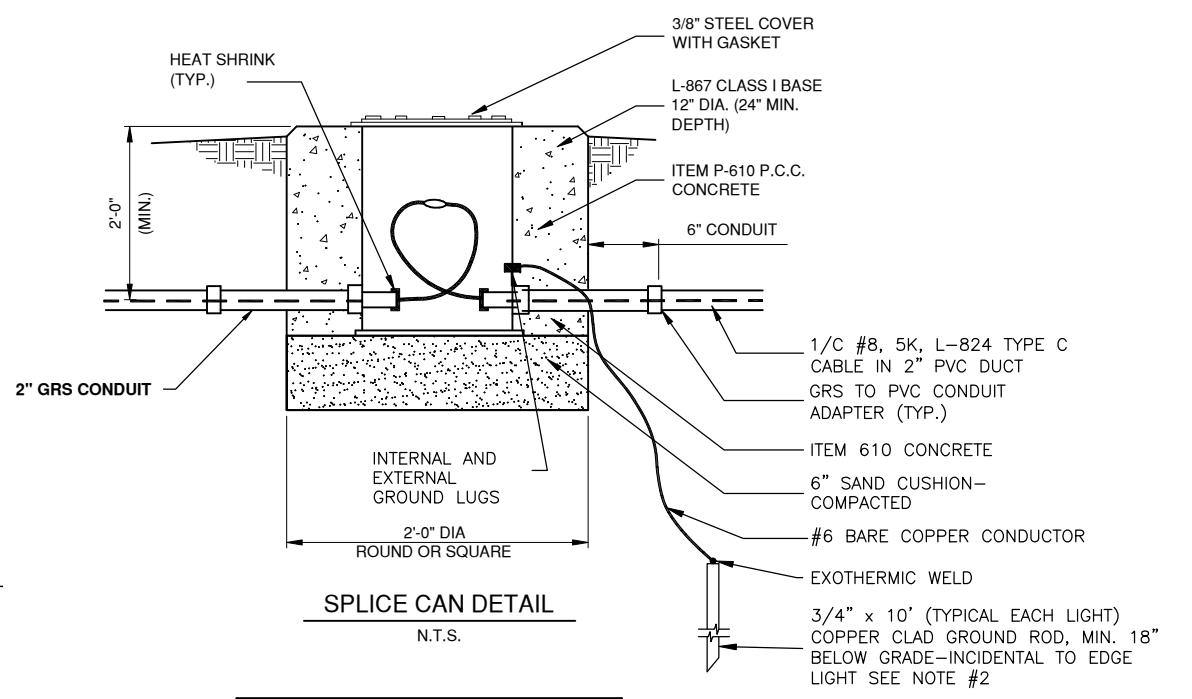
PREFORMED PROTECTIVE APRON DETAIL
 N.T.S.

LIGHT NOTES

1. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.

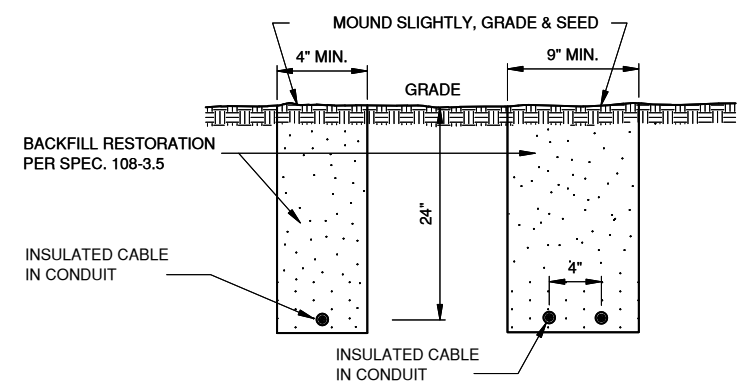
NOTES

1. DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
2. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
3. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.



SPLICE CAN DETAIL
 N.T.S.

UNIT DUCT SHALL BE TERMINATED AT THE L-823 CONNECTORS AND SEALED WITH HEAT SHRINK AS SPECIFIED.



TRENCH DETAIL
 N.T.S.

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 PEORIA, ILLINOIS

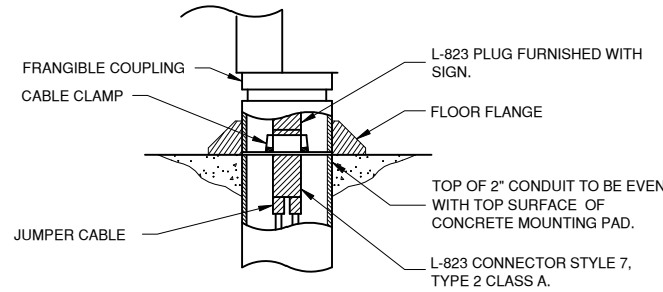
EXPAND TERMINAL APRON
 ELECTRICAL DETAILS 1

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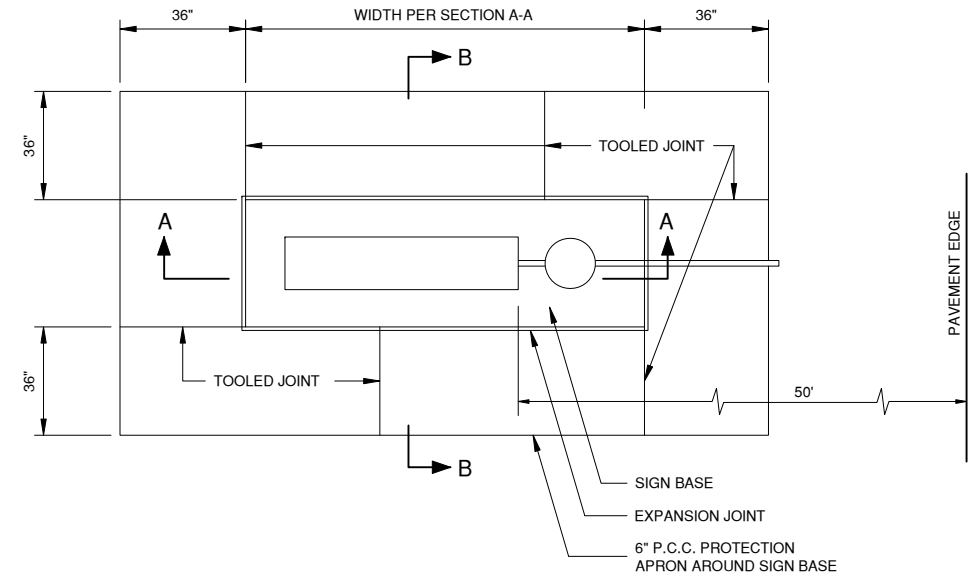
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 PEORIA INTERNATIONAL AIRPORT

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JOB No:	12061-03
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AIP PROJ. NO. 3-17-0080-XX	
SHEET 23 OF 35 SHEETS	

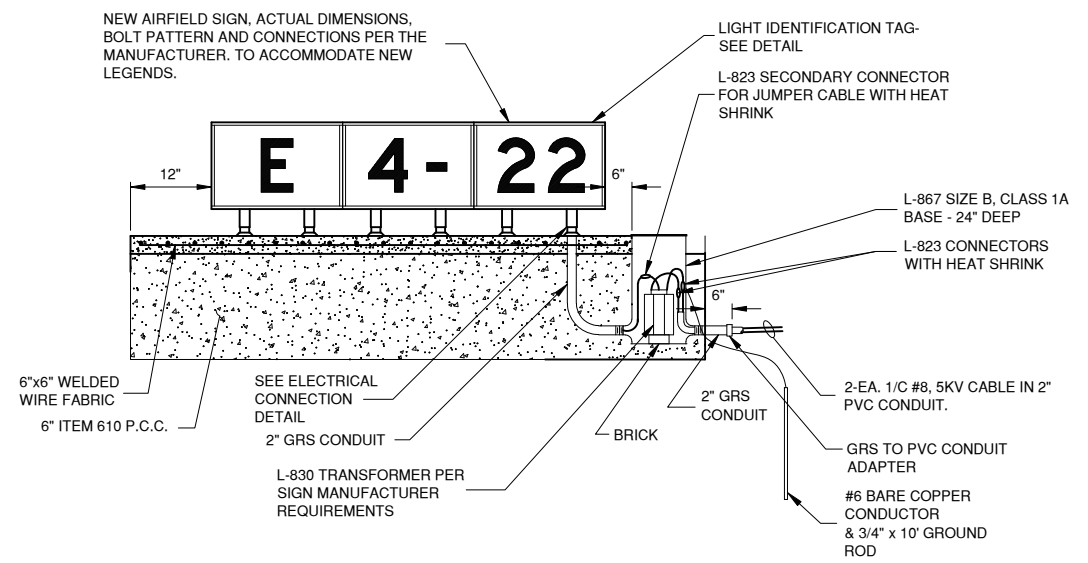
NOTE: THIS SHEET IS SHOWN FOR INFORMATIONAL PURPOSES ONLY. ALL SIGNS TO BE INSTALLED SHALL BE EXISTING SIGNS RELOCATED.



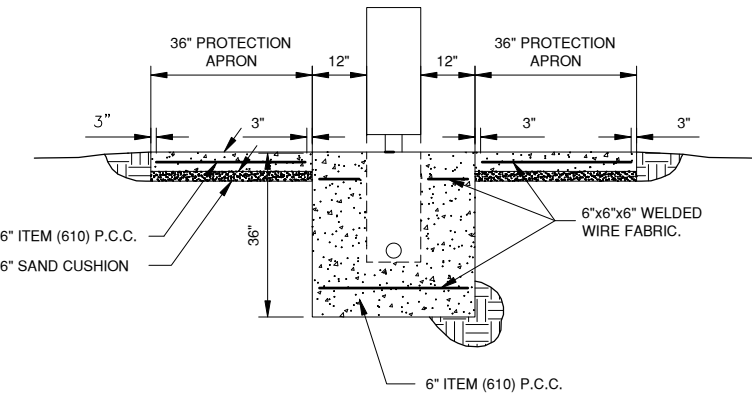
ELECTRICAL CONNECTION DETAIL
N.T.S.



PLAN

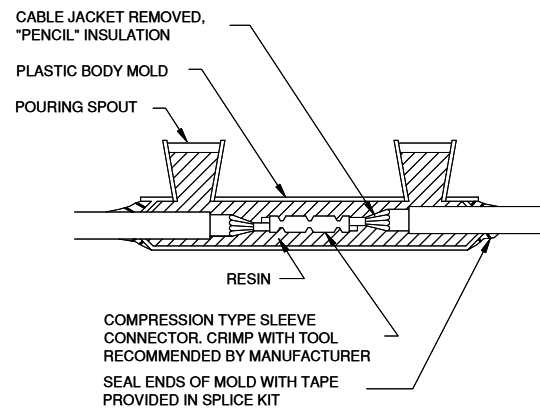


SECTION A-A



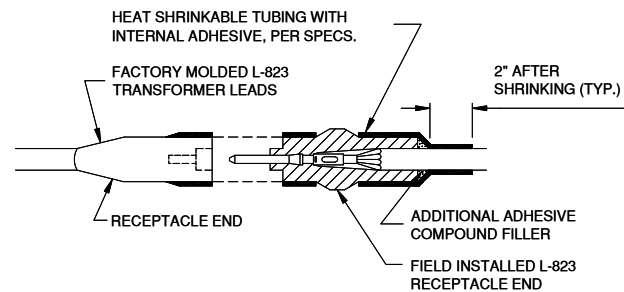
SECTION B-B

L-858 AIRFIELD SIGN DETAILS
N.T.S.



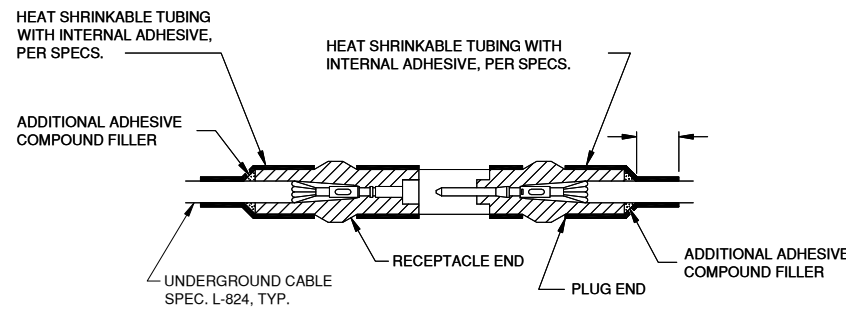
FOR IN-LINE CONNECTIONS OF EXISTING CABLES CUT DURING CONSTRUCTIONS.

TYPE A



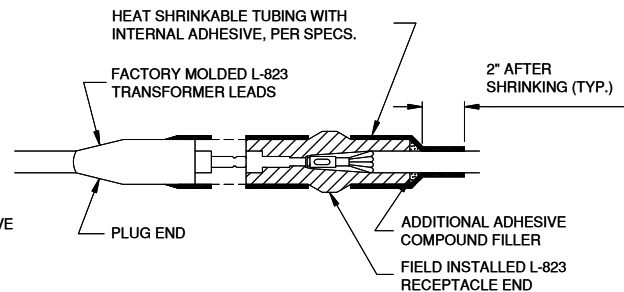
FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

TYPE C



NOT TO BE USED IN THIS PROJECT UNLESS OTHERWISE DIRECTED BY ENGINEER

TYPE B



FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

TYPE D

CABLE SPLICES

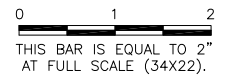
N.T.S.
NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

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NUMBER	BY	DATE



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 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 ELECTRICAL DETAILS 2

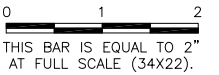


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AIP PROJ. NO. 3-17-0080-XX	
SHEET 24 OF 35 SHEETS	

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REVISIONS

NUMBER	BY	DATE



NOTES

- DIMENSIONS FROM EXISTING HANDHOLE SHOP DRAWINGS ARE SHOWN AND VARY SLIGHTLY FROM SURVEY DATA. CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS PRIOR TO SHOP DRAWING PREPARATION AND FABRICATION.
- CONCRETE SHALL BE SAWCUT FOR CONCRETE REMOVAL. SAWCUT SHALL BE 1½" DEEP OR TO FACE OF REINFORCING STEEL, WHICHEVER IS LESS. EXISTING REINFORCING STEEL SHALL NOT BE DAMAGED AND SHALL REMAIN FOR INCORPORATION INTO NEW WORK.
- EXISTING FRAMES AND COVERS ARE SPRING-ASSIST AIRCRAFT RATED. EXISTING FRAMES AND COVERS SHALL BE REMOVED IN THEIR ENTIRETY. IF EXISTING FRAMES AND COVERS ARE NOT DAMAGED, THEY MAY BE RE-USED IN THE NEW WORK. IF NEW FRAMES AND COVERS ARE USED, THEY SHALL BE NEENAH R-3498-R2S, OR EQUAL.
- HANDHOLE TOP EXTENSION THICKNESS VARIES WITH SLOPE OF TAXIWAY. SEE SHEET 11 FOR STAKING PLAN. CONTRACTOR SHALL SURVEY TOP OF EXISTING HANDHOLES TO ESTABLISH THICKNESSES OF HANDHOLE EXTENSIONS.

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 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 ELECTRICAL DETAILS 3

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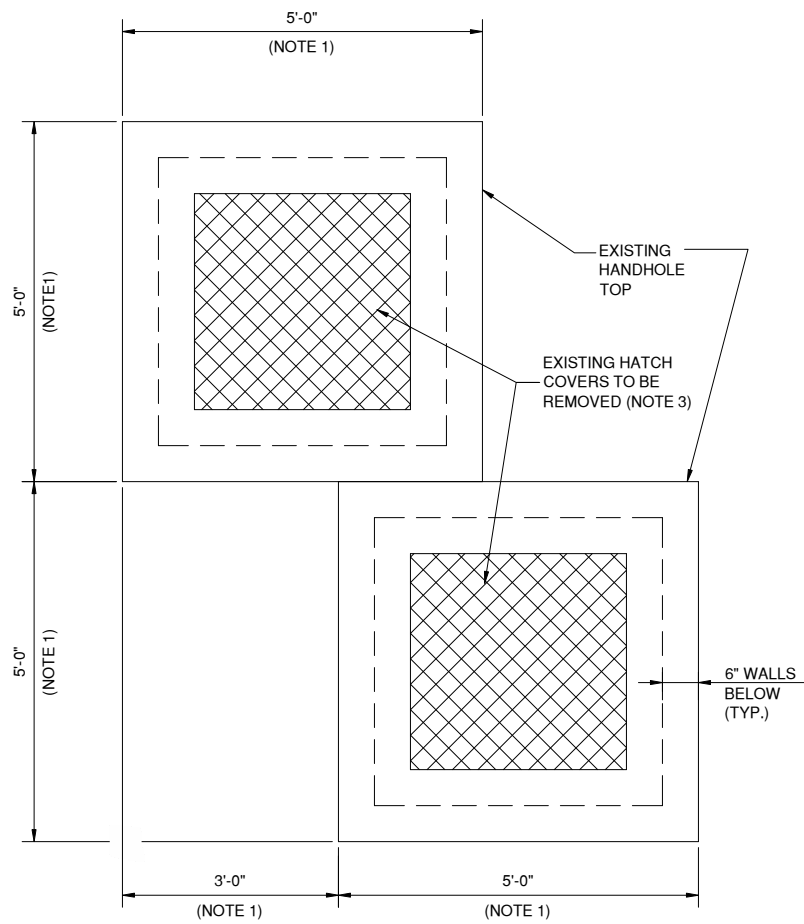
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DATE: APRIL 26, 2013

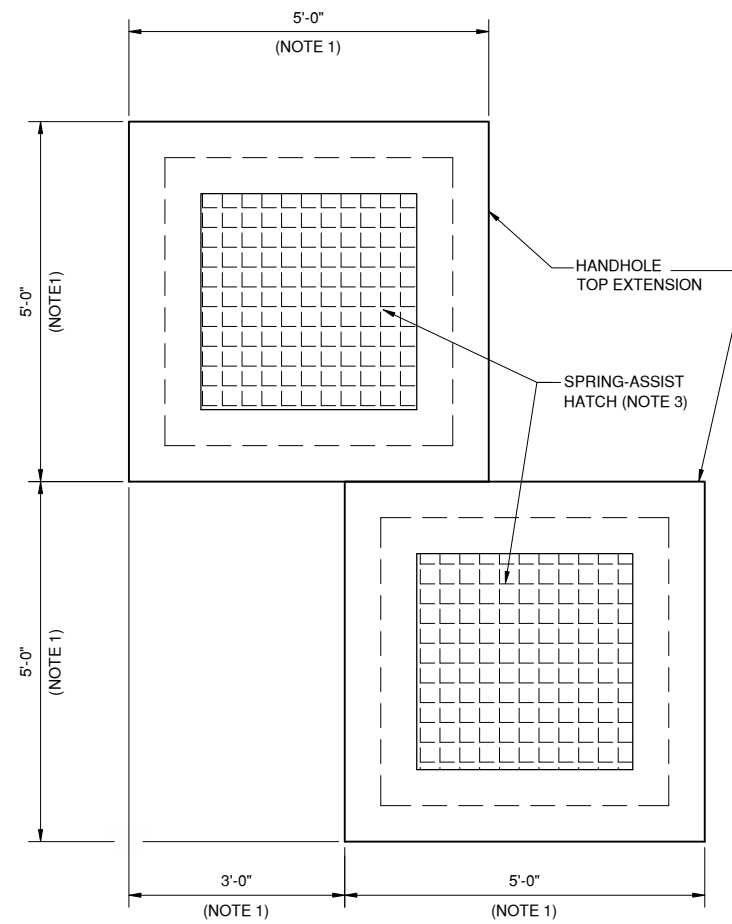
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SHEET 25 OF 35 SHEETS

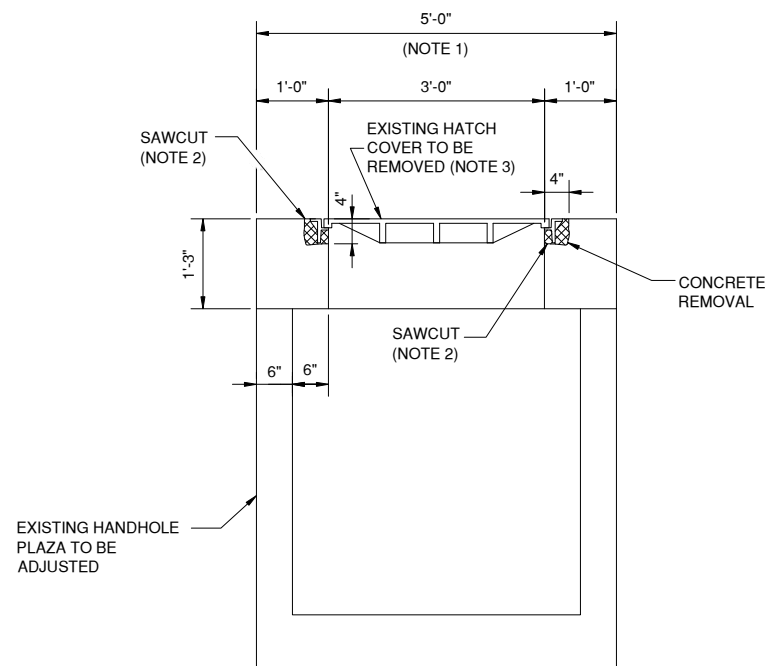


PLAN



PLAN

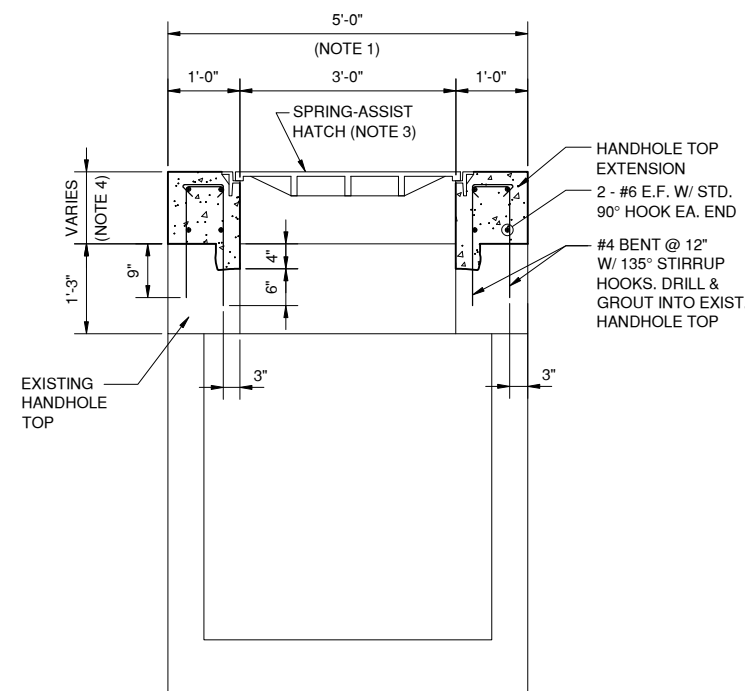
N.T.S.



SECTION

REMOVAL

3/4" = 1'-0"



SECTION

MODIFICATIONS

3/4" = 1'-0"



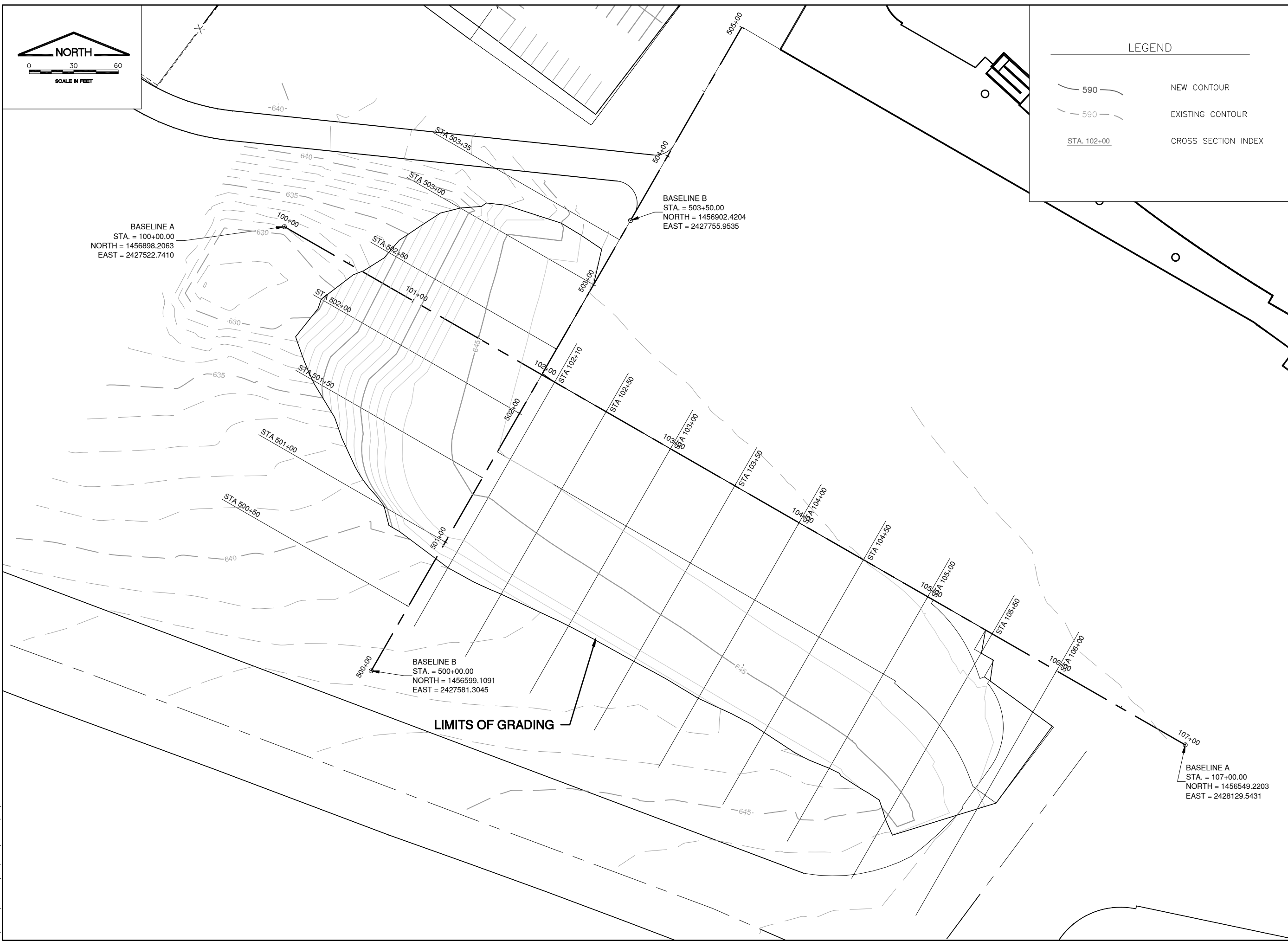
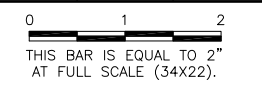
LEGEND

- 590 NEW CONTOUR
- 590 EXISTING CONTOUR
- STA. 102+00 CROSS SECTION INDEX

FILE: TERMINAL APRON GRADING PLAN
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 11:21 AM
 PIA-Base-Existing
 1006104-C-7200
 1006104-C-GP00

PE094

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BASELINE A
 STA. = 100+00.00
 NORTH = 1456898.2063
 EAST = 2427522.7410

BASELINE B
 STA. = 503+50.00
 NORTH = 1456902.4204
 EAST = 2427755.9535

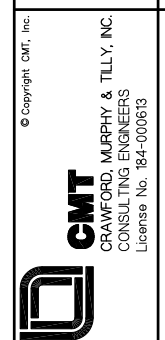
BASELINE B
 STA. = 500+00.00
 NORTH = 1456599.1091
 EAST = 2427581.3045

BASELINE A
 STA. = 107+00.00
 NORTH = 1456549.2203
 EAST = 2428129.5431

LIMITS OF GRADING

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 TERMINAL APRON GRADING PLAN 1



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SHEET 26 OF 35 SHEETS	

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LEGEND

- NEW CONTOUR
- EXISTING CONTOUR
- NEW ELEVATION
- EXISTING ELEVATION
- CROSS SECTION INDEX
- STAKING GRID

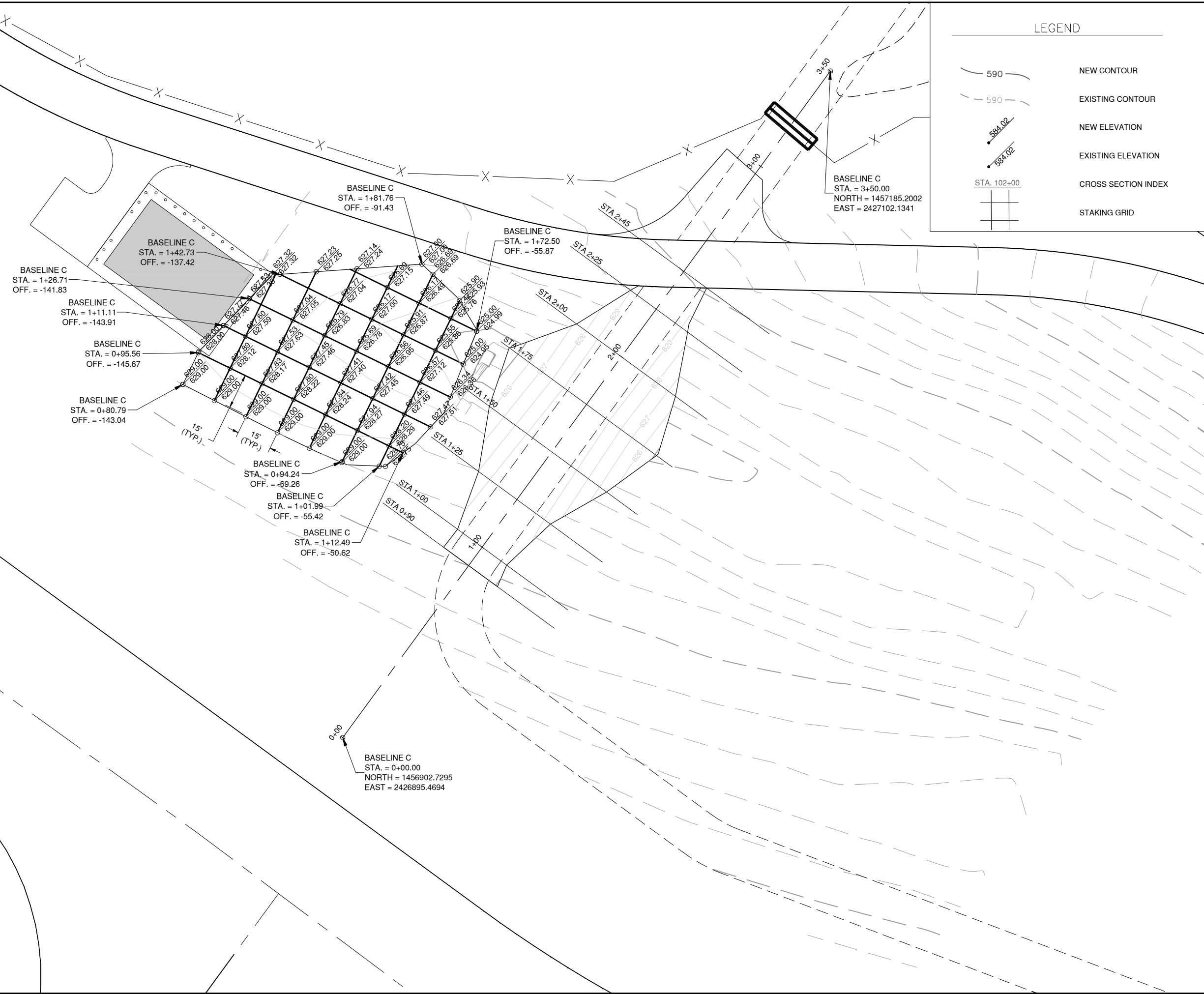
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 PIA-Base-Existing
 1006104-C-GP002

PE094

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



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 PEORIA, ILLINOIS

EXPAND TERMINAL APRON

TERMINAL APRON GRADING PLAN 2

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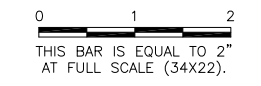
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SHEET	27 OF 35 SHEETS

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PE094


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**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**EXPAND TERMINAL APRON
 TURFING AND EROSION CONTROL PLAN**

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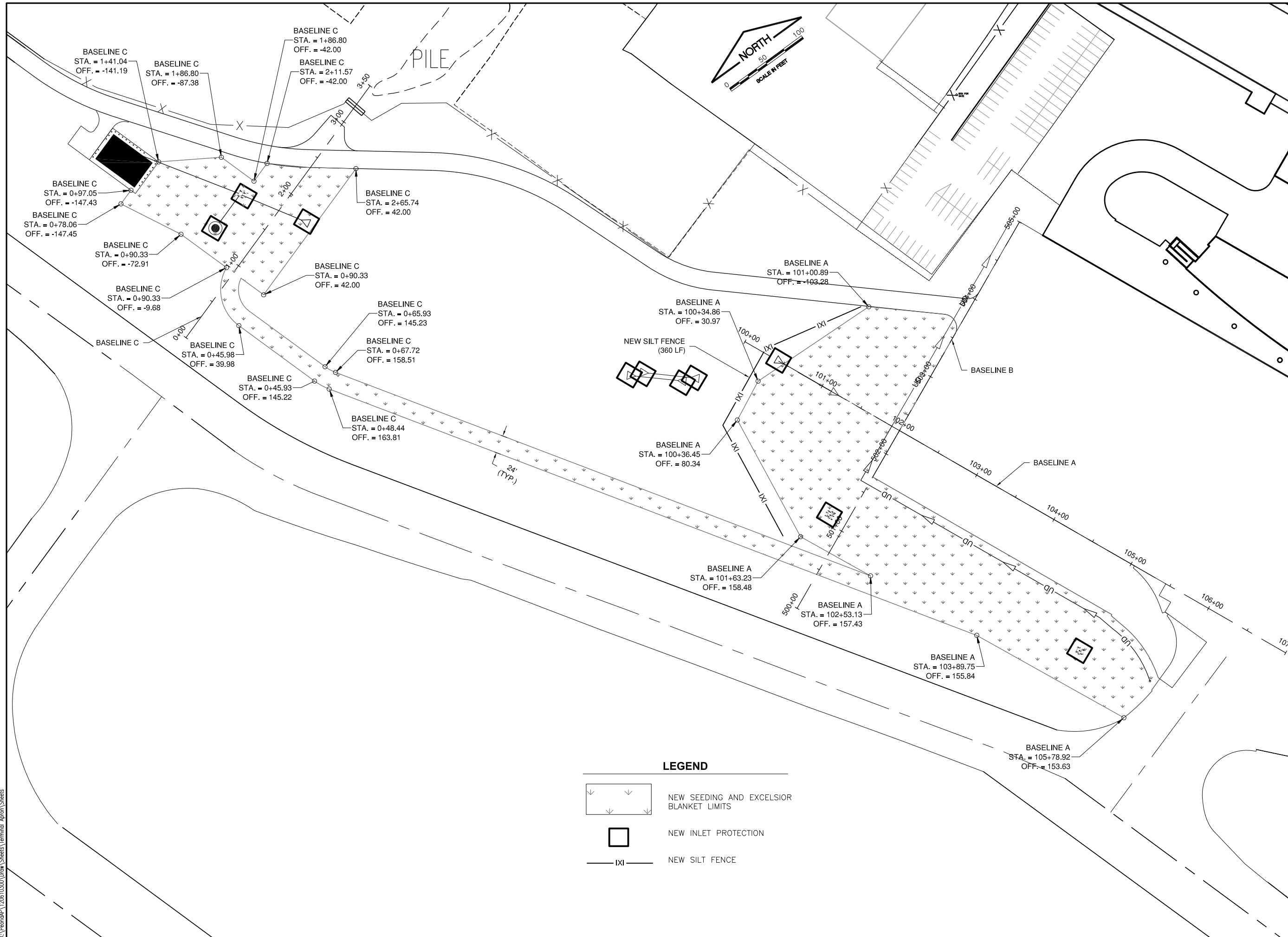


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AIP PROJ. NO.	3-17-0080-XX
SHEET	28 OF 35 SHEETS



LEGEND

-  NEW SEEDING AND EXCELSIOR BLANKET LIMITS
-  NEW INLET PROTECTION
-  NEW SILT FENCE

K:\Peoria\PIA\12061\0300\Draw\Sheets\Terminal_Apron\Sheets

PE094

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

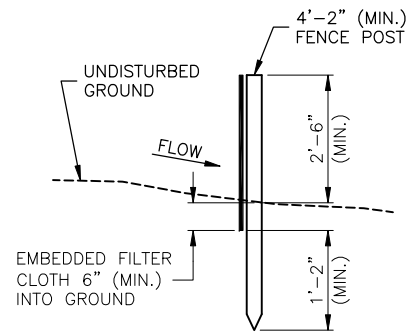
GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

EXPAND TERMINAL APRON
 EROSION CONTROL DETAILS

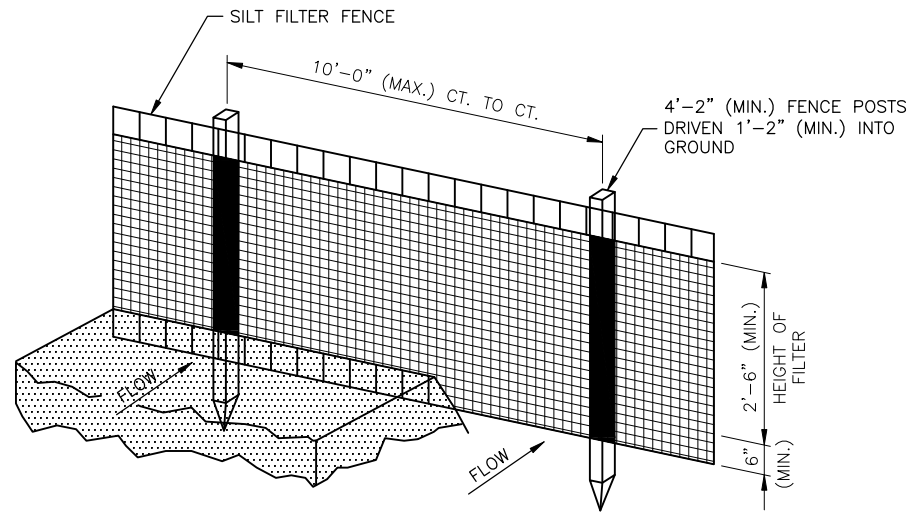
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SHEET	29 OF 35 SHEETS



SECTION



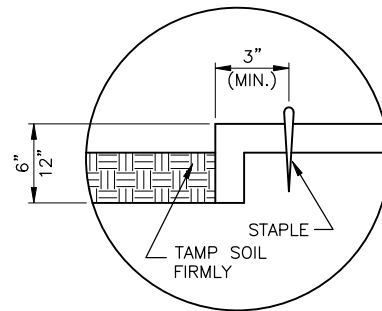
PERSPECTIVE VIEW

EROSION CONTROL FABRIC FENCE DETAILS

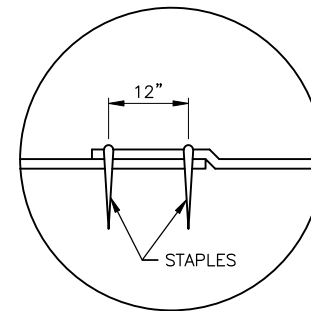
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EROSION CONTROL FABRIC FENCE NOTES

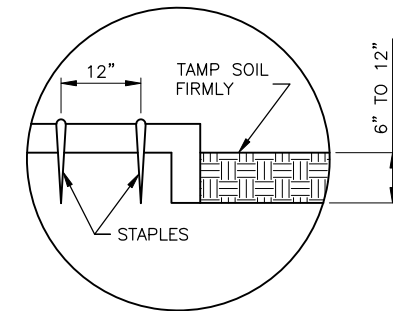
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



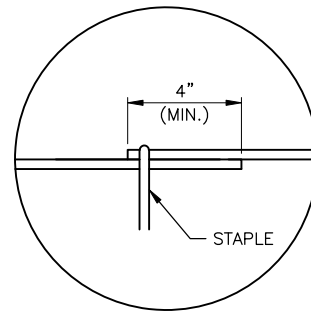
DETAIL 1 - TERMINAL FOLD



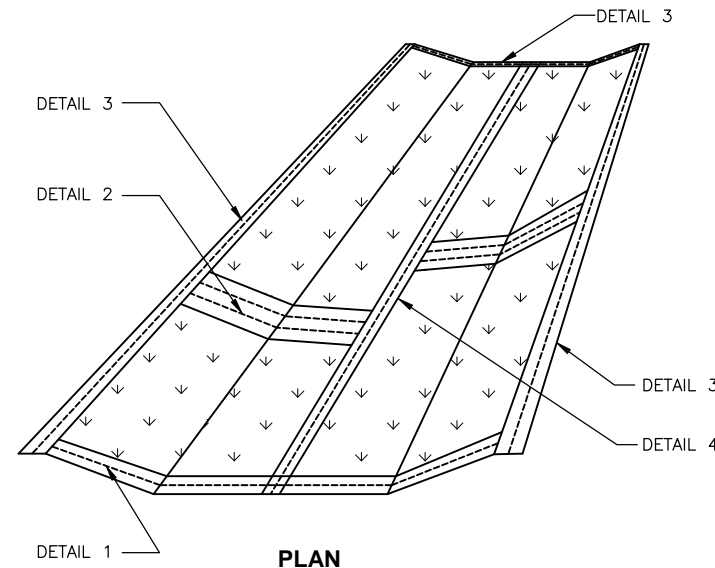
DETAIL 2 - JUNCTION SLOT



DETAIL 3 - ANCHOR SLOT



DETAIL 4 - LAP JOINT



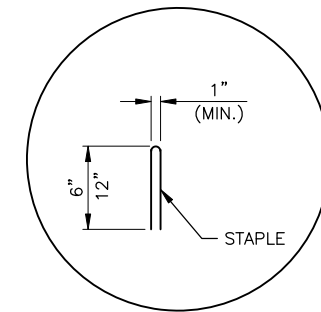
PLAN

EXCELSIOR BLANKET DETAILS

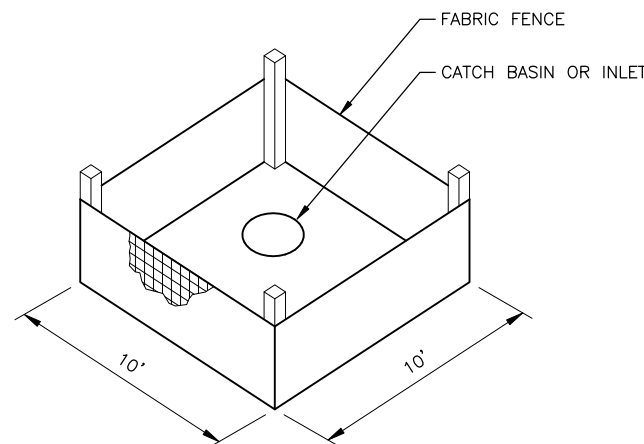
N.T.S.

EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINALS ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.

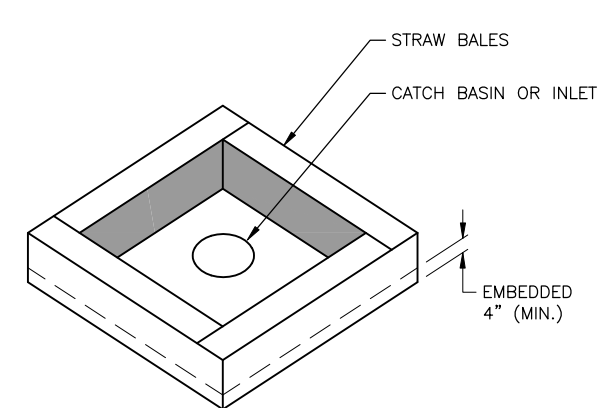


DETAIL 5 - STAPLE DETAIL



INLET PROTECTION WITH FABRIC

N.T.S.

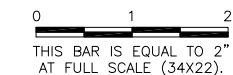


INLET PROTECTION WITH STRAW BALES

N.T.S.

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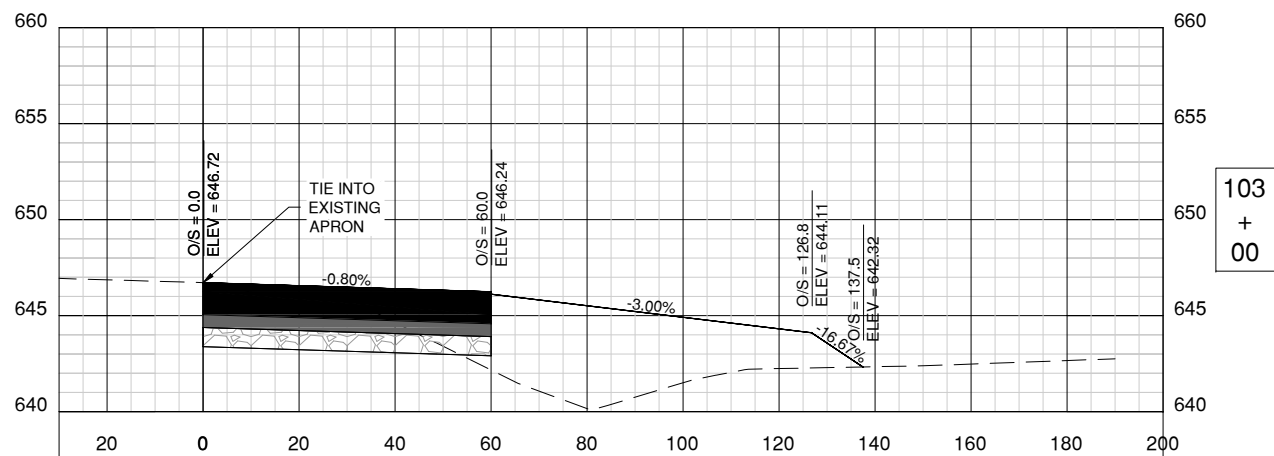
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 PEORIA, ILLINOIS**

**EXPAND TERMINAL APRON
 TERMINAL APRON EXPANSION SECTIONS 1**

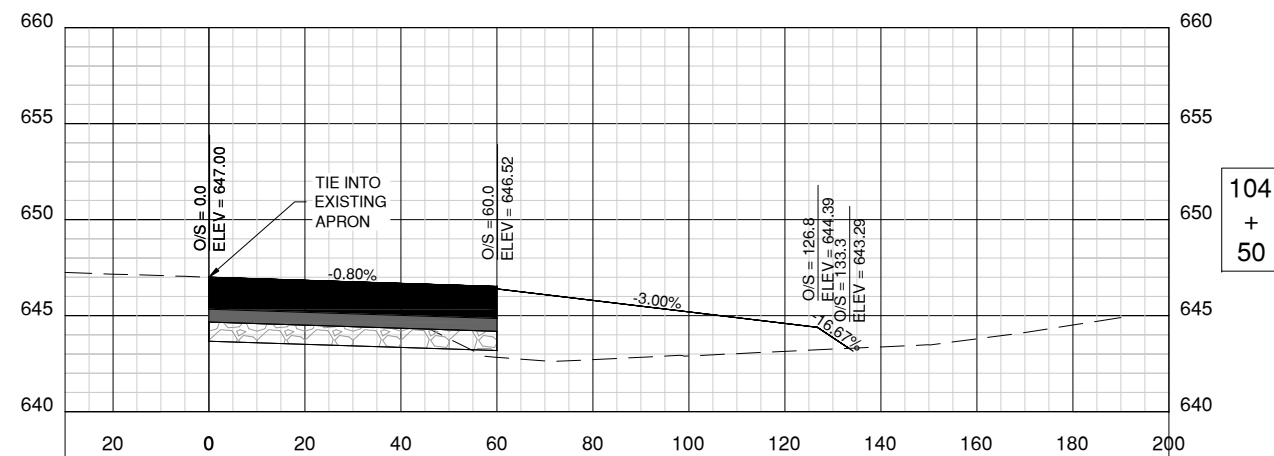


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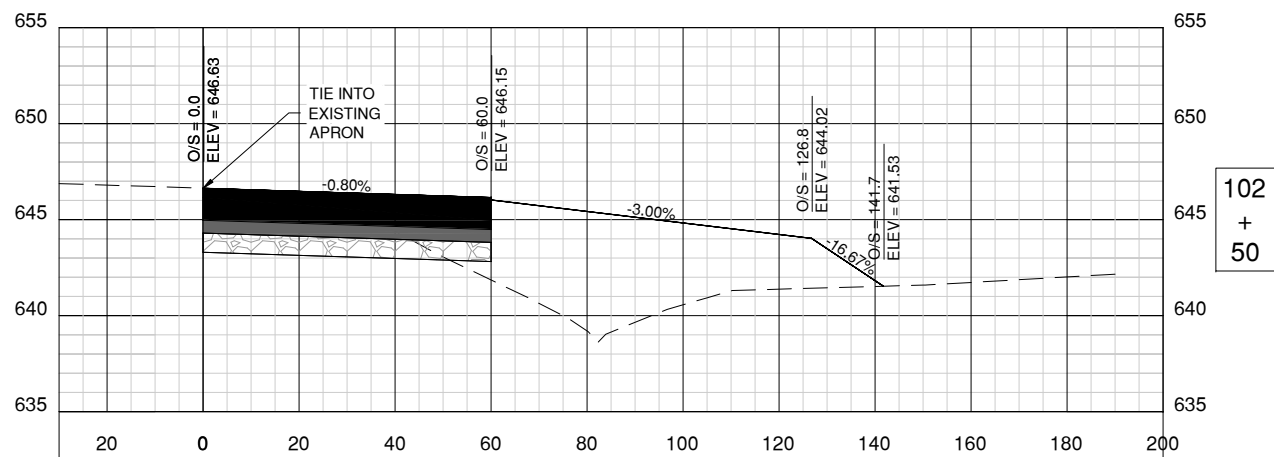
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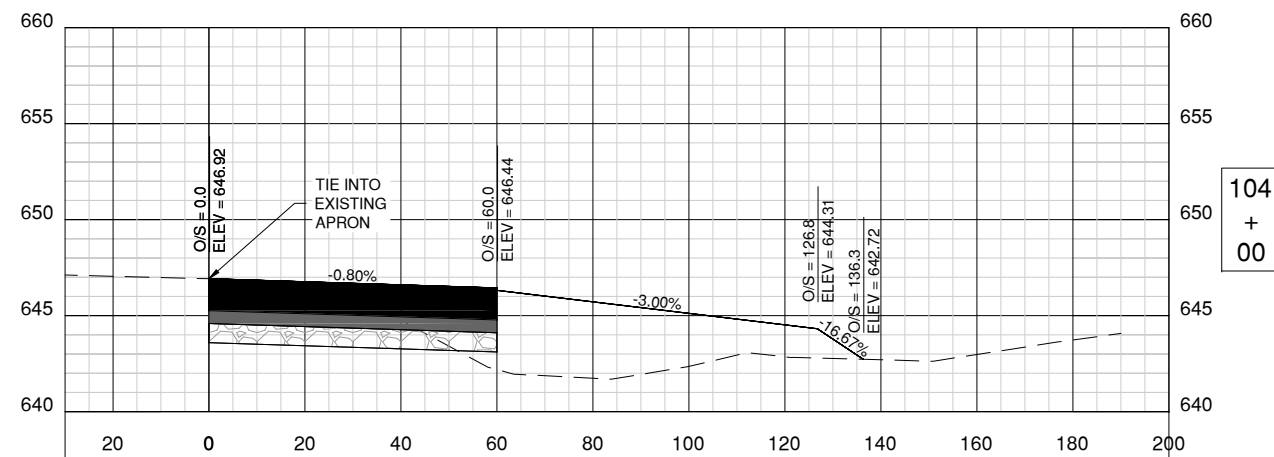
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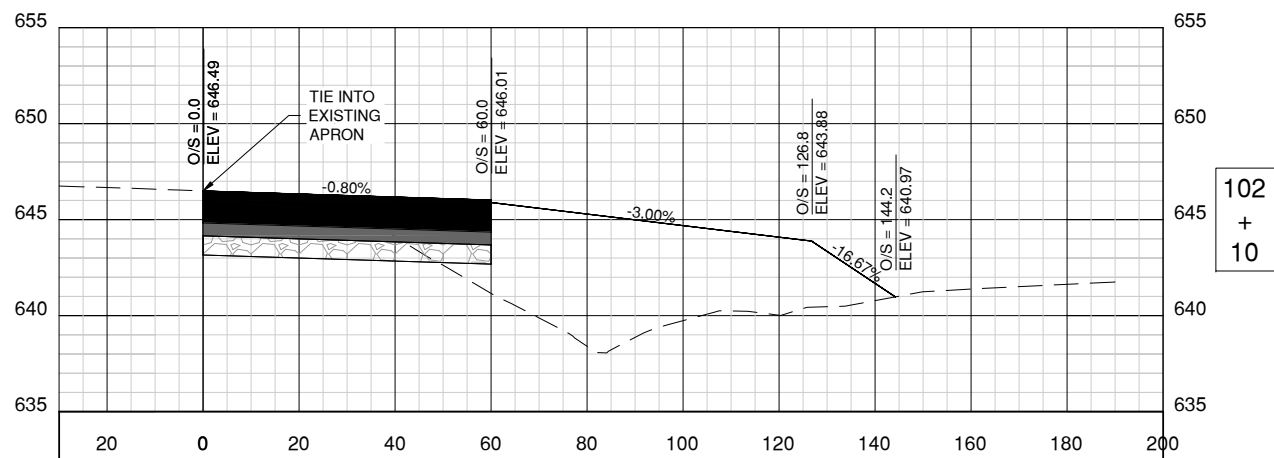
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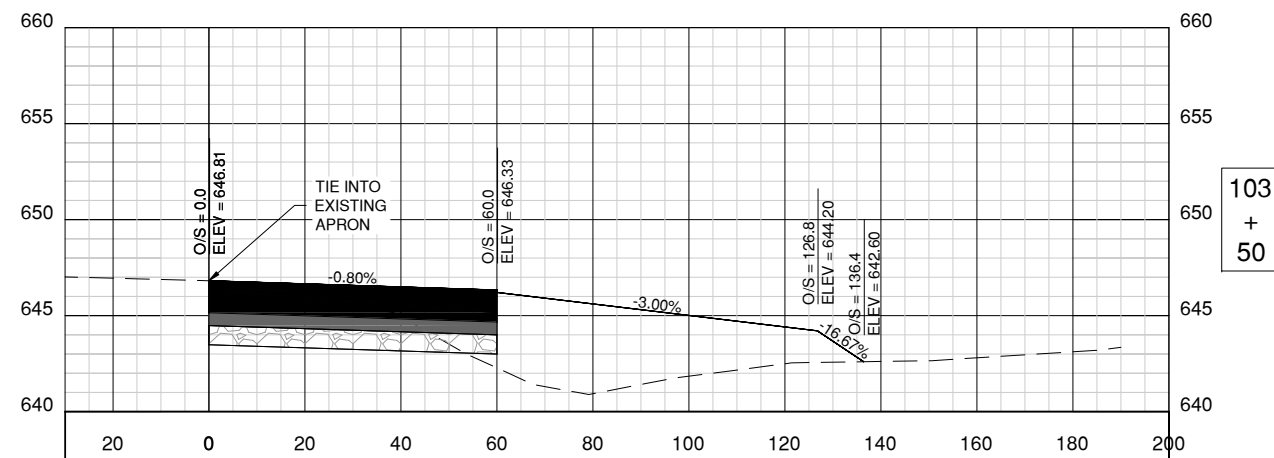
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104
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102
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103
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PE094

REVISIONS

NUMBER	BY	DATE

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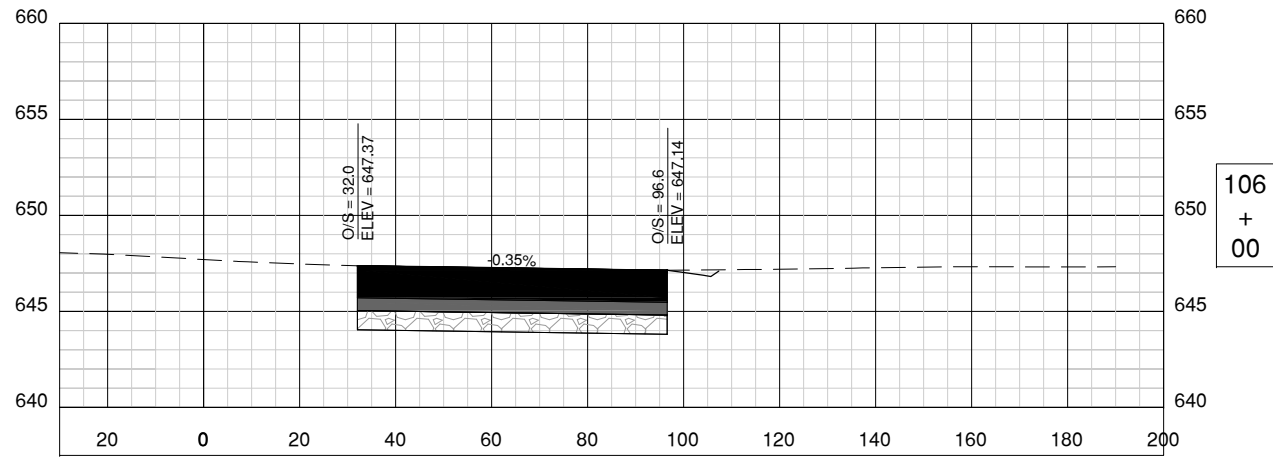
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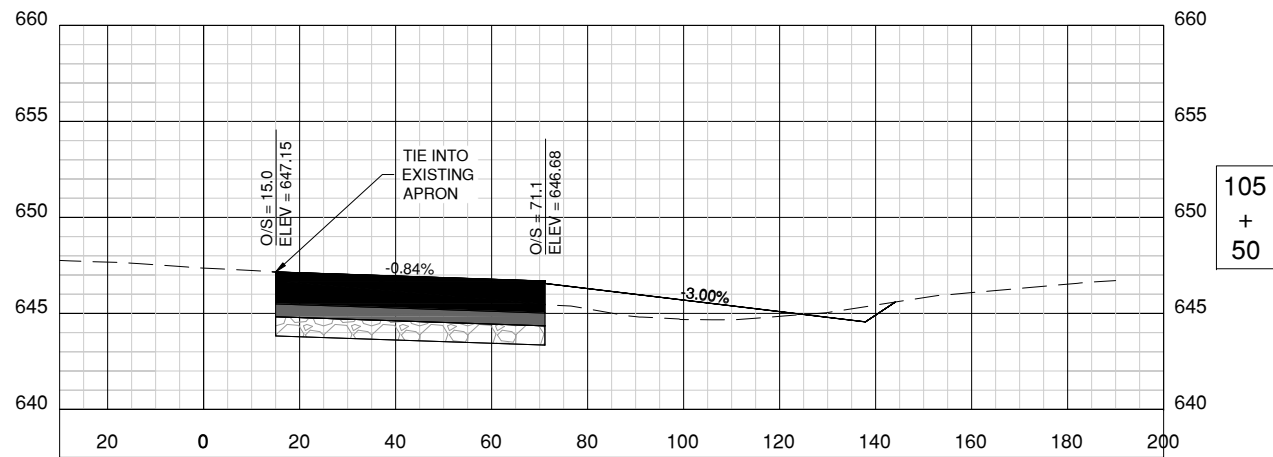
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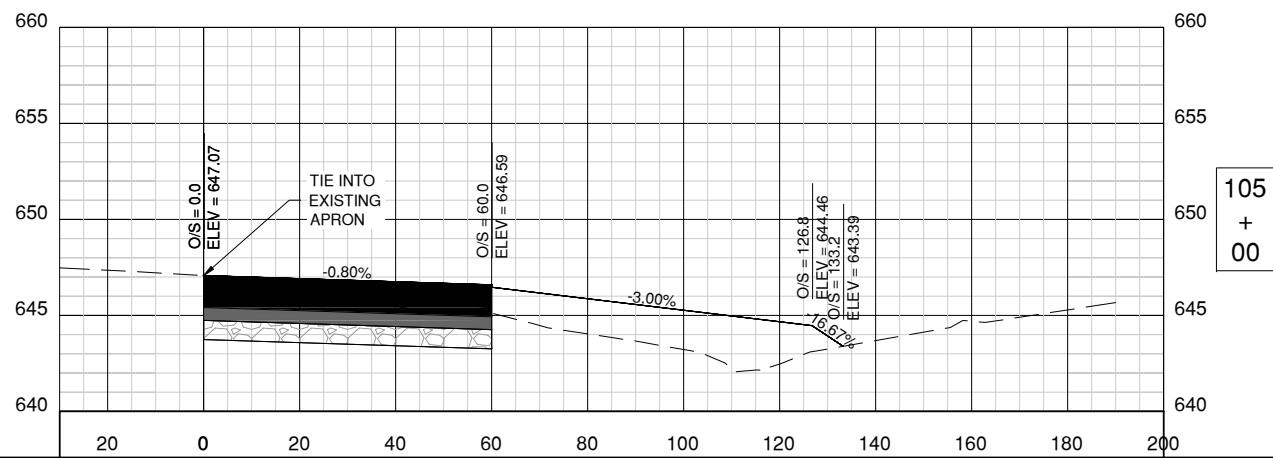


106
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TERMINAL APRON	
STATION	EMBANK END AREA (S.F.)
102+10	103.1
102+50	110.0
103+00	109.9
103+50	115.0
104+00	108.3
104+50	115.4
105+00	148.0
105+50	161.4
106+00	212.6



105
+
50



105
+
00

PE094

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0 1 2
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EXPAND TERMINAL APRON

TERMINAL APRON GRADING SECTIONS 1

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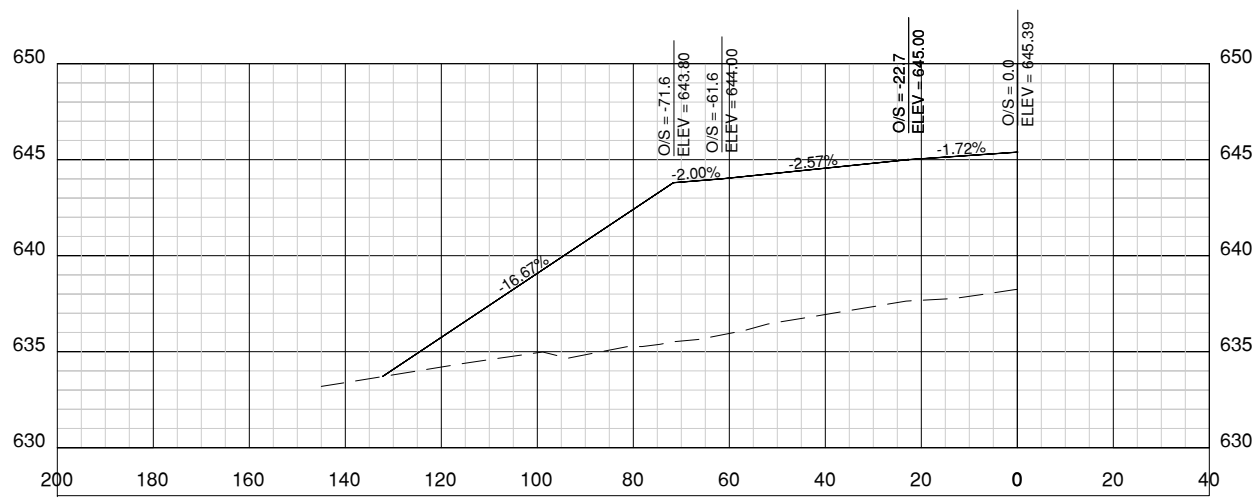


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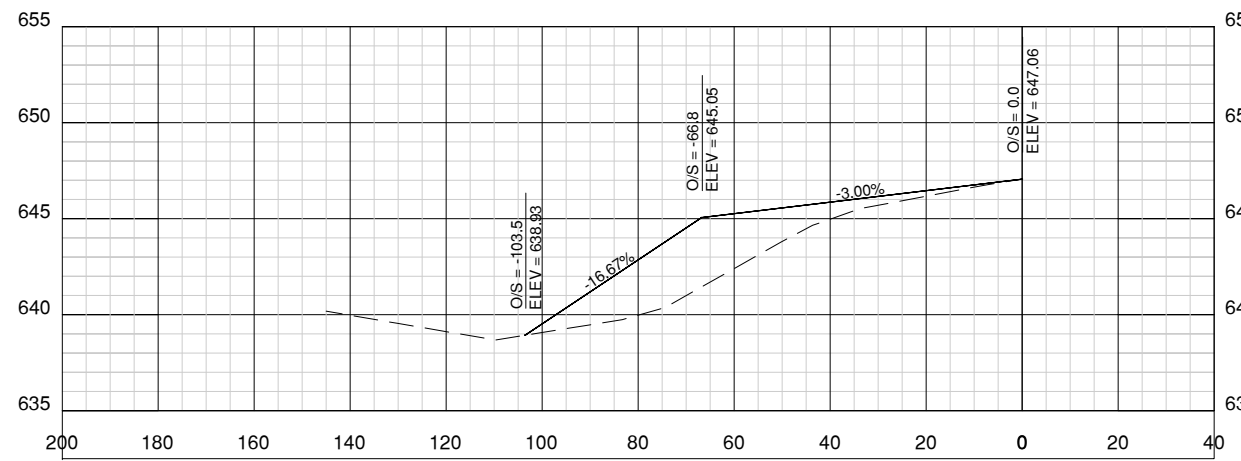
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IL. PROJ. NO. PIA-4180
 AIP PROJ. NO. 3-17-0080-XX

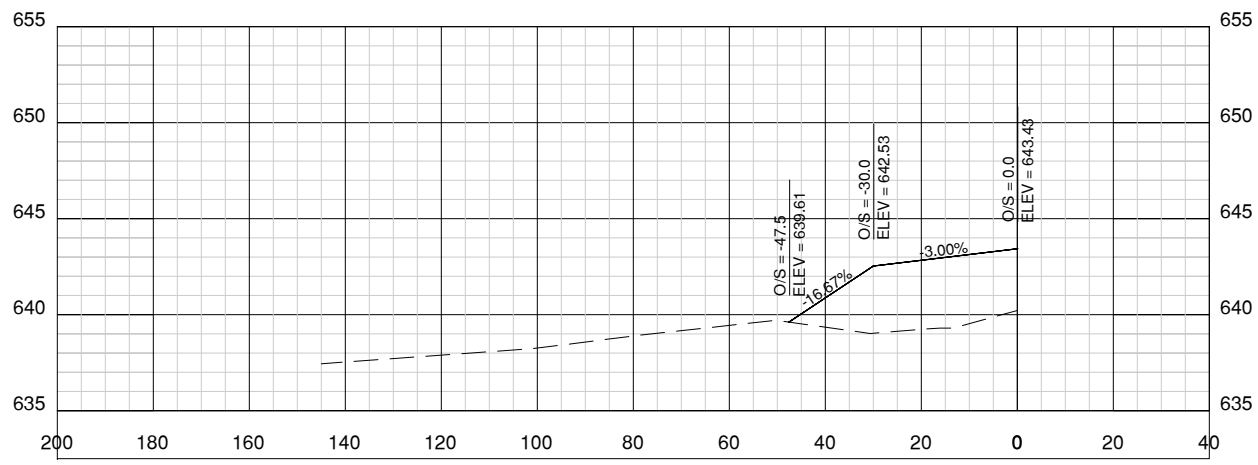
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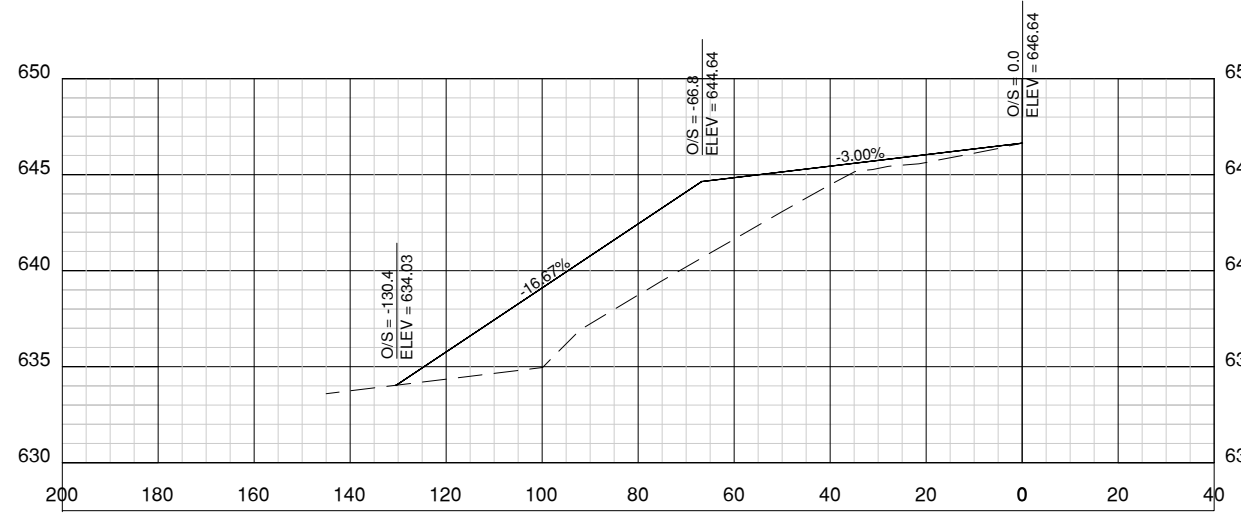
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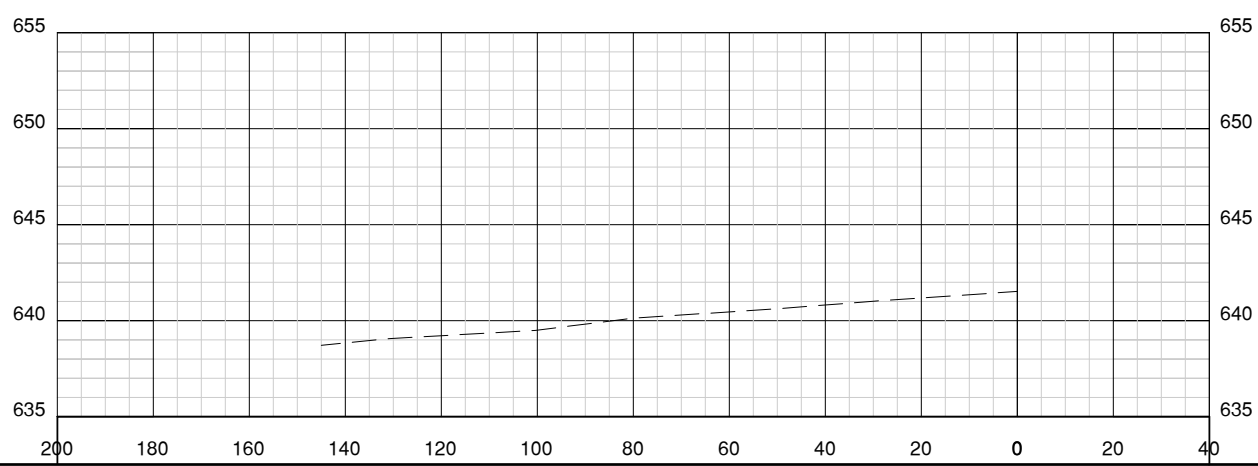
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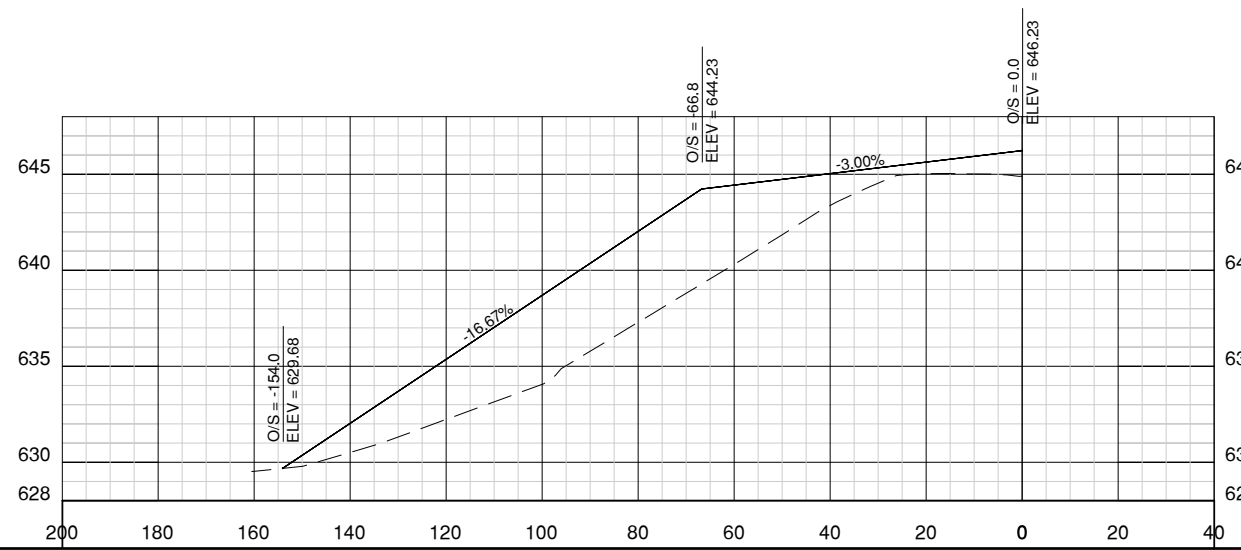
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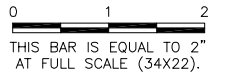


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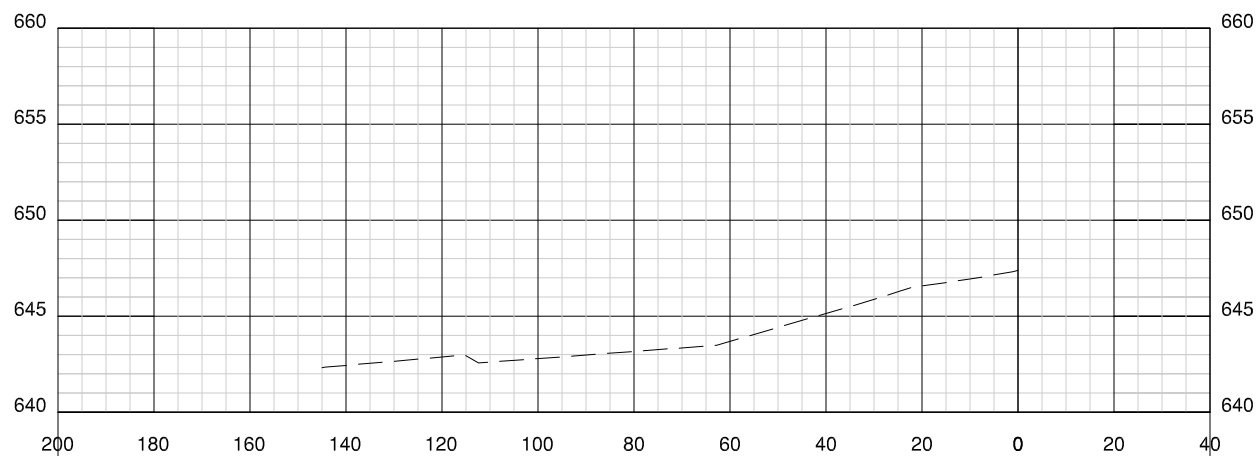
**EXPAND TERMINAL APRON
 TERMINAL APRON GRADING SECTIONS 2**

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APRON GRADING	
STATION	EMBANK END AREA (S.F.)
500+50	0.0
501+00	137.3
501+50	794.4
502+00	417.9
502+50	267.3
503+00	149.2
503+35	0.0

PE094

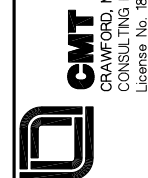
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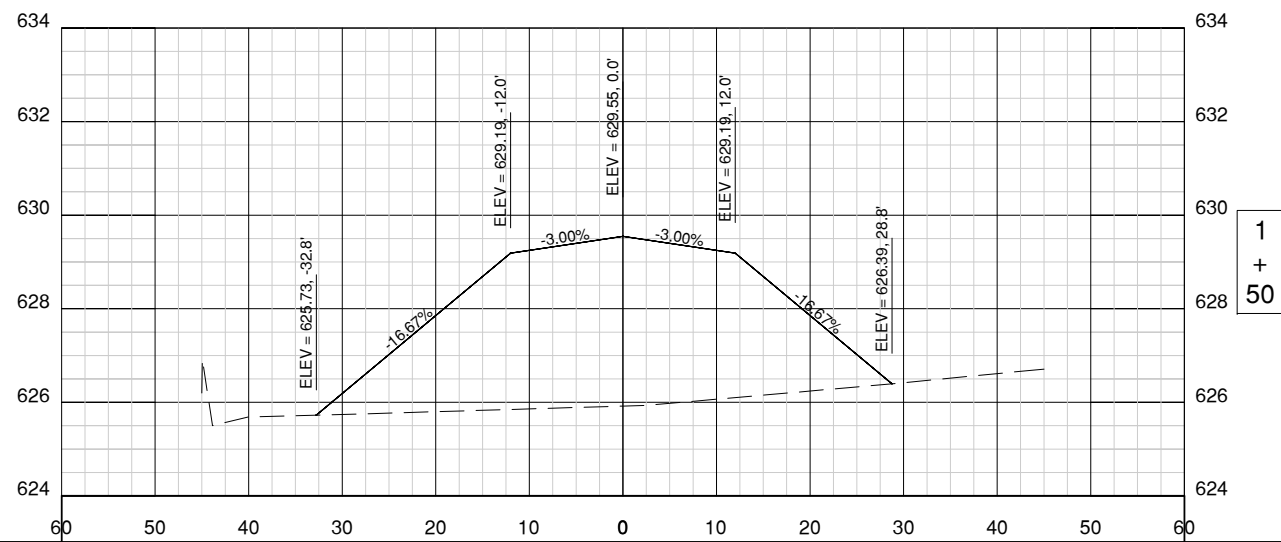
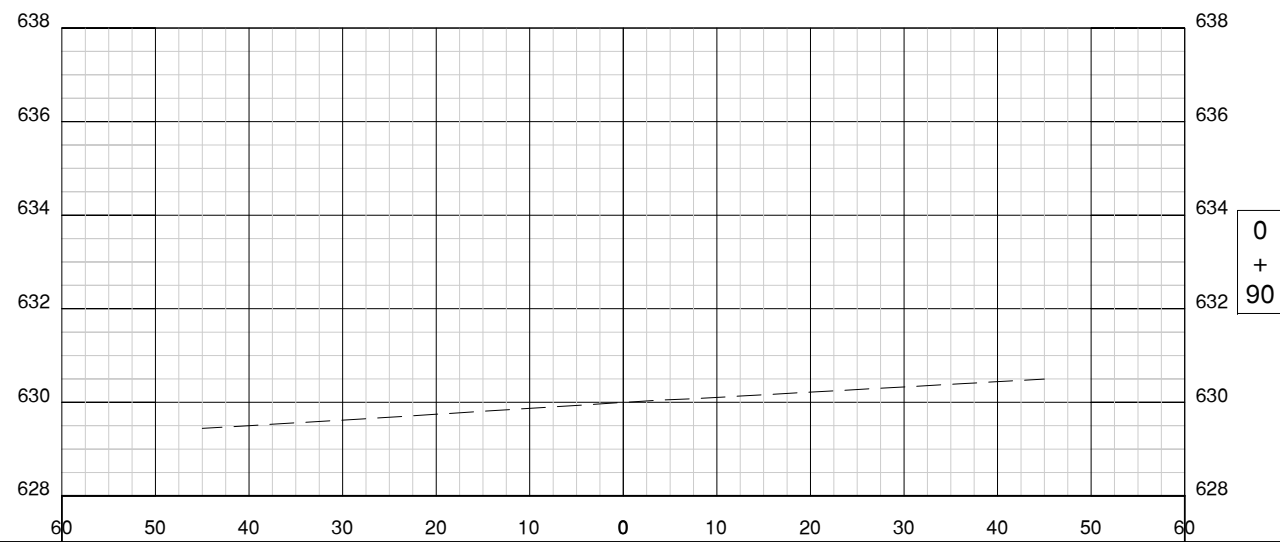
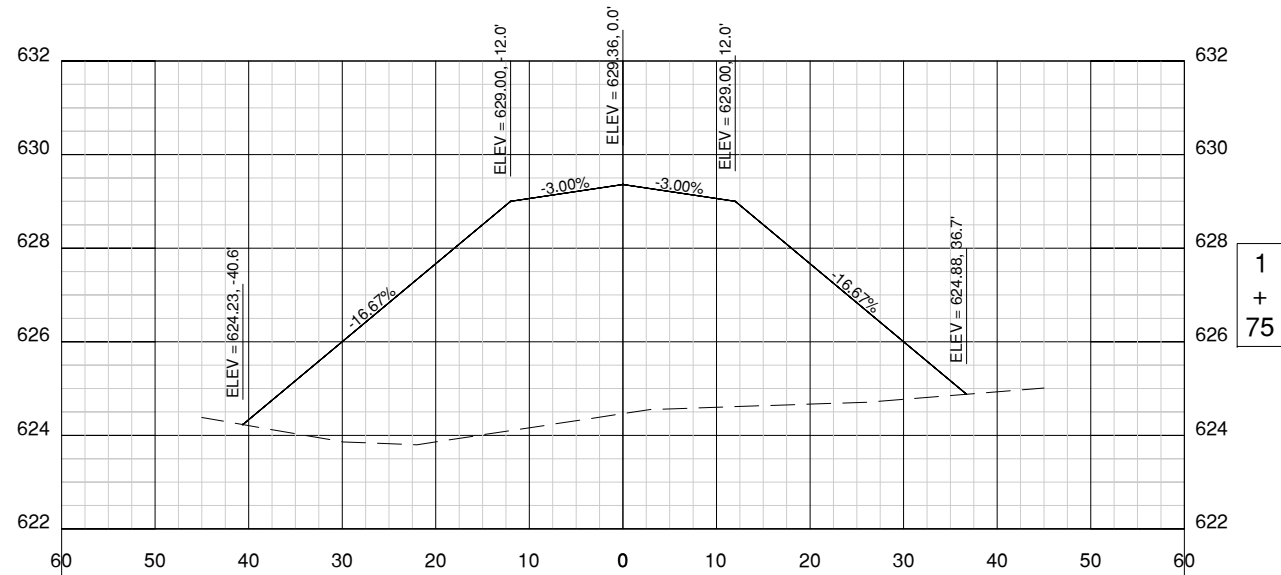
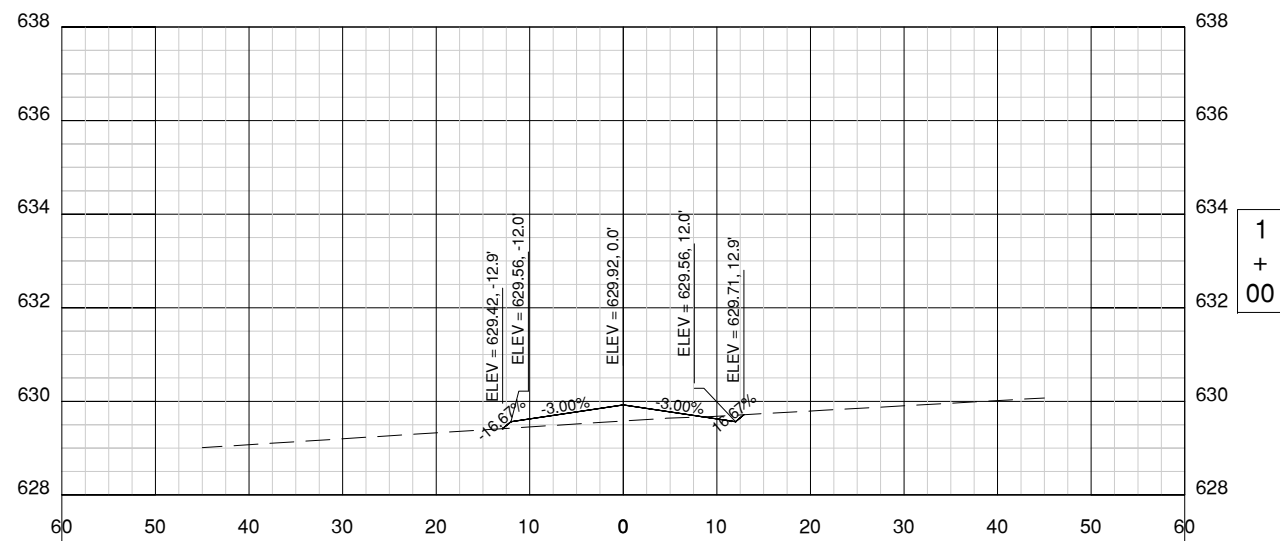
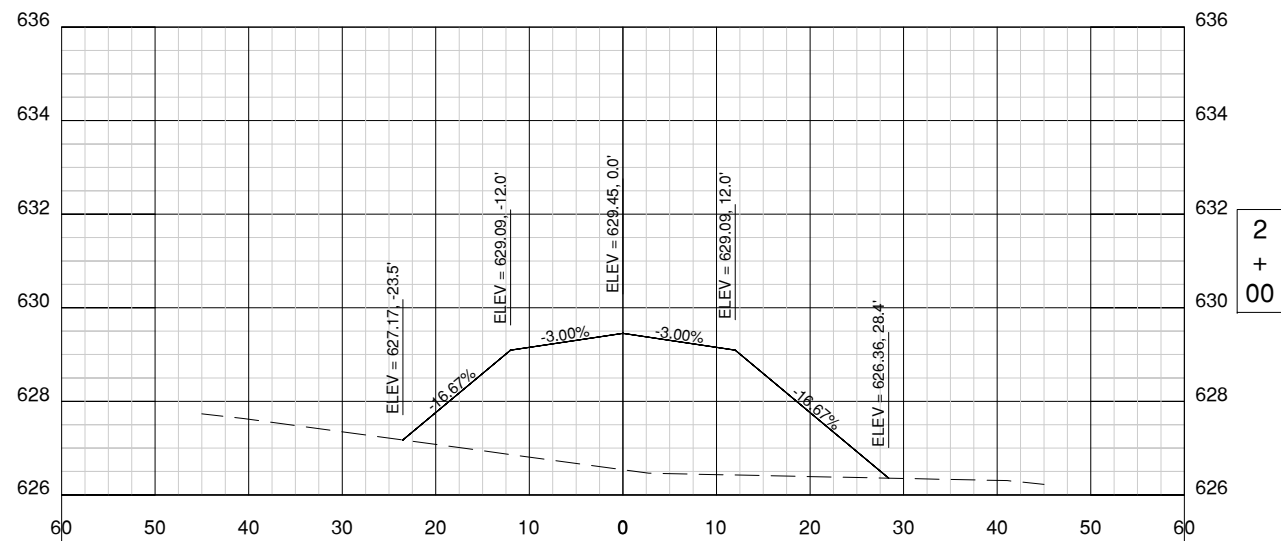
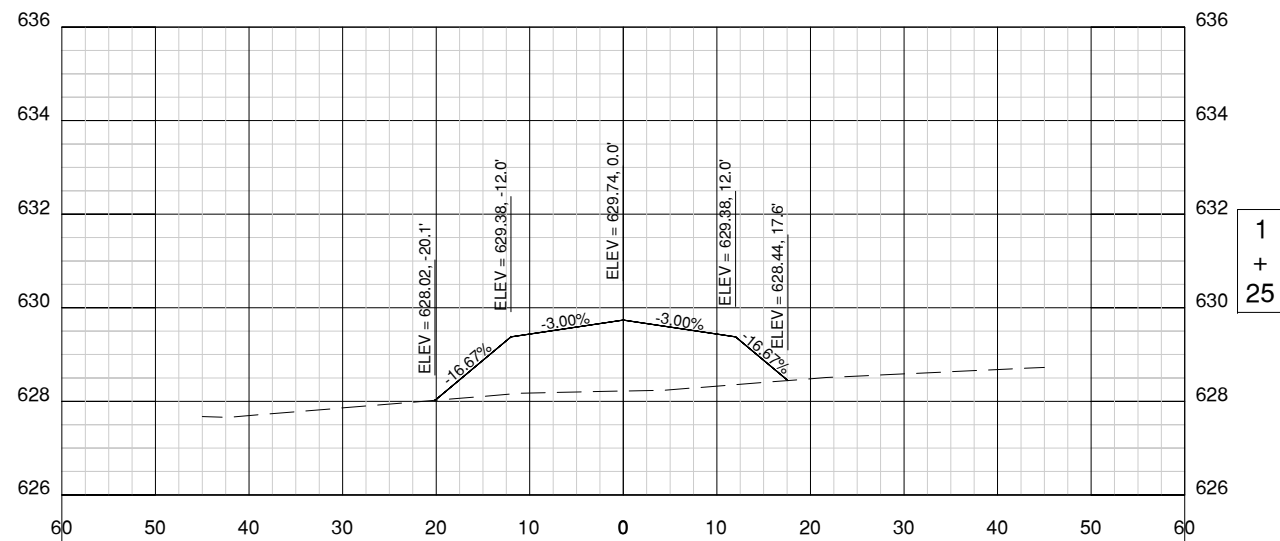
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**EXPAND TERMINAL APRON
 TERMINAL HAUL ROAD SECTIONS 1**

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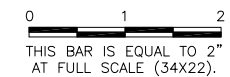


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SHEET	34 OF 35 SHEETS



PE094

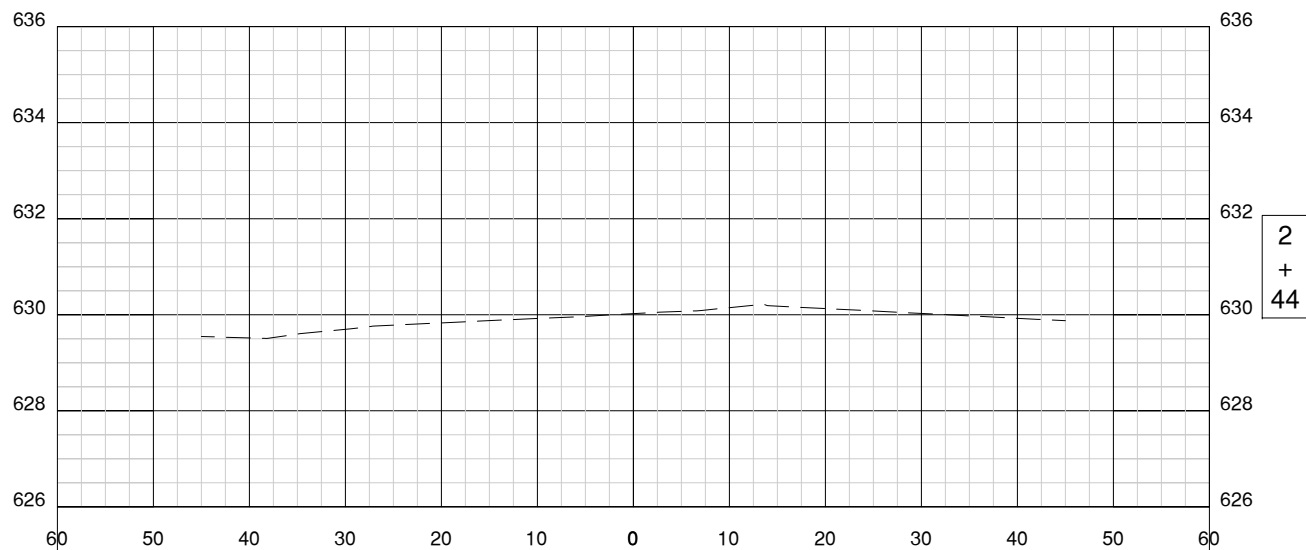
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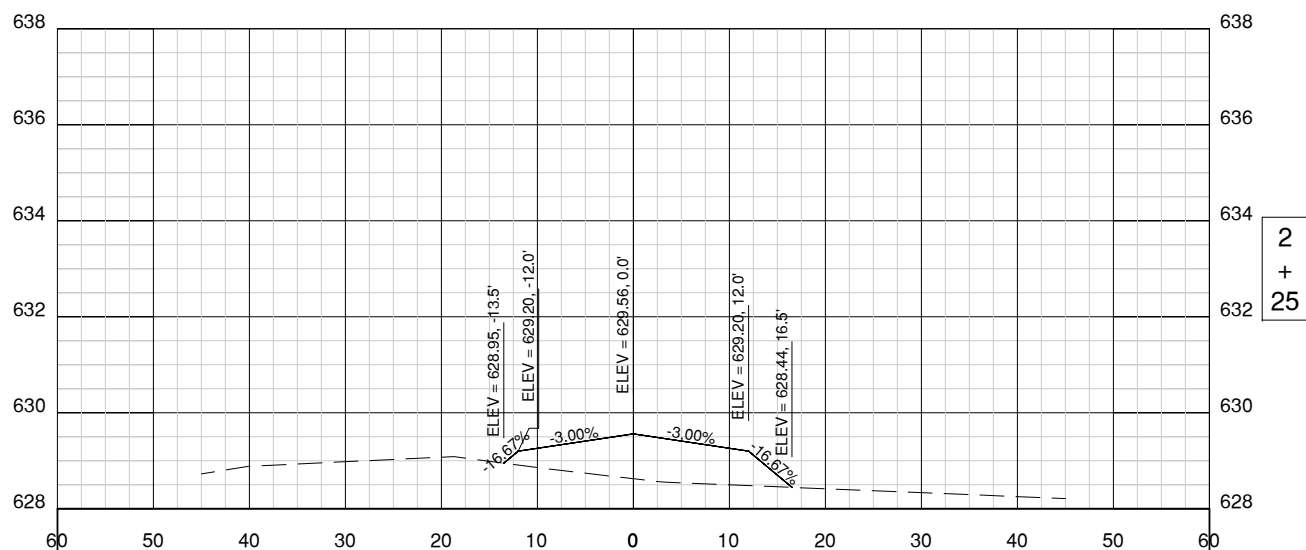
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**EXPAND TERMINAL APRON
 TERMINAL HAUL ROAD SECTIONS 2**

HAUL ROAD	
STATION	EMBANK END AREA (S.F.)
0+90	0.0
1+00	4.4
1+25	39.6
1+50	142.8
1+75	245.3
2+00	99.4
2+25	19.3
2+44	0.0

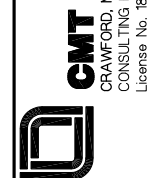


2
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44



2
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25

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SHEET	35 OF 35 SHEETS