

06-14-2024 LETTING ITEM 120

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED FEDERAL AID PROJECT

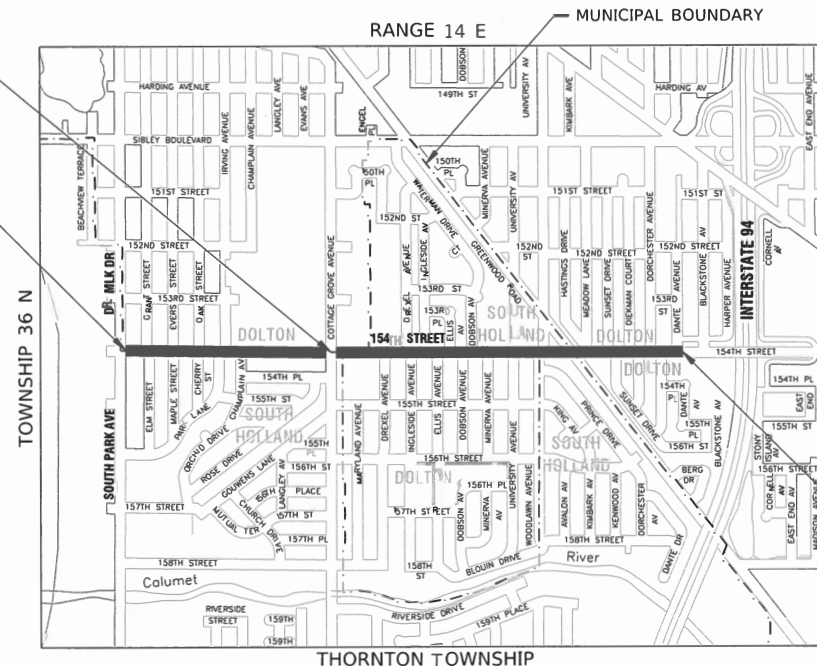
FAU ROUTE 1607 (154TH STREET)
SOUTH PARK AVENUE/DR. MLK JR. DRIVE TO DANTE AVENUE
ROADWAY RESURFACING
SECTION NO.: 23-00120-00-RS
PROJECT NO.: FX97(514)
VILLAGE of DOLTON
VILLAGE of SOUTH HOLLAND
COOK COUNTY
JOB NO.: C-91-185-24



FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR HIGHWAY STANDARDS, SEE SHEET NO. 2

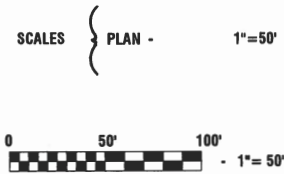
154TH STREET	
SOUTH PARK AVE TO DANTE AVE	
2023 ADT-	7,650
POSTED SPEED LIMIT-	35 MPH
FUNCTIONAL CLASSIFICATION-	MAJOR COLLECTOR

PAVING OMISSION
STA 35+42.00
TO STA 37+47.00
BEGIN IMPROVEMENT
STA 10+80.00



LOCATION MAP
NOT TO SCALE

GROSS LENGTH= 6,929.63 FEET= 1.31 MILES
NET LENGTH= 6,724.63 FEET= 1.27 MILES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1 - 800 - 892 - 0123
OR 811

END IMPROVEMENT
STA 80+09.63



John D. Hilse
02-29-24

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
APPROVED	3-4-2024
	<i>[Signature]</i> VILLAGE OF DOLTON, MAYOR
APPROVED	2-29-2024
	<i>[Signature]</i> VILLAGE OF SOUTH HOLLAND, MAYOR
PASSED	MARCH 25, 2024
	<i>[Signature]</i> DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	March 25, 2024
	<i>[Signature]</i> REGIONAL ENGINEER

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THE STATE OF ILLINOIS

FEDERAL AID PROGRAM ENGINEER: CARMEN RAMOS, PE, SCHAUMBURG, IL
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700

CONTRACT NO. 61K47

INDEX OF SHEETS

1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS AND GENERAL NOTES
3-6	SUMMARY OF QUANTITIES
7	TYPICAL CROSS SECTIONS
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19-20	CONSTRUCTION DETAILS
21-32	IDOT DISTRICT 1 STANDARDS

HIGHWAY STANDARDS

424006-05	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424026-03	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
604001-05	FRAMES AND LIDS TYPE 1
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF ROAD OPERATIONS, MULTILANE, 15'(4.5m) TO 24"(600mm) FROM PAVEMENT EDGE
701106-02	OFF FOAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS \leq 40 MPH
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-09	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
886001-01	DETECTOR LOOP INSTALLATIONS

DISTRICT ONE DETAILS

BD-08	FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINTS AND HMA TAPER
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-26	DRIVEWAY ENTRANCE SIGNING
TS-05	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS
TS-07	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING

GENERAL NOTES

- ITEMS OF WORK LISTED IN THE SUMMARY OF QUANTITIES NOT SPECIFICALLY CALLED OUT ON THE PLANS SHALL BE PERFORMED AS DIRECTED BY THE ENGINEER.
- THE ROBINSON ENGINEERING, LTD. FIELD OFFICE (708-331-6700), AND THE PUBLIC WORKS DIRECTOR, AT THE VILLAGE OF SOUTH HOLLAND AND THE VILLAGE OF DOLTON, SHALL BE NOTIFIED TWO (2) WORKING DAYS BEFORE CONSTRUCTION BEGINS.
- BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 AND (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED)
- A NICOR REPRESENTATIVE MUST BE PRESENT WHEN WORKING WITHIN 25' OF THE PIPELINE.
- UTILITIES INDICATED ON THE PLANS ARE PROVIDED FOR THE CONTRACTOR'S USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF UTILITY INFORMATION.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE THICKNESS OF HMA MIXTURE STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA SURFACE IS PLACED.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SHALL BE REMOVED BY THE END OF EACH DAY BY THE CONTRACTOR.
- CLASS D PATCHING QUANTITIES FOR THIS CONTRACT SHALL BE PERFORMED AT THE DIRECTION OF THE ENGINEER AFTER PAVEMENT MILLING.
- CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN.
- ALL EQUIPMENT SHALL BE REMOVED OFF THE VILLAGE STREETS DURING ALL HOLIDAY WEEKENDS AS COORDINATED WITH THE VILLAGE.
- HMA PAVING SHALL BE PERFORMED WITH HOT JOINTS.
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- WHEN WORKING ADJACENT TO THE ROAD AND UTILIZING DAILY LANE CLOSURES, DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. DROP-OFFS GREATER THAN (OR GREATER THAN OR EQUAL TO) 12" WILL NOT BE ALLOWED AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FT OF THE EDGE OF THE TRAVEL LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION DURING THE TIME THAT THE ADJACENT LANE IS CLOSED. AS NOTED ABOVE, PRIOR TO REOPENING THE LANE TO TRAFFIC THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN 12". THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME OF THE DAILY LANE CLOSURE. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT.
- CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL TRAFFIC STAGE CHANGES ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGN MESSAGES SHALL BE REVISED TWO WEEKS THEREAFTER WITH MESSAGES WARNING TRAFFIC OF POTENTIAL TRAFFIC DELAYS, QUEUING AND/OR WITH MESSAGES NOTIFYING TRAFFIC TO USE ALTERNATE ROUTES. THE SIGN LOCATIONS AND MESSAGES SHALL BE DETERMINED BY THE ENGINEER.
- TEMPORARY INFORMATION SIGNS ON TEMPORARY SUPPORTS SHALL BE PROVIDED FOR ALL COMMERCIAL DRIVEWAYS THAT ARE LOCATED WITHIN A WORK AREA. THIS WORK SHALL BE PAID FOR PER DISTRICT 1 DETAIL TC-26. THESE SIGNS SHALL BE RELOCATED AS REQUIRED FOR EACH CONSTRUCTION STAGE AND SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THIS SIGN RELOCATION WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR TEMPORARY INFORMATION SIGNING.
- CONTRACTOR SHALL REMOVE OR COVER ALL W21-1 (WORKERS PRESENT) AND W20-7 (FLAGGER) SIGNS WHEN WORKERS ARE NOT PRESENT OR IF THE SIGN IS NOT APPLICABLE
- DRUMS ALONG ARTERIAL ROADWAYS SHALL BE PLACED AS FOLLOWS: 50' C-C ALONG TANGENTS; 20' C-C ALONG TAPERS; & 10' C-C ALONG RADII/CURVES
- CONTRACTOR IS TO MAINTAIN FULL ACCESS TO AT LEAST ONE SIDE OF THE SIDEWALK (SHARED-USE PATH) AT ALL TIMES. CROSSWALKS ARE TO BE MAINTAINED THROUGH THE USE OF EXISTING OR TEMPORARY PAVEMENT MARKINGS AND TEMPORARY SIDEWALK, AS REQUIRED.

COMMITMENTS

NONE.

FILE NAME = 23R0457-INDX-01 - P01	USER NAME =	DESIGNED -- MPN	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	154TH STREET ROADWAY RESURFACING			F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		CHECKED -- WPD	REVISED --		INDEX OF SHEETS & STATE STANDARDS, STANDARDS & GENERAL NOTES	1607	23-00120-00-RS	COOK	32	2			
	PLOT SCALE =	DRAWN -- RG	REVISED --		SCALE: NONE	SHEET NO. 2	OF 32 SHEETS	STA.	TO STA.	CONTRACT NO. 61K47			
	PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)								

S.I.	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	FED 100%	FED 100%
					VILLAGE 0%	VILLAGE 0%
					ROADWAY	SAFETY
CONSTRUCTION TYPE CODE						
					0005	0021
	20200100	EARTH EXCAVATION	CU YD	10	10	
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	50	50	
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1,521	1,521	
	25200110	SODDING, SALT TOLERANT	SQ YD	1,521	1521	
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	50	50	
	35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	2,055		2,055
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	30,246	30,246	
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	22,950	22,950	
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	67	67	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	580	580	
	40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	5,646	5,646	
	40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	5,019	5,019	
	42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	159	159	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	10,681		10,681
	42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	230		230
	42400800	DETECTABLE WARNINGS	SQ FT	878		878

FILE NAME = 23R0457-QUAN-01-Q01

USER NAME =	DESIGNED -- MPN	REVISED --
	CHECKED -- JDH	REVISED --
PLOT SCALE =	DRAWN -- BG	REVISED --
PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

154TH STREET
ROADWAY RESURFACING
SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 3 OF 32 SHEETS STA. TO STA.

FAU RATE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	3
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				

S.I.	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	FED 100%	FED 100%
					VILLAGE 0%	VILLAGE 0%
					ROADWAY	SAFETY
CONSTRUCTION TYPE CODE						
					0005	0021
	44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SQ YD	44,809	44,809	
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	756	756	
	44000300	CURB REMOVAL	FOOT	10	10	
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	3,705	3,705	
	44000600	SIDEWALK REMOVAL	SQ FT	12,338		12,338
	44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	10	10	
	44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	20	20	
	44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	24	24	
	44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	1,265	1,265	
	60266600	VALVE BOXES TO BE ADJUSTED	EACH	20	20	
	60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1	1	
	60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	2	2	
	60600605	CONCRETE CURB, TYPE B	FOOT	10	10	
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1,507	1,507	
	60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	37	37	
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1,993	1,993	

FILE NAME = 23R0457-QUAN-01 - 002

USER NAME =	DESIGNED -- MPN	REVISED --
PLOT SCALE =	CHECKED -- JDH	REVISED --
PLOT DATE = 04-02-24	DRAWN -- BG	REVISED --
	CHECKED -- APG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

154TH STREET ROADWAY RESURFACING SUMMARY OF QUANTITIES		F.A.U. RTE. 1607	SECTION 23-00120-00-RS	COUNTY COOK	TOTAL SHEETS 32	SHEET NO. 4
SCALE:	SHEET NO. 4 OF 32 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514) CONTRACT NO. 61K47		

S.I.	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	FED 100%	FED 100%
					VILLAGE 0%	VILLAGE 0%
					ROADWAY	SAFETY
CONSTRUCTION TYPE CODE						
					0005	0021
	60608562	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12	FOOT	168	168	
	67100100	MOBILIZATION	L SUM	1	1	
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1	
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	50	50	
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	4,730		4,730
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1,580		1,580
*	72000100	SIGN PANEL - TYPE 1	SQ FT	425		425
	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	3		3
	72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	1		1
*	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	456		456
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	328		328
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	15,571		15,571
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,029		1,029
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	3,881		3,881

FILE NAME = 23R0457-QUAN-01 - 003

USER NAME --	DESIGNED -- MPN	REVISED --
	CHECKED -- JDH	REVISED --
PLOT SCALE --	DRAWN -- BG	REVISED --
PLOT DATE -- 04-02-24	CHECKED -- APG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

154TH STREET ROADWAY RESURFACING SUMMARY OF QUANTITIES		
SCALE:	SHEET NO. 5 OF 32 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	5
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				

S.I.	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	FED 100%	FED 100%
					VILLAGE 0%	VILLAGE 0%
					ROADWAY	SAFETY
CONSTRUCTION TYPE CODE						
					0005	0021
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	788		788
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	585		585
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	585		585
	Z0004530	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8"	SQ YD	597	597	
	Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	75	75	
	Z0017700	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED	EACH	2	2	
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	125	125	
*	K1004595	PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE	L SUM	1	1	
	X1700021	BRICK PAVER REMOVAL AND REINSTALLATION, SPECIAL	SQ FT	39	39	
	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	13	13	
	X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	10	10	
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	63	63	
	X6066016	HOT-MIX ASPHALT MEDIAN SURFACE, 4 INCH	SQ FT	87		87
*	X7800200	PAINT PAVEMENT MARKING CURB	FOOT	31		31
*	X8860105	DETECTOR LOOP REPLACEMENT	FOOT	1,165		1,165

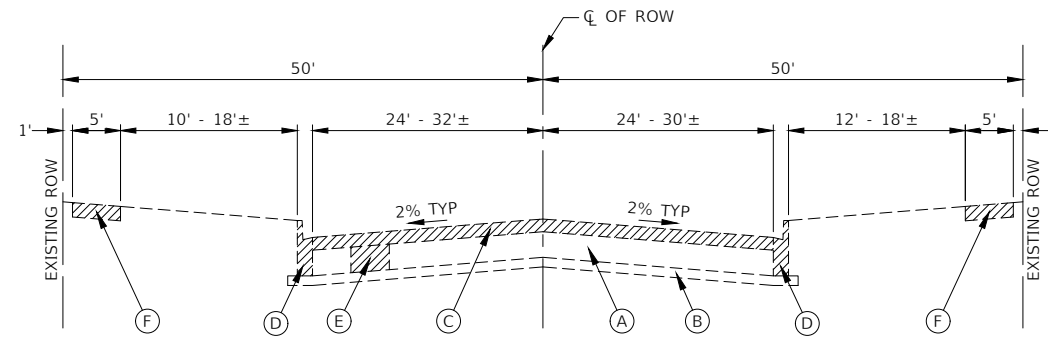
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USER NAME --	DESIGNED -- MPN	REVISED --
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PLOT SCALE --	DRAWN -- BG	REVISED --
PLOT DATE -- 04-02-24	CHECKED -- APG	REVISED --

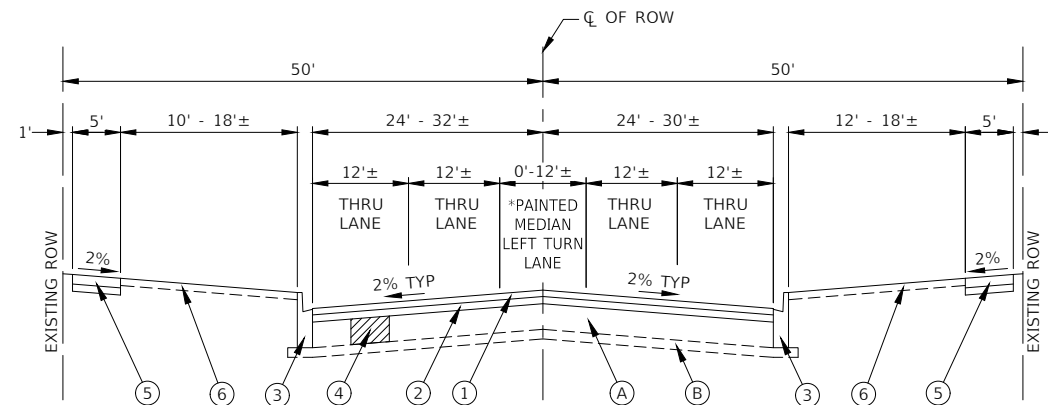
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

154TH STREET ROADWAY RESURFACING SUMMARY OF QUANTITIES			
SCALE:	SHEET NO. 6	OF 32 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	6
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				



EXISTING TYPICAL SECTION
154TH STREET
SOUTH PARK AVE/DR. MLK JR. DRIVE TO DANTE AVENUE



PROPOSED TYPICAL SECTION
154TH STREET
SOUTH PARK AVE/DR. MLK JR. DRIVE TO DANTE AVENUE

*PAINTED MEDIAN TRANSITIONS:
 STA 30+85 TO STA 33+25 FROM 0' TO 12'
 STA 39+75 TO STA 42+15 FROM 12' TO 0'
 STA 60+14 TO STA 62+14 FROM 0' TO 12'
 STA 70+00 TO STA 72+00 FROM 12' TO 0'

EXISTING LEGEND

- (A) EXISTING PAVEMENT, 4 1/2" TO 16"±
- (B) EXISTING SUBBASE, 6" TO 18"±
- (C) HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- (D) EXISTING CURB AND GUTTER, TYPE B-6.24
- (E) PAVEMENT REMOVAL FOR CLASS D PATCHES, 8 INCH
- (F) EXISTING SIDEWALK
- ITEM TO BE REMOVED (AS DIRECTED BY ENGINEER)

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4"
- (3) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (AS DIRECTED BY ENGINEER)
- (4) CLASS D PATCHES, 8 INCH (AS DIRECTED BY ENGINEER)
- (5) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH OR 8 INCH AT DRIVEWAYS AGGREGATE BASE COURSE, TYPE B 4" (AS DIRECTED BY ENGINEER)
- (6) TOPSOIL FURNISH & PLACE, 4" SODDING, SALT TOLERANT

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	QMP
ROADWAY RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, 1L-9.5, MIX "D", N50, 2"	4% @ 50 Gyr.	LR 1030-2
HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N50, 2 1/4"	4% @ 50 Gyr.	LR 1030-2
HMA DRIVEWAY PAVEMENT, 8"		
HOT-MIX ASPHALT SURFACE COURSE, 1L-9.5, MIX "D", N50, 2"	4% @ 50 Gyr.	LR 1030-2
HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N50, 6"	4% @ 50 Gyr.	LR 1030-2
PATCHING		
CLASS D PATCHES (HMA BINDER 1L-19.0 mm), 8 INCH	4% @ 70 Gyr.	LR 1030-2
HOT-MIX ASPHALT PARKWAY (HMA MEDIAN SURFACE, 4 INCH)		
HOT-MIX ASPHALT SURFACE COURSE, 1L-9.5, MIX "D", N50, 4"	4% @ 50 Gyr.	LR 1030-2
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) PER LR1030-2		

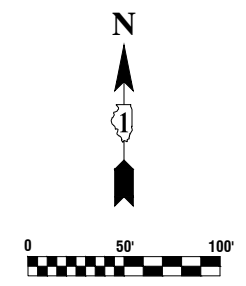
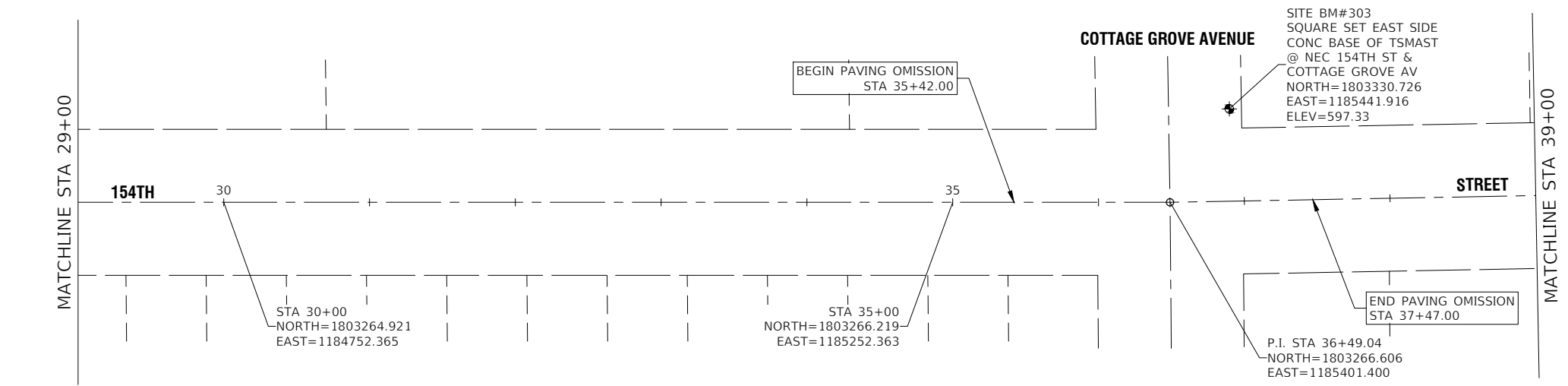
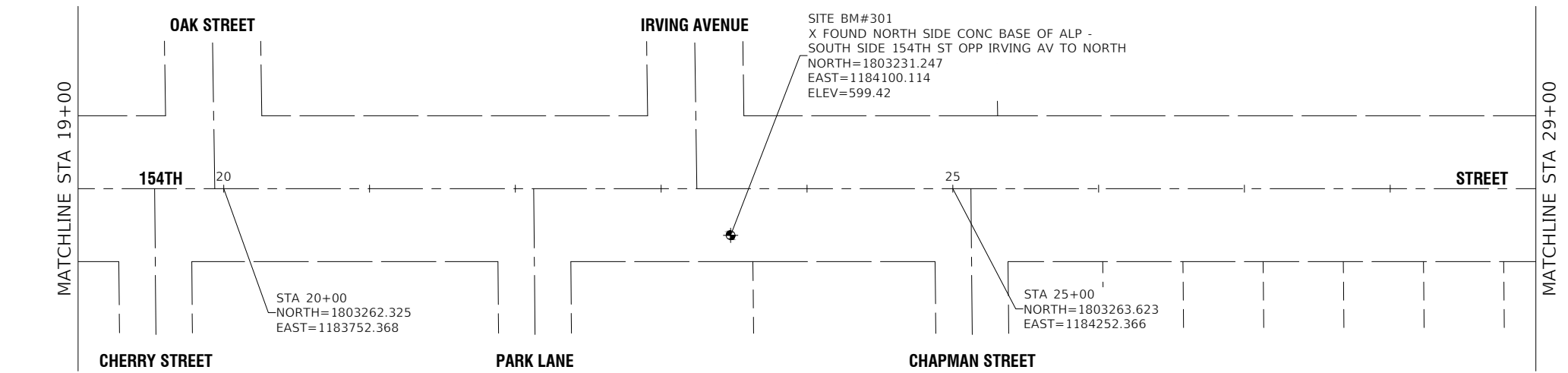
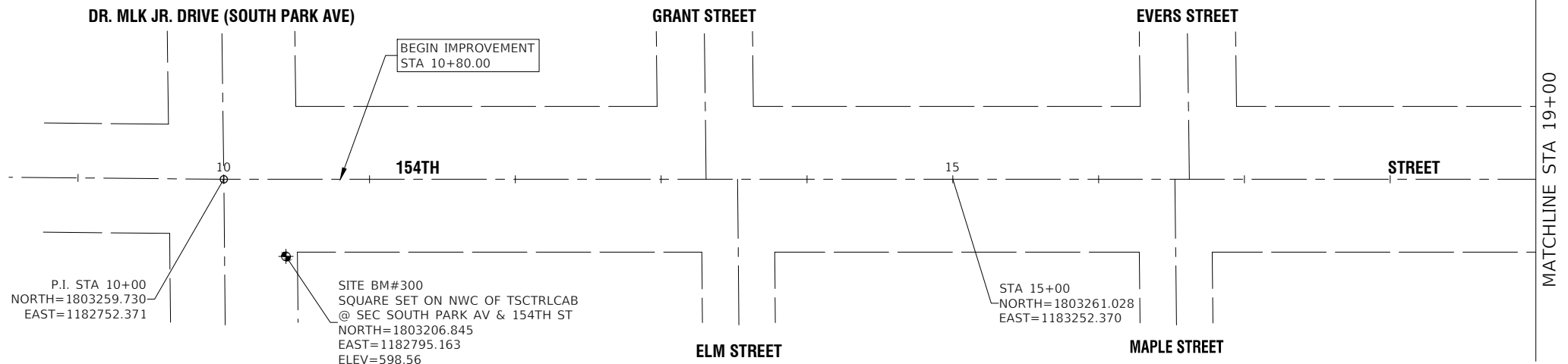
NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE IS 112 LBS/SQ/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG-64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

NOTE

1. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50
2. ALL PATCHING OPERATIONS SHALL TAKE PLACE AFTER SURFACE MILLING HAS BEEN COMPLETED.

FILE NAME = 23R0457-TYPX-01 - P01	USER NAME =	DESIGNED -- MPN	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	154TH STREET ROADWAY RESURFACING TYPICAL CROSS SECTIONS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
		CHECKED -- JDH	REVISED --			1607	23-00120-00-RS	COOK	32	7		
	PLOT SCALE =	DRAWN -- BG	REVISED --			CONTRACT NO. 61K47						
	PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --			SCALE: NONE	SHEET NO. 7	OF 32 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS



NOTES

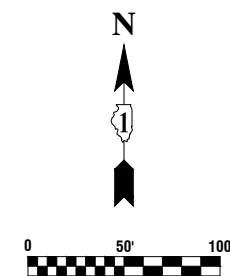
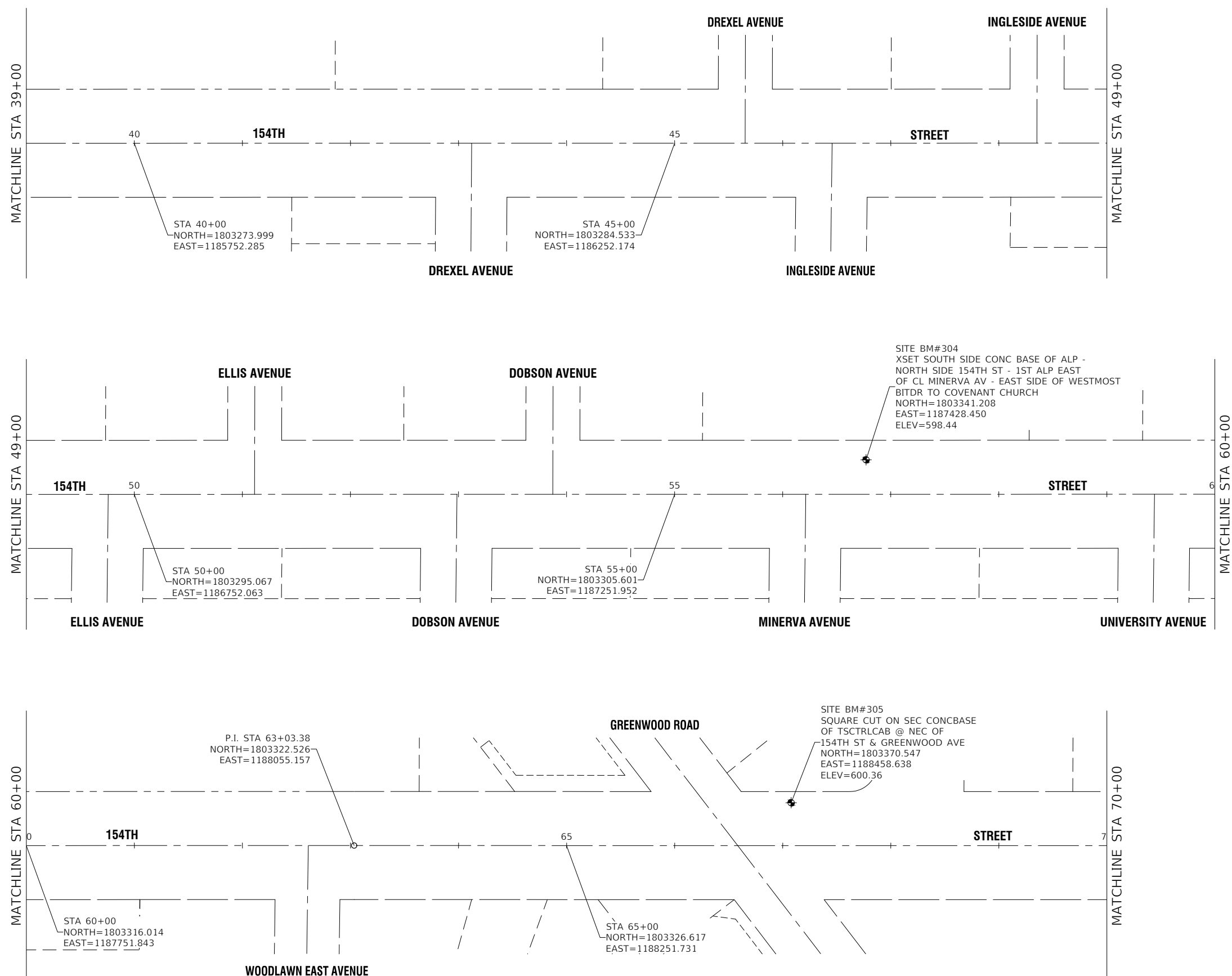
1. COORDINATES ARE BASED ON ILLINOIS STATE PLANE EASTERN ZONE NORTH AMERICAN DATUM 1983.
2. SURVEY FEET UNITS WERE USED.
3. ALL ELEVATIONS REFER TO NAVD88 DATUM.

FILE NAME = 23R0457-TIES-01 - P01	USER NAME =	DESIGNED -- MPN	REVISED --
		CHECKED -- JDH	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

154TH STREET ROADWAY RESURFACING ALIGNMENT AND TIES			
SCALE: 1"=50'	SHEET NO. 8	OF 32 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	8
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				



NOTES

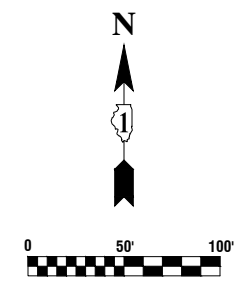
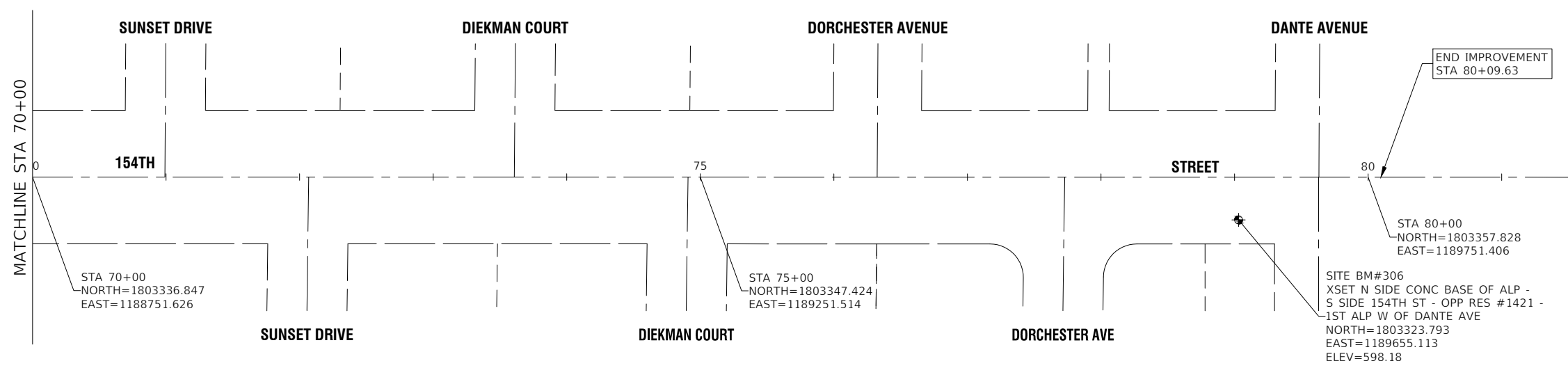
1. COORDINATES ARE BASED ON ILLINOIS STATE PLANE EASTERN ZONE NORTH AMERICAN DATUM 1983.
2. SURVEY FEET UNITS WERE USED.
3. ALL ELEVATIONS REFER TO NAVD88 DATUM.

FILE NAME = 23R0457-TIES-01 - P02	USER NAME =	DESIGNED -- MPN	REVISED --
		CHECKED -- JDH	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

154TH STREET ROADWAY RESURFACING ALIGNMENT AND TIES			
SCALE: 1"=50'	SHEET NO. 9	OF 32 SHEETS	STA. TO STA.

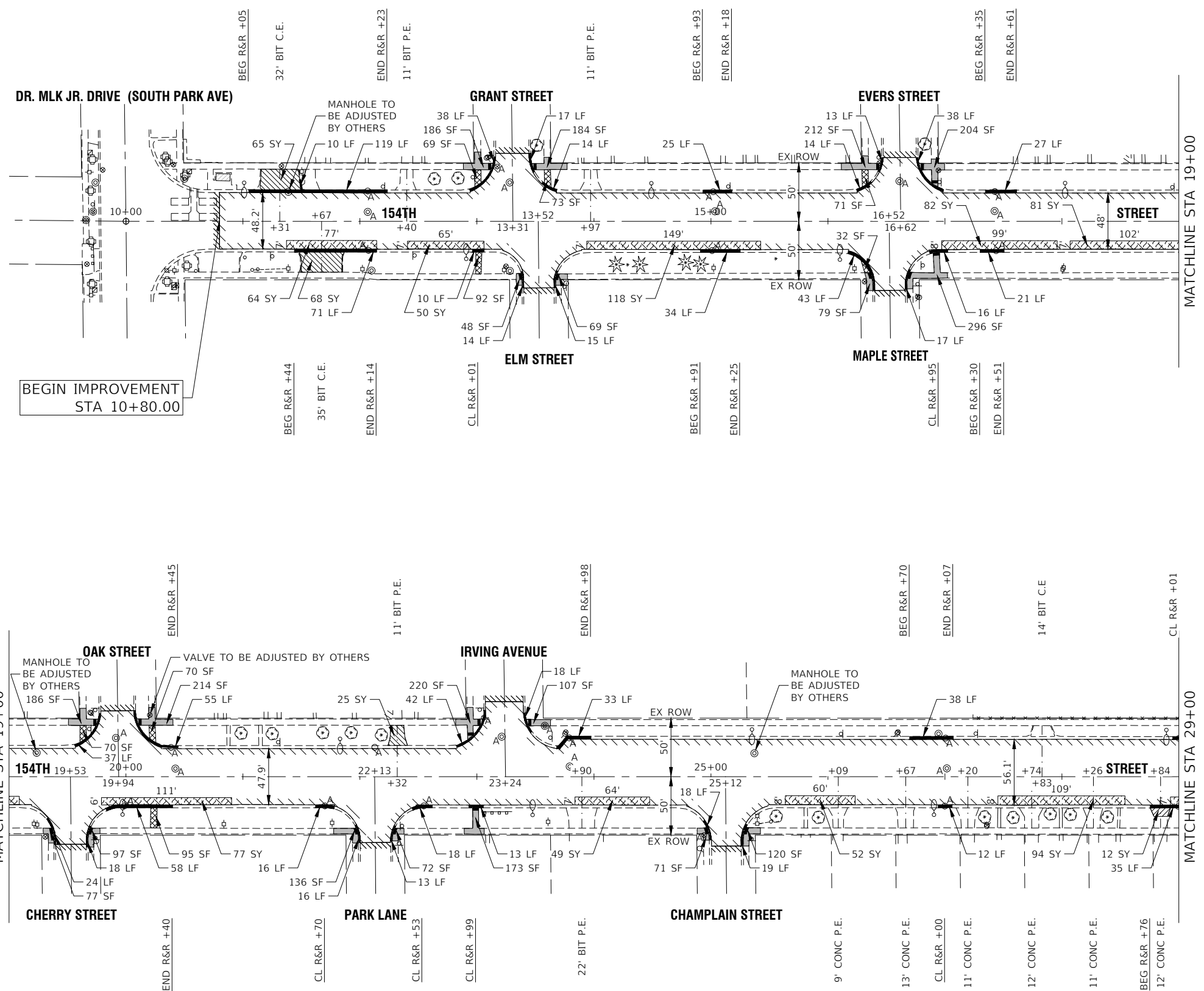
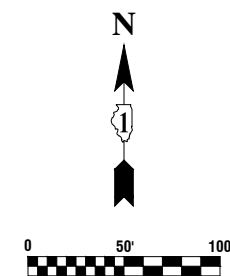
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	9
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				



NOTES

1. COORDINATES ARE BASED ON ILLINOIS STATE PLANE EASTERN ZONE NORTH AMERICAN DATUM 1983.
2. SURVEY FEET UNITS WERE USED.
3. ALL ELEVATIONS REFER TO NAVD88 DATUM.

FILE NAME = 23R0457-TIES-01 - P03	USER NAME =	DESIGNED -- MPN	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	154TH STREET ROADWAY RESURFACING ALIGNMENT AND TIES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		CHECKED -- JDH	REVISED --		SCALE: 1"=50'	SHEET NO. 10 OF 32 SHEETS	1607	23-00120-00-RS	COOK	32	10
	PLOT SCALE =	DRAWN -- RG	REVISED --		STA.	TO STA.	CONTRACT NO. 61K47				
	PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)						



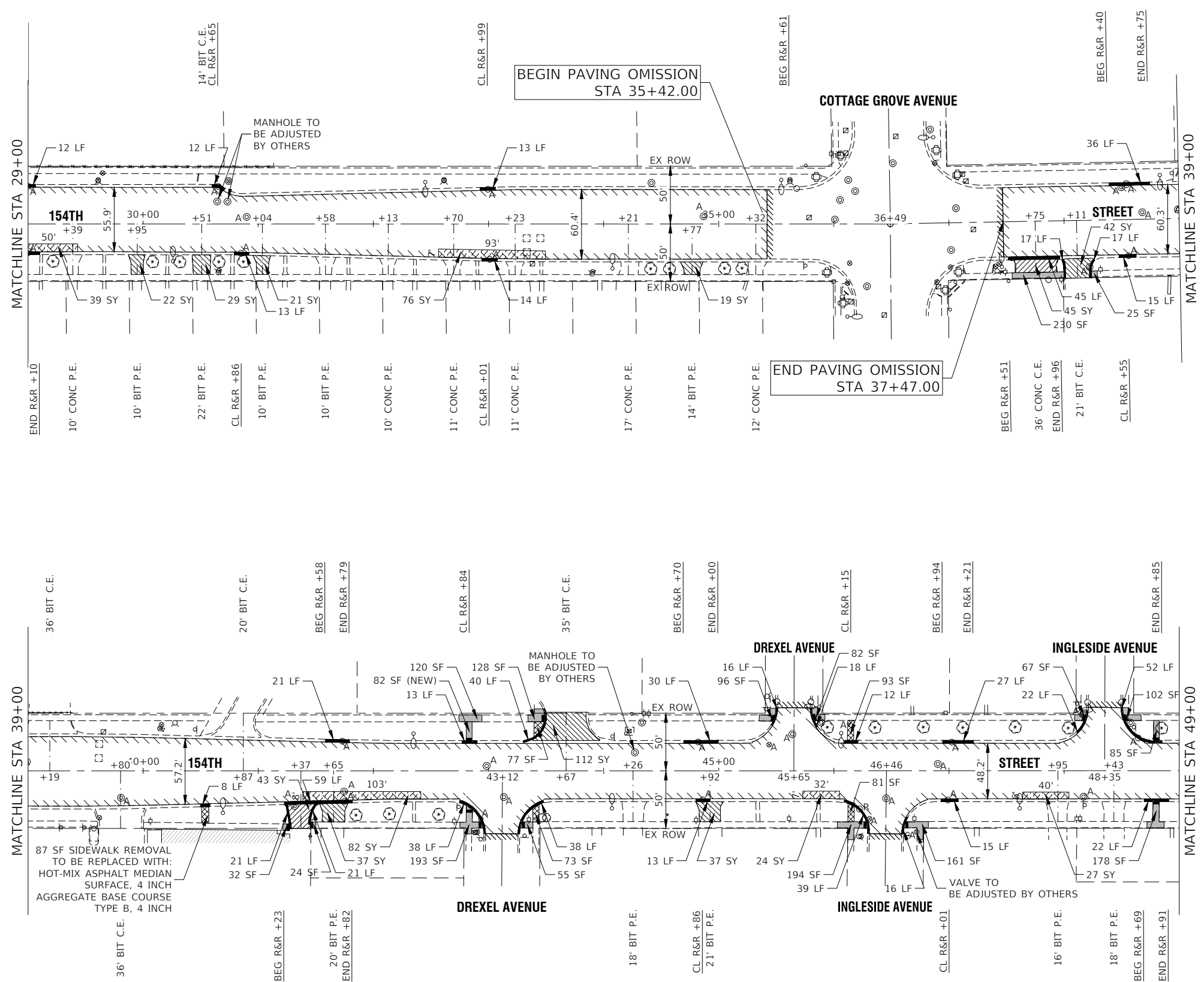
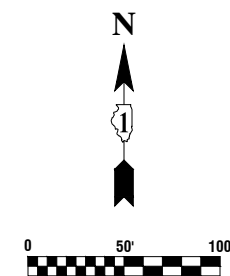
- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL, 4" HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4" HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
 - SIDEWALK REMOVAL
 - SIDEWALK REMOVAL PCC SIDEWALK 5 INCH (8 INCH THRU DRIVEWAYS) AGGREGATE BASE COURSE, TYPE B 4"
 - CLASS D PATCHES, 8 INCH REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (AS DIRECTED BY ENGINEER) AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY ENGINEER)
 - DRIVEWAY PAVEMENT REMOVAL HMA DRIVEWAY PAVEMENT, 8"
 - DRIVEWAY PAVEMENT REMOVAL PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8"
 - DETECTABLE WARNINGS - DW
 - COMBINATION CURB AND GUTTER REMOVAL COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12 CONCRETE CURB, TYPE B
 - "A" DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED VALVE BOXES TO BE ADJUSTED FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - "R" DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
 - EXISTING DETECTOR LOOP
 - HMA SURFACE REMOVAL - BUTT JOINT
 - BEG R&R +XX BEGIN STATION FOR CURB AND GUTTER REMOVAL AND REPLACEMENT
 - END R&R +XX END STATION OF CURB AND GUTTER REMOVAL AND REPLACEMENT
 - CL R&R +XX CENTER STATION OF CURB AND GUTTER REMOVAL AND REPLACEMENT (UNDER 20 LINEAL FEET)

NOTE:

ALL SIDEWALK RAMPS SHALL BE IN ACCORDANCE WITH THE HIGHWAY STANDARDS REFERENCED ON SHEET 2.

AREAS WHERE SIDEWALK IS TO BE REMOVED AND NOT REPLACED SHALL BE REPLACED WITH "TOPSOIL FURNISH AND PLACE, 4" AND "SODDING, SALT TOLERANT" UNLESS OTHERWISE NOTED.

FILE NAME = 23R0457-PLAN-01 - P01	USER NAME =	DESIGNED -- MPN	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	154TH STREET ROADWAY RESURFACING PROPOSED IMPROVEMENT PLAN			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	DRAWN -- RG	REVISED --		1607	23-00120-00-RS	COOK	32	11			
PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --	REVISED --	SCALE: 1"=50'	SHEET NO. 11 OF 32 SHEETS	STA.	TO STA.	CONTRACT NO. 61K47				
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				



LEGEND

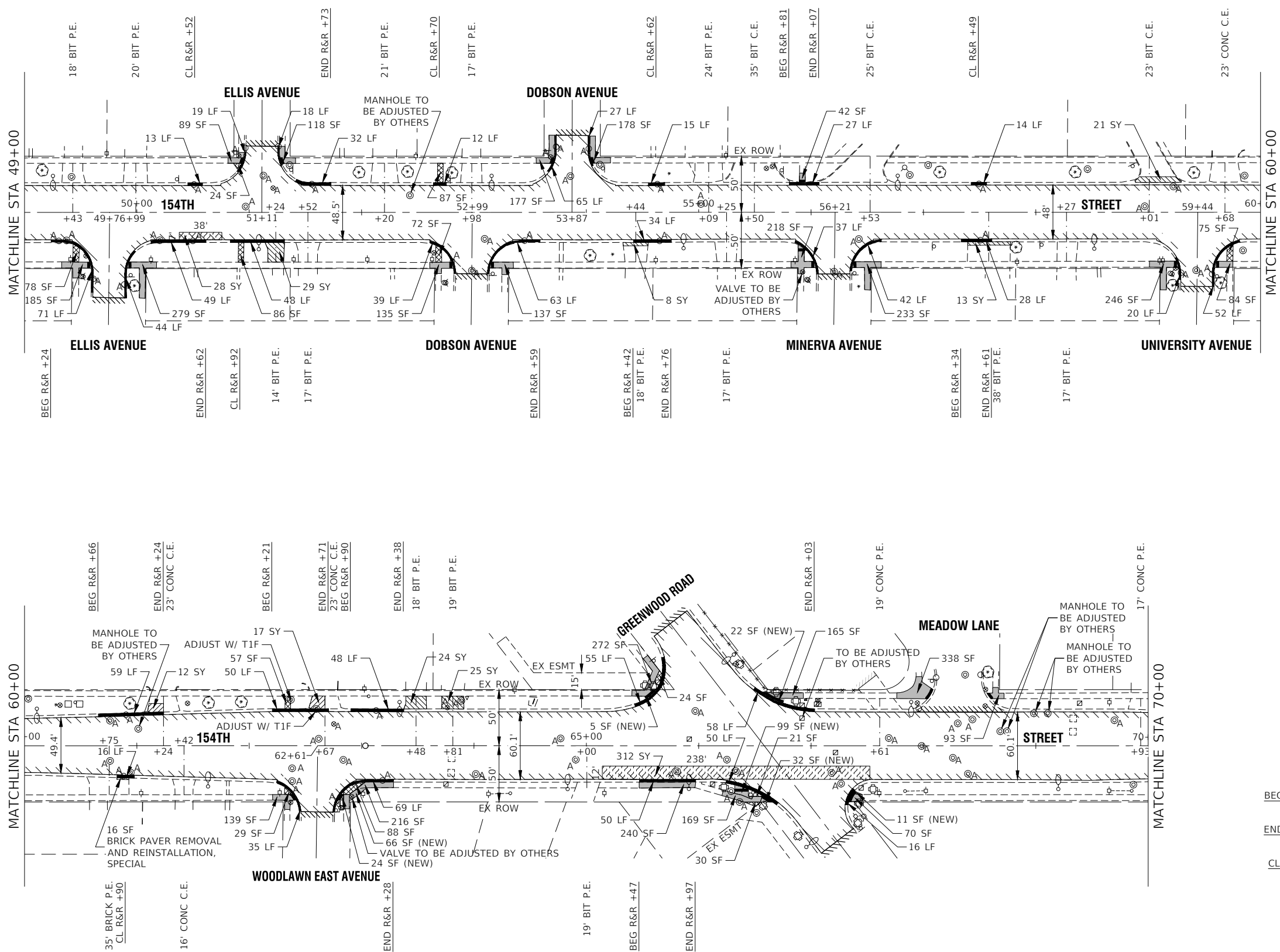
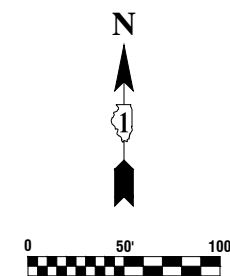
- HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4"
- HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
- SIDEWALK REMOVAL
- SIDEWALK REMOVAL
PCC SIDEWALK 5 INCH (8 INCH THRU DRIVEWAYS)
AGGREGATE BASE COURSE, TYPE B 4"
- CLASS D PATCHES, 8 INCH
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (AS DIRECTED BY ENGINEER)
AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY ENGINEER)
- DRIVEWAY PAVEMENT REMOVAL
HMA DRIVEWAY PAVEMENT, 8"
- DRIVEWAY PAVEMENT REMOVAL
PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8"
- DETECTABLE WARNINGS - DW
- COMBINATION CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12
CONCRETE CURB, TYPE B
- "A"
DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
VALVE BOXES TO BE ADJUSTED
FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- "R"
DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
- EXISTING DETECTOR LOOP
- HMA SURFACE REMOVAL - BUTT JOINT
- BEG R&R +XX
BEGIN STATION FOR CURB AND GUTTER REMOVAL AND REPLACEMENT
- END R&R +XX
END STATION OF CURB AND GUTTER REMOVAL AND REPLACEMENT
- CL R&R +XX
CENTER STATION OF CURB AND GUTTER REMOVAL AND REPLACEMENT (UNDER 20 LINEAL FEET)

NOTE:

ALL SIDEWALK RAMPS SHALL BE IN ACCORDANCE WITH THE HIGHWAY STANDARDS REFERENCED ON SHEET 2.

AREAS WHERE SIDEWALK IS TO BE REMOVED AND NOT REPLACED SHALL BE REPLACED WITH "TOPSOIL FURNISH AND PLACE, 4" AND "SODDING, SALT TOLERANT" UNLESS OTHERWISE NOTED.

FILE NAME = 23R0457-PLAN-01 - P02	USER NAME =	DESIGNED -- MPN	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	154TH STREET ROADWAY RESURFACING PROPOSED IMPROVEMENT PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
		CHECKED -- JDH	REVISED --		SCALE: 1"=50'	SHEET NO. 12	OF 32 SHEETS	STA.	TO STA.	1607	23-00120-00-RS	COOK	32	12
	PLOT SCALE =	DRAWN -- RG	REVISED --		CONTRACT NO. 61K47									
	PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)									



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4"
- HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
- SIDEWALK REMOVAL
- SIDEWALK REMOVAL
PCC SIDEWALK 5 INCH (8 INCH THRU DRIVEWAYS)
AGGREGATE BASE COURSE, TYPE B 4"
- CLASS D PATCHES, 8 INCH
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (AS DIRECTED BY ENGINEER)
AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY ENGINEER)
- DRIVEWAY PAVEMENT REMOVAL
HMA DRIVEWAY PAVEMENT, 8"
- DRIVEWAY PAVEMENT REMOVAL
PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8"
- DETECTABLE WARNINGS - DW
- COMBINATION CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12
CONCRETE CURB, TYPE B
- "A"
DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
VALVE BOXES TO BE ADJUSTED
FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- "R"
DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
- EXISTING DETECTOR LOOP
- HMA SURFACE REMOVAL - BUTT JOINT
- BEG R&R +XX
BEGIN STATION FOR CURB AND GUTTER REMOVAL AND REPLACEMENT
- END R&R +XX
END STATION OF CURB AND GUTTER REMOVAL AND REPLACEMENT
- CL R&R +XX
CENTER STATION OF CURB AND GUTTER REMOVAL AND REPLACEMENT (UNDER 20 LINEAL FEET)

NOTE:

ALL SIDEWALK RAMPS SHALL BE IN ACCORDANCE WITH THE HIGHWAY STANDARDS REFERENCED ON SHEET 2.

AREAS WHERE SIDEWALK IS TO BE REMOVED AND NOT REPLACED SHALL BE REPLACED WITH "TOPSOIL FURNISH AND PLACE, 4" AND "SODDING, SALT TOLERANT" UNLESS OTHERWISE NOTED.

FILE NAME = 23R0457-PLAN-01 - P03

USER NAME =	DESIGNED -- MPN	REVISED --
	CHECKED -- JDH	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --

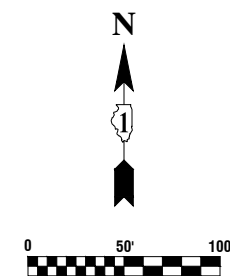
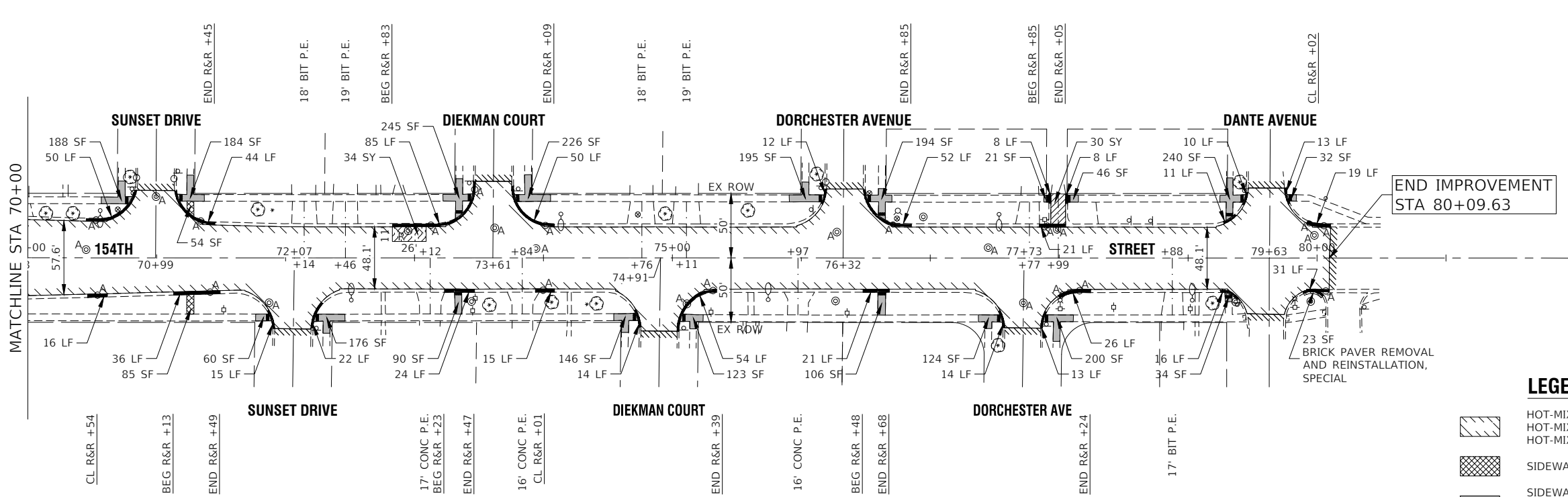
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**154TH STREET
ROADWAY RESURFACING
PROPOSED IMPROVEMENT PLAN**

SCALE: 1"=50'

SHEET NO. 13 OF 32 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	13
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	FX97(514)	



- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL, 4"
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/4"
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
 - SIDEWALK REMOVAL
 - SIDEWALK REMOVAL
PCC SIDEWALK 5 INCH (8 INCH THRU DRIVEWAYS)
AGGREGATE BASE COURSE, TYPE B 4"
 - CLASS D PATCHES, 8 INCH
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (AS DIRECTED BY ENGINEER)
AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY ENGINEER)
 - DRIVEWAY PAVEMENT REMOVAL
HMA DRIVEWAY PAVEMENT, 8"
 - DRIVEWAY PAVEMENT REMOVAL
PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8"
 - DETECTABLE WARNINGS - DW
 - COMBINATION CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12
CONCRETE CURB, TYPE B
 - "A" DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
VALVE BOXES TO BE ADJUSTED
FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - "R" DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED
 - EXISTING DETECTOR LOOP
 - HMA SURFACE REMOVAL - BUTT JOINT
 - BEG R&R +XX BEGIN STATION FOR CURB AND GUTTER REMOVAL AND REPLACEMENT
 - END R&R +XX END STATION OF CURB AND GUTTER REMOVAL AND REPLACEMENT
 - CL R&R +XX CENTER STATION OF CURB AND GUTTER REMOVAL AND REPLACEMENT (UNDER 20 LINEAL FEET)

NOTE:

ALL SIDEWALK RAMPS SHALL BE IN ACCORDANCE WITH THE HIGHWAY STANDARDS REFERENCED ON SHEET 2.

AREAS WHERE SIDEWALK IS TO BE REMOVED AND NOT REPLACED SHALL BE REPLACED WITH "TOPSOIL FURNISH AND PLACE, 4" AND "SODDING, SALT TOLERANT" UNLESS OTHERWISE NOTED.

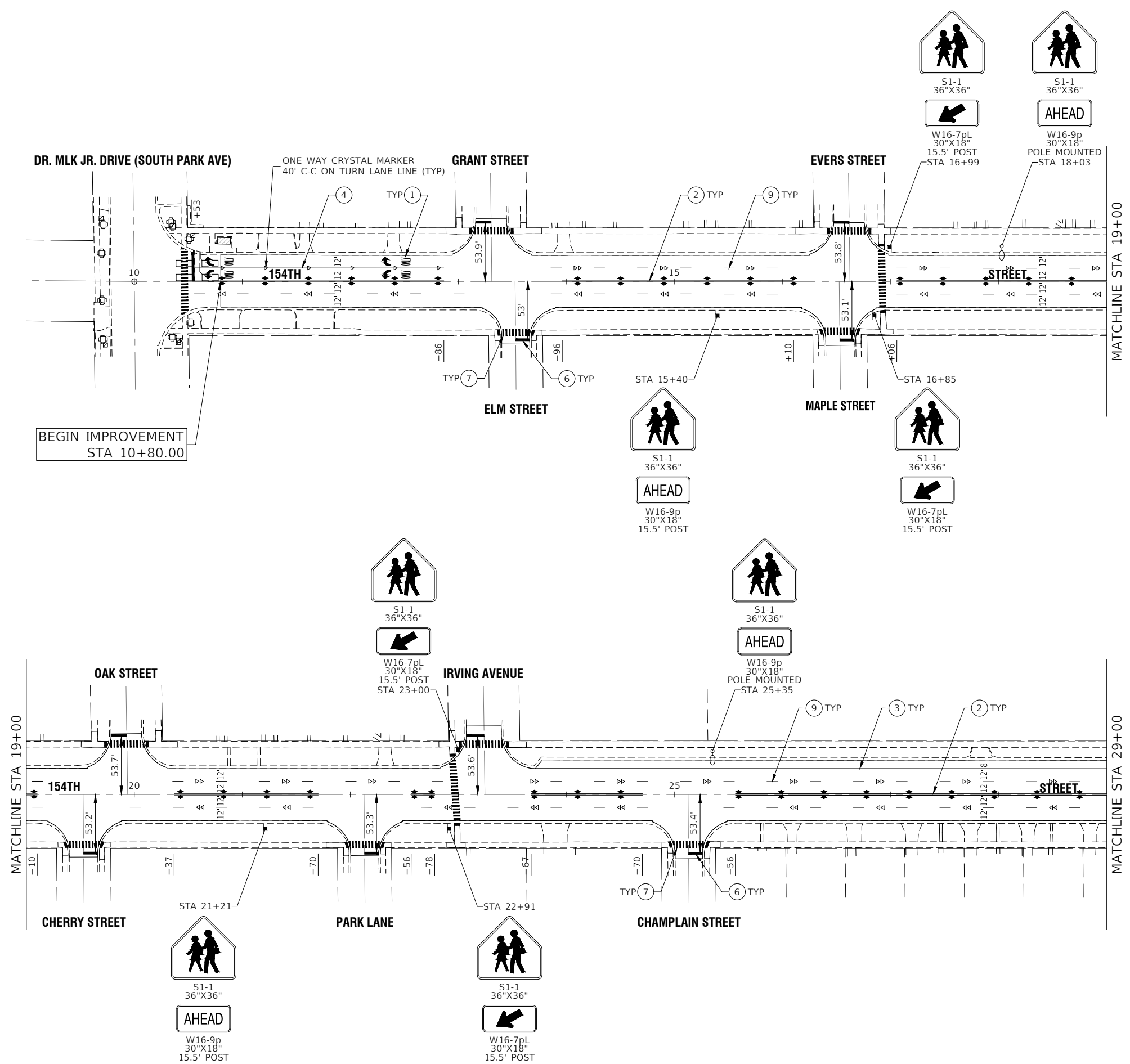
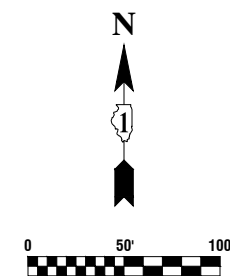
FILE NAME = 23R0457-PLAN-01 - P04	USER NAME =	DESIGNED -- MPN	REVISED --
		CHECKED -- JDH	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**154TH STREET
ROADWAY RESURFACING
PROPOSED IMPROVEMENT PLAN**

SCALE: 1"=50' SHEET NO. 14 OF 32 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	14
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				



PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - WHITE LETTERS & SYMBOLS
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" DOUBLE YELLOW LINE (11" C-C)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE 6" WHITE LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 6" WHITE SKIP DASH (2' LINE-6' SPACE)
- ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 24" WHITE STOP BAR LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE 12" WHITE CROSS-WALK (3' C-C)
- ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE 12" YELLOW DIAGONAL LINE (45° ANGLE, 20' C-C)
- ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE SKIP DASH (10' LINE-30' SPACE)
- ⑩ PAINT PAVEMENT MARKING CURB YELLOW
- EXISTING DETECTOR LOOP
- DETECTOR LOOP REPLACEMENT

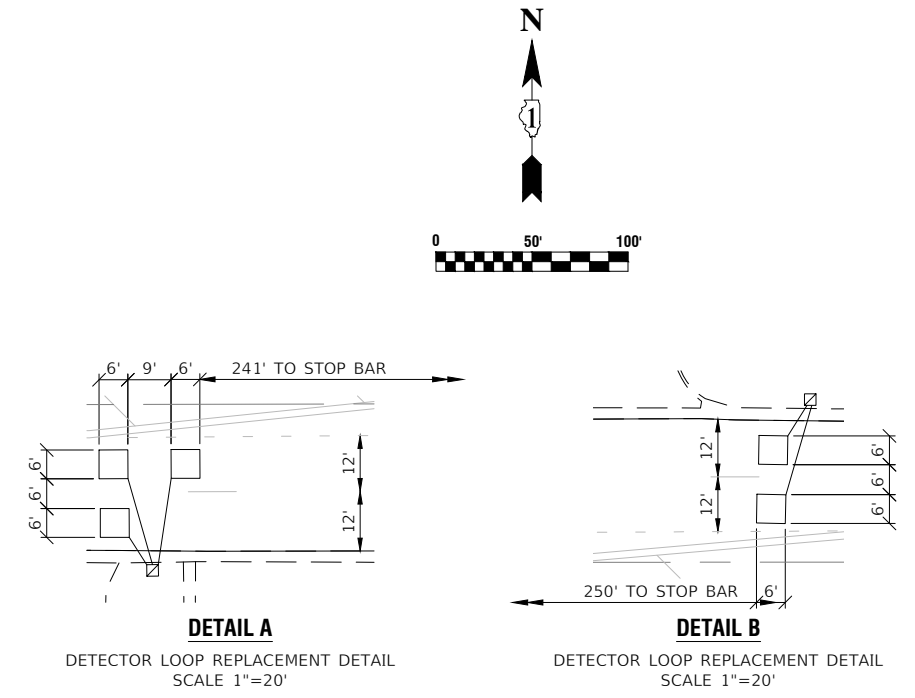
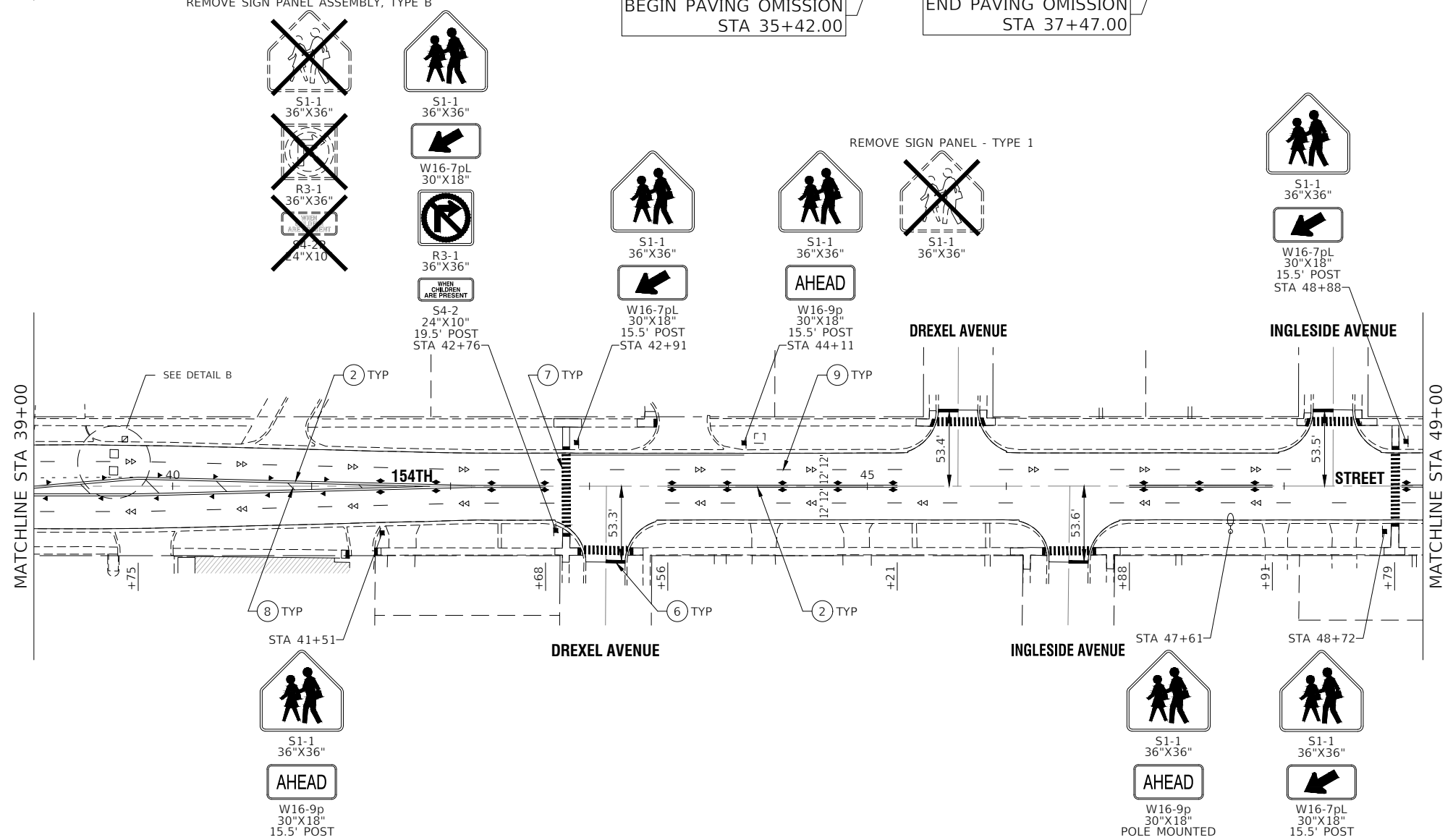
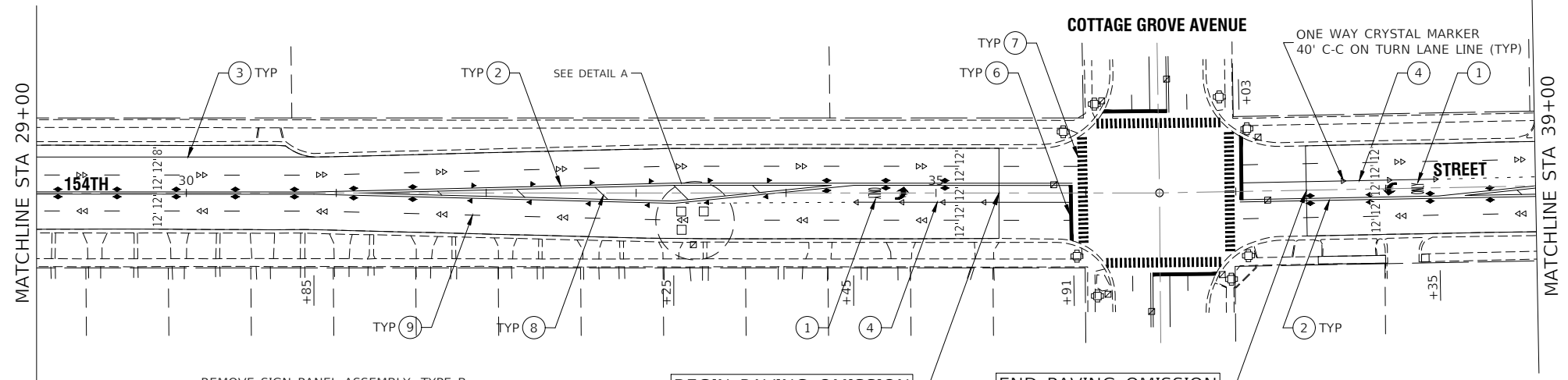
RAISED REFLECTIVE PAVEMENT MARKER

- ◀ ONE-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ◄ ONE-WAY CRYSTAL MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ◆ TWO-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED

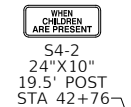
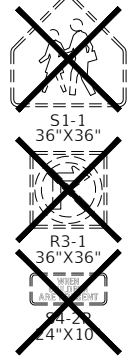
NOTES

1. SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE.
2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SHOWN.
3. PROPOSED SIGN POST TYPE SHALL BE TELESCOPING STEEL SIGN SUPPORT.

FILE NAME = 23R0457-PVMK-01 - P01	USER NAME =	DESIGNED -- MPN	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	154TH STREET ROADWAY RESURFACING PAVEMENT MARKING & SIGNING	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		CHECKED -- JDH	REVISED --		SCALE: 1"=50'	SHEET NO. 15 OF 32 SHEETS	1607	23-00120-00-RS	COOK	32	15
	PLOT SCALE =	DRAWN -- RG	REVISED --		STA.	TO STA.	CONTRACT NO. 61K47				
	PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)						



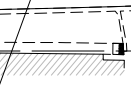
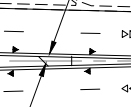
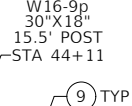
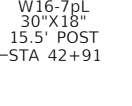
REMOVE SIGN PANEL ASSEMBLY, TYPE B



BEGIN PAVING OMISSION
STA 35+42.00

END PAVING OMISSION
STA 37+47.00

REMOVE SIGN PANEL - TYPE 1



DREXEL AVENUE

INGLESIDE AVENUE

INGLESIDE AVENUE

PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - WHITE LETTERS & SYMBOLS
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" DOUBLE YELLOW LINE (11" C-C)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE 6" WHITE LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 6" WHITE SKIP DASH (2' LINE-6' SPACE)
- ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 24" WHITE STOP BAR LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE 12" WHITE CROSS-WALK (3' C-C)
- ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE 12" YELLOW DIAGONAL LINE (45° ANGLE, 20' C-C)
- ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE SKIP DASH (10' LINE-30' SPACE)
- ⑩ PAINT PAVEMENT MARKING CURB YELLOW
- [] EXISTING DETECTOR LOOP
- [] DETECTOR LOOP REPLACEMENT

RAISED REFLECTIVE PAVEMENT MARKER

- ▲ ONE-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ◄ ONE-WAY CRYSTAL MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ◆ TWO-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED

NOTES

1. SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE.
2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SHOWN.
3. PROPOSED SIGN POST TYPE SHALL BE TELESCOPING STEEL SIGN SUPPORT.

FILE NAME = 23R0457-PVMK-01 - P02

USER NAME =	DESIGNED -- MPN	REVISED --
	CHECKED -- JDH	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 04-02-24	CHECKED -- AG	REVISED --

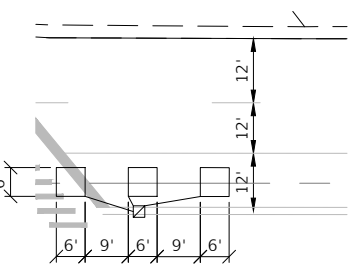
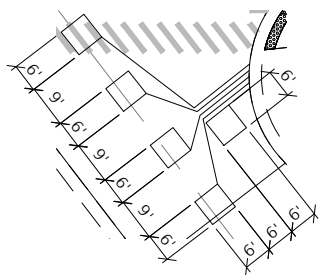
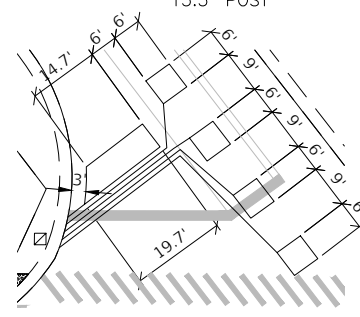
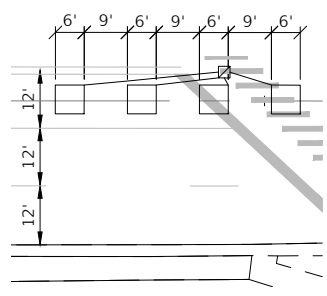
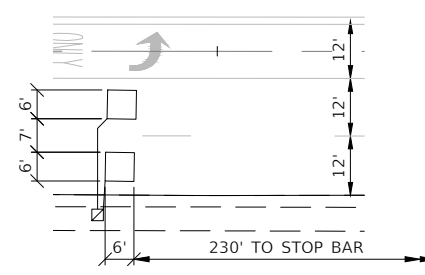
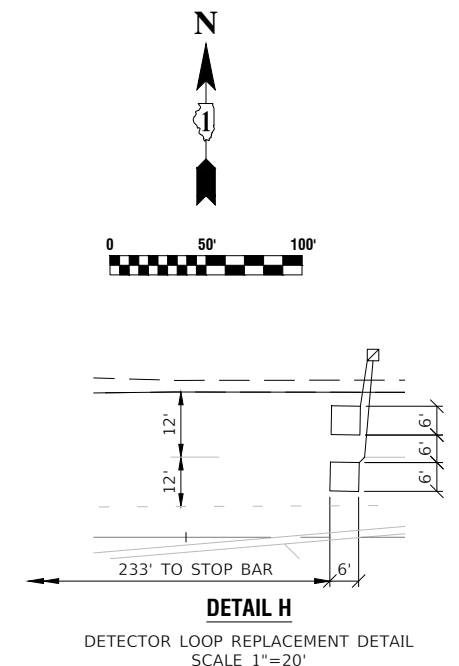
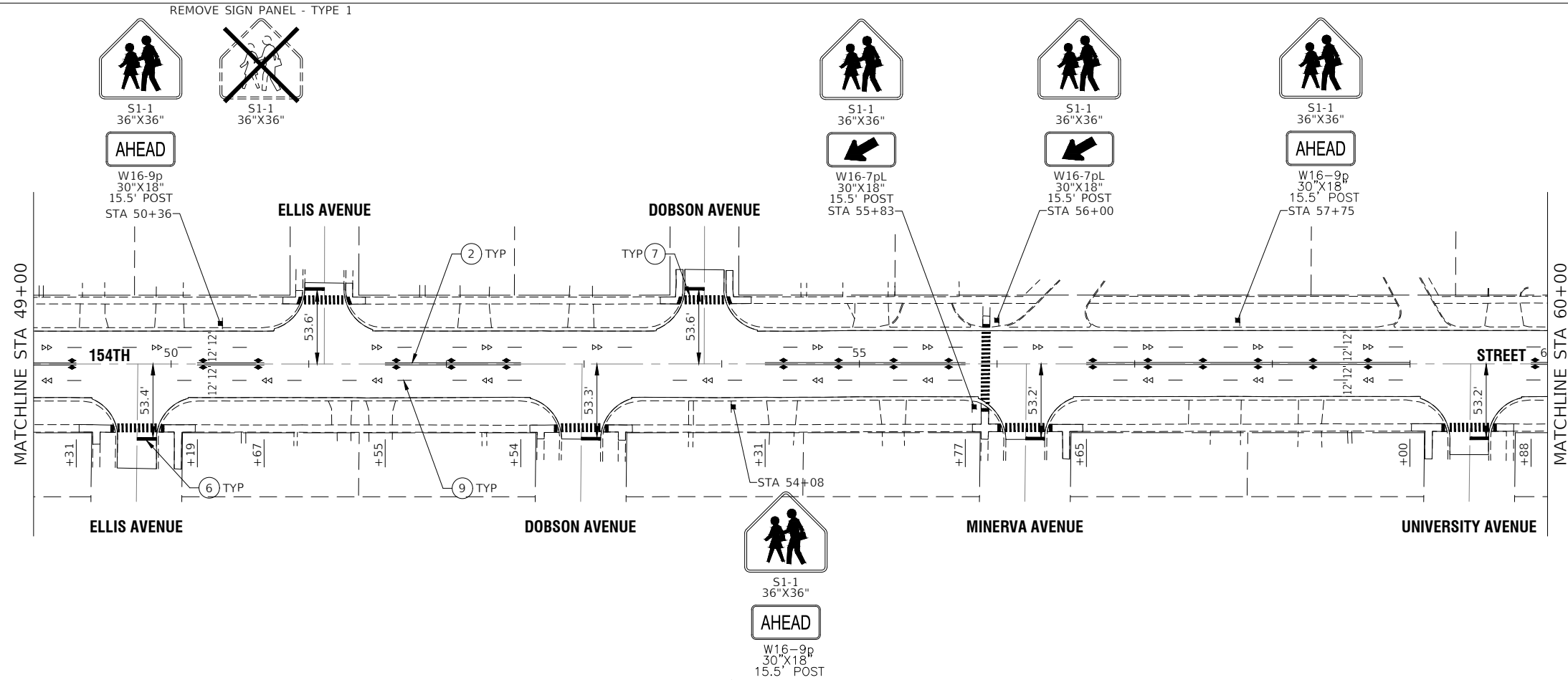
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

154TH STREET
ROADWAY RESURFACING
PAVEMENT MARKING & SIGNING

SCALE: 1"=50' SHEET NO. 16 OF 32 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	16
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				

REMOVE SIGN PANEL - TYPE 1



PAVEMENT MARKING LEGEND

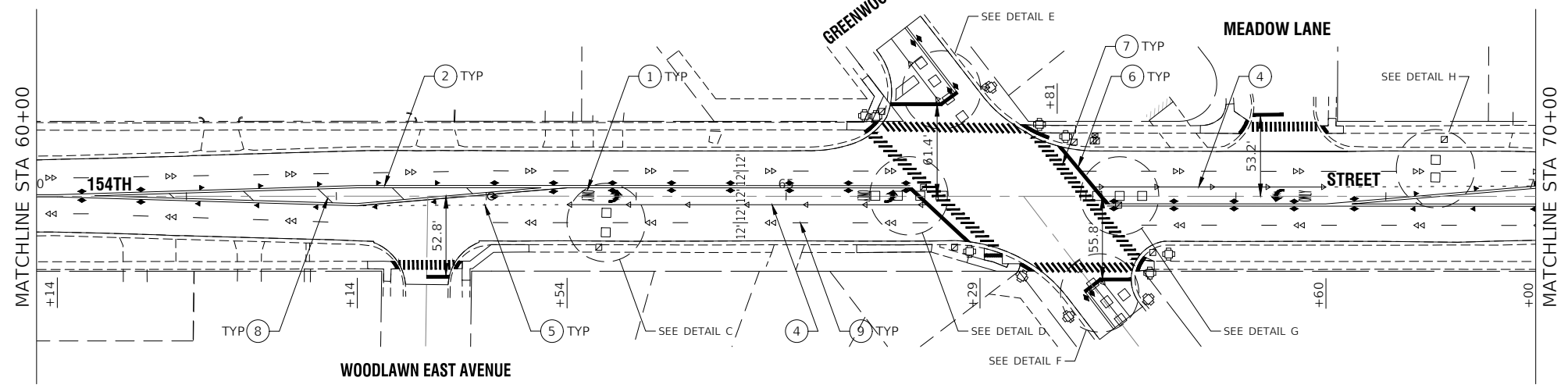
- ① THERMOPLASTIC PAVEMENT MARKING - WHITE LETTERS & SYMBOLS
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" DOUBLE YELLOW LINE (11" C-C)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
 - ④ THERMOPLASTIC PAVEMENT MARKING - LINE 6" WHITE LANE LINE
 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 6" WHITE SKIP DASH (2' LINE-6' SPACE)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 24" WHITE STOP BAR LINE
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE 12" WHITE CROSS-WALK (3' C-C)
 - ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE 12" YELLOW DIAGONAL LINE (45° ANGLE, 20' C-C)
 - ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE SKIP DASH (10' LINE-30' SPACE)
 - ⑩ PAINT PAVEMENT MARKING CURB YELLOW
- EXISTING DETECTOR LOOP
 DETECTOR LOOP REPLACEMENT

RAISED REFLECTIVE PAVEMENT MARKER

- ▲ ONE-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ▲ ONE-WAY CRYSTAL MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ◆ TWO-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED

NOTES

1. SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE.
2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SHOWN.
3. PROPOSED SIGN POST TYPE SHALL BE TELESCOPING STEEL SIGN SUPPORT.



FILE NAME = 23R0457-PVMK-01 - P03

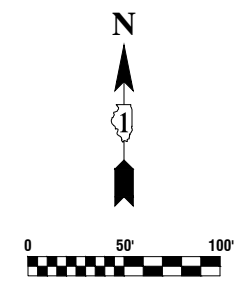
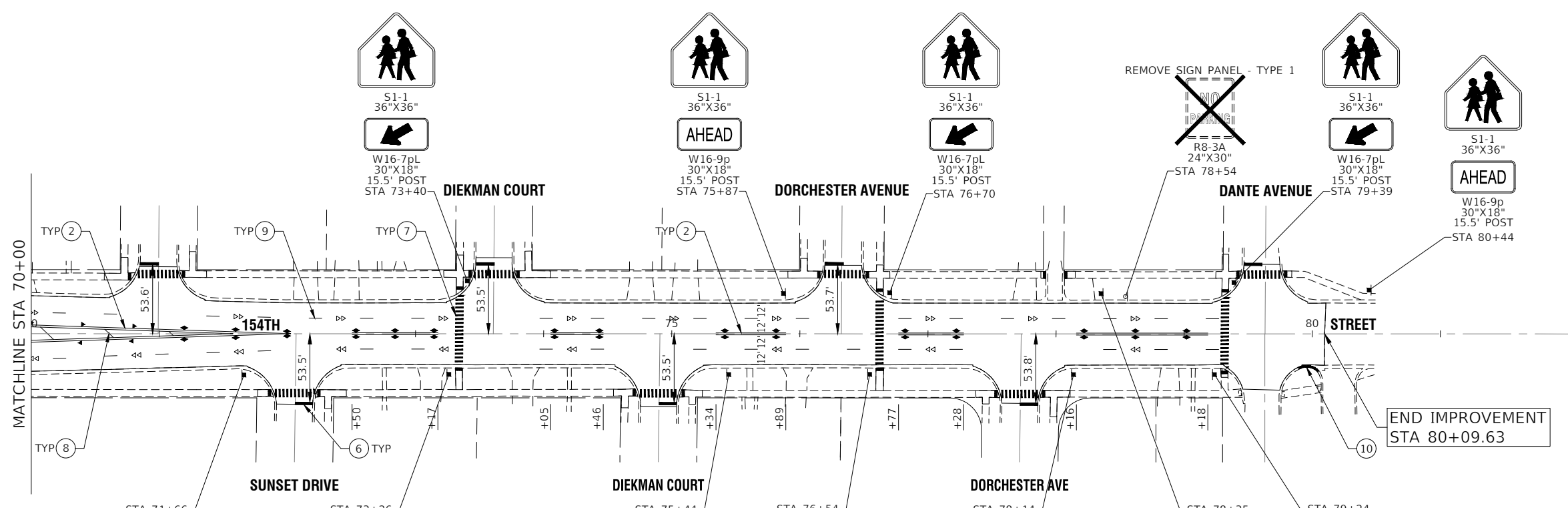
USER NAME =	DESIGNED -- MPN	REVISED --
PLOT SCALE =	CHECKED -- JDH	REVISED --
PLOT DATE = 04-02-24	DRAWN -- RG	REVISED --
	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

154TH STREET
ROADWAY RESURFACING
PAVEMENT MARKING & SIGNING

SCALE: 1"=50' SHEET NO. 17 OF 32 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	17
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				



PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - WHITE LETTERS & SYMBOLS
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" DOUBLE YELLOW LINE (11" C-C)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE 6" WHITE LANE LINE
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 6" WHITE SKIP DASH (2' LINE-6' SPACE)
- ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 24" WHITE STOP BAR LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING - LINE 12" WHITE CROSS-WALK (3' C-C)
- ⑧ THERMOPLASTIC PAVEMENT MARKING - LINE 12" YELLOW DIAGONAL LINE (45° ANGLE, 20' C-C)
- ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE SKIP DASH (10' LINE-30' SPACE)
- ⑩ PAINT PAVEMENT MARKING CURB YELLOW
- EXISTING DETECTOR LOOP
- DETECTOR LOOP REPLACEMENT

RAISED REFLECTIVE PAVEMENT MARKER

- ◀ ONE-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ◀ ONE-WAY CRYSTAL MARKER
40' C/C UNLESS OTHERWISE INDICATED
- ◆ TWO-WAY AMBER MARKER
40' C/C UNLESS OTHERWISE INDICATED

NOTES

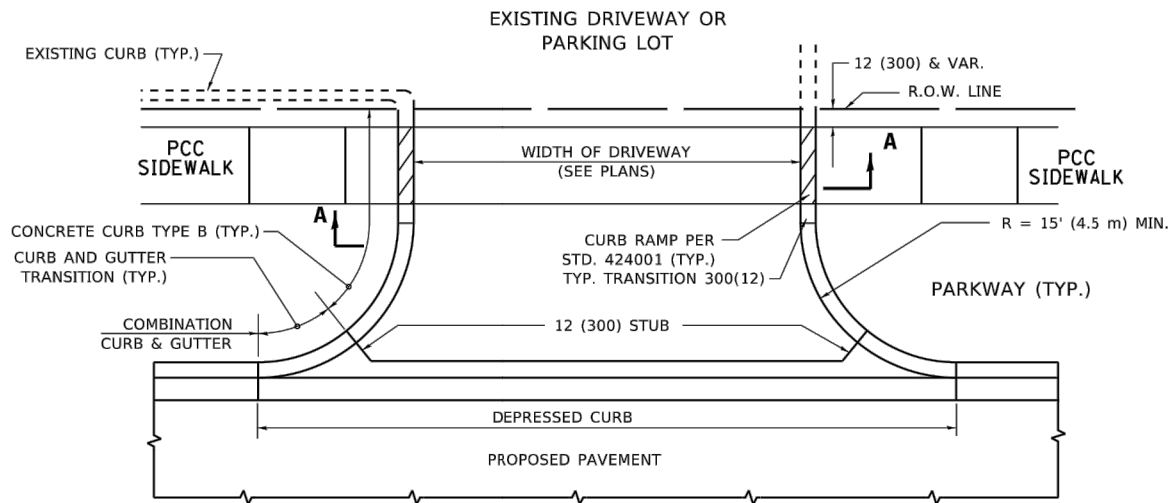
1. SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE.
2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE SHOWN.
3. PROPOSED SIGN POST TYPE SHALL BE TELESCOPING STEEL SIGN SUPPORT.

FILE NAME = 23R0457-PVMK-01 - P04	USER NAME =	DESIGNED -- MPN	REVISED --
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	PLOT SCALE =	DRAWN -- RG	REVISED --
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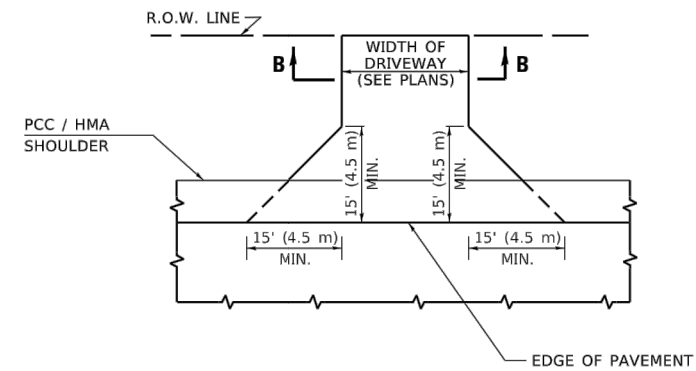
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

154TH STREET ROADWAY RESURFACING PAVEMENT MARKING & SIGNING			
SCALE: 1"=50'	SHEET NO. 18 OF 32 SHEETS	STA. TO STA.	

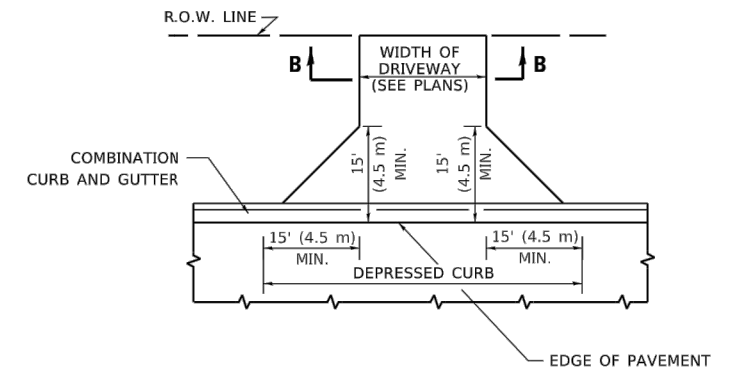
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	18
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				



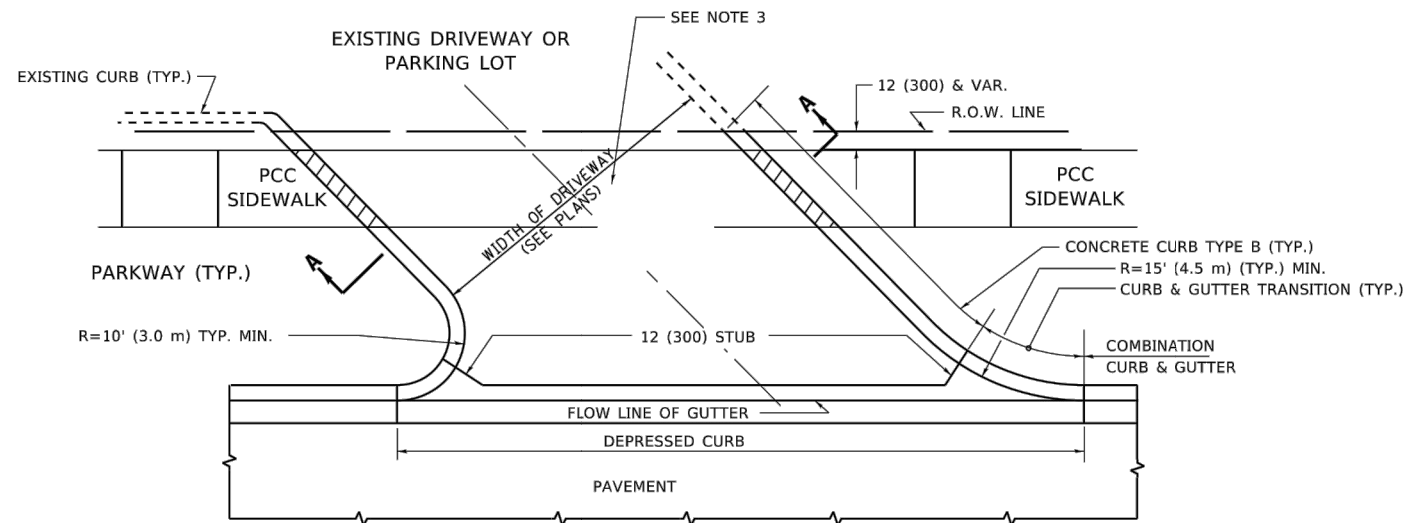
WITH CONCRETE CURB, TYPE B



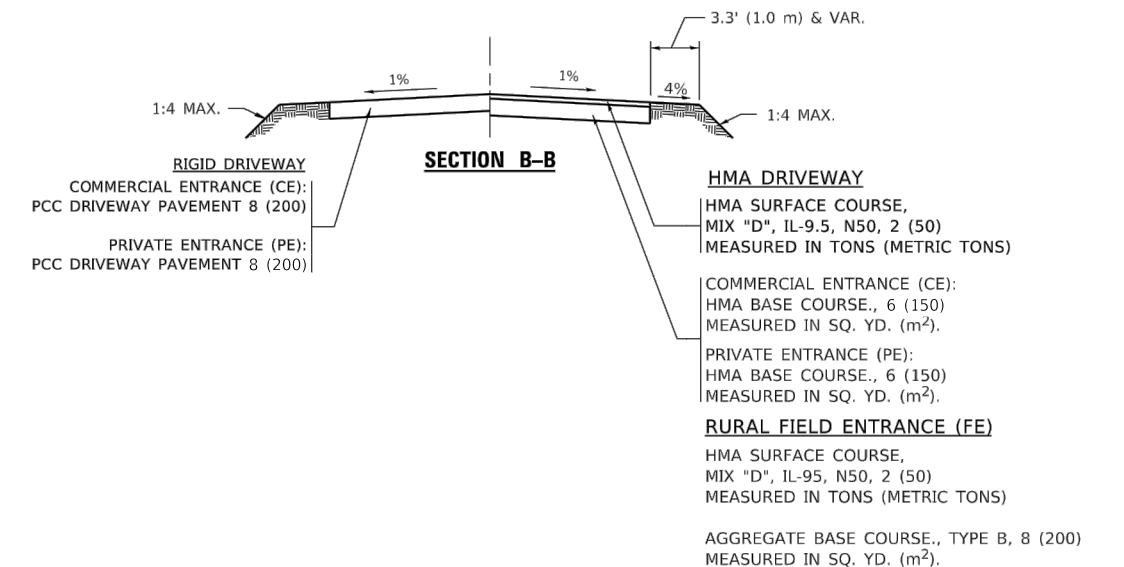
ADJACENT TO PCC /HMA SHOULDER



ADJACENT TO CURB AND GUTTER

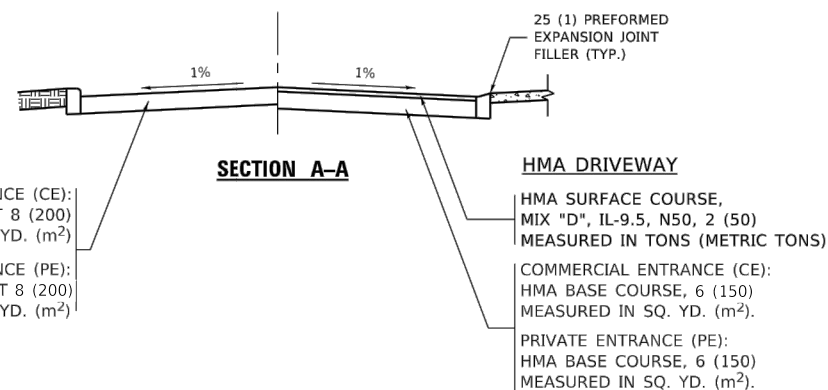


WITH CONCRETE CURB, TYPE B



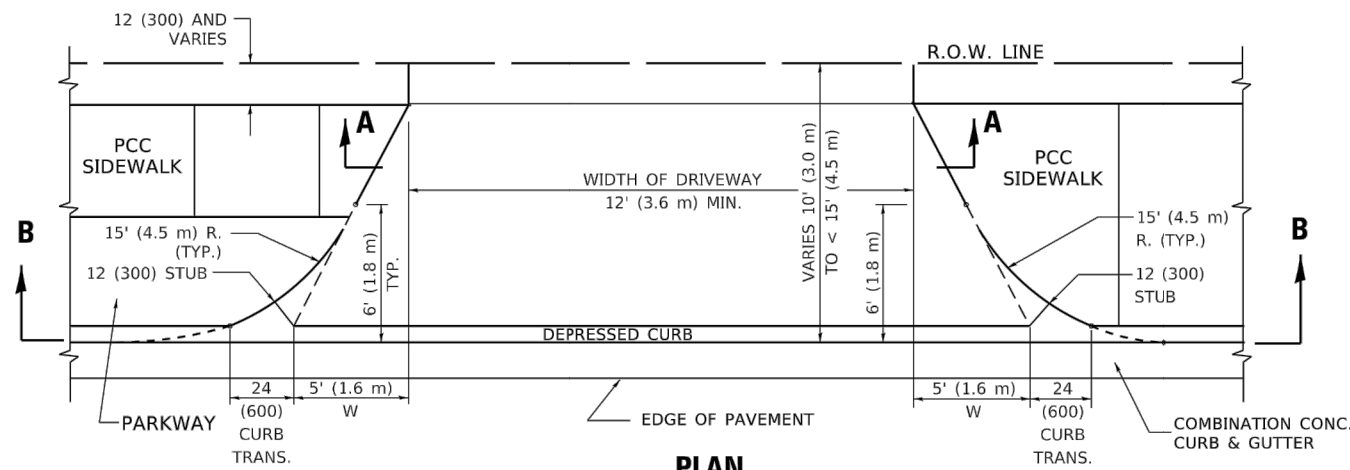
GENERAL NOTES

- DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.
- COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

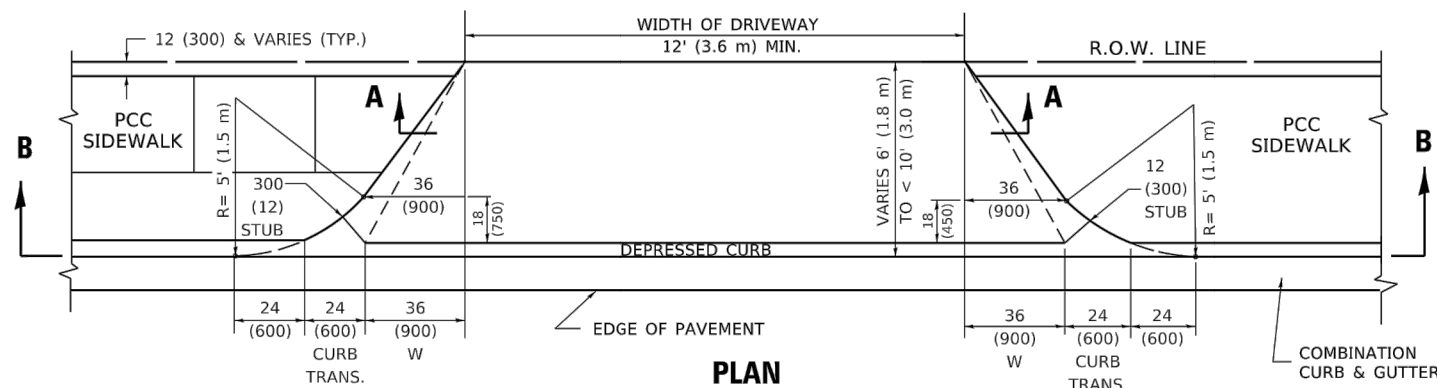


ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

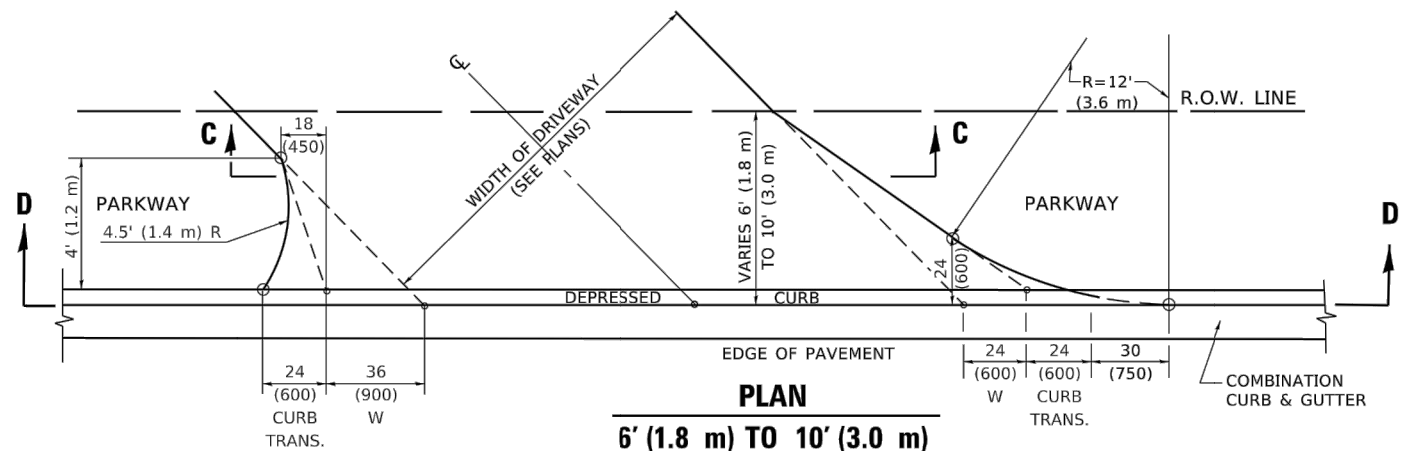
FILE NAME = 23R0457-DTLS-02 - P01	USER NAME =	DESIGNED --	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	154TH STREET ROADWAY RESURFACING CONSTRUCTION DETAILS - BD-01 (MODIFIED)		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED --	REVISED --		1607	23-00120-00-RS	COOK	32	19		
	PLOT SCALE =	DRAWN --	REVISED --		CONTRACT NO. 61K47						
	PLOT DATE =	CHECKED --	REVISED --		SCALE:	SHEET NO. 19 OF 32 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT FX97(514)	



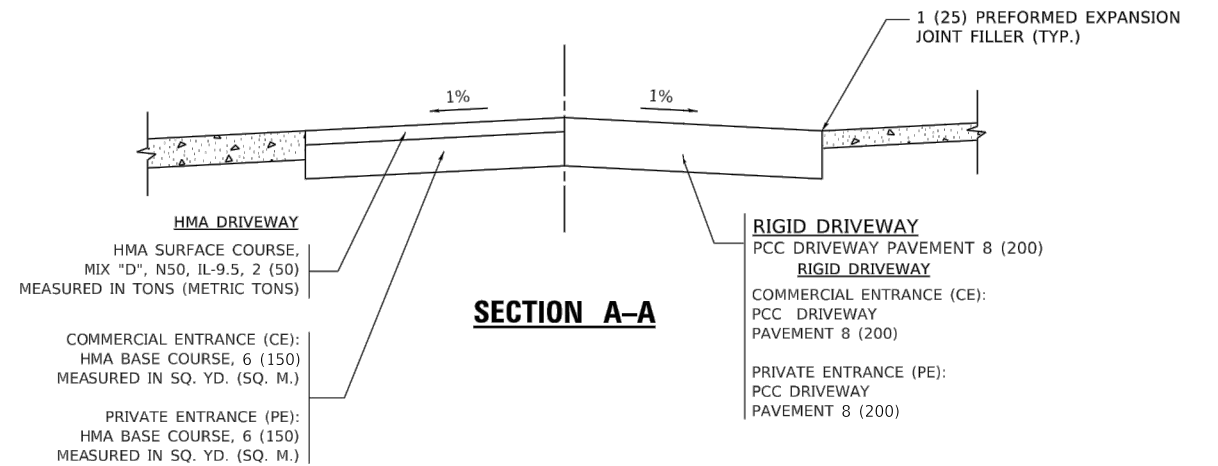
PLAN
10' (3.0 m) TO < 15' (4.5 m)



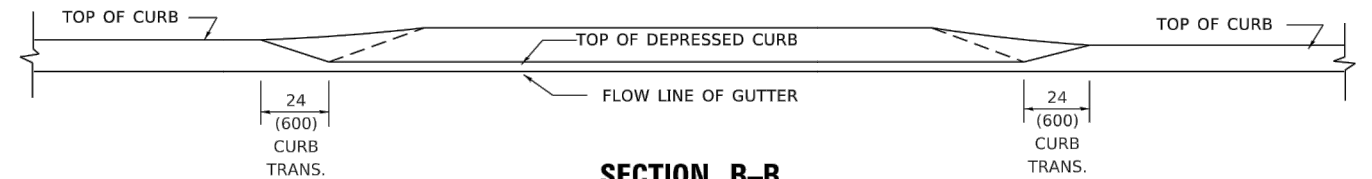
PLAN
6' (1.8 m) TO < 10' (3.0 m)



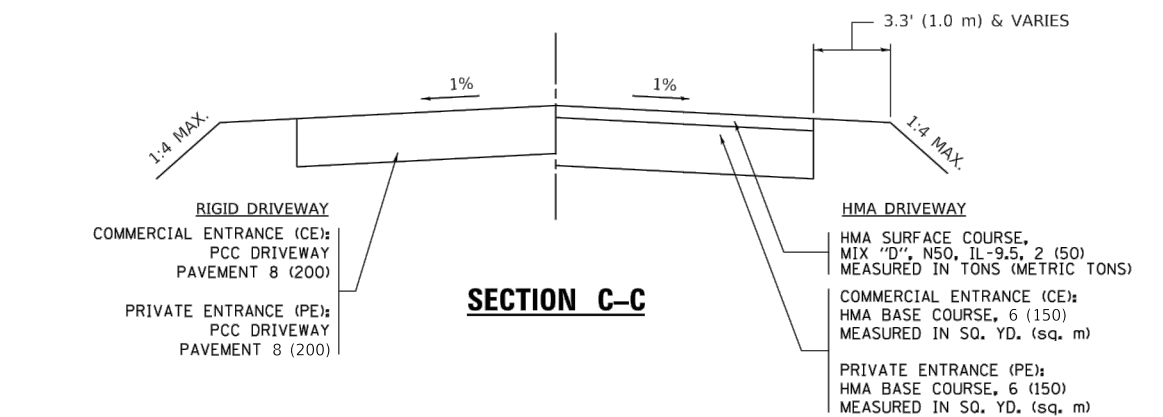
PLAN
6' (1.8 m) TO 10' (3.0 m)



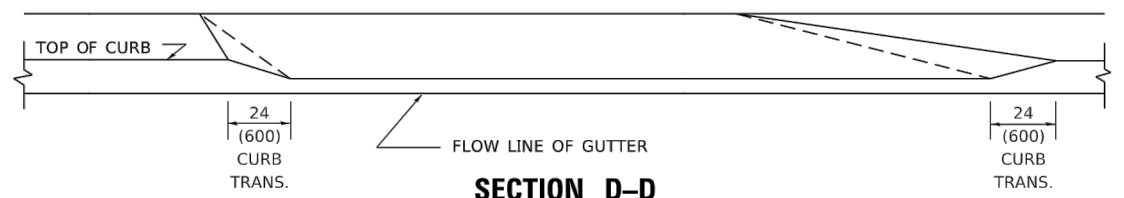
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

- DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.
- WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE PCC SIDEWALK SHALL EXTEND TO THE BACK OF CURB.
- "W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME = 23R0457-DTLS-02 - P02

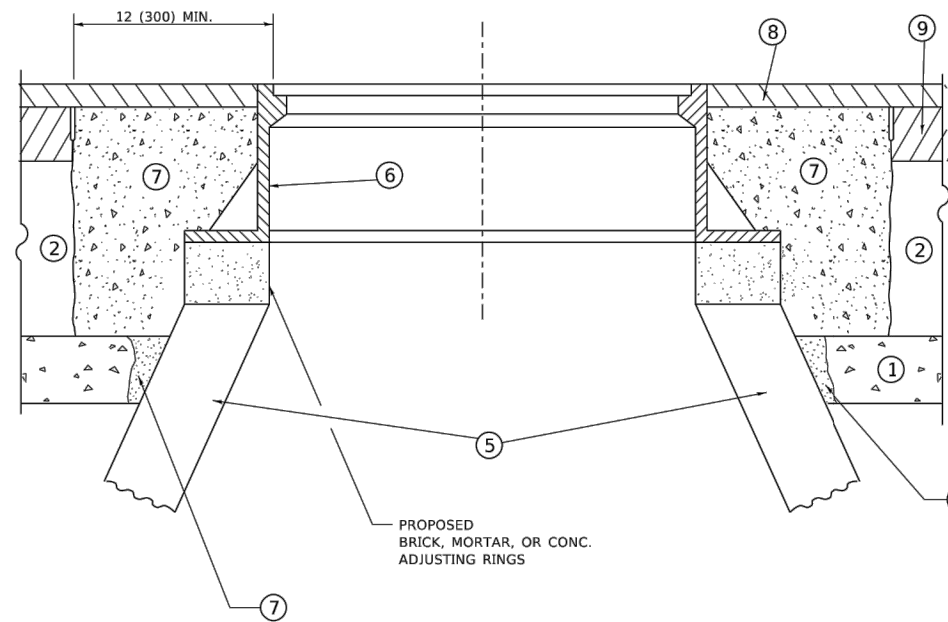
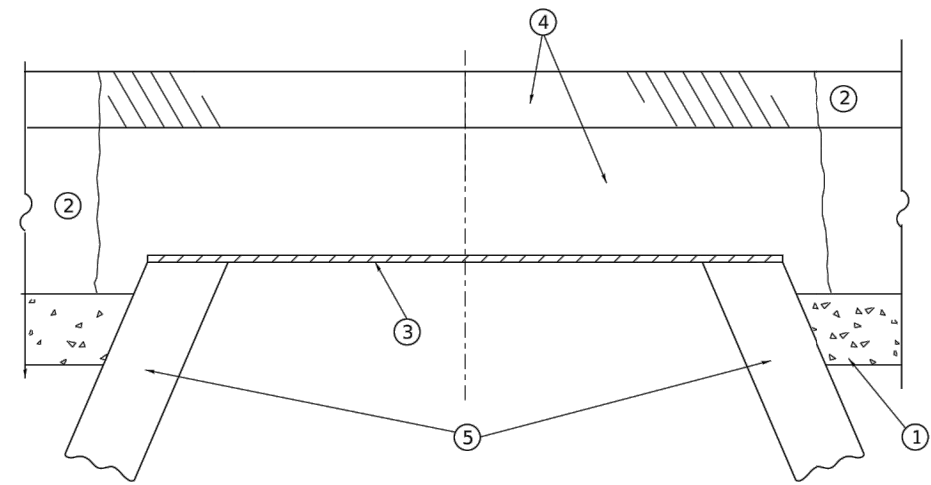
USER NAME =	DESIGNED --	REVISED --
	CHECKED --	REVISED --
PLOT SCALE =	DRAWN --	REVISED --
PLOT DATE =	CHECKED --	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

154TH STREET
ROADWAY RESURFACING
CONSTRUCTION DETAILS - BD-02 (MODIFIED)

SCALE: SHEET NO. 20 OF 32 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	20
CONTRACT NO. 61K47				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)				



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

NOTES

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

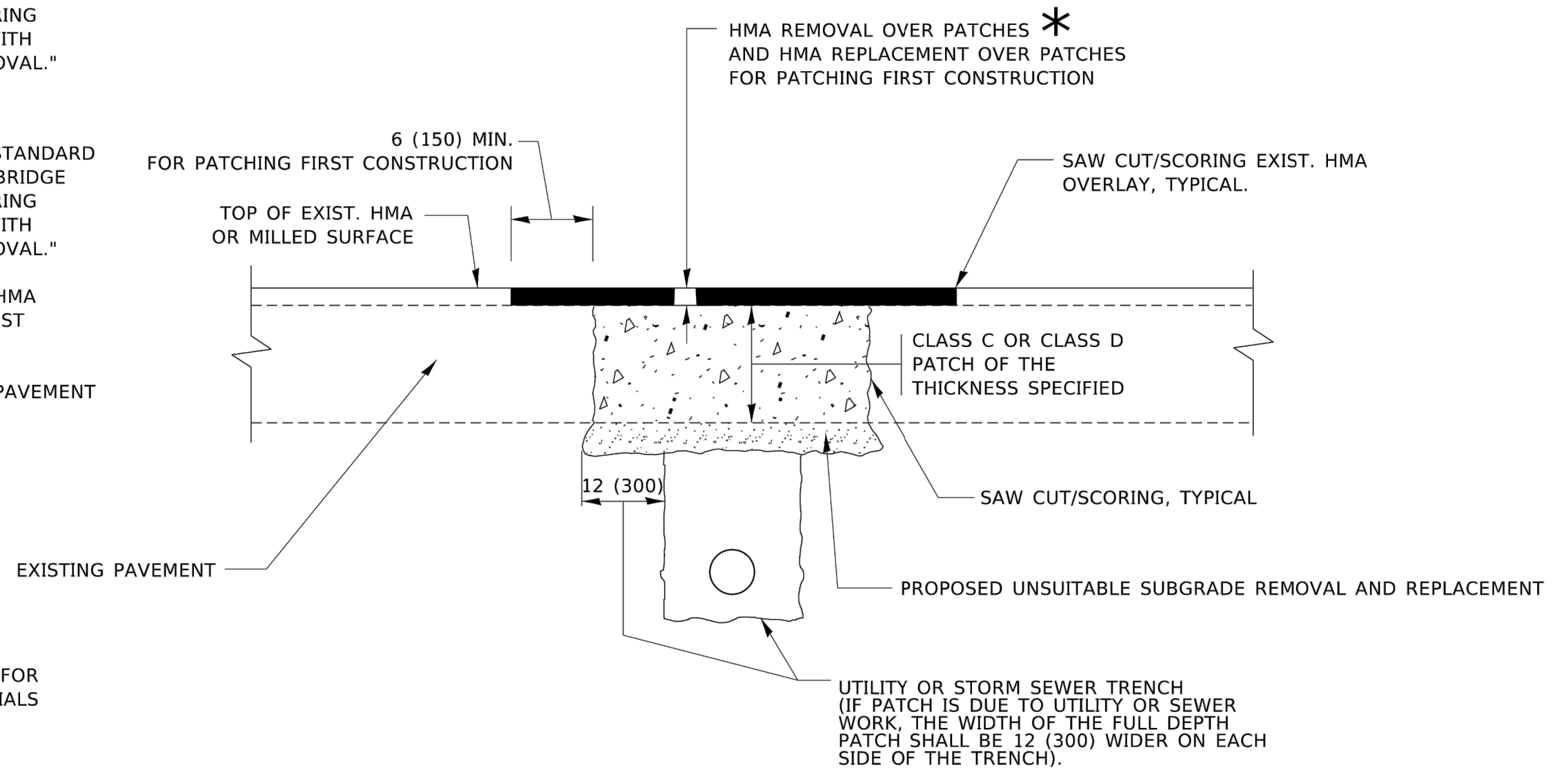
USER NAME = Lawrence.DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 03-09-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED - R. BORO 12-06-11		SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	1607	23-00120-00-RS	COOK	32	21
PLOT DATE = 9/15/2023	CHECKED -	REVISED - K. SMITH 11-18-22				BD600-03 (BD-08)		CONTRACT NO. 61K47			
	DATE - 10-25-94	REVISED - K. SMITH 09-15-23						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)			

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

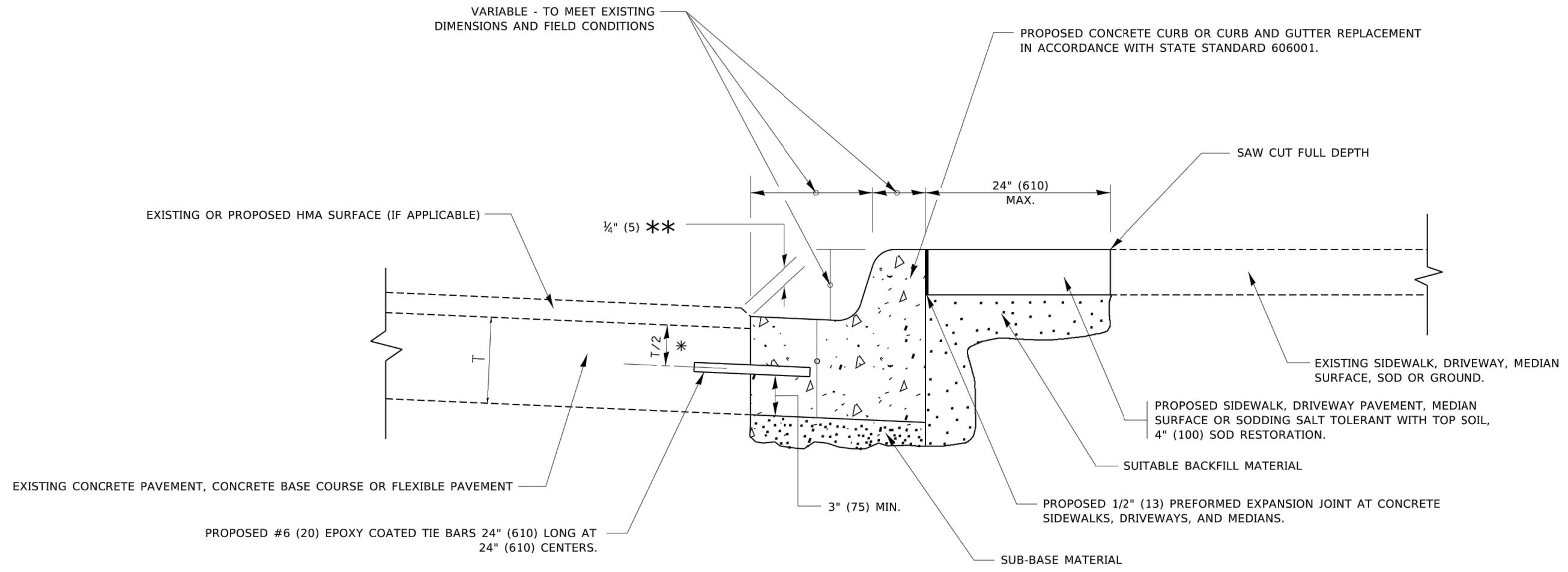
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = Lawrence.DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	DRAWN -	REVISED - R. BORO 09-04-07			1607	23-00120-00-RS	COOK	32	22	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - K. ENG 10-27-08			BD400-04 (BD-22)		CONTRACT NO. 61K47			
PLOT DATE = 11/18/2022	DATE - 10-25-94	REVISED - K. SMITH 11-18-22			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS

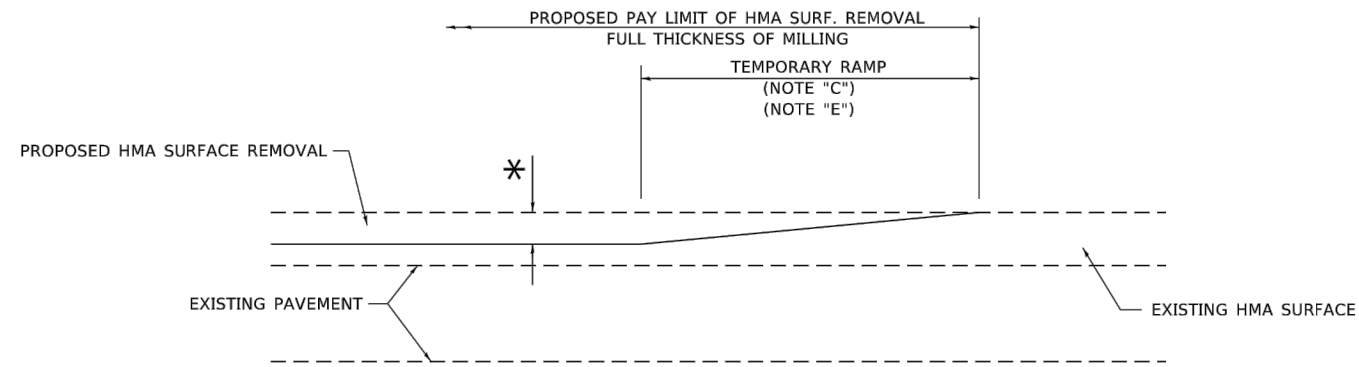


- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

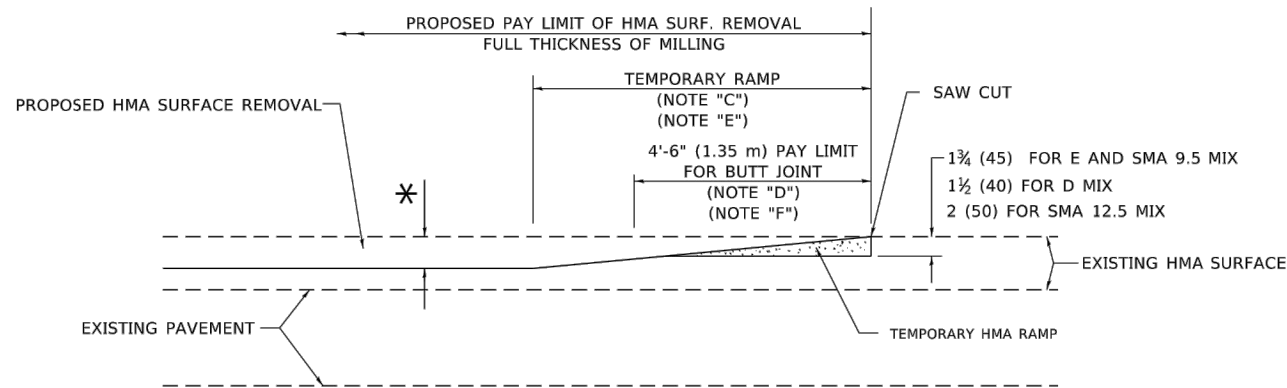
USER NAME = footemj PLOT SCALE = 50.0000 ' / in. PLOT DATE = 7/11/2019	DESIGNED - A. HOUSEH DRAWN - CHECKED - DATE - 03-11-94	REVISED - A. ABBAS 03-21-97 REVISED - M. GOMEZ 01-22-01 REVISED - R. BORO 12-15-09 REVISED - K. SMITH 07-11-19	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE. 1607 SECTION 23-00120-00-RS BD600-06 (BD-24) FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT FX97(514)	COUNTY COOK TOTAL SHEETS 32 SHEET NO. 23 CONTRACT NO. 61K47
SCALE: NONE				SHEET 1 OF 1 SHEETS	STA. TO STA.	



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

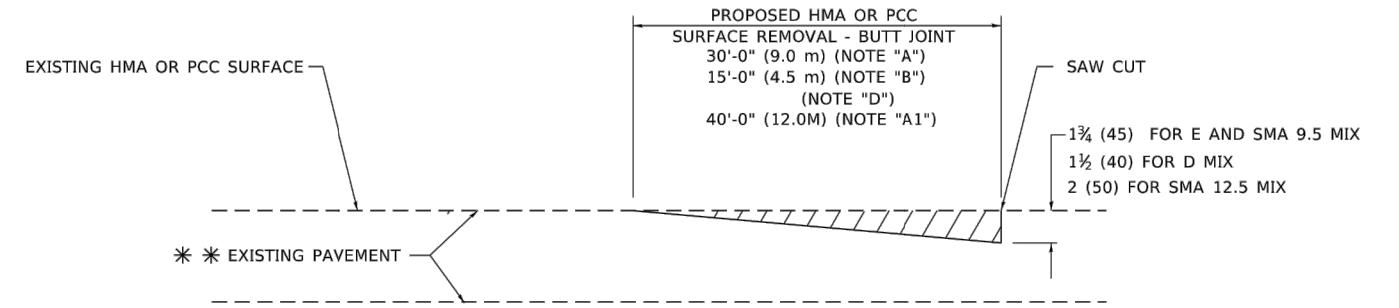


HMA CONSTRUCTED TEMPORARY RAMP

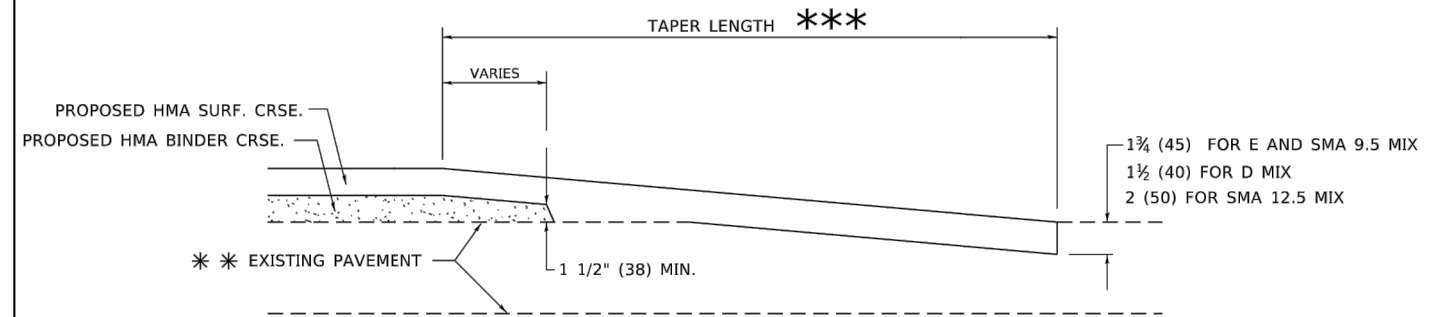
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

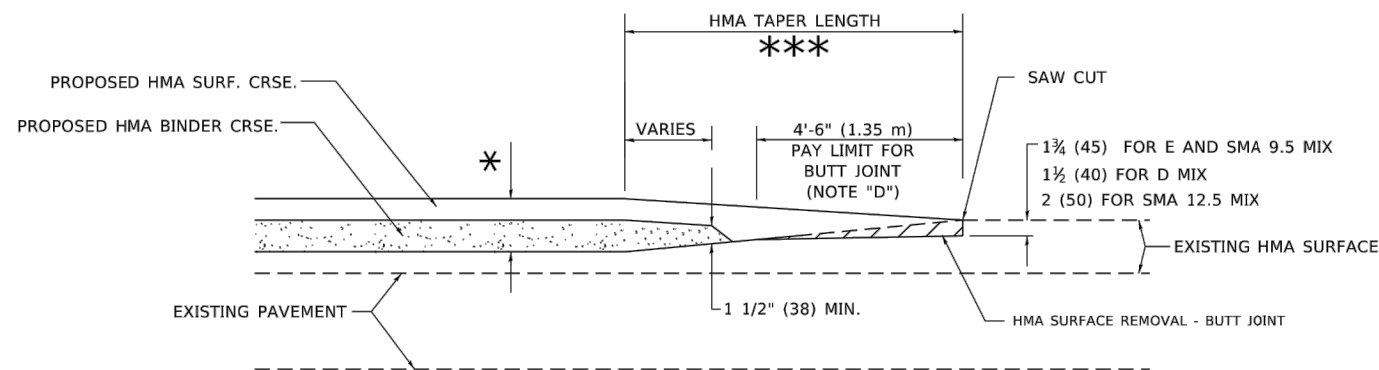
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

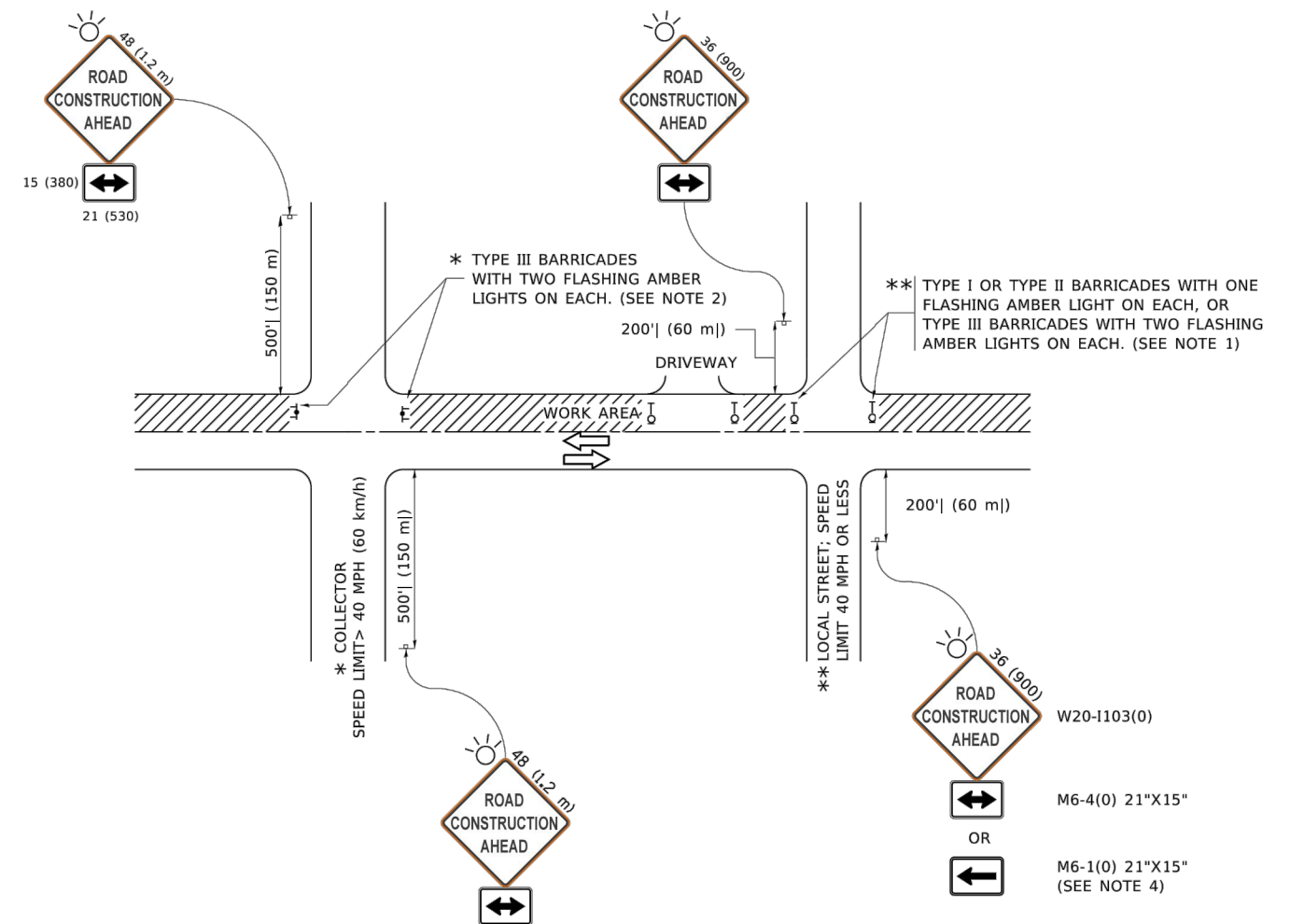
USER NAME = Lawrence.DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	24
BD400-05 BD-32		CONTRACT NO. 61K47		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT FX97(614)		



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

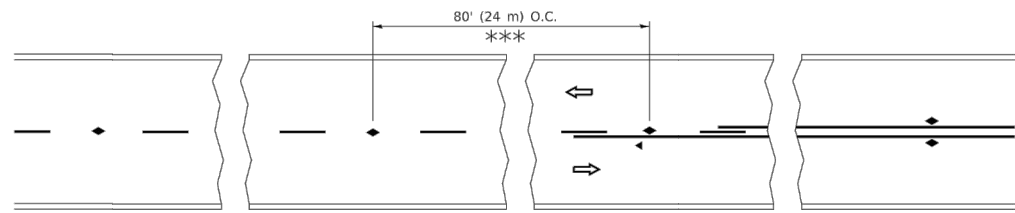
USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

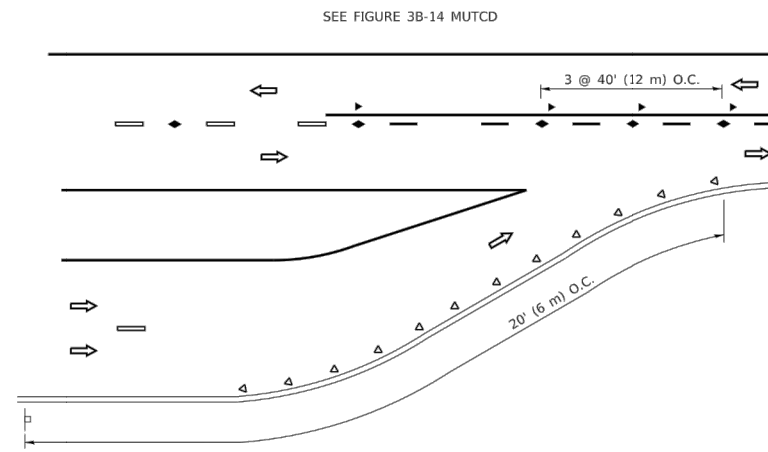
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	25
TC-10		CONTRACT NO. 61K47		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT FX97(514)		

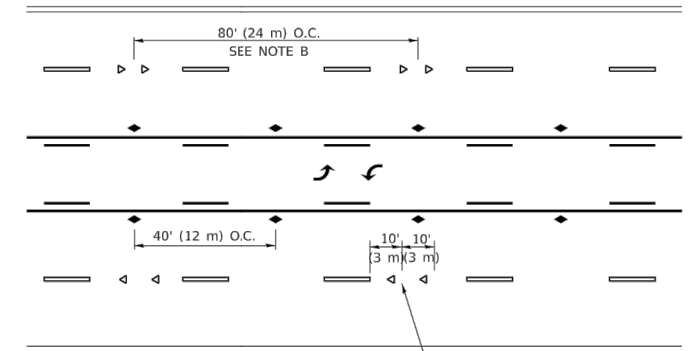


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

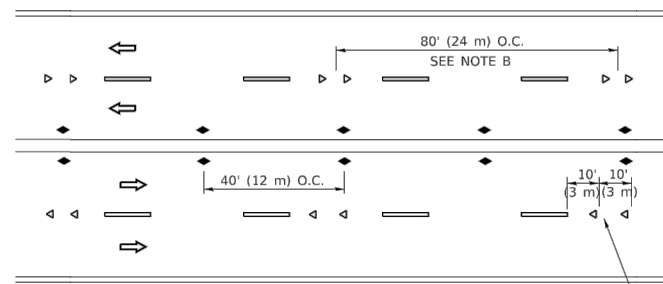
TWO-LANE/TWO-WAY



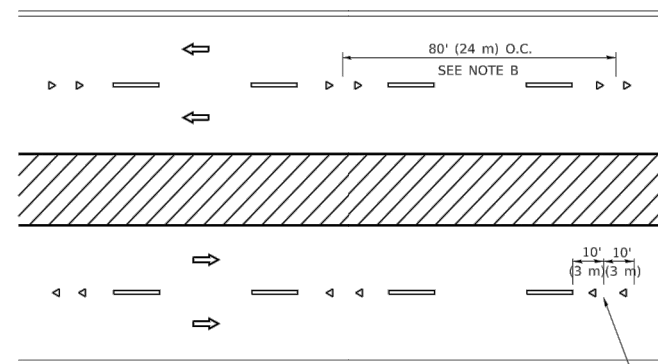
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

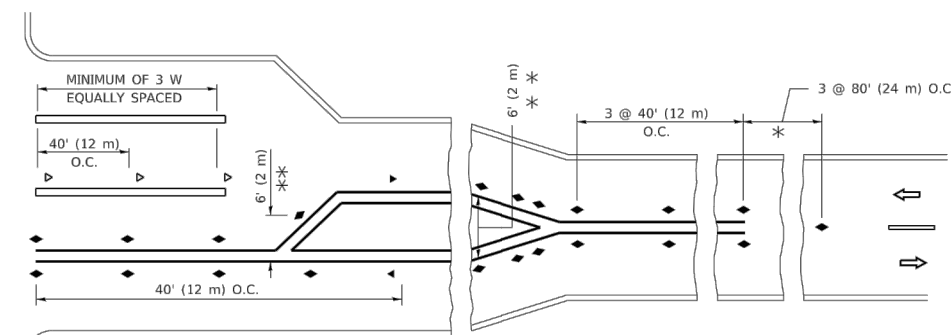
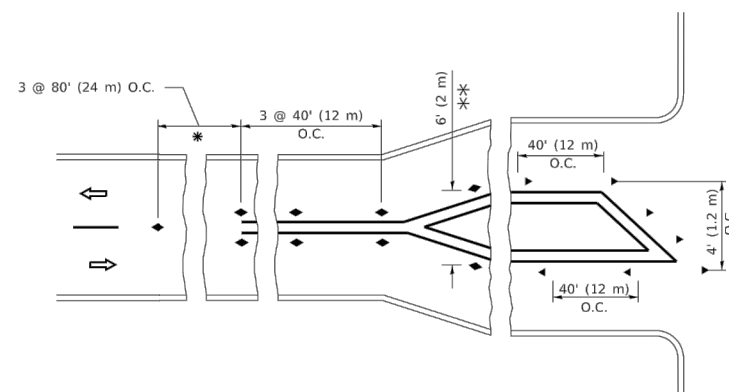
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

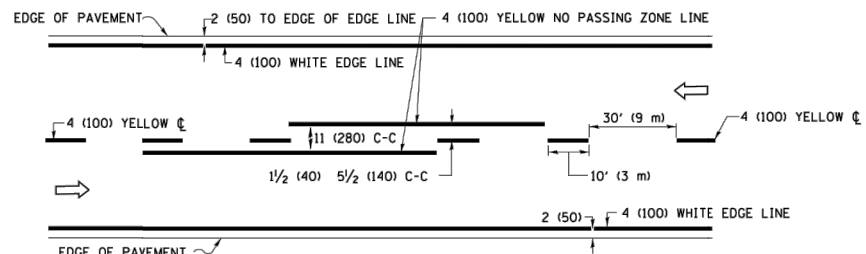


* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

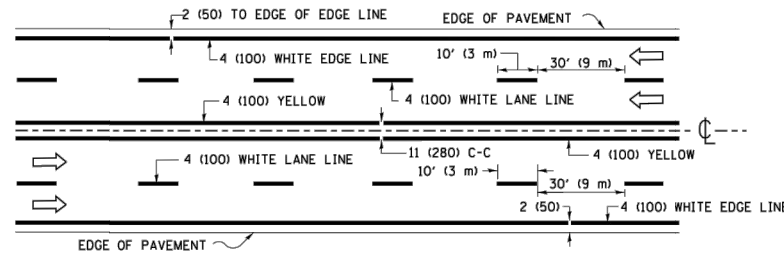
TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

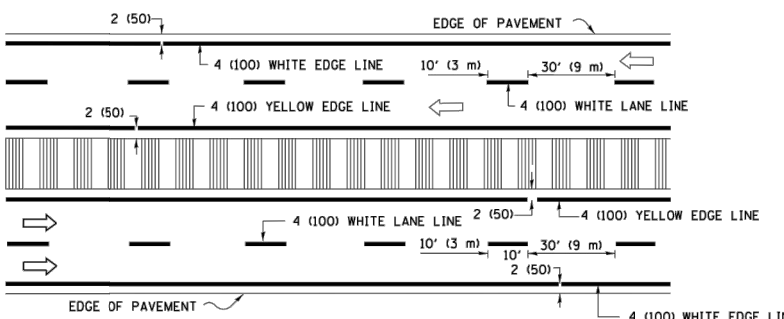
USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-12-99	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -	REVISED - T. RAMMACHER 01-06-00					1607	23-00120-00-RS	COOK	32	26
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09	SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	TC-11		CONTRACT NO. 61K47			
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13				FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT FX97(614)		



2-LANE ROADWAY

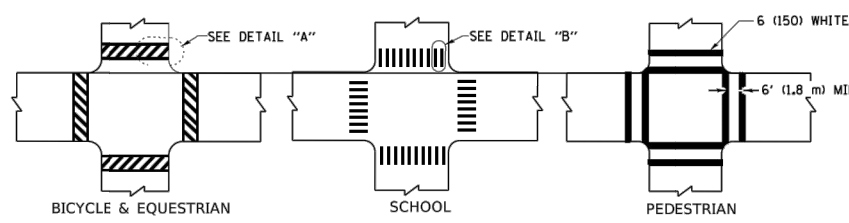


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

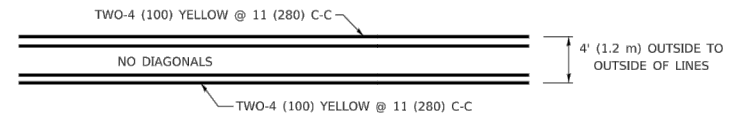


DETAIL "A"

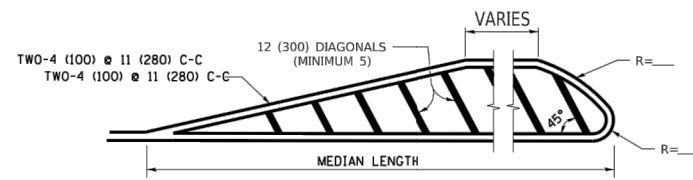
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



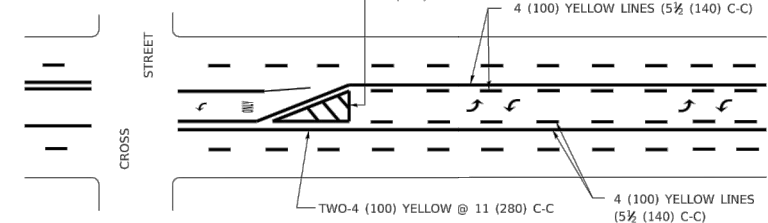
4' (1.2 m) WIDE MEDIANS ONLY



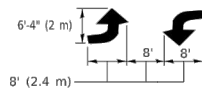
MEDIANS OVER 4' (1.2 m) WIDE

FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

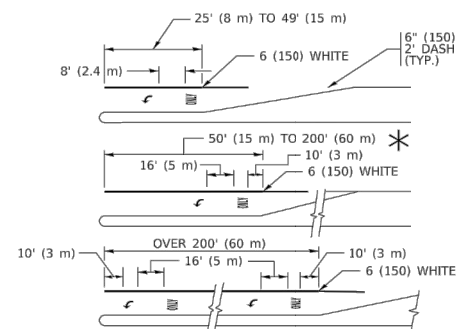


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

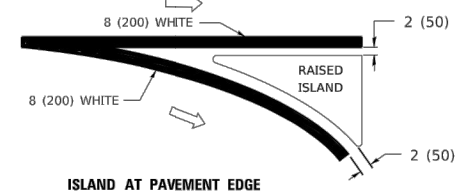
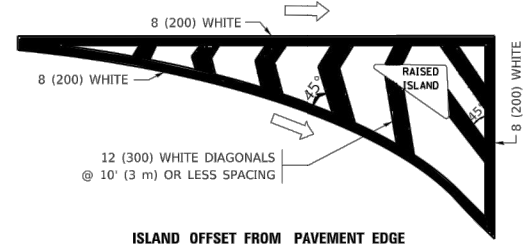


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

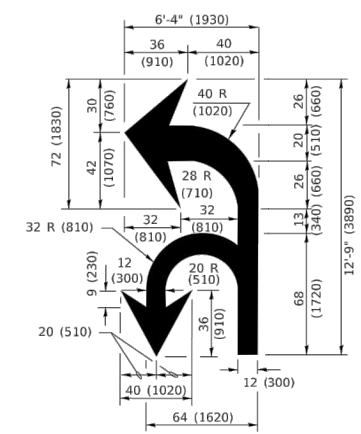
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

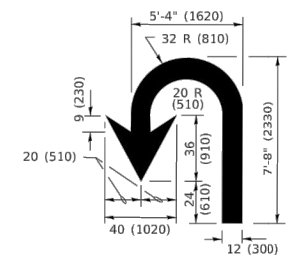
TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footem	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	27
TC-13		CONTRACT NO. 61K47		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT FX97(614)		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

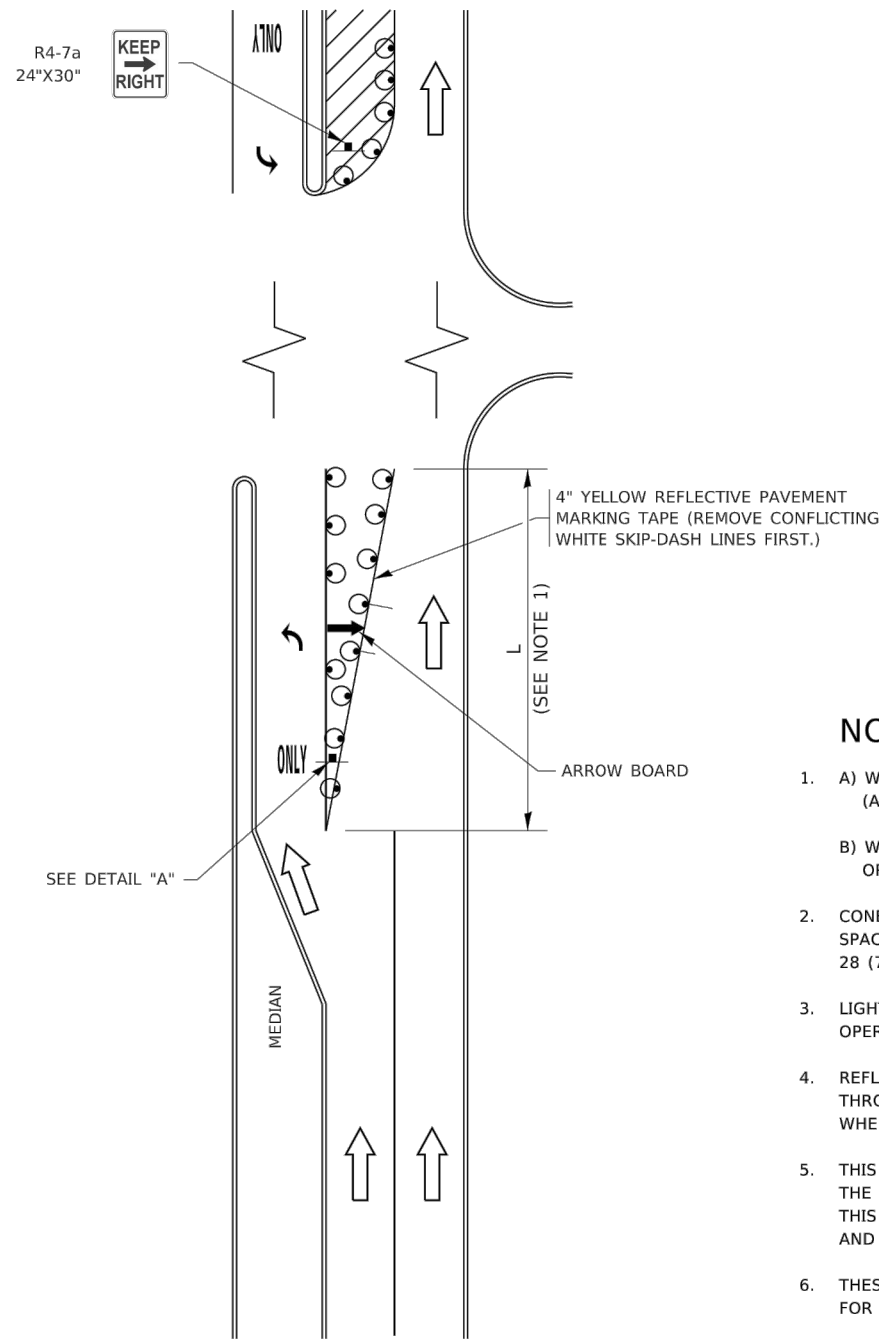


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

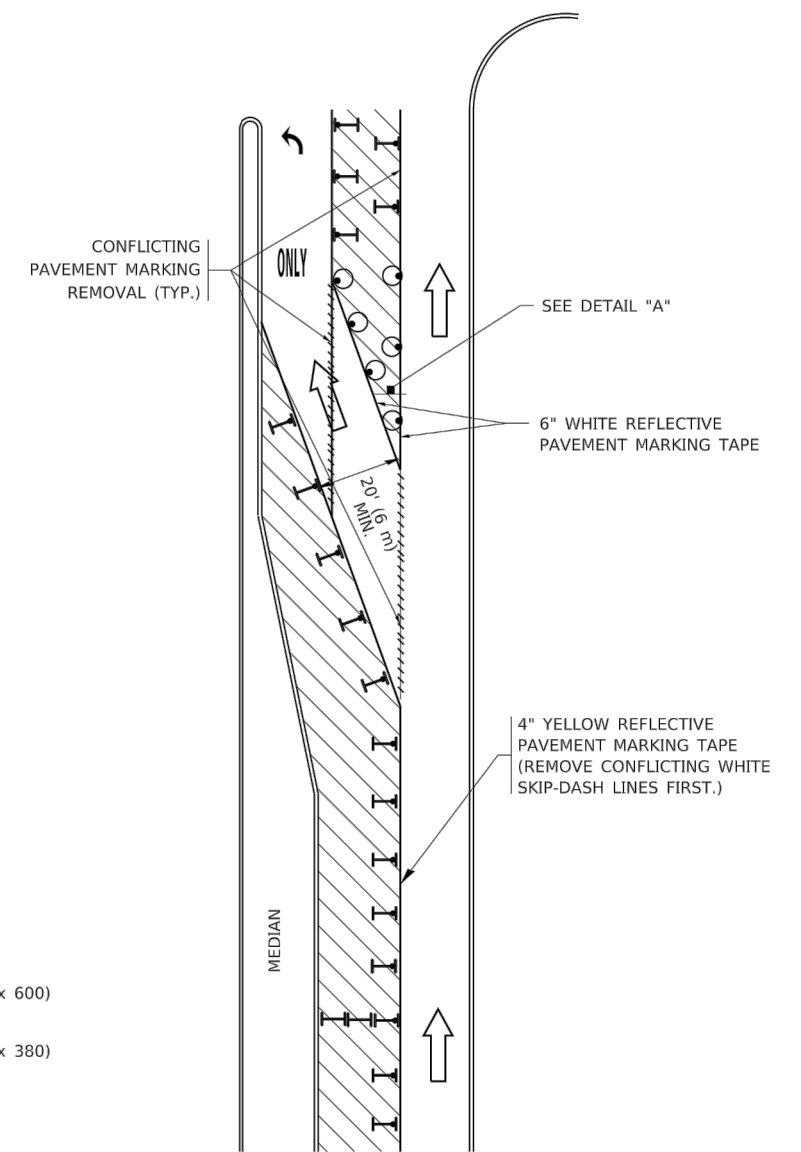


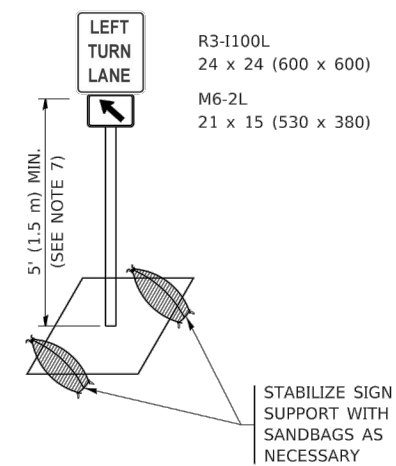
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

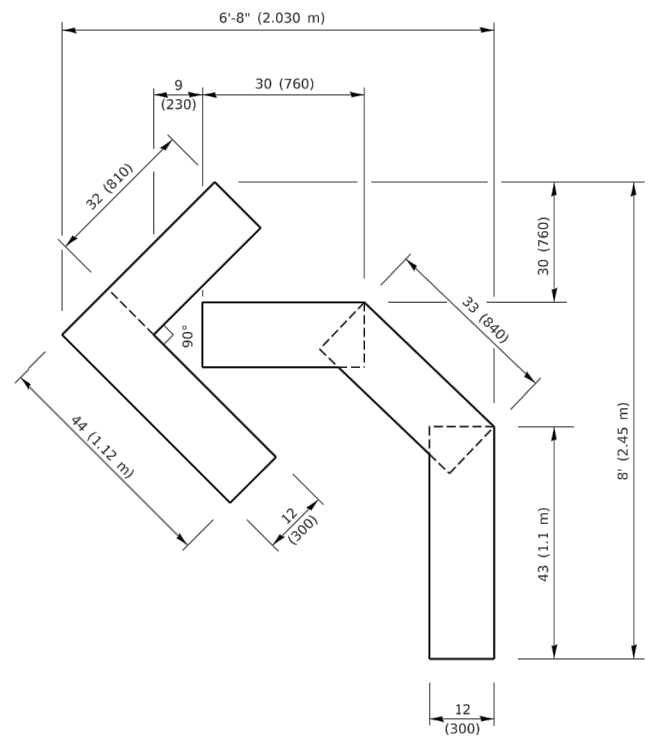
1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



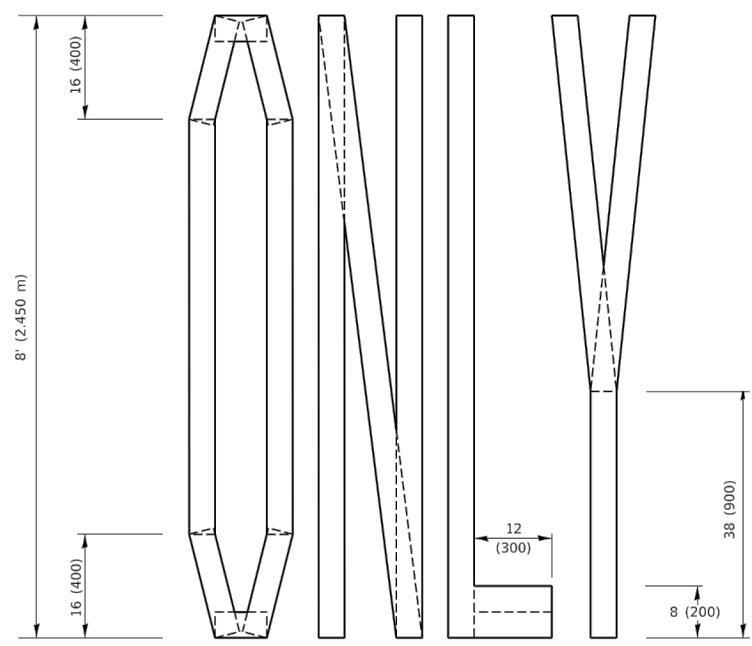
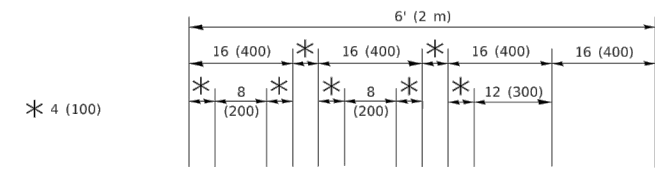
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

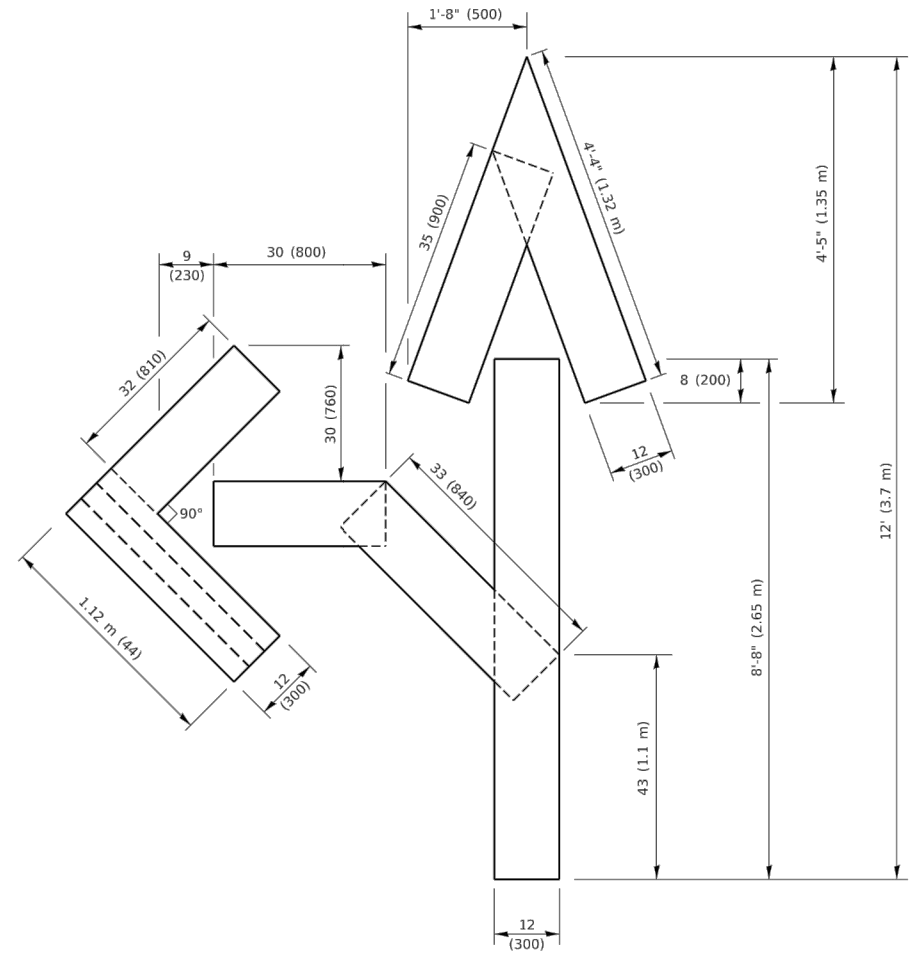
USER NAME = footemj	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13			1607	23-00120-00-RS	COOK	32	28	
PLOT SCALE = 50.0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 61K47			
PLOT DATE = 3/4/2019	DATE - T. RAMMACHER 01-06-00	REVISED -			FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	FX97(514)		



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

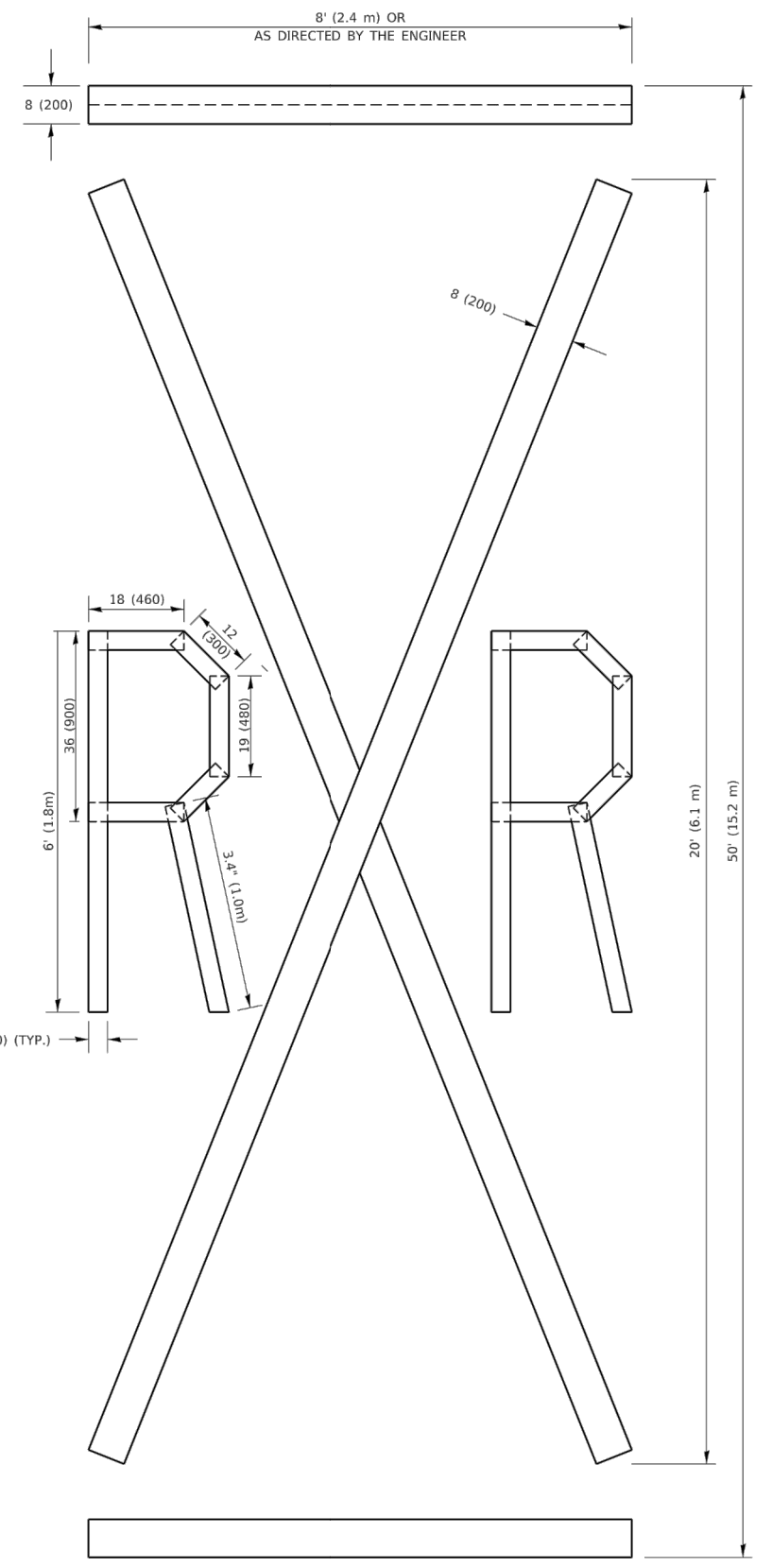


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

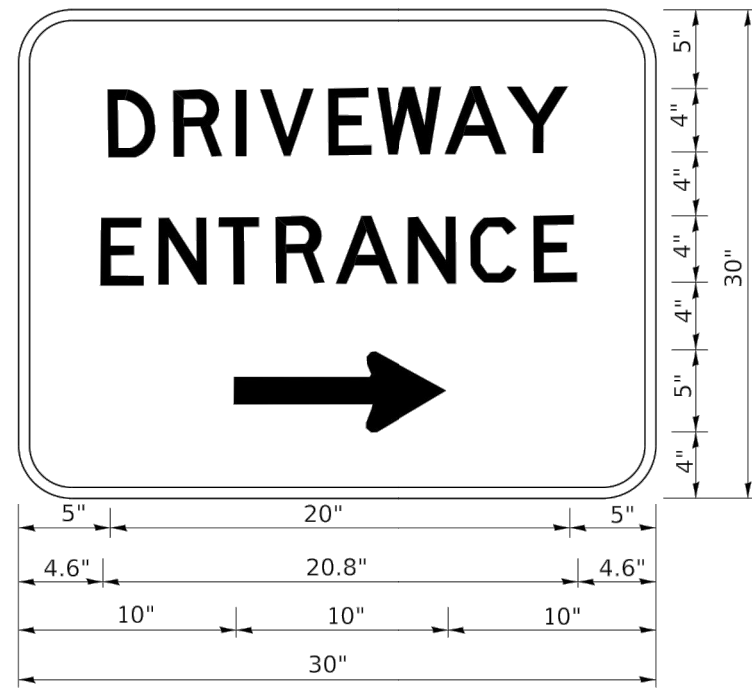
USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 50.0068 ' / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	29
TC-16		CONTRACT NO. 61K47		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT FX97(514)		



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

USER NAME = leysa	DESIGNED -	REVISED - C. JUCIUS 02-15-07
	DRAWN -	REVISED -
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 8/6/2021	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

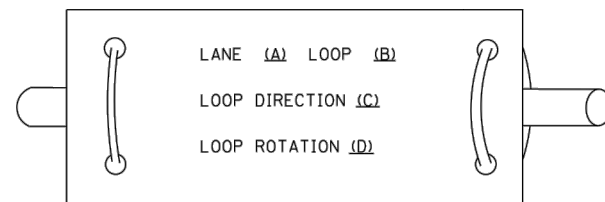
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	30
TC-26			CONTRACT NO. 61K47	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	FX97(514)	

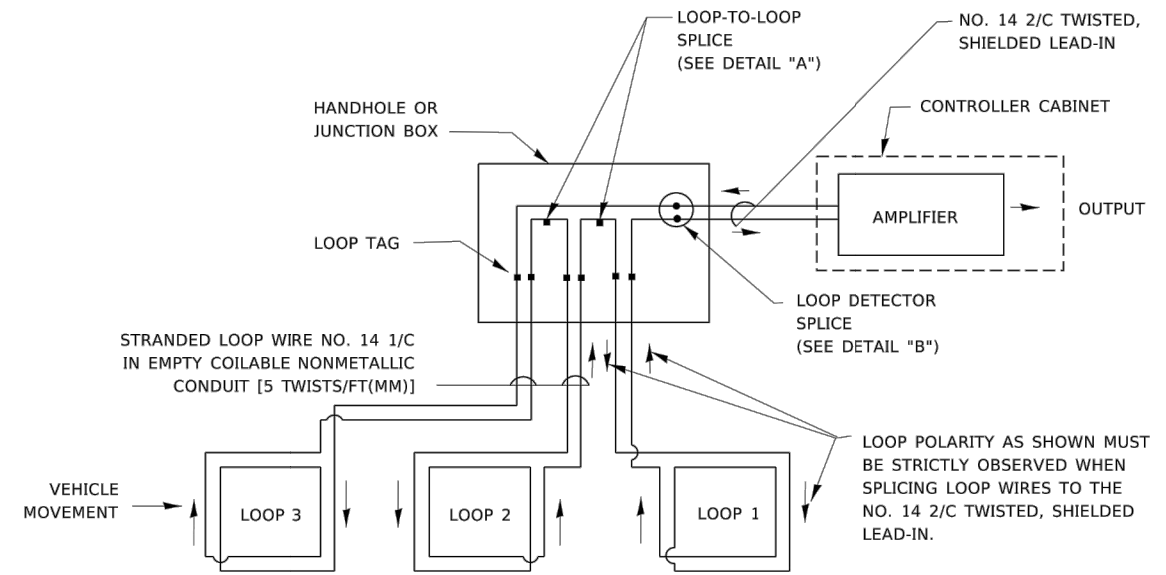
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

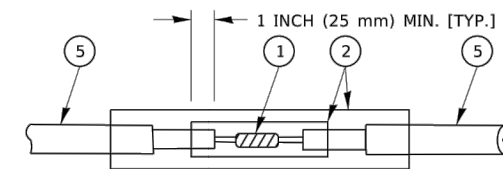


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

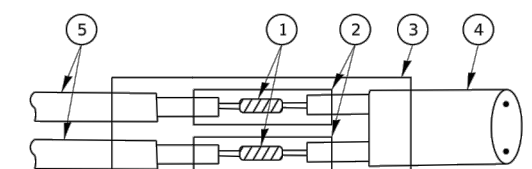


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

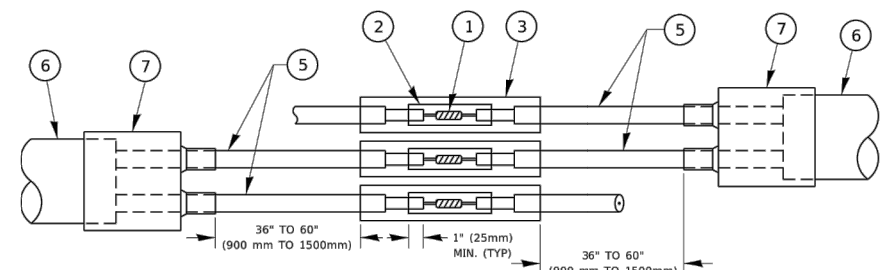


DETAIL "A"
LOOP-TO-LOOP SPLICE

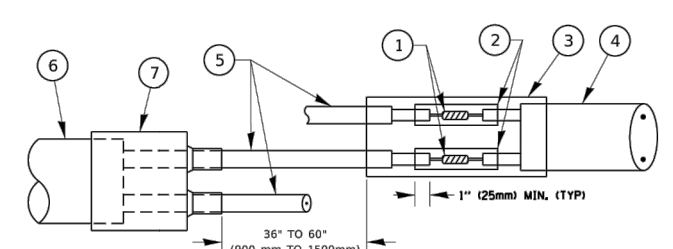


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

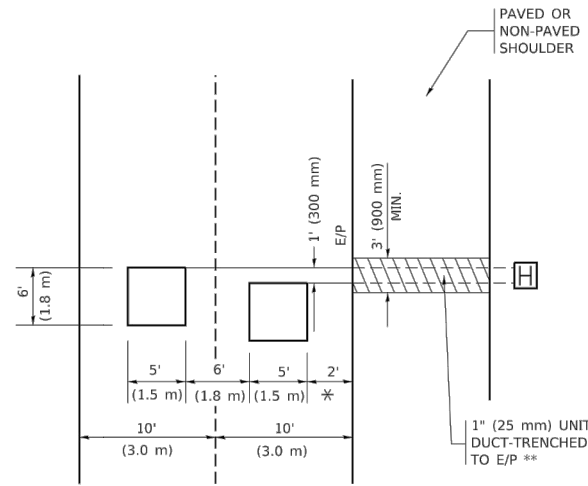
LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

USER NAME = footemj	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED -			1607	23-00120-00-RS	COOK	32	31	
PLOT DATE = 3/4/2019	CHECKED -	REVISED -			TS-05		CONTRACT NO. 61K47			
	DATE -	REVISED -			SCALE: NONE		SHEET 2 OF 7 SHEETS		STA. TO STA.	

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

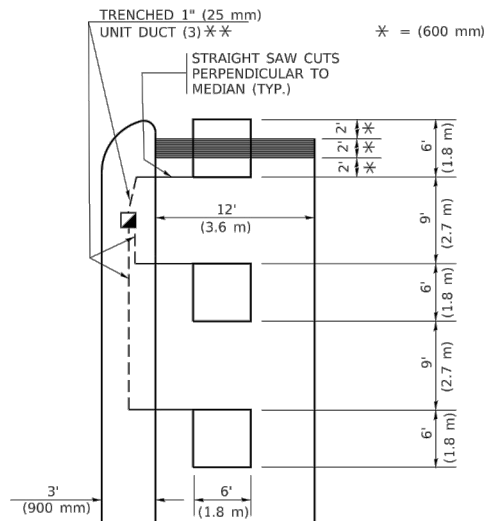


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

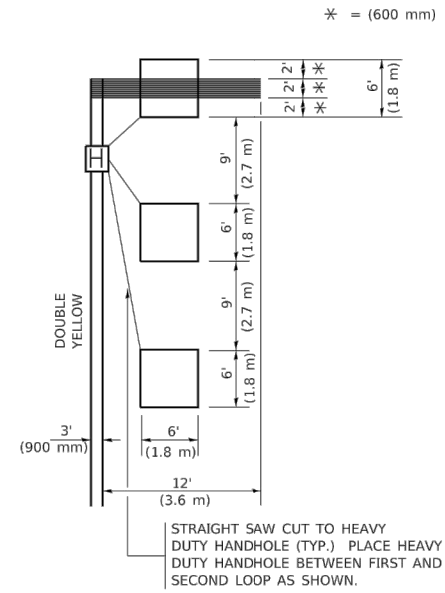


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

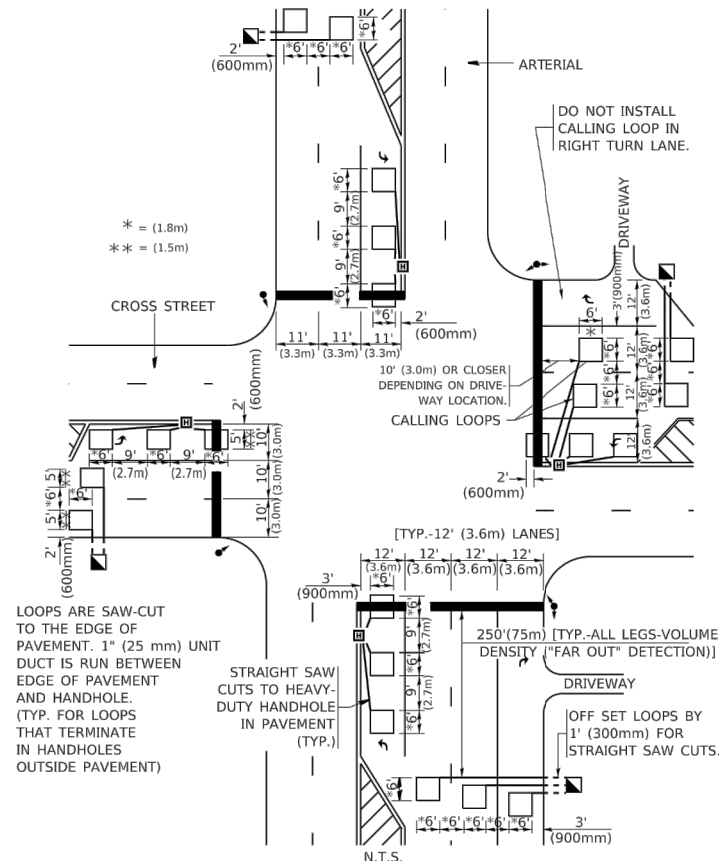
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)

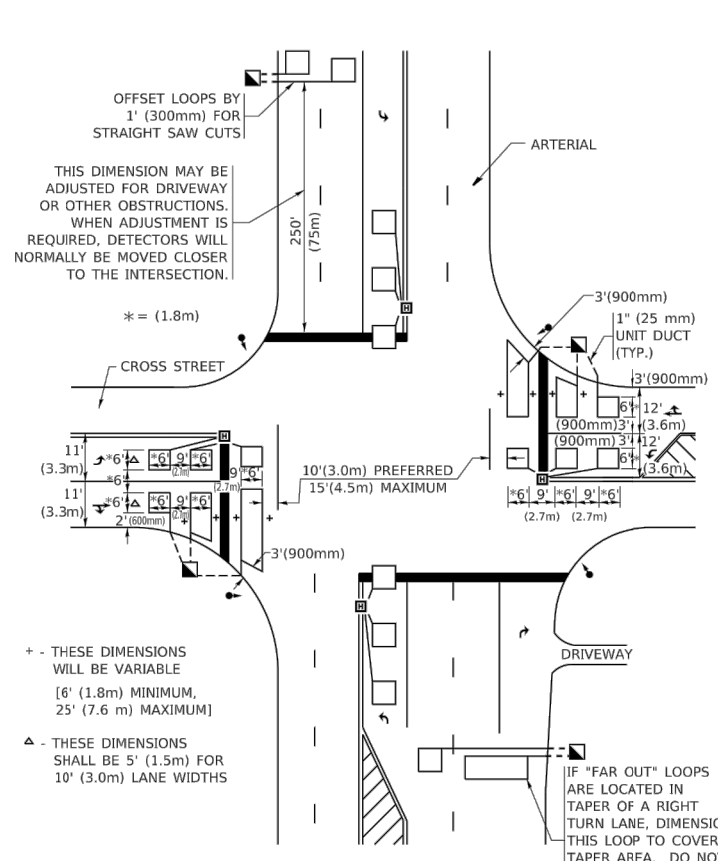


* = (1.8m)
 ** = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

DETAIL 1
 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
 ▲ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
 N.T.S.

USER NAME = footem	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 50.0000' / in.	CHECKED - R.K.F.	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1607	23-00120-00-RS	COOK	32	32
TS-07		CONTRACT NO. 61K47		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	FX97(614)	