

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE VILLAGE OF RIVERDALE.

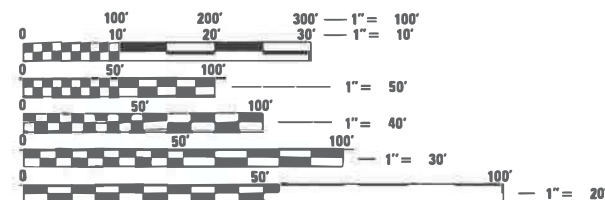
EXISTING ADT = 15,900 (2021)
 POSTED SPEED LIMIT = 40 MPH
 CLASSIFICATION = MINOR ARTERIAL



Amish T. Bhatt
 AMISH T. BHATT, P.E., S.E.
 LICENSE NO.: 081-006249
 EXPIRES: NOVEMBER 30, 2024
 FOR SHEETS 38-51, 53-90 ONLY



Brian M. Czosnyka
 BRIAN M. CZOSNYKA, P.E.
 EXPIRES: NOVEMBER 30, 2025
 FOR SHEETS 1-37 & 91-109 ONLY



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

IDOT PROJECT MANAGER: VESELIN VELICHKOV
 CONSULTANT PROJECT MANAGER: BRIAN CZOSNYKA

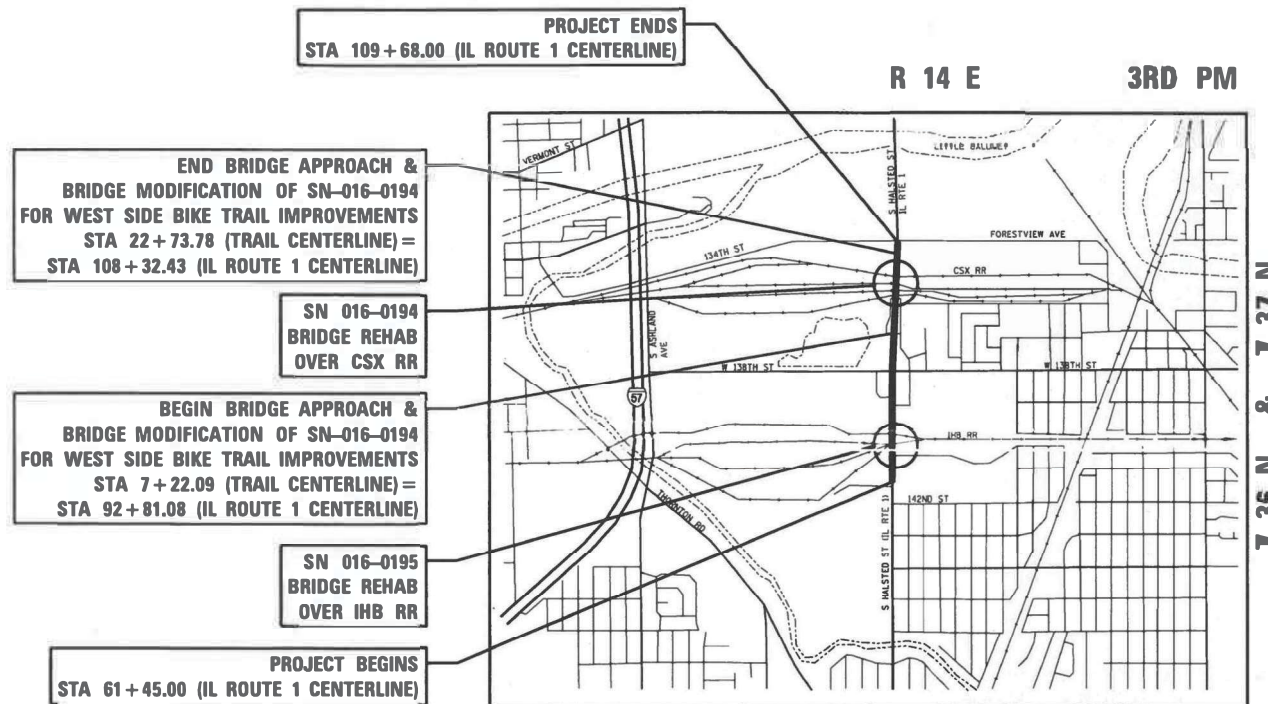
CONTRACT NO. 62W48 (former BLRS 61D99)

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

**PROPOSED
 HIGHWAY PLANS**

F.A.U. ROUTE 3730: IL 1 (HALSTED STREET)
 OVER IHB RR AND OVER CSX RR
 BRIDGE REHABILITATION / BIKEWAY
 SECTION 15-00131-01-BR
 PROJECT NO. STP- HTDL (389)
 COOK COUNTY

C-91-294-19

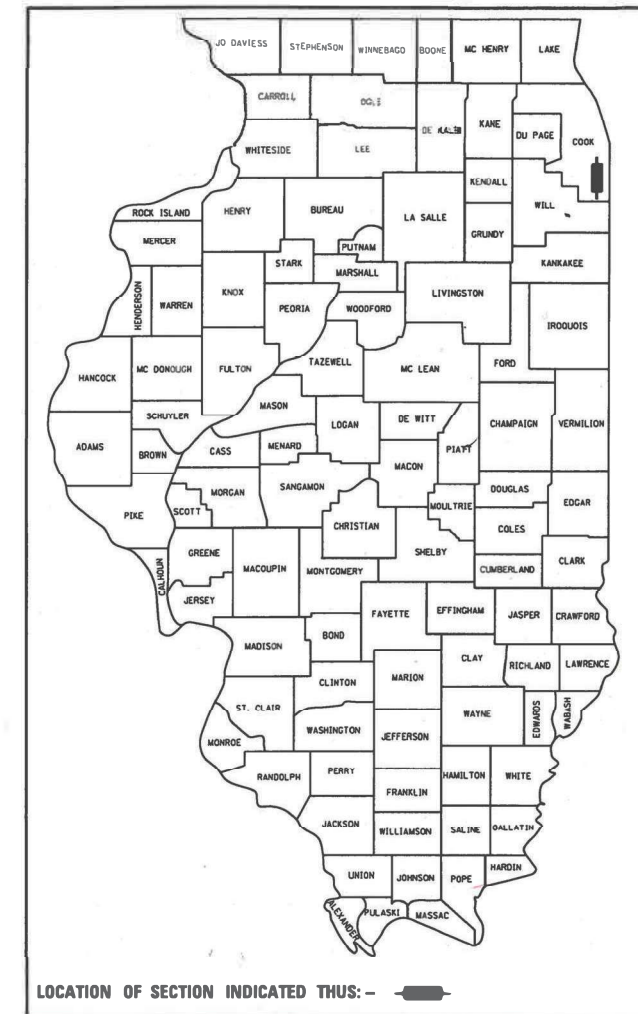


CALUMET & THORNTON TOWNSHIP
 LOCATION MAP
 NOT TO SCALE

GROSS LENGTH = 4,823.00 FT. = 0.91 MILE
 NET LENGTH = 4,823.00 FT. = 0.91 MILE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	1
ILLINOIS			CONTRACT NO. 62W48	

D-91-097-19



LOCATION OF SECTION INDICATED THIS: -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUBMITTED March 18, 2024
Joseph R. ...
 REGIONAL ENGINEER

May 10, 2024 *Scott A. ...*
 ENGINEER OF DESIGN AND ENVIRONMENT

May 10, 2024 *Stephen M. ...*
 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS



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606301-04	PC CONCRETE ISLANDS AND MEDIANS
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701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
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GENERAL NOTES - ROADWAY

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED.)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE LOCAL MUNICIPALITY. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE, AND THE DEPARTMENT AND DISTRICT DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL BE REQUIRED TO ASCERTAIN THE EXACT LOCATIONS OF SUCH UTILITIES AND EXERCISE CARE DURING CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM, IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.20 OF THE "STANDARD SPECIFICATIONS". THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE UTILITY OWNERS SO THAT THEIR FACILITIES MAY BE ADJUSTED OR RELOCATED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS UNLESS OTHERWISE NOTED IN THE PLANS. ALL RELOCATION WORK ON EXISTING PRIVATE UTILITIES WILL BE DONE BY THE OWNER OF THAT UTILITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE OR DESTRUCTION OF PUBLIC OR PRIVATE PROPERTY, AND SHALL RESTORE SUCH PROPERTY IN ACCORDANCE WITH ARTICLE 107.20.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- WHEN MILLED PAVEMENT IS OPENED TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH "BUTT JOINT AND HOT- MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREA.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE- ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN, AT PATRICE.HARRIS@ILLINOIS.GOV A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE "ADVANCE WARNING SIGN DETAIL FOR ARTERIAL TRAFFIC" LOCATED IN THE SPECIAL PROVISIONS FOR "TEMPORARY INFORMATION SIGNING FOR LANE CLOSURES" IS APPLICABLE ONLY TO ARTERIAL ROADS.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM DISTRICT 1 BRIDGE INSPECTORS.
- ALL ELEVATIONS IN THE PLANS ARE BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88). ALL BEARINGS AND COORDINATES REFERENCED IN THE PLAN DRAWINGS ARE BASED ON THE ILLINOIS STATE PLAN COORDINATE SYSTEM, EAST ZONE, NAD 83 (2007).
- PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATION. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION OR ORDERING MATERIALS
- THE CONTRACTOR SHALL BE REQUIRED TO DISPOSE OF ALL SIDEWALK, CURB AND GUTTER, PAVEMENT, AND ALL OTHER MATERIALS EXCAVATED OR REMOVED DUE TO CONSTRUCTION OPERATIONS.
- BEFORE REMOVAL OF ANY UNSUITABLE MATERIAL, THE CONTRACTOR SHALL TREAT THE SUBGRADE AS PER ARTICLE 301.04 OF THE "STANDARD SPECIFICATIONS" TO THE SATISFACTION OF THE ENGINEER. UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT OR FILL UNDER THE PROPOSED HMA TRAIL AS SHOWN ON THE TYPICAL CROSS SECTIONS.
- RAILROAD FLAGGING MUST BE COORDINATED AND PROVIDED FOR WORK ON CSX AND IHB RIGHT-OF-WAY. FLAGGING FOR EACH BRIDGE STRUCTURE (SN 016-0194 & SN 016-0195) SHALL BE PROVIDED FOR A MINIMUM OF 25 WORKING DAYS EACH. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05

- STOCKPILES OF TOPSOIL AND OTHER MATERIALS SHALL NOT BE LOCATED WITHIN A SPECIAL MANAGEMENT AREA. APPROVAL OF THE LOCATION MUST BE OBTAINED FROM THE ENGINEER PRIOR TO PLACEMENT. IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, EROSION CONTROL MEASURES SHALL BE PROVIDED. STOCKPILES TO REMAIN IN PLACE FOR 30 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO ABUTTING PROPERTY DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION AS APPROVED BY THE ENGINEER. SHOULD CLOSURE OF ACCESS BE REQUIRED, THE CONTRACTOR SHALL NOTIFY THE PROPERTY OWNER A MINIMUM OF 24 HOURS IN ADVANCE OF THE SHORT DURATION CLOSURE.
- GRASS AREAS DISTURBED DURING CONSTRUCTION SHALL BE RESTORED WITH SIX INCHES OF TOPSOIL AND SEED OR SOD.
- ALL STATION-OFFSET CALL OUTS AND CURVE DATA ON THE PLANS REFER TO THE PROPOSED CENTERLINE UNLESS OTHERWISE SHOWN.
- ANY WORK ON HOLIDAYS AND WEEKENDS WILL NEED PRIOR APPROVAL FROM THE ENGINEER.
- DRAINAGE STRUCTURES: DURING CONSTRUCTION OPERATIONS, WHENEVER ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED BY THE CONTRACTOR AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS, AND DISCHARGE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET, AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE. BEFORE ORDERING PIPE CULVERTS, THE CONTRACTOR SHALL CONTACT THE ENGINEER AS TO THE EXACT LENGTH AND QUANTITY REQUIRED.
- THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.
- GEOTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAVE BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL FIELD FOR REMOVAL AND REPLACEMENT WITH ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

GENERAL NOTES - TRAFFIC CONTROL & PROTECTION

- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OF CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION WITHIN TWO HOURS FROM THE TIME OF NOTIFICATION.
- ALL TEMPORARY PAVEMENT MARKINGS PROPOSED WITHIN THE WORK AREA SHALL BE COMPLETED PRIOR TO THE CONSTRUCTION PHASE CHANGE.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
ILLINOIS ROUTE 1 (HALSTED STREET)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	2
				CONTRACT NO. 62W48
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



303 East Wacker Drive, Suite 1400, Chicago, IL 60601
T 1-312-373-7700 F 1-312-373-8800

GENERAL NOTES - LANDSCAPING:

1. THE CONTRACTOR WILL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT 847-705-4171, TO SCHEDULE A WALK THROUGH TO DETERMINE TREES FOR PRUNING, ROOT PRUNING, REMOVAL, AND PROTECTION AT LEAST 14 DAYS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
2. ALL TREE PROTECTION, TREE REMOVAL, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.
3. THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO THE JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIALS STORED, OR VEHICLES DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE". REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.
4. TREE ROOT PRUNING IS TO BE USED ON EXISTING TREES/SHRUBS WHEN NECESSARY TO CUT MAJOR TREE/SHRUB ROOTS TO PREVENT THE RIPPING UP OF ROOTS WHEN TRENCHING OR EXCAVATION IS WITHIN THE ROOT ZONE OF ADJACENT TREES/SHRUBS TO REMAIN. SUPPLEMENTAL WATERING IS SPECIFIED FOR TREES AND SHRUBS THAT WILL BE DISTURBED BY CONSTRUCTION BUT WILL REMAIN. SUPPLEMENTAL WATERING OF TREES/SHRUBS SHALL BEGIN IMMEDIATELY AFTER ROOT PRUNING OF THE TREES/SHRUBS HAS OCCURRED.
5. THE CONTRACTOR SHALL ENSURE THAT NO CONSTRUCTION ACTIVITY, STORING, OR PARKING OF EQUIPMENT OR VEHICLES OCCURS BEYOND THE PERIMETER EROSION CONTROL BARRIER AND/ OR LIMITS OF CONSTRUCTION.
6. THE CONTRACTOR SHALL TAKE EXTRA CARE IN GRADING AND EXCAVATING NEAR TREES WHICH ARE NOT MARKED FOR REMOVAL SO AS NOT TO CAUSE INJURY TO THE ROOT SYSTEM OR TRUNKS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
7. THE ENGINEER WILL CONTACT FABIOLA QUIROZ OF THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4596, AT LEAST 7 DAYS PRIOR TO PLANTING FOR LAYOUT OF THE SEEDING AND TREES.
8. THE CONTRACTOR SHALL OBSERVE AND COMPLY WITH ALL SECTIONS OF THE ILLINOIS CUSTOM SPRAY LAW, INCLUDING LICENSING. CONTRACTOR PERSONNEL APPLYING HERBICIDES SHALL HAVE A VALID PESTICIDE APPLICATOR LICENSE ISSUED BY THE ILLINOIS DEPARTMENT OF AGRICULTURE. THE LICENSED PESTICIDE APPLICATOR SHALL SUBMIT THEIR CURRENT LICENSE TO THE ENGINEER. THE LICENSED PESTICIDE APPLICATOR SHALL BE QUALIFIED AT A MINIMUM IN RIGHT-OF-WAY AND AQUATICS. THE LICENSED APPLICATOR SHALL WORK ON-SITE.

SEDIMENTATION AND EROSION CONTROL NOTES:

- A. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
- B. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE.
- C. TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DISTURBED AREAS WITHIN 14 CALENDAR DAYS OF THE END OF ACTIVE HYDROLOGIC DISTURBANCE. PERMANENT STABILIZATION SHALL BE DONE WITHIN 14 DAYS AFTER COMPLETION OF FINAL GRADING.
- D. ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED, OR OTHERWISE TREATED TO REMOVE SEDIMENT.
- E. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
- F. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES SHALL BE MAINTAINED AND REPAIRED AS NEEDED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSPECTION AND REPAIR DURING CONSTRUCTION.
- G. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.
- H. THE CONTRACTOR SHALL INSTALL ALL EROSION CONTROL PRIOR TO THE START OF ANY EARTHWORK.
- I. IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, EROSION CONTROL MEASURES SHALL BE PROVIDED BY THE CONTRACTOR.
- J. EROSION CONTROL MEASURES SHALL COMPLY WITH THE MINIMUM REQUIREMENTS OF THE COOK COUNTY STORMWATER AND FLOODPLAIN ORDINANCE SPECIFICATIONS AT ALL TIMES.

GENERAL NOTES - RAILROAD:

1. REFER TO THE CSX TRANSPORTATION PUBLIC PROJECT INFORMATION MANUAL FOR ADDITIONAL REQUIREMENTS NEEDED FOR WORKING ON/ABOVE/ADJACENT TO CSXT. SPECIFIC SECTIONS THAT PERTAIN TO THIS PROJECT ARE: SPECIAL PROVISIONS FOR CONSTRUCTION NEAR CSXT PROPERTY, OVERHEAD BRIDGE CRITERIA, CONSTRUCTION SUBMISSION CRITERIA, SOIL AND WATER MANAGEMENT POLICY, AND INSURANCE REQUIREMENTS FOR PUBLIC PROJECTS.
2. CONTRACTOR ACCESS WILL BE LIMITED TO THE IMMEDIATE PROJECT AREA ONLY. THE CSXT RIGHT-OF-WAY OUTSIDE THE PROJECT AREA MAY NOT BE USED FOR CONTRACTOR ACCESS TO THE PROJECT SITE AND NO TEMPORARY AT-GRADE CROSSINGS WILL BE ALLOWED.
3. CSXT MAY REQUIRE THE CONTRACTOR TO INSTALL FILTER FABRIC OVER THE TRACK(S) AND BALLAST TO PREVENT ANY CONSTRUCTION DEBRIS FROM FOULING THE BALLAST. THIS WILL BE DETERMINED DURING ACTUAL CONSTRUCTION ACTIVITIES BY CSXT OR ITS REPRESENTATIVE. FABRIC WILL REMAIN IN PLACE UNTIL ALL CONSTRUCTION ACTIVITIES ARE COMPLETE.
4. IF CONTRACTOR HAS THE POTENTIAL TO PENETRATE THE DECK DURING THE DECK REHABILITATION WORK, THEN CONTRACTOR WILL BE REQUIRED TO INSTALL FALSEWORK/DEMO SHIELD PROTECTION DIRECTLY OVER THE CSXT RAILROAD SPAN. THE FALSEWORK/DEMO SHIELD PROTECTION WILL BE INSTALLED PRIOR TO THE DECK BEING PENETRATED AND WILL STAY IN PLACE FOR THE DURATION OF THE CONSTRUCTION ACTIVITIES. THE FALSEWORK/DEMO SHIELD SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE TO CSXT'S CONSTRUCTION SUBMISSION CRITERIA.
5. THE CONTRACTOR MAY NOT USE CSXT RIGHT-OF-WAY FOR STORAGE OF MATERIALS OR EQUIPMENT DURING CONSTRUCTION WITHOUT PRIOR CSXT APPROVAL. THE CSXT RIGHT-OF-WAY MUST REMAIN CLEAR FOR RAILROAD USE AT ALL TIMES. EQUIPMENT MAY NOT BE POSITIONED TO BLOCK THE RAILROAD ACCESS ROAD, TRACK AREA OR ANY PART OF THE CSXT RIGHT-OF-WAY WITHOUT PRIOR CSXT APPROVAL.
6. TEMPORARY CONSTRUCTION CLEARANCE - ENSURE ALL FALSEWORK, BRACING OR FORMS HAVE A MINIMUM HORIZONTAL CLEARANCE OF 9 FEET OR EXISTING, WHICHEVER IS LESS, MEASURED PERPENDICULAR TO THE CENTERLINE OF THE NEAREST TRACK, AND A MINIMUM VERTICAL CLEARANCE OF 21.5 FEET OR EXISTING, WHICHEVER IS LESS, AS MEASURED FROM THE TOP OF RAIL PROFILE.
7. THE CONTRACTOR WILL BE REQUIRED TO ABIDE BY THE PROVISIONS OF THE AGENCY/CSXT CONSTRUCTION AGREEMENT. PERIODICALLY, THROUGHOUT THE PROJECT DURATION, THE CONTRACTOR WILL BE REQUIRED TO MEET, DISCUSS AND, IF NECESSARY, TAKE IMMEDIATE ACTION AT THE DISCRETION OF CSXT PERSONNEL AND/OR THEIR AUTHORIZED REPRESENTATIVE, TO COMPLY WITH PROVISIONS OF THAT AGREEMENT AND THESE SPECIFICATIONS
8. UPON COMPLETION OF THE WORK ON CSXT PROPERTY, THE CONTRACTOR SHALL REQUEST THE OWNER TO ARRANGE A FINAL INSPECTION OF THE PROJECT WITH THE RAILROAD'S PROJECT ENGINEER OR HIS AUTHORIZED REPRESENTATIVE.
9. CSXT SHALL BE NOTIFIED AT LEAST 5 DAYS IN ADVANCE OF THE PRE-CONSTRUCTION MEETING.
10. CSXT HAS SOLE AUTHORITY TO DETERMINE THE NEED FOR TRACK PROTECTION REQUIRED TO PROTECT ITS OPERATIONS AND PROPERTY. IN GENERAL, TRACK PROTECTION WILL BE REQUIRED WHENEVER CONTRACTOR OR EQUIPMENT ARE, OR ARE LIKELY TO BE, WORKING WITHIN FIFTY FEET OF TRACK OR OTHER TRACK CLEARANCES AS SPECIFIED BY CSX.

MODEL: #MODELNAME\$
 FILE NAME: P:\aecom\ne\pawben\laycom\ECOM_USA_Illinois\Documents\6067159-1\DOT-Halsted-St-Bridges-Riverdale\900_CAD\9100_CAD\9100_SHEETS\Civil\60671591_SHT_General_Notes-02



USER NAME = ken.moy	DESIGNED - KLM	REVISED -
	DRAWN - KLM	REVISED -
PLOT SCALE = 100.0000' / 1in.	CHECKED - BMC	REVISED -
PLOT DATE = 4/2/2024	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES CONTINUED
ILLINOIS ROUTE 1 (HALSTED STREET)**

SCALE: NONE SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	3
				CONTRACT NO. 62W48
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE					
				ROADWAY	ROADWAY	BRIDGE	BRIDGE	BIKE TRAIL	
				80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	
				SN 016-0194	SN 016-0195	SN 016-0194	SN 016-0195	SN 016-0194	
			URBAN	0005	0005	0013	0013	0021	
* 20101000	TEMPORARY FENCE	FOOT	310						310
20101200	TREE ROOT PRUNING	EACH	11						11
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	6						6
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	8						8
20200100	EARTH EXCAVATION	CUYD	30						30
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CUYD	455						455
20400800	FURNISHED EXCAVATION	CUYD	375						375
20800150	TRENCH BACKFILL	CUYD	20						20
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQYD	1132						1132
* 21101625	TOPSOIL FURNISH AND PLACE, 6"	SQYD	1230						1230
* 25000210	SEEDING, CLASS 2A	ACRE	0.25						0.25
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	8						8
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	8						8
* 25100115	MULCH, METHOD 2	ACRE	0.25						0.25

* = SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 1 OF 10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE				
				ROADWAY	ROADWAY	BRIDGE	BRIDGE	BIKE TRAIL
				80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE
				SN 016-0194	SN 016-0195	SN 016-0194	SN 016-0195	SN 016-0194
			URBAN	0005	0005	0013	0013	0021
25200200	SUPPLEMENTAL WATERING	UNIT	330					330
* 28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	27					27
* 28000400	PERIMETER EROSION BARRIER	FOOT	818					818
* 28000510	INLET FILTERS	EACH	4					4
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CUYD	68					68
31101400	SUBBASE GRANULAR MATERIAL, TYPE B 6"	SQYD	39	39				
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQYD	905					905
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	2036					2036
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	1296	485	540			271
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQYD	648	288	360			
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	4	4				
40602985	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	44					44
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	102					102
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	275	106	130			39

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 2 OF 10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE				
				ROADWAY		BRIDGE		BIKE TRAIL
				80% FED / 20% STATE		80% FED / 20% STATE		80% FED / 20% STATE
				SN 016-0194	SN 016-0195	SN 016-0194	SN 016-0195	SN 016-0194
			URBAN	0005	0005	0013	0013	0021
42001300	PROTECTIVE COAT	SQYD	222		115			107
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	352		352			
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQYD	139					139
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1037		223			814
44000600	SIDEWALK REMOVAL	SQ FT	1986		973			1013
44002210	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2 1/2"	SQYD	35					35
44003100	MEDIAN REMOVAL	SQ FT	105					105
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	4501					4501
44200956	CLASS B PATCHES, TYPE II, 9 INCH	SQYD	35					35
44201298	DOWEL BARS 1 1/4"	EACH	27					27
44213200	SAW CUTS	FOOT	105					105
44213208	TIE BARS 1 1/4"	EACH	27					27
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQYD	59					59
50102400	CONCRETE REMOVAL	CUYD	190.4			41.7	57.0	91.7

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 3 OF 10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	6
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE					
				ROADWAY	ROADWAY	BRIDGE	BRIDGE	BIKE TRAIL	
				80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	
				SN 016-0194	SN 016-0195	SN 016-0194	SN 016-0195	SN 016-0194	
				0005	0005	0013	0013	0021	
			URBAN						
50157300	PROTECTIVE SHIELD	SQ YD	9151			4004	5147		
50300255	CONCRETE SUPERSTRUCTURE	CUYD	519.9			106.1	175.6	238.2	
50300300	PROTECTIVE COAT	SQ YD	11401			3738	6075	1588	
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	29090			11490	17600		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	53620			11180	16650	25790	
50800515	BAR SPLICERS	EACH	504			204	300		
50901720	BICYCLE RAILING	FOOT	789					789	
50901750	PARAPET RAILING	FOOT	1379					1379	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	605			270	335		
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	140			60	80		
52100520	ANCHOR BOLTS, 1"	EACH	280			120	160		
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	19					19	
55100500	STORM SEWER REMOVAL 12"	FOOT	24					24	
59000200	EPOXY CRACK INJECTION	FOOT	358			208	150		

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 4 OF 10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	7
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE				
				ROADWAY		BRIDGE		BIKE TRAIL
				80% FED / 20% STATE		80% FED / 20% STATE		80% FED / 20% STATE
				SN 016-0194		SN 016-0195		SN 016-0194
				0005	0005	0013	0013	0021
			URBAN					
60201330	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 23 FRAME AND GRATE	EACH	4					4
60255500	MANHOLES TO BE ADJUSTED	EACH	1					1
60500050	REMOVING CATCH BASINS	EACH	4					4
60620800	CONCRETE MEDIAN, TYPE SB-9,12	SQ FT	48					48
60624600	CORRUGATED MEDIAN	SQ FT	56					56
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	1					1
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1					1
63200310	GUARDRAIL REMOVAL	FOOT	288					288
* 66900200	NON-SPECIAL WASTE DISPOSAL	CUYD	390					390
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2					2
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1					1
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1					1
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	5					5
67100100	MOBILIZATION	L SUM	1			0.25	0.50	0.25

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STATE OF ILLINOIS
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SUMMARY OF QUANTITIES

SHEET NO. 5 OF 10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	8
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE				
				ROADWAY	ROADWAY	BRIDGE	BRIDGE	BIKE TRAIL
				80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE
				SN 016-0194	SN 016-0195	SN 016-0194	SN 016-0195	SN 016-0194
			URBAN	0005	0005	0013	0013	0021
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	75	45	30			
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	60	36	24			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	10963	5395	5568			
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2029	1220	809			
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	44405	26609	17796			
70307130	TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE	FOOT	109	109				
70400100	TEMPORARY CONCRETE BARRIER	FOOT	3187.5	3187.5				
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	6912.5	2587.5	4325.0			
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	2	2			
70600350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	2	2			
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	1					1
72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	1					1
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	183	110	73			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6204	2584	3620			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	570	435	135			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	32	32				
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	1724					1724
* 78004635	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 7"	FOOT	630			270	360	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 6 OF 10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	9
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE					
				ROADWAY		BRIDGE		BIKE TRAIL	
				80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	
				SN 016-0194	SN 016-0195	SN 016-0194	SN 016-0195	SN 016-0194	
				0005	0005	0013	0013	0021	
			URBAN						
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	5102	2228	2874				
78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	2551			1114	1437		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	81	81					
							36		
* 78200006	GUARDRAIL REFLECTORS, TYPE B	EACH	2						2
									18
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	64			28	36		
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	3348	1795	1553				
* 81100605	CONDUIT ATTACHED TO STRUCTURE, 2" DIA., PVC COATED GALVANIZED STEEL	FOOT	1362			600	762		
* 81300945	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 24" X 24" X 8"	EACH	4			2	2		
* A2002820	TREE, CATALPA SPECIOSA (NORTHERN CATALPA), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	5						5
* A2002916	TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 2" CALIPER, BALLED AND BURLAPPED	EACH	8						8
* A2005020	TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEETREE), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	9						9
* A2006516	TREE, QUERCUS BICOLOR (SWAMP WHITE OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	6						6
* A2008468	TREE, ULMUS AMERICANA PRINCETON (PRINCETON AMERICAN ELM), 2" CALIPER, BALLED AND BURLAPPED	EACH	5						5
* K0029618	WEED CONTROL, BROADLEAF INTURF	GALLON	0.50	0.25	0.25				
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	47.0			37.0	10.0		

* = SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 7 OF 10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	10
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE				
				ROADWAY	ROADWAY	BRIDGE	BRIDGE	BIKE TRAIL
				80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE
				SN 016-0194	SN 016-0195	SN 016-0194	SN 016-0195	SN 016-0194
			URBAN	0005	0005	0013	0013	0021
Z0001899	JACK AND REMOVE EXISTING BEARINGS	EACH	74.0			22.0	52.0	
Z0001903	STRUCTURAL STEEL REMOVAL	POUND	2430				2430	
Z0001905	STRUCTURAL STEEL REPAIR	POUND	430				430	
Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	7166			3014	4152	
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	7528			3376	4152	
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	1229			709	520	
Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	1819			1130	689	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1					1
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	33.0			22.0	6.0	5.0
Z0018002	DRAINAGE SCUPPERS, DS-11	EACH	4					4
Z0018010	DRAINAGE SCUPPERS, DS-33	EACH	4				4	
Z0018051	DRAINAGE SCUPPERS TO BE ADJUSTED	EACH	2					2
Z0029090	DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	6535			2865	3670	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	102	51	51			
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1			0.5	0.5	

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STATE OF ILLINOIS
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SUMMARY OF QUANTITIES

SHEET NO. 8 OF 10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	11
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE				
				ROADWAY		BRIDGE		BIKE TRAIL
				80% FED / 20% STATE		80% FED / 20% STATE		80% FED / 20% STATE
				SN 016-0194		SN 016-0195		SN 016-0194
				0005	0005	0013	0013	0021
			URBAN					
Z0055800	RUSTIC RAIL FENCE	FOOT	130					130
Z0073200	TEMPORARY SHORING AND CRIBBING	EACH	66			38	28	
* Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	2	1	1			
X0325748	ACRYLIC COATING	SQ YD	171			108	63	
X0325749	FIBER WRAP	SQ FT	1531			970	561	
X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	167			53	114	
* X0327018	DECORATIVE SIGN POST	EACH	1					1
* X1700053	MONUMENT TYPE A FOUNDATION	EACH	1					1
* X2010350	TREE REMOVAL, ACRES (SPECIAL)	ACRE	0.25					0.25
* X2502014	SEEDING, CLASS 4A (MODIFIED)	ACRE	0.50					0.50
X2503000	MAINTENANCE MOWING	ACRE	5	2.5	2.5			
* X2511630	EROSION CONTROL BLANKET (SPECIAL)	SQ YD	1288					1288
X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	5		5			
X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	326	220				106
X4404400	PAVEMENT REMOVAL (SPECIAL)	SQ YD	10			8	2	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 9 OF 10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	12
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE					
				ROADWAY	ROADWAY	BRIDGE	BRIDGE	BIKE TRAIL	
				80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	80% FED / 20% STATE	
				SN 016-0194	SN 016-0195	SN 016-0194	SN 016-0195	SN 016-0194	
				0005	0005	0013	0013	0021	
			URBAN						
X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	6769			2937	3832		
X5091600	STEEL RAILING REMOVAL	FOOT	789					789	
X5230142	BRIDGE DRAINAGE SYSTEM	L SUM	1			0.5	0.5		
X6330705	RUB RAIL	FOOT	75					75	
* X6370048	CONCRETE BARRIER BASE (SPECIAL)	FOOT	242					242	
* X6370050	CONCRETE BARRIER WALL (SPECIAL)	FOOT	590					590	
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12			4	5	3	
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	0.5	0.5				
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	121	81	40				
X7830052	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REPLACEMENT	EACH	40		40				

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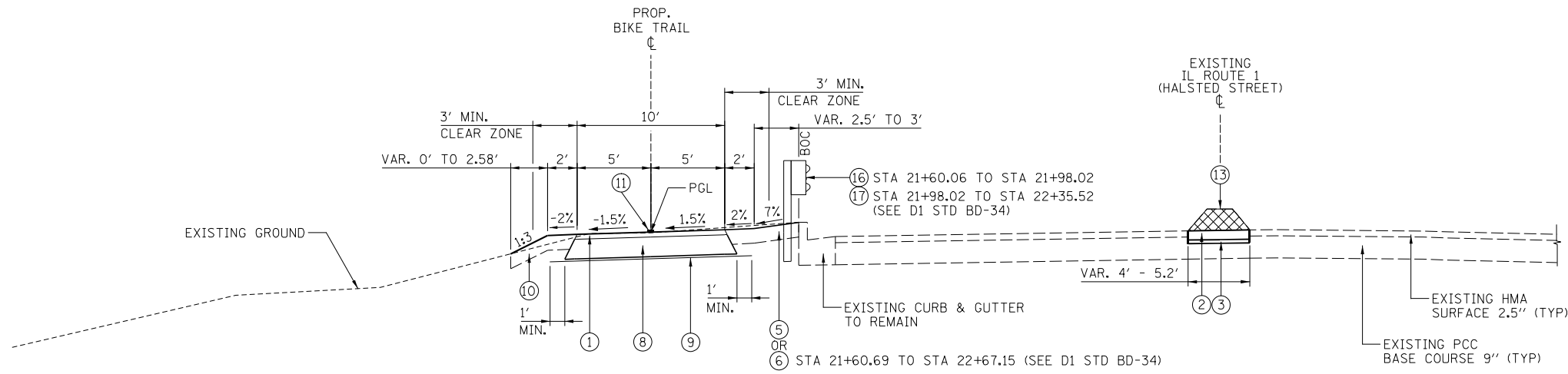
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

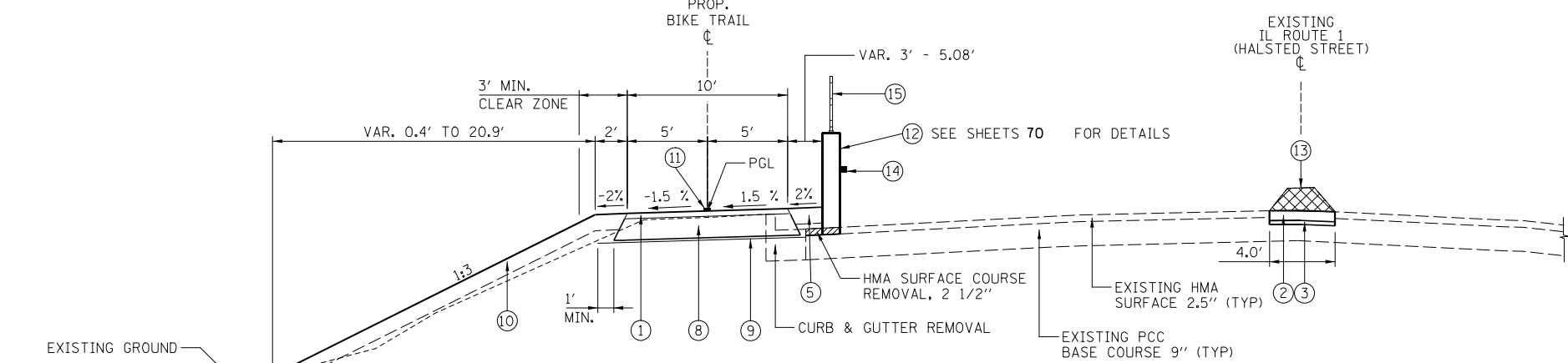
SHEET NO. 10 OF 10 SHEETS STA. TO STA.

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	13
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	



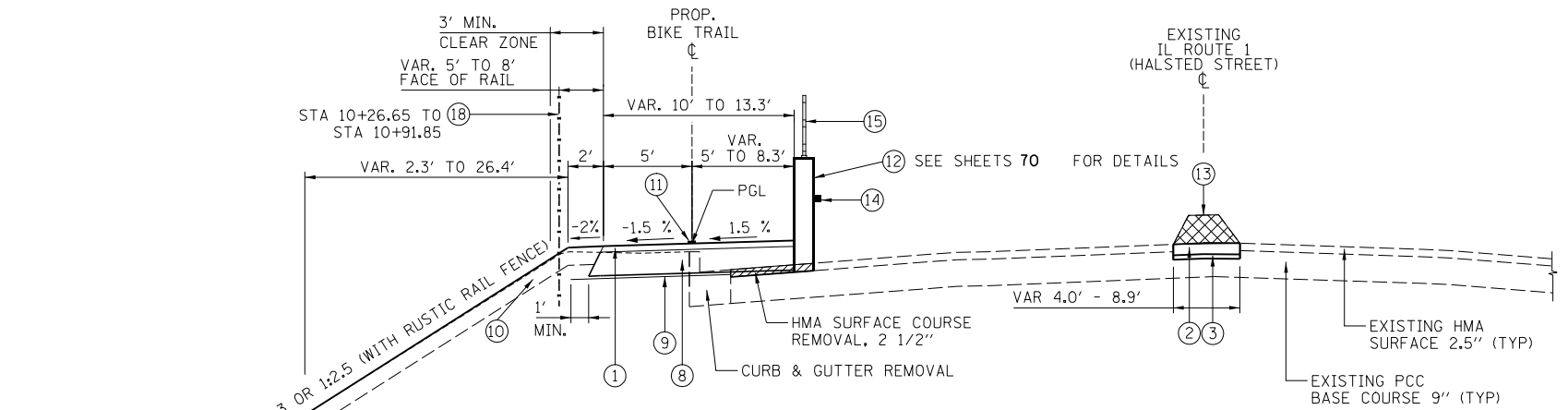
PROPOSED TYPICAL SECTION
 STA 7+22.09 TO STA 7+83.69
 STA 21+60.90 TO STA 22+73.78
 (LOOKING NORTH)

TRAIL CROSS SLOPE TRANSITION:
 (20' APPROACH TRANSITIONS TO 0% CROSS SLOPE, TYP)
 STA. 7+30.00 TO STA. 7+50.00 (0%)
 STA. 7+50.00 (0%) TO STA. 7+70.00
 STA. 22+30.00 TO STA. 22+50.00 (0%)
 STA. 22+50.00 (0%) TO STA. 22+70.00



PROPOSED TYPICAL SECTION
 STA 7+83.69 TO STA 9+11.71
 (LOOKING NORTH)

TRAIL CROSS SLOPE TRANSITION:
 (20' APPROACH TRANSITIONS TO 0% CROSS SLOPE, TYP)
 STA. 10+50.00 TO STA. 10+70.00 (0%)
 STA. 10+70.00 (0%) TO STA. 10+90.00



PROPOSED TYPICAL SECTION
 STA 9+11.71 TO STA 10+91.85
 STA 19+37.35 TO STA 21+60.90
 (LOOKING NORTH)

TRAIL CROSS SLOPE TRANSITION:
 (20' APPROACH TRANSITIONS TO 0% CROSS SLOPE, TYP)
 STA. 10+50.00 TO STA. 10+70.00 (0%)
 STA. 10+70.00 (0%) TO STA. 10+90.00

LEGEND:

- ① HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
- ② HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 3/4"
- ③ HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ④ NOT USED
- ⑤ TOPSOIL FURNISH AND PLACE, 6", AND SEEDING CLASS 2A
- ⑥ HOT-MIX ASPHALT SHOULDERS, 6"
- ⑦ NOT USED
- ⑧ AGGREGATE BASE COURSE, TYPE B, 8"
- ⑨ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ⑩ TOPSOIL FURNISH AND PLACE, 6", EROSION CONTROL BLANKET (SPECIAL), AND SEEDING, CLASS 4A (MODIFIED)
- ⑪ PAINT PAVEMENT MARKING, LINE 4" YELLOW
- ⑫ CONCRETE BARRIER WALL (SPECIAL)
- ⑬ MEDIAN REMOVAL PARTIAL DEPTH
- ⑭ BARRIER WALL REFLECTORS, TYPE C (CRYSTAL)
- ⑮ PARAPET RAILING
- ⑯ TRAFFIC BARRIER TERMINAL, TYPE 6
- ⑰ TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT
- ⑱ RUSTIC RAIL FENCE

NOTE:

WHERE UNSUITABLE MATERIAL UNDERLIES THE BIKE TRAIL, SHOULDERS AND/OR EMBANKMENT, AS DETERMINED BY THE ENGINEER, THE SUBGRADE TREATMENT WILL CONSIST OF EXCAVATION OF SUCH UNSUITABLE MATERIAL TO A DEPTH 12" BELOW AGGREGATE BASE COURSE AND PLACEMENT OF 12" OF AGGREGATE SUBGRADE IMPROVEMENT AND A GEOTECHNICAL FABRIC FOR GROUND STABILIZATION.

HMA MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	USAGE	OMP
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"	4% @ 50 GYR	BIKE TRAIL SURFACE	QC/OA
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 3/4"	4% @ 70 GYR	HALSTED ST. PAVEMENT RESTORATION AT THE MEDIAN	QC/OA
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"	4% @ 70 GYR	HALSTED ST. PAVEMENT RESTORATION AT THE MEDIAN	QC/OA
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 3/4"	4% @ 70 GYR	HALSTED ST. PCC APPROACH PAVEMENT SURFACE	QC/OA
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES: HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 2 1/2"	4% @ 70 GYR	HALSTED ST. SURFACE OVER CLASS B PATCHES	QC/OA
HMA SHOULDERS 6": HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 6"	4% @ 70 GYR	HALSTED ST. SHOULDER TREATMENT AT TRAFFIC BARRIER TERMINAL TYPE 1 (SPECIAL) TANGENT	QC/OA

OMP DESIGNATIONS:
 QUALITY CONTROL/QUALITY ASSURANCE (QC/OA),
 QUALITY CONTROL FOR PERFORMANCE (QCP), OR PAY FOR PERFORMANCE (PFP)

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

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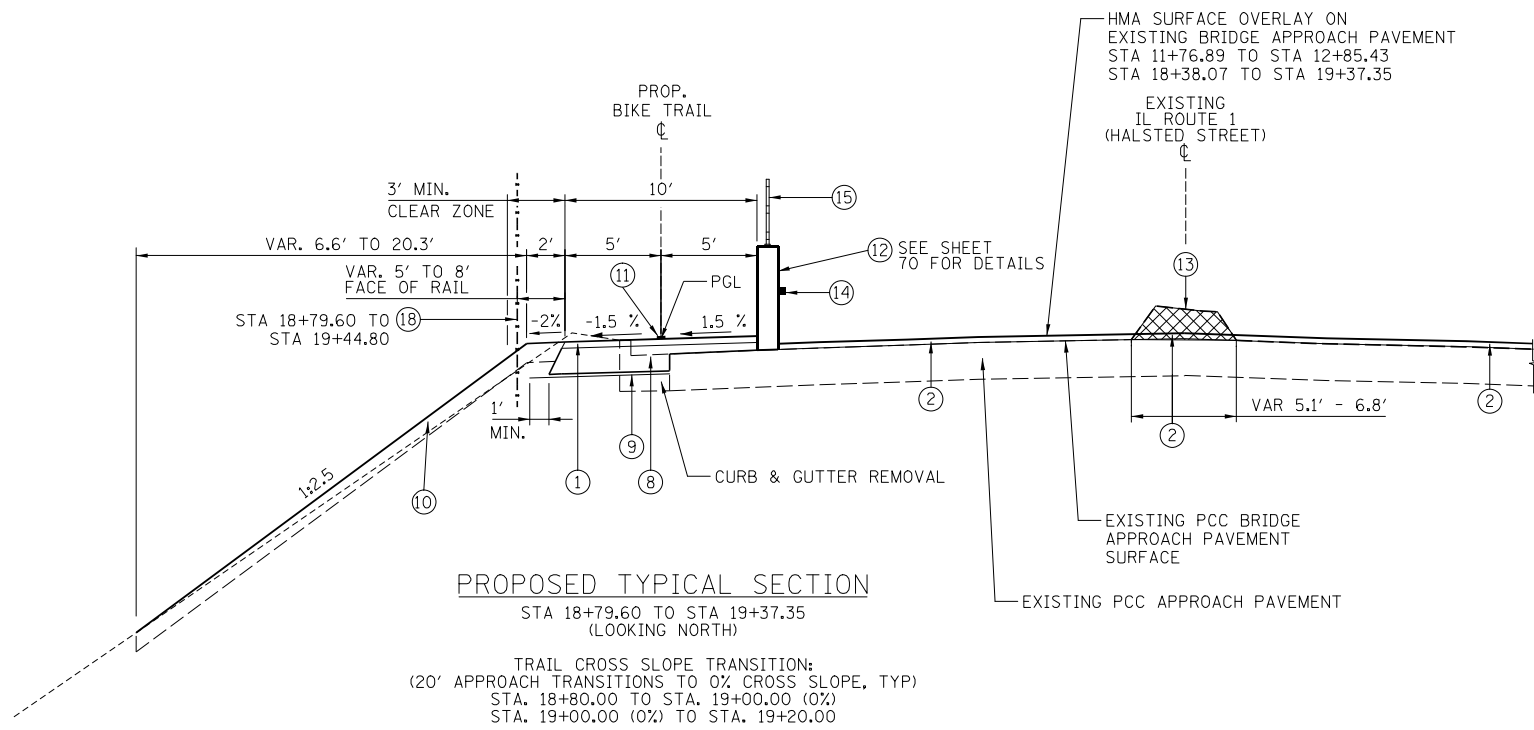
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PLOT SCALE = 100.0000' / 1"	DRAWN - KLM	REVISED -
PLOT DATE = 4/2/2024	CHECKED - BMC	REVISED -
	DATE - 04/02/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HALSTED STREET BIKE TRAIL (STRUCTURE NO. 016-0194)
PROPOSED TYPICAL SECTIONS

SHEET NO. 1 OF 2 SHEETS STA. TO STA.

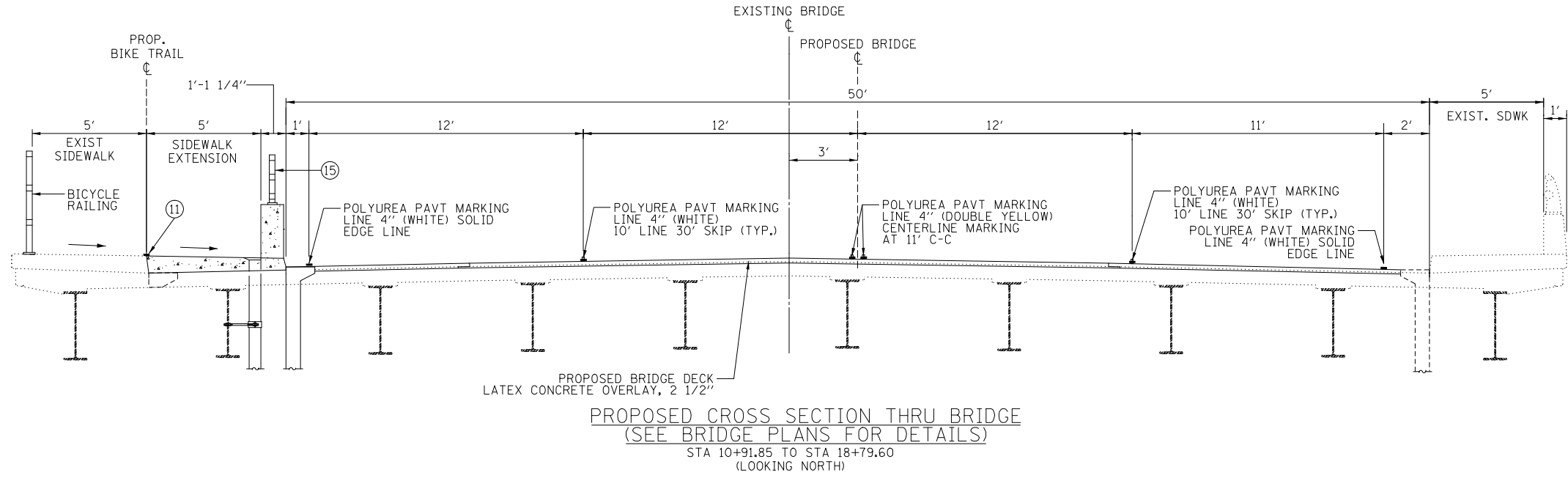
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	14
			CONTRACT NO. 62W48	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAIL CROSS SLOPE TRANSITION:
 (20' APPROACH TRANSITIONS TO 0% CROSS SLOPE, TYP)
 STA. 18+80.00 TO STA. 19+00.00 (0%)
 STA. 19+00.00 (0%) TO STA. 19+20.00

- LEGEND:**
- ① HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 2"
 - ② HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 3/4"
 - ③ HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
 - ④ NOT USED
 - ⑤ TOPSOIL FURNISH AND PLACE, 6", AND SEEDING CLASS 2A
 - ⑥ HOT-MIX ASPHALT SHOULDERS, 6"
 - ⑦ NOT USED
 - ⑧ AGGREGATE BASE COURSE, TYPE B, 8"
 - ⑨ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - ⑩ TOPSOIL FURNISH AND PLACE, 6", EROSION CONTROL BLANKET (SPECIAL), AND SEEDING, CLASS 4A (MODIFIED)
 - ⑪ PAINT PAVEMENT MARKING, LINE 4" YELLOW
 - ⑫ CONCRETE BARRIER WALL (SPECIAL)
 - ⑬ MEDIAN REMOVAL PARTIAL DEPTH
 - ⑭ BARRIER WALL REFLECTORS, TYPE C (CRYSTAL)
 - ⑮ PARAPET RAILING
 - ⑯ TRAFFIC BARRIER TERMINAL, TYPE 6
 - ⑰ TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT
 - ⑱ RUSTIC RAIL FENCE

NOTE:
 WHERE UNSUITABLE MATERIAL UNDERLIES THE BIKE TRAIL, SHOULDERS AND/OR EMBANKMENT, AS DETERMINED BY THE ENGINEER, THE SUBGRADE TREATMENT WILL CONSIST OF EXCAVATION OF SUCH UNSUITABLE MATERIAL TO A DEPTH 12" BELOW AGGREGATE BASE COURSE AND PLACEMENT OF 12" OF AGGREGATE SUBGRADE IMPROVEMENT AND A GEOTECHNICAL FABRIC FOR GROUND STABILIZATION.



PROPOSED CROSS SECTION THRU BRIDGE
 (SEE BRIDGE PLANS FOR DETAILS)
 STA 10+91.85 TO STA 18+79.60
 (LOOKING NORTH)

EARTHWORK SCHEDULE											
STATION	TO	STATION	EARTH EXCAVATION CU YD	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (15%) CU YD	EMBANKMENT CU YD	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) FURNISHED EXCAVATION CU YD	UNSUITABLE EXCAVATION (REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL) CU YD	UNSUITABLE MATERIAL ADJUSTED FOR SHRINKAGE (25%) CU YD	TOPSOIL FURNISH AND PLACE, 6" CU YD	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (UNSUITABLE MATERIAL) CU YD	
											CU YD
07+22.09		10+91.83	7	6	186	-180	216	162	101	61	
18+79.60		22+73.78	23	20	214	-194	239	179	104	75	
TOTAL:			30	26	400	-375	455	341	205	136	

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 PLOT DATE = 4/2/2024

DESIGNED - KLM
 DRAWN - KLM
 CHECKED - BMC
 DATE - 04/02/2024

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

HALSTED STREET BIKE TRAIL (STRUCTURE NO. 016-0194)
 PROPOSED TYPICAL SECTIONS

SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	15
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

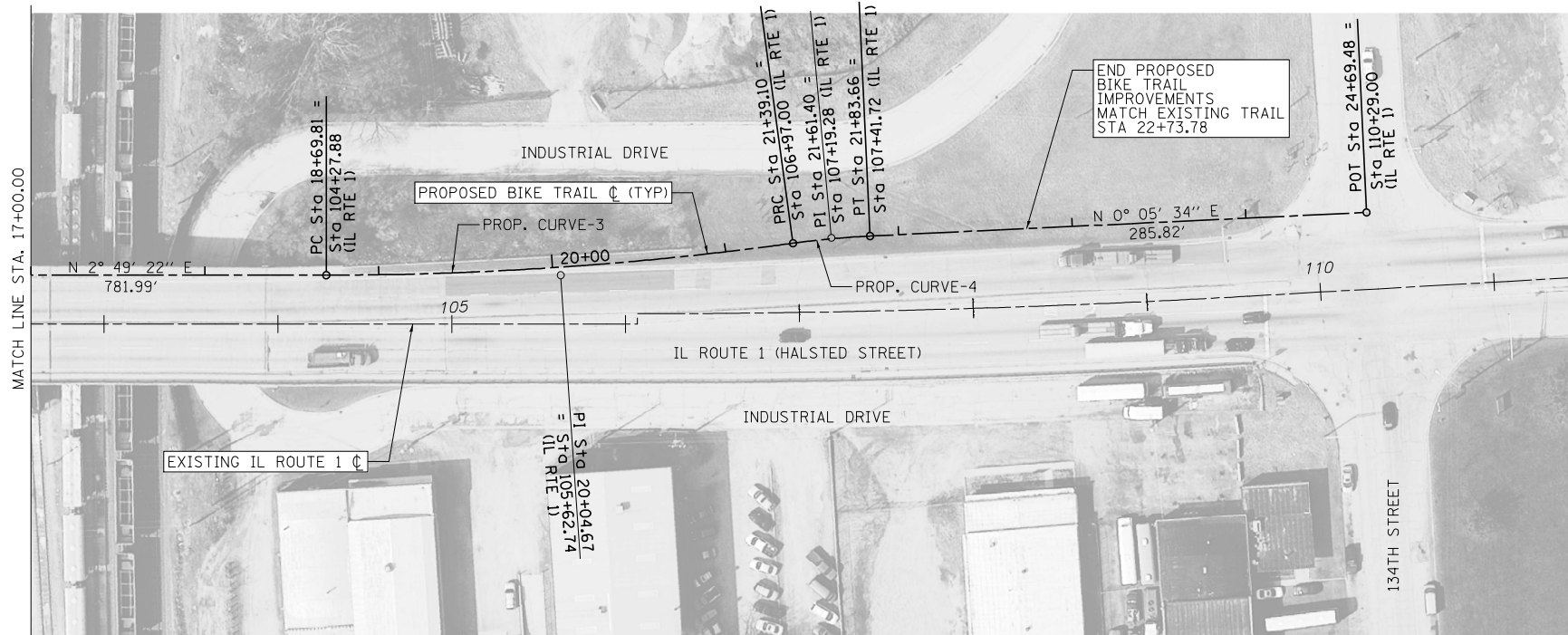
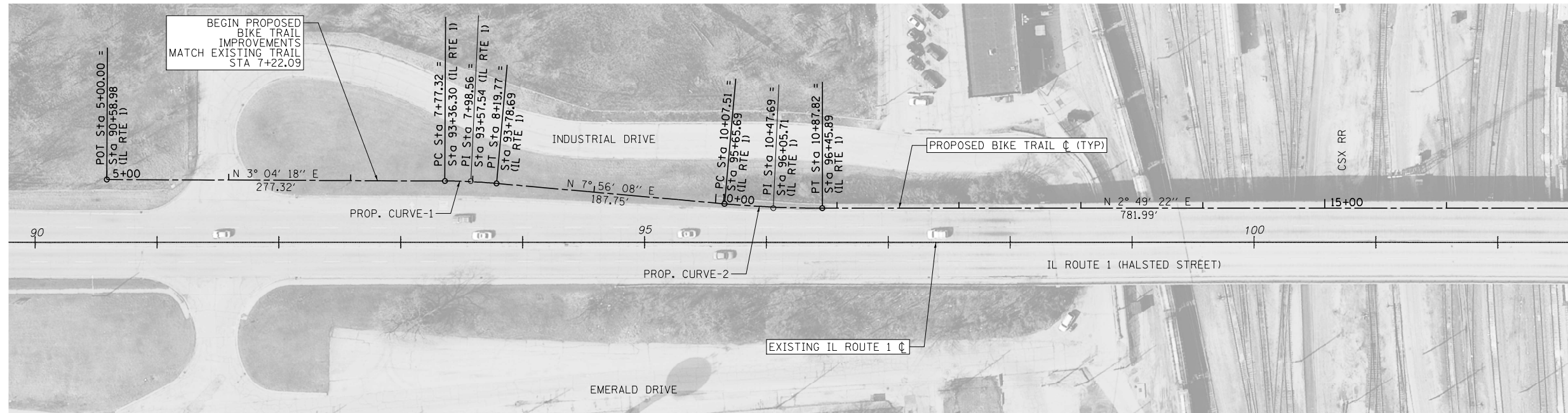
PROPOSED BIKE TRAIL CURVE DATA

PROP. CURVE-1
 PI STA. = 7+98.56
 $\Delta = 4^\circ 51' 50''$ (RT)
 $D = 11^\circ 27' 33''$
 $R = 500.00'$
 $T = 21.24'$
 $L = 42.45'$
 $E = 0.45'$
 P.C. STA. = 7+77.32
 P.T. STA. = 8+19.77

PROP. CURVE-2
 PI STA. = 10+47.69
 $\Delta = 5^\circ 06' 46''$ (LT)
 $D = 6^\circ 21' 58''$
 $R = 900.00'$
 $T = 40.18'$
 $L = 80.31'$
 $E = 0.90'$
 P.C. STA. = 10+07.51
 P.T. STA. = 10+87.82

PROP. CURVE-3
 PI STA. = 20+04.67
 $\Delta = 7^\circ 50' 11''$ (LT)
 $D = 2^\circ 54' 36''$
 $R = 1,968.90'$
 $T = 134.86'$
 $L = 269.29'$
 $E = 4.61'$
 P.C. STA. = 18+69.81
 P.R.C. STA. = 21+39.10

PROP. CURVE-4
 PI STA. = 21+61.40
 $\Delta = 5^\circ 06' 23''$ (RT)
 $D = 11^\circ 27' 33''$
 $R = 500.00'$
 $T = 22.30'$
 $L = 44.56'$
 $E = 0.50'$
 P.R.C. STA. = 21+39.10
 P.T. STA. = 21+83.66



ALIGNMENT COORDINATES PROPOSED BIKE TRAIL				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		5+00.00	1,814,501.08	1,173,315.37
PC	CURVE-1	7+77.32	1,814,778.01	1,173,330.23
PI	CURVE-1	7+98.56	1,814,799.21	1,173,331.37
PT	CURVE-1	8+19.77	1,814,820.24	1,173,334.30
PC	CURVE-2	10+07.51	1,815,006.19	1,173,360.23
PI	CURVE-2	10+47.69	1,815,045.99	1,173,365.77
PT	CURVE-2	10+87.82	1,815,086.12	1,173,367.75
PC	CURVE-3	18+69.81	1,815,867.16	1,173,406.26
PI	CURVE-3	20+04.67	1,816,001.85	1,173,412.91
PRC	CURVE-3	21+39.10	1,816,136.19	1,173,401.12
PRC	CURVE-4	21+39.10	1,816,136.19	1,173,401.12
PI	CURVE-4	21+61.40	1,816,158.40	1,173,399.17
PT	CURVE-4	21+83.66	1,816,180.70	1,173,399.17
POT		24+69.48	1,816,466.52	1,173,399.67



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USER NAME = ken.moy
 PLOT SCALE = 100.0000' / 1" =
 PLOT DATE = 4/2/2024

DESIGNED - KLM
 DRAWN - KLM
 CHECKED - BMC
 DATE - 04/02/2024

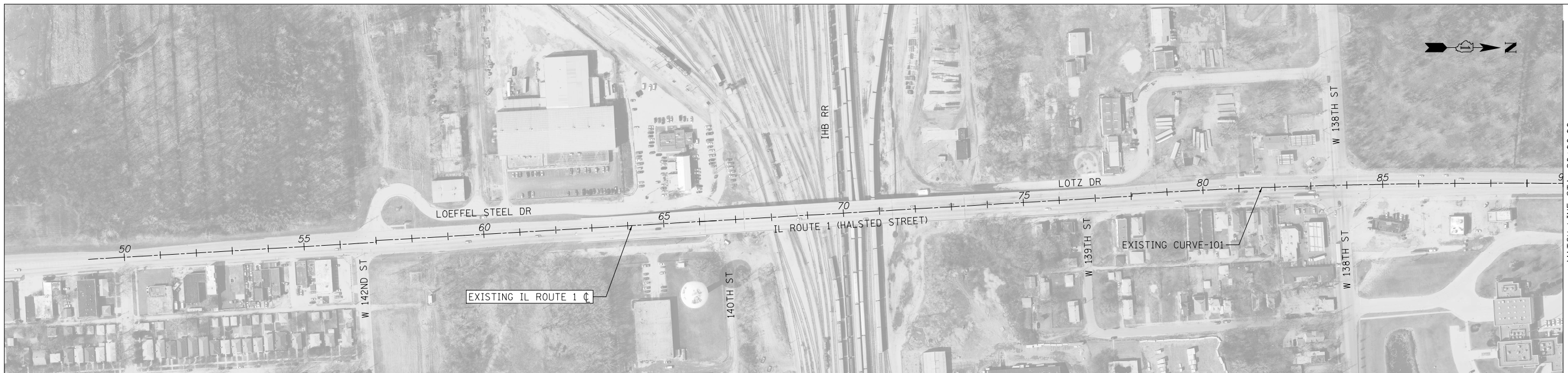
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ALIGNMENT AND TIES
 STRUCTURE NO. 016-0194

SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	16
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 90+00.00



MATCH LINE STA. 90+00.00

ALIGNMENT COORDINATES EXISTING HALSTED ST (IL ROUTE 1)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		49+00.00	1,810,340.29	1,173,373.00
POT		57+00.00	1,811,140.25	1,173,365.39
POT		57+00.00	1,811,140.20	1,173,360.59
POT		78+02.44	1,813,242.55	1,173,340.59
PC	CURVE-101	78+02.44	1,813,242.50	1,173,334.59
PI	CURVE-101	83+08.28	1,813,748.45	1,173,329.78
PT	CURVE-101	88+13.96	1,814,253.82	1,173,354.74
POT		100+00.00	1,815,438.42	1,173,413.24
POT		106+06.92	1,816,044.60	1,173,443.18
PC	CURVE-102	106+06.92	1,816,044.89	1,173,437.19
PI	CURVE-102	107+83.03	1,816,220.76	1,173,445.87
PT	CURVE-102	109+58.99	1,816,396.84	1,173,444.48
POT		110+00.00	1,816,437.85	1,173,444.15
PC	CURVE-103	112+57.83	1,816,695.67	1,173,442.11
PI	CURVE-103	114+08.19	1,816,846.04	1,173,440.92
PT	CURVE-103	115+59.57	1,816,996.32	1,173,435.79
POT		121+00.00	1,817,536.43	1,173,417.33

NOTE: THE ABOVE DATA IS FOR INFORMATION ONLY.
THE CONTRACTOR MUST FIELD VERIFY.



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 PLOT DATE = 4/2/2024

DESIGNED - KLM
 DRAWN - KLM
 CHECKED - BMC
 DATE - 04/02/2024

REVISED -
 REVISED -
 REVISED -
 REVISED -


STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ALIGNMENT AND TIES
 EX HALSTED ST (IL RT 1)

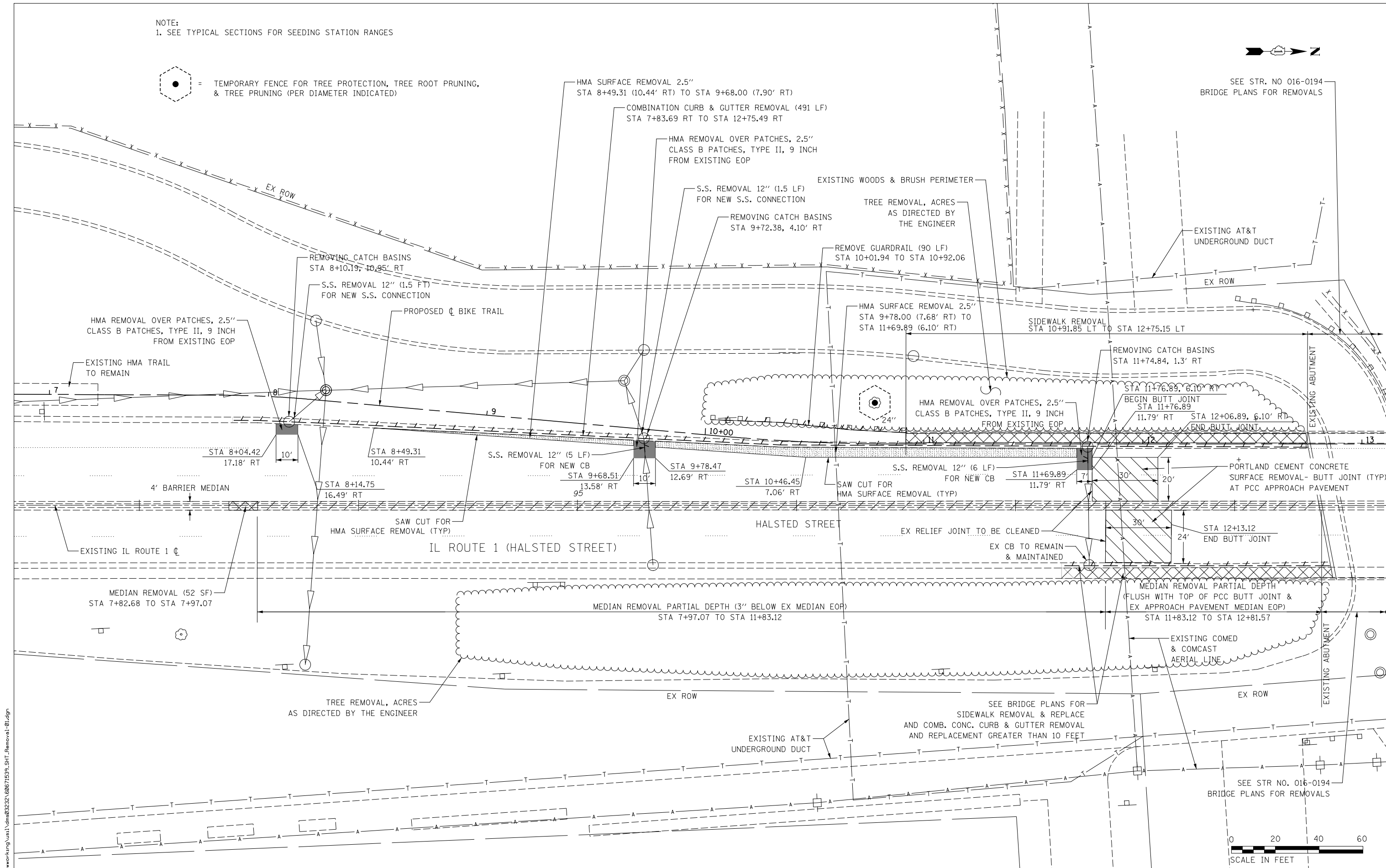
SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	17
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

NOTE:
1. SEE TYPICAL SECTIONS FOR SEEDING STATION RANGES

 = TEMPORARY FENCE FOR TREE PROTECTION, TREE ROOT PRUNING, & TREE PRUNING (PER DIAMETER INDICATED)

SEE STR. NO 016-0194
BRIDGE PLANS FOR REMOVALS



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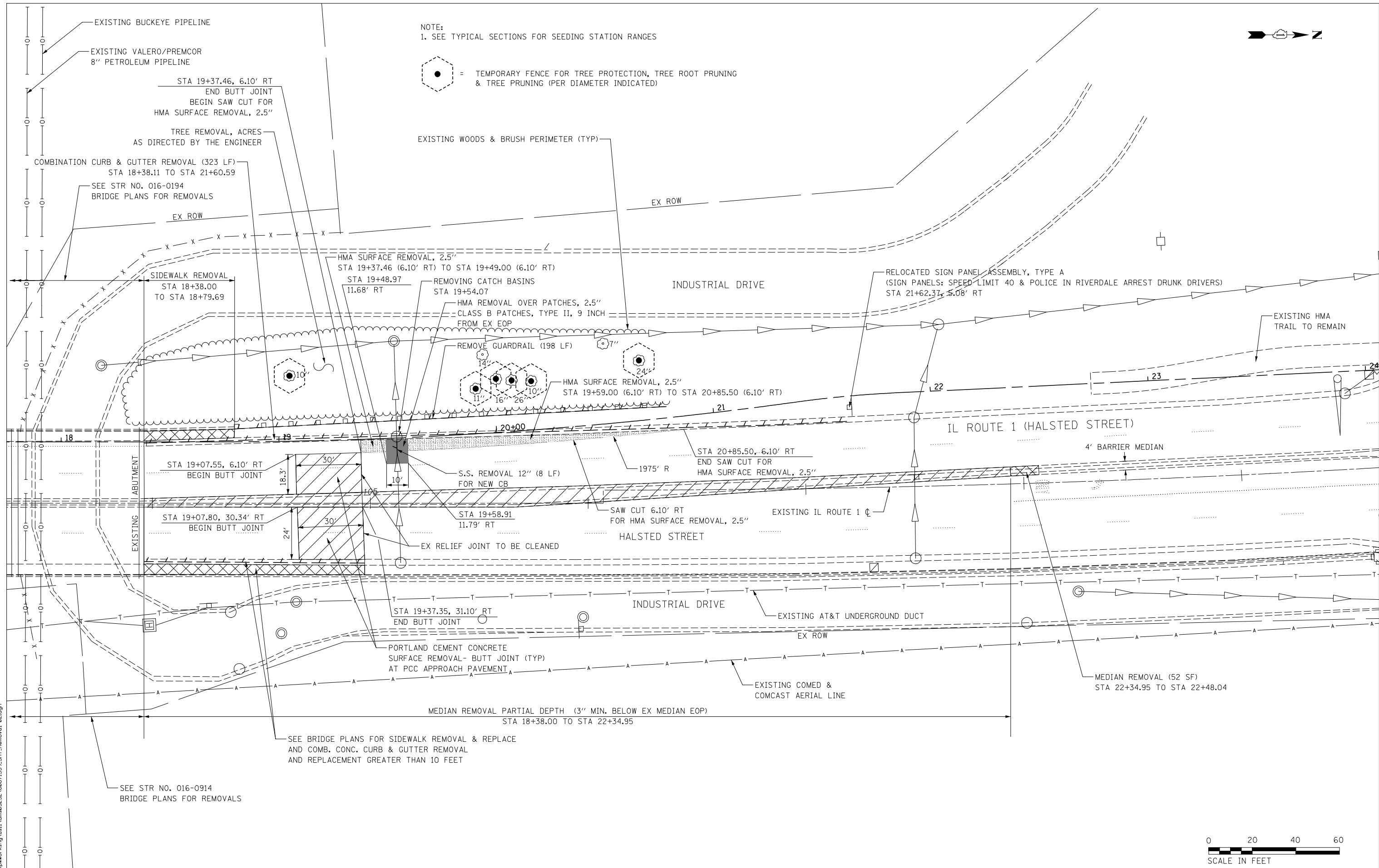
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PLOT DATE = 4/2/2024	CHECKED - BMC	REVISED -
	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY REMOVAL PLANS
STRUCTURE NO. 016-0194**

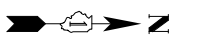
SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	18
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTE:
 1. SEE TYPICAL SECTIONS FOR SEEDING STATION RANGES

= TEMPORARY FENCE FOR TREE PROTECTION, TREE ROOT PRUNING & TREE PRUNING (PER DIAMETER INDICATED)



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USER NAME = ken.moy
 DESIGNED - KLM
 DRAWN - KLM
 CHECKED - BMC
 DATE - 04/02/2024

REVISIONS:
 REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY REMOVAL PLANS
 STRUCTURE NO. 016-0194**

SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	19
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SEE BRIDGE PLANS FOR SIDEWALK REMOVAL & REPLACE AND COMB. CONC. CURB & GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET

SEE STR NO. 016-0914 BRIDGE PLANS FOR REMOVALS

STA 19+07.55, 6.10' RT
 BEGIN BUTT JOINT

STA 19+07.80, 30.34' RT
 BEGIN BUTT JOINT

STA 19+37.35, 31.10' RT
 END BUTT JOINT

HMA SURFACE REMOVAL, 2.5"
 STA 19+37.46 (6.10' RT) TO STA 19+49.00 (6.10' RT)

REMOVING CATCH BASINS
 STA 19+54.07

REMOVE GUARDRAIL (198 LF)

HMA SURFACE REMOVAL, 2.5"
 STA 19+59.00 (6.10' RT) TO STA 20+85.50 (6.10' RT)

STA 20+85.50, 6.10' RT
 END SAW CUT FOR
 HMA SURFACE REMOVAL, 2.5"

SAW CUT 6.10' RT
 FOR HMA SURFACE REMOVAL, 2.5"

PORTLAND CEMENT CONCRETE
 SURFACE REMOVAL- BUTT JOINT (TYP)
 AT PCC APPROACH PAVEMENT

MEDIAN REMOVAL PARTIAL DEPTH (3" MIN. BELOW EX MEDIAN EOP)
 STA 18+38.00 TO STA 22+34.95

RELOCATED SIGN PANEL ASSEMBLY, TYPE A
 (SIGN PANELS: SPEED LIMIT 40 & POLICE IN RIVERDALE ARREST DRUNK DRIVERS)
 STA 21+62.37, 5.08' RT

4' BARRIER MEDIAN

MEDIAN REMOVAL (52 SF)
 STA 22+34.95 TO STA 22+48.04

EXISTING HMA TRAIL TO REMAIN

HALSTED STREET

IL ROUTE 1 (HALSTED STREET)

INDUSTRIAL DRIVE

INDUSTRIAL DRIVE

COMBINATION CURB & GUTTER REMOVAL (323 LF)
 STA 18+38.11 TO STA 21+60.59

SEE STR NO. 016-0194
 BRIDGE PLANS FOR REMOVALS

TREE REMOVAL, ACRES
 AS DIRECTED BY THE ENGINEER

STA 19+37.46, 6.10' RT
 END BUTT JOINT
 BEGIN SAW CUT FOR
 HMA SURFACE REMOVAL, 2.5"

EXISTING VALERO/PREMCOR
 8" PETROLEUM PIPELINE

EXISTING BUCKEYE PIPELINE

EXISTING WOODS & BRUSH PERIMETER (TYP)

EX ROW

EX ROW

SIDEWALK REMOVAL
 STA 18+38.00
 TO STA 18+79.69

S.S. REMOVAL 12" (8 LF)
 FOR NEW CB

STA 19+58.91
 11.79' RT

EX RELIEF JOINT TO BE CLEANED

1975' R

EXISTING IL ROUTE 1

EXISTING AT&T UNDERGROUND DUCT

EXISTING COMED &
 COMCAST AERIAL LINE

EX ROW

EXISTING ABUTMENT

EXISTING

EXISTING

EXISTING

EXISTING

EXISTING

EXISTING

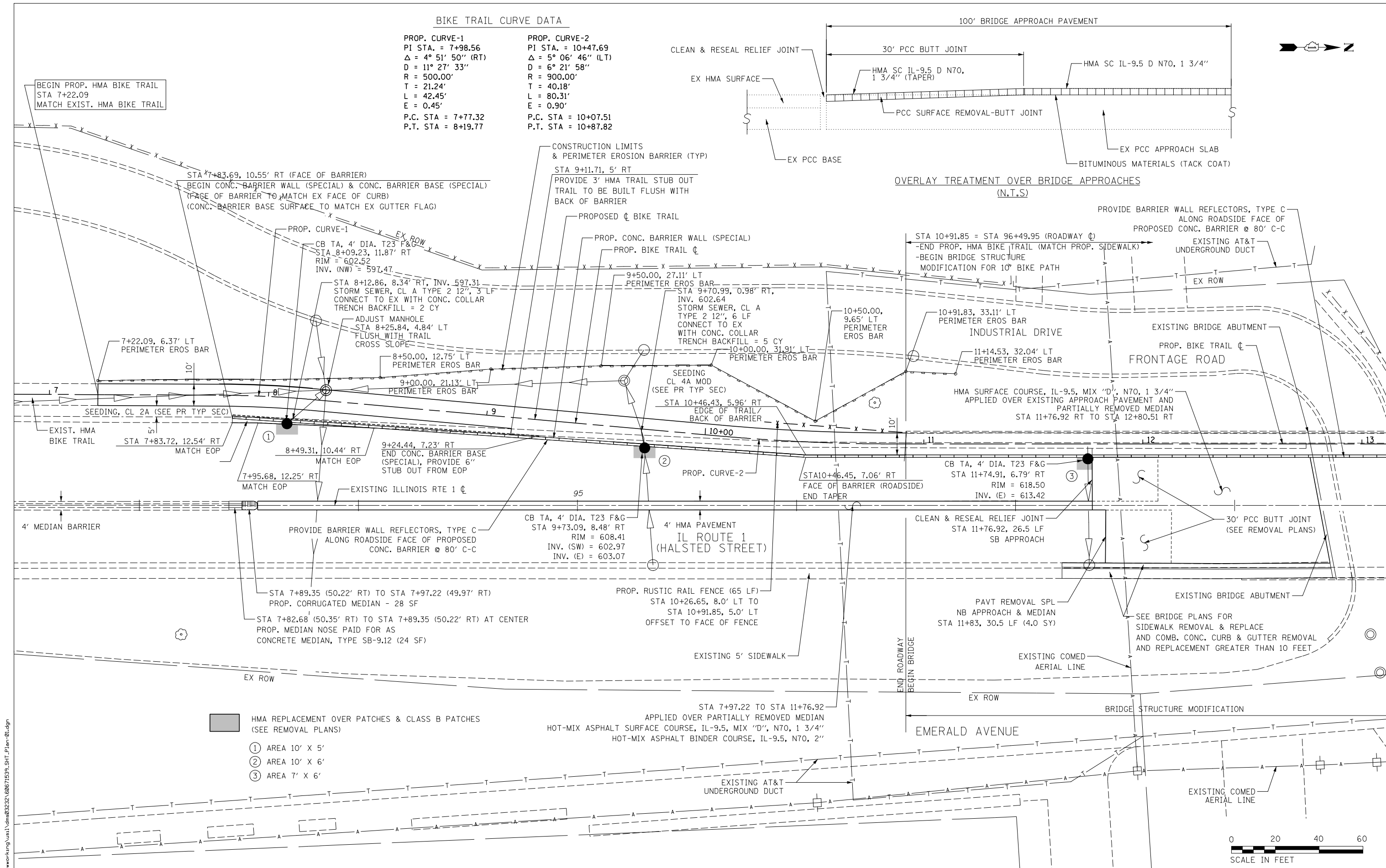
EXISTING

EXISTING

EXISTING

BIKE TRAIL CURVE DATA

PROP. CURVE-1	PROP. CURVE-2
PI STA. = 7+98.56	PI STA. = 10+47.69
Δ = 4° 51' 50" (RT)	Δ = 5° 06' 46" (LT)
D = 11° 27' 33"	D = 6° 21' 58"
R = 500.00'	R = 900.00'
T = 21.24'	T = 40.18'
L = 42.45'	L = 80.31'
E = 0.45'	E = 0.90'
P.C. STA = 7+77.32	P.C. STA = 10+07.51
P.T. STA = 8+19.77	P.T. STA = 10+87.82



- HMA REPLACEMENT OVER PATCHES & CLASS B PATCHES (SEE REMOVAL PLANS)
- ① AREA 10' X 5'
- ② AREA 10' X 6'
- ③ AREA 7' X 6'



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 PLOT SCALE = 48.0000' / in.
 PLOT DATE = 4/2/2024

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DRAWN - KLM	REVISED -
CHECKED - BMC	REVISED -
DATE - 04/02/2024	REVISED -

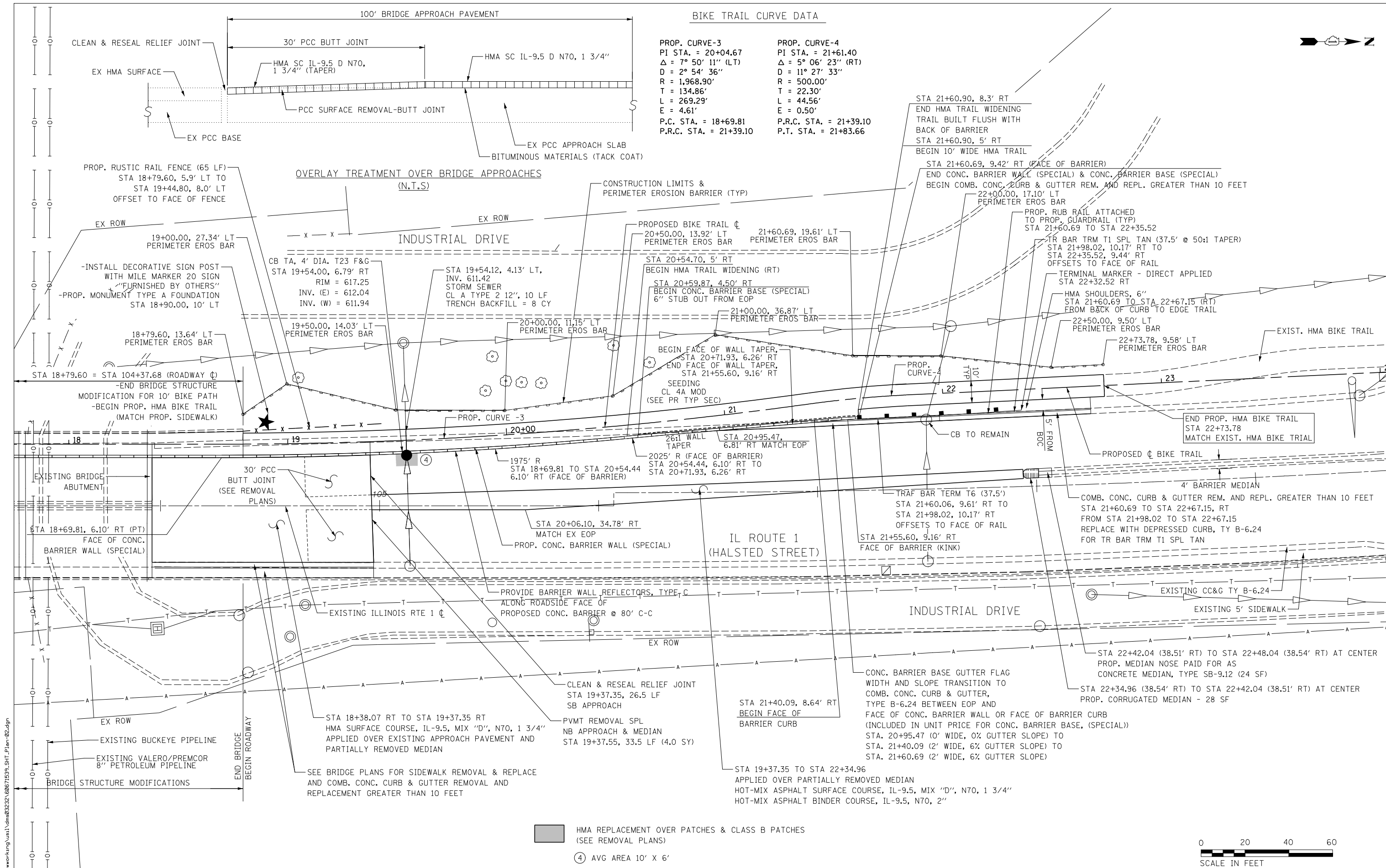
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED ROADWAY & BIKE TRAIL PLANS
STRUCTURE NO. 016-0194

SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	20

CONTRACT NO. 62W48
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



BIKE TRAIL CURVE DATA

PROP. CURVE-3	PROP. CURVE-4
PI STA. = 20+04.67	PI STA. = 21+61.40
$\Delta = 7^\circ 50' 11''$ (LT)	$\Delta = 5^\circ 06' 23''$ (RT)
D = 2° 54' 36"	D = 11° 27' 33"
R = 1,968.90'	R = 500.00'
T = 134.86'	T = 22.30'
L = 269.29'	L = 44.56'
E = 4.61'	E = 0.50'
P.C. STA. = 18+69.81	P.R.C. STA. = 21+39.10
P.R.C. STA. = 21+39.10	P.T. STA. = 21+83.66

HMA REPLACEMENT OVER PATCHES & CLASS B PATCHES (SEE REMOVAL PLANS)
4 AVG AREA 10' X 6'



USER NAME = ken.moy	DESIGNED - KLM	REVISED -
PLOT SCALE = 48.0000' / in.	DRAWN - KLM	REVISED -
PLOT DATE = 4/2/2024	CHECKED - BMC	REVISED -
	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED ROADWAY & BIKE TRAIL PLANS
STRUCTURE NO. 016-0194**

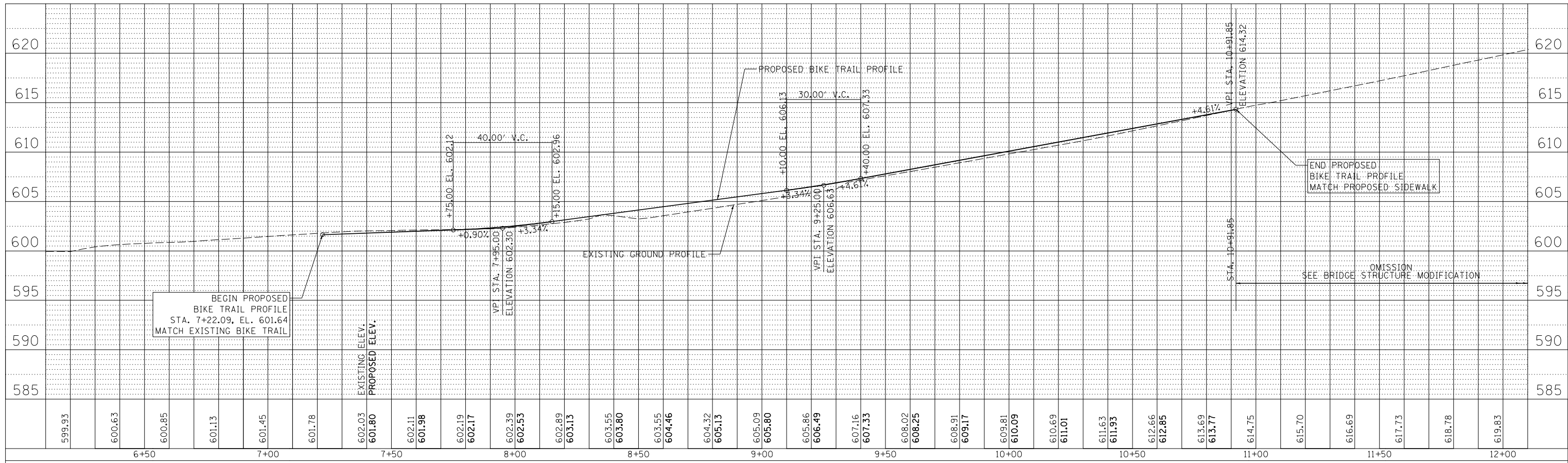
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	21

CONTRACT NO. 62W48
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

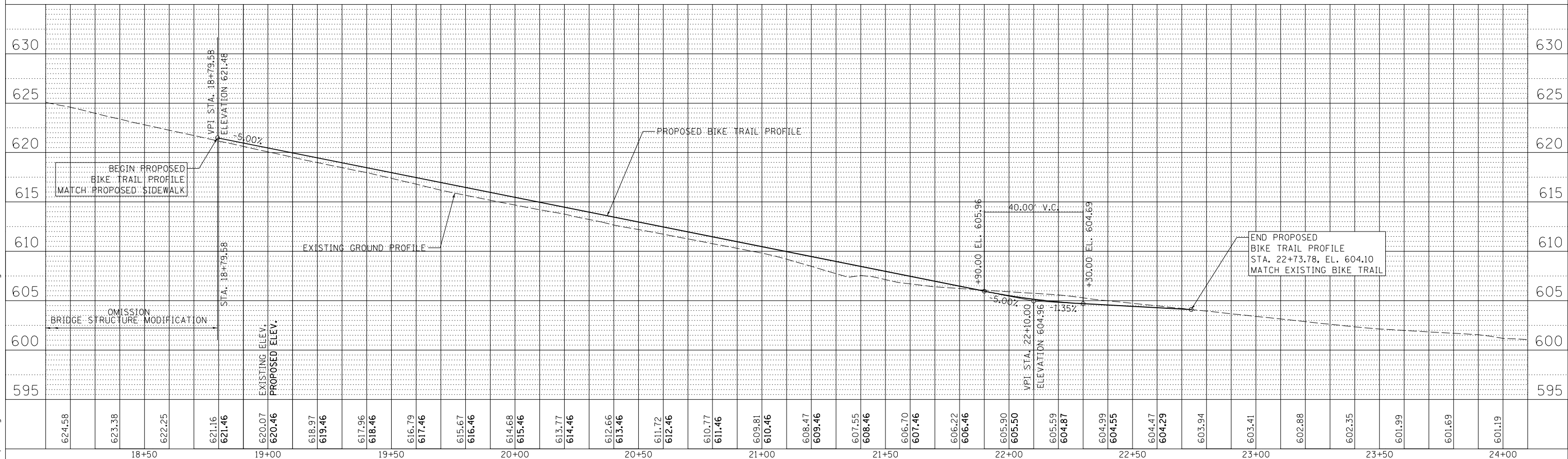
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PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	RT. OF WAY		
	NO.		
	PAID FILE NAME		



BIKE TRAIL PROFILE

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	STRUCTURE		
	NOTATIONS		
	NO.		



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED HALSTED STREET BIKE TRAIL PROFILE
STRUCTURE NO. 016-0194



USER NAME = ken.moy
PLOT SCALE = 48.0000' / in.
PLOT DATE = 4/2/2024

DESIGNED - KLM
DRAWN - KLM
CHECKED - BMC
DATE - 04/02/2024

REVISED -
REVISED -
REVISED -
REVISED -

SCALE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR		109	22
CONTRACT NO.			62W48	
ILLINOIS FED. AID PROJECT				

FILE NAME = c:\pwworking\ust\1\dm0322\60671535_SHT_P_Profile-01.dgn

MOT GENERAL NOTES:

1. UNLESS OTHERWISE NOTED IN THE SPECIAL PROVISIONS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER 28 DAYS PRIOR TO ANY ANTICIPATED CLOSURES.
2. EXACT LOCATION OF ALL WARNING SIGNS AND BARRICADES SHALL BE STAKED IN THE FIELD FOR APPROVAL BY THE ENGINEER PRIOR TO INSTALLATION.
3. THE CONTRACTOR SHALL VERIFY LOCATION OF ALL BUILDING ACCESS, COORDINATE WITH BUILDING OWNERS AND LOCAL AUTHORITIES AND PROVIDE FULL ACCESS TO BUSINESSES OR PROPERTIES DURING THEIR NORMAL WORKING HOURS IN ACCORDANCE WITH ADA AND APPLICABLE CODE REQUIREMENTS.
4. THE CONTRACTOR SHALL MAINTAIN TRAFFIC ON ALL STREETS EXCEPT WHERE NOTED ON THE PLANS AND PROHIBIT PARKING WITHIN FIFTY (50) FEET OF THE CONSTRUCTION AREA AT ALL TIMES.
5. PROVIDE CONTINUOUS TEMPORARY ACCESS TO ALL SIDE STREETS, ALLEYS, DRIVEWAYS, AND PARKING LOTS UNLESS SPECIFICALLY IDENTIFIED ON THE PLANS FOR TEMPORARY CLOSURE. LOTS WITH MORE THAN ONE DRIVEWAY MUST BE STAGED TO KEEP AT LEAST ONE DRIVEWAY OPEN AT ALL TIMES.
6. MAINTAIN ACCESS TO FIRE HYDRANTS, BUILDING STANDPIPES AND OTHER EMERGENCY FACILITIES WITHIN THE CONSTRUCTION ZONE.
7. SIGNS W21-1 AND W20-7 SHALL BE TAKEN DOWN OR COVERED WHEN THE WORKERS ARE NOT PRESENT FOR MORE THAN 1 HOUR.
8. ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE PAVEMENT MARKING TAPE, TYPE IV (AND/OR MODIFIED URETHANE) USED FOR STAGING SHALL BE REMOVED. THIS WORK SHALL BE PAID FOR AS "PAVEMENT MARKING REMOVAL-WATER BLASTING".
9. ALL EXISTING RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS LOCATED WITHIN TEMPORARY LANE CLOSURE TAPERS, LANE SHIFT TAPERS OR IN LOCATIONS THAT CONFLICT WITH THE TEMPORARY PAVEMENT MARKING TAPE USED FOR STAGING SHALL BE REMOVED. THE EXISTING RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS THAT WERE REMOVED SHALL BE RESTORED AFTER THE COMPLETION OF THE STAGING UNLESS OTHERWISE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER.
10. PAVEMENT MARKING TAPE, TYPE IV SHOWN ON THE PLANS FOR ANY CONSTRUCTION STAGE THAT THE CONTRACTOR PROPOSES TO EXTEND OVER THE WINTER PERIOD SHALL MEAN MODIFIED URETHANE PAVEMENT MARKING AND WILL BE PAID FOR THEIR RESPECTIVE CONTRACT UNIT PRICE.
11. THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
12. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL TRAFFIC STAGE CHANGES ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGN MESSAGES SHALL BE REVISED TWO WEEKS THEREAFTER WITH MESSAGES WARNING TRAFFIC OF POTENTIAL TRAFFIC DELAYS, QUEUING AND/OR WITH MESSAGES NOTIFYING TRAFFIC TO USE ALTERNATE ROUTES. THE SIGN LOCATIONS AND MESSAGES SHALL BE DETERMINED BY THE ENGINEER.
13. TEMPORARY CONCRETE BARRIER SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 704 OF THE IDOT STANDARD SPECIFICATIONS. ALL TEMPORARY CONCRETE BARRIER APPROACH AND DEPARTING END UNITS SHALL BE ANCHORED TO THE PAVEMENT USING SIX ANCHOR PINS AS SHOWN IN IDOT STANDARD 704001. PINNING OF ADDITIONAL BARRIER UNITS WITH THREE ANCHOR PINS ON THE TRAFFIC SIDE HOLES WITHIN THE INSTALLATION SHALL BE REQUIRED WHEN EQUIPMENT, VEHICLES, MATERIALS, FIXED OBJECTS, OR A DROP-OFF IS LOCATED WITHIN 24" BEHIND THE BARRIER. THE 24" OF CLEAR PAVEMENT MEASUREMENT SHALL BE FROM THE BASE OF THE NON-TRAFFIC SIDE OF THE BARRIER. TRAFFIC SIDE PINNED BARRIER SHALL HAVE A MINIMUM OF 6" OF CLEAR PAVEMENT BEHIND THE BARRIER. WHERE BOTH PINNED AND UNPINNED BARRIER UNITS ARE USED IN A CONTINUOUS INSTALLATION, A TRANSITION SHALL BE PROVIDED BETWEEN THEM. THE TRANSITION FROM PINNED TO UNPINNED BARRIER SHALL CONSIST OF TWO ANCHOR PINS INSTALLED IN THE END HOLES ON THE TRAFFIC SIDE OF THE FIRST BARRIER BEYOND THE PINNED SECTION AND ONE ANCHOR PIN INSTALLED IN THE MIDDLE HOLE OF THE TRAFFIC SIDE OF THE SECOND BARRIER BEYOND THE PINNED SECTION. THE THIRD BARRIER BEYOND THE PINNED SECTION SHALL THEN BE UNPINNED.
14. ONE SIDE OF THE BRIDGE SIDEWALK SHALL BE KEPT ACCESSIBLE TO PEDESTRIANS AT ALL TIMES.

SEQUENCE OF CONSTRUCTION NOTES:

1. THE WORK AT EACH BRIDGE SHALL BE CONDUCTED IN SEPARATE STAGES AS INDICATED IN THE SUGGESTED MAINTENANCE OF TRAFFIC PLANS.
2. WORK ASSOCIATED WITH BRIDGE SN 016-0194 BETWEEN 134TH STREET (NORTH LIMIT) AND 138TH STREET (SOUTH LIMIT) SHALL BE COMPLETED IN STAGES 1A AND 1B BEFORE STARTING THE WORK ASSOCIATED WITH BRIDGE SN 016-0195 (STAGE 2).
3. WORK ASSOCIATED WITH BRIDGE SN 016-0195 BETWEEN 138TH STREET (NORTH LIMIT) AND 142ND STREET (SOUTH LIMIT) SHALL BE COMPLETED IN STAGES 2A AND 2B.

TEMPORARY TRAFFIC SIGNAL TIMING REQUIRMENTS:

TEMPORARY TRAFFIC SIGNAL TIMING SHALL BE PROVIDED AT THE BELOW INTERSECTIONS ALONG ILLINOIS ROUTE 1 (HALSTED STREET).

INTERSECTION	Z0073510
	TEMPORARY TRAFFIC SIGNAL TIMING
	EACH
IL 1 (HALSTED ST) & 134TH ST	1
IL 1 (HALSTED ST) & 138TH ST	1
TOTAL:	2

MODEL: #MODELNAME\$
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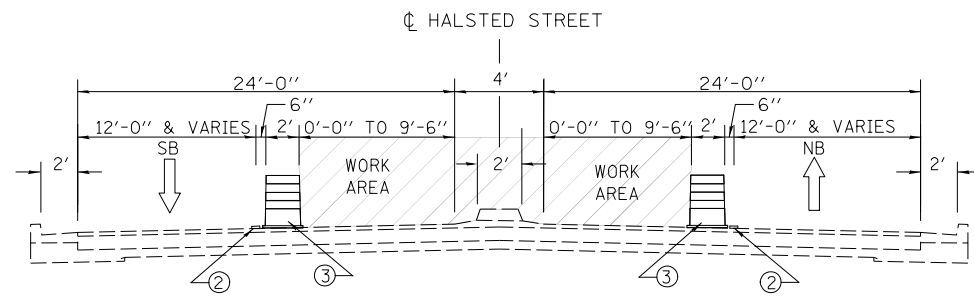
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	DRAWN - KLM	REVISED -
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PLOT DATE = 5/10/2024	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
 GENERAL NOTES**

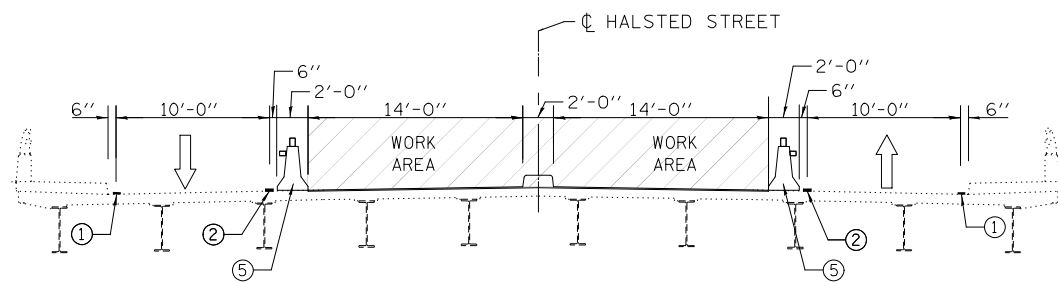
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	23
			CONTRACT NO. 62W48	
ILLINOIS FED. AID PROJECT				



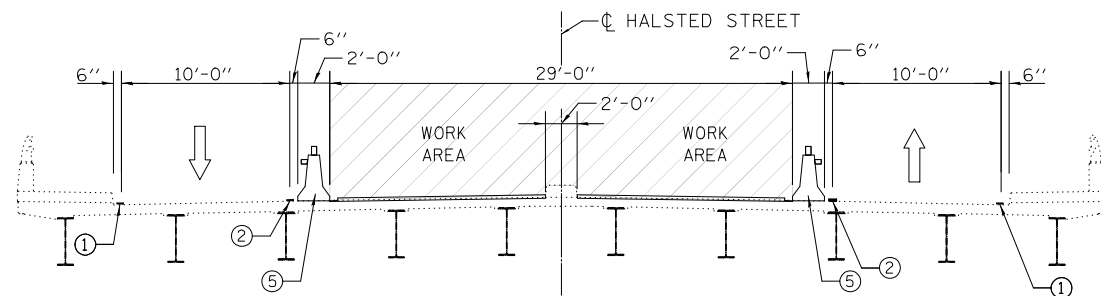
STAGE 1A (SN 0194) & 2A (SN 0195) TYPICAL ROADWAY SECTION

1A: STA 88+36 TO STA 93+44 (STA 7+86 TRAIL)
 1A: STA 108+11 (STA 22+52 TRAIL) TO STA 113+20
 2A: STA 61+10 TO STA 65+10
 2A: STA 74+35 TO STA 78+52



STAGE 1A (SN 0194)

STA 93+44 (STA 7+86 TRAIL) TO STA 108+11 (STA 22+52 TRAIL)

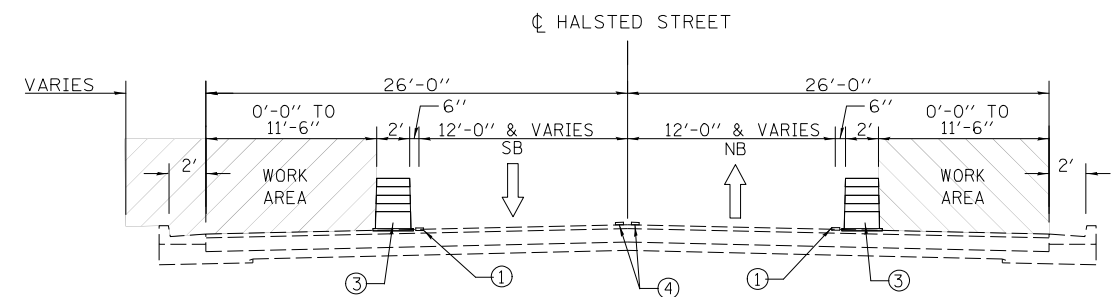


STAGE 2A (SN 0195)

STA 65+10 TO STA 74+35

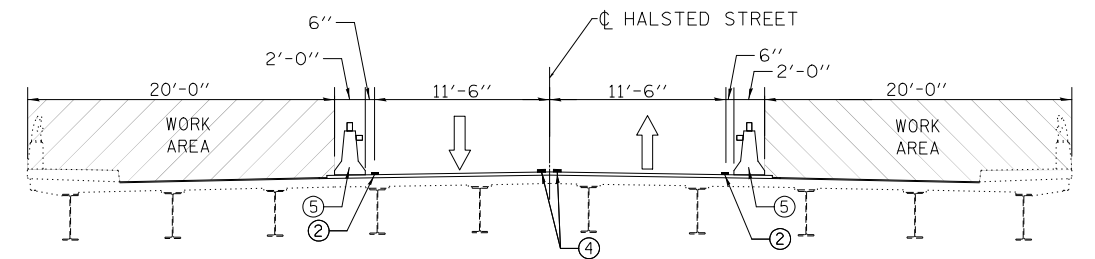
LEGEND:

- ① TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE - WHITE
- ② TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE - YELLOW
- ③ DRUMS/TYPE II BARRICADES OR TEMPORARY CONCRETE BARRIER WALL
- ④ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE - (DOUBLE YELLOW)
- ⑤ TEMPORARY CONC. BARRIER & TYPE C REFLECTORS PER STANDARD 704001



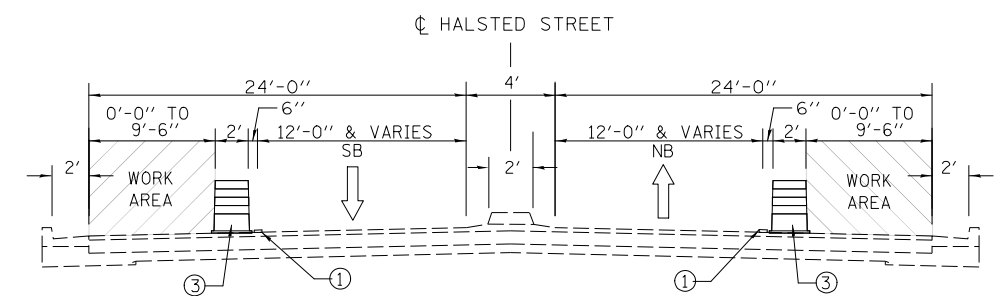
STAGE 1B (SN 0194) TYPICAL ROADWAY SECTION

STA 92+12 TO STA 97+33 (STA 11+67 TRAIL)
 STA 108+35 (STA 22+77 TRAIL) TO STA 115+20



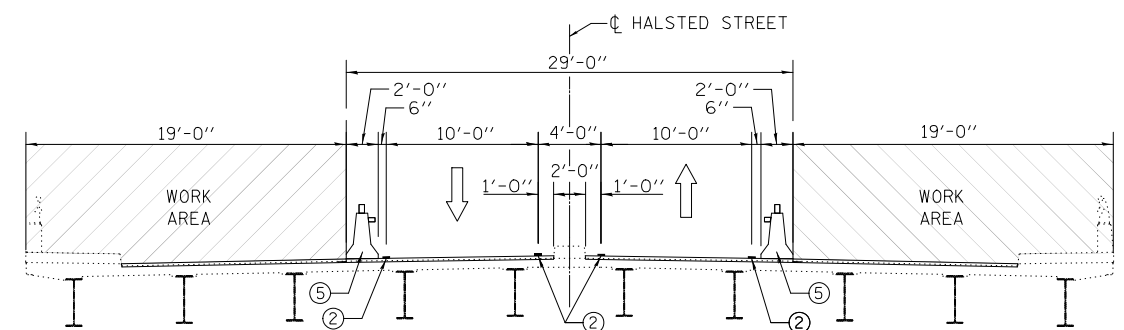
STAGE 1B (SN 0194)

STA STA 97+33 (STA 11+67 TRAIL) TO STA 108+35 (STA 22+77 TRAIL)



STAGE 2B (SN 0195) TYPICAL ROADWAY SECTION

STA 61+10 TO STA 65+10
 STA 74+35 TO STA 78+38



STAGE 2B (SN 0195)

STA 65+10 TO STA 74+35

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USER NAME = ken.moy	DESIGNED - KLM	REVISED -
PLOT SCALE = 100.0000' / 1in.	DRAWN - KLM	REVISED -
PLOT DATE = 5/10/2024	CHECKED - BMC	REVISED -
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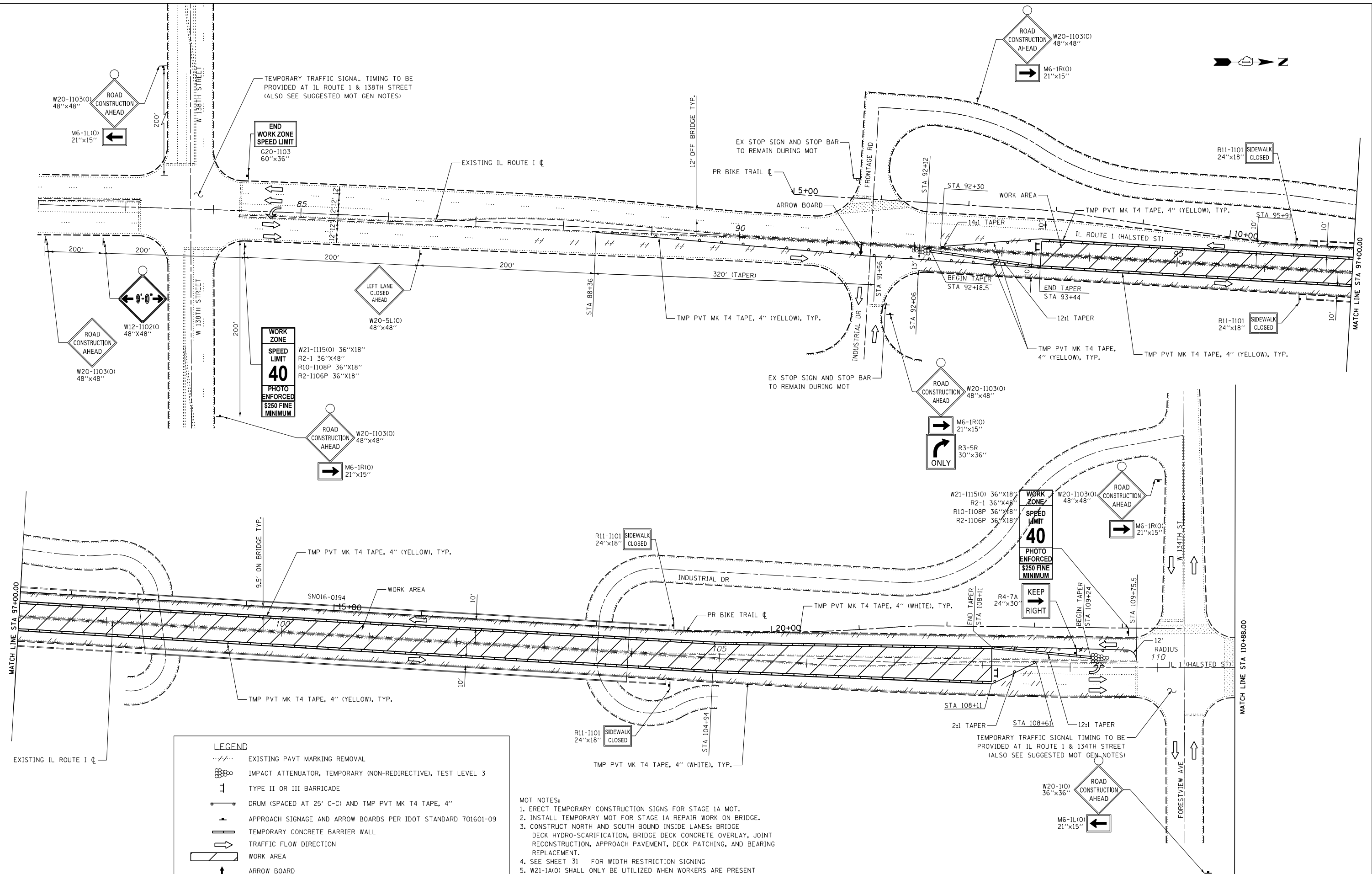
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUGGESTED MAINTENANCE OF TRAFFIC
 STAGING SECTIONS

SCALE: NTS SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	24
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				

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TEMPORARY TRAFFIC SIGNAL TIMING TO BE PROVIDED AT IL ROUTE 1 & 138TH STREET (ALSO SEE SUGGESTED MOT GEN NOTES)

END WORK ZONE SPEED LIMIT
G20-1103
60"x36"

WORK ZONE
SPEED LIMIT
40
PHOTO ENFORCED
\$250 FINE MINIMUM

W21-1115(O) 36"x18"
R2-1 36"x48"
R10-1108P 36"x18"
R2-1106P 36"x18"

ROAD CONSTRUCTION AHEAD
W20-1103(O) 48"x48"
M6-1R(O) 21"x15"

WORK ZONE
SPEED LIMIT
40
PHOTO ENFORCED
\$250 FINE MINIMUM
KEEP RIGHT

W21-1115(O) 36"x18"
R2-1 36"x48"
R10-1108P 36"x18"
R2-1106P 36"x18"

ROAD CONSTRUCTION AHEAD
W20-1103(O) 48"x48"
M6-1R(O) 21"x15"

LEGEND

	EXISTING PAVT MARKING REMOVAL
	IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
	TYPE II OR III BARRICADE
	DRUM (SPACED AT 25' C-C) AND TMP PVT MK T4 TAPE, 4"
	APPROACH SIGNAGE AND ARROW BOARDS PER IDOT STANDARD 701601-09
	TEMPORARY CONCRETE BARRIER WALL
	TRAFFIC FLOW DIRECTION
	WORK AREA
	ARROW BOARD

- MOT NOTES:**
1. ERECT TEMPORARY CONSTRUCTION SIGNS FOR STAGE 1A MOT.
 2. INSTALL TEMPORARY MOT FOR STAGE 1A REPAIR WORK ON BRIDGE.
 3. CONSTRUCT NORTH AND SOUTH BOUND INSIDE LANES: BRIDGE DECK HYDRO-SCARIFICATION, BRIDGE DECK CONCRETE OVERLAY, JOINT RECONSTRUCTION, APPROACH PAVEMENT, DECK PATCHING, AND BEARING REPLACEMENT.
 4. SEE SHEET 31 FOR WIDTH RESTRICTION SIGNING
 5. W21-1A(O) SHALL ONLY BE UTILIZED WHEN WORKERS ARE PRESENT



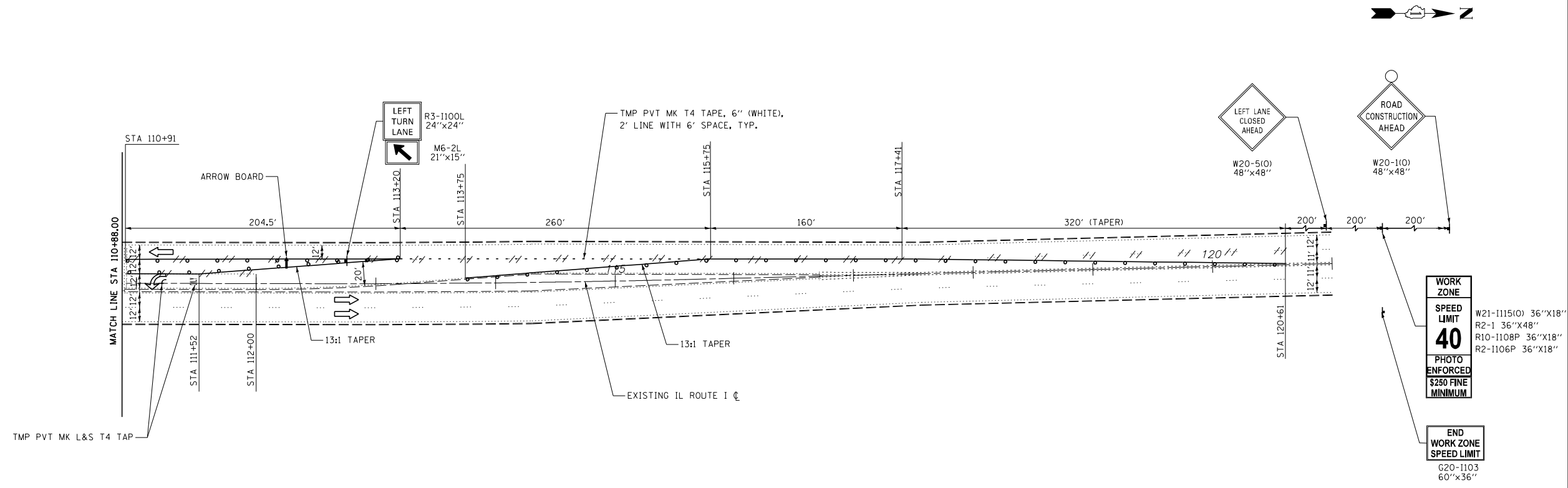
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PLOT SCALE = 100.0000' / 1"	DRAWN - KLM	REVISED -
PLOT DATE = 5/10/2024	CHECKED - BMC	REVISED -
	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
STAGE 1A - SN016-0194**

SCALE: 1"=50'	SHEET NO. 1 OF 4 SHEETS	STA. TO STA.
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F.A.U. RTE. 3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 25
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



LEGEND

- EXISTING PAVT MARKING REMOVAL
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
- TYPE II OR III BARRICADE
- DRUM (SPACED AT 25' C-C) AND TMP PVT MK T4 TAPE, 4"
- APPROACH SIGNAGE AND ARROW BOARDS PER IDOT STANDARD 701601-09
- TEMPORARY CONCRETE BARRIER WALL
- TRAFFIC FLOW DIRECTION
- WORK AREA
- ARROW BOARD

- MOT NOTES:**
1. ERECT TEMPORARY CONSTRUCTION SIGNS FOR STAGE 1A MOT.
 2. INSTALL TEMPORARY MOT FOR STAGE 1A REPAIR WORK ON BRIDGE.
 3. CONSTRUCT NORTH AND SOUTH BOUND INSIDE LANES; BRIDGE DECK HYDRO-SCARIFICATION, BRIDGE DECK CONCRETE OVERLAY, JOINT RECONSTRUCTION, APPROACH PAVEMENT, DECK PATCHING, AND BEARING REPLACEMENT.
 4. SEE SHEET 31 FOR WIDTH RESTRICTION SIGNING
 5. W21-1A(O) SHALL ONLY BE UTILIZED WHEN WORKERS ARE PRESENT



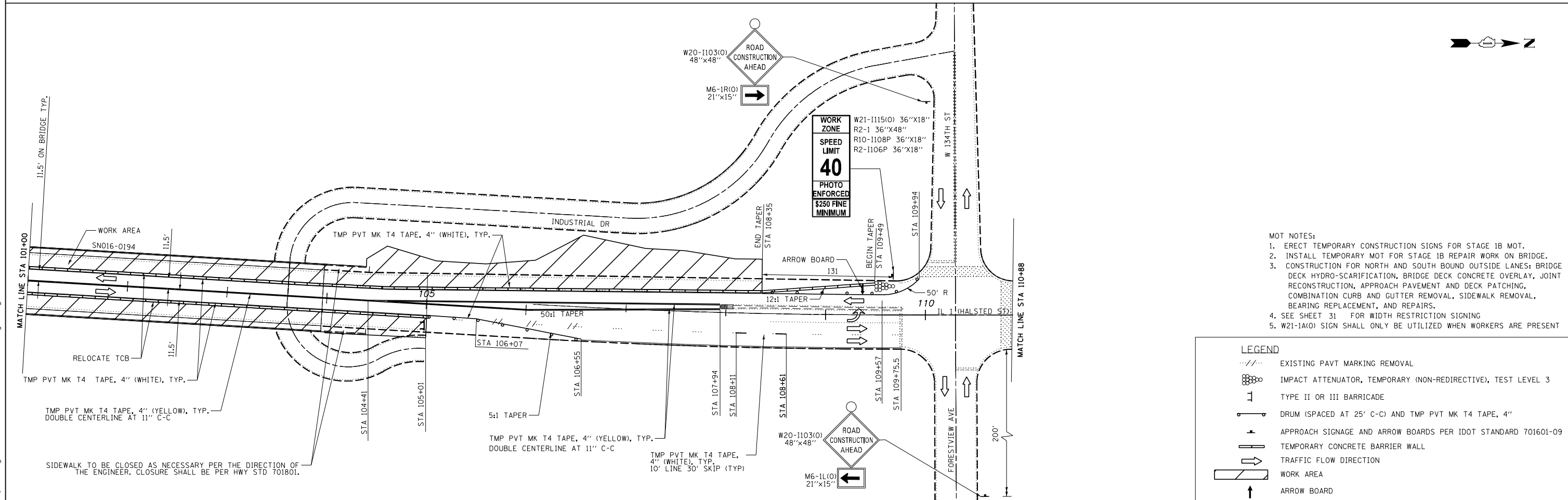
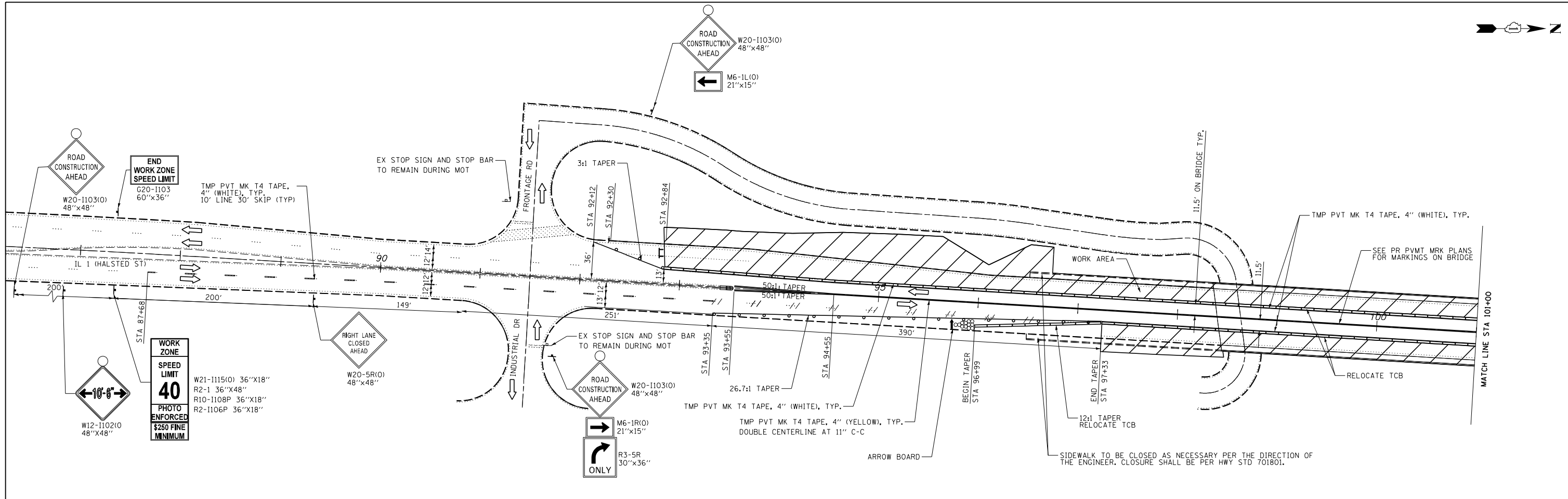
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PLOT SCALE = 100.0000' / 1" =	CHECKED - BMC	REVISED -
PLOT DATE = 5/10/2024	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUGGESTED MAINTENANCE OF TRAFFIC	
STAGE 1A - SN016-0194	
SCALE: 1"=50'	SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	26
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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- MOT NOTES:
1. ERECT TEMPORARY CONSTRUCTION SIGNS FOR STAGE 1B MOT.
 2. INSTALL TEMPORARY MOT FOR STAGE 1B REPAIR WORK ON BRIDGE.
 3. CONSTRUCTION FOR NORTH AND SOUTH BOUND OUTSIDE LANES: BRIDGE DECK HYDRO-SCARIFICATION, BRIDGE DECK CONCRETE OVERLAY, JOINT RECONSTRUCTION, APPROACH PAVEMENT AND DECK PATCHING, COMBINATION CURB AND GUTTER REMOVAL, SIDEWALK REMOVAL, BEARING REPLACEMENT, AND REPAIRS.
 4. SEE SHEET 31 FOR WIDTH RESTRICTION SIGNING
 5. W21-1A(O) SIGN SHALL ONLY BE UTILIZED WHEN WORKERS ARE PRESENT

LEGEND

	EXISTING PAVT MARKING REMOVAL
	IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
	TYPE II OR III BARRICADE
	DRUM (SPACED AT 25' C-C) AND TMP PVT MK T4 TAPE, 4"
	APPROACH SIGNAGE AND ARROW BOARDS PER IDOT STANDARD 701601-09
	TEMPORARY CONCRETE BARRIER WALL
	TRAFFIC FLOW DIRECTION
	WORK AREA
	ARROW BOARD



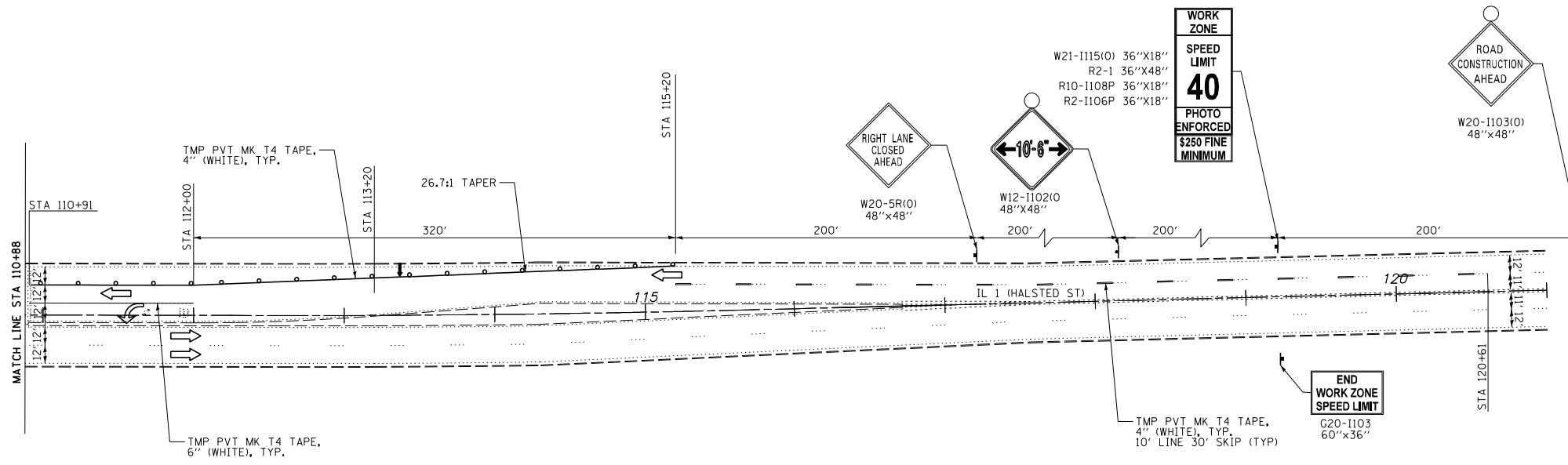
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PLOT SCALE = 100.0000' / 1"	DRAWN - KLM	REVISED -
PLOT DATE = 5/10/2024	CHECKED - BMC	REVISED -
	DATE = 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUGGESTED MAINTENANCE OF TRAFFIC	
STAGE 1B - SN016-0194	
SCALE: 1"=50'	SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	27
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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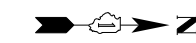


LEGEND

- /--- EXISTING PAVT MARKING REMOVAL
- ⊗ IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
- ⊥ TYPE II OR III BARRICADE
- ⊣ DRUM (SPACED AT 25' C-C) AND TMP PVT MK T4 TAPE, 4"
- ⊥ APPROACH SIGNAGE AND ARROW BOARDS PER IDOT STANDARD 701601-09
- ▬ TEMPORARY CONCRETE BARRIER WALL
- ➡ TRAFFIC FLOW DIRECTION
- ▭ WORK AREA
- ↑ ARROW BOARD

MOT NOTES:

1. ERECT TEMPORARY CONSTRUCTION SIGNS FOR STAGE 1B MOT.
2. INSTALL TEMPORARY MOT FOR STAGE 1B REPAIR WORK ON BRIDGE.
3. CONSTRUCTION FOR NORTH AND SOUTH BOUND OUTSIDE LANES: BRIDGE DECK HYDRO-SCARIFICATION, BRIDGE DECK CONCRETE OVERLAY, JOINT RECONSTRUCTION, APPROACH PAVEMENT AND DECK PATCHING, COMBINATION CURB AND GUTTER REMOVAL, SIDEWALK REMOVAL, BEARING REPLACEMENT, AND REPAIRS.
4. SEE SHEET 31 FOR WIDTH RESTRICTION SIGNING
5. W21-1A(O) SIGN SHALL ONLY BE UTILIZED WHEN WORKERS ARE PRESENT



USER NAME = ken.moy	DESIGNED - KLM	REVISED -
PLOT SCALE = 100.0000' / 1in.	DRAWN - KLM	REVISED -
PLOT DATE = 5/10/2024	CHECKED - BMC	REVISED -
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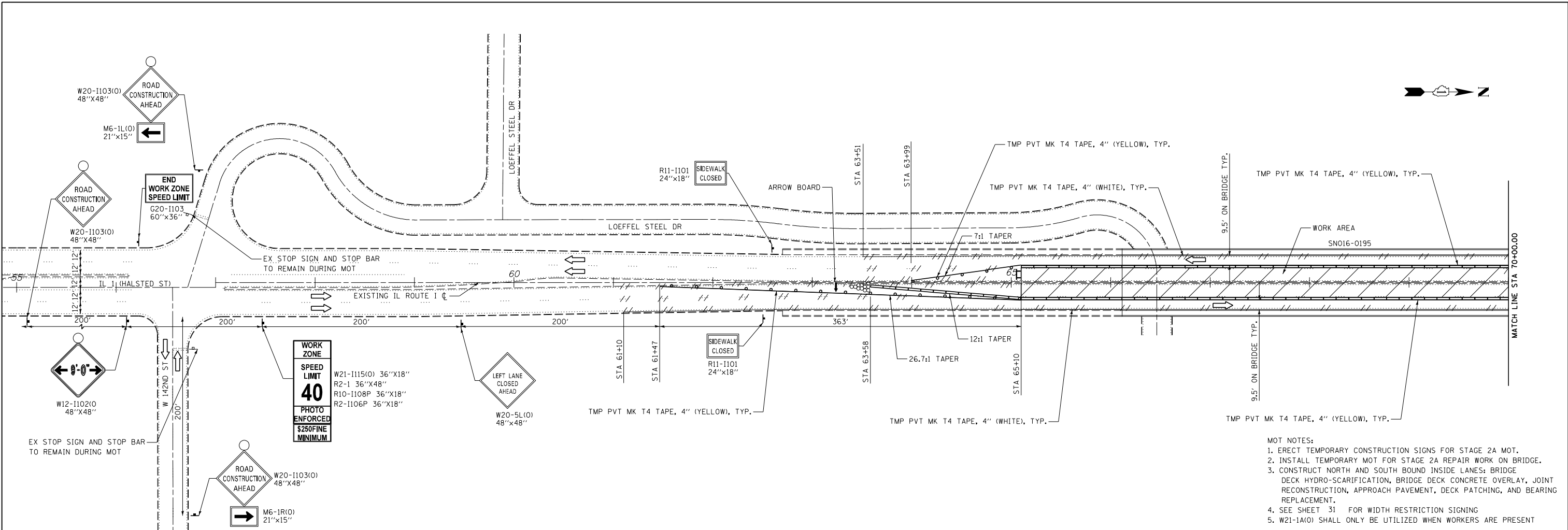
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
STAGE 1B - SN016-0194**

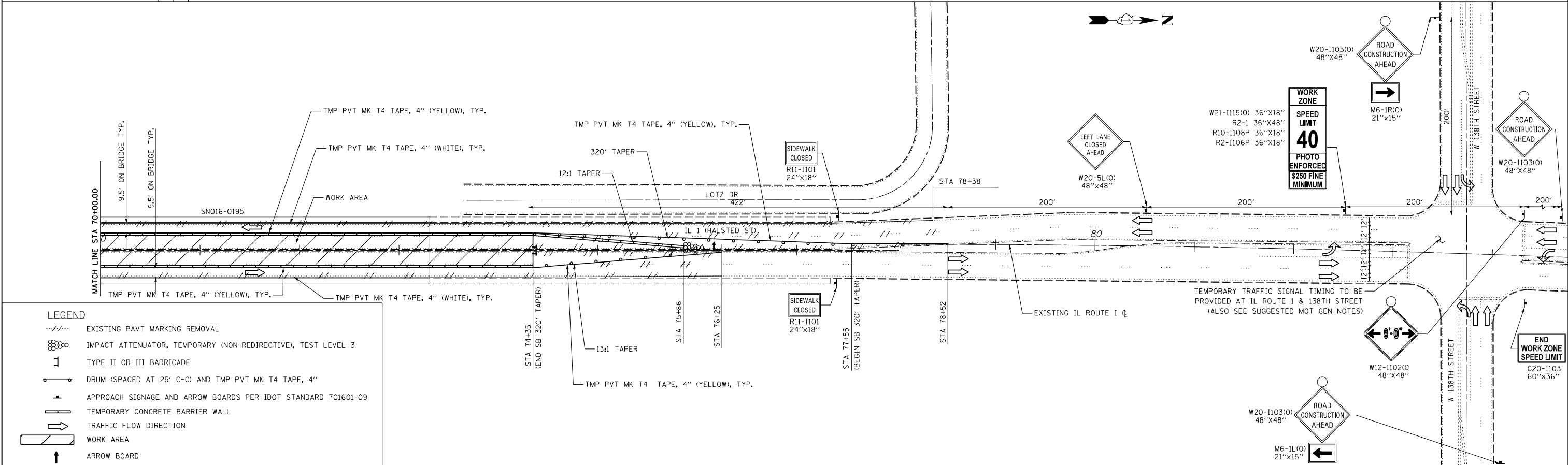
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	28
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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- MOT NOTES:**
1. ERECT TEMPORARY CONSTRUCTION SIGNS FOR STAGE 2A MOT.
 2. INSTALL TEMPORARY MOT FOR STAGE 2A REPAIR WORK ON BRIDGE.
 3. CONSTRUCT NORTH AND SOUTH BOUND INSIDE LANES; BRIDGE DECK HYDRO-SCARIFICATION, BRIDGE DECK CONCRETE OVERLAY, JOINT RECONSTRUCTION, APPROACH PAVEMENT, DECK PATCHING, AND BEARING REPLACEMENT.
 4. SEE SHEET 31 FOR WIDTH RESTRICTION SIGNING
 5. W21-1A(O) SHALL ONLY BE UTILIZED WHEN WORKERS ARE PRESENT



TEMPORARY TRAFFIC SIGNAL TIMING TO BE PROVIDED AT IL ROUTE 1 & 138TH STREET (ALSO SEE SUGGESTED MOT GEN NOTES)

LEGEND

- EXISTING PAVT MARKING REMOVAL
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
- TYPE II OR III BARRICADE
- DRUM (SPACED AT 25' C-C) AND TMP PVT MK T4 TAPE, 4"
- APPROACH SIGNAGE AND ARROW BOARDS PER IDOT STANDARD 701601-09
- TEMPORARY CONCRETE BARRIER WALL
- TRAFFIC FLOW DIRECTION
- WORK AREA
- ARROW BOARD

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	DATE - 04/02/2024	REVISED -

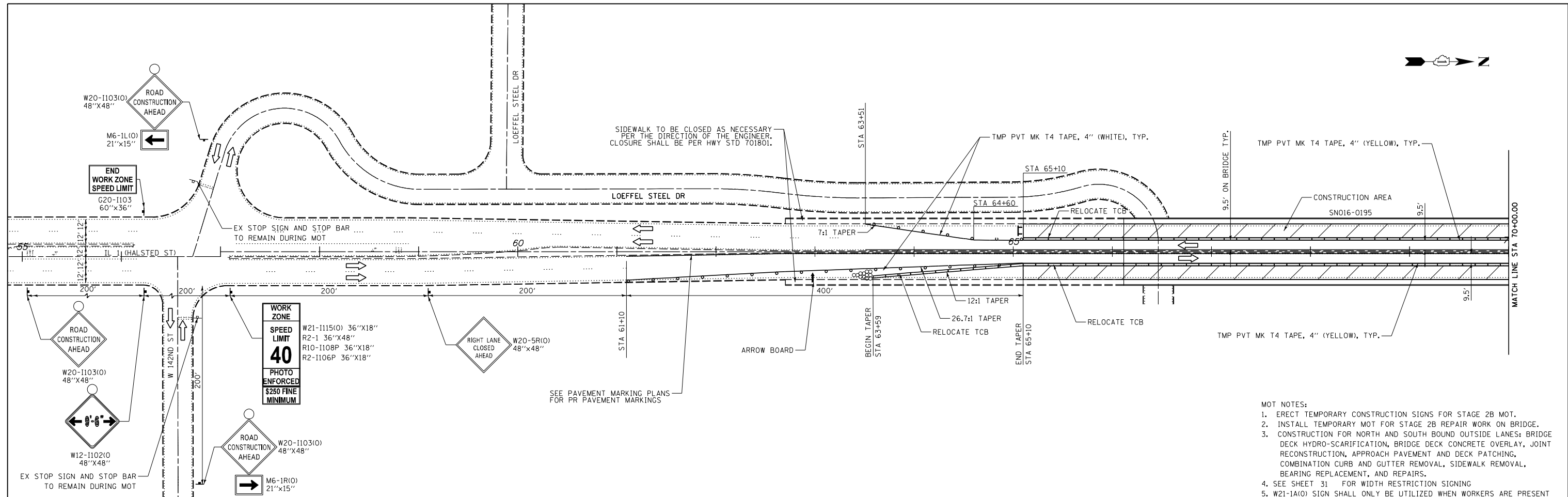
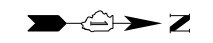
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUGGESTED MAINTENANCE OF TRAFFIC

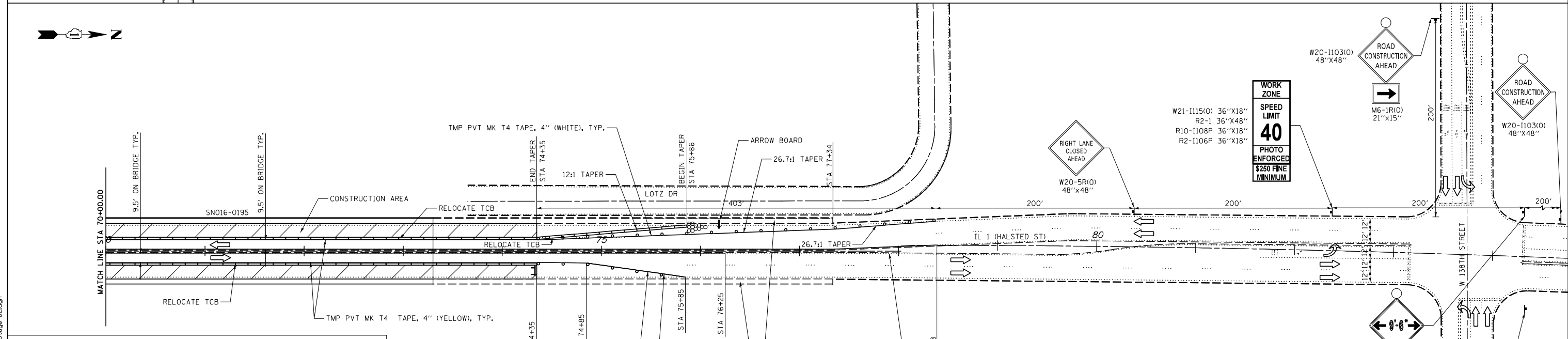
STAGE 2A - SN016-0195

SCALE: 1"=50' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	29
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- MOT NOTES:**
1. ERECT TEMPORARY CONSTRUCTION SIGNS FOR STAGE 2B MOT.
 2. INSTALL TEMPORARY MOT FOR STAGE 2B REPAIR WORK ON BRIDGE.
 3. CONSTRUCTION FOR NORTH AND SOUTH BOUND OUTSIDE LANES: BRIDGE DECK HYDRO-SCARIFICATION, BRIDGE DECK CONCRETE OVERLAY, JOINT RECONSTRUCTION, APPROACH PAVEMENT AND DECK PATCHING, COMBINATION CURB AND GUTTER REMOVAL, SIDEWALK REMOVAL, BEARING REPLACEMENT, AND REPAIRS.
 4. SEE SHEET 31 FOR WIDTH RESTRICTION SIGNING
 5. W21-1A(O) SIGN SHALL ONLY BE UTILIZED WHEN WORKERS ARE PRESENT



LEGEND	
	EXISTING PAVT MARKING REMOVAL
	IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3
	TYPE II OR III BARRICADE
	DRUM (SPACED AT 25' C-C) AND TMP PVT MK T4 TAPE, 4"
	APPROACH SIGNAGE AND ARROW BOARDS PER IDOT STANDARD 701601-09
	TEMPORARY CONCRETE BARRIER WALL
	TRAFFIC FLOW DIRECTION
	WORK AREA
	ARROW BOARD

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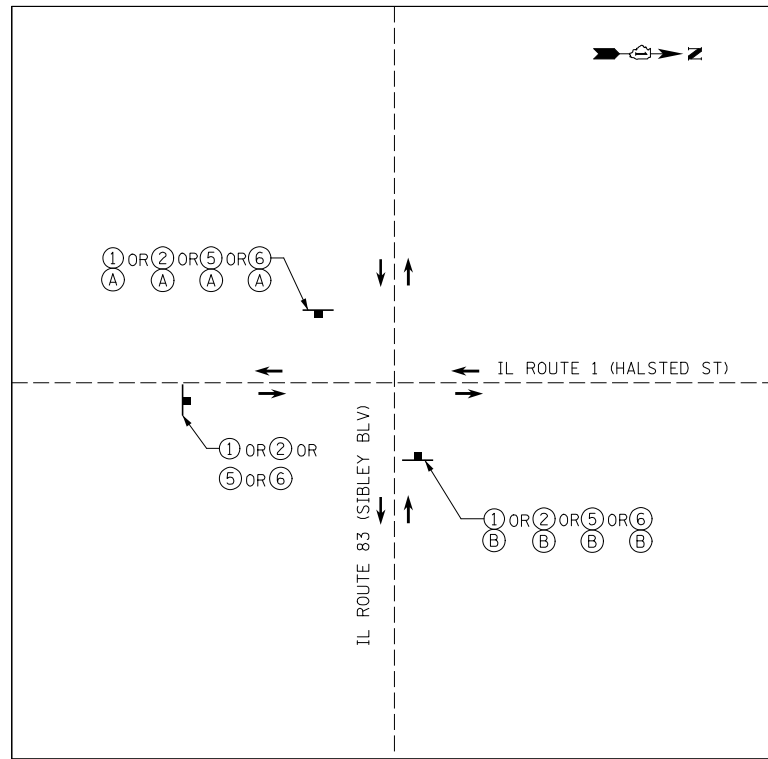
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
STAGE 2B - SN016-0195**

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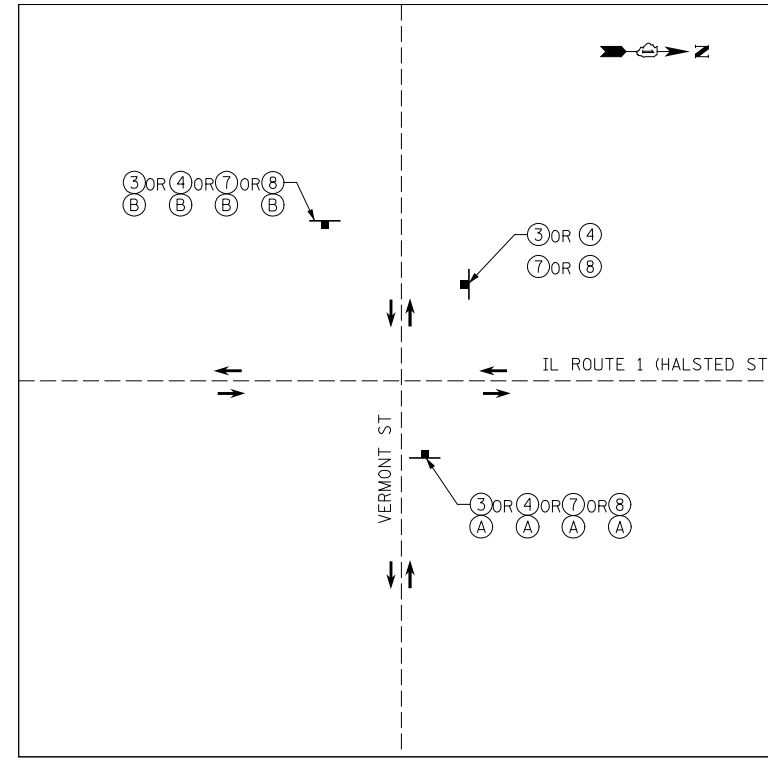
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	30
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

OVERSIZE LOAD SIGNAGE PLACEMENT FOR IL ROUTE 1 (HALSTED ST) AND IL ROUTE 83 (SIBLEY BLV)

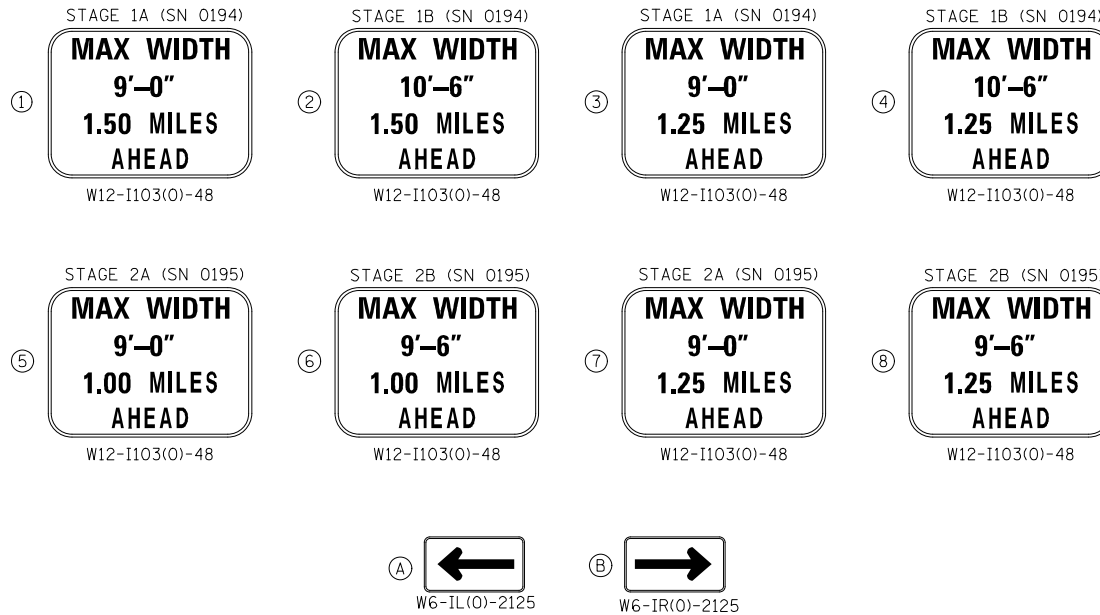


(NOT TO SCALE)

OVERSIZE LOAD SIGNAGE PLACEMENT FOR IL ROUTE 1 (HALSTED ST) AND VERMONT ST



(NOT TO SCALE)



NOTE:
 1. OVERSIZED LOAD SIGNAGE SHALL BE ERECTED FOR EACH RESPECTIVE CONSTRUCTION STAGE ONLY.
 2. SIGNAGE SHALL BE PLACED 200 FEET IN ADVANCE OF THE INTERSECTION.
 3. STAGE 1 = WORK ASSOCIATED WITH SN 016-0194
 4. STAGE 2 = WORK ASSOCIATED WITH SN 016-0195
 5. SEE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS FOR STAGING DETAILS.

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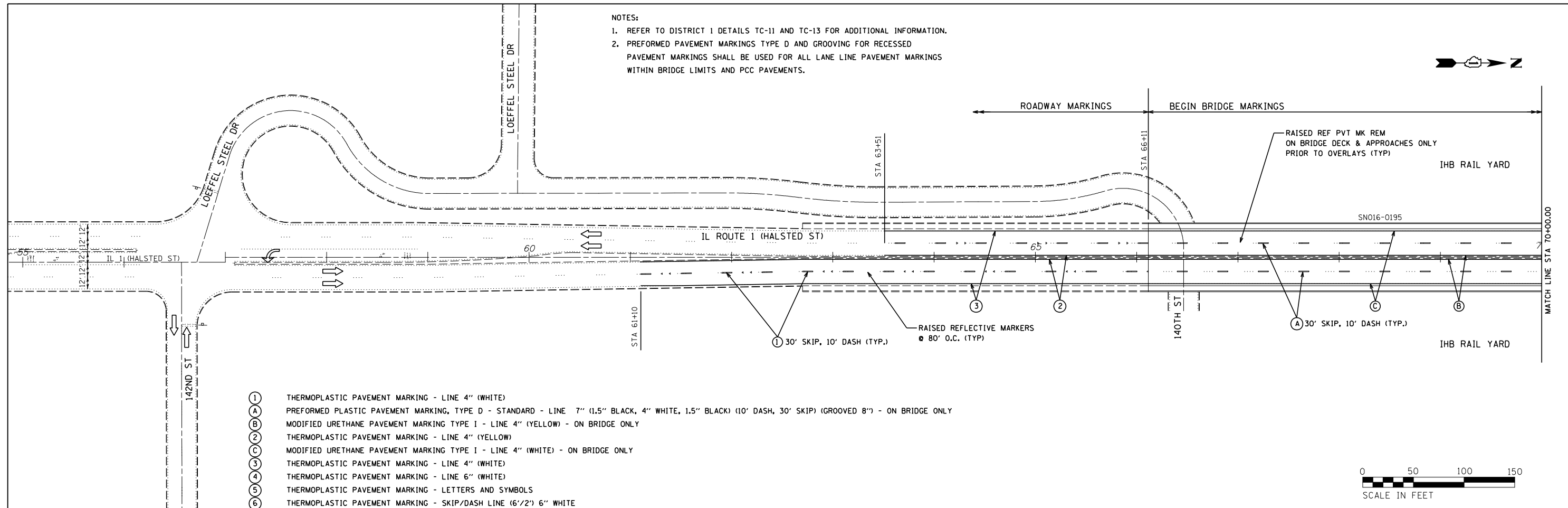
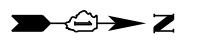
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

OVERSIZED LOAD SIGNAGE PLACEMENT DIAGRAM

SCALE: 1" = NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	31
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

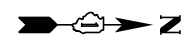
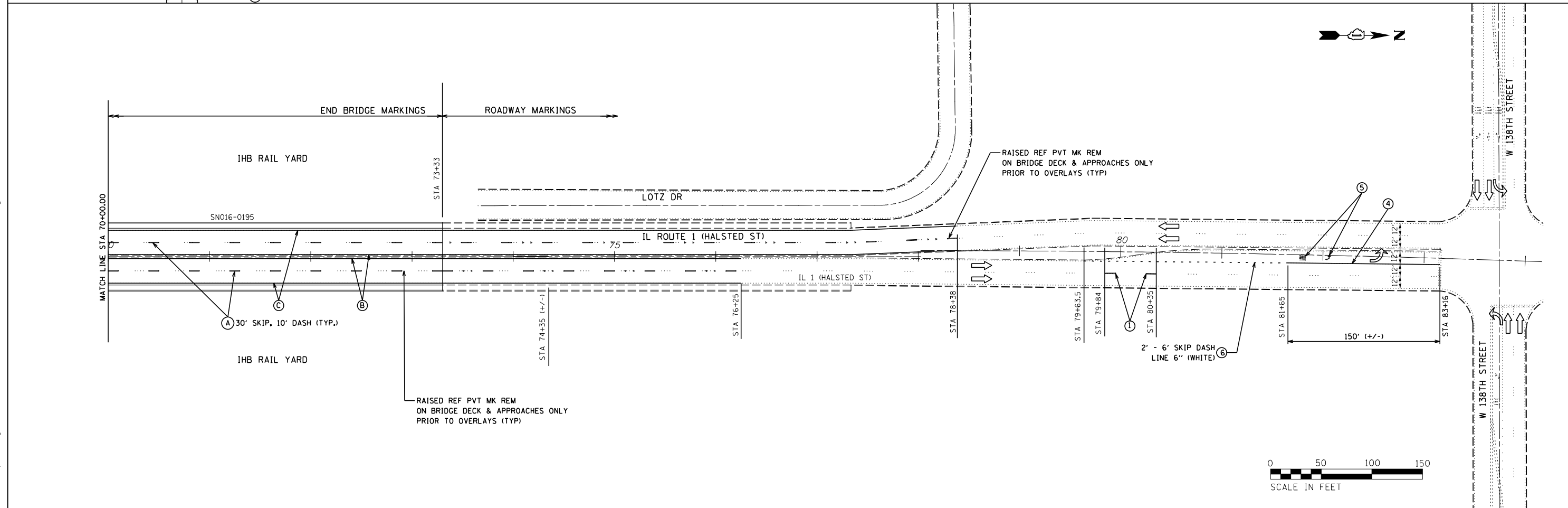
NOTES:
 1. REFER TO DISTRICT 1 DETAILS TC-11 AND TC-13 FOR ADDITIONAL INFORMATION.
 2. PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.



- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ⑥ THERMOPLASTIC PAVEMENT MARKING - SKIP/DASH LINE (6'/2') 6" WHITE



MODEL: #MODELNAME\$
 FILE: NAME\$.prt
 303 East Wacker Drive, Suite 1400, Chicago, IL 60601
 T 1-312-373-7700 F 1-312-373-8800



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PLOT SCALE = 100.0000' / 1"	DRAWN - KLM	REVISED -
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	DATE - 04/02/2024	REVISED -

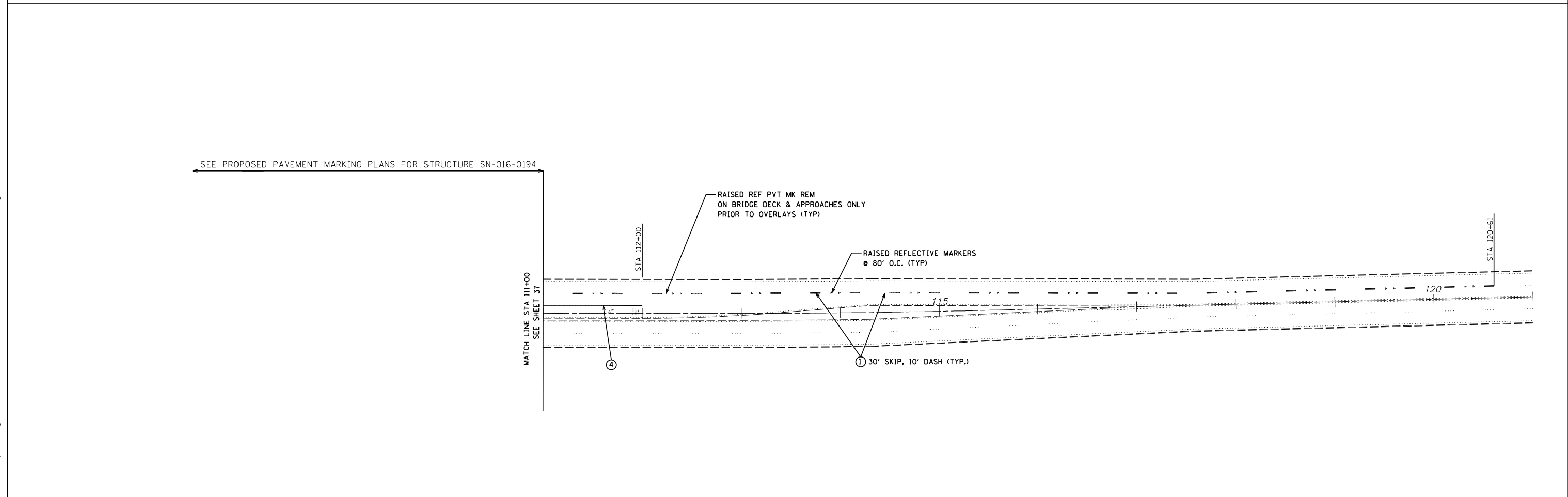
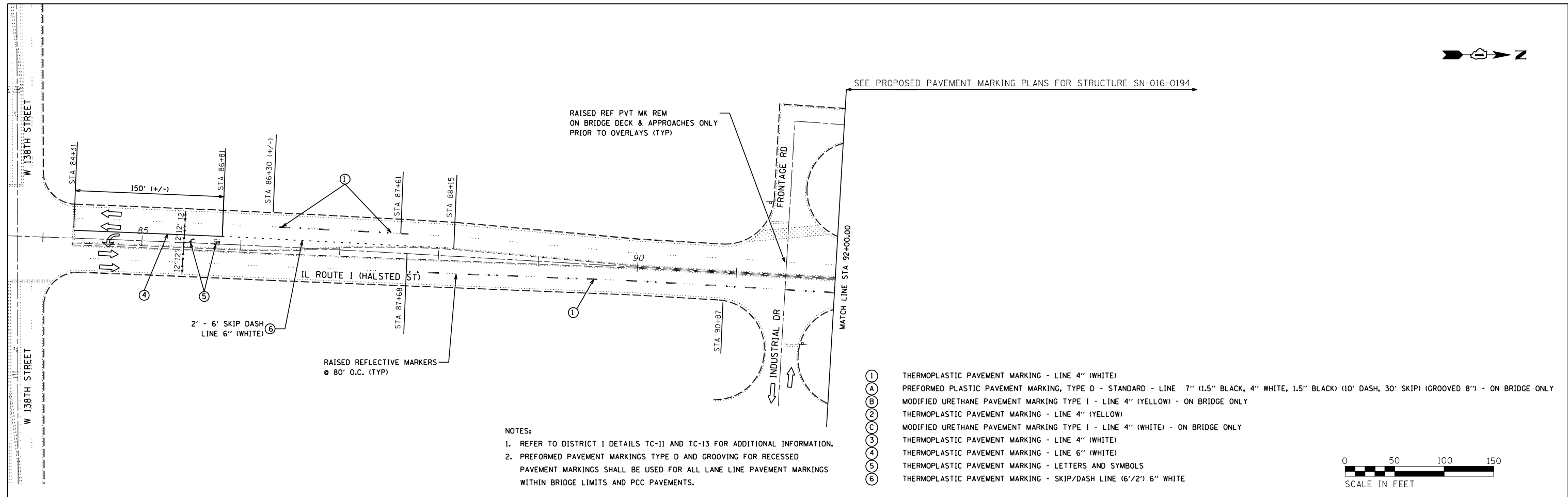
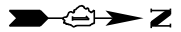
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PROPOSED PAVEMENT MARKINGS
 STRUCTURE NO. 016-0195**

SCALE: 1"=50' SHEET 1 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	32
CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

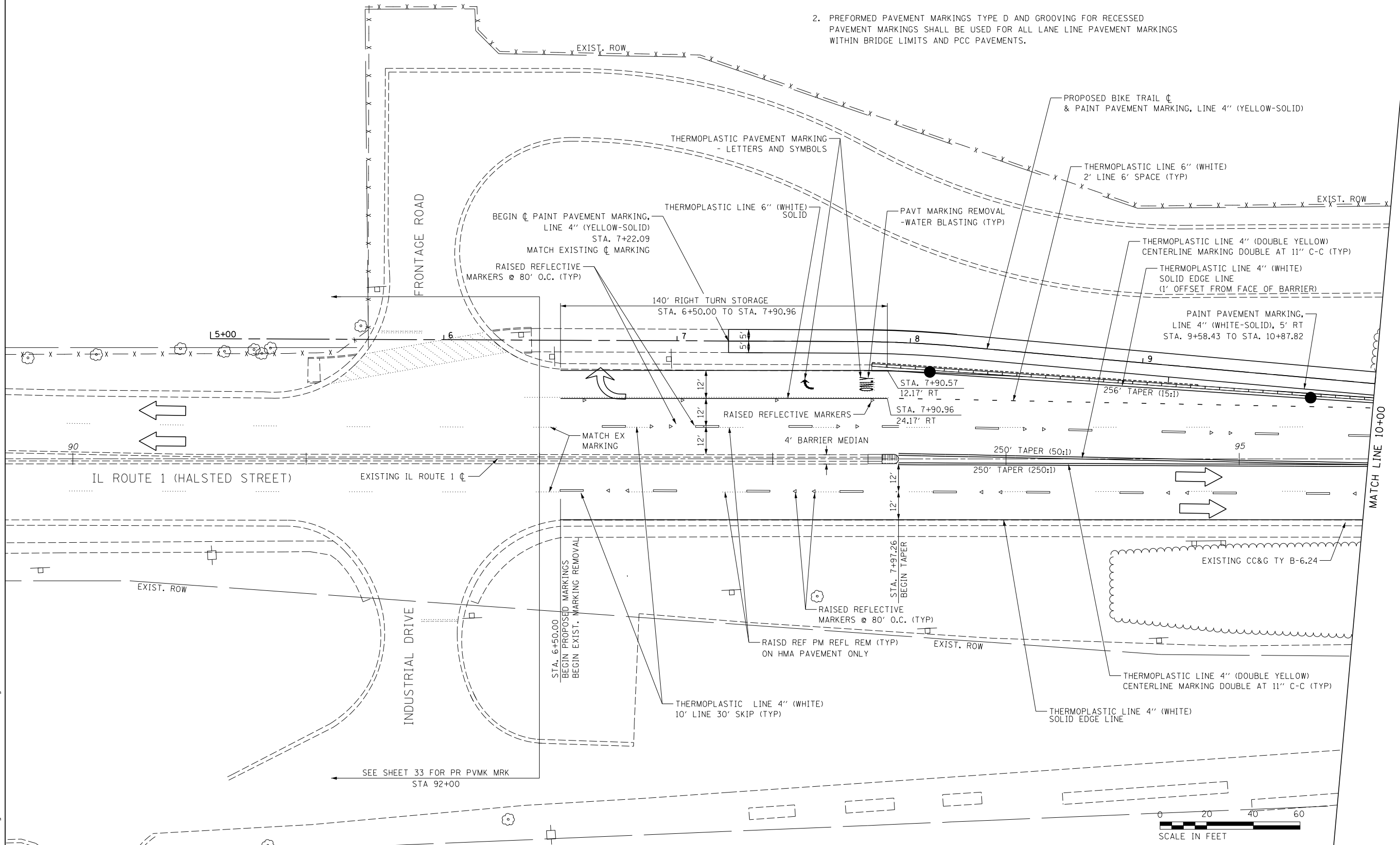
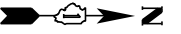
**PROPOSED PAVEMENT MARKINGS
STRUCTURE NO. 016-0194**

SCALE: 1"=50' SHEET 2 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	33
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				

NOTES:

1. REFER TO DISTRICT 1 DETAILS TC-11 AND TC-13 FOR ADDITIONAL INFORMATION.
2. PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.



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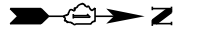
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PAVEMENT MARKING PLAN
STRUCTURE NO. 016-0194**

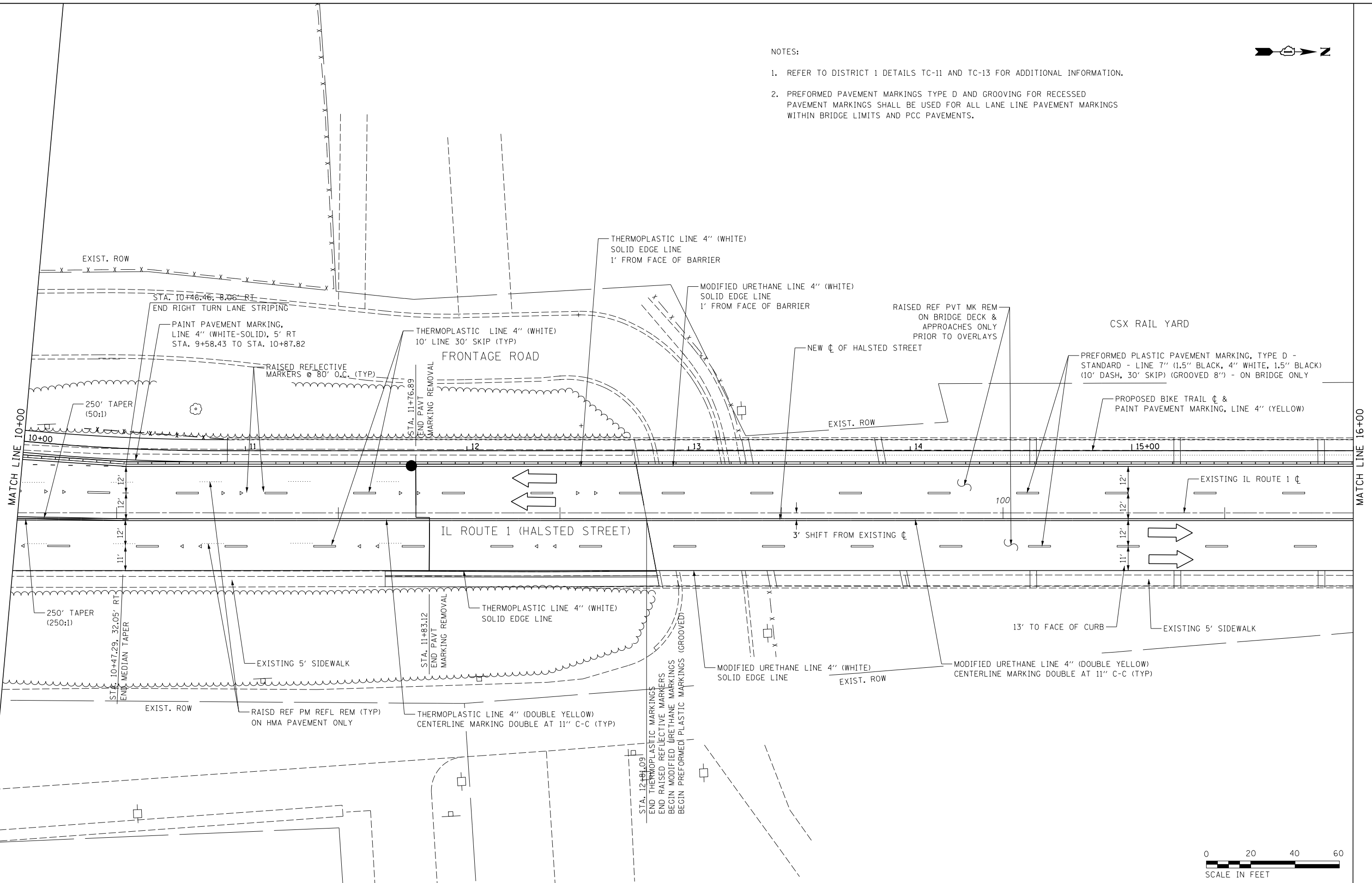
SHEET NO. 3 OF 6 SHEETS STA. 6+50 TO STA. 10+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	34
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. REFER TO DISTRICT 1 DETAILS TC-11 AND TC-13 FOR ADDITIONAL INFORMATION.
2. PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

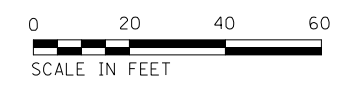
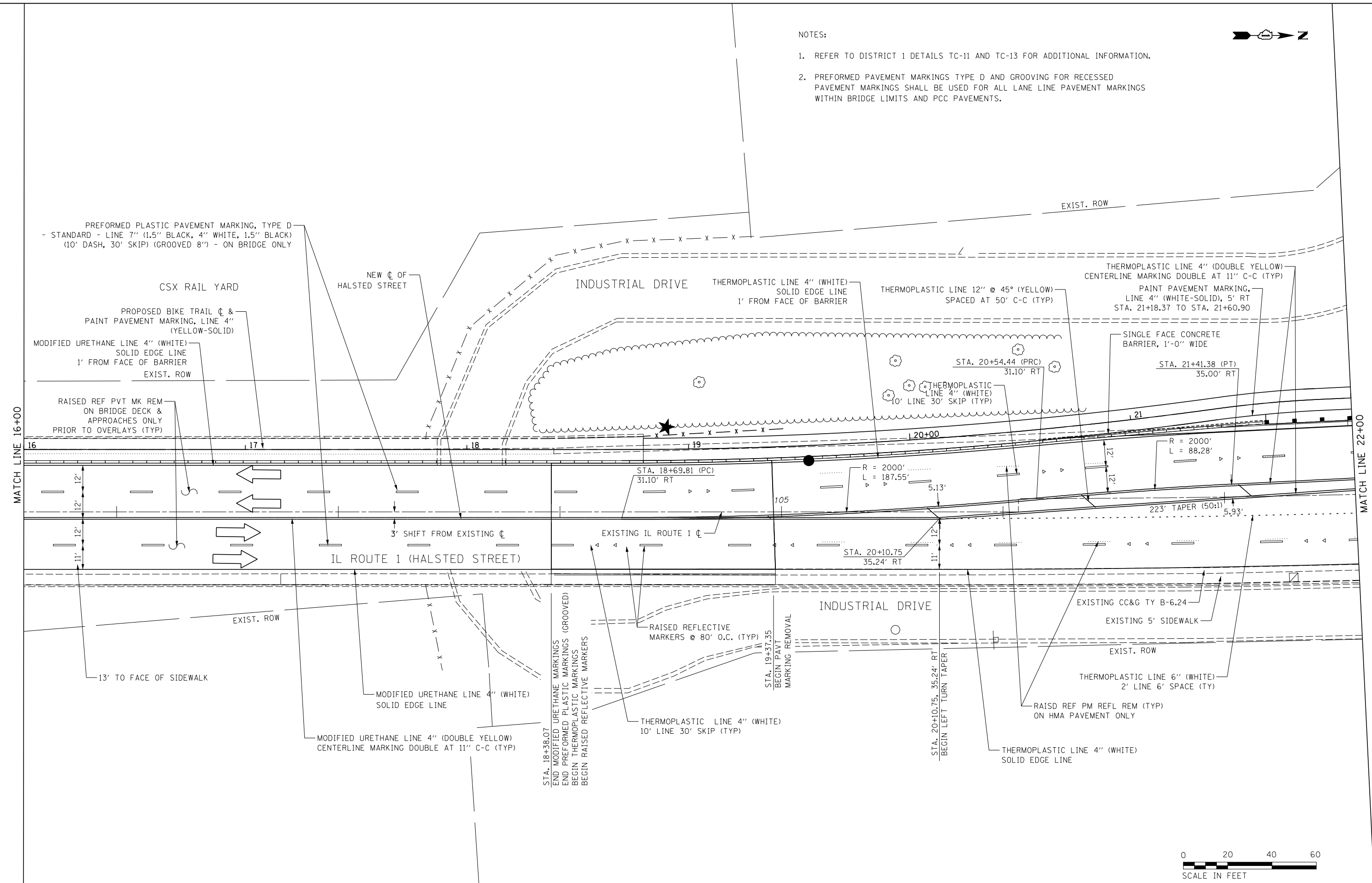
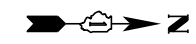
**PROPOSED PAVEMENT MARKING PLAN
STRUCTURE NO. 016-0194**

SHEET NO. 4 OF 6 SHEETS STA. 10+00 TO STA. 16+00

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3730	15-00131-01-BR	COOK	109	35
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

NOTES:

- REFER TO DISTRICT 1 DETAILS TC-11 AND TC-13 FOR ADDITIONAL INFORMATION.
- PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.



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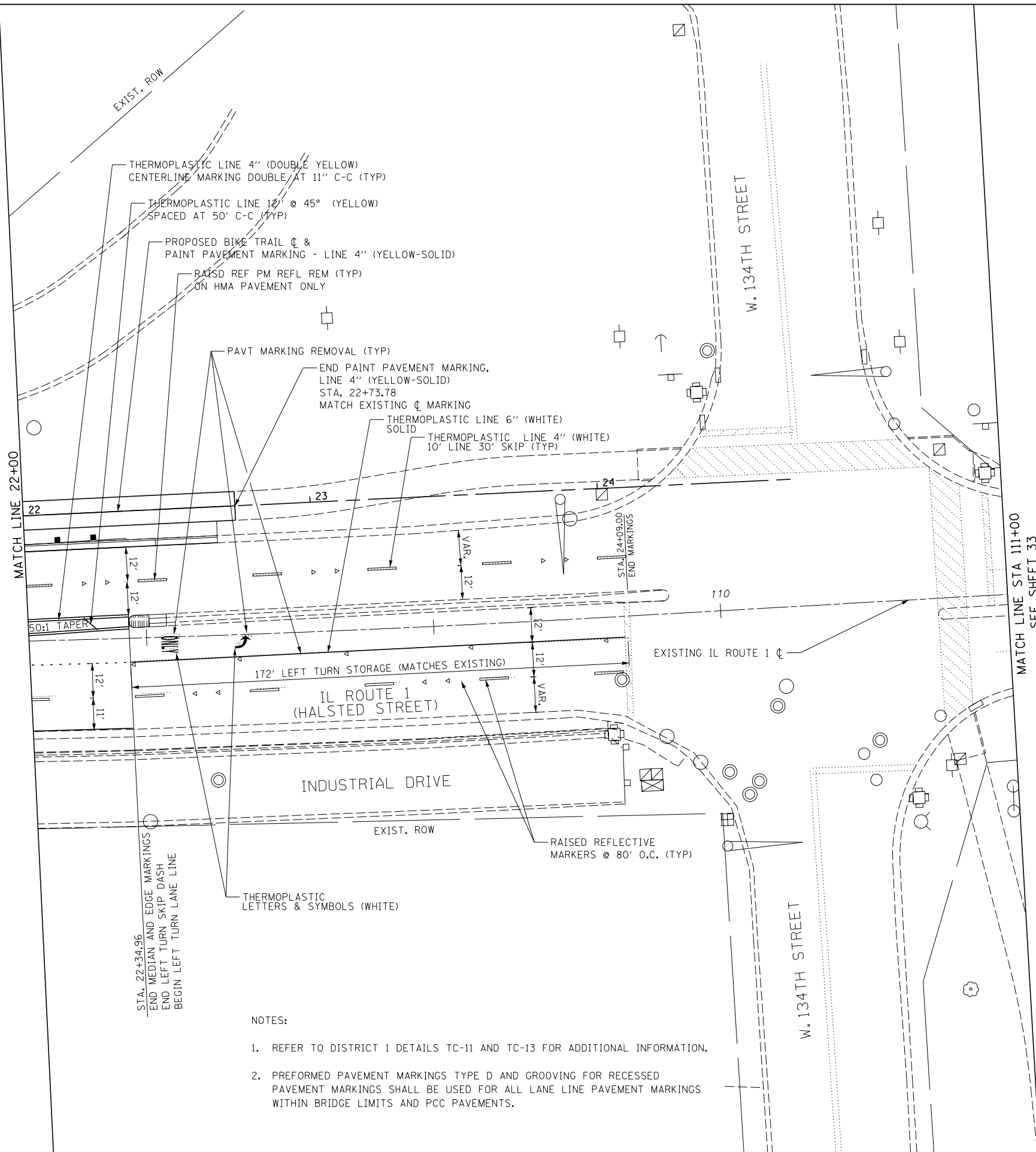
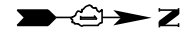
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	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PAVEMENT MARKING PLAN
STRUCTURE NO. 016-0194**

SHEET NO. 5 OF 6 SHEETS STA. 16+00 TO STA. 22+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	36
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. REFER TO DISTRICT 1 DETAILS TC-11 AND TC-13 FOR ADDITIONAL INFORMATION.
2. PREFORMED PAVEMENT MARKINGS TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS AND PCC PAVEMENTS.



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	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PAVEMENT MARKING PLAN
STRUCTURE NO. 016-0194**

SHEET NO. 6 OF 6 SHEETS STA. 22+00 TO STA. 24+08

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	37
CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Existing Structure:
 Existing Structure No. 016-0195 was built in 1927 as SBI 1 Section 3262-VB.
 In 1979, under SBI-1 Section 3262-VB, the superstructure was replaced.
 Existing Structure is a twelve span reinforced concrete slab on steel beams
 between closed abutments, 722'-0" bk. to bk. abutments, 68'-0" out to out deck.
 Traffic to be staged during construction.

DESIGN SPECIFICATIONS

(NEW CONSTRUCTION)

2002 AASHTO Standar Specifications
 For Highway Bridges

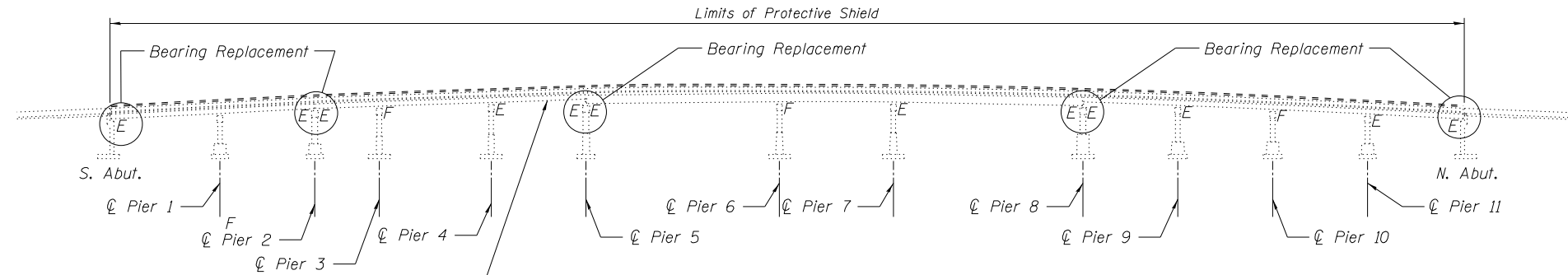
DESIGN STRESSES

FIELD UNITS (NEW CONSTRUCTION)

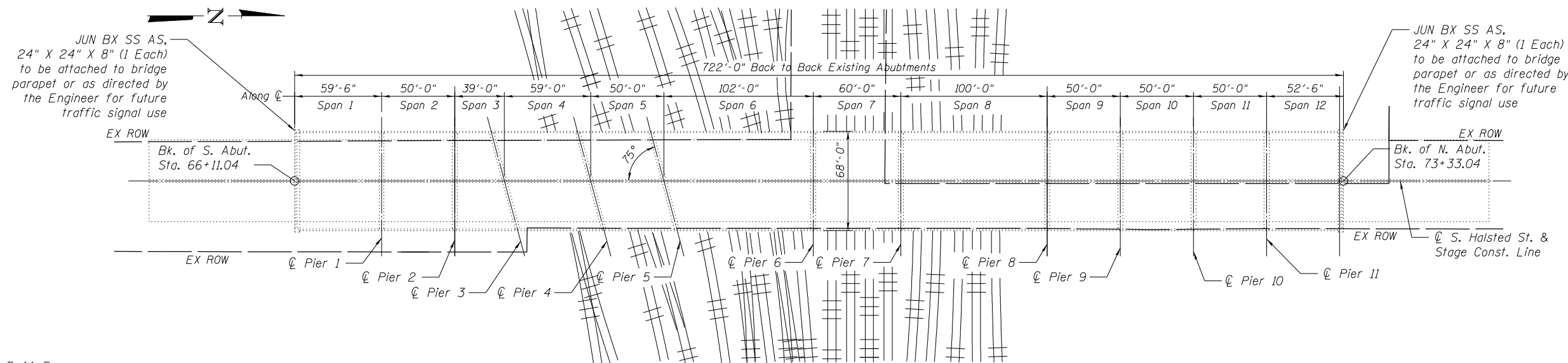
$f'_c = 3,500$ psi
 $f'_c = 4,000$ psi (Superstructure & Aprpr. Slab)
 $f_y = 60,000$ psi (Reinforcement)
 $f_y = 50,000$ psi (Structural Steel)

SCOPE OF WORK

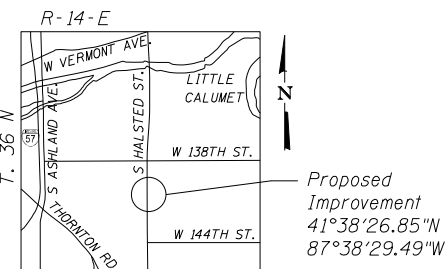
1. Remove existing expansion joints at the abutments and Piers 2, 5 and 8 and install new preformed joint strip seals.
2. Scarify existing bridge deck surface $\frac{3}{4}$ ".
3. Perform concrete repairs to bridge deck and approach slab.
4. Replace drainage scuppers and construct new drainage system at Piers 3 and 8.
5. Place $2\frac{3}{4}$ " latex concrete overlay on bridge deck and perform $\frac{1}{4}$ " diamond grinding of new overlay.
6. Jack and remove existing expansion bearings at the abutments and at Piers 2, 5 and 8 and replace with elastomeric bearings.
7. Perform steel repairs to beam ends at Piers 2 and 8.
8. Replace steel diaphragms at over Pier 8.
9. Construct concrete encasement at Piers 2, 5 & 8.
10. Perform substructure repairs.
11. Apply protective coat to inside and top faces of existing parapets, sidewalks, medians, new concrete at expansion joints and latex overlay.
12. Apply $1\frac{3}{4}$ " HMA resurfacing to approach slabs.



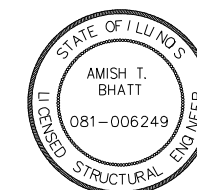
ELEVATION



PLAN



LOCATION SKETCH



I hereby certify that these plans were prepared by me or under my direct personal supervision and that I am a duly licensed structural engineer under the laws of the State of Illinois.

Amish T. Bhatt DATE: 2-29-2024

AMISH T. BHATT S.E. License #081-006249
 LICENSE EXP. 11-30-2024

Applies to Sheet SA-1 thru SA-14 and SA-16 thru SA-23.

MODEL: \\MODEL\NAME: FILE: \\NAME: ...



USER NAME = ken.joy	DESIGNED - RJL/SEA	REVISED -
PLOT SCALE = N.T.S.	CHECKED - ATB	REVISED -
PLOT DATE = 4/2/2024	DRAWN - JJE/KAD	REVISED -
	CHECKED - ATB	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN & ELEVATION
 STRUCTURE NO. 016-0195**

SHEET NO. SA-1 OF SA-23 SHEETS

FAU. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	38
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement bars designated (E) shall be epoxy coated.

Existing reinforcement bars extending into the new construction shall be cleaned and straightened prior to incorporating into new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved embedded reinforcement or Mechanical Bar Splicer System. Cost included with Concrete Removal.

All structural steel shall conform to AASHTO Classification M-270 Grade 50, unless otherwise noted.

Fasteners shall be ASTM F 3125 Grade A325 Type 1, mechanically galvanized bolts. Bolts 3/4" φ, open holes 13/16" φ, unless otherwise noted.

Prior to pouring the new concrete deck or overlay, all heavy or loose rust, loose mill scale and other loose and potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost shall be included with Concrete Removal.

Up to 1/4" may be ground off the bridge deck. Bridge Deck Grooving shall be completed only after Diamond Grinding (Bridge Section) is completed.

Existing raised reflective pavement markers on the bridge deck and approach slab shall be removed and replaced. See Proposed Pavement Marking Plans for location of proposed markers on the bridge structure.

Cost of removal and re-installation of all structural steel members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Furnishing and Erecting Structural Steel.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

All new structural steel and bearing assemblies shall be hot-dip galvanized. See Special Provisions for Hot Dip Galvanizing for Structural Steel.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.

No free fall deck drains will be permitted in the spans over the tracks.

Substructure repairs and bearing replacement shall be performed under staged construction when no live load is present.

The Contractor is advised that the existing structure may contain members that are in deteriorated condition with reduced load carrying capacity. It is the Contractor's responsibility to account for the condition of the existing structure when developing construction procedures. An existing structure information package will be provided by the Department to the Contractor upon request.

INDEX OF SHEETS

SA-1	General Plan & Elevation
SA-2	Index of Sheets, General Notes & Bill of Materials
SA-3	Stage Construction Details
SA-4	Temporary Concrete Barrier
SA-5	Approach Slab & Deck Repairs
SA-6	Existing Expansion Joint Removal
SA-7	Expansion Joint Replacement at South Abutment
SA-8	Expansion Joint Replacement at Pier 2
SA-9	Expansion Joint Replacement at Pier 5
SA-10	Expansion Joint Replacement at Pier 8
SA-11	Expansion Joint Replacement at North Abutment
SA-12	Beam Encasement Cross Section Piers 2, 5 & 8
SA-13	Preformed Joint Strip Seal
SA-14	Preformed Joint Strip Seal
SA-15	Beam End Repairs
SA-16	Steel Diaphragm Replacement
SA-17	Drainage Scupper, DS-33
SA-18	Drainage System
SA-19	Type I Bearing Details
SA-20	Type I Bearing Details
SA-21	Abutment Repairs
SA-22	Pier Repairs
SA-23	Bar Splicer Assembly and Mechanical Splicer Details

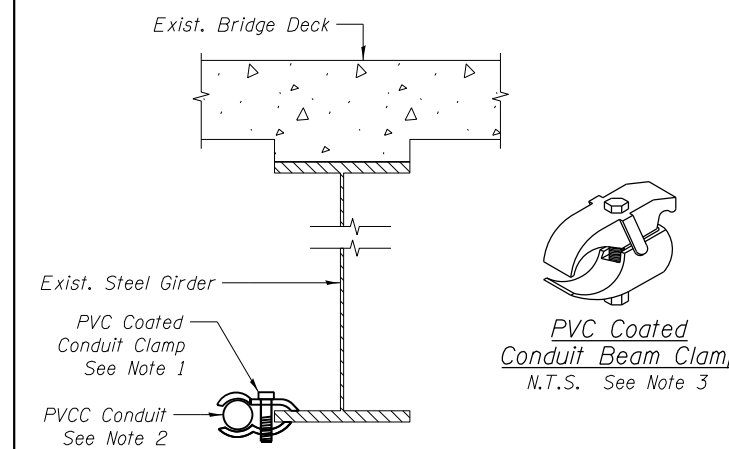
TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Bituminous Materials (Tack Coat)	Pound	540		540
Portland Cement Concrete Surface Removal - Butt Joint	Sq. Yd.	360		360
Hot-Mix Asphalt Surface Course, IL-9.5 Mix "D", N70	Ton	130		130
Concrete Removal	Cu. Yd.	57.0		57.0
Protective Shield	Sq. Yd.	5,147		5,147
Concrete Superstructure	Cu. Yd.	175.6		175.6
Protective Coat	Sq. Yd.	6,075		6,075
Furnishing and Erecting Structural Steel	Pound	17,600		17,600
Reinforcement Bars, Epoxy Coated	Pound	16,650		16,650
Bar Splicers	Each	300		300
Preformed Joint Strip Seal	Foot	335		335
Elastomeric Bearing Assembly, Type I	Each	80		80
Anchor Bolts, 1"	Each	160		160
Epoxy Crack Injection	Foot		150	150
Fiber Wrap	Sq. Ft.		561	561
Clean and Reseal Relief Joint	Foot	114		114
Bridge Drainage System	L. Sum		0.5	0.5
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	3,832		3,832
Pavement Removal (Special)	Sq. Yd.	2		2
Approach Slab Repair (Partial Depth)	Sq. Yd.	10		10
Jack and Remove Existing Bearings	Each	52		52
Structural Steel Removal	Pound	2,430		2,430
Structural Steel Repair	Pound	430		430
Combination Curb and Gutter Removal and Replacement Less Than or Equal to 10 Feet	Foot	5		5
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq. Yd.	4,152		4,152
Bridge Deck Scarification, 3/4"	Sq. Yd.	4,152		4,152
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	93	427	520
Structural Repair of Concrete (Depth Greater than 5 inches)	Sq. Ft.		689	689
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	6		6
Drainage Scuppers, DS-33	Each	4		4
Diamond Grinding (Bridge Section)	Sq. Yd.	3,670		3,670
Temporary Shoring and Cribbing	Each	28		28
Conduit Attached to Structure, 2" Dia., PVC Coated Galvanized Steel	Foot	762		762
Junction Box, Stainless Steel, Attached to Structure, 24" X 24" X 8"	Each	2		2
Acrylic Coating	Sq. Yd.		63	63

* Quantity includes repairs to median and sidewalk. See Sheet SA-5.

** See Temporary Shoring and Cribbing notes on sheet SA-21 & SA-22.

Conduit Beam Clamp Installation Detail



NOTES:

- All mounting hardware for the PVCC RGC conduit must be PVC coated.
- Secure the conduit with PVC coated conduit clamps or conduit beam clamps as shown at 5'-0" intervals for laterals and within 2'-0" maximum from any junction box, flexible conduit, or change in direction. All PVC coated conduit clamps or beam clamps will be included with the cost of the "conduit attached to structure" pay item.
- All conduit attached to structure shall be PVC coated rigid steel conduit (PVCC RGC) typical.

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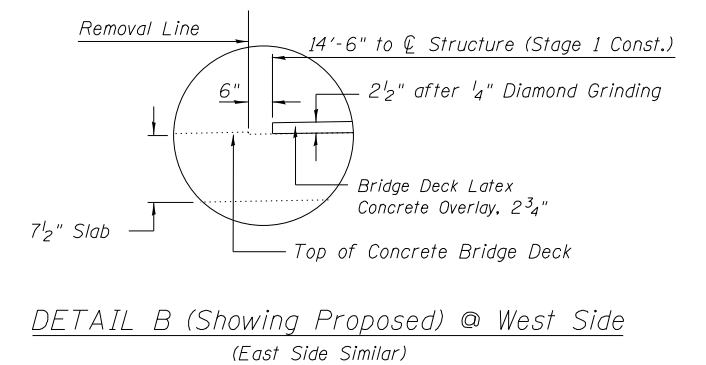
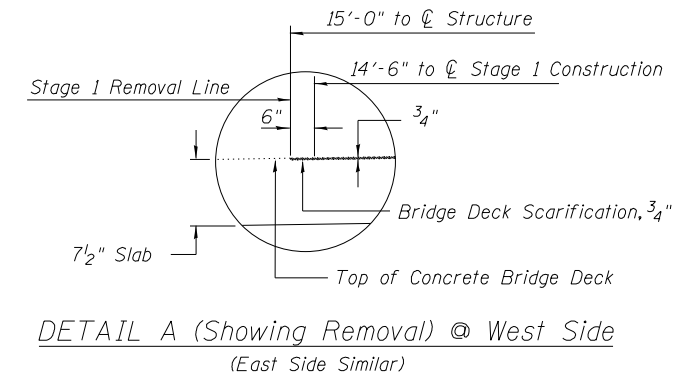
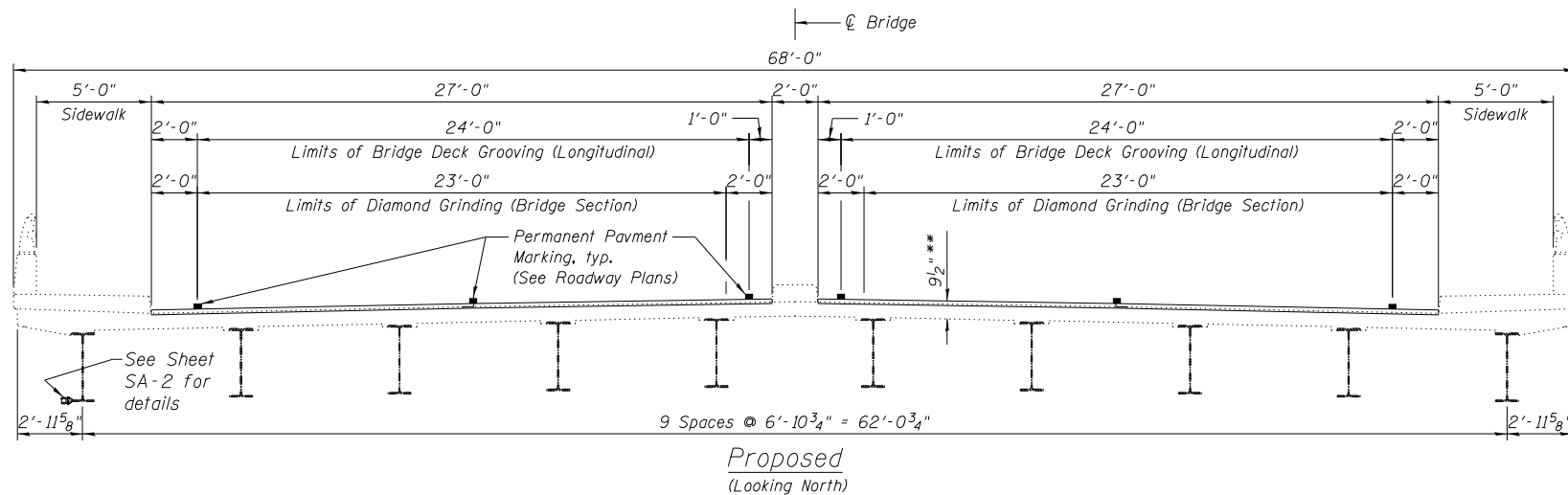
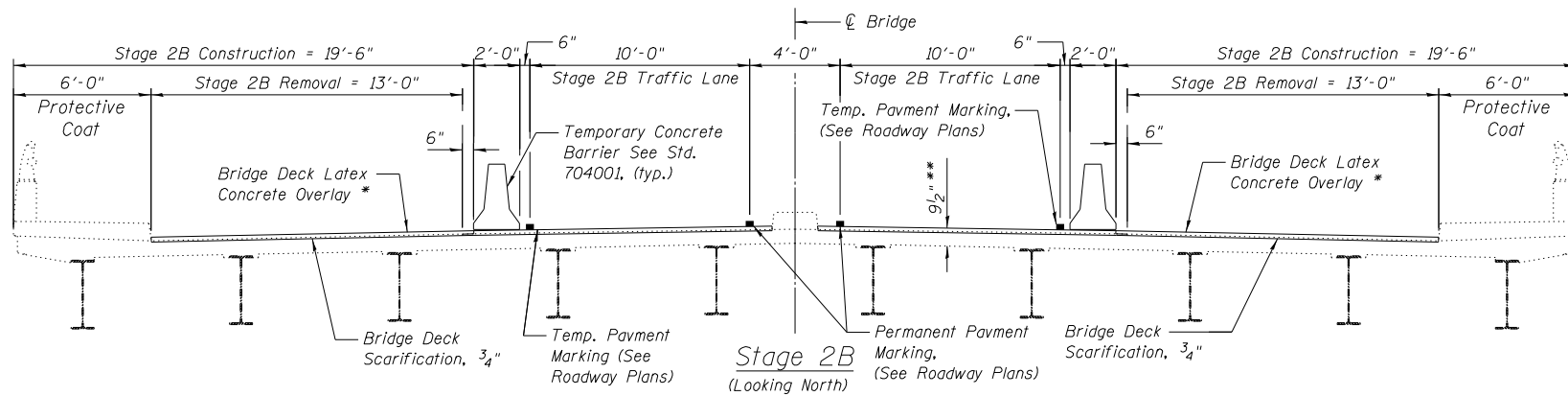
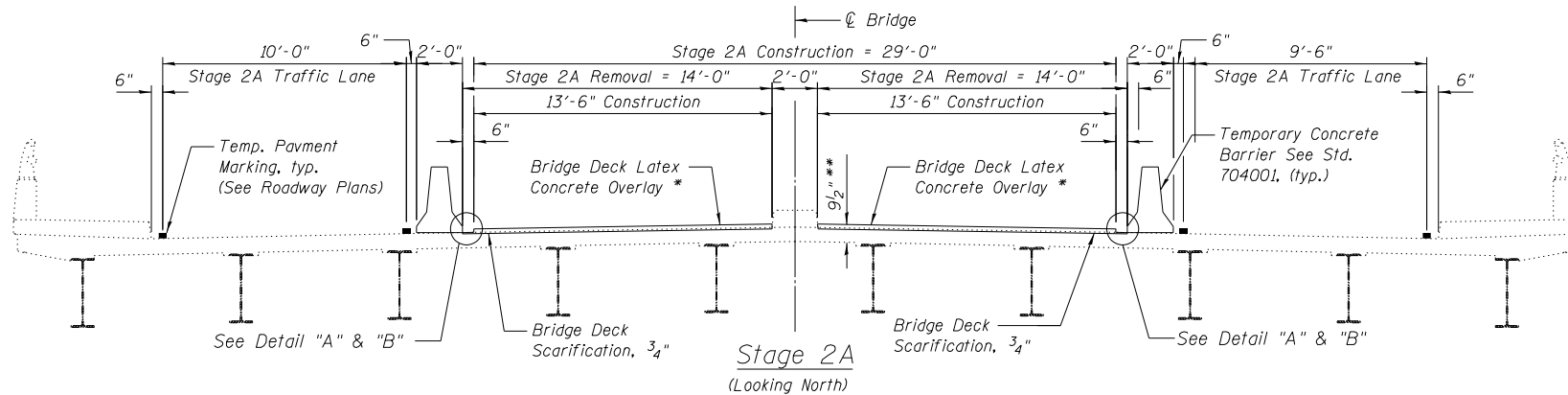
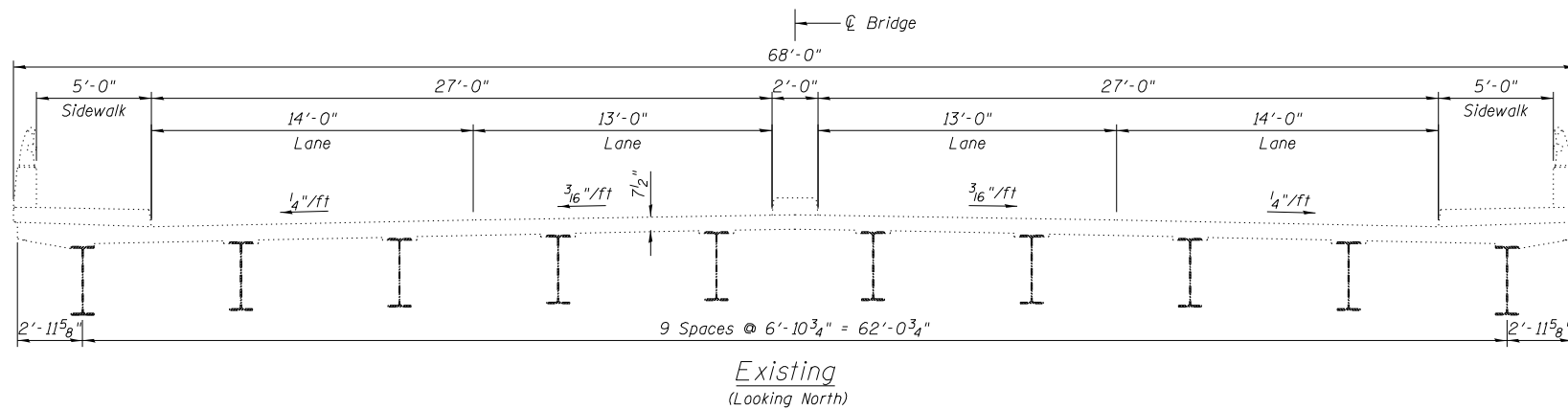
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PLOT DATE = 5/10/2024	CHECKED - ATB	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, GENERAL NOTES & BILL OF MATERIALS
SHEET NO. 016-0195**

SHEET NO. SA-2 OF SA-23 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	39
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	



* Place 2 3/4" latex concrete overlay on bridge deck. Final overlay thickness shall be 2 1/2" after 1/4" Diamond Grinding. (See Special Provisions.)

** Prior to grinding

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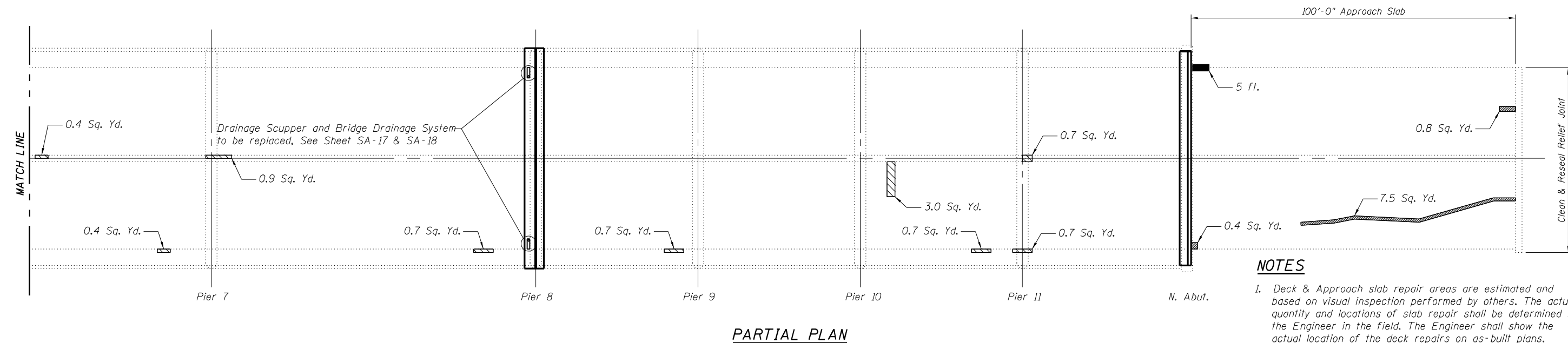
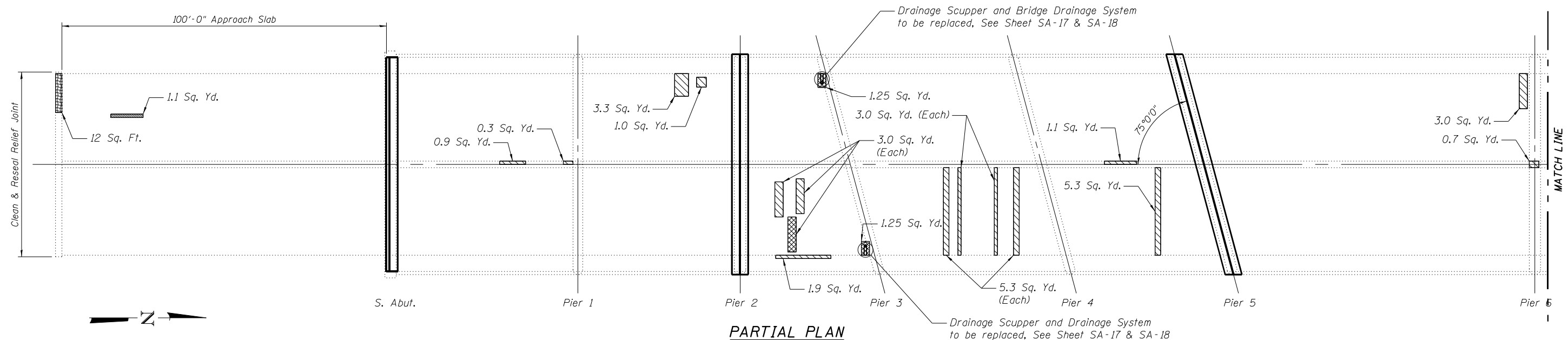
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**STATE OF ILLINOIS
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**STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 016-0195**

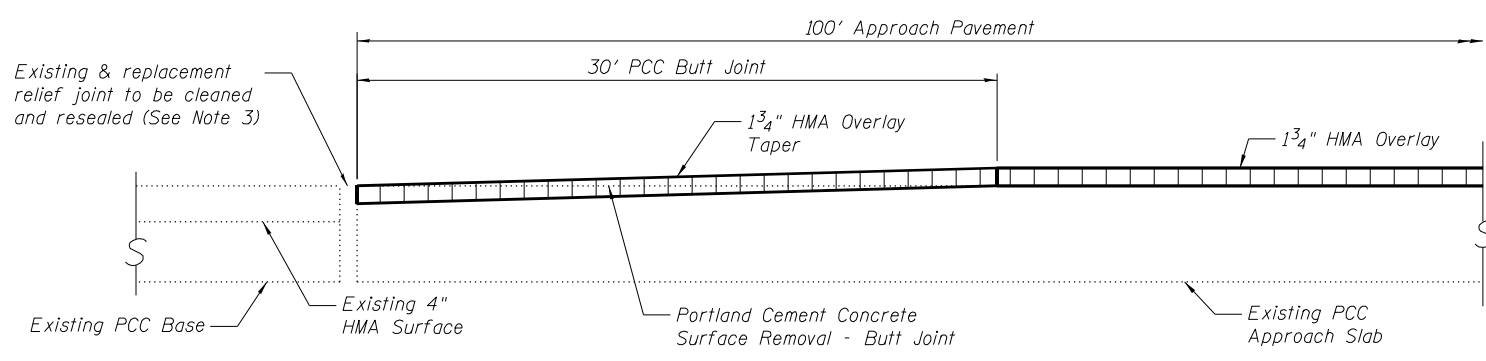
SHEET NO. SA-3 OF SA-23 SHEETS

FAU. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	40
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



- NOTES**
- Deck & Approach slab repair areas are estimated and based on visual inspection performed by others. The actual quantity and locations of slab repair shall be determined by the Engineer in the field. The Engineer shall show the actual location of the deck repairs on as-built plans.
 - The Contractor shall take precautions not to damage the existing beams and shear studs during the construction. Any damage shall be repaired by the Contractor to the satisfaction of the Engineer at no additional cost.
 - Existing pavement relief joint removal and replacement shall be paid for as Pavement Removal (Special). See Special Provisions.

- LEGEND**
- Expansion Joint Replacement (See Sheet SA-7 to SA-11)
 - Pavement Relief Joint Removal and Replacement (See Note 3)
 - Approach slab repair
 - Partial depth concrete deck slab repair (See Note A)
 - Full depth concrete deck slab repair
 - Combination Curb and Gutter Removal and Replacement Less Than or Equal To 10 ft



NOTE "A"
 Partial depth repairs quantities on the bridge deck is shown for information only. Formed concrete repairs to the bridge median and sidewalks shall be paid for as Structural Repair of Concrete (Depth Equal to or Less than 5 inches)

BILL OF MATERIAL

Item	Unit	Total
Pavement Removal (Special)	Sq. Yd.	2
Clean & Reseal Relief Joint	Foot	114
Approach Slab Repair (Partial Depth)	Sq. Yd.	10
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	6
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. ft.	93
Combination Curb and Gutter Removal and Replacement Less Than or Equal to 10 ft	Foot	5

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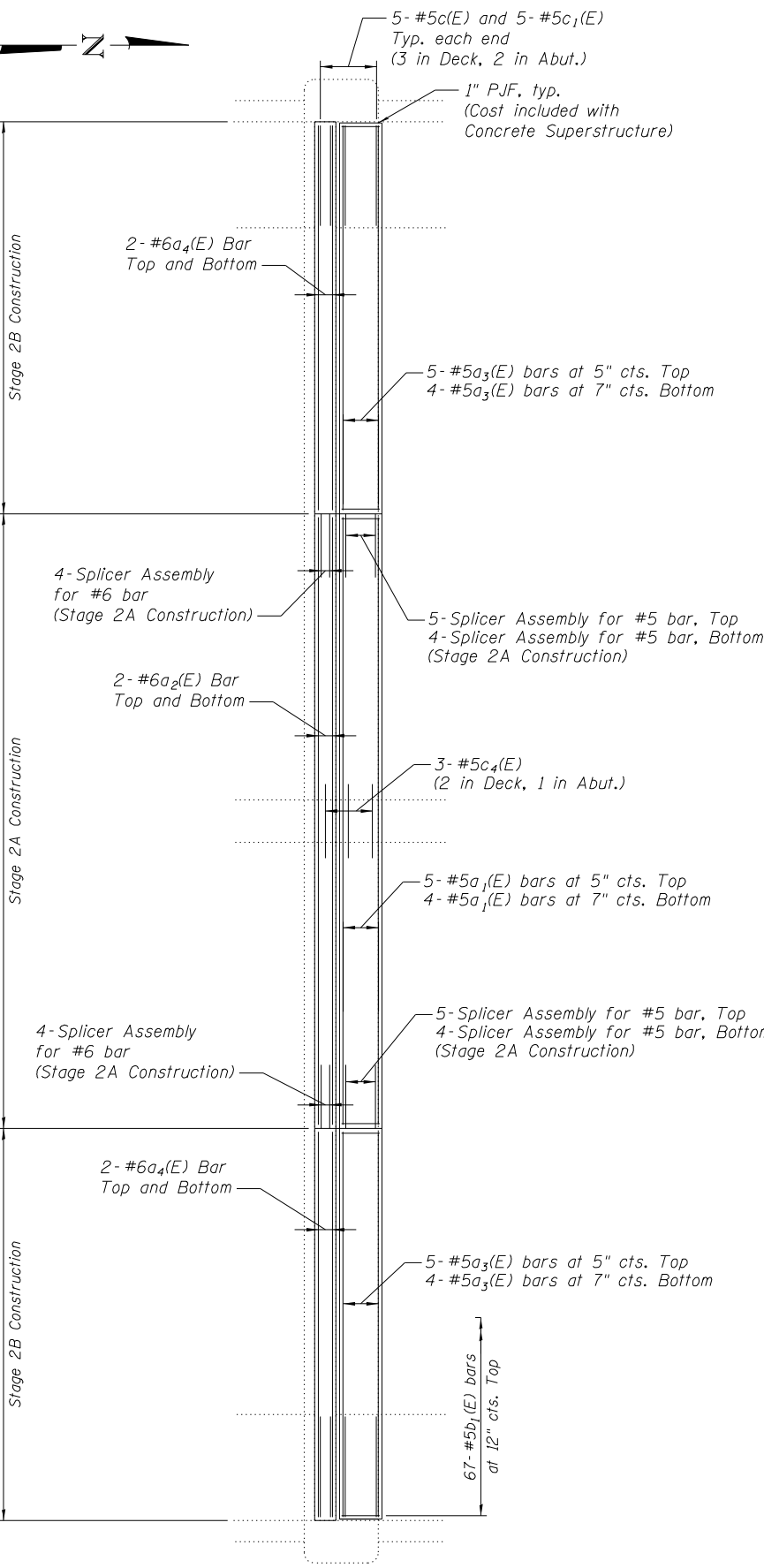
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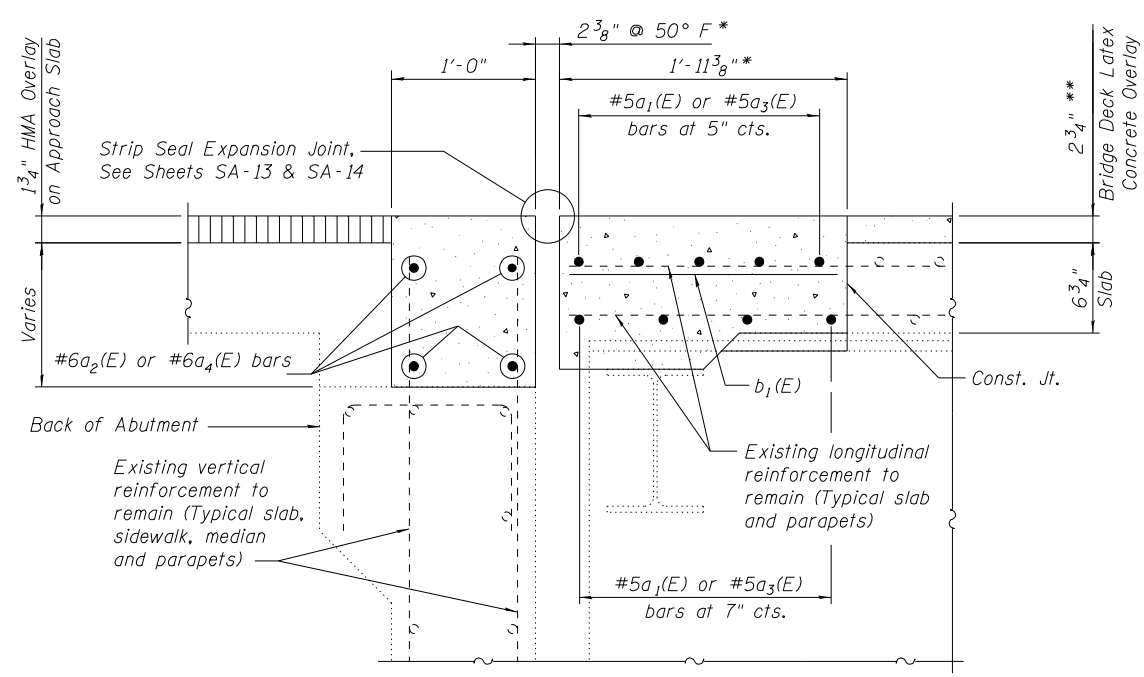
**APPROACH SLAB & DECK REPAIRS
 STRUCTURE NO. 016-0195**

SHEET NO. SA-5 OF SA-23 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	42
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				

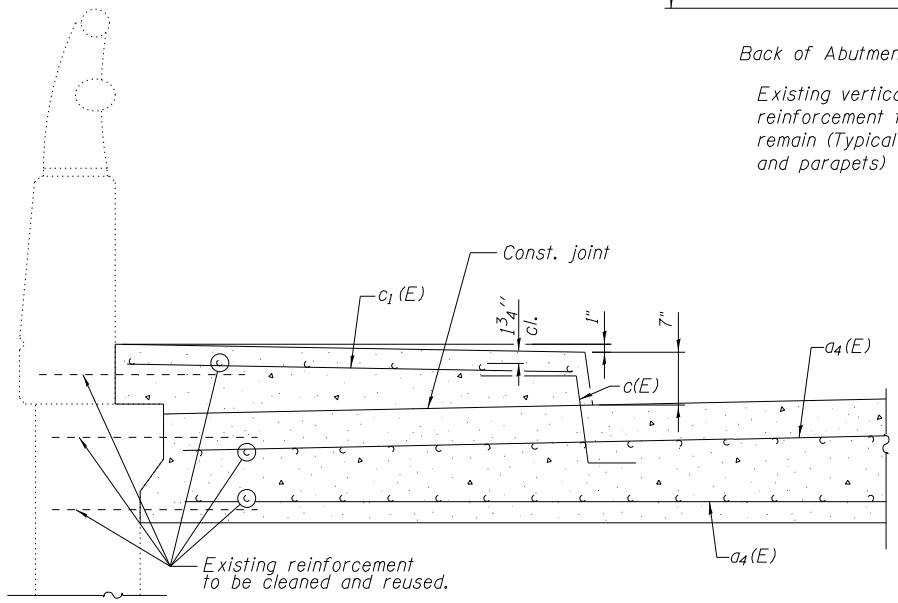


EXPANSION JOINT PLAN
SOUTH ABUTMENT

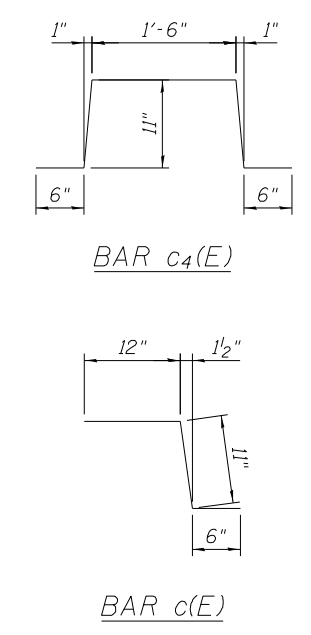


TYPICAL SECTION AT SOUTH ABUTMENT EXPANSION JOINT

* Showing dimension for rolled rail joint. Contractor shall adjust for welded rail joint. See Sheet SA-13 & SA-14 for details.
** Prior to grinding

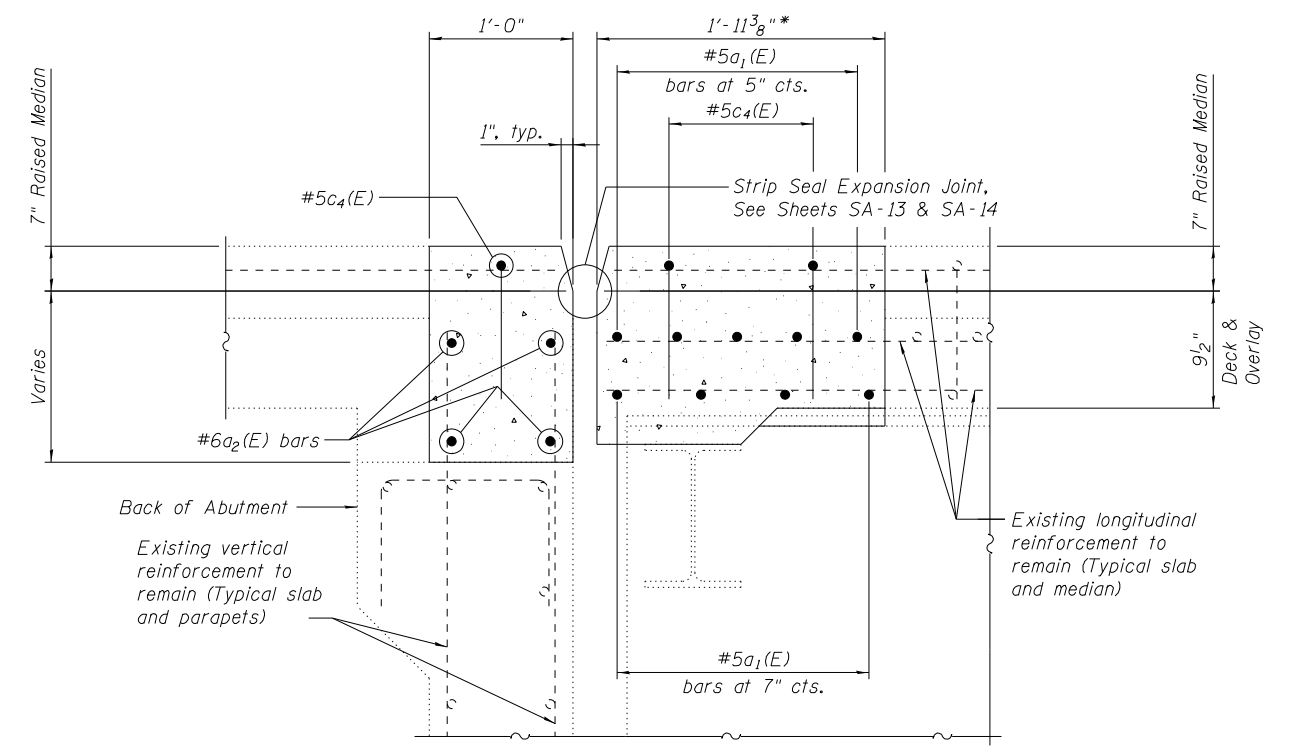


TYPICAL SIDEWALK SECTION THRU ABUTMENT
(Match Adjacent)



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₁ (E)	9	#5	28'-9"	—
a ₂ (E)	4	#6	28'-9"	—
a ₃ (E)	18	#5	18'-2"	—
a ₄ (E)	8	#6	18'-2"	—
b ₁ (E)	67	#5	1'-9"	—
c(E)	10	#5	2'-5"	U
c ₁ (E)	10	#5	4'-8"	—
c ₄ (E)	3	#5	4'-4"	U
Concrete Superstructure			Cu. Yd.	10.0
Reinforcement Bars, Epoxy Coated			Pound	1,220



TYPICAL SECTION THRU RAISED MEDIAN

NOTES
For details of Strip Seal Expansion Joints, see Sheets SA-13 & SA-14.
Existing longitudinal parapet reinforcement shall remain in place.
See Sheet SA-23 for details of Bar Splicers.

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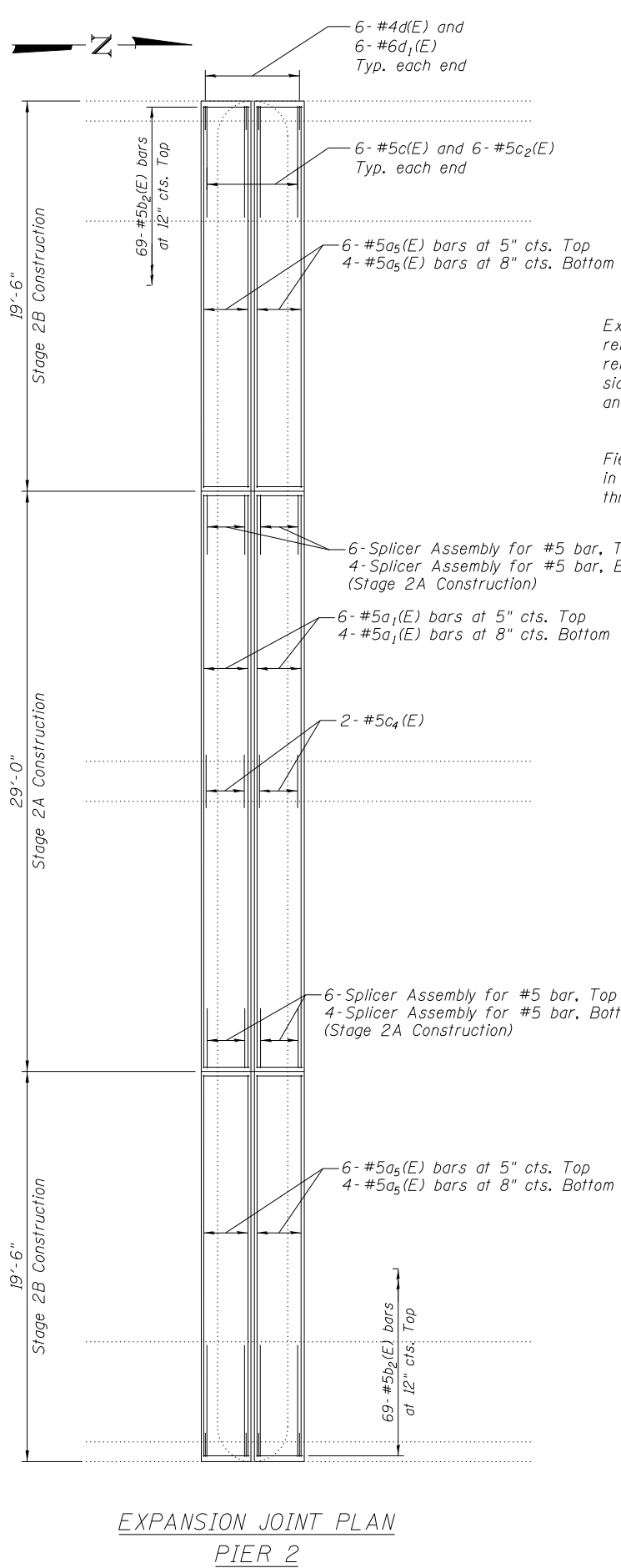
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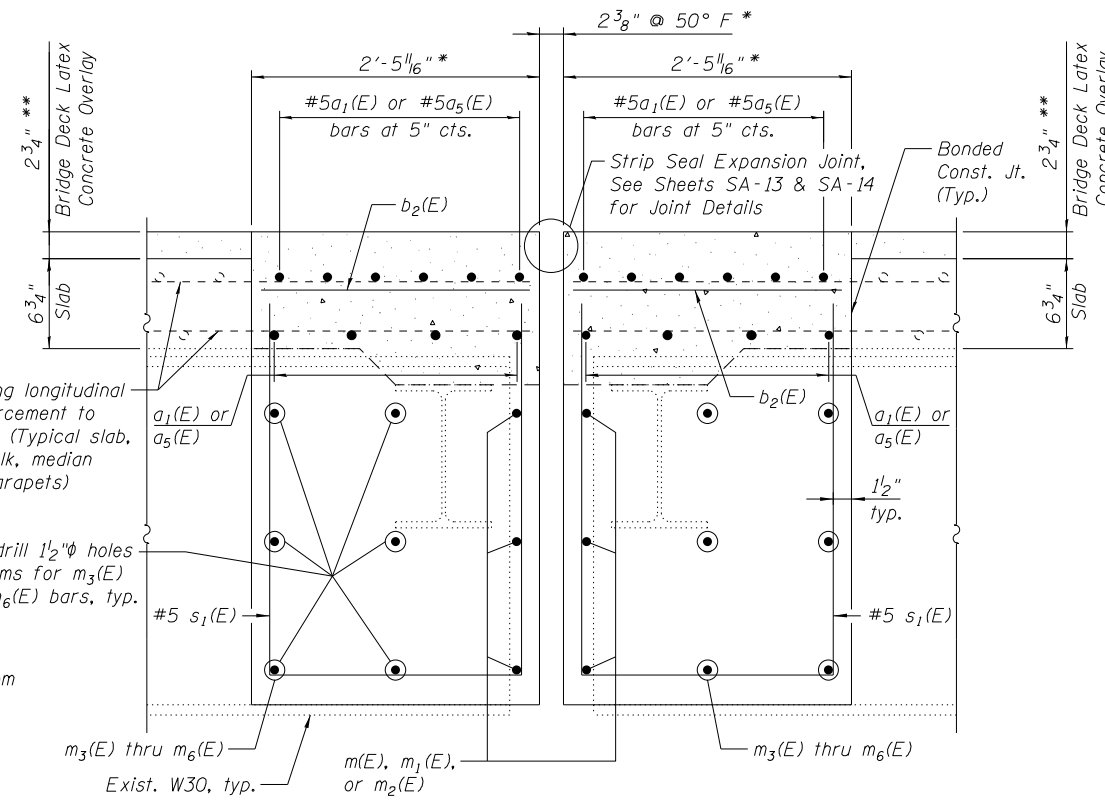
EXPANSION JOINT REPLACEMENT AT SOUTH ABUTMENT
STRUCTURE NO. 016-0195

SHEET NO. SA-7 OF SA-23 SHEETS

FAU-RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W48			ILLINOIS FED. AID PROJECT	

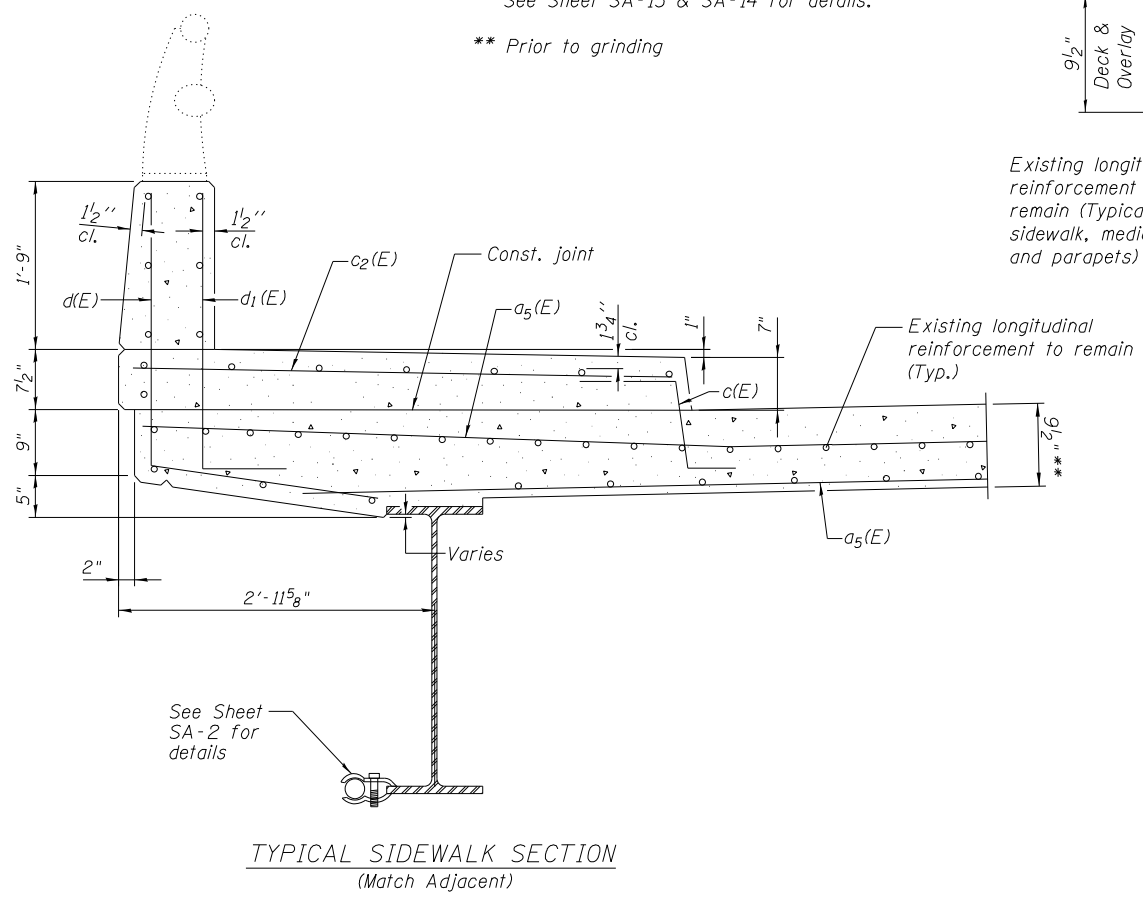


EXPANSION JOINT PLAN
PIER 2

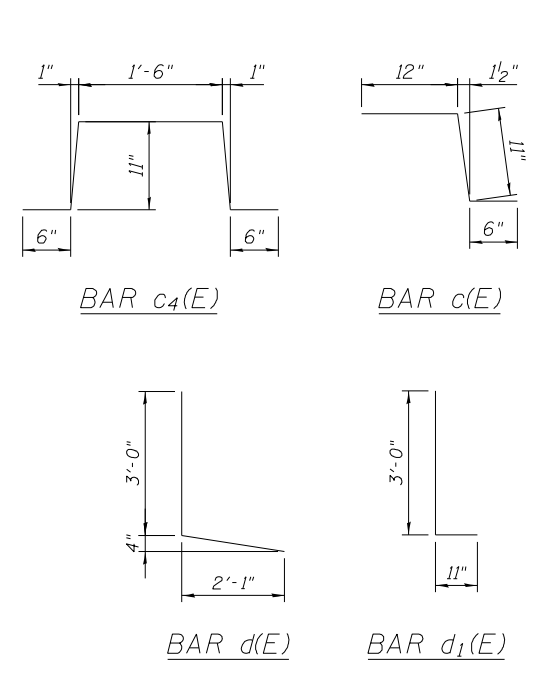


TYPICAL SECTION AT EXPANSION JOINT PIER 2

* Showing dimension for rolled rail joint.
Contractor shall adjust for welded rail joint.
See Sheet SA-13 & SA-14 for details.
** Prior to grinding

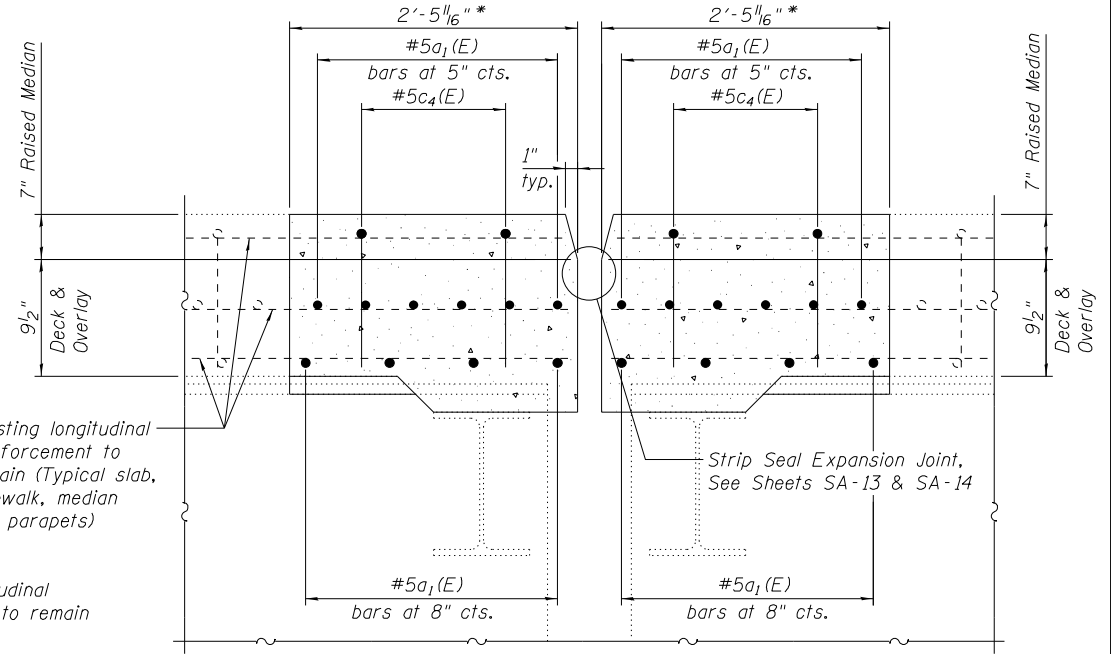


TYPICAL SIDEWALK SECTION
(Match Adjacent)



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₁ (E)	20	#5	28'-9"	
a ₅ (E)	40	#5	19'-2"	
b ₂ (E)	138	#5	2'-3"	
c(E)	12	#5	2'-5"	
c ₂ (E)	12	#5	5'-8"	
c ₄ (E)	4	#5	4'-4"	
d(E)	12	#4	5'-1"	L
d ₁ (E)	12	#6	3'-11"	L
Concrete Superstructure			Cu. Yd.	13.7
Reinforcement Bars, Epoxy Coated			Pound	1,960



TYPICAL SECTION THRU RAISED MEDIAN

Note: Concrete encasement omitted for clarity.

NOTES

- For details of Strip Seal Expansion Joints, see Sheets SA-13 & SA-14.
- See Sheet SA-12 for Beam Encasement Cross Section Piers 2, 5 & 8 details and bar bill.
- Existing longitudinal parapet reinforcement shall remain in place.
- See Sheet SA-23 for details of Bar Splicers.

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 AECOM, USA
 303 EAST WACKER DRIVE, SUITE 1400
 CHICAGO, IL 60601-5276
 PHONE: (312) 373-7700 FAX: (312) 373-6800
 16-0195-01-0195-08 Joint Repair Pier 2.dgn



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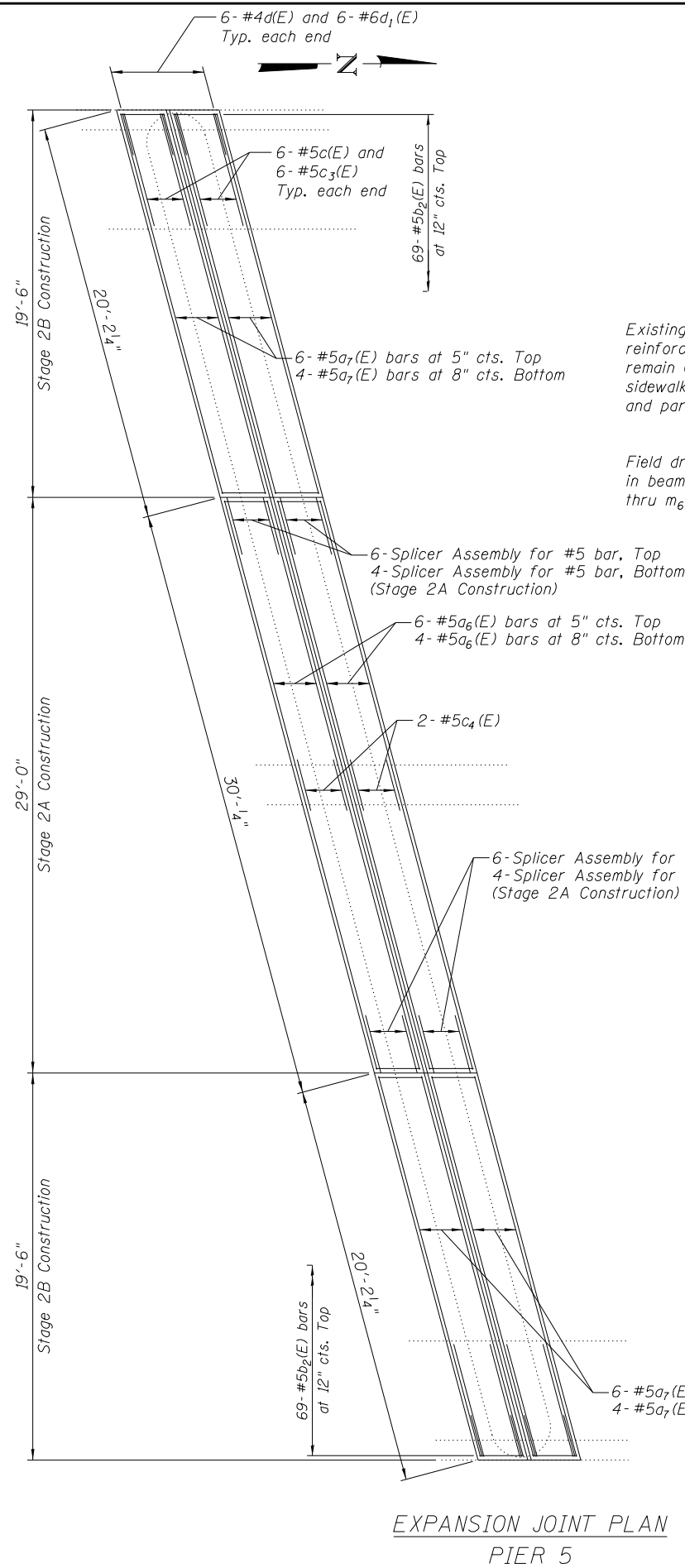
EXPANSION JOINT REPLACEMENT AT PIER 2
 STRUCTURE NO. 016-0195

SHEET NO. SA-8 OF SA-23 SHEETS

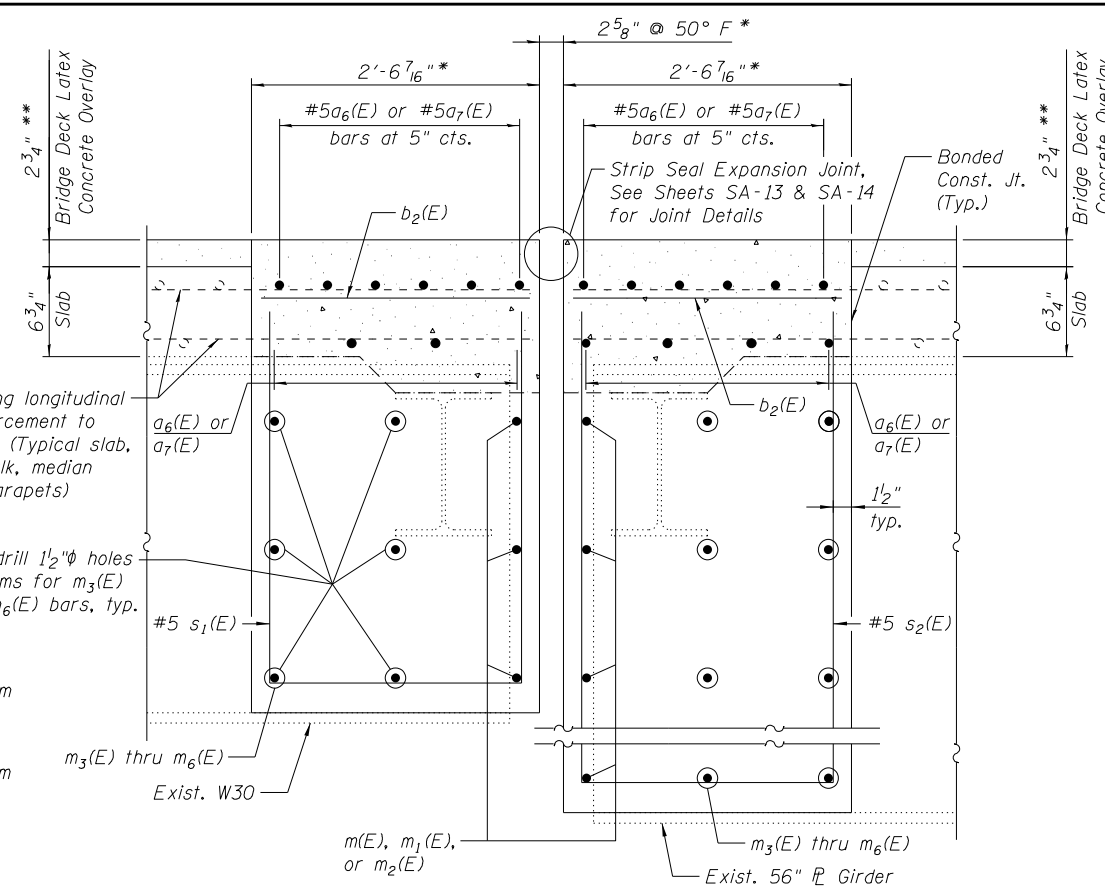
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CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT

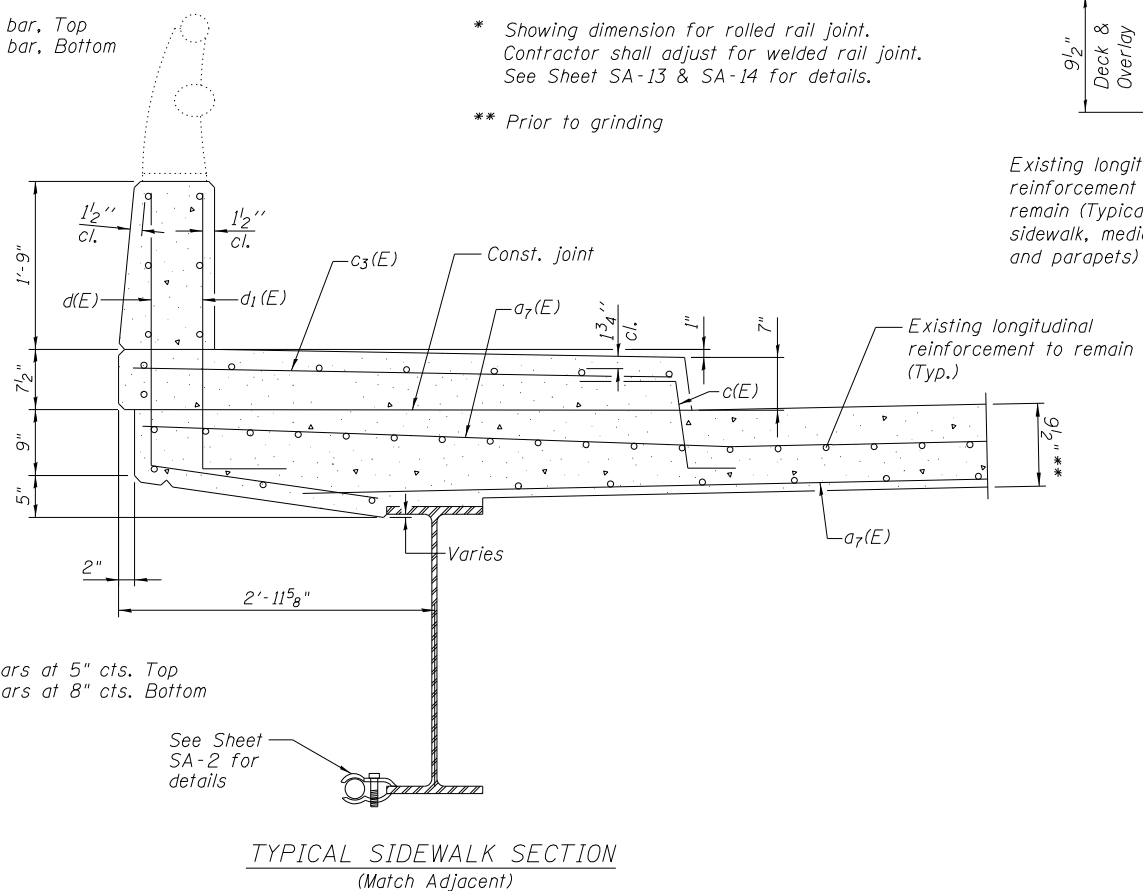
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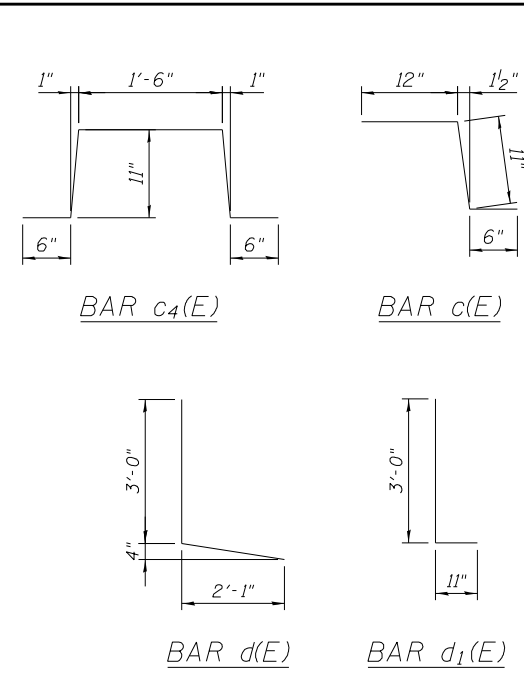
EXPANSION JOINT PLAN
PIER 5



TYPICAL SECTION AT EXPANSION JOINT PIER 5
(© Right Angle to Joint)

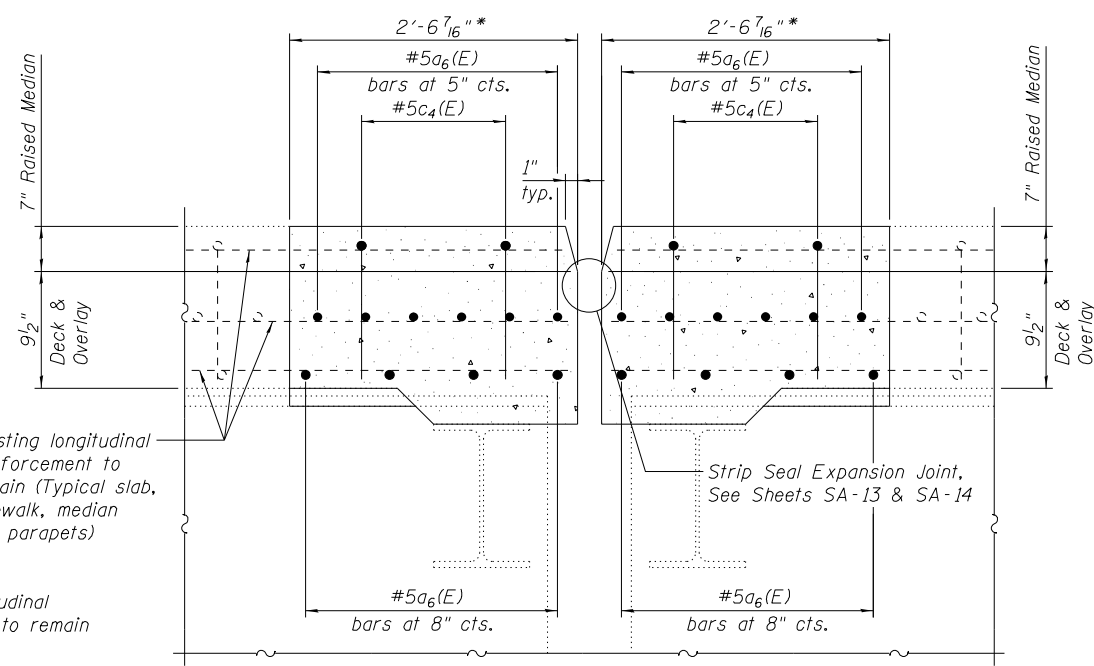


TYPICAL SIDEWALK SECTION
(Match Adjacent)



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a6(E)	20	#5	29'-9"	
a7(E)	40	#5	19'-10"	
b2(E)	138	#5	2'-3"	
c(E)	12	#5	2'-5"	
c3(E)	12	#5	5'-10"	
c4(E)	4	#5	4'-4"	
d(E)	12	#4	5'-1"	L
d1(E)	12	#6	3'-11"	L
Concrete Superstructure			Cu. Yd.	14.1
Reinforcement Bars, Epoxy Coated			Pound	2,010



TYPICAL SECTION THRU RAISED MEDIAN
Note: Concrete encasement omitted for clarity.

NOTES
 For details of Strip Seal Expansion Joints, see Sheets SA-13 & SA-14.
 See Sheet SA-12 for Beam Encasement Cross Section Piers 2, 5 & 8 details and bar bill.
 Existing longitudinal parapet reinforcement shall remain in place.
 See Sheet SA-23 for details of Bar Splicers.



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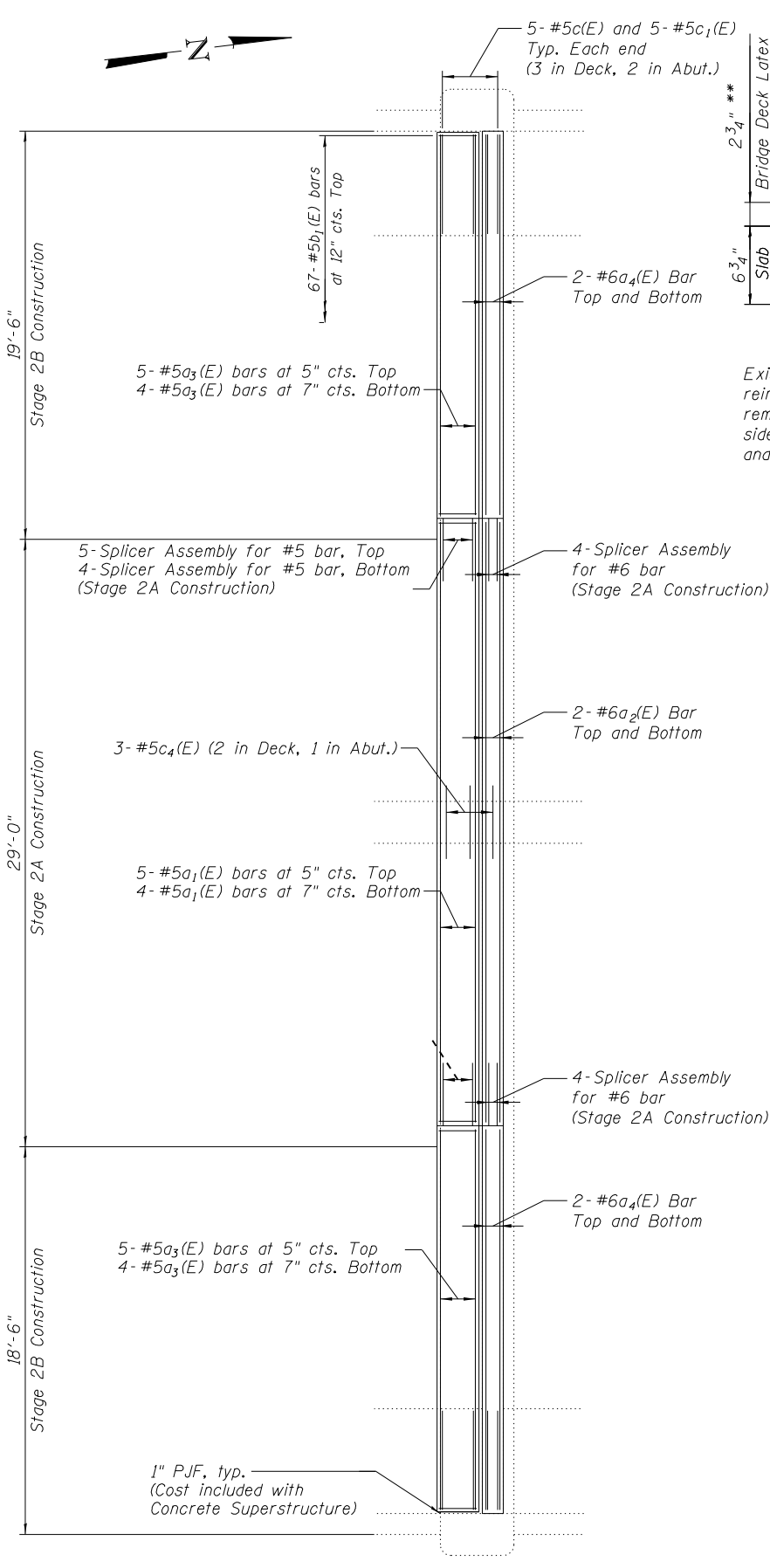
EXPANSION JOINT REPLACEMENT AT PIER 5
STRUCTURE NO. 016-0195

SHEET NO. SA-9 OF SA-21 SHEET 3

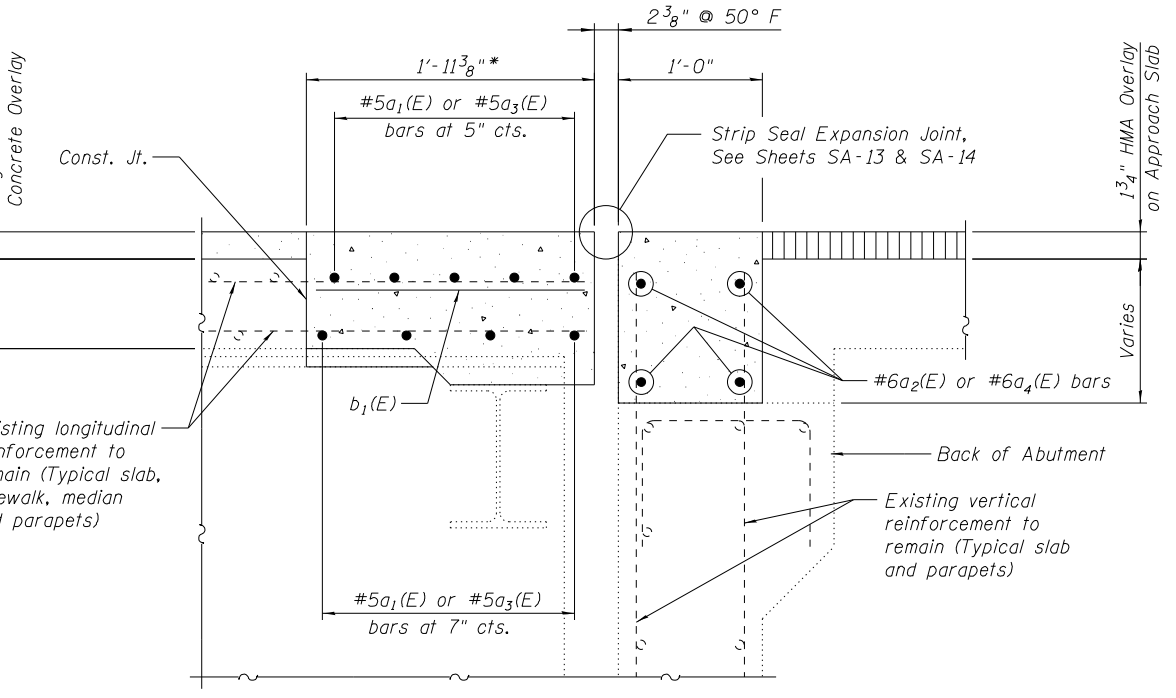
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CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT

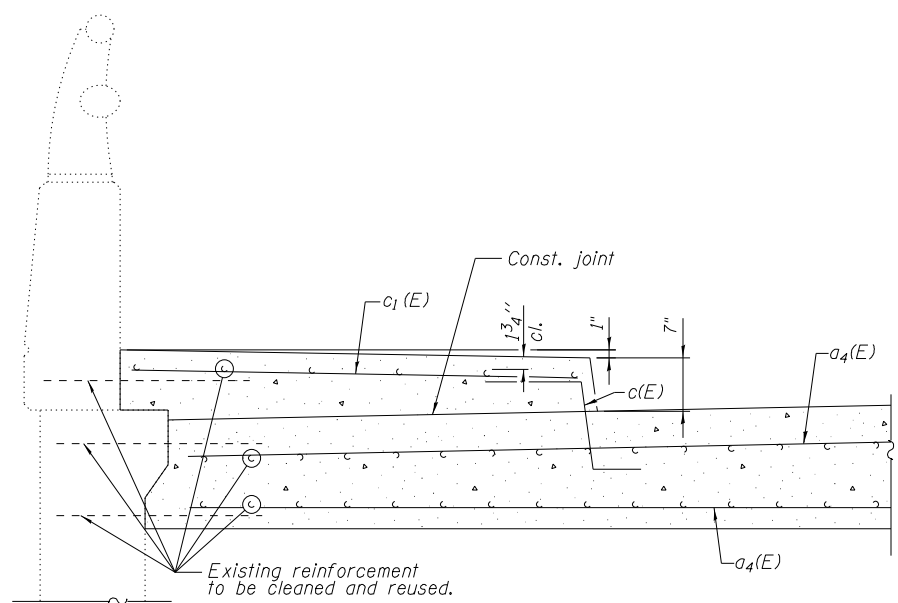
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 AECOM, USA
 CHICAGO, IL 60601-5276
 PHONE: (312) 373-7700 FAX: (312) 373-6800



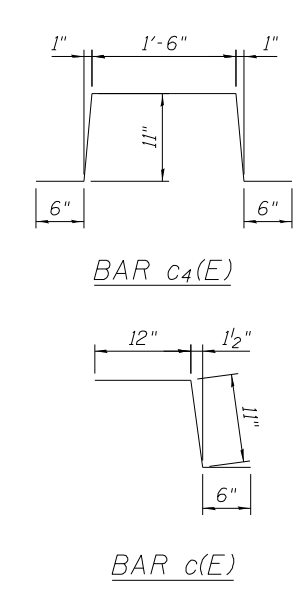
EXPANSION JOINT PLAN
NORTH ABUTMENT



TYPICAL SECTION AT EXPANSION JOINT NORTH ABUTMENT

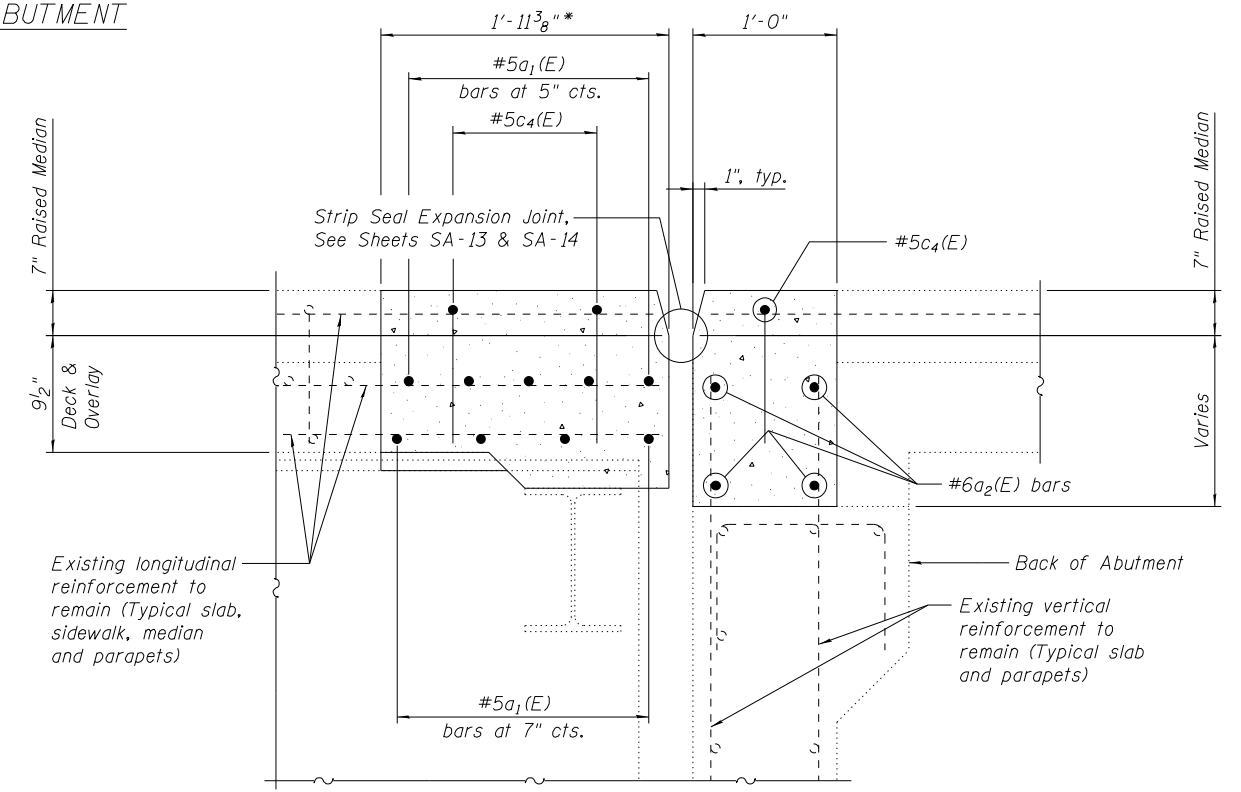


TYPICAL SIDEWALK SECTION THRU ABUTMENT
(Match Adjacent)



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₁ (E)	9	#5	28'-9"	—
a ₂ (E)	4	#6	28'-9"	—
a ₃ (E)	18	#5	18'-2"	—
a ₄ (E)	8	#6	18'-2"	—
b ₁ (E)	67	#5	1'-9"	—
c(E)	10	#5	2'-5"	U
c ₁ (E)	10	#5	4'-8"	—
c ₄ (E)	3	#5	4'-4"	U
Concrete Superstructure			Cu. Yd.	10.0
Reinforcement Bars, Epoxy Coated			Pound	1,220



TYPICAL SECTION THRU RAISED MEDIAN

NOTES
 For details of Strip Seal Expansion Joints, see Sheets SA-13 & SA-14.
 Existing longitudinal parapet reinforcement shall remain in place.
 See Sheet SA-23 for details of Bar Splicers.



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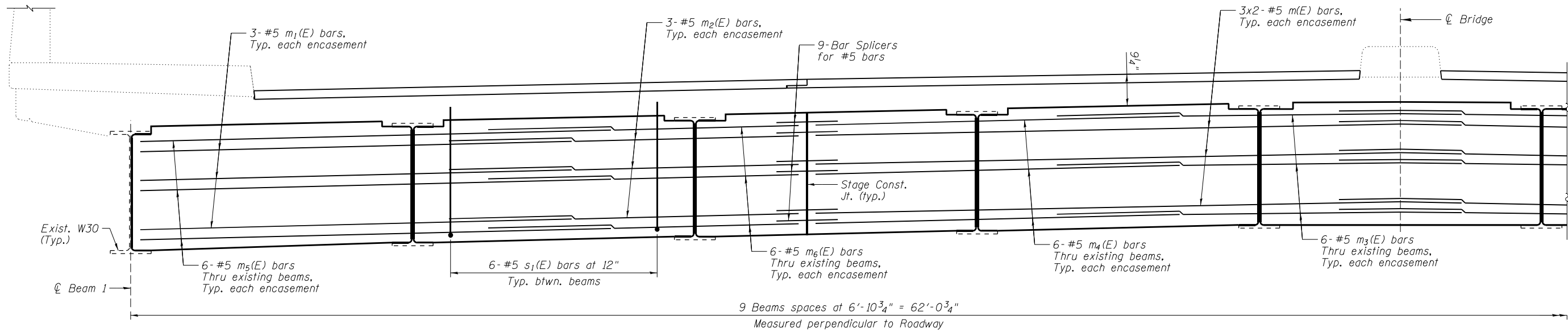
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT REPLACEMENT AT NORTH ABUTMENT
 STRUCTURE NO. 016-0195

SHEET NO. SA-11 OF SA-23 SHEETS

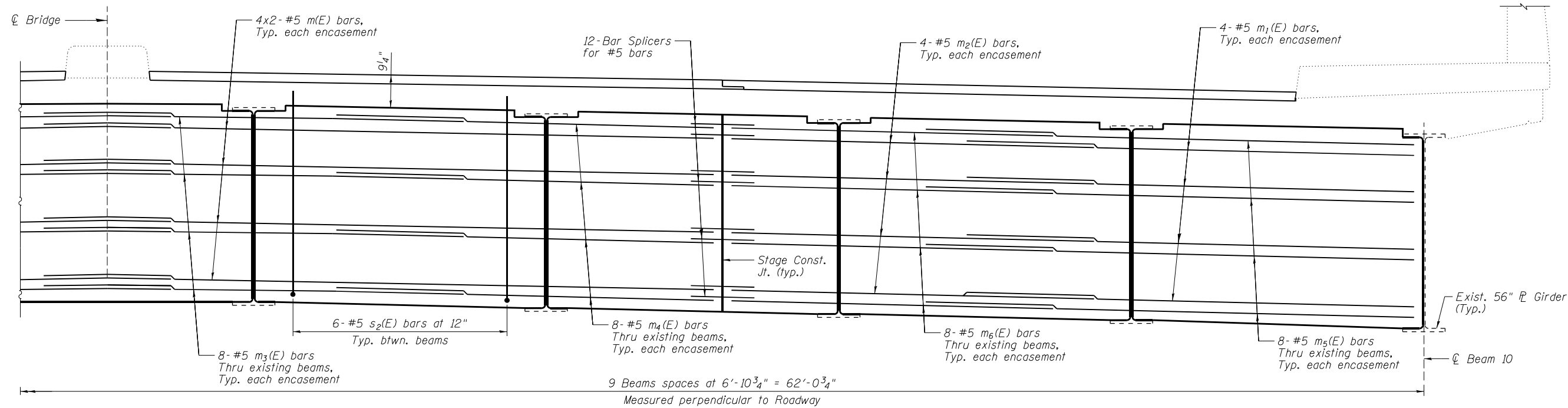
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3730	15-00131-01-BR	COOK	109	48
CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT



SECTION AT PIERS 2, 5 & 8 - SPANS 2, 3, 5 & 9

(Looking North, Showing West Side; Opp. side similar)



SECTION AT PIERS 5 & 8 - SPANS 6 & 8

(Looking North, Showing West Side; Opp. side similar)

MINIMUM BAR LAP

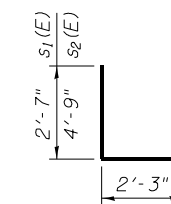
#5 bar = 3'-0"

NOTES:

See Sheets SA-3 for Stage Construction Details.

See Sheets SA-8 thru SA-10 for typical section thru expansion joint and beam encasement.

Clean beam ends of all loose rust and debris prior to pouring encasement concrete.



BARS s₁(E) & s₂(E)

SUPERSTRUCTURE BILL OF MATERIAL

Bar	No.	Size	Length	Shape
m(E)	40	#5	16'-4"	—
m ₁ (E)	40	#5	8'-11"	—
m ₂ (E)	40	#5	10'-11"	—
m ₃ (E)	80	#5	10'-5"	—
m ₄ (E)	80	#5	9'-5"	—
m ₅ (E)	80	#5	11'-11"	—
m ₆ (E)	80	#5	7'-11"	—
s ₁ (E)	216	#5	7'-5"	U
s ₂ (E)	108	#5	11'-9"	U
Concrete Superstructure			Cu. Yd.	111.7
Reinforcement Bars, Epoxy Coated			Pound	7,820

MODEL: 41001.DWG
 FILE: 41001.DWG
 USER: ken.moy
 DATE: 4/2/2024
 PROJECT: I-55/US-41
 SHEET: SA-12 OF SA-23 SHEETS
 CONTRACT: 15-00131-01-BR



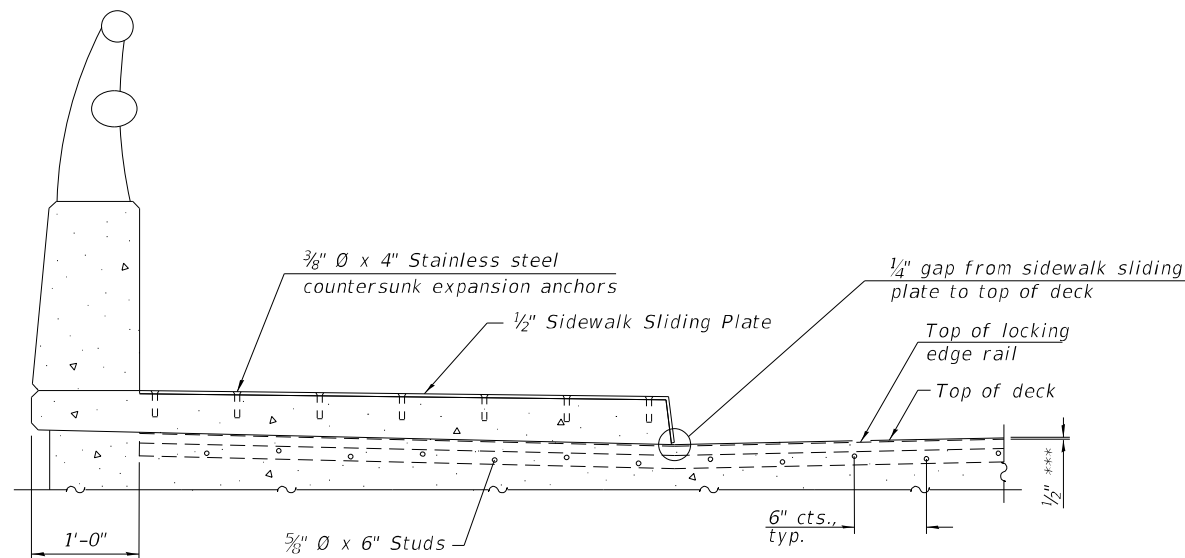
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PLOT SCALE = N.T.S.	CHECKED - ATB	REVISIONS -
PLOT DATE = 4/2/2024	DRAWN - MCC	REVISIONS -
	CHECKED - ATB	REVISIONS -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BEAM ENCASEMENT CROSS SECTION PIERS 2, 5 & 8
STRUCTURE NO. 016-0195**

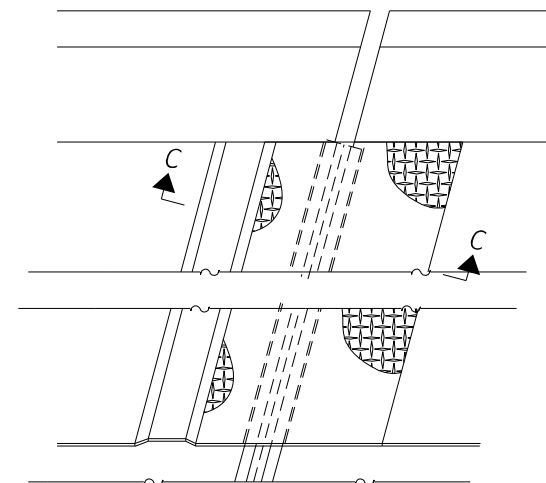
SHEET NO. SA-12 OF SA-23 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	49
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



SECTION AT RAISED SIDEWALK

*** Prior to grinding



PLAN AT RAISED SIDEWALK

(For skews ≤ 30°)
See Sheet SA-14 for Section C-C

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

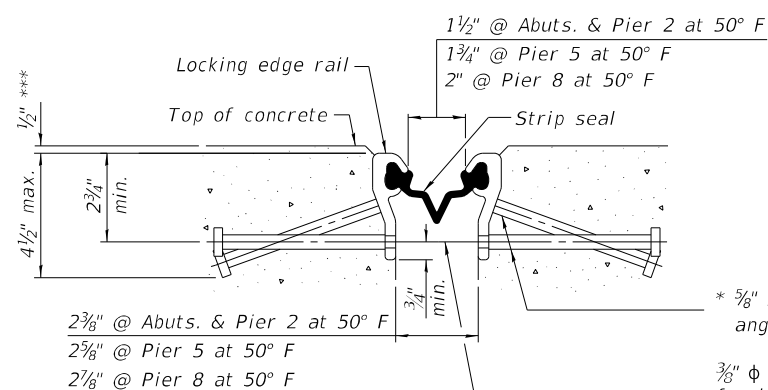
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

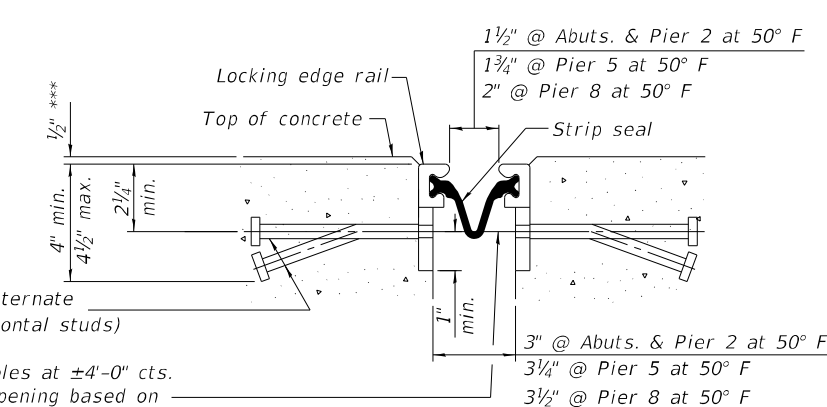
The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.

Cost of sidewalk sliding plates, embedded plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required.



SHOWING ROLLED RAIL JOINT

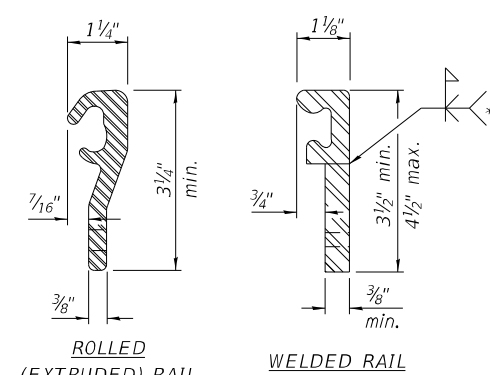


SHOWING WELDED RAIL JOINT

* 5/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)
3/8" Ø threaded rods in 1/16" Ø holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

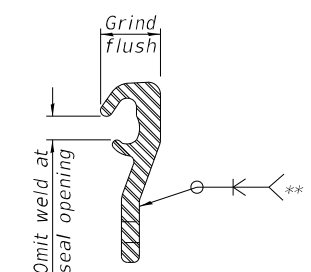
SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	335

(Sheet 1 of 2)

**PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 016-0195**

SHEET NO. SA-13 OF SA-23 SHEETS

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

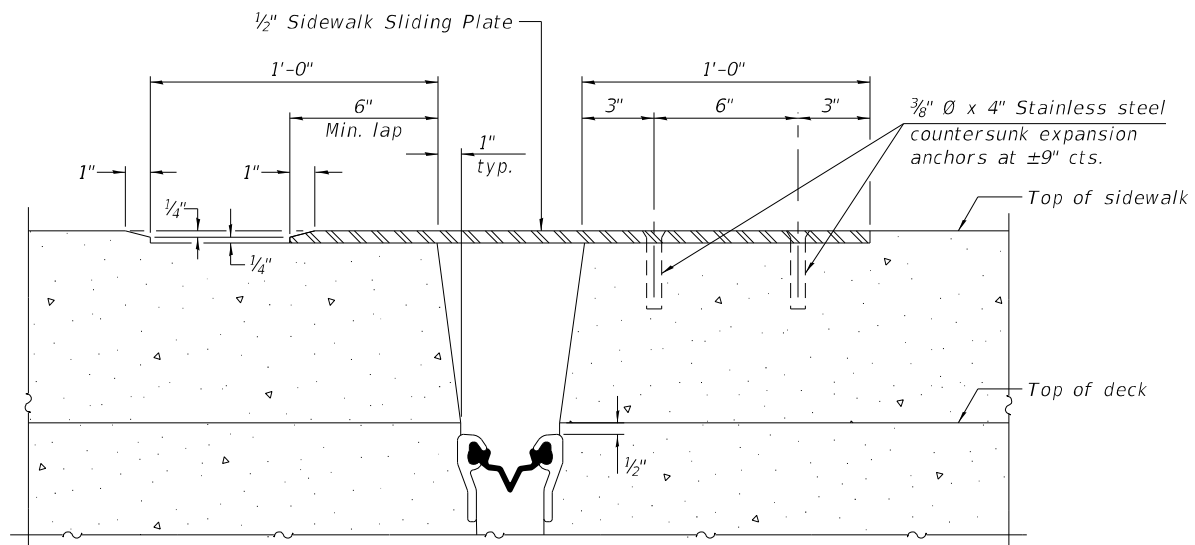
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	50
				CONTRACT NO. 62W48

ILLINOIS FED. AID PROJECT

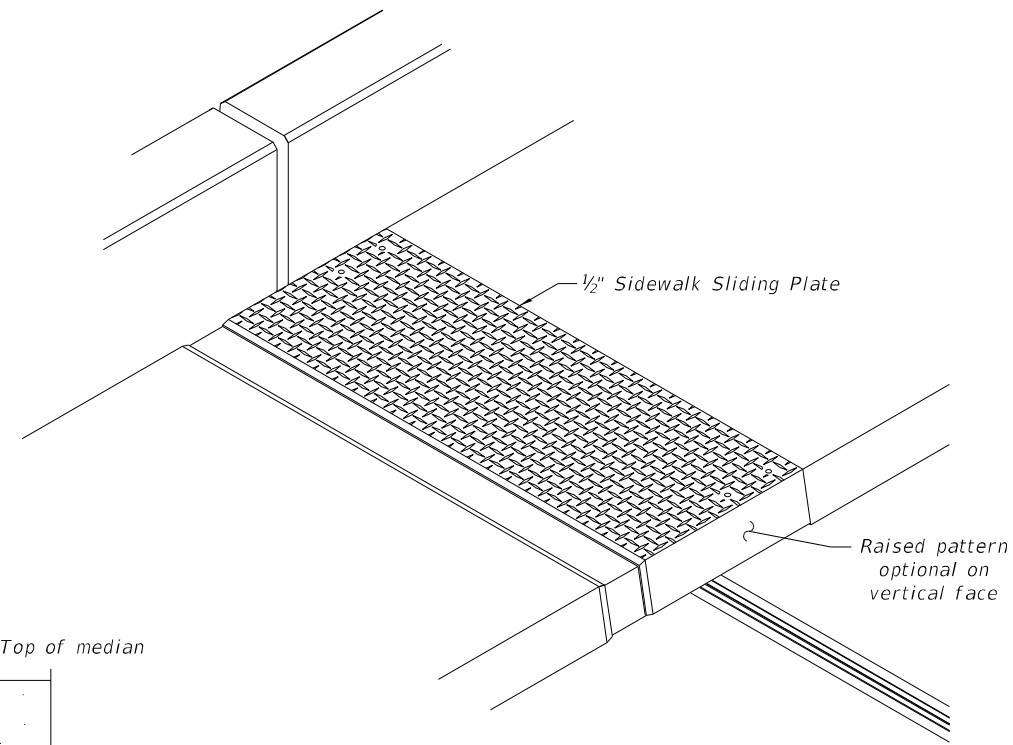
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 FILE: 1400DEL1M4MF8_PreformedJointStripSeal.dwg
 USER: ken.jinoy
 PLOT DATE: 4/2/2024
 AECOM, USA, Illinois Documents 60671539.DOT-Halsted-St. Bridge-Structure 016-0195-01-13 Ident: Details1.dwg



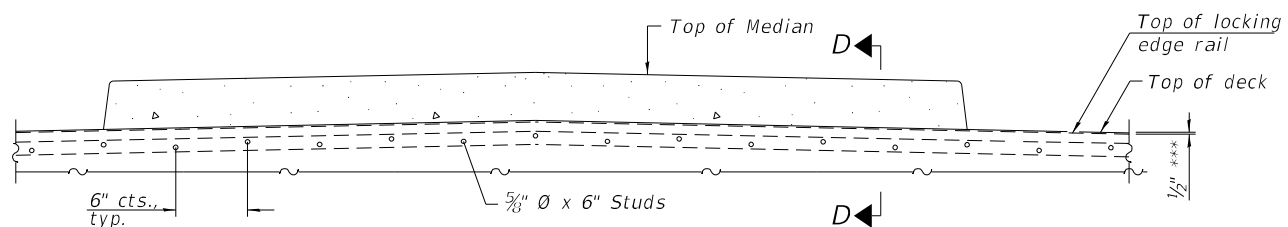
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	CHECKED - ATB	REVISED -



SECTION C-C

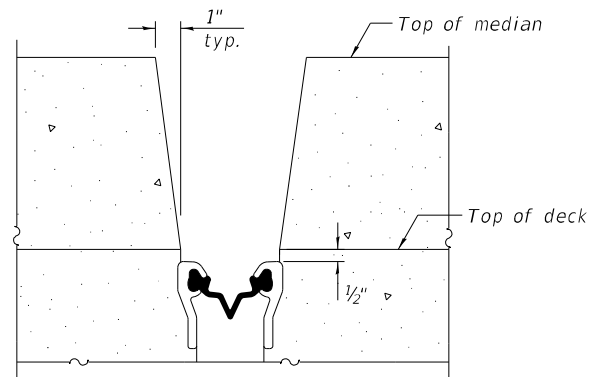


TRIMETRIC VIEW



SECTION AT MEDIAN

*** Prior to grinding



SECTION D-D
(at Rt. L's)

(Sheet 2 of 2)



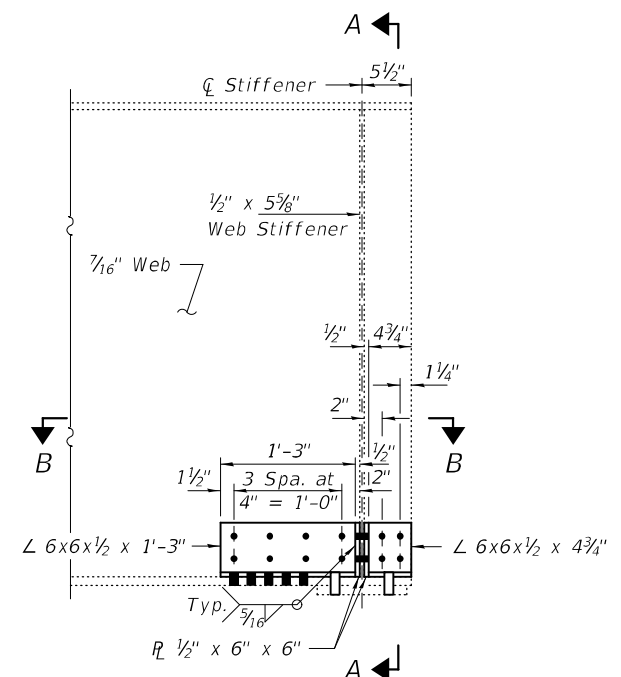
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

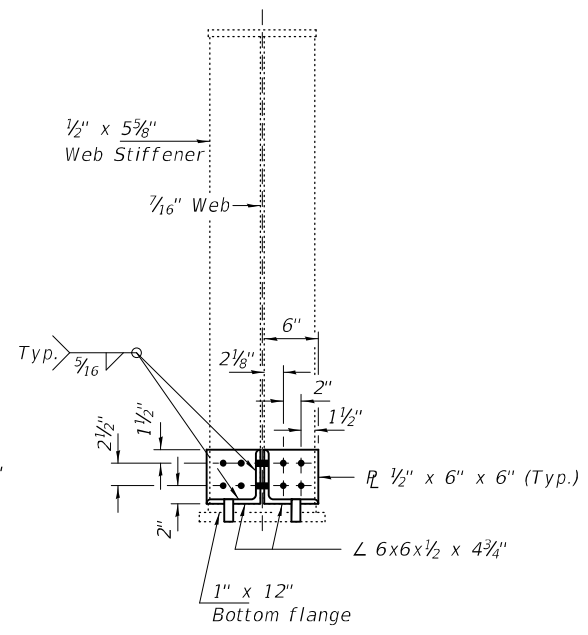
PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 016-0195

SHEET NO. SA-14 OF SA-23 SHEETS

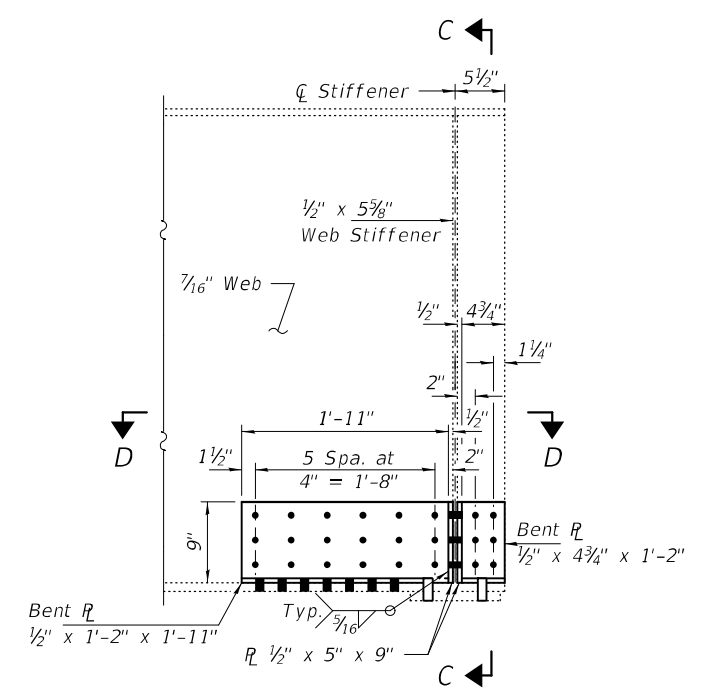
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	51
CONTRACT NO. 62W48				
		ILLINOIS	FED. AID PROJECT	



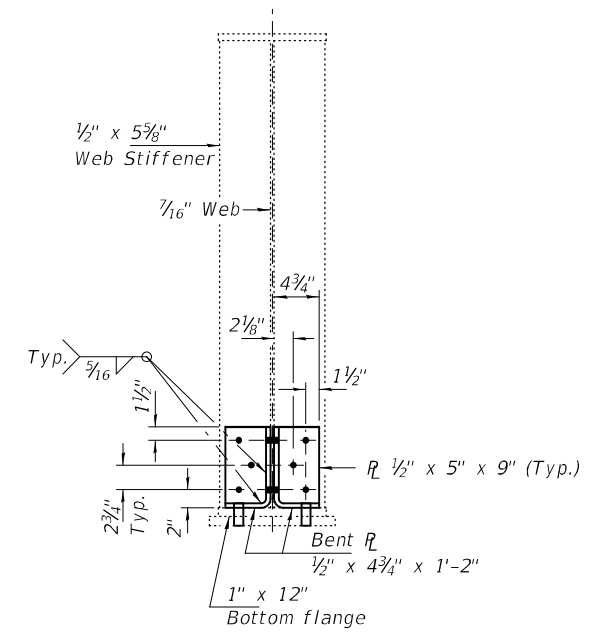
REPAIR C DETAILS
Pier 8, Beam 8, Span 8
Pier 8, Beam 9, Span 8



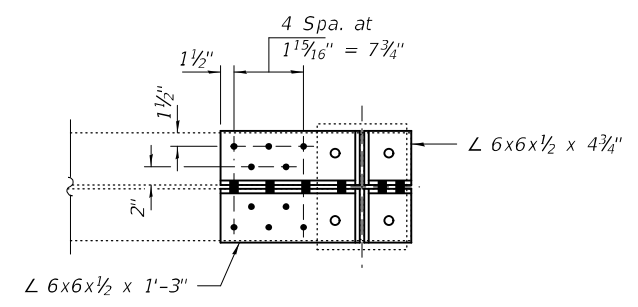
SECTION A-A



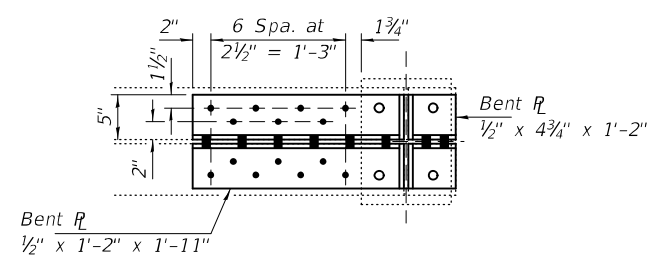
REPAIR B DETAILS
Pier 8, Beam 2, Span 8



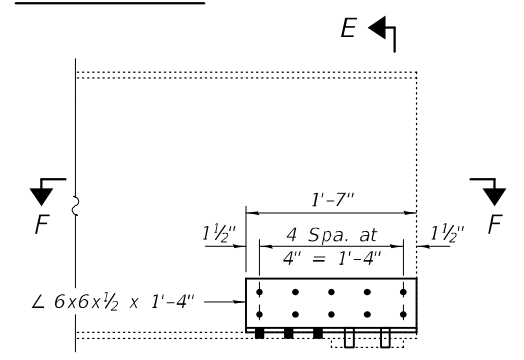
SECTION C-C



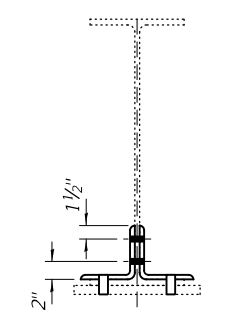
SECTION B-B



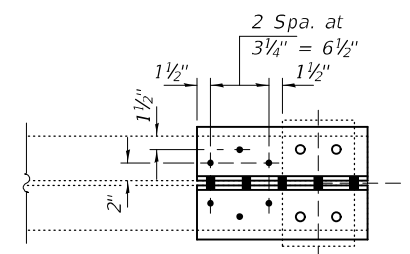
SECTION D-D



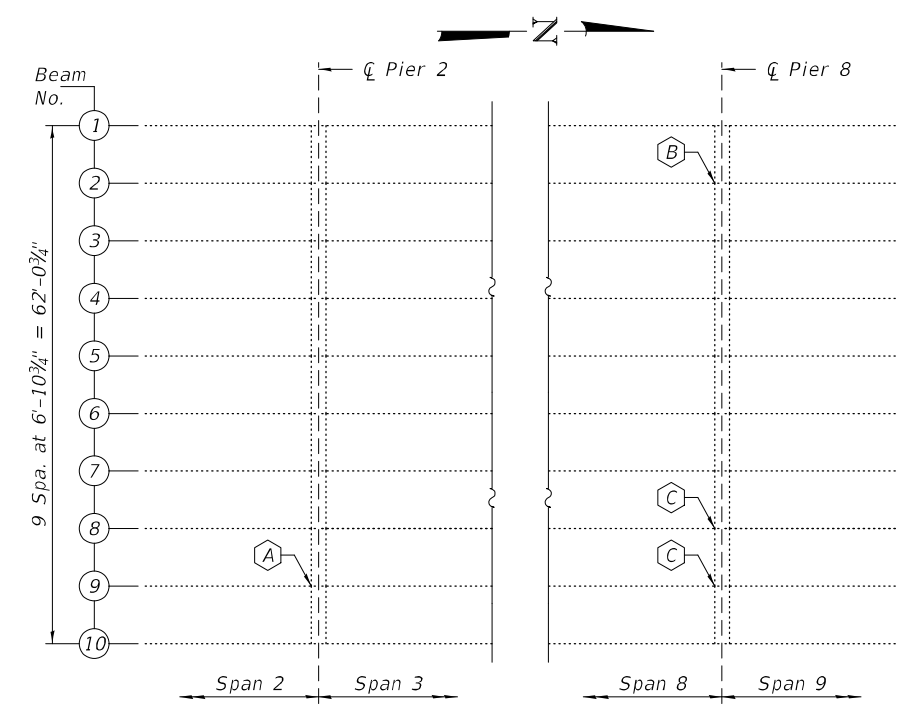
REPAIR A DETAILS
Pier 2, Beam 9, Span 2



SECTION E-E



SECTION F-F



LOCATION SKETCH

GENERAL NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Fasteners shall be ASTM F 3125 Grade 325 Type 1 mechanically galvanized bolts. Bolts 3/4"Ø, holes 1 3/16"Ø, unless otherwise noted.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

All new structural steel shall be hot dip galvanized. See Special Provisions for "Hot Dip Galvanizing for Structural Steel".

Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the special provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

LEGEND

- - Holes to be field drilled in existing steel using new steel as template.
- - Holes to be field drilled in new steel using existing holes as template.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Steel Repair	Pound	430



EXPIRES 11-30-2024
(Seal applies to this sheet only.)

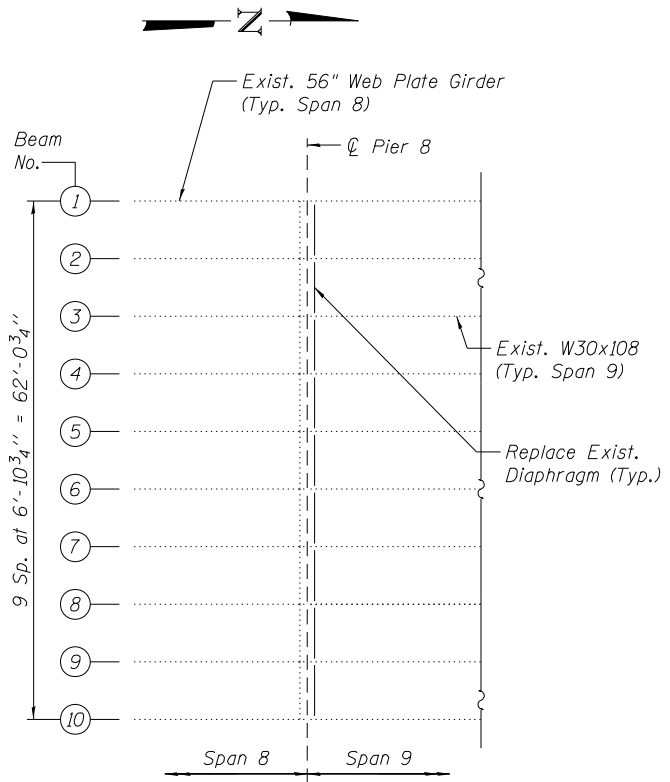
DESIGNED - Victor H. Veliz	EXAMINED	DATE - MAY 1, 2024
CHECKED - Adrian T. Holloway	ENGINEER OF STRUCTURAL SERVICES	
DRAWN - daburdell	PASSED	REVISED -
CHECKED - VHV ATH	ENGINEER OF BRIDGES AND STRUCTURES	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

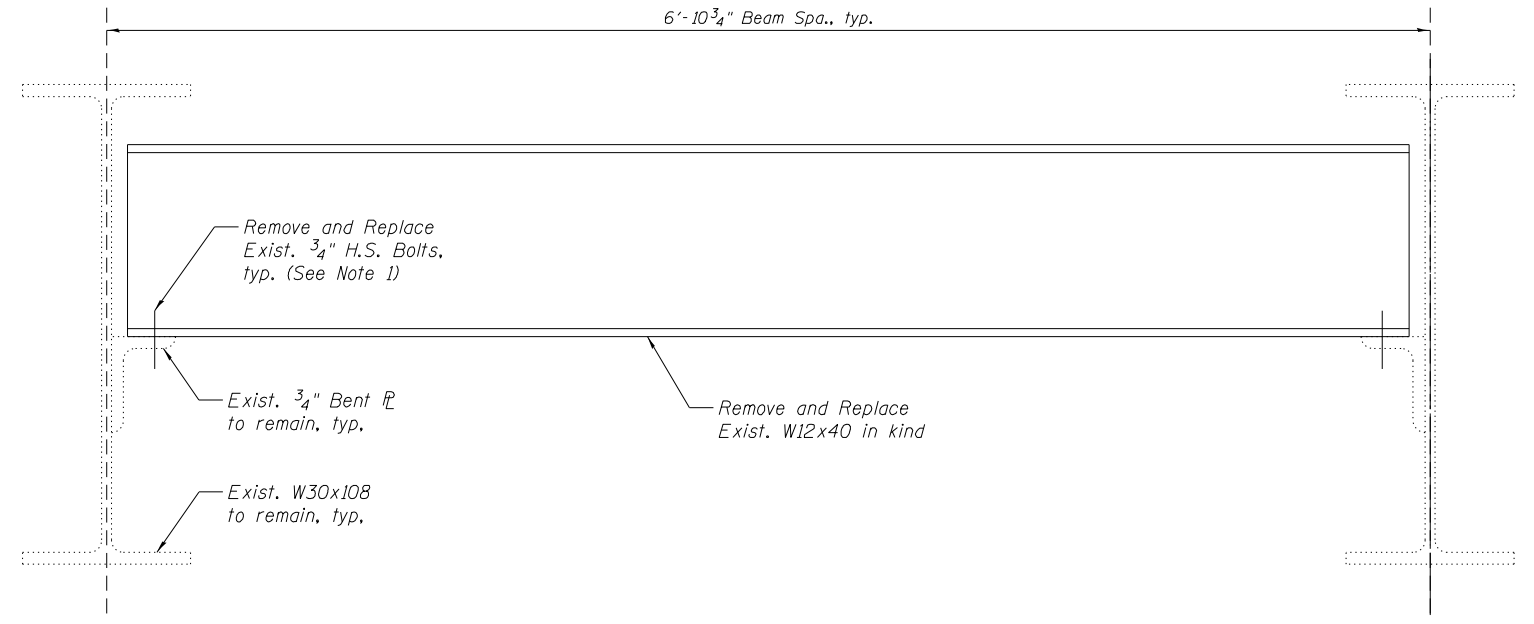
BEAM END REPAIRS
SN 016-0195

SHEET NO. SA-15 OF SA-23 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	52
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



LOCATION SKETCH



DIAPHRAGM REPLACEMENT

NOTES:

1. 15/16" holes in bottom flange of new diaphragm shall be field drilled to match holes in existing Bent PL.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Steel Removal	Pound	2,430
Furnishing and Erecting Structural Steel	Pound	2,430

MODEL: \\MODEL\NAME FILE: NAME.plt Date: 4/2/2024 10:00:00 AM User: ken.joy



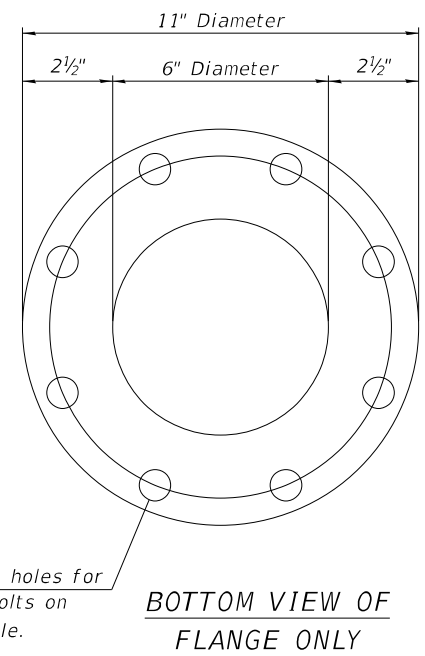
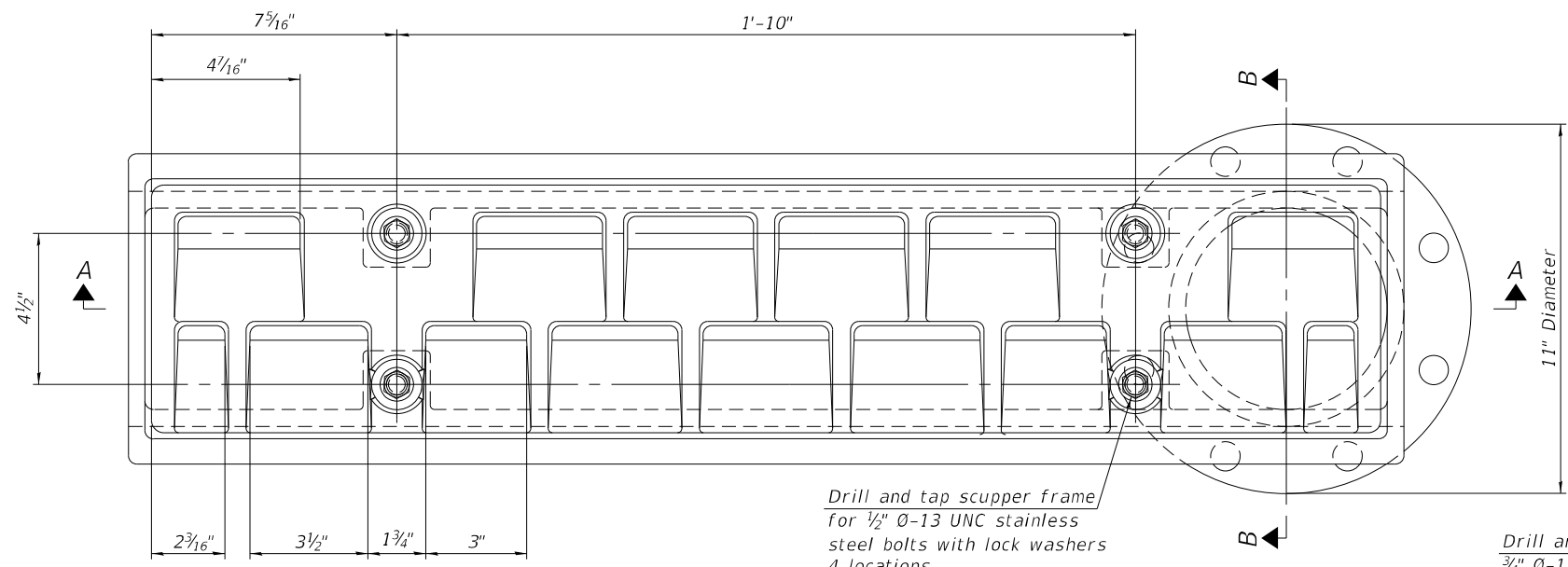
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STEEL DIAPHRAGM REPLACEMENT
STRUCTURE NO. 016-0195

SHEET NO. SA-16 OF SA-23 SHEETS

FAU- RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	53
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



Notes:

All cast iron parts shall be gray iron conforming to the requirements of AASHTO M105, Class 35B and AASHTO M306. Bolts, nuts and washers shall be according to ASTM A307 and shall be galvanized according to AASHTO M232. As an alternate stainless steel may be used.

Stainless steel hardware shall be according to Article 1006.29(d) of the Standard Specifications.

Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frames and downspouts; however, the scupper grates shall remain cast iron. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval.

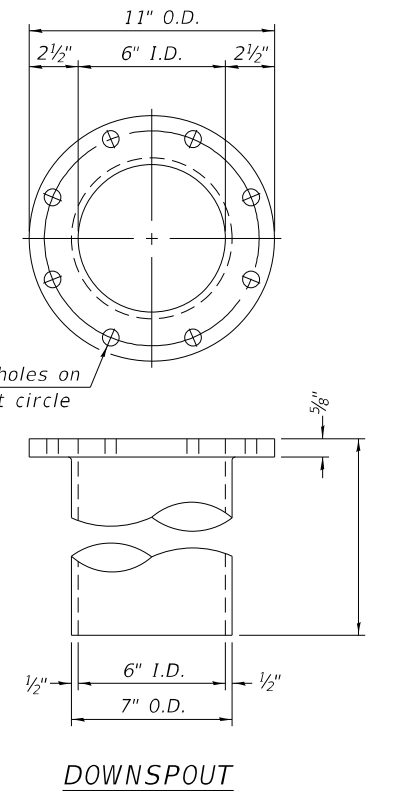
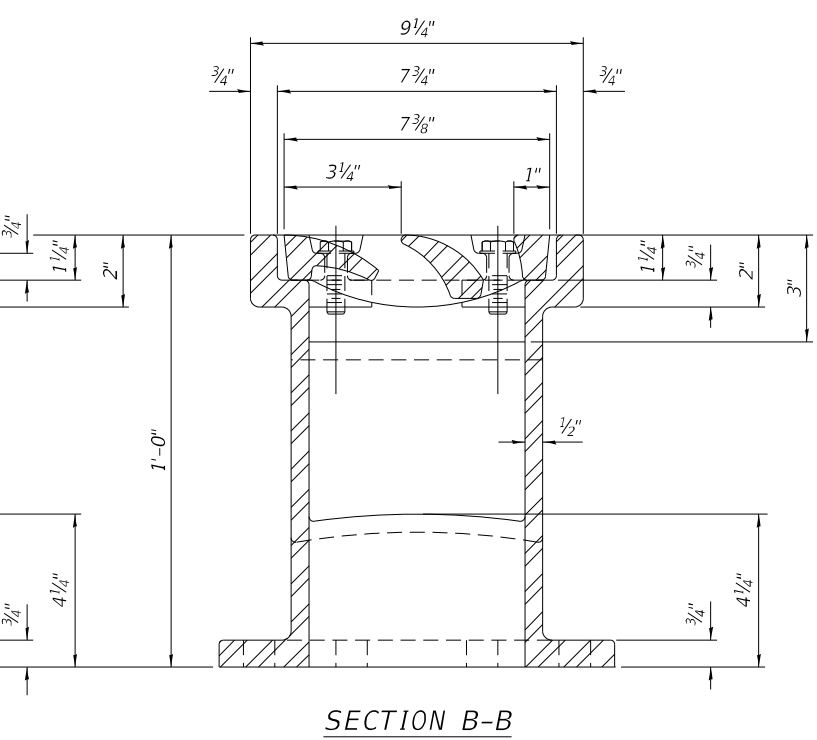
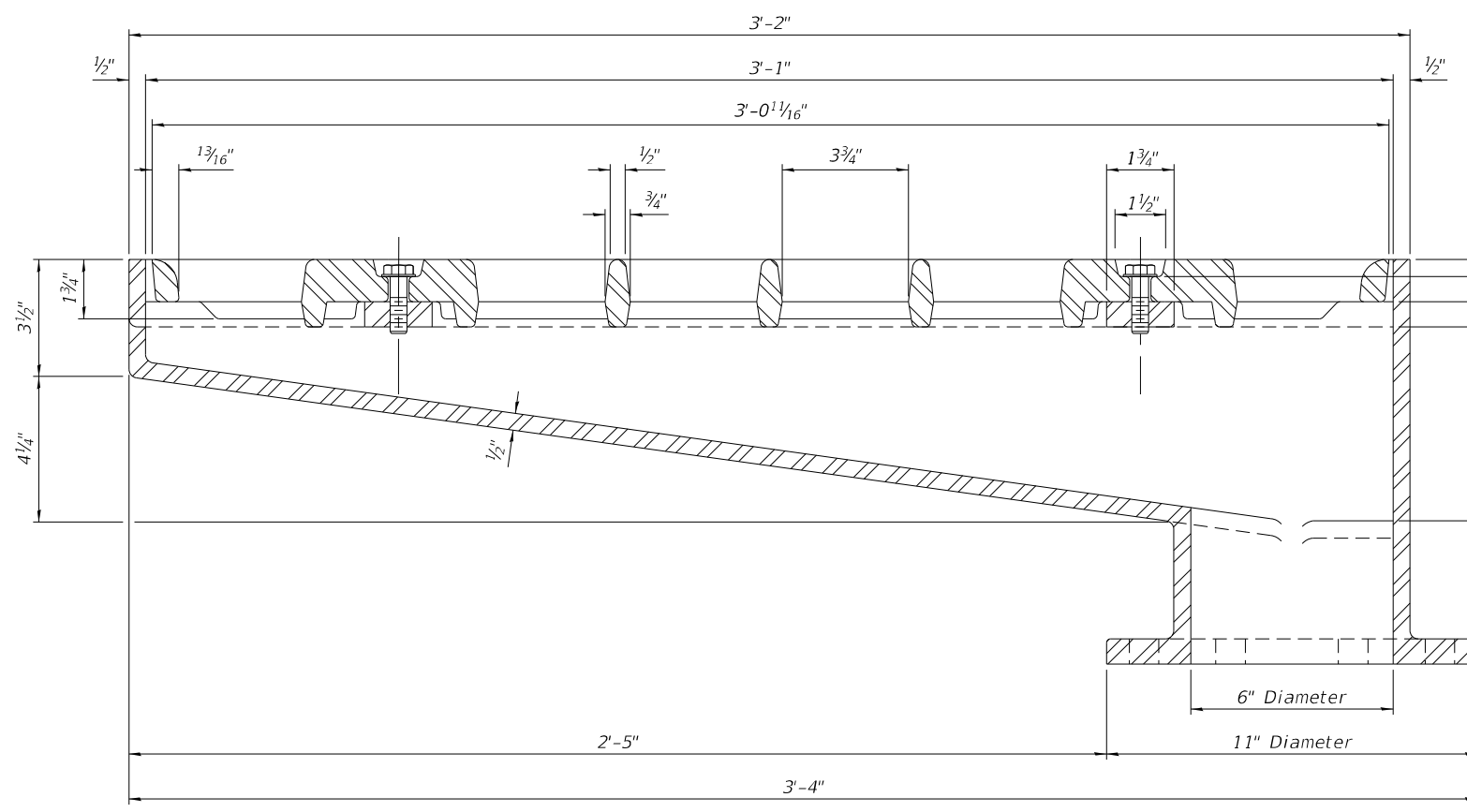
Structural steel scupper frames and downspouts, when utilized, shall be galvanized according to AASHTO M111.

As an alternate, fiberglass may be used for downspouts according to ASTM D2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. in lieu of the cast iron or structural steel.

Exterior surfaces of downspouts and exterior exposed surface of the scupper frame below deck shall be painted to match finish coat of the existing exterior beams.

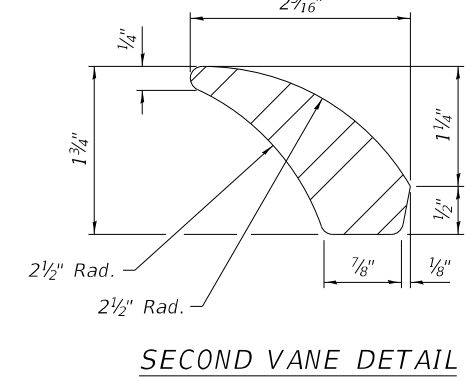
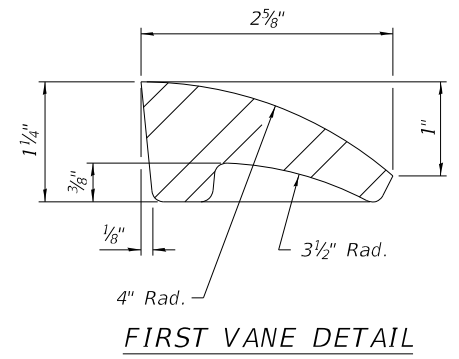
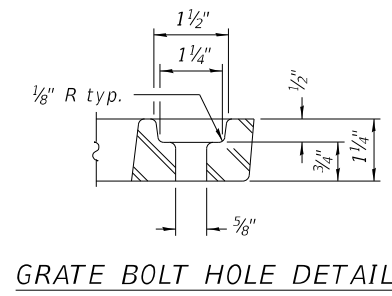
The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.

Cost of the grate, frame, downspout, nuts and washers including complete installation of the scupper shall be paid for at the contract unit price for Drainage Scupper, DS-33.



BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper, DS-33	Each	4



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 CHECKED - ATB
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 DRAWN - MCC
 CHECKED - ATB
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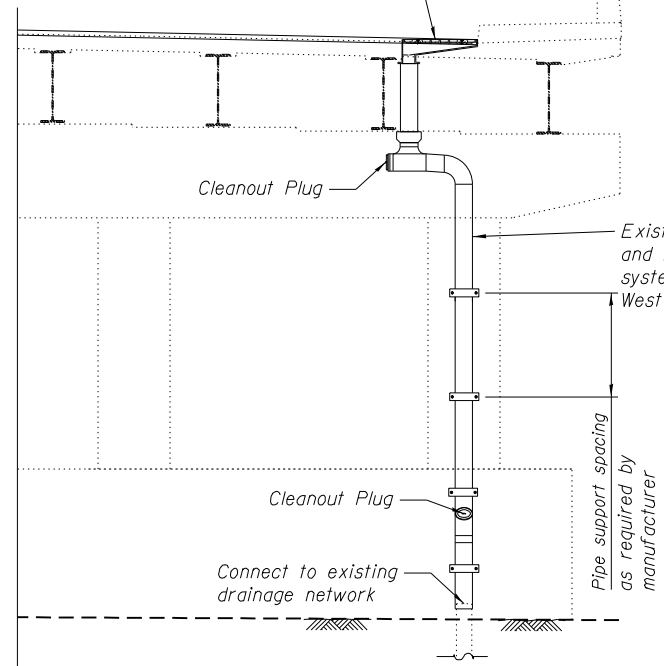
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DRAINAGE SCUPPER, DS-33
 STRUCTURE NO. 016-0195**

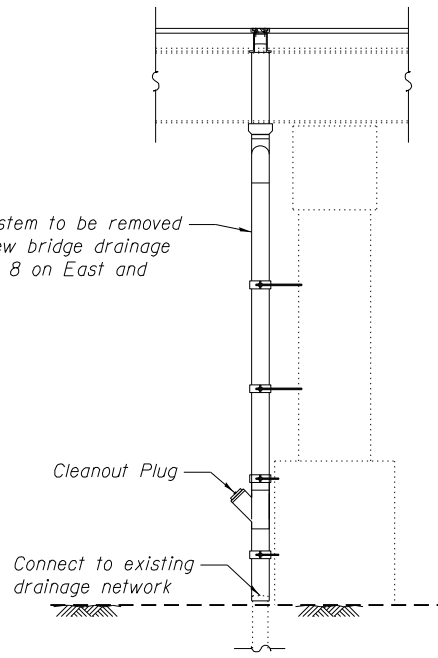
SHEET NO. SA-17 OF SA-23 SHEETS

FAU. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	54
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				

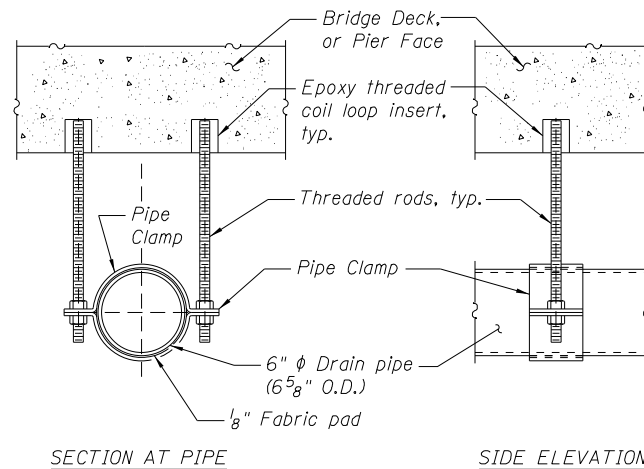
Existing drainage scupper to be removed and replaced with new DS-33 scupper at Piers 3 & 8 on East and West sides. See Sheet SA-17 for scupper details.



SECTION THRU DECK AT PIER
(at Pier 3 & Pier 8)



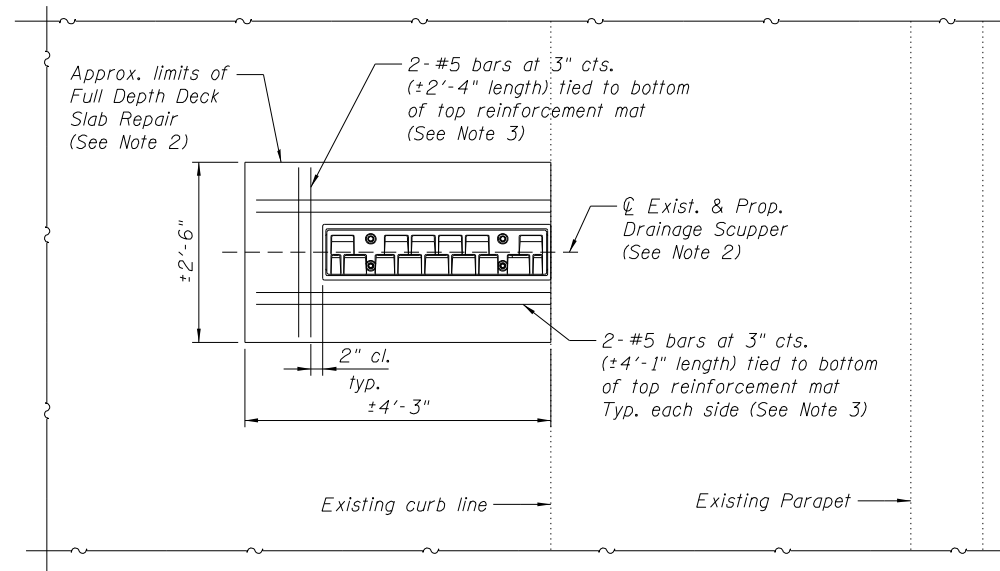
TYPICAL SIDE ELEVATION
(at Pier 3 & Pier 8)



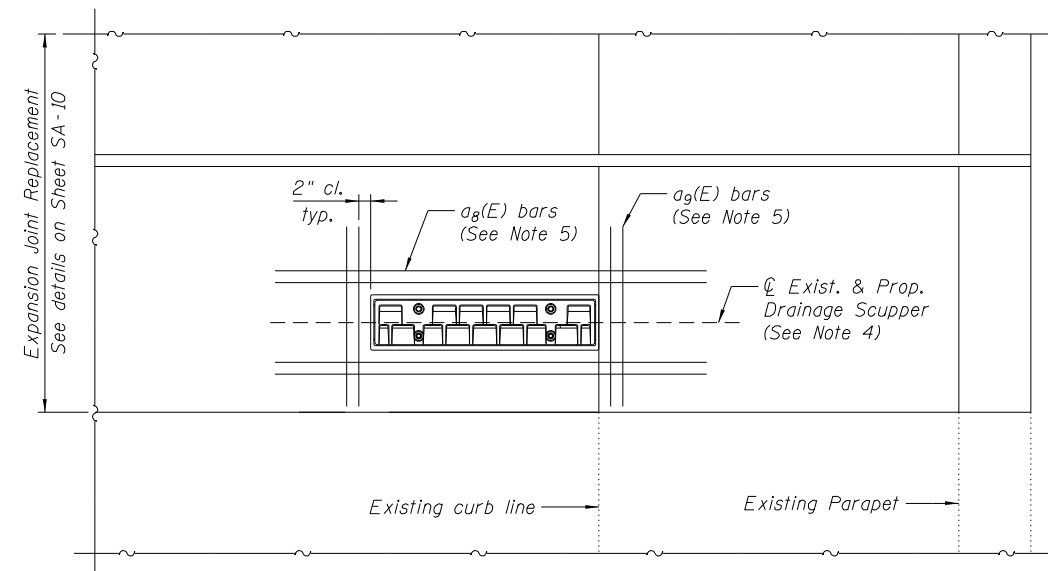
PIPE SUPPORT DETAIL

NOTES

1. Drainage scupper top elevation shall be recessed to accommodate diamond grinding. Place scupper 1/4" below the top of slab.
2. The removal of the existing scuppers near Pier 3 shall not be paid for separately but shall be included with Deck Slab Repair (Full Depth, Type II). See SA-5 for scupper location and deck removal limits.
3. The additional reinforcement bars shown at the Pier 3 scupper replacement shall not be paid for separately but shall be included in the cost of Deck Slab Repair (Full Depth, Type II).
4. The removal of the existing scuppers near Pier 8 shall not be paid for separately but shall be included in the concrete removal for the expansion joint. See SA-6 for concrete removal limits.
5. Reinforcement bars for drainage scuppers near Pier 8 included with expansion joint replacement. See Sheet SA-10 for bar list and quantity.
6. Remove and install new bridge drainage system at Pier 3 and Pier 8. Cost shall be included with Bridge Drainage System.



TOP PLAN AT DRAINAGE SCUPPER
NEAR PIER 3



TOP PLAN AT DRAINAGE SCUPPER
NEAR PIER 8

BILL OF MATERIAL

Item	Unit	Total
Bridge Drainage System	L. Sum	0.5

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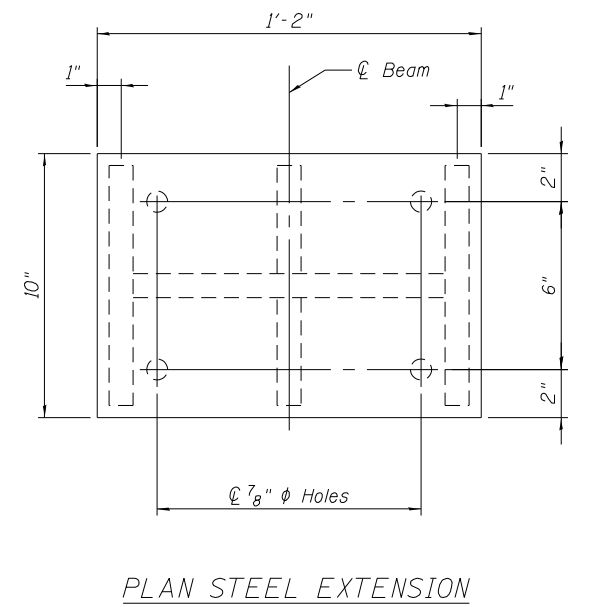
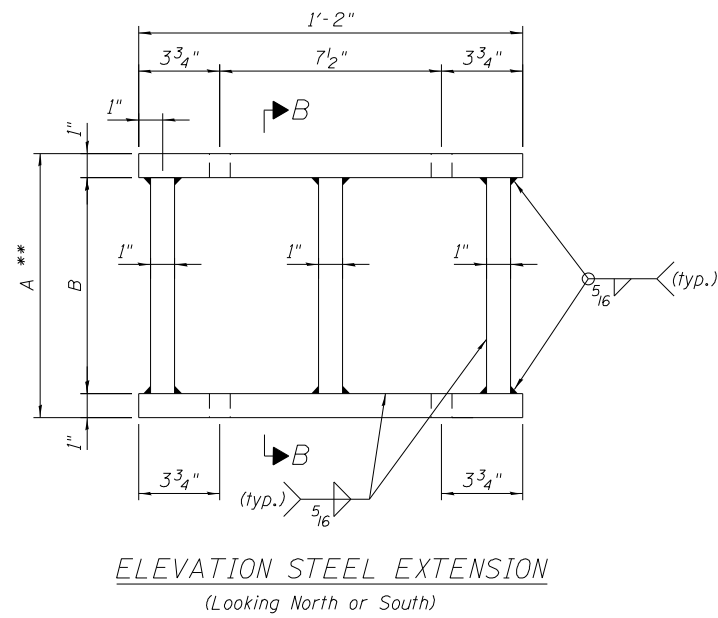
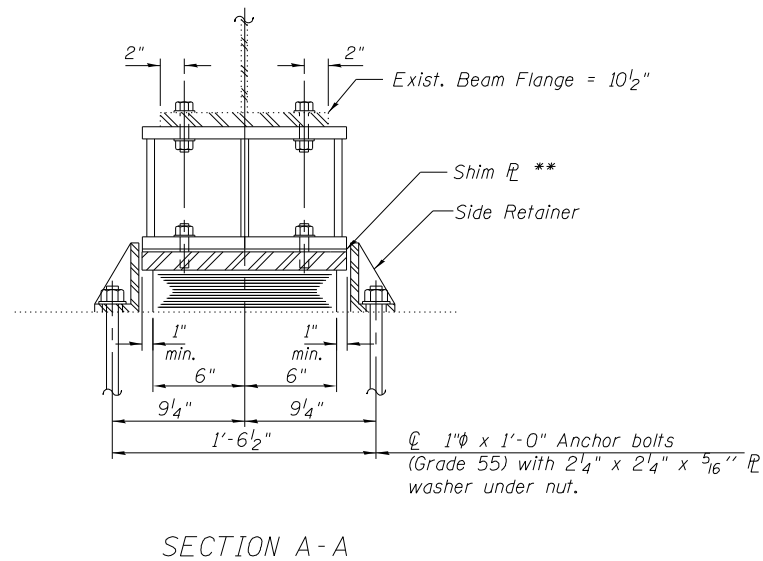
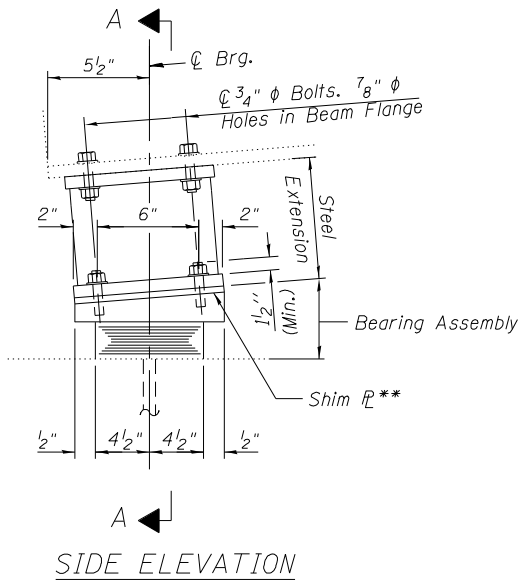
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE SYSTEM
STRUCTURE NO. 016-0195

SHEET NO. SA-18 OF SA-23 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	55
CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT



TYPE I ELASTOMERIC EXP. BRG.

North & South Abutments,
Pier 2 South, and Pier 2 North

BEARING DIMENSIONS

Location	Slope @ P.G.L.
South Abut.	5.21%
Pier 2 South	5.21%
Pier 2 North	5.21%
North Abut.	-5.21%

Notes:

Existing bearing height (from top of the bearing seat to bottom of girder flange) information obtained from available existing plans is used to compute proposed steel extension height. Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions.

Anchor bolts shall be ASTM F1554 all-thread of the grade(s) and diameter(s) specified.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

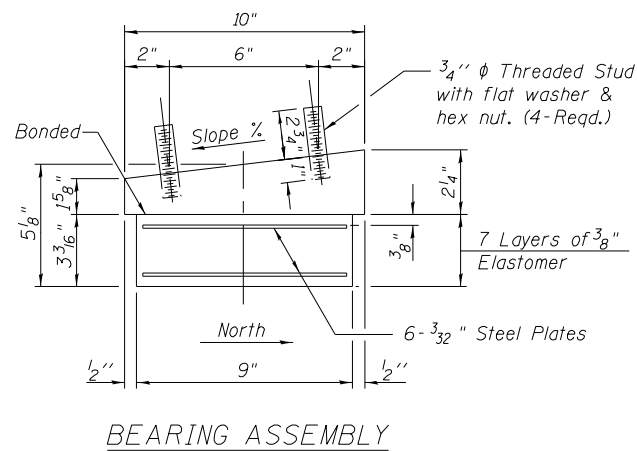
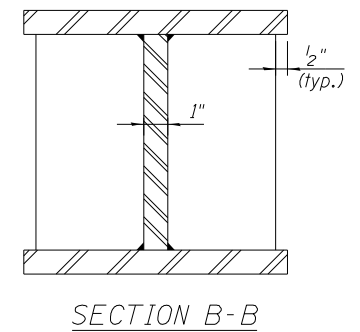
Cost of side retainers and stainless steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.

Two 1/8" in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details. Cost included with Elastomeric Bearing Assembly, Type I.

The structural steel plates of the Bearing Assembly shall conform to the requirements of AASHTO M 270 Grade 50.

New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.

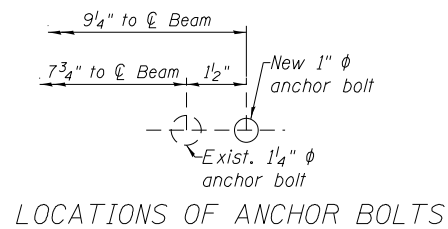
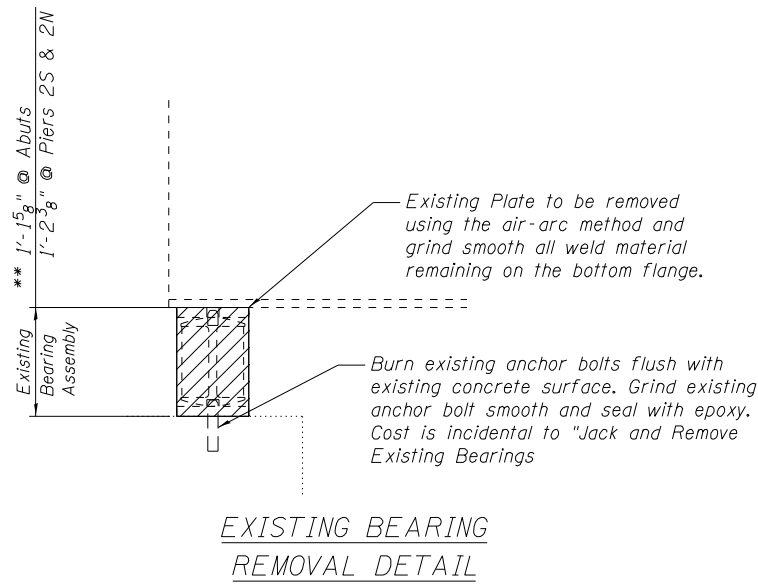
Anchor bolts and side retainers at all supports shall be installed as each member is erected unless and equivalent temporary means of lateral restraint is used.



Note:
Shim plates shall not be placed under Bearing Assembly.

STEEL EXTENSION DIMENSIONS

Location	A	B
South Abut.	8 1/2"	6 1/2"
Pier 2 South	9 1/4"	7 1/4"
Pier 2 North	9 3/4"	7 3/4"
North Abut.	8 1/2"	6 1/2"



REACTION TABLE (KIPS)

Type	S. Abut.	Pier 2S	Pier 2N	N. Abut.
R DL	30.7	30.6	31.1	27.0
R LL	40.7	38.1	38.6	39.5
R IMP	11.0	10.8	11.6	11.2
R Total	82.4	79.5	81.3	77.7

Minimum jack capacity required = 45 Tons

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	40
Anchor Bolts, 1"	Each	80
Furnishing and Erecting Structural Steel	Pound	6,840

(Sheet 1 of 2)

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPE I BEARING DETAILS
STRUCTURE NO. 016-0195**

SHEET NO. SA-19 OF SA-23 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	56

CONTRACT NO. 62W48

ILLINOIS FED. AID PROJECT

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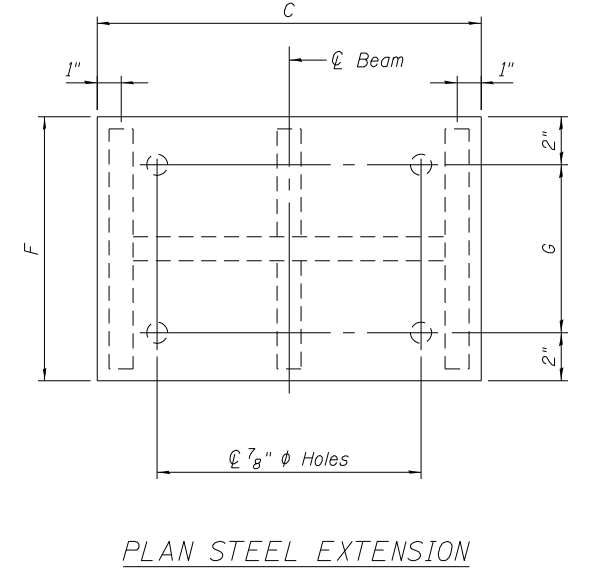
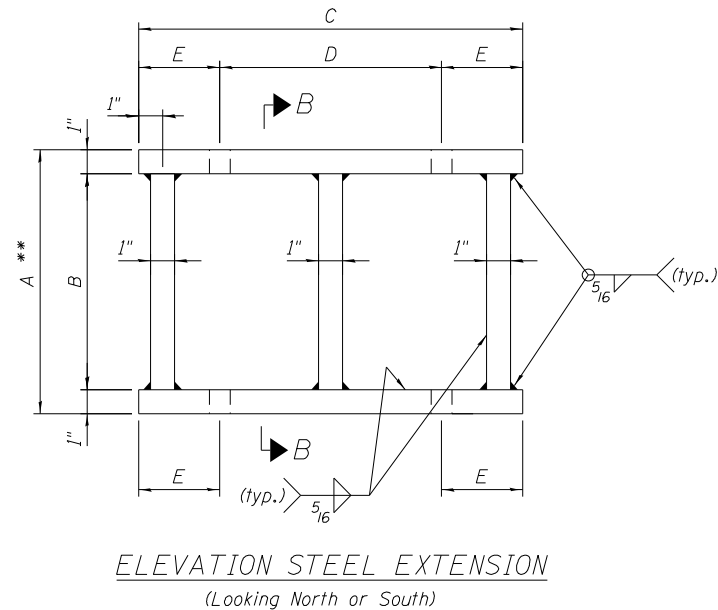
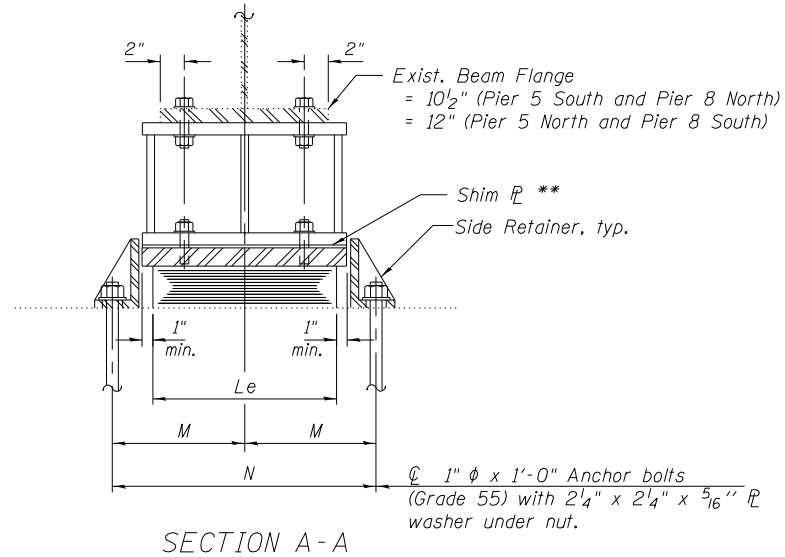
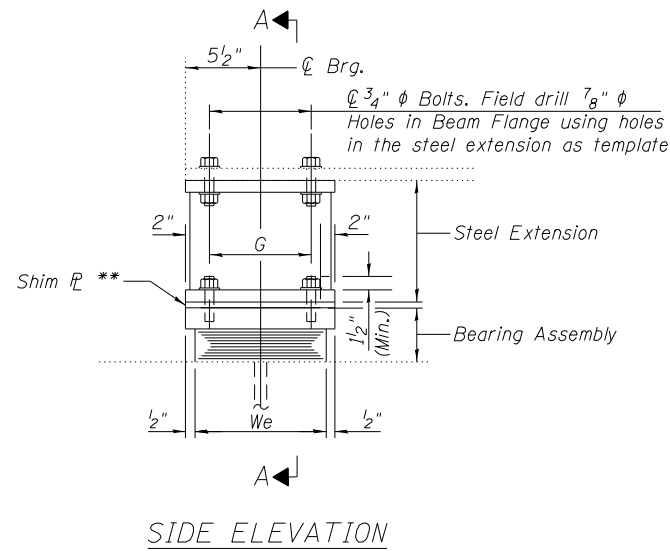


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** Existing Bearings @ Pier 2N have an 1/16" shim R for Beams 1-5 and 7/16" for Beams 6-10. An allowance of 1/2" has been included in Dimension N.



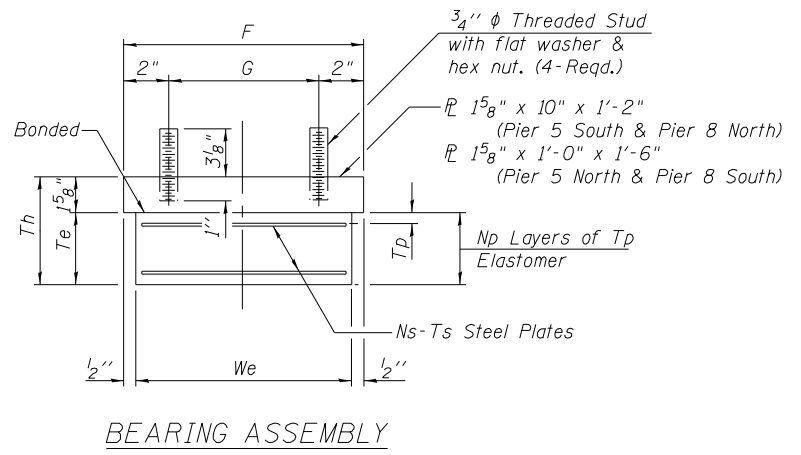
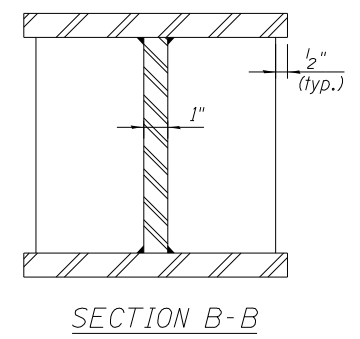
TYPE I ELASTOMERIC EXP. BRG.
 Pier 5 South, Pier 5 North
 Pier 8 South, and Pier 8 North

** Provide one additional 3/8" shim \mathbb{P}
 for beam line 6 at Pier 5 South.

BEARING DIMENSIONS

Location	Le	We	Np	Tp	Ns	Ts	Te	Th	Y	Z	M	N
Pier 5 South	12"	9"	7	3/8"	6	3/32"	3/16"	4 13/16"	4 5/8"	5 1/2"	9 1/4"	1'-6 1/2"
Pier 5 North	16"	11"	6	1/2"	5	3/8"	3/8"	5 1/4"	5"	6 1/4"	11 1/4"	1'-10 1/2"
Pier 8 South	16"	11"	6	1/2"	5	3/8"	3/8"	5 1/4"	5"	6 1/4"	11 1/4"	1'-10 1/2"
Pier 8 North	12"	9"	7	3/8"	6	3/32"	3/16"	4 13/16"	4 5/8"	5 1/2"	9 1/4"	1'-6 1/2"

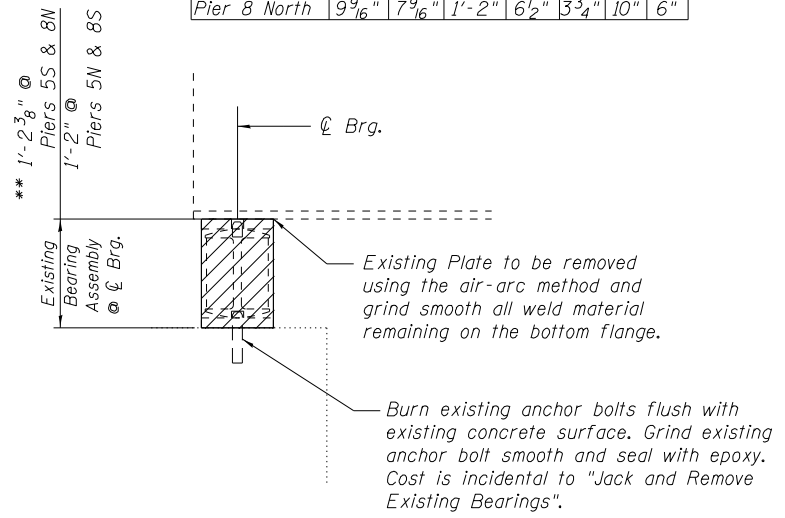
Notes:
 Existing bearing height (from top of the bearing seat to bottom of girder flange) information obtained from available existing plans is used to compute proposed steel extension height. Prior to ordering any material, the Contractor shall verify in the field all bearing heights and shim thickness dimensions.
 Anchor bolts shall be ASTM F1554 all-thread of the grade(s) and diameter(s) specified.
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
 Cost of side retainers and stainless steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.
 Two 1/8" in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details. Cost included with Elastomeric Bearing Assembly, Type I.
 The structural steel plates of the Bearing Assembly shall conform to the requirements of AASHTO M 270 Grade 50.
 Existing bearing at beam line 6 at Pier 5 South has an additional 3/8" shim \mathbb{P} . Cost of this shim plate is included with Furnishing and Erecting Structural Steel.
 New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
 Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.



Note:
 Shim plates shall not be placed under Bearing Assembly.

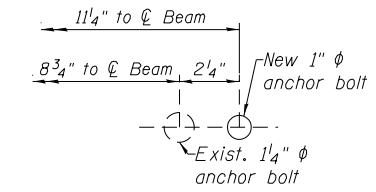
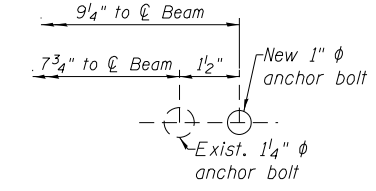
STEEL EXTENSION DIMENSIONS

Location	A	B	C	D	E	F	G
Pier 5 South	9 9/16"	7 9/16"	1'-2"	6 1/2"	3 3/4"	10"	6"
Pier 5 North	8 3/4"	6 3/4"	1'-6"	8"	5"	12"	8"
Pier 8 South	8 3/4"	6 3/4"	1'-6"	8"	5"	12"	8"
Pier 8 North	9 9/16"	7 9/16"	1'-2"	6 1/2"	3 3/4"	10"	6"



EXISTING BEARING REMOVAL DETAIL

Note:
 Prior to jacking, the Contractor shall verify that hold-down rods installed at North and South abutments during construction have been removed according to original details.



REACTION TABLE (KIPS)

Type	Pier 5S	Pier 5N	Pier 8S	Pier 8N
R DL	32.5	76.3	71.3	32.9
R LL	40.0	43.7	43.9	39.4
R IMP	10.8	9.6	9.7	11.2
R Total	83.3	129.6	124.9	83.5

Minimum jack capacity required
 = 45 Tons (Pier 5S & Pier 8N)
 = 80 Tons (Pier 5N & Pier 8S)

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	40
Anchor Bolts, 1"	Each	80
Furnishing and Erecting Structural Steel	Pound	8,320

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 AECOM, USA
 303 EAST WACKER DRIVE, SUITE 1400
 CHICAGO, IL 60601-5276
 PHONE: (312) 373-7700 FAX: (312) 373-6800



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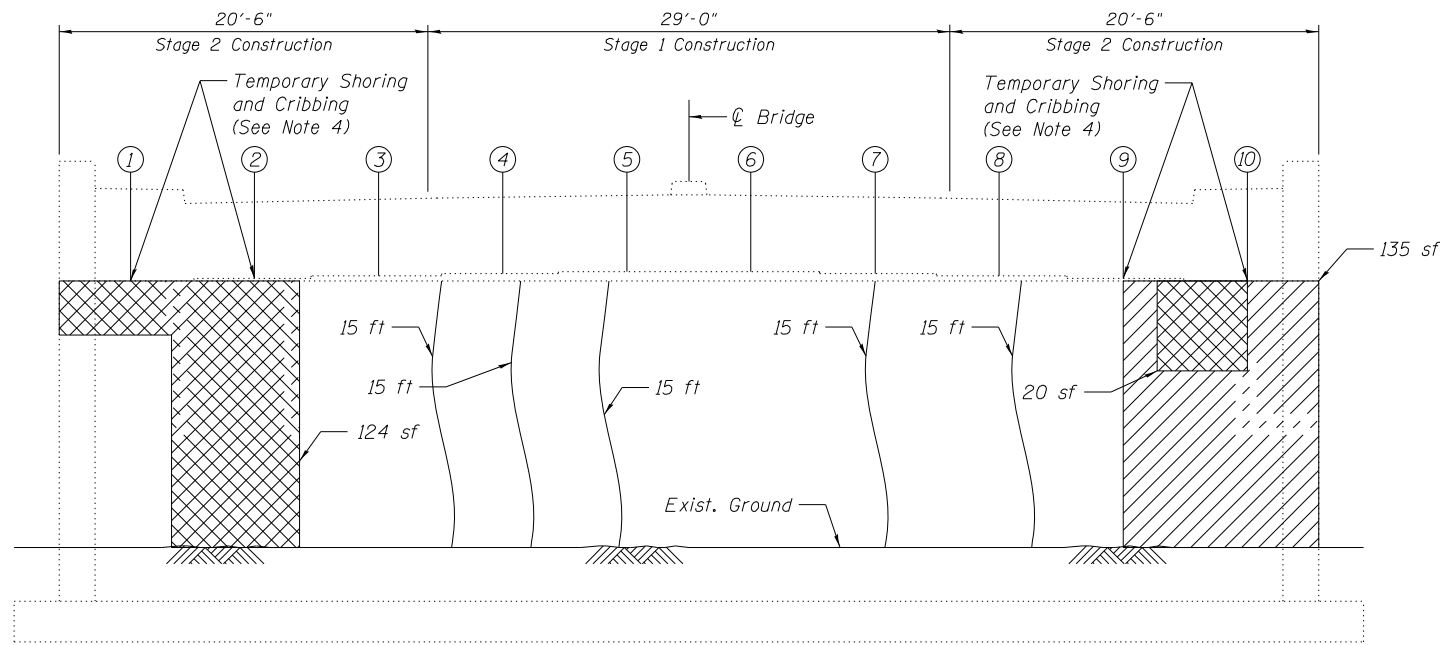
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 DEPARTMENT OF TRANSPORTATION**

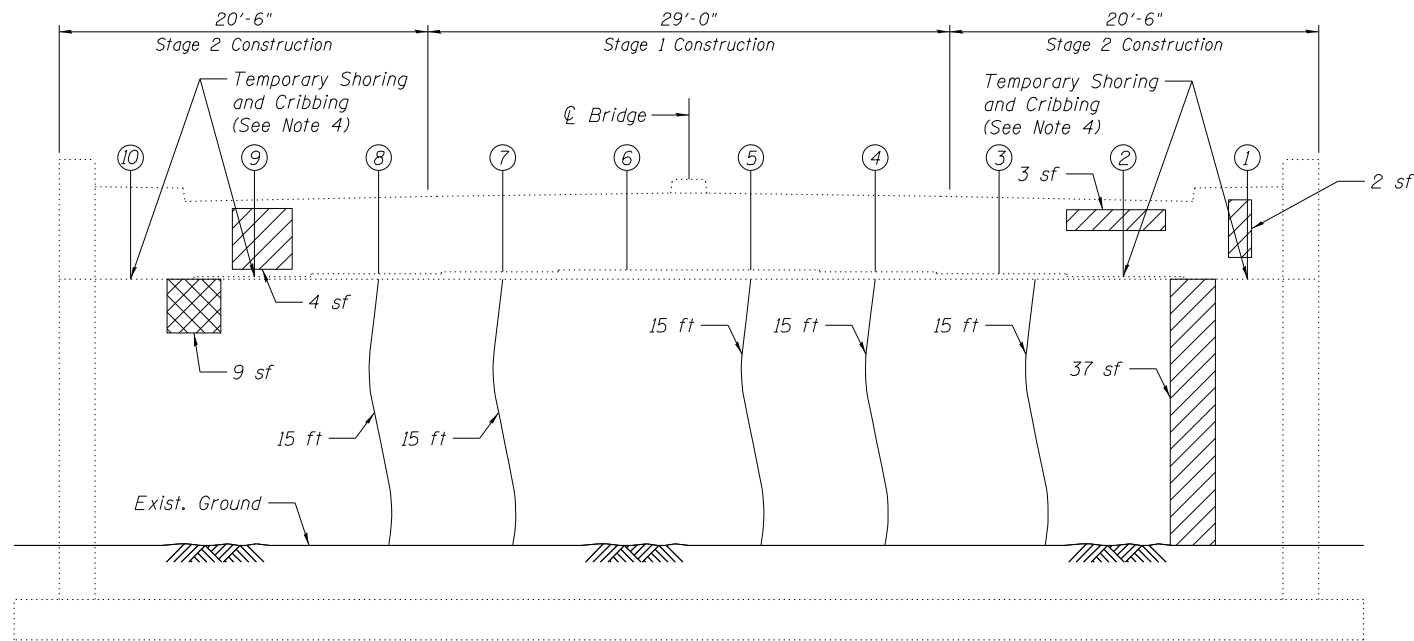
**TYPE I BEARING DETAILS
 STRUCTURE NO. 016-0195**

SHEET NO. SA-20 OF SA-23 SHEETS

F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
 3730 15-00131-01-BR COOK 109 57
 CONTRACT NO. 62W48
 ILLINOIS FED. AID PROJECT



NORTH ABUTMENT



SOUTH ABUTMENT

NOTES:

1. Contractor shall be responsible for removing and reattaching any utility that falls within any substructure repair.
2. Substructure repairs shall be performed when no live load is present over the area of repair work.
3. Concrete repair areas shown based on visual inspection. The actual sizes and locations of repairs shall be determined by the Engineer.
4. Girder lines 1, 2, 9, and 10 shall be temporarily supported off the North and South Abutments during the concrete repairs below the bearing seats. The temporary supports shall be paid for as "Temporary Shoring and Cribbing". The existing bearing removal at these locations shall not be paid for separately but shall be included in the unit cost of "Temporary Shoring and Cribbing".
5. For bearing reactions, see SA-19 and SA-20.

LEGEND

- Structural Repair Of Concrete (Depth equal to or less than 5 inches)
- Structural Repair Of Concrete (Depth greater than 5 inches)
- Epoxy Crack Injection

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	181
Structural Repair of Concrete (Depth Greater than 5 inches)	Sq. Ft.	153
Epoxy Crack Injection	Foot	150
Temporary Shoring and Cribbing	Each	8

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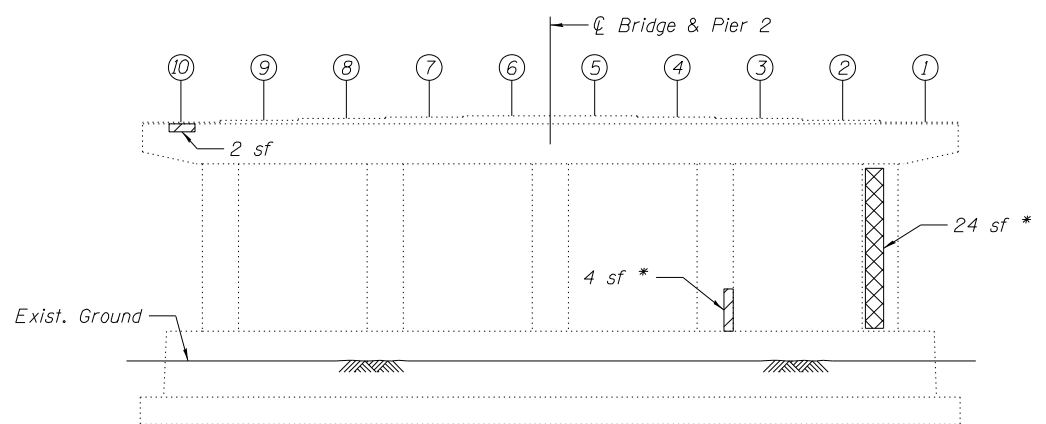
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

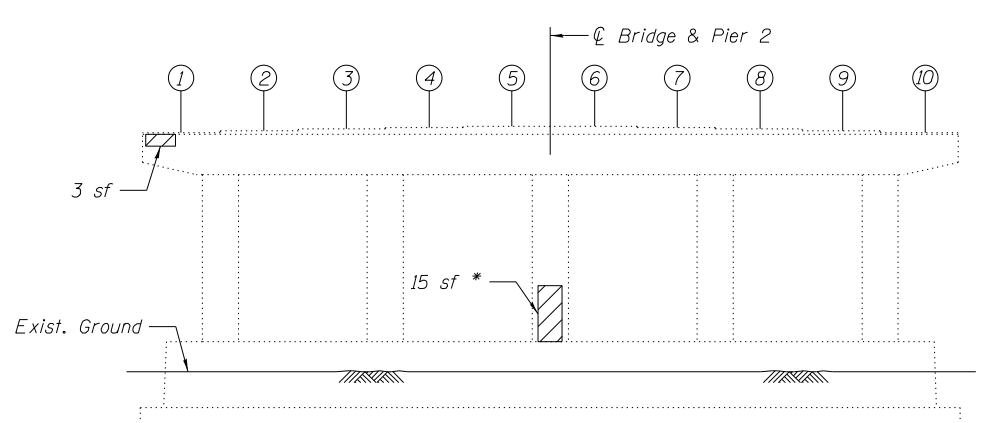
ABUTMENT REPAIRS
STRUCTURE NO. 016-0195

SHEET NO. SA-21 OF SA-23 SHEETS

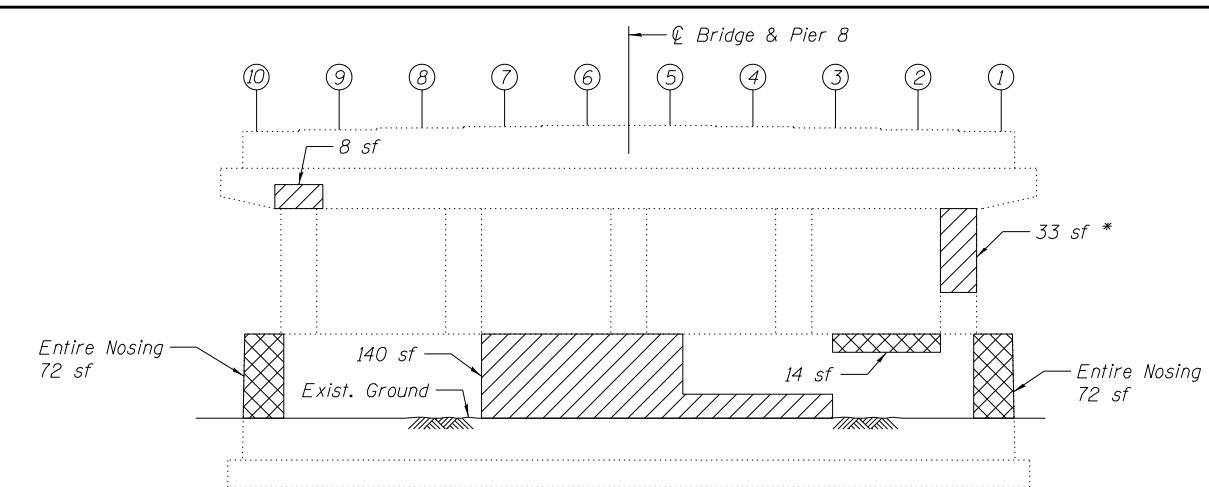
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3730	15-00131-01-BR	COOK	109	58
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



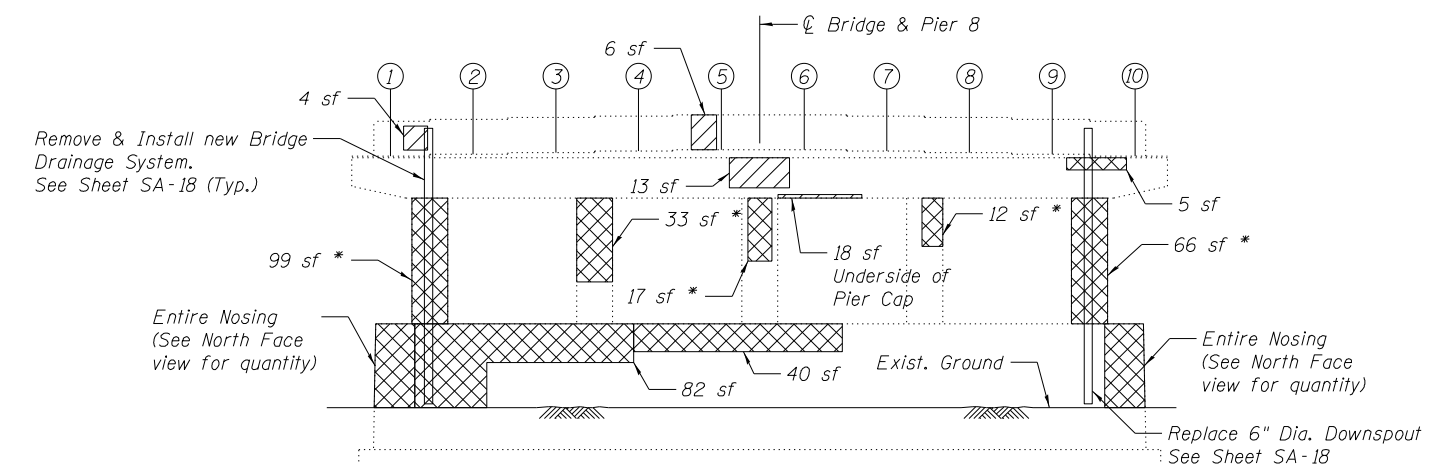
PIER 2
(North Face)



PIER 2
(South Face)



PIER 8
(North Face)



PIER 8
(South Face)

* Fiber Wrap shall be applied to the circumference of the pier column after structural repair of concrete. See Special Provisions.

NOTES:

1. Existing cribbing is installed near the existing bearings between the concrete pier cap and diaphragms near beams 1, 2, 3, 8 and 10 at Pier 8, Span 9 side. Contractor shall monitor Pier 8 existing bearings and existing cribbing pending bearing replacement.
2. Contractor shall be responsible for removing and reattaching any utility that falls within any substructure repair.
3. Substructure repairs shall be performed when no live load is present over the area of repair work.
4. Concrete repair areas shown based on visual inspection. The actual sizes and locations of repairs shall be determined by the Engineer.
5. All girder lines shall be temporarily supported off of Pier 8 during the concrete repairs to the pier cap and columns below. The temporary supports shall be paid for as "Temporary Shoring and Cribbing". The existing bearing removal at these location shall not be paid for separately but shall be included in the unit cost of "Temporary Shoring and Cribbing".
6. For bearing reactions, see SA-19 and SA-20.
7. Overhead concrete repairs shall be performed using Shotcrete. See Special Provisions.

LEGEND

- Structural Repair Of Concrete (Depth equal to or less than 5 inches)
- Structural Repair Of Concrete (Depth greater than 5 inches)

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	246
Structural Repair of Concrete (Depth Greater than 5 inches)	Sq. Ft.	536
Fiber Wrap	Sq. Ft.	561
Temporary Shoring and Cribbing	Each	20
Acrylic Coating	Sq. Yd.	63

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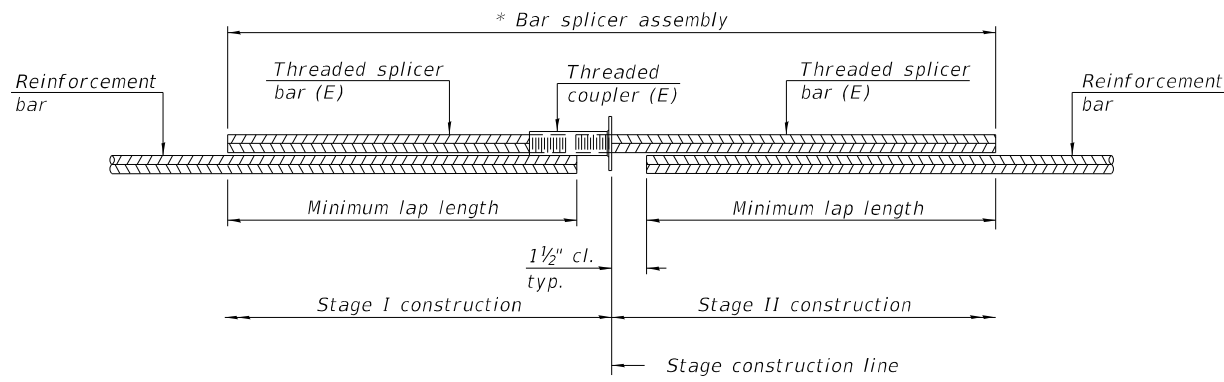
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER REPAIRS
STRUCTURE NO. 016-0195**

SHEET NO. SA-22 OF SA-23 SHEETS

F.A.U. RTE. 3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 59
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				

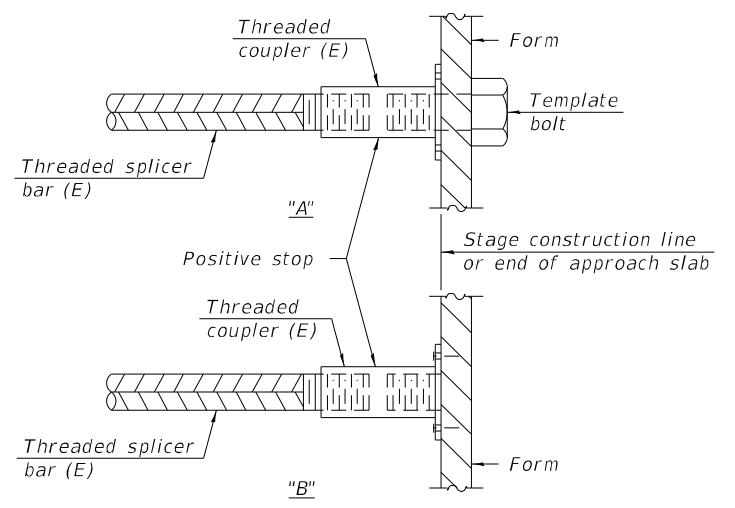


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

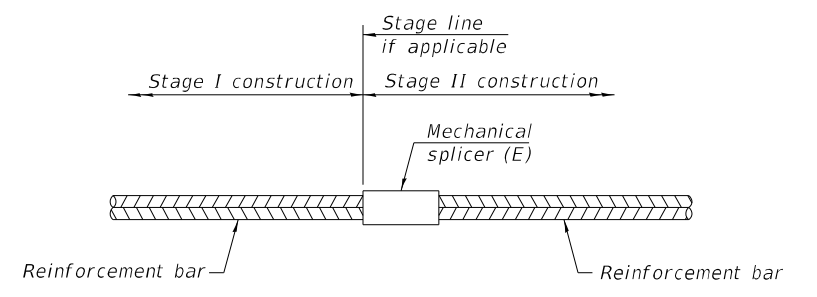
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
South Abutment	#6	8	3'-7"
South Abutment	#5	18	3'-6"
Pier 2	#5	40	3'-6"
Pier 5	#5	40	3'-6"
Pier 8	#5	48	3'-6"
North Abutment	#6	8	3'-7"
North Abutment	#5	18	3'-6"
Beam Encasement	#5	120	3'-0"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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BSD-1

1-1-2020



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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 016-0195

SHEET NO. SA-23 OF SA-23 SHEETS

FAU-RT. 3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 60
CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT

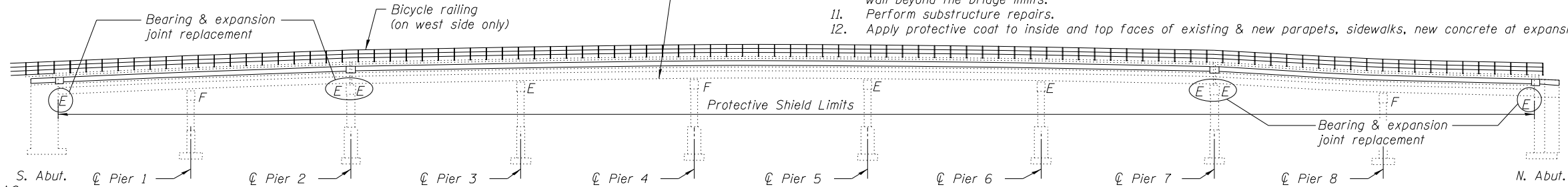
Existing Structure No. 016-0194, built in 1979 as FA 876 Section 161-VB, W & RS, is a nine span steel wide flange bridge with a reinforced concrete deck and closed south abutment and north abutment with retaining wall, 560'-11 1/2" bk. to bk. abutments, 68'-0" out to out deck. Traffic to be staged during construction.

Existing Steel Bridge Rail to be salvaged & delivered to a location to be designated by the District.

SCOPE OF WORK

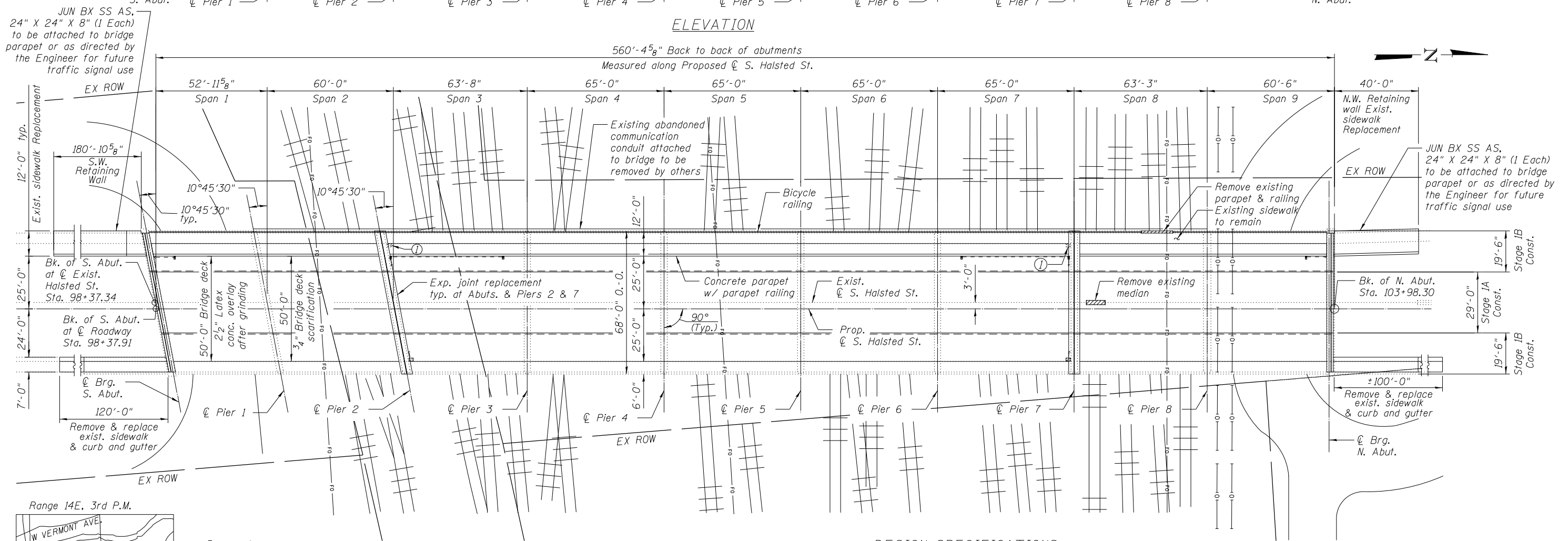
1. Remove median, west parapet and railing.
2. Jack and remove existing expansion bearings at abutments and at Piers 2 & 7 and replace with elastomeric bearings.
3. Remove and replace expansion joint devices at abutments and at Piers 2 & 7 and construct concrete beam end encasement at Piers 2 & 7.
4. Scarify existing bridge deck surface 3/4"
5. Perform concrete repairs to bridge deck and approach slabs.
6. Plug, adjust and replace drainage scuppers (in Spans 3 & 7) and construct new drainage system (at Piers 2 & 7).
7. Place 2 3/4" latex concrete overlay on bridge deck and perform 1/4" diamond grinding of new overlay.
8. Extend west sidewalk and construct concrete barrier at west side between roadway and Bike Path. Construct Bicycle Railing at the west edge.
9. Remove existing sidewalk, parapet and railing on the west side within the limits of west retaining walls on the west side and south west retaining wall beyond the bridge limits.
10. Construct sidewalk, parapet and parapet railing on the west side within the limits of west retaining walls on the west side and south west retaining wall beyond the bridge limits.
11. Perform substructure repairs.
12. Apply protective coat to inside and top faces of existing & new parapets, sidewalks, new concrete at expansion joints and latex overlay.

Conduit Attached to Structure, 2" Dia., PVC Coated Galvanized Steel to be clamped parallel to the west side of Beam 1 flange and connected to new Junction Box attached to the west side of structure at each end of the bridge for future traffic signal use (See Plan below and Conduit Beam Clamped Installation Detail on Sheet No. SB-2)



ELEVATION

560'-4 5/8" Back to back of abutments
Measured along Proposed \O S. Halsted St.



PLAN

DESIGN SPECIFICATIONS

(NEW CONSTRUCTION)
2002 AASHTO Standard Specifications
for Highway Bridges

DESIGN STRESSES

FIELD UNITS (NEW CONSTRUCTION)

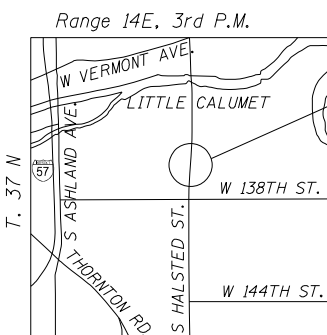
f'_c = 3,500 psi
 f'_c = 4,000 psi (Superstructure & Appr. Slab)
 f_y = 60,000 psi (Reinforcement)
 f_y = 50,000 psi (Structural Steel)

LEGEND

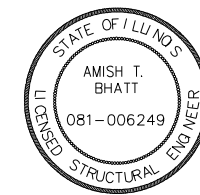
- ① Cut downspout to the bottom of deck & fill existing scupper with concrete
- fo— Exist. Underground Fiber Optic
- o— Exist. Underground Oil Pipeline

NOTE:

For Drainage System Details See sheet SB-22.



LOCATION SKETCH



I hereby certify that these plans were prepared by me or under my direct personal supervision and that I am a duly licensed structural engineer under the laws of the State of Illinois.

Amish T. Bhatt DATE: 2-29-2024

AMISH T. BHATT S.E. License #081-006249
LICENSE EXP. 11-30-2024

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 016-0194

SHEET NO. 5B-1 OF 5B-30 SHEETS

FAU. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	61
CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT

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PLOT SCALE	= N.T.S.	DRAWN	- KO	REVISED	-
PLOT DATE	= 4/2/2024	CHECKED	- JRT	REVISED	-

303 EAST WACKER DRIVE, SUITE 1400
CHICAGO, IL 60601-5276
PHONE: (312) 373-7700 FAX: (312) 373-6800

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement bars designated (E) shall be epoxy coated.

Existing reinforcement bars extending into the new construction shall be cleaned and straightened prior to incorporating into new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved embedded reinforcement or Mechanical Bar Splicer System. Cost included with Concrete Removal.

Cut existing vertical reinforcement flush with top of west sidewalk at the parapet and top of the deck at the median during removal of west parapet and median and coat the bar ends with epoxy. Level the existing concrete surface smooth with an approved cement grout after removal of the west parapet. The cost of this work is included with Concrete Removal.

All structural steel shall conform to AASHTO Classification M-270 Grade 50, unless otherwise noted.

Fasteners shall be ASTM F 3125 Grade A325 Type 1, mechanically galvanized bolts. Bolts $\frac{3}{4}$ " ϕ , open holes $\frac{13}{16}$ " ϕ , unless otherwise noted.

Prior to pouring the new concrete deck or overlay, all heavy or loose rust, loose mill scale and other loose and potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included with Concrete Removal.

Bridge Deck Grooving shall be completed only after Diamond Grinding (Bridge Section) is completed.

Existing raised reflective pavement markers on the bridge deck and approach slab shall be removed and replaced. See Proposed Pavement Marking Plans for location of proposed markers on the bridge structure.

Cost of removal and re-installation of all structural steel members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Furnishing and Erecting Structural Steel.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

All new structural steel and bearing assemblies shall be hot-dip galvanized. See Special Provisions for Hot Dip Galvanizing for Structural Steel.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.

No free fall deck drains will be permitted in the spans over the tracks.

Substructure repairs and bearing replacement shall be performed under staged construction when no live load is present.

The Contractor is advised that the existing structure may contain members that are in deteriorated condition with reduced load carrying capacity. It is the Contractor's responsibility to account for the condition of the existig structure when developing construction procedures. An existing structure information package will be provided by the Department to the Contractor upon request.

INDEX OF SHEETS

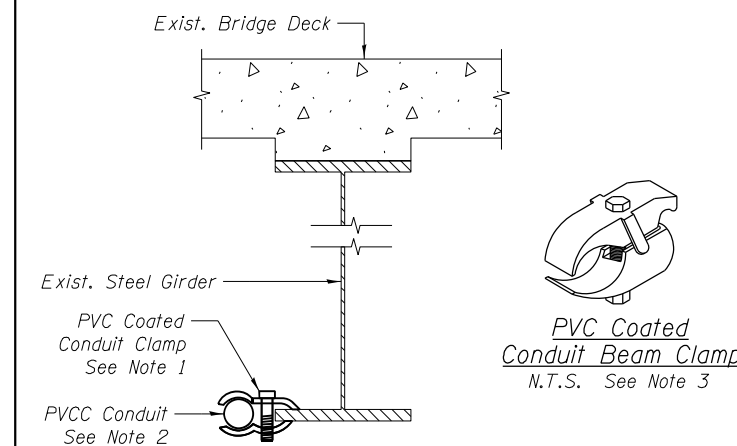
SB-1	General Plan & Elevation
SB-2	Index of Sheets, General Notes & Bill of Materials
SB-3	Stage Construction Details 1
SB-4	Stage Construction Details 2
SB-5	Temporary Concrete Barrier
SB-6	Superstructure Plan
SB-7	Superstructure Sections
SB-8	Superstructure Details 1
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SB-11	Bicycle Railing & Parapet Railing
SB-12	Existing Expansion Joint Removal
SB-13	Expansion Joint Replacement at South Abutment
SB-14	Expansion Joint Replacement at Pier 2
SB-15	Expansion Joint Replacement at Pier 7
SB-16	Expansion Joint Replacement at North Abutment
SB-17	Beam Encasement Cross Section Piers 2 & 7
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SB-19	Preformed Joint Strip Seal
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SB-22	Drainage System
SB-23	Type I Bearing Details
SB-24	Type I Bearing Details
SB-25	Abutment Repairs
SB-26	Piers 1 & 2 Repairs
SB-27	Piers 3 & 4 Repairs
SB-28	Piers 5 & 6 Repairs
SB-29	Piers 7 & 8 Repairs
SB-30	Bar Splicer Assembly and Mechanical Splicer Details

TOTAL BILL OF MATERIAL

PAY ITEMS	UNIT	BRIDGE REPAIR		BIKE PATH		TOTALS
		SUPER	SUB	SUPER	SUB	
Subbase Granular Material, Type B 6"	Sq. Yd.	39				39
Portland Cement Concrete Sidewalk 5 Inch	Sq. Ft.	352				352
Sidewalk Removal	Sq. Ft.			973		973
Concrete Removal	Cu. Yd.	41.7		91.7		133.4
Protective Shield	Sq. Yd.	4,004				4,004
Concrete Superstructure	Cu. Yd.	106.1		238.2		344.3
Protective Coat	Sq. Yd.	3,738		1,588		5,326
Furnishing and Erecting Structural Steel	Pound	11,490				11,490
Reinforcement Bars, Epoxy Coated	Pound	11,180		25,790		36,970
Bar Splicers	Each	204				204
Bicycle Railing	Foot			789		789
Parapet Railing	Foot			1,379		1,379
Preformed Joint Strip Seal	Foot	270				270
Elastomeric Bearing Assembly, Type I	Each	60				60
Anchor Bolts, 1"	Each	120				120
Epoxy Crack Sealing	Foot		208			208
Fiber Wrap	Sq. Ft.		970			970
Steel Railing Removal	Foot			789		789
Bridge Drainage System	L Sum	0.5				0.5
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	2,937				2,937
Approach Slab Repair (Partial Depth)	Sq. Yd.	37				37
Jack and Remove Existing Bearings	Each	22				22
Combination Curb and Gutter Removal and Replacement Greater Than 10 Feet	Foot	220				220
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	Sq. Yd.	3,014				3,014
Bridge Deck Scarification 3/4"	Sq. Yd.	3,376				3,376
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.		709			709
Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq. Ft.		1,130			1,130
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	22		5		27
Drainage Scuppers, DS-11	Each					4
Drainage Scuppers to Be Adjusted	Each			4		2
Diamond Grinding (Bridge Section)	Sa. Yd.	2,865		2		2,865
Temporary Shoring and Cribbing	Each	38				38
Conduit Attached to Structure, 2" Dia., PVC Coated Galvanized Steel	Foot	600				600
Junction Box, Stainless Steel, Attached to Structure, 24" X 24" X 8"	Each	2				2
Acrylic Coating	Sq. Yd.		108			108

* See Temporary Shring and Cribbing notes on Sheet SB-24 thru. SB-26

Conduit Beam Clamp Installation Detail



NOTES:

- All mounting hardware for the PVCC RGC conduit must be PVC coated.
- Secure the conduit with PVC coated conduit clamps or conduit beam clamps as shown at 5'-0" intervals for laterals and within 2'-0" maximum from any junction box, flexible conduit, or change in direction. All PVC coated conduit clamps or beam clamps will be included with the cost of the "conduit attached to structure" pay item.
- All conduit attached to structure shall be PVC coated rigid steel conduit (PVCC RGC) typical.

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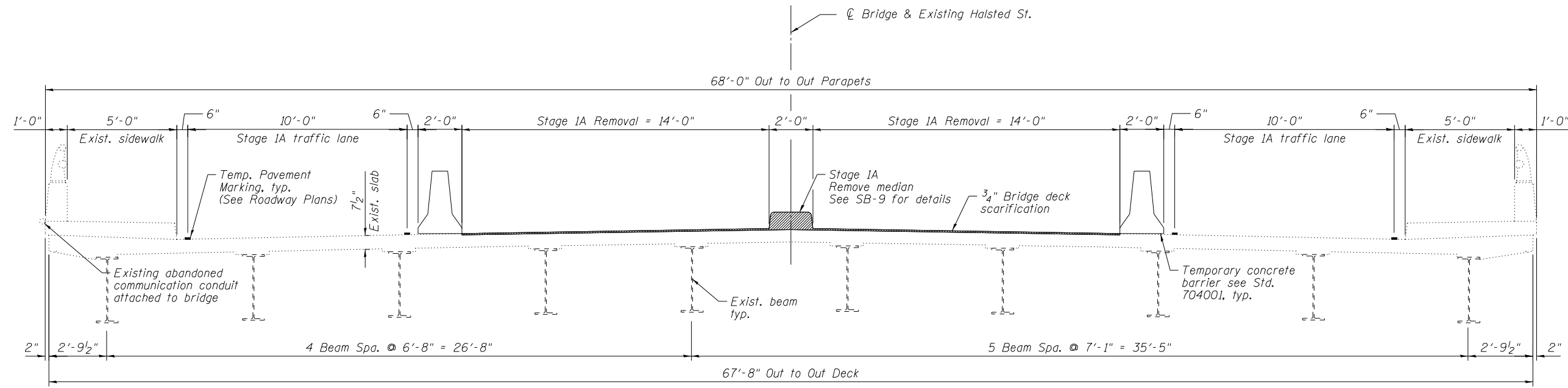
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, GENERAL NOTES & BILL OF MATERIAL
STRUCTURE NO. 016-0194**

SHEET NO. 5B-2 OF 5B-30 SHEETS

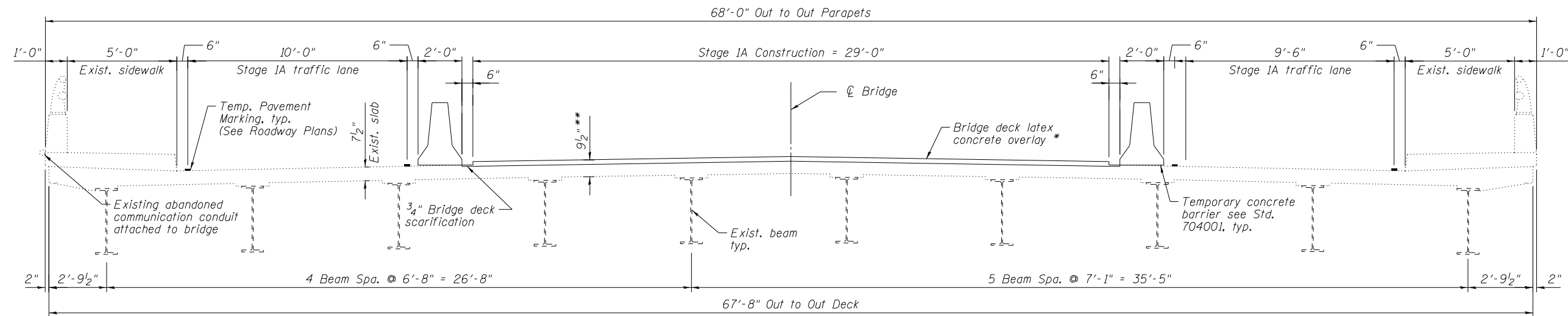
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3730	15-00131-01-BR	COOK	109	62
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



STAGE 1A REMOVAL AND TRAFFIC
(Looking North)

LEGEND

 Indicates Removal



STAGE 1A CONSTRUCTION AND TRAFFIC
(Looking North)

* Place 2 3/4" latex concrete overlay on bridge deck. Final overlay thickness shall be 2 1/2" after 1/4" Diamond Grinding. (See Special Provisions.)

** Prior to grinding

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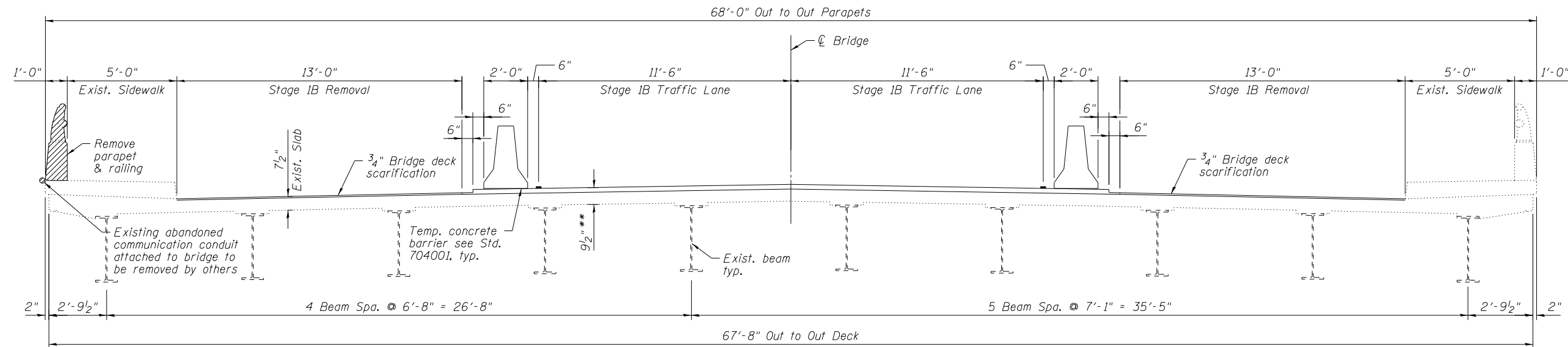
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STAGE CONSTRUCTION DETAILS 1
STRUCTURE NO. 016-0194

SHEET NO. 5B-3 OF 5B-30 SHEETS

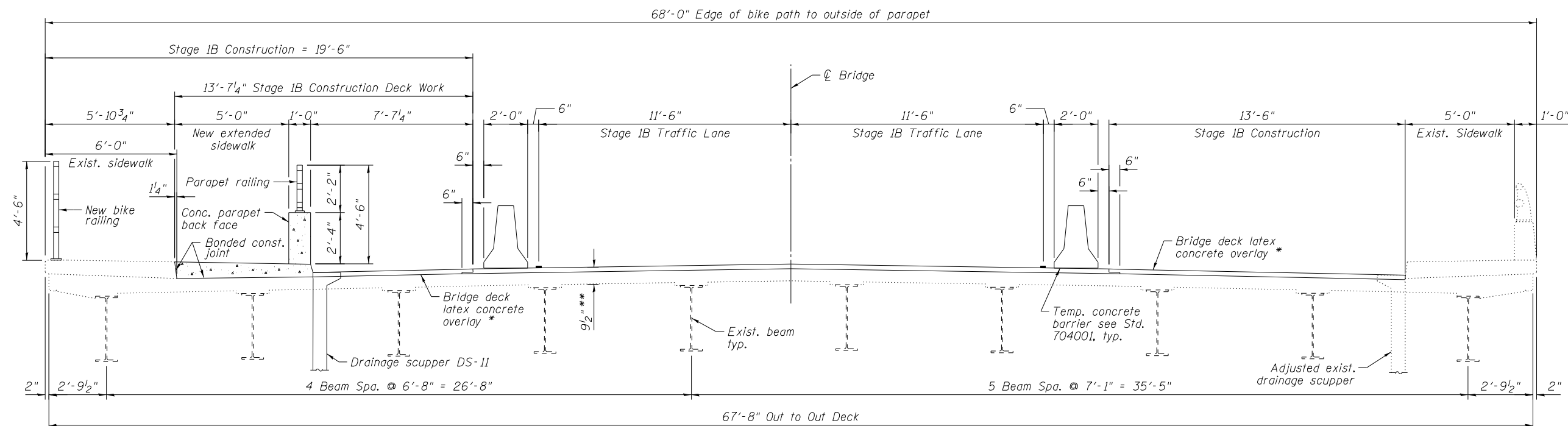
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3730	15-00131-01-BR	COOK	109	63
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



STAGE 1B REMOVAL AND TRAFFIC
(Looking North)

LEGEND

Indicates removal



STAGE 1B CONSTRUCTION AND TRAFFIC
(Looking North)

* Place 2 3/4" latex concrete overlay on bridge deck. Final overlay thickness shall be 2 1/2" after 1/4" Diamond Grinding. (See Special Provisions.)

** Prior to grinding

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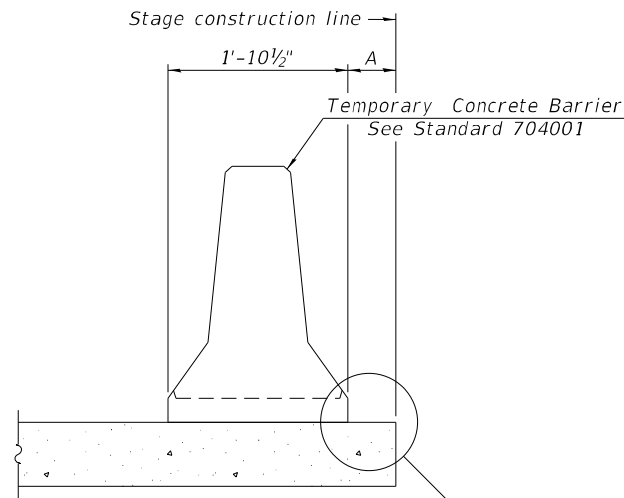
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 DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS 2
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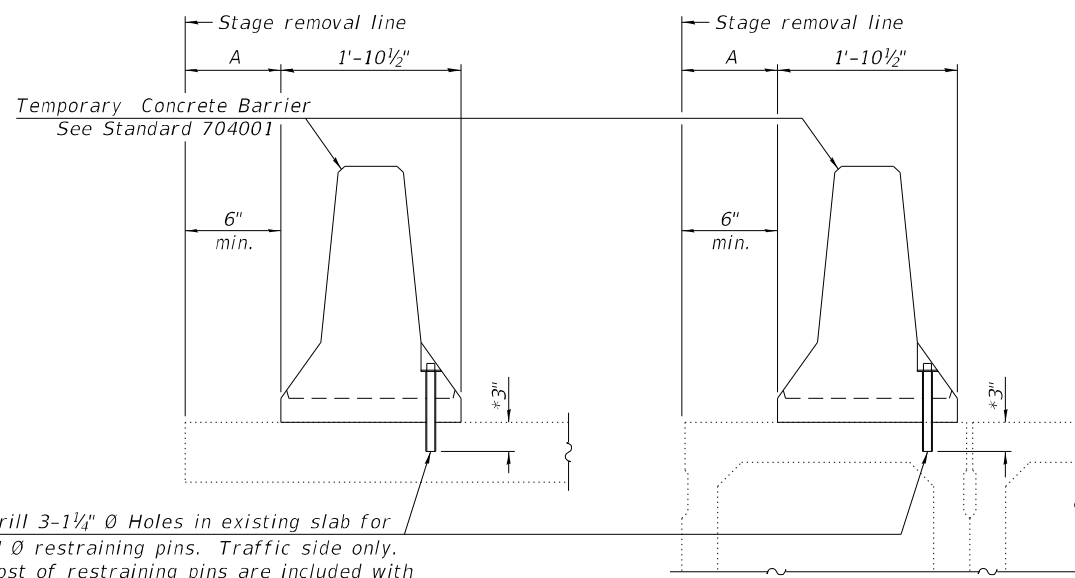
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3730	15-00131-01-BR	COOK	109	64
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ILLINOIS FED. AID PROJECT				



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



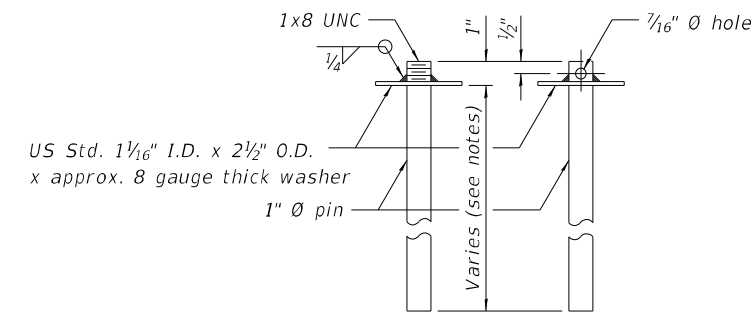
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

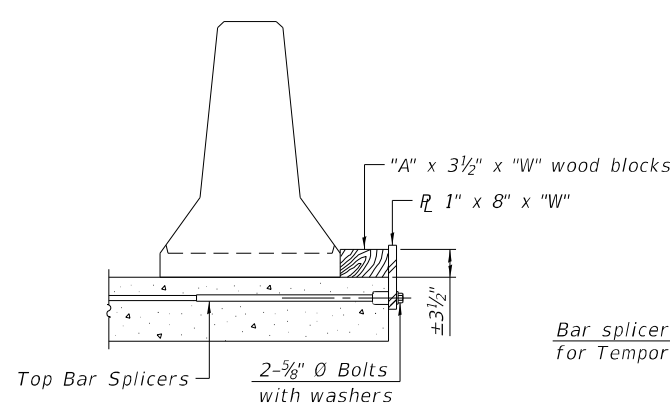
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

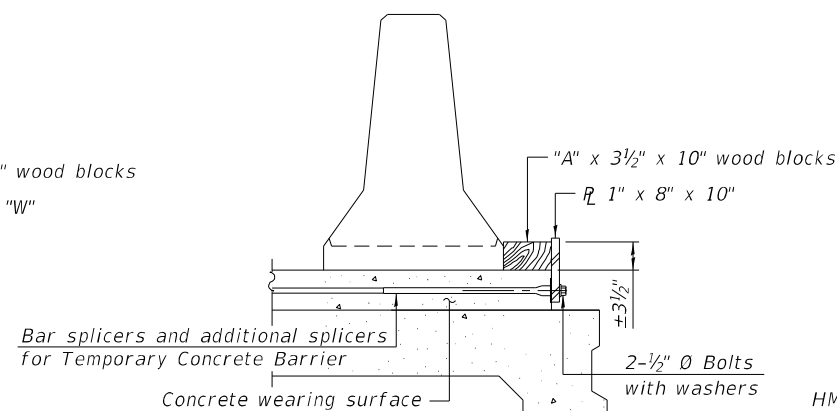
SECTIONS THRU SLAB OR DECK BEAM



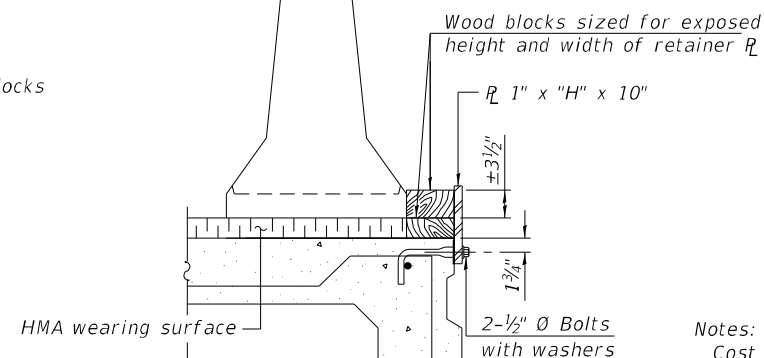
RESTRAINING PIN



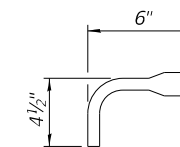
DETAIL I



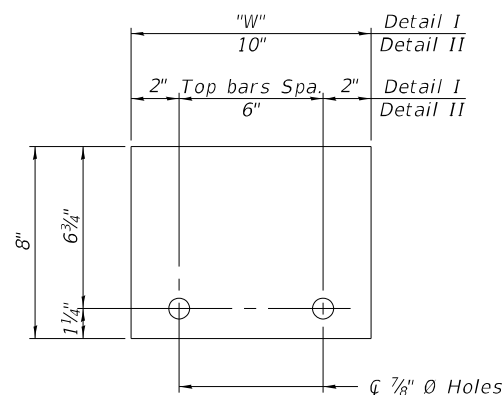
DETAIL II



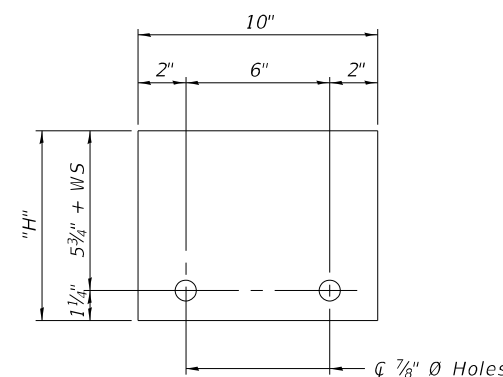
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W" (Detail I and II)



STEEL RETAINER 1" x "H" x 10" (Detail III)

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier. A retainer assembly shall be located at the approximate center of each temporary concrete barrier.

The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6' to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27 10-12-2021



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER
SHEET NO. 016-0194

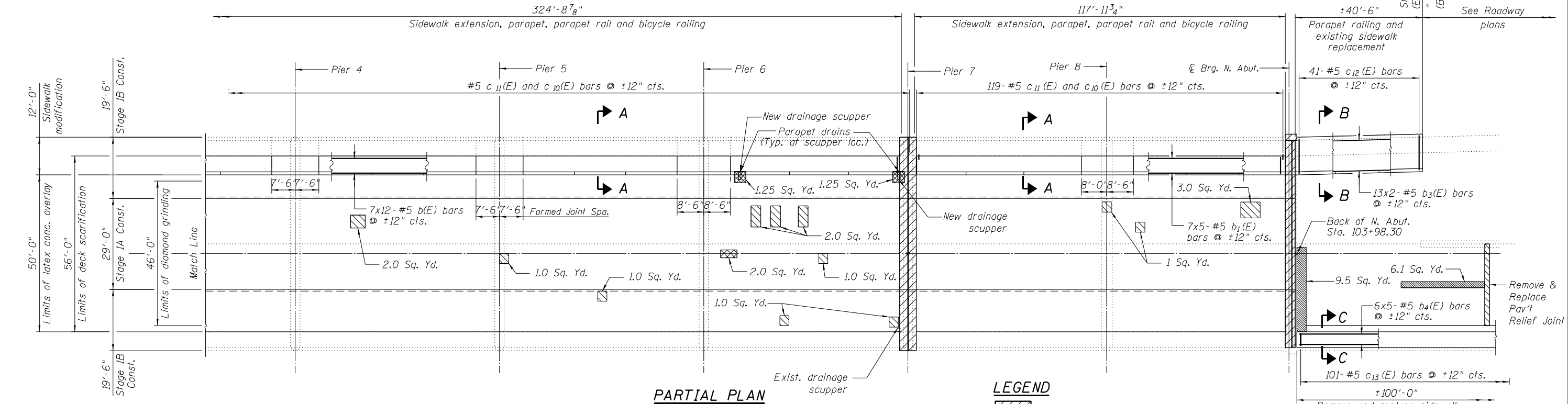
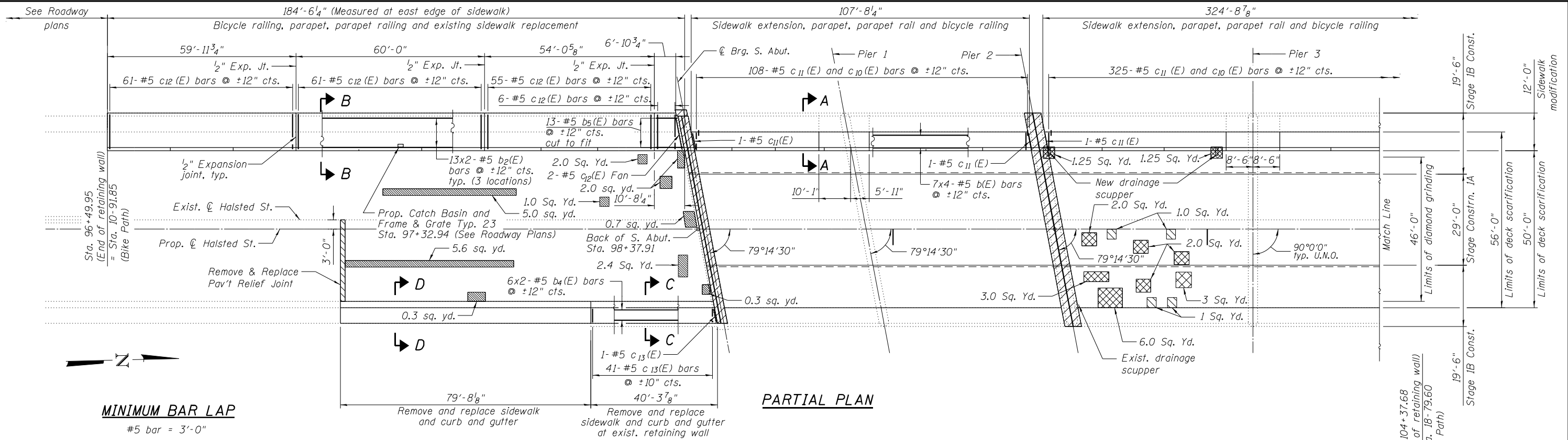
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CONTRACT NO. 62W48				

SHEET NO. 5B-5 OF 5B-30 SHEETS

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NOTES:

See Sheets SB-8 and SB-9 for Sections A-A, B-B, C-C and D-D.
 For concrete removal details see Sheet SB-7, SB-9 and SB-12.
 For stage construction details see Sheets SB-3 and SB-4.
 See Sheet SB-8 and SB-9 for superstructure details and Bill of Materials.
 See Sheet SB-8 and SB-9 for parapet reinforcement.
 See Sheet SB-18 and SB-19 for formed joint in sidewalk details.
 See Sheet SB-8, SB-10, SB-21 and SB-22 for Drainage Details.
 Bars indicated thus 7 x 8-#5 etc. indicates 7 lines of bars with 8 lengths per line.

Deck Repair Notes:

Deck & Approach slab repair areas are estimated and based on visual inspection performed by others. The actual quantity and locations of slab repair shall be determined by the Engineer in the field. The Engineer shall show the actual locations of the deck repairs on as-built plans.

The Contractor shall take precautions not to damage the existing beams and shear studs during the construction. Any damage shall be repaired by the Contractor to the satisfaction of the Engineer at no additional cost.

LEGEND

	Expansion joint removal and replacement
	Partial depth concrete deck repair
	Partial depth approach slab repair
	Full depth concrete deck repair

BILL OF MATERIAL

Item	Unit	Total
Approach Slab Repair (Partial Depth)	Sq. Yd.	37
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	27

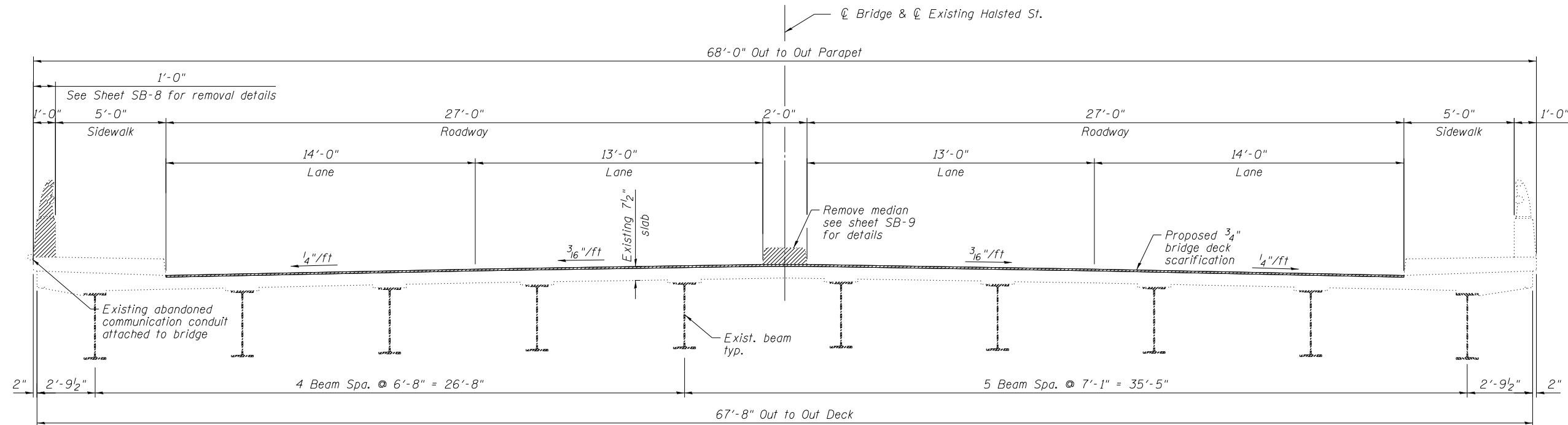


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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SUPERSTRUCTURE PLAN
 STRUCTURE NO. 016-019A**

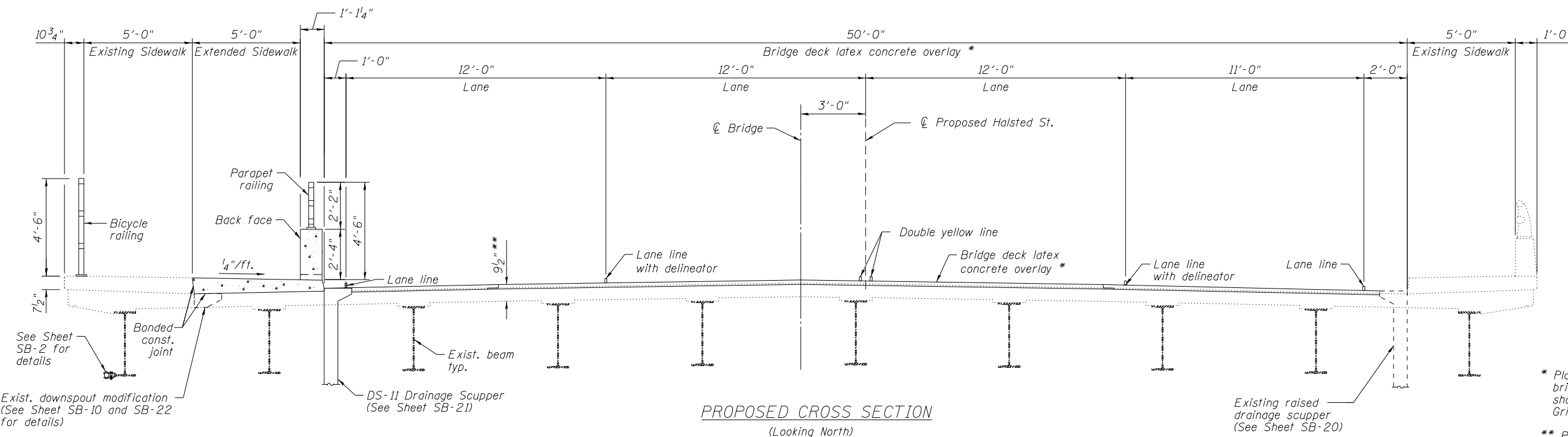
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CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



EXISTING CROSS SECTION
(Looking North)

LEGEND

Indicates concrete removal



PROPOSED CROSS SECTION
(Looking North)

NOTES

1. See Sheet SB-22 "Drainage System" for downspout extension details.
2. See Sheet SB-3 & SB-4 for stage construction details.
3. See Roadway Plans for pavement marking limits and details.

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 AECOM, Inc. 303 East Wacker Drive, Suite 1400 Chicago, IL 60601-5276 Phone: (312) 373-7700 Fax: (312) 373-6800



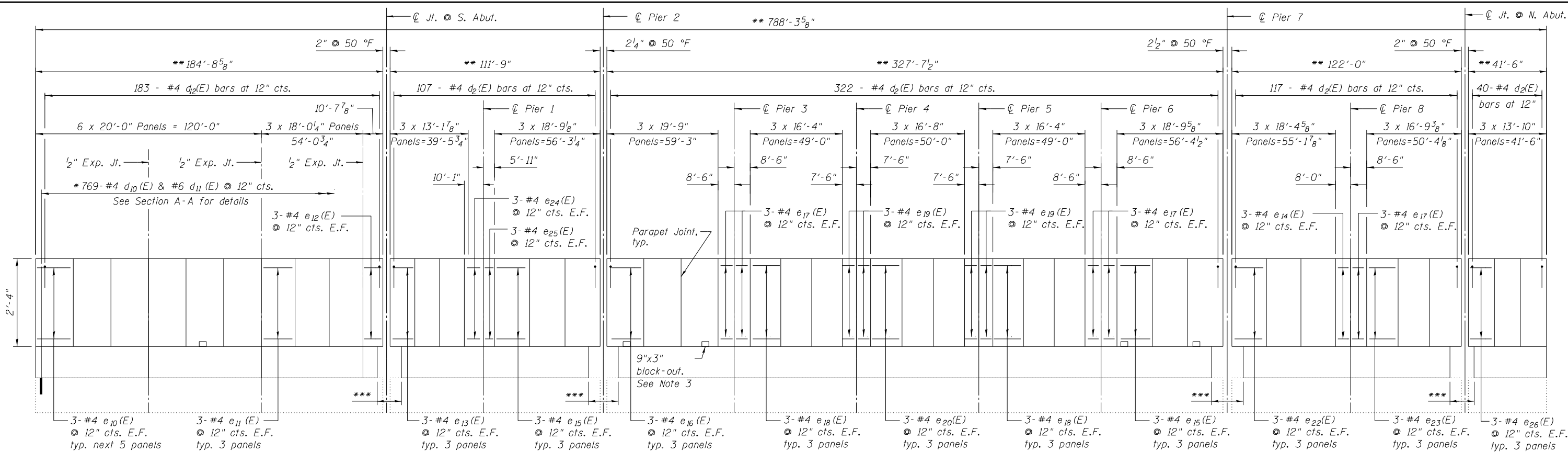
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE SECTIONS
STRUCTURE NO. 016-0194

SHEET NO. 5B-7 OF 5B-30 SHEETS

F.A.U. RTE. 3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 67
CONTRACT NO. 62W48				
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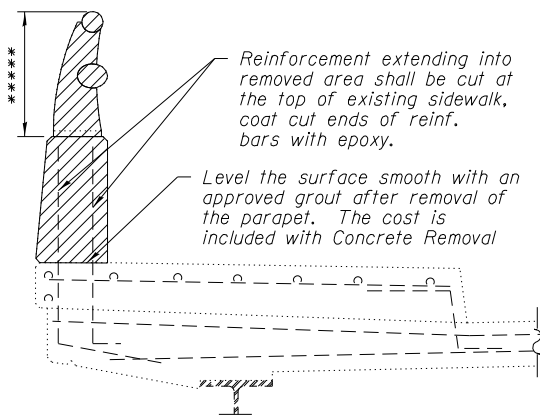
INSIDE ELEVATION OF PARAPET
(Looking West)

* Prior to drilling holes, the Contractor shall locate existing reinforcement and layout the hole locations to miss the existing rebar. The Contractor shall not cut and/or damage the existing reinforcement. Any damage to the existing reinforcement shall be repaired by the Contractor at no additional cost.

** Measured along front face of parapet.

*** See sheets SB-13 thru SB-16 for reinforcement details associated with the expansion joint replacement.

**** Prior to grinding

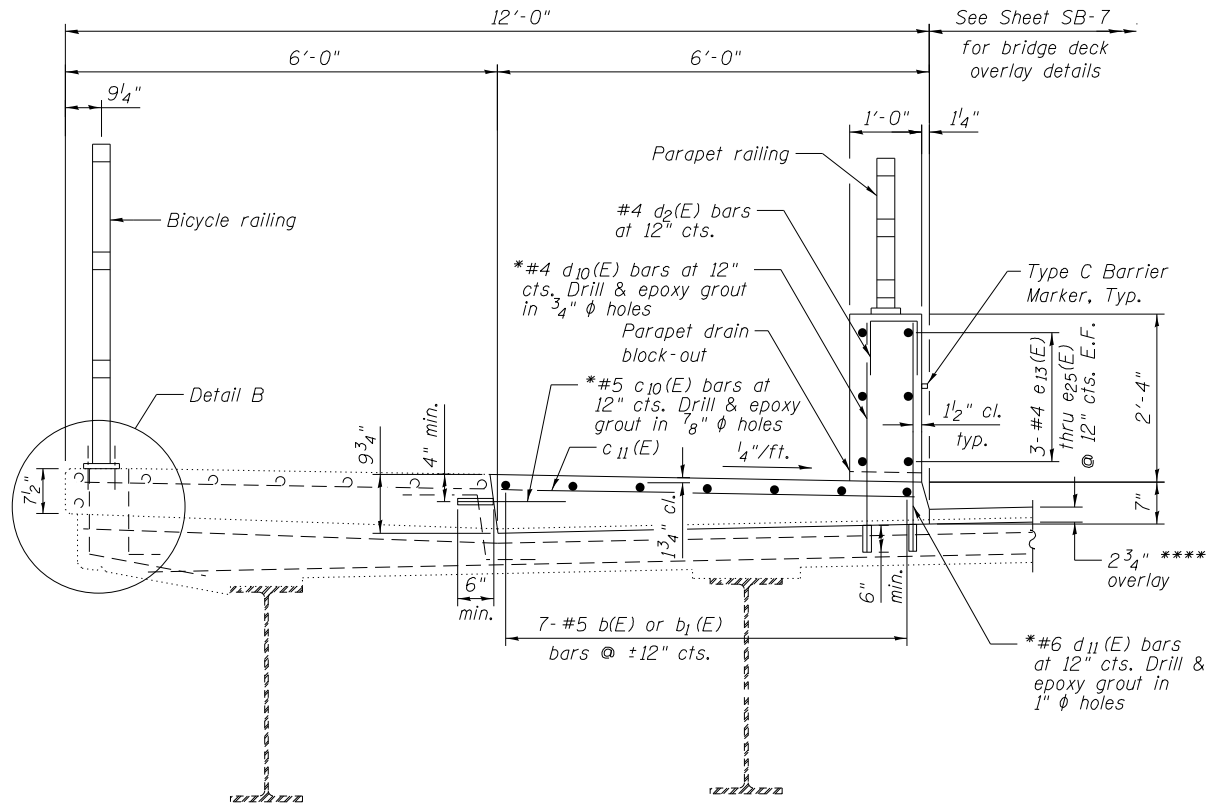


SECTION A-A REMOVAL

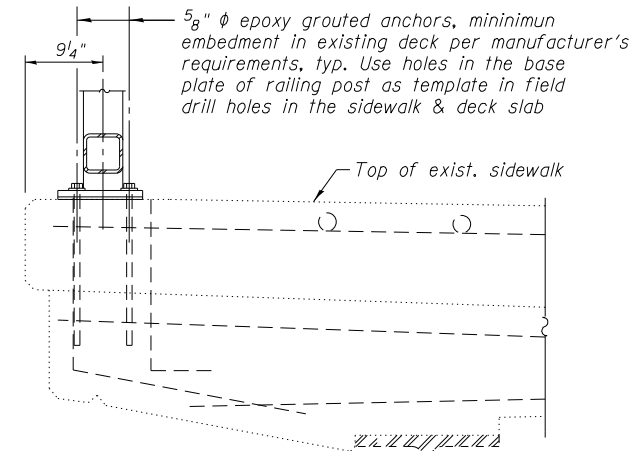
***** Salvage the existing metal railing and deliver to the Department as directed by the Engineer.

LEGEND

- Indicates concrete structure removal



SECTION A-A CONSTRUCTION



DETAIL B

BAR d2(E) & d2(E)

NOTES:

1. See sheet SB-6 for location of Section A-A.
2. See sheet SB-10 for Parapet Joint Details.
3. Provide block-outs at base of parapet at all locations in front of drainage scuppers. Cost included in Concrete Superstructure.
4. Drilling and grouting of anchor rods and bars shall be per Standard Specification Section 584.

SUPERSTRUCTURE
BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
b1(E)	112	#5	30'-2"	—	
b2(E)	35	#5	26'-3"	—	
b3(E)	78	#5	31'-4"	—	
b4(E)	26	#5	21'-7"	—	
b5(E)	42	#5	22'-5"	—	
b6(E)	13	#5	9'-4"	—	
c10(E)	552	#5	1'-9"	—	
c11(E)	555	#5	5'-6"	—	
c12(E)	226	#5	11'-6"	—	
c13(E)	143	#5	4'-1"	—	
d2(E)	546	#4	2'-0"	□	
d10(E)	769	#4	3'-2"	—	
d11(E)	769	#6	3'-2"	—	
d12(E)	223	#4	3'-0"	□	
e10(E)	36	#4	19'-8"	—	
e11(E)	18	#4	17'-8"	—	
e12(E)	6	#4	10'-4"	—	
e13(E)	18	#4	12'-10"	—	
e14(E)	6	#4	7'-8"	—	
e15(E)	36	#4	18'-5"	—	
e16(E)	18	#4	19'-5"	—	
e17(E)	30	#4	8'-2"	—	
e18(E)	36	#4	16'-0"	—	
e19(E)	24	#4	7'-2"	—	
e20(E)	18	#4	16'-4"	—	
e22(E)	18	#4	18'-0"	—	
e23(E)	18	#4	16'-5"	—	
e24(E)	6	#4	9'-9"	—	
e25(E)	6	#4	5'-7"	—	
e26(E)	18	#4	13'-6"	—	
Reinforcement Bars, Epoxy Coated				Lbs.	25,790
Concrete Superstructure				Cu. Yds.	236.0

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS 1
STRUCTURE NO. 016-0194

SHEET NO. 58-8 OF 58-30 SHEETS



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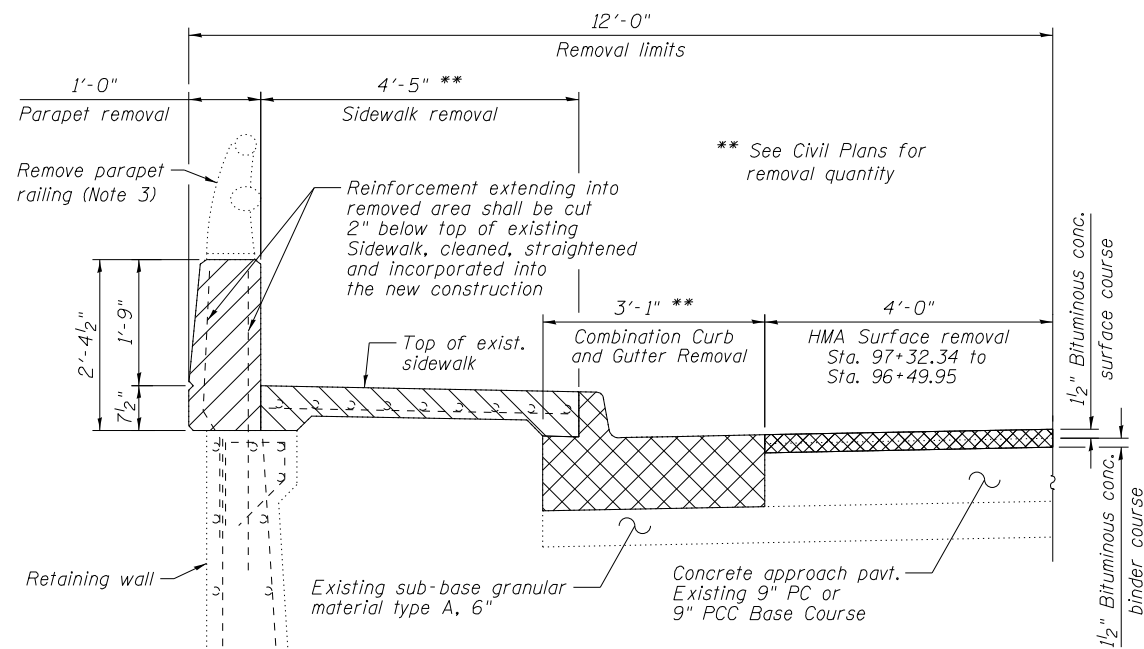
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STRUCTURE NO. 016-0194

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CONTRACT NO. 62W48				

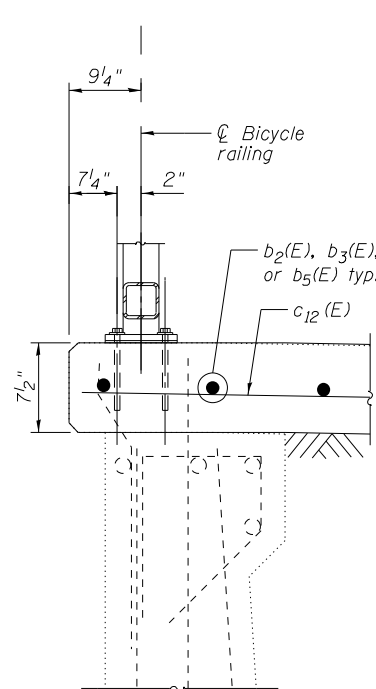
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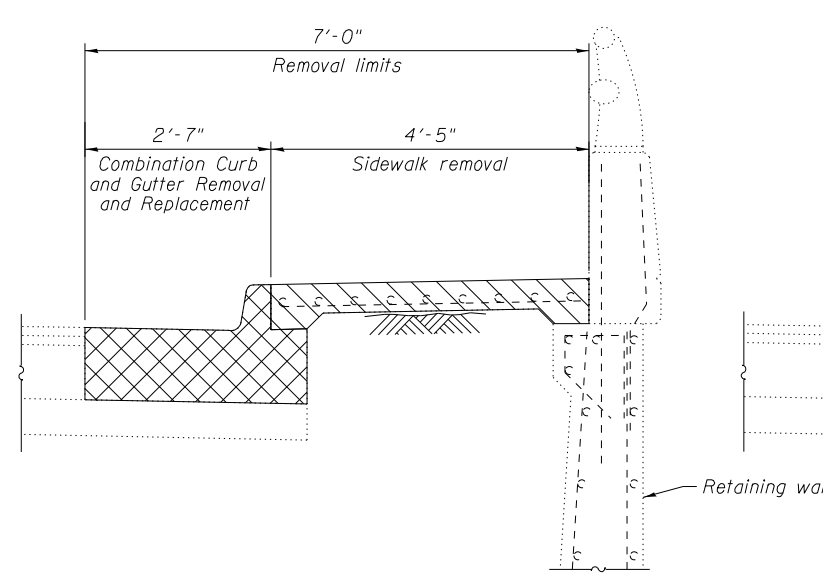


SECTION B-B REMOVAL

Removal limits from Sta. 96+49.95 to Sta. 98+34.52 and Sta. 103+97.80 to Sta. 104+38.30 (Measured along east edge of sidewalk)

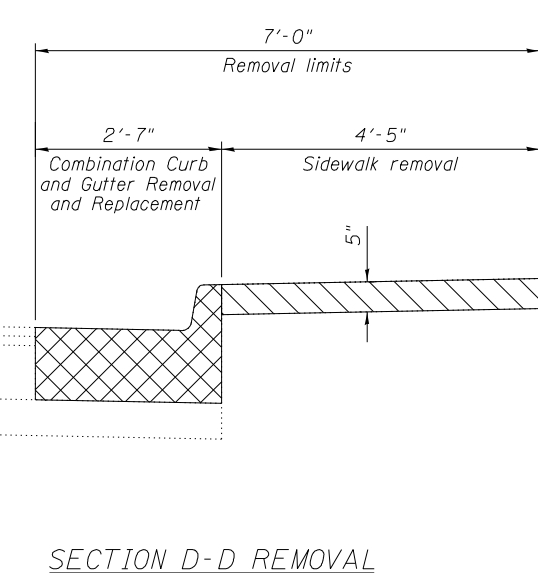


DETAIL A



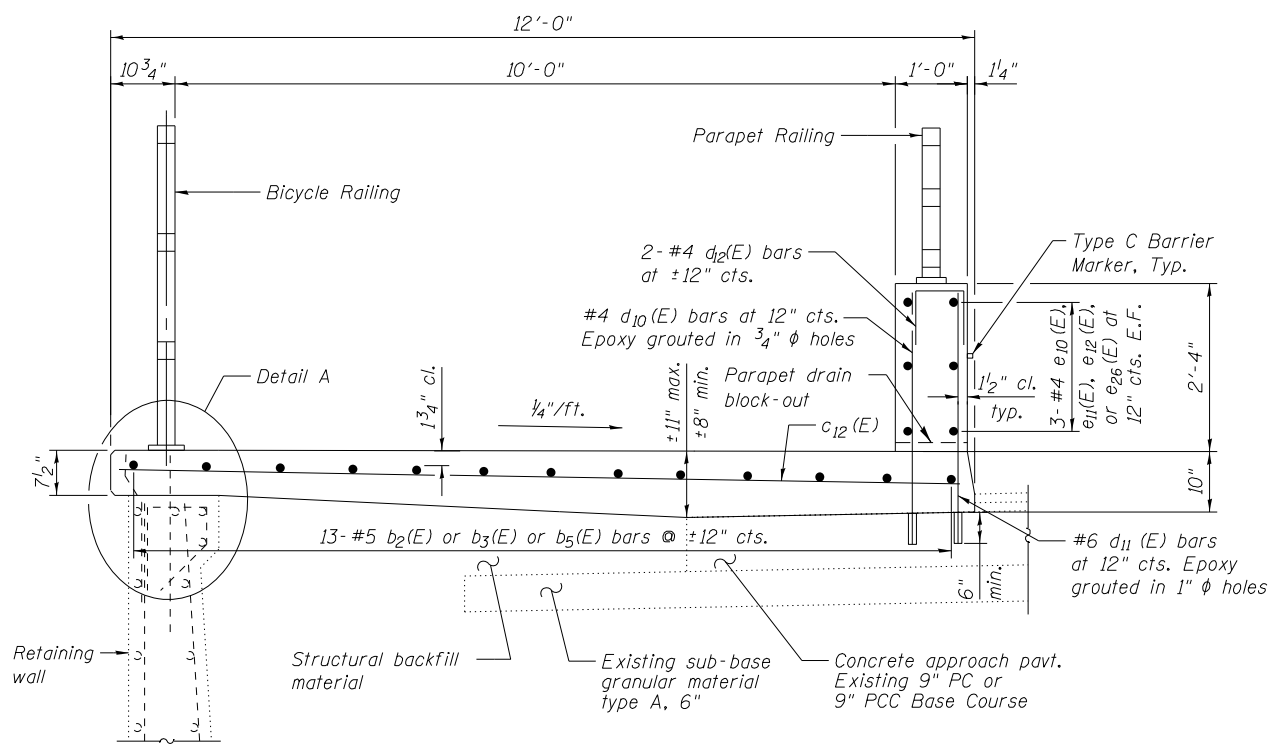
SECTION C-C REMOVAL

Removal limits from Sta. 98+02.75 to Sta. 98+43.27 (South Approach) Sta. 103+97.80 to Sta. 104+97.80 (North Approach) (Measured along west edge of sidewalk)



SECTION D-D REMOVAL

Removal limits from Sta. 97+23.07 to Sta. 98+02.75



SECTION B-B CONSTRUCTION

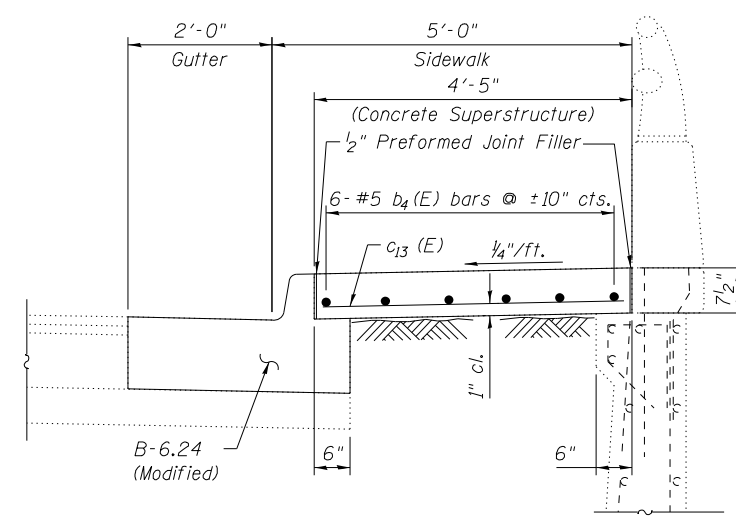
Parapet and bike path included under Concrete Superstructure

NOTES:

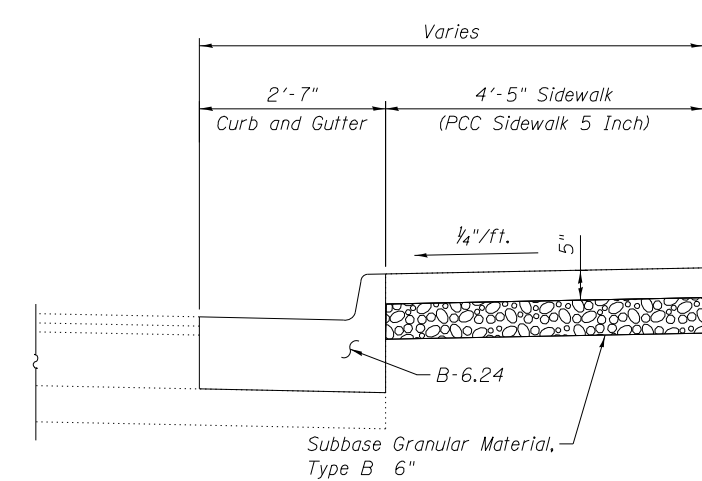
1. See Sheet SB-6 for location of Sections B-B, C-C and D-D.
2. Rebar and concrete superstructure quantity of the sidewalk shown in Section B-B and Section C-C this sheet is billed with the bridge superstructure. See Sheet SB-8 for rebar and concrete quantity.
3. Salvage the existing metal railing and deliver to the Department as directed by the Engineer.
4. Epoxy grout bars in accordance with Article 584 of the Standard Specifications.

LEGEND

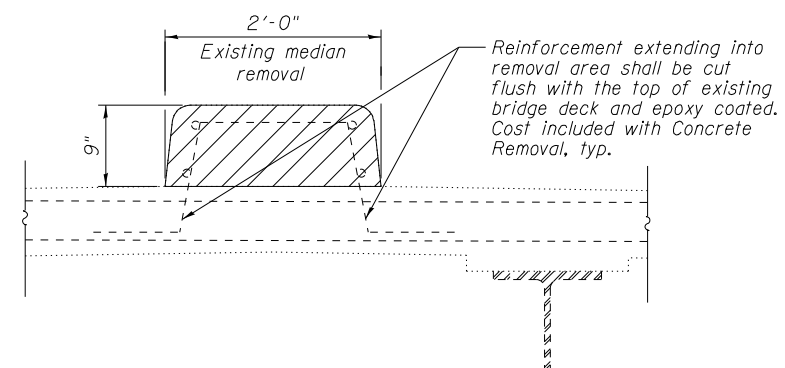
- Indicates concrete removal
- Indicates HMA surface removal
- Indicates sidewalk removal
- Indicates curb and gutter removal



SECTION C-C CONSTRUCTION



SECTION D-D CONSTRUCTION



MEDIAN REMOVAL DETAILS

Removal limits from Sta. 98+36.43 to Sta. 103+96.80 Structural concrete removal only (Measured along centerline of existing Halsted St.)

BILL OF MATERIAL

Item	Unit	Quantity
* Comb. Conc. Curb and Gutter Removal and Replacement	Ft.	220
* Sidewalk Removal	Sq. Ft.	973
* Concrete Removal	Cu. Yd.	91.7
* Portland Cement Concrete Sidewalk 5"	Sq. Ft.	352
* Subbase Granular Material, Type B 6"	Sq. Yd.	39

* East Side

Concrete removal quantity shown this sheet for median, parapets and abutment pilasters.

MODEL: 4100.DEL.NAMES FILE: NAME: 016-019A-09 SUPER.DWG DATE: 4/2/2024



USER NAME = ken.moy
DESIGNED - KO
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PLOT SCALE = N.T.S.
DRAWN - KO
PLOT DATE = 4/2/2024

REVISIONS
REVISOR
DATE
REVISIONS
REVISOR
DATE

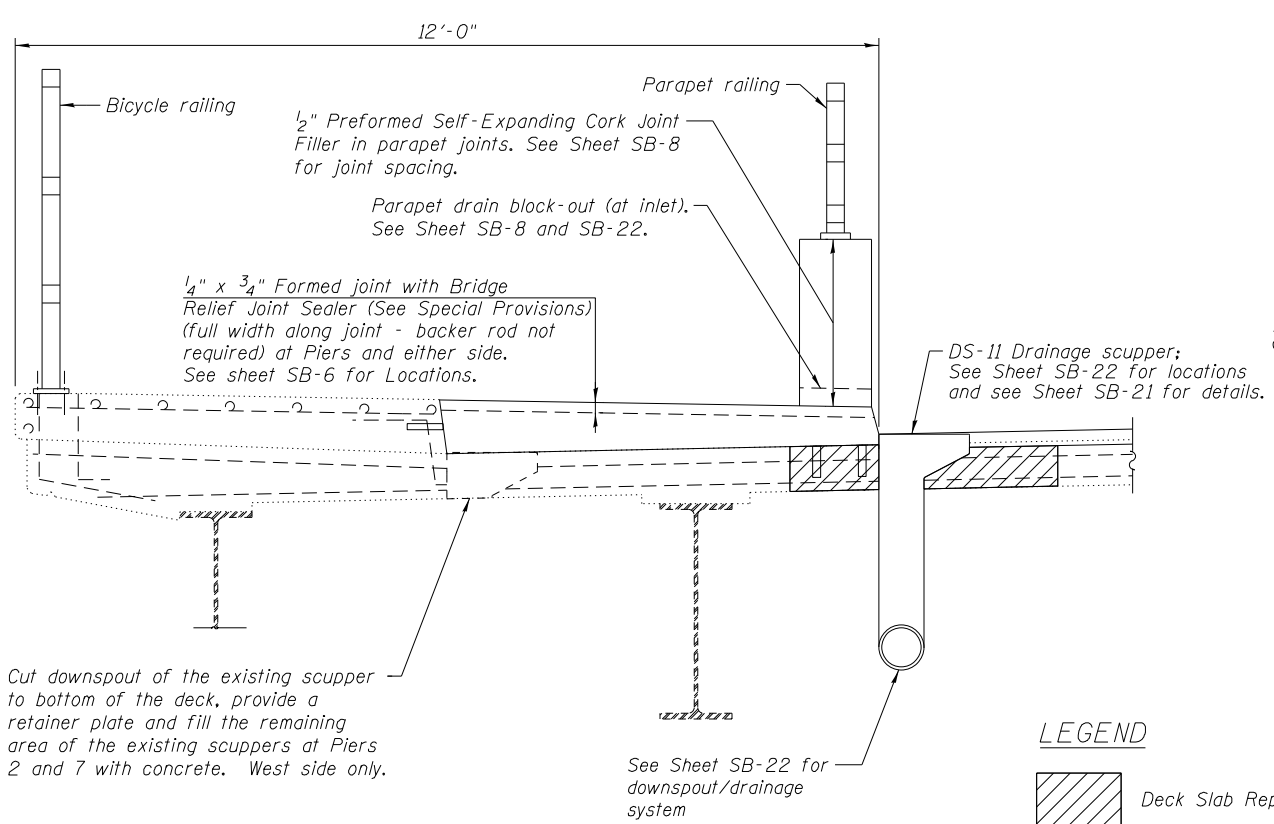
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUPERSTRUCTURE DETAILS 2
STRUCTURE NO. 016-0194**

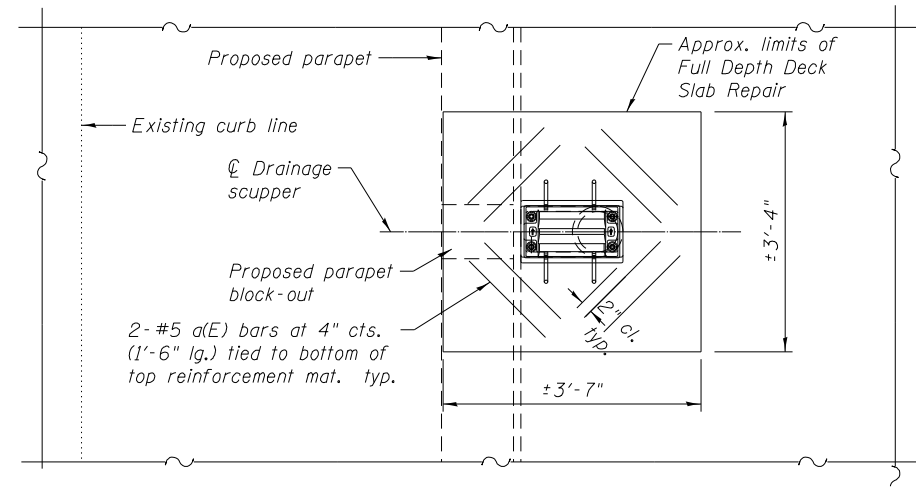
SHEET NO. 5B-9 OF 5B-30 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	69
CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT

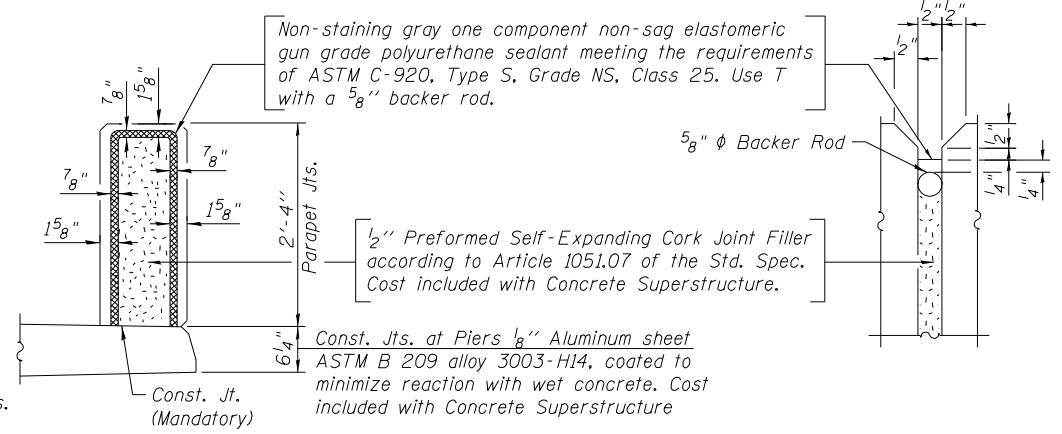


SECTION THRU PARAPET

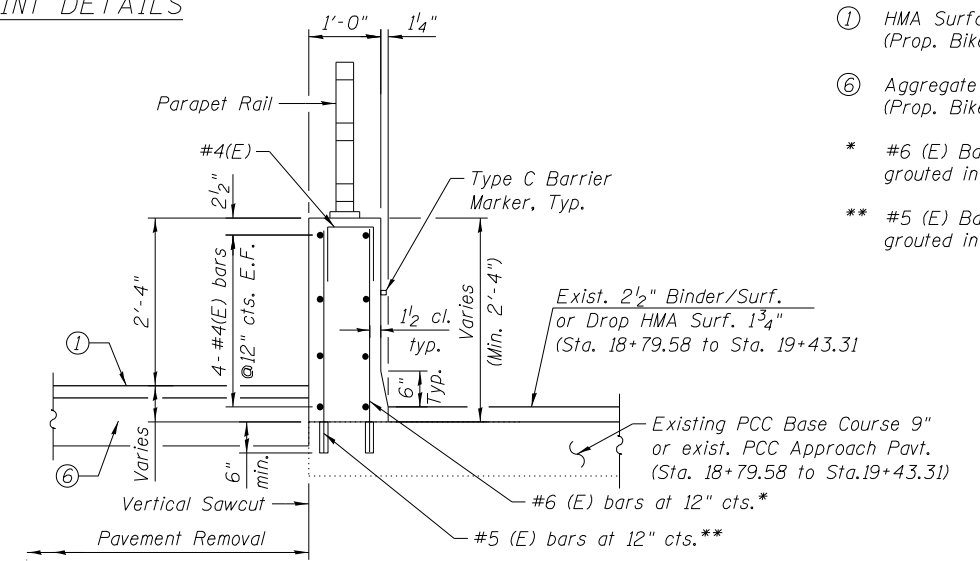


TOP PLAN AT DRAINAGE SCUPPER

Notes:
 Deck removal and replacement for drainage scupper installation shall be included with Deck Slab Repair (Full Depth, Type II). See Sheet SB-8 for quantity.
 The cost for reinforcement bars for the drainage scuppers shall be included in the cost for Drainage Scuppers, DS-11.
 Drainage scupper top elevation shall be recessed to accommodate diamond grinding. Place drainage scupper 1/4" below the top of slab.



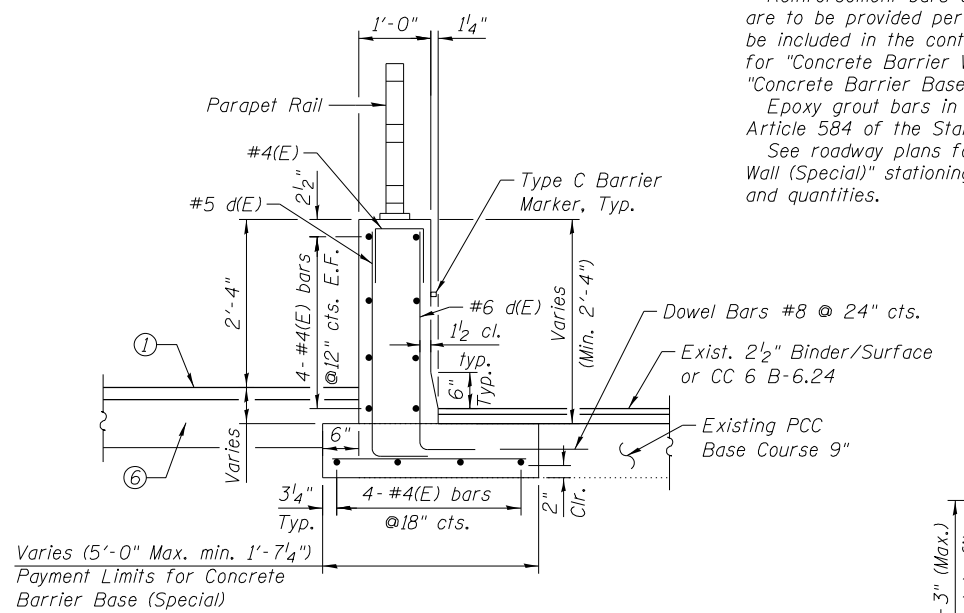
PARAPET JOINT DETAILS



CONCRETE BARRIER WALL (SPECIAL)

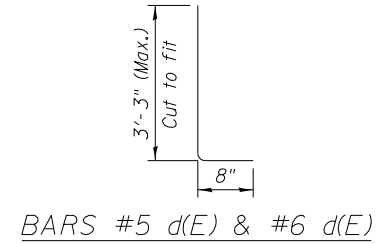
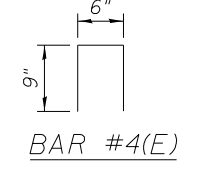
Sta. 9+24.44 to Sta. 10+91.85
 Sta. 18+79.60 to Sta. 20+59.87

Notes:
 Reinforcement bars and dowel bars are to be provided per detail and shall be included in the contract cost per foot for "Concrete Barrier Wall (Special) & "Concrete Barrier Base (Special)".
 Epoxy grout bars in accordance with Article 584 of the Standard Specifications.
 See roadway plans for "Concrete Barrier Wall (Special)" stationing, additional details and quantities.



CONCRETE BARRIER WALL (SPECIAL)

(Stationing based on bike path layout)
 Sta. 7+83.69 to Sta. 9+24.44
 Sta. 20+59.87 to Sta. 21+60.69



MODEL: 1400ELMAMF6
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 PROJECT: 15-01-194-10
 SHEET: SB-10 OF SB-30 SHEETS
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 DESIGNED: KO
 CHECKED: MCC
 DRAWN: KO
 CHECKED: MCC
 PLOT SCALE: = N.T.S.
 PLOT DATE: = 4/2/2024



USER NAME = ken.joy	DESIGNED - KO	REVISD -
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PLOT DATE = 4/2/2024	CHECKED - MCC	REVISD -

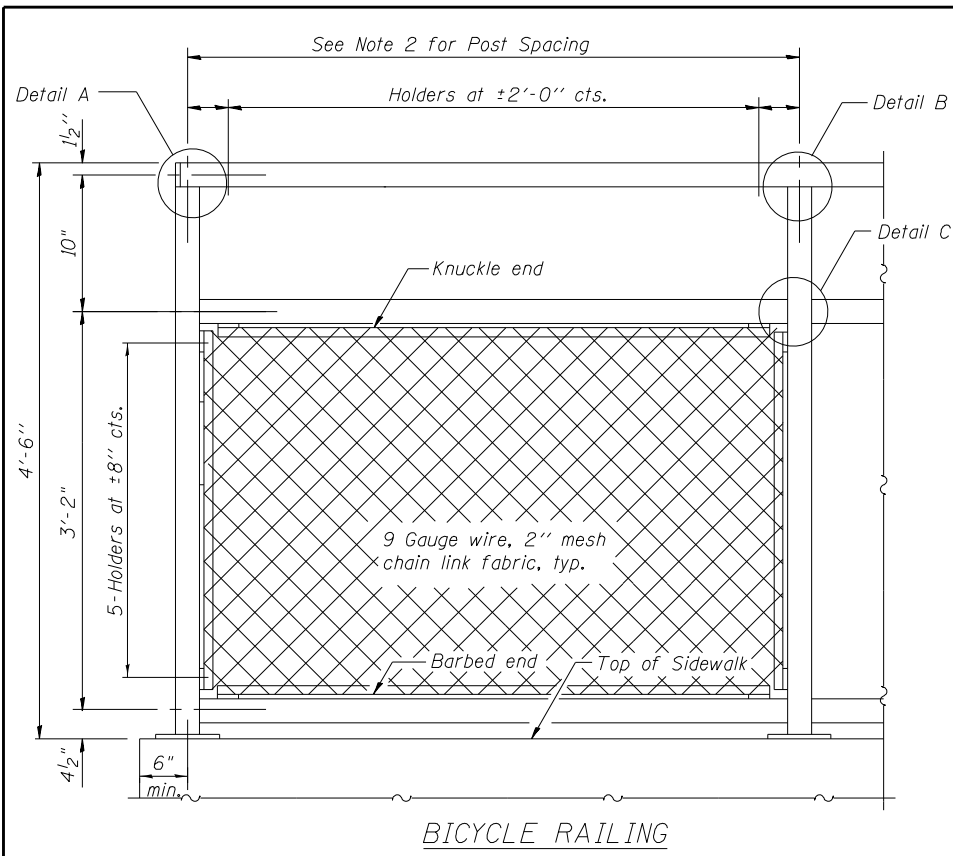
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS 3
 STRUCTURE NO. 016-0194

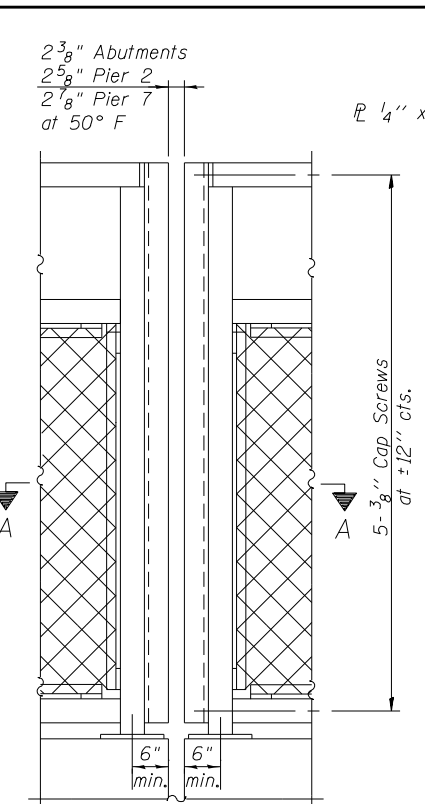
SHEET NO. SB-10 OF SB-30 SHEETS

FAU. RTE. 3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 70
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				

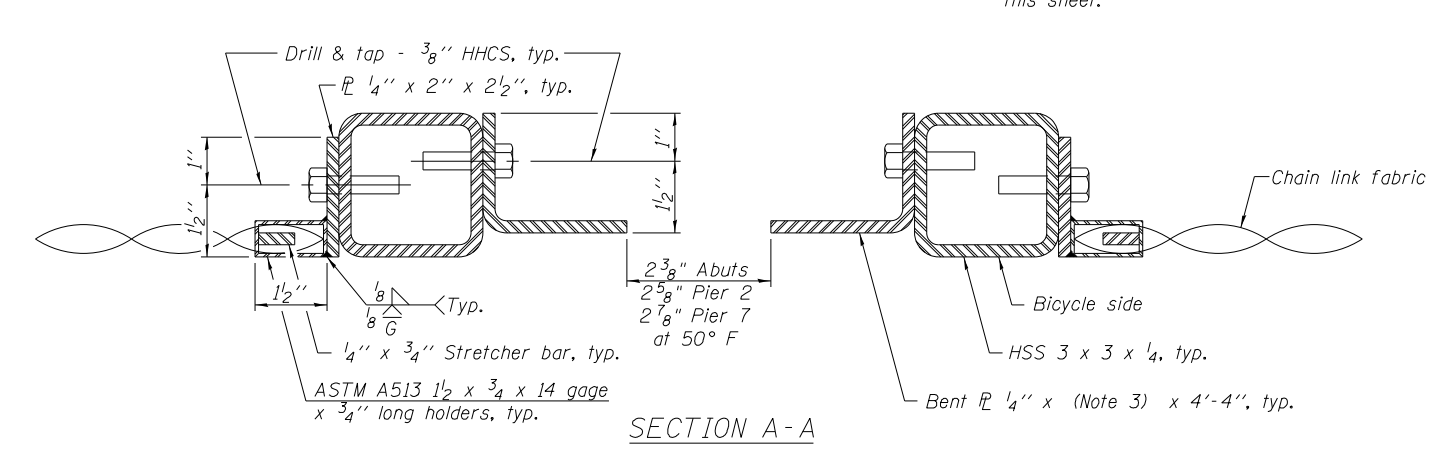
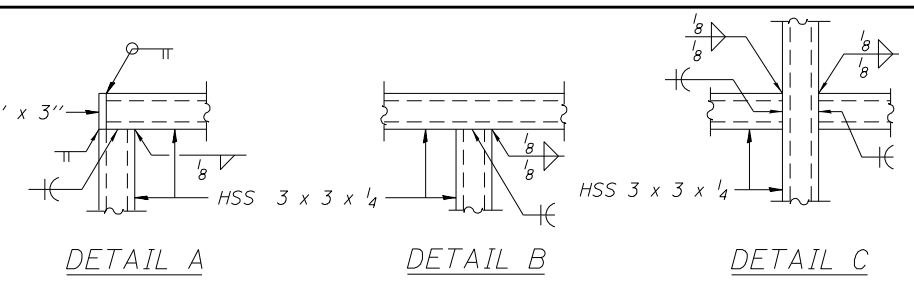
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 AECOM USA
 303 EAST WACKER DRIVE, SUITE 1400
 CHICAGO, IL 60601-5276
 PHONE: (312) 373-7700 FAX: (312) 373-6800



BICYCLE RAILING

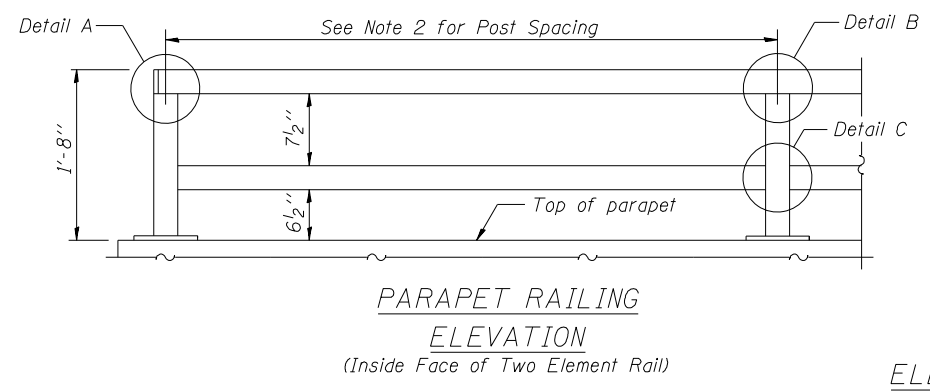


BICYCLE RAILING ELEVATION AT EXPANSION JOINT
 (Two Element Rail Shown - Three Element Rail Similar)

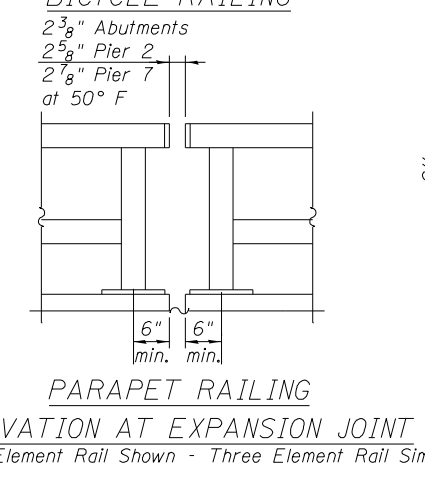


NOTES:

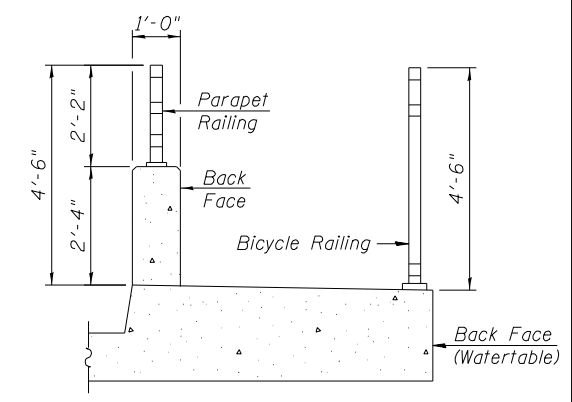
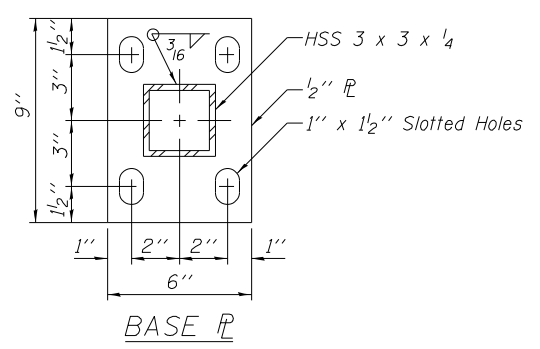
- All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.
- Railing fabricator shall determine post spacing based on dimensions shown on Sheet SB-6 Superstructure Plan and Sheet SB-8 Superstructure Details 1. 10'-0" Maximum Post Spacing.
- Railing fabricator shall determine plate size based on post spacing selected and dimensions shown this sheet.



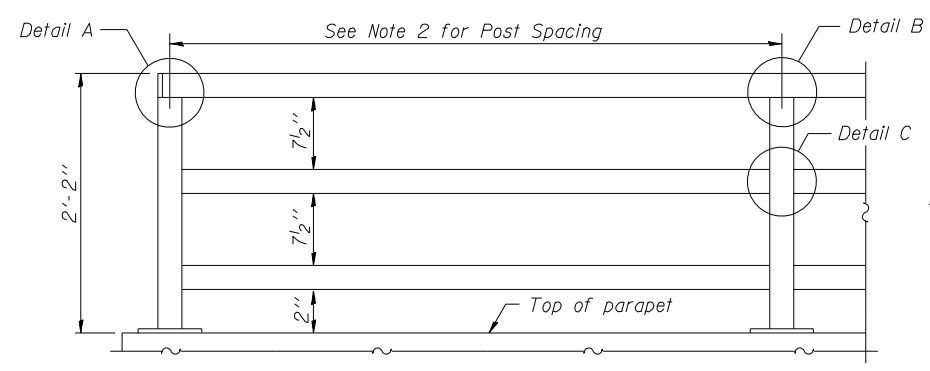
PARAPET RAILING ELEVATION
 (Inside Face of Two Element Rail)



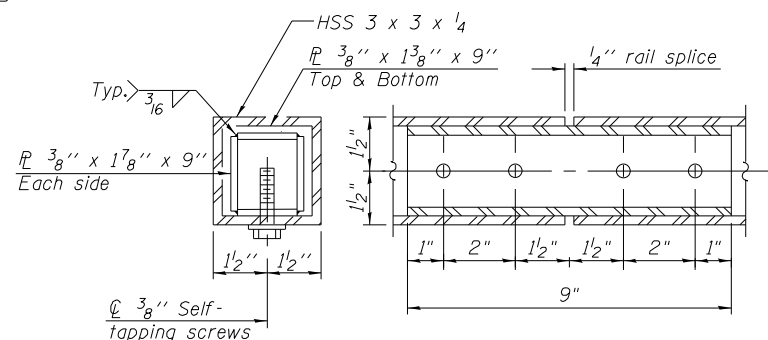
PARAPET RAILING ELEVATION AT EXPANSION JOINT
 (Two Element Rail Shown - Three Element Rail Similar)



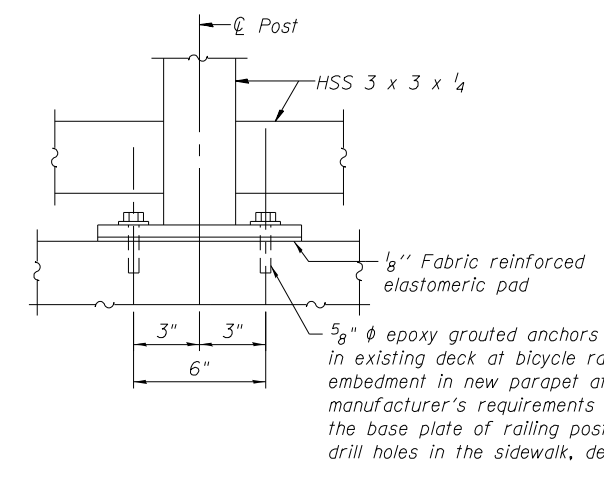
SECTION THRU SIDEWALK



PARAPET RAILING ELEVATION
 (Inside Face of Three Element Rail)



RAIL SPLICE



ANCHOR BOLT DETAILS

The Contractor shall use holes in the base plate of fabricated bicycle railing post and parapet railing post for 5/8" dia. anchors as template to field drill holes in existing sidewalk, deck slab and new parapet.

BILL OF MATERIAL

Item	Unit	Quantity
Bicycle Railing	Foot	789
Parapet Railing	Foot	789



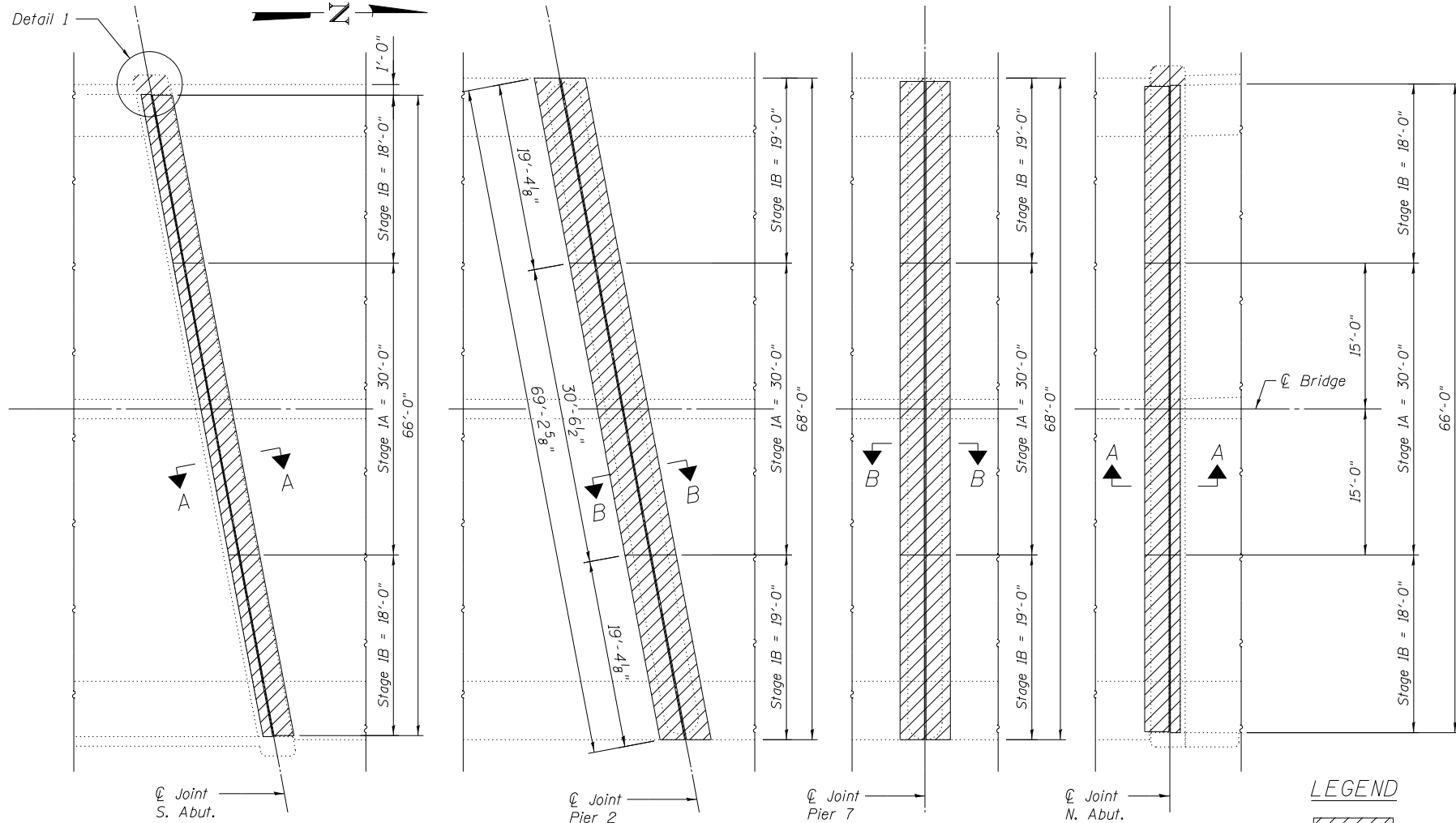
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PLOT SCALE = N.T.S.	CHECKED - MCC	REVISD -
PLOT DATE = 4/2/2024	DRAWN - KO	REVISD -
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

BICYCLE RAILING & PARAPET RAILING STRUCTURE NO. 016-0194

SHEET NO. SB-11 OF SB-30 SHEETS

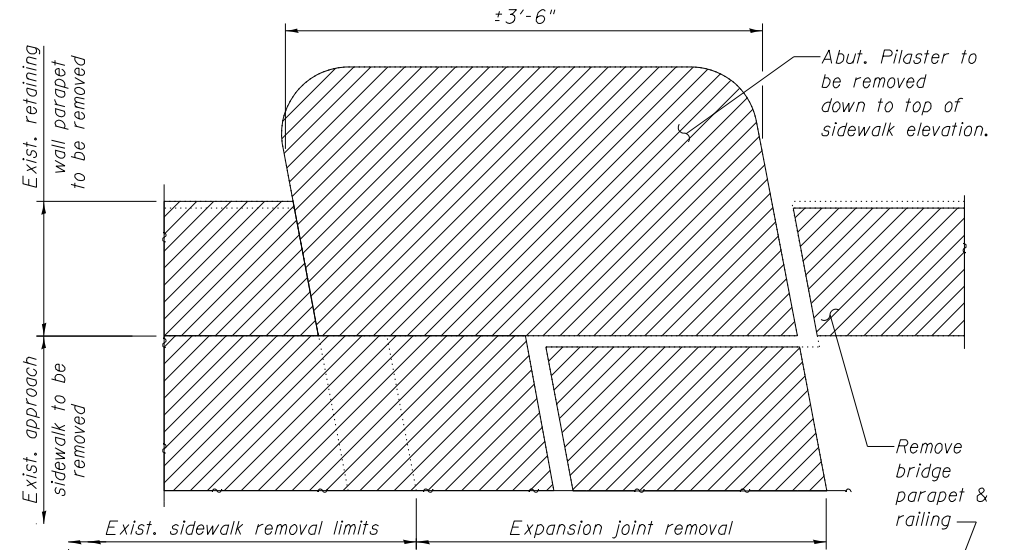
FAU. RTE. 3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 71
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



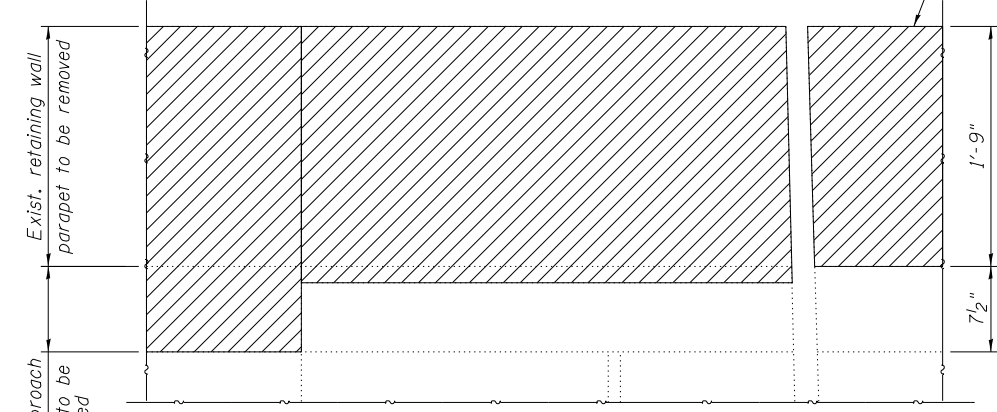
PARTIAL PLAN

LEGEND

Indicates concrete removal



DETAIL 1 - PLAN



DETAIL 1 - ELEVATION

(South Abutment west side shown. North Abutment west side similar)

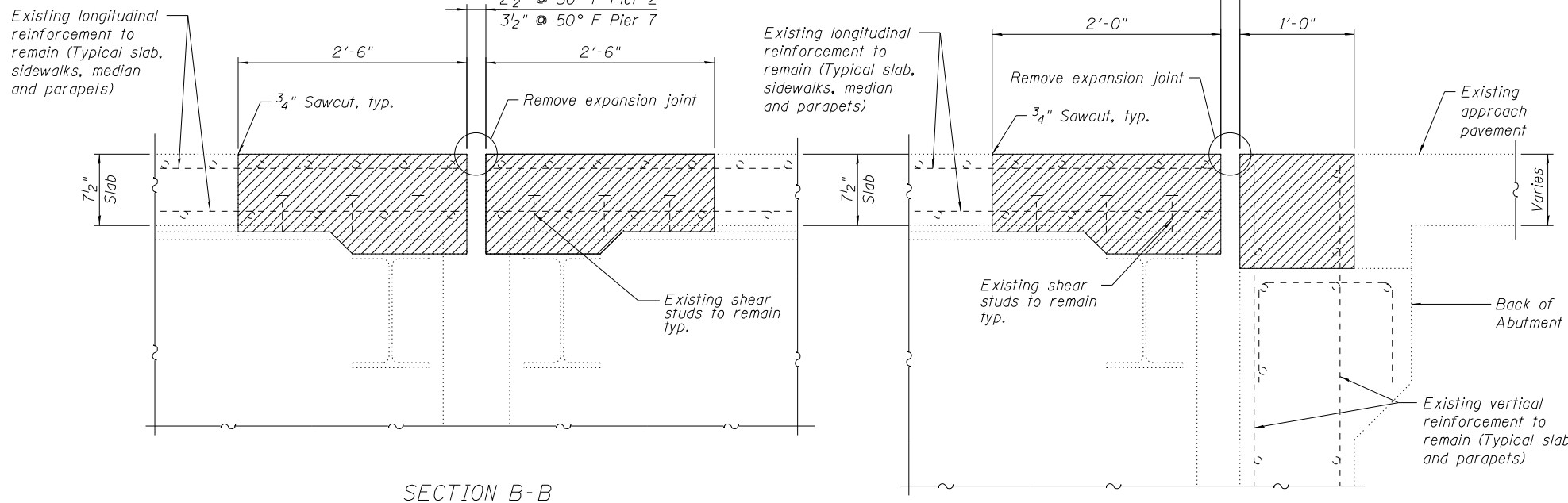
NOTES

- Removal of the existing expansion joints will not be paid for separately but shall be included in the cost for Concrete Removal.
- Hatched areas indicate concrete sections to be removed. Care shall be exercised by the Contractor during and following concrete removal to ensure the existing reinforcement remaining in place is not damaged. All existing reinforcing to be incorporated into new construction shall be blast-cleaned, straightened and properly positioned prior to concrete placement. Any reinforcement damaged during concrete removal shall be replaced using an approved embedded reinforcement or Mechanical Bar Splicer System. Cost of removal shall be included with pay item for Concrete Removal. Cost of replacement shall be included with pay item for Concrete Superstructure.
- Existing parapets and sidewalks shall be removed and replaced within the limits of hatched areas shown. All longitudinal parapet and sidewalk reinforcement shall remain in place.
- The existing railing and posts on the east parapet within the limits of concrete removal shall remain. The Contractor shall take precautions not to damage the existing parapet railing during construction. Any damage to the railing shall be repaired by the Contractor to the satisfaction of the Engineer at no additional cost.
- The Contractor shall take precautions not to damage the existing steel beams and shear studs during the construction. Any damage shall be repaired by the Contractor to the satisfaction of the Engineer at no additional cost.

BILL OF MATERIAL

Item	Unit	Total
Concrete Removal	Cu. Yd.	41.7

Removal Quantity for Expansion Joint only
See Sheet SB-9 for Pilaster and remaining railing removal Quantity.



SECTION B-B

SECTION A-A

Note:
Shear studs are not present at South Abutment.

MODEL: 4100 DEL NAME: FILE: NAME: ...



USER NAME = ken.moy	DESIGNED - NPP	REVISD -
PLOT SCALE = N.T.S.	CHECKED - MCC	REVISD -
PLOT DATE = 4/2/2024	DRAWN - NPP	REVISD -
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DEPARTMENT OF TRANSPORTATION

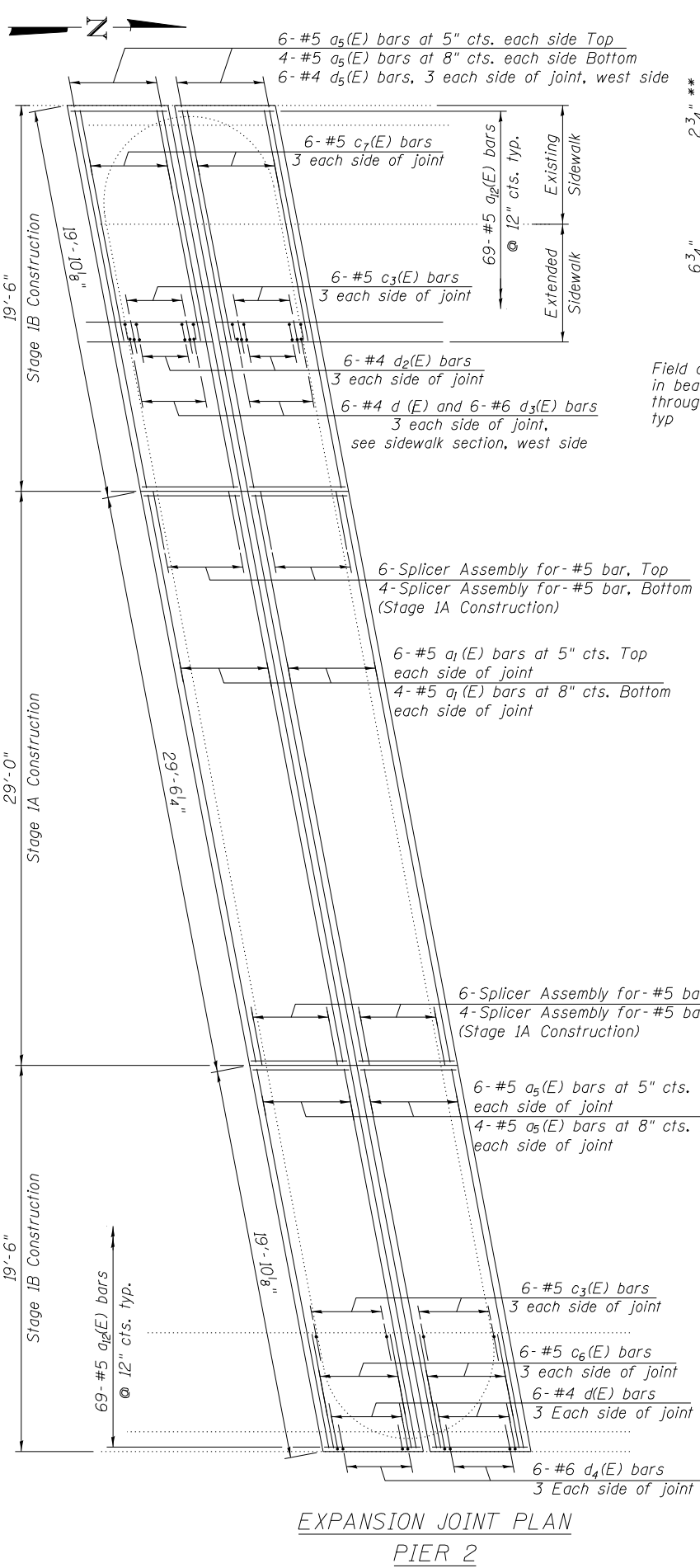
EXISTING EXPANSION JOINT REMOVAL
STRUCTURE NO. 016-0194

SHEET NO. 5B-12 OF 5B-30 SHEETS

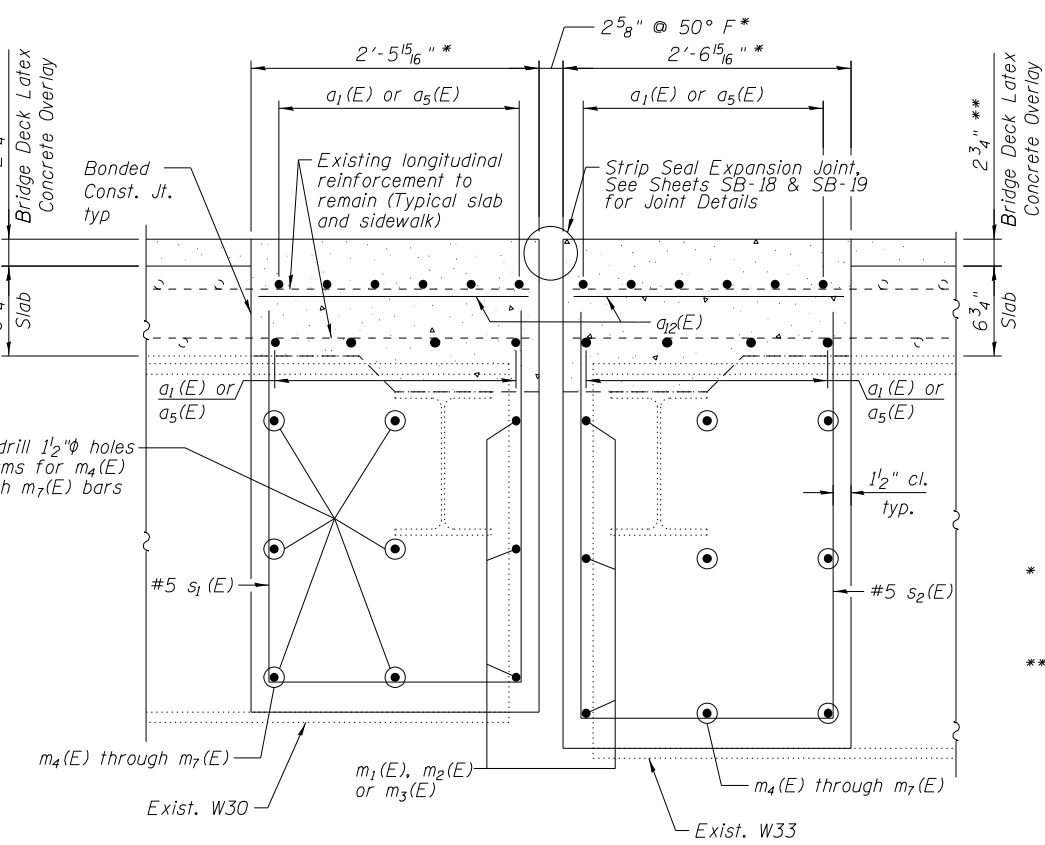
FAU-RT-3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 72
CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT

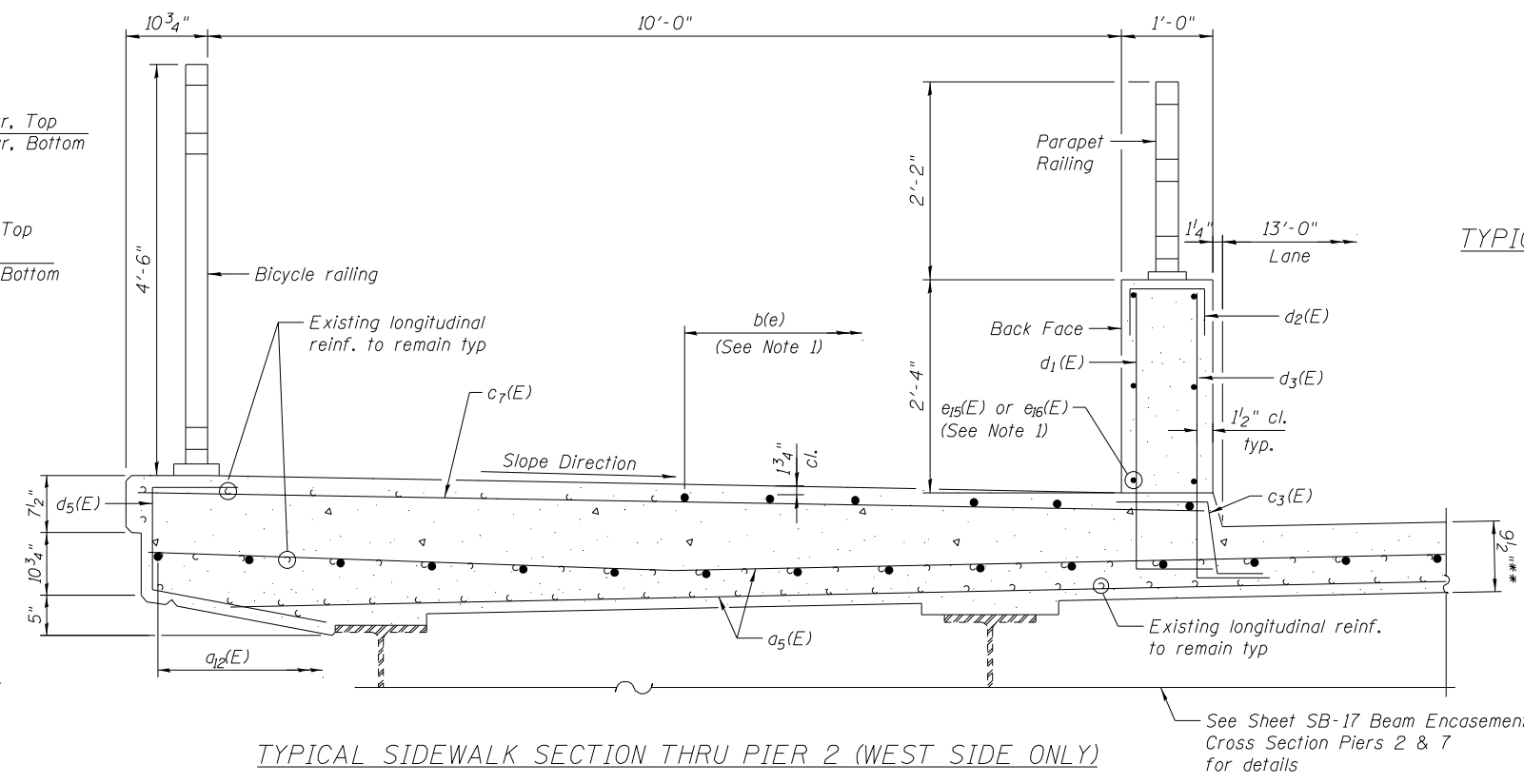
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 AECOM USA, 303 East Wacker Drive, Suite 1400, Chicago, IL 60601-5276, Phone: (312) 373-7700, Fax: (312) 373-6800



EXPANSION JOINT PLAN
PIER 2



TYPICAL SECTION AT EXPANSION JOINT PIER 2
(@ Right Angle to Joint)
(Looking West)

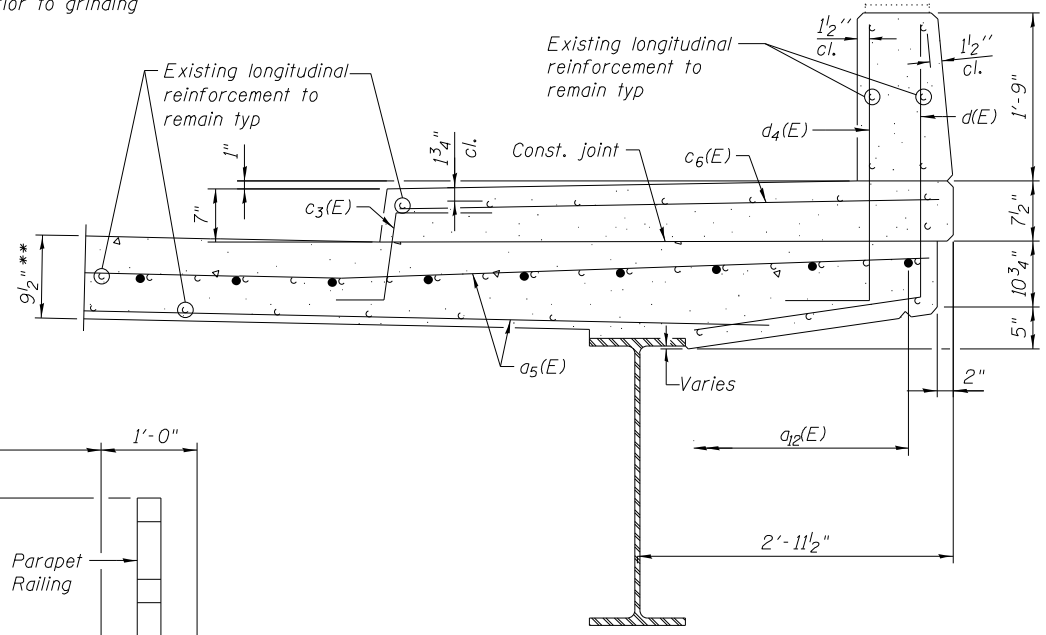
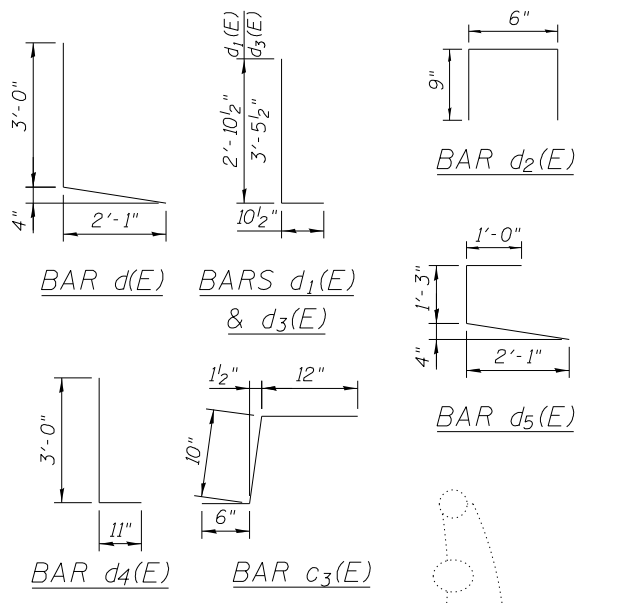


TYPICAL SIDEWALK SECTION THRU PIER 2 (WEST SIDE ONLY)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₁ (E)	20	#5	29'-3"	—
a ₅ (E)	40	#5	19'-4"	—
a ₂ (E)	138	#5	2'-3"	—
c ₃ (E)	12	#5	2'-4"	⌒
c ₆ (E)	6	#5	5'-9"	—
c ₇ (E)	6	#5	11'-8"	—
d(E)	6	#4	5'-1"	⌒
d ₁ (E)	6	#4	3'-9"	⌒
d ₂ (E)	6	#4	2'-0"	⌒
d ₃ (E)	6	#6	4'-4"	⌒
d ₄ (E)	6	#6	3'-11"	⌒
d ₅ (E)	6	#4	4'-4"	⌒
Concrete Superstructure			Cu. Yd.	14.3
Reinforcement Bars, Epoxy Coated			Pound	2,020

* Showing dimension for rolled rail joint. Contractor shall adjust for welded rail joint. See Sheets SB-18 & SB-19 for details.
 ** Prior to grinding



TYPICAL SIDEWALK SECTION
(EAST SIDE ONLY)
(Match Adjacent)

- NOTES**
1. Bars b(e), e₁₅(E), e₁₆(E) and parapet concrete billed with superstructure. See Sheet SB-6 for bar location in sidewalk and parapet and Sheet SB-8 for rebar and concrete quantity.
 2. See Sheet SB-17 for beam encasement cross section Piers 2 & 7 details and bar bill.
 3. For details of Strip Seal Expansion Joints, see Sheets SB-18 & SB-19.
 4. Existing longitudinal reinforcement in east parapet shall remain in place.
 5. See Sheet SB-30 for details of bar splicers.



USER NAME = ken.moy
 DESIGNED - NPP
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 PLOT SCALE = N.T.S.
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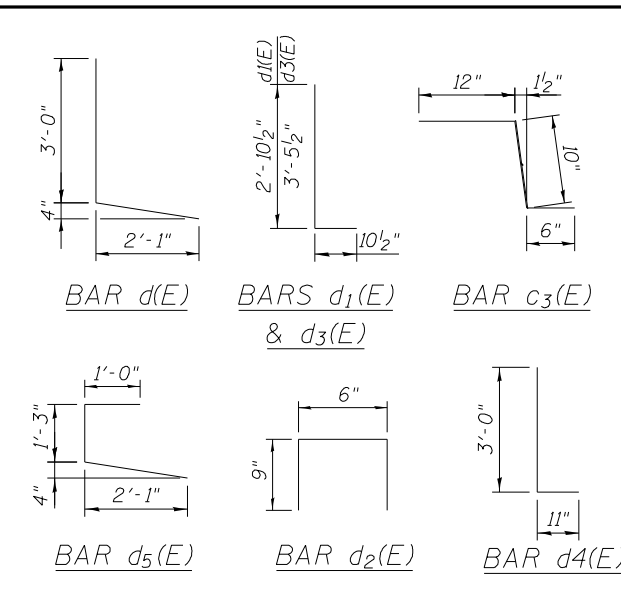
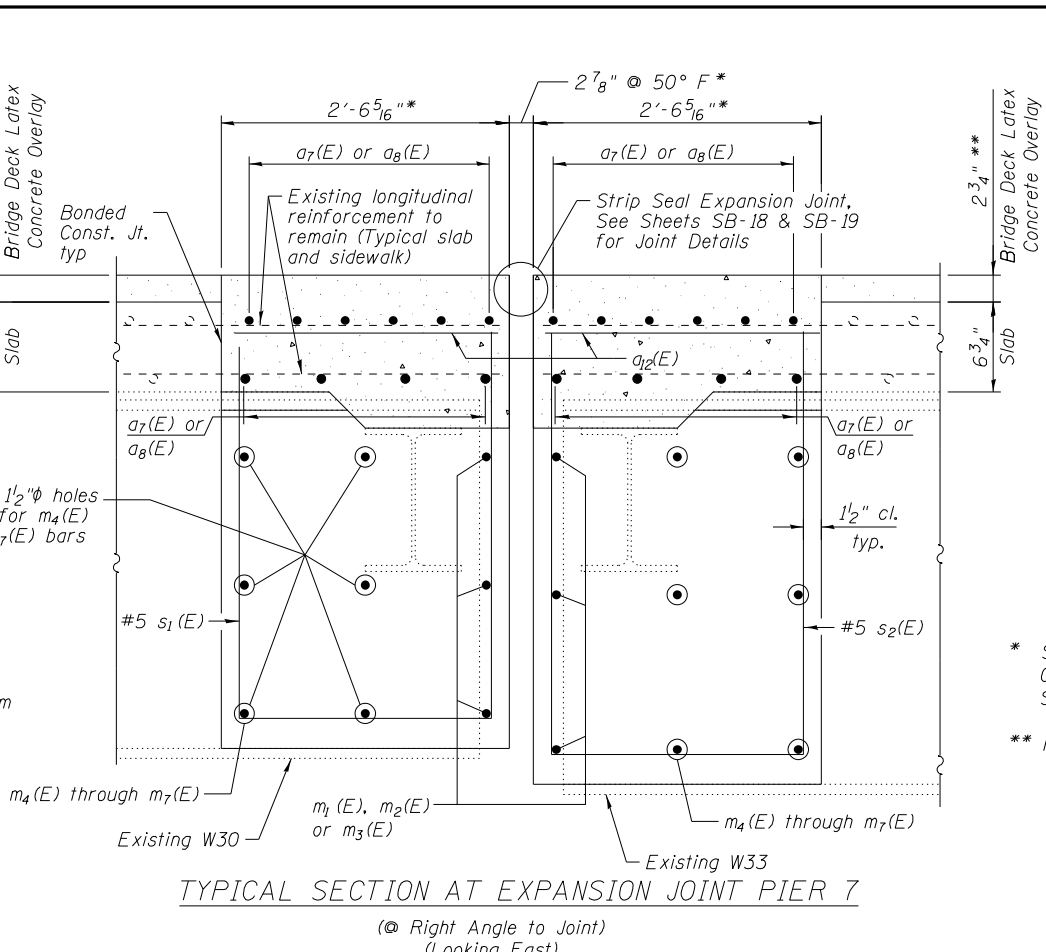
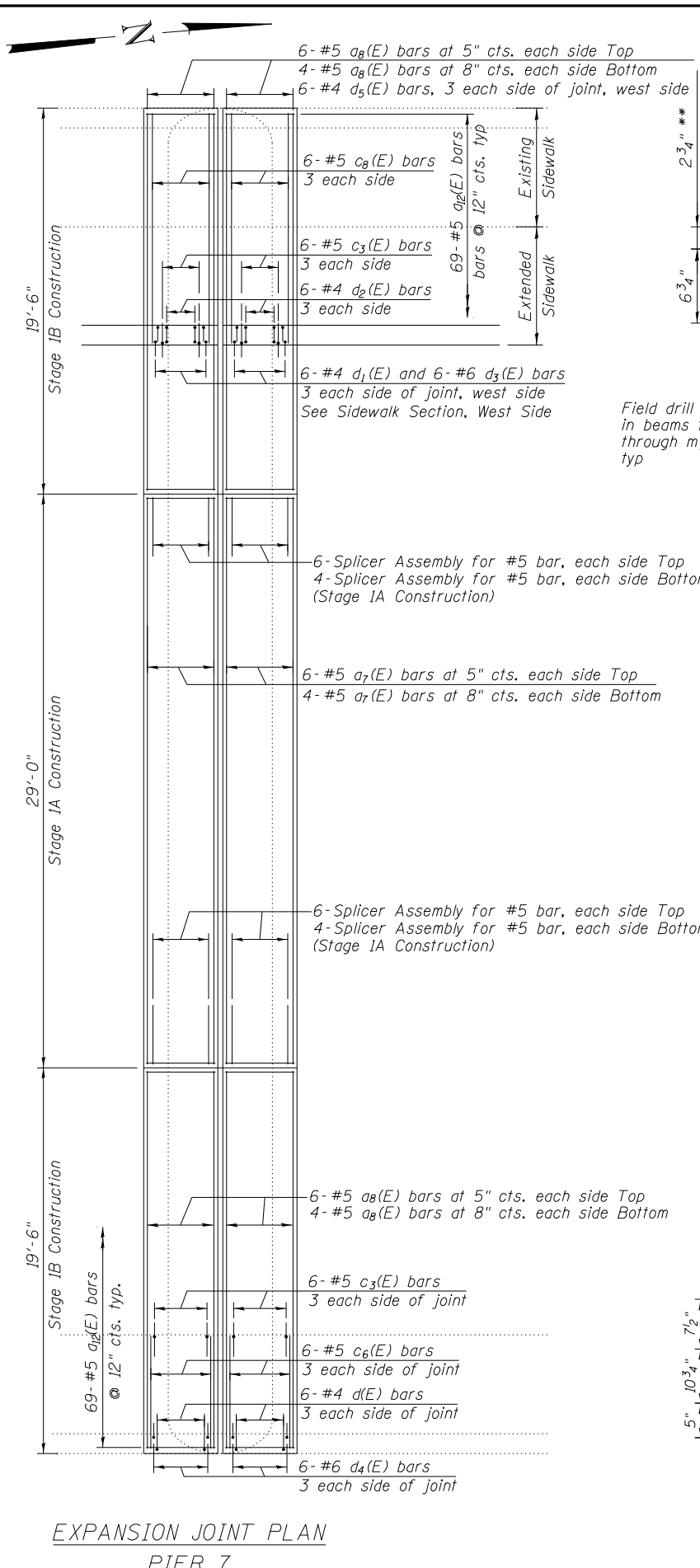
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT REPLACEMENT AT PIER 2
STRUCTURE NO. 016-0194

SHEET NO. SB-14 OF SB-30 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	74
CONTRACT NO. 62W48				

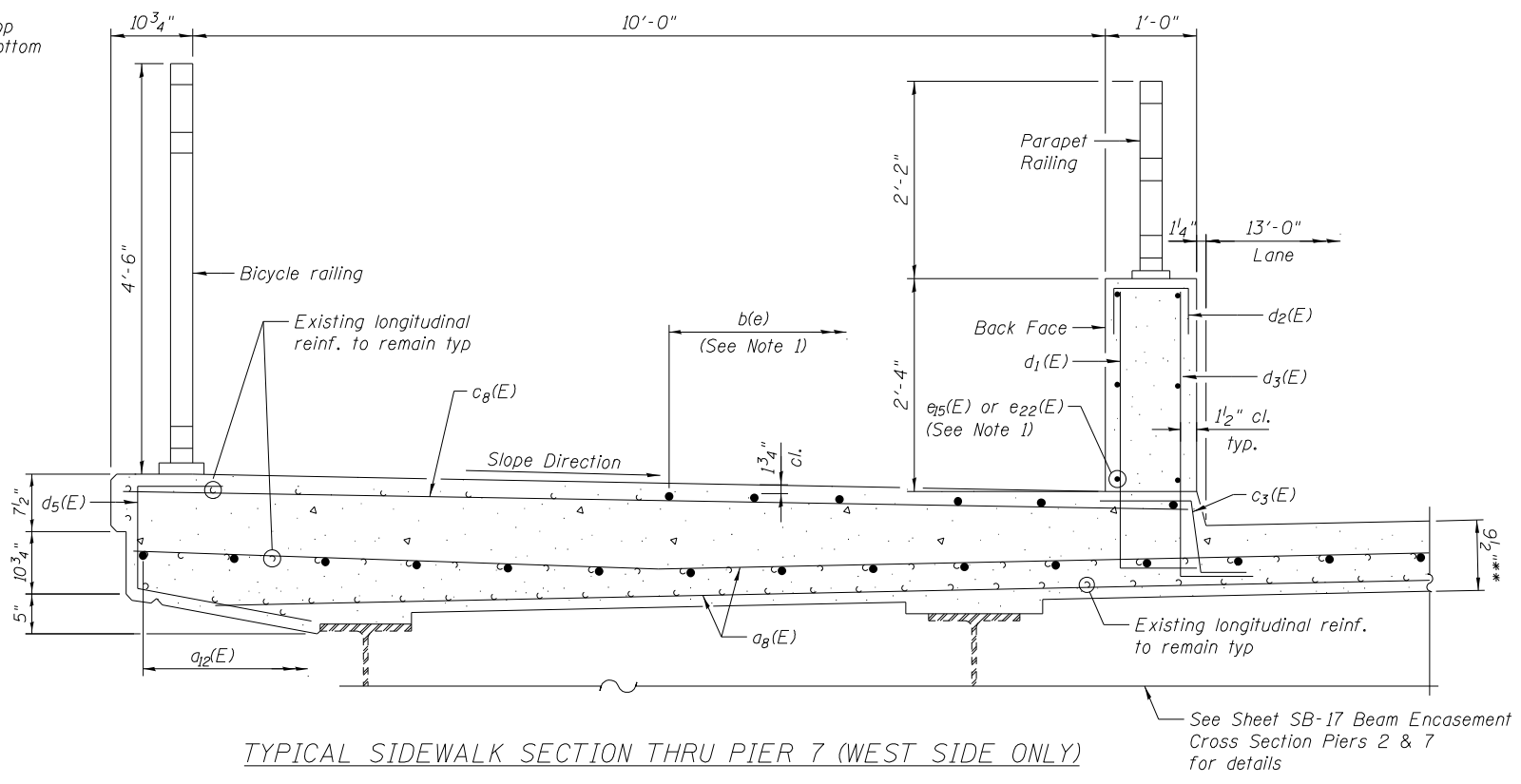
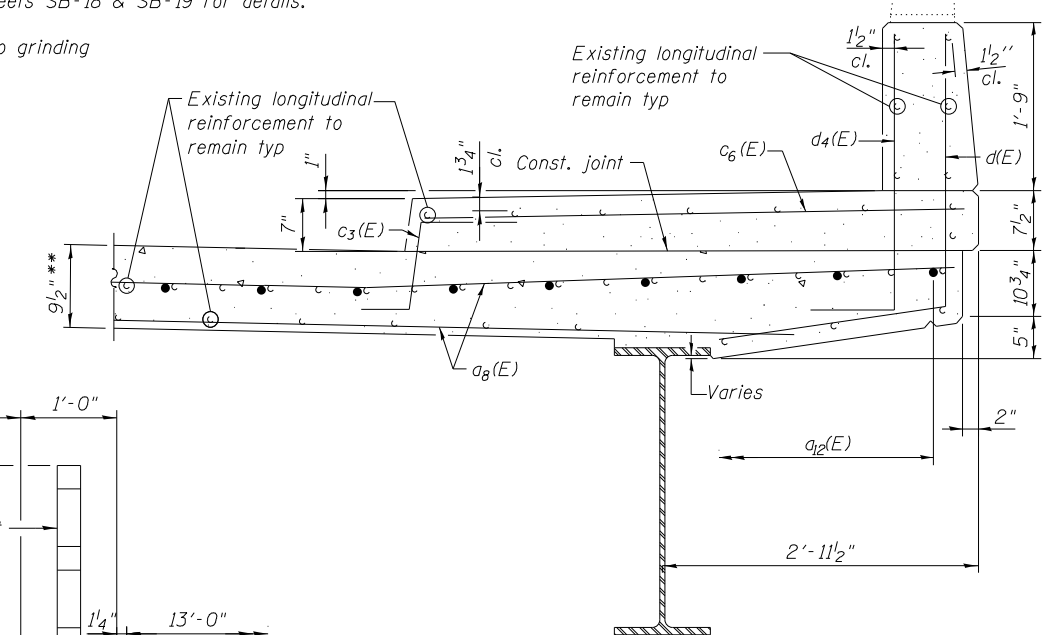
ILLINOIS FED. AID PROJECT



BILL OF MATERIAL				
Bar	No.	Size	Length	Shape
a7(E)	20	#5	28'-9"	—
a8(E)	40	#5	19'-2"	—
a2(E)	138	#5	2'-3"	—
c3(E)	12	#5	2'-4"	—
c6(E)	6	#5	5'-9"	—
c8(E)	6	#5	11'-6"	—
d(E)	6	#4	5'-1"	—
d1(E)	6	#4	3'-9"	—
d2(E)	6	#4	2'-0"	—
d3(E)	6	#6	4'-4"	—
d4(E)	6	#6	3'-11"	—
d5(E)	6	#4	4'-4"	—
Concrete Superstructure			Cu. Yd.	14.1
Reinforcement Bars, Epoxy Coated			Pound	2,000

* Showing dimension for rolled rail joint. Contractor shall adjust for welded rail joint. See Sheets SB-18 & SB-19 for details.

** Prior to grinding



TYPICAL SIDEWALK SECTION (EAST SIDE ONLY) (Match Adjacent)

NOTES

1. Bars b(e), e15(E), e22(E) and parapet concrete billed with superstructure. See Sheet SB-6 for bar location in sidewalk and parapet and Sheet SB-8 for rebar and concrete quantity.
2. See Sheet SB-17 for beam encasement cross section Piers 2 & 7 details and bar bill.
3. For details of Strip Seal Expansion Joints, see Sheets SB-18 & SB-19.
4. Existing longitudinal reinforcement in east parapet shall remain in place.
5. See Sheet SB-30 for details of bar splicers.

EXPANSION JOINT PLAN PIER 7

TYPICAL SIDEWALK SECTION THRU PIER 7 (WEST SIDE ONLY)

See Sheet SB-17 Beam Encasement Cross Section Piers 2 & 7 for details



USER NAME = ken.moy
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 PLOT SCALE = N.T.S.
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 PLOT DATE = 4/2/2024

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT REPLACEMENT AT PIER 7
 STRUCTURE NO. 016-0194

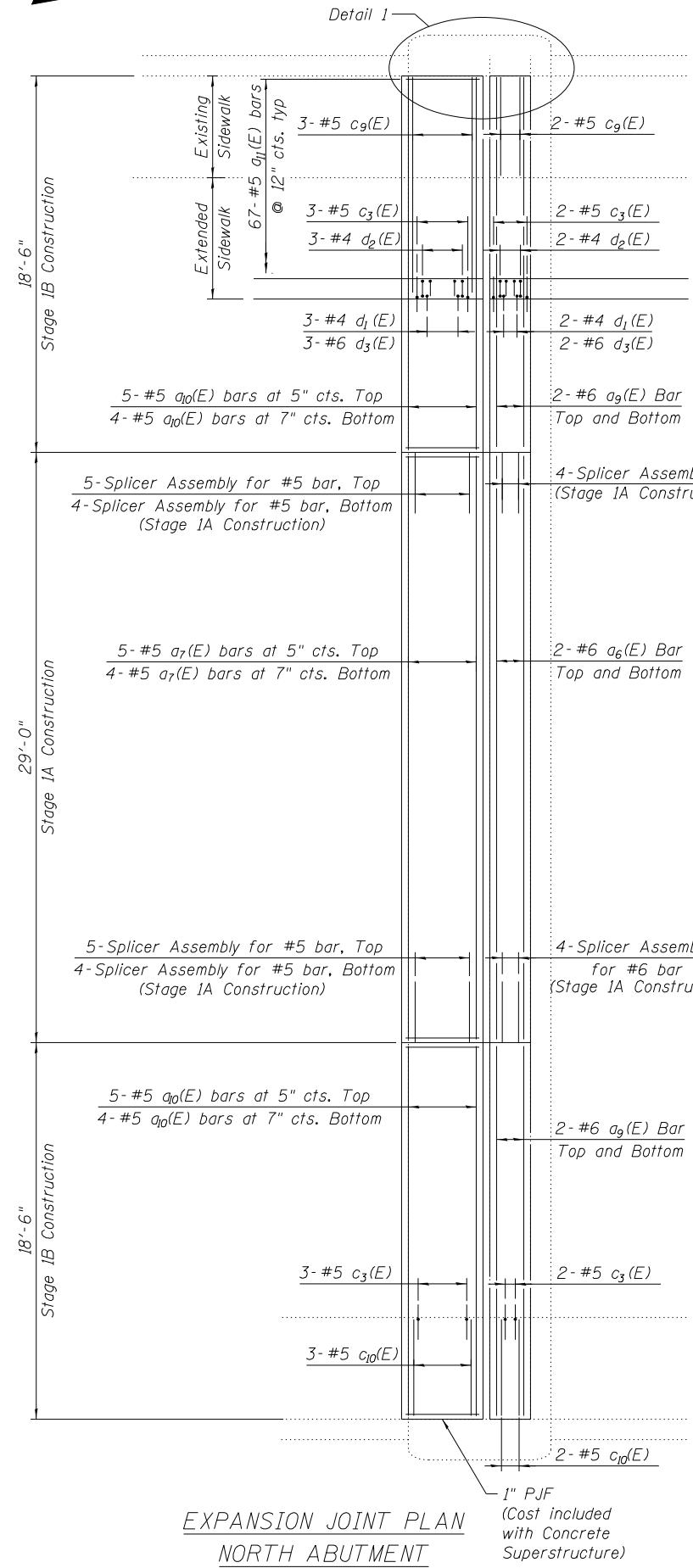
SHEET NO. SB-15 OF SB-30 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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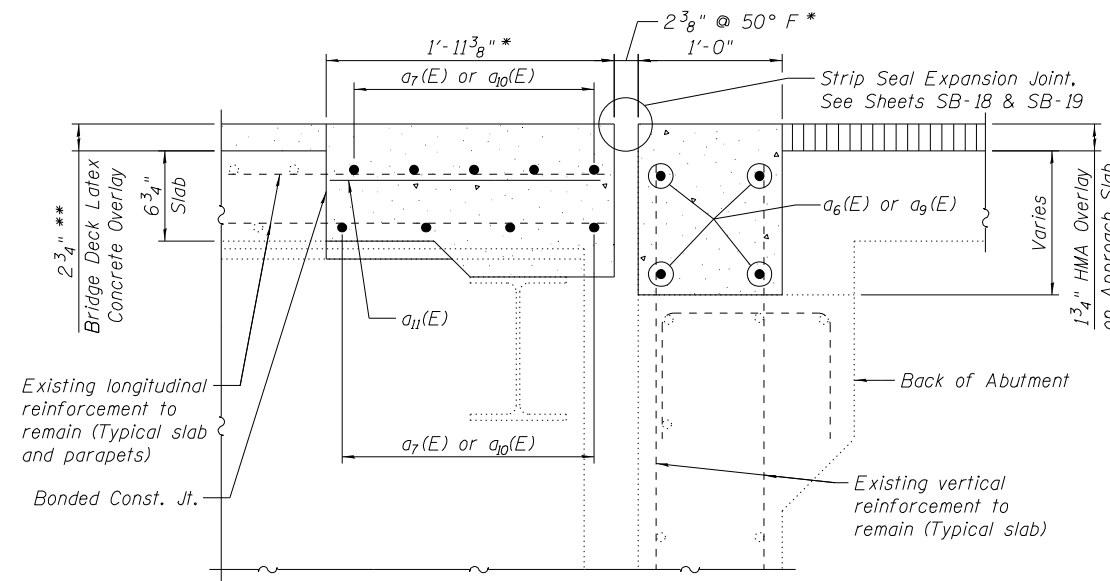
CONTRACT NO. 62W48

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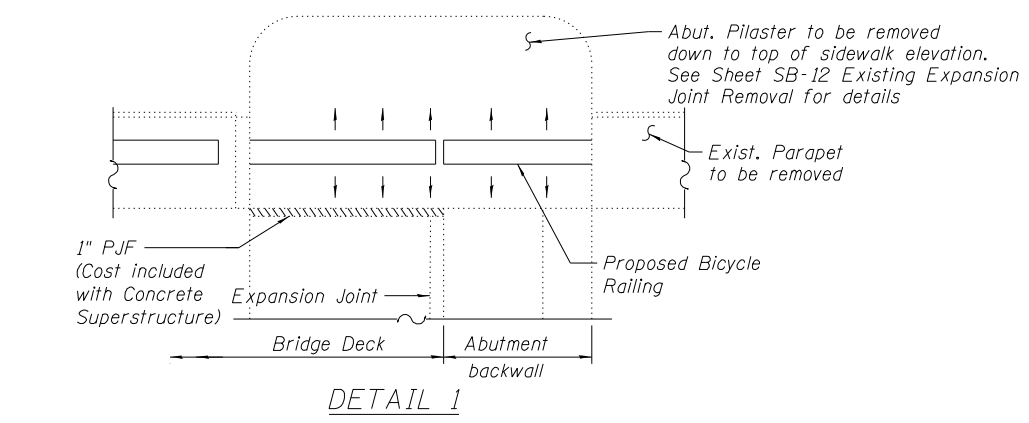
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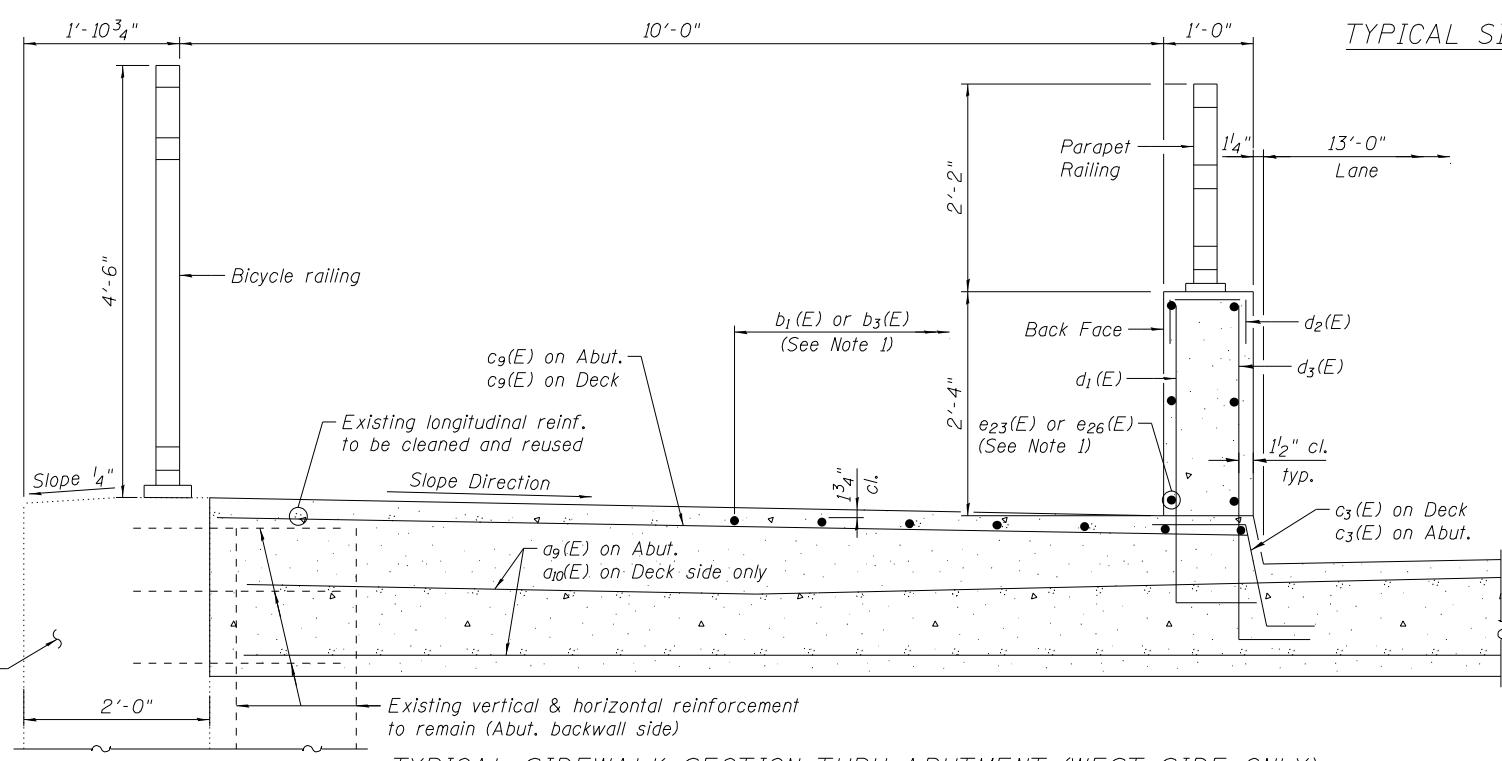
EXPANSION JOINT PLAN
NORTH ABUTMENT



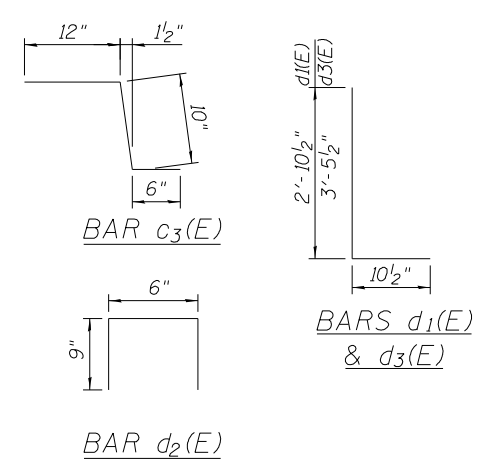
TYPICAL SECTION AT EXPANSION JOINT NORTH ABUTMENT



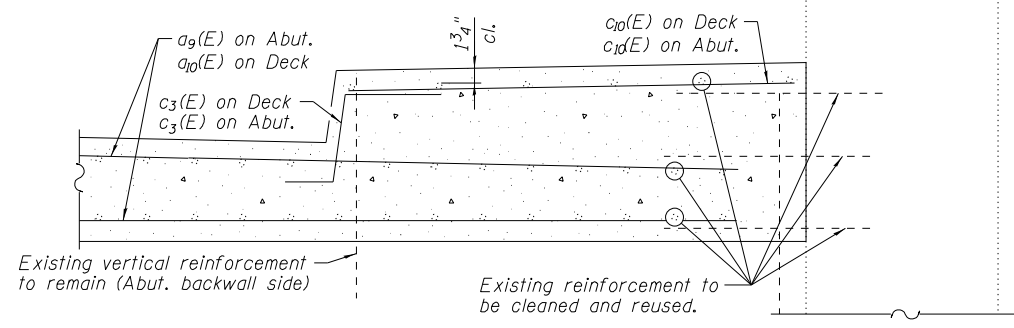
DETAIL 1



TYPICAL SIDEWALK SECTION THRU ABUTMENT (WEST SIDE ONLY)



* Showing dimension for rolled rail joint. Contractor shall adjust for welded rail joint. See Sheets SB-18 & SB-19 for details.
** Prior to grinding



TYPICAL SIDEWALK SECTION THRU ABUTMENT
(Match adjacent)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
a6(E)	4	#6	28'-9"	—	
a7(E)	9	#5	28'-9"	—	
a9(E)	8	#6	18'-2"	—	
a10(E)	18	#5	18'-2"	—	
a11(E)	67	#5	1'-9"	—	
c3(E)	10	#5	2'-4"	L	
c9(E)	5	#5	10'-7"	—	
c10(E)	5	#5	4'-8"	—	
d1(E)	5	#4	3'-9"	L	
d2(E)	5	#4	2'-0"	—	
d3(E)	5	#6	4'-4"	L	
Concrete Superstructure				Cu. Yd.	10.3
Reinforcement Bars, Epoxy Coated				Pound	1,280

NOTES

1. Bars b1(e), b3(E), e23(E), e26(E) and parapet concrete billed with superstructure. See Sheet SB-6 for bar location in sidewalk and parapet and Sheet SB-8 for rebar and concrete quantity.
2. For details of Strip Seal Expansion Joints, see Sheets SB-18 & SB-19.
3. Existing longitudinal reinforcement in east parapet shall remain in place.
4. See Sheet SB-30 for details of bar splicers.

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PLOT SCALE = N.T.S.	CHECKED - MCC	REVISIONS -
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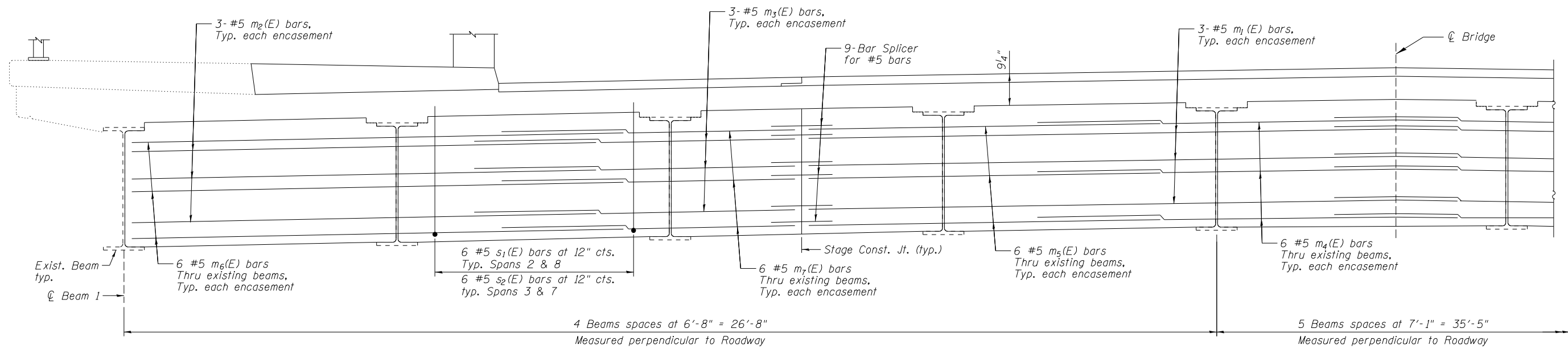
EXPANSION JOINT REPLACEMENT AT NORTH ABUTMENT
STRUCTURE NO. 016-0194

SHEET NO. SB-16 OF SB-30 SHEETS

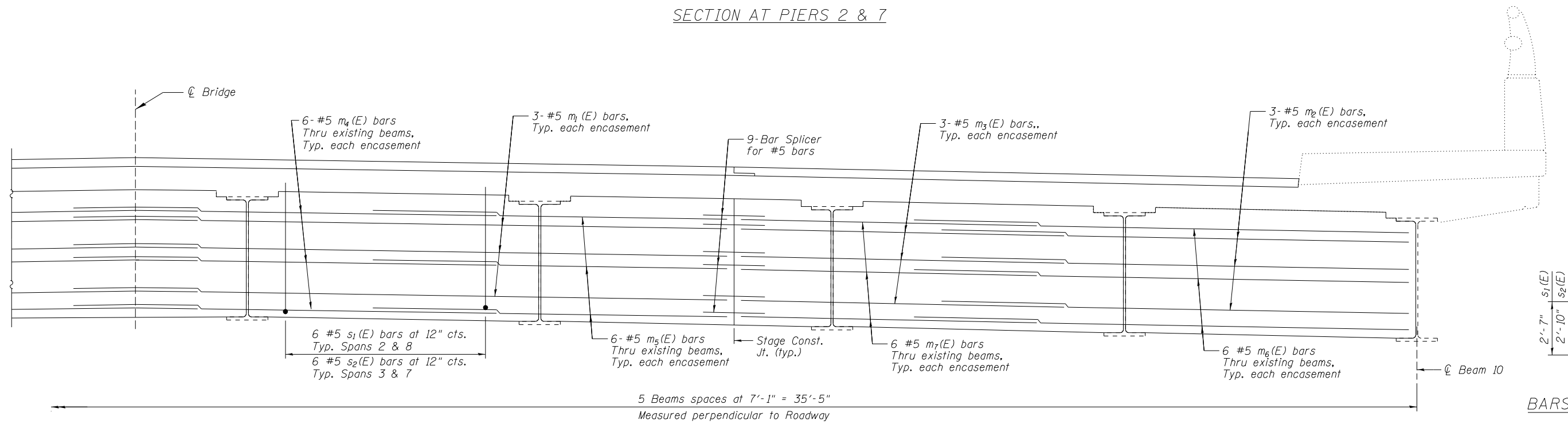
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3730	15-00131-01-BR	COOK	109	76
CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT

MODEL: 41001.DWG
 FILE: 41001.DWG
 USER: ken.moy
 PROJECT: 15-0194-01-17
 SHEET: SB-17
 DATE: 4/2/2024



SECTION AT PIERS 2 & 7



SECTION AT PIERS 2 & 7

BARS s1(E) & s2(E)

MINIMUM BAR LAP
 #5 bar = 3'-0"

NOTES:
 See Sheets SB-3 & SB-4 for Stage Construction Details.
 See Sheets SB-14 and SB-15 for typical section thru expansion joint and beam encasement.
 Clean beam ends of all loose rust and debris prior to pouring encasement concrete.

**SUPERSTRUCTURE
 BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
m1(E)	24	#5	16'-2"	—
m2(E)	24	#5	11'-7"	—
m3(E)	24	#5	8'-0"	—
m4(E)	48	#5	10'-6"	—
m5(E)	48	#5	8'-10"	—
m6(E)	48	#5	12'-3"	—
m7(E)	48	#5	7'-4"	—
s1(E)	108	#5	7'-5"	□
s2(E)	108	#5	7'-11"	□
Concrete Superstructure			Cu. Yd.	59.3
Reinforcement Bars, Epoxy Coated			Pound	4,580



USER NAME = ken.moy
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 PLOT DATE = 4/2/2024

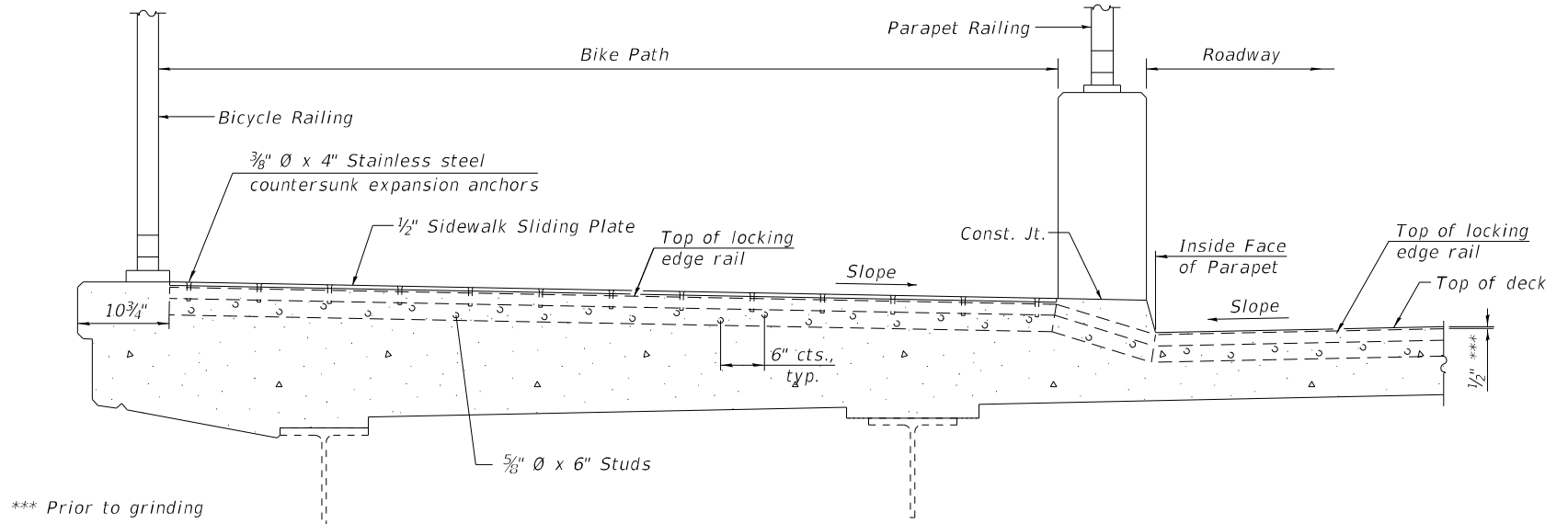
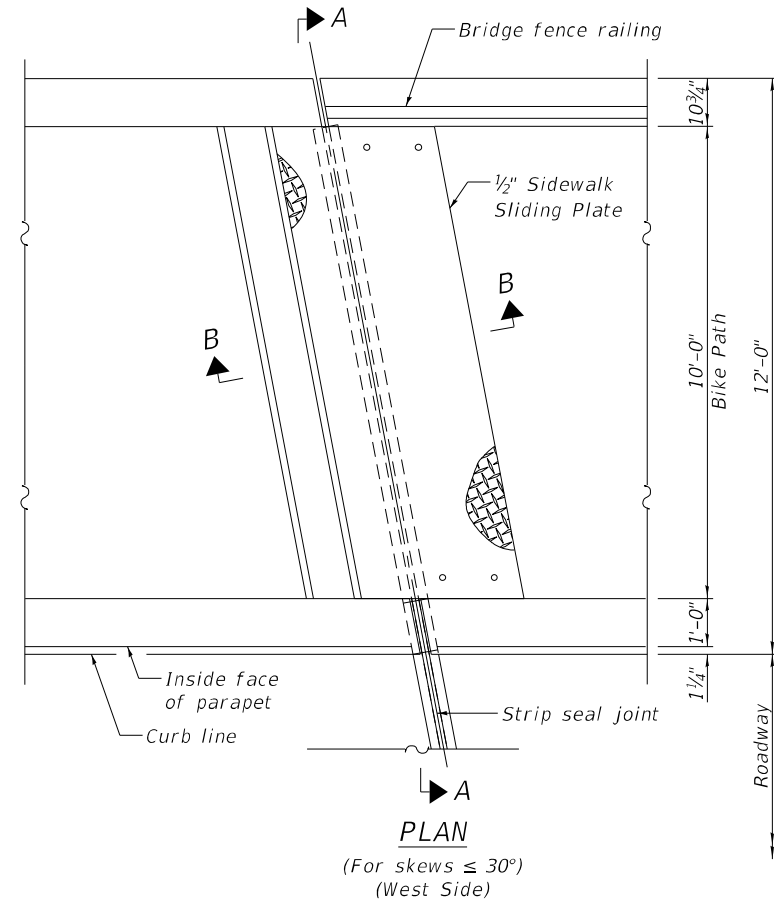
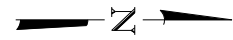
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

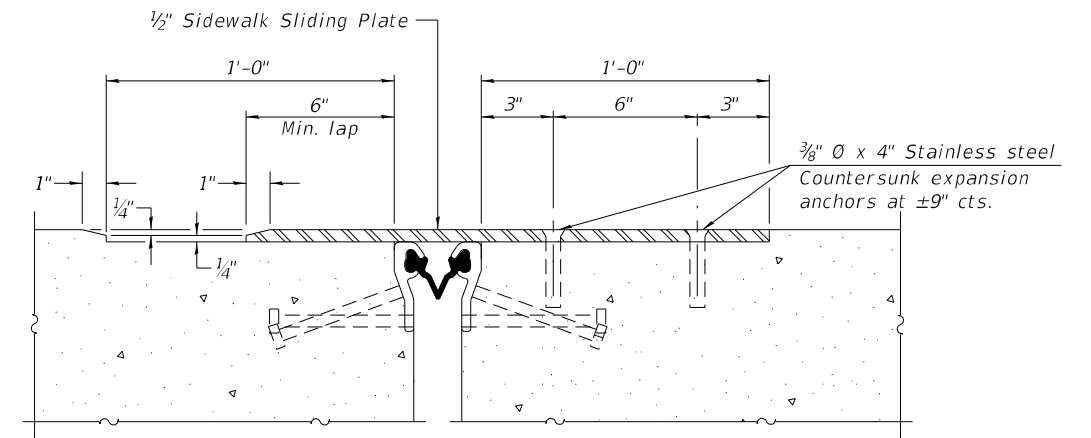
**BEAM ENCASUREMENT CROSS SECTION PIERS 2 & 7
 STRUCTURE NO. 016-0194**

SHEET NO. SB-17 OF SB-30 SHEETS

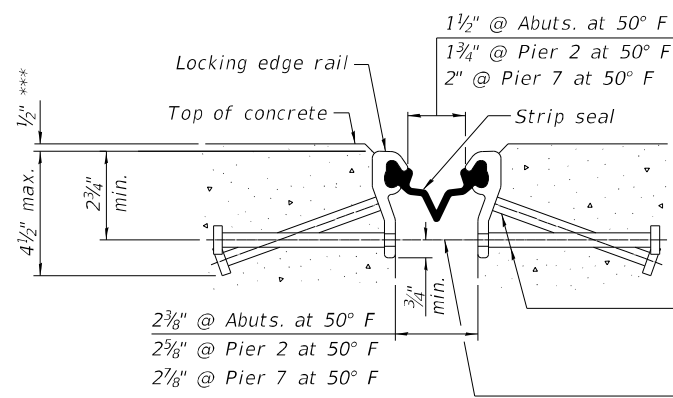
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CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



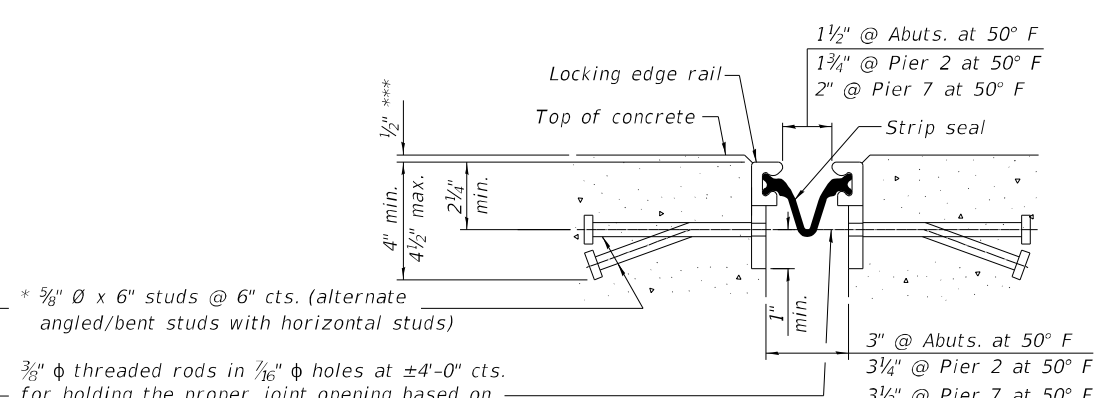
SECTION A-A
(West Side)



SECTION B-B



SHOWING ROLLED RAIL JOINT

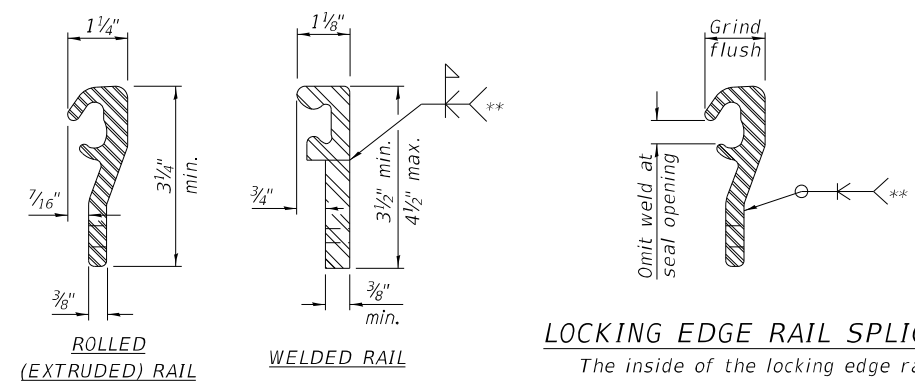


SHOWING WELDED RAIL JOINT

* 5/8" ϕ x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)
3/8" ϕ threaded rods in 1/16" ϕ holes at ± 4 -0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.

LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	270

(Sheet 1 of 2)



USER NAME = ken.moy	DESIGNED - MCC	REVISED -
PLOT SCALE = N.T.S.	CHECKED - ATB	REVISED -
PLOT DATE = 4/2/2024	DRAWN - MCC	REVISED -
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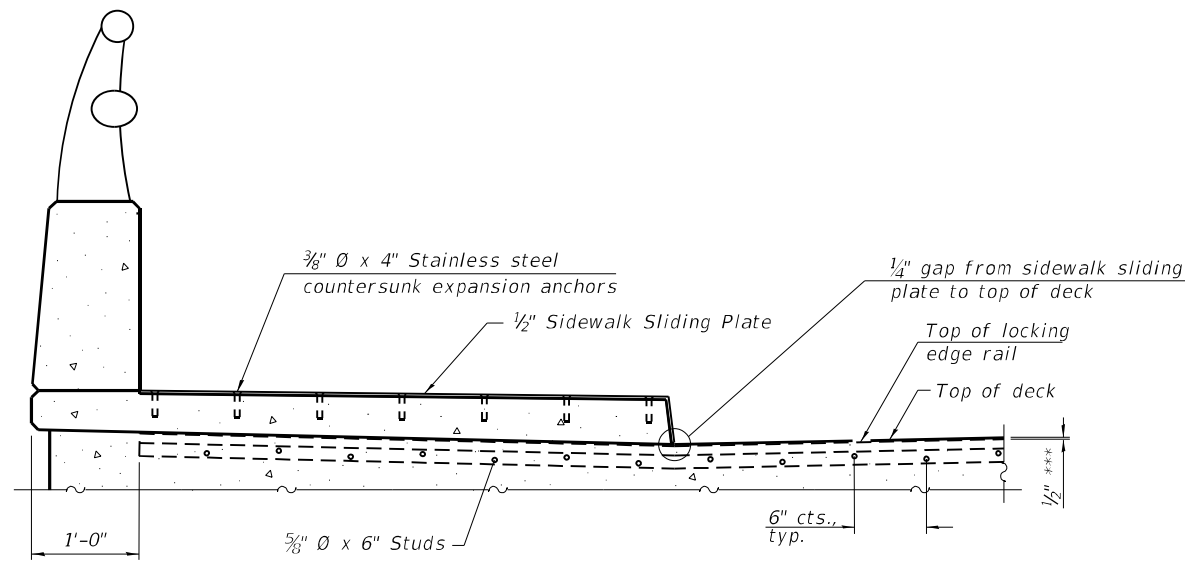
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 016-0194**

SHEET NO. 5B-18 OF 5B-30 SHEETS

FAU. RTL.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	78
				CONTRACT NO. 62W48

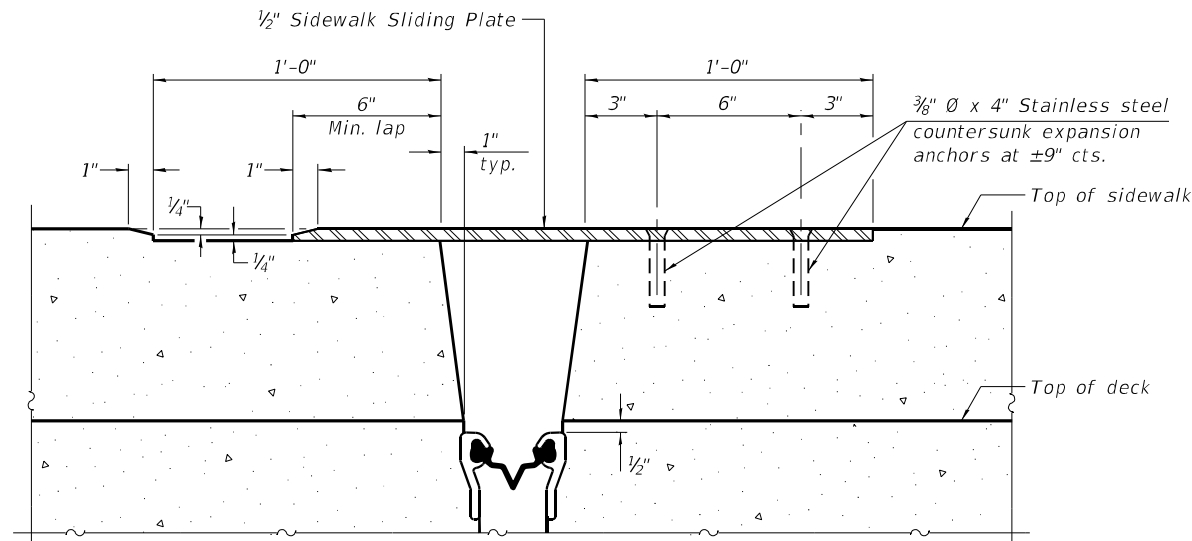
ILLINOIS FED. AID PROJECT



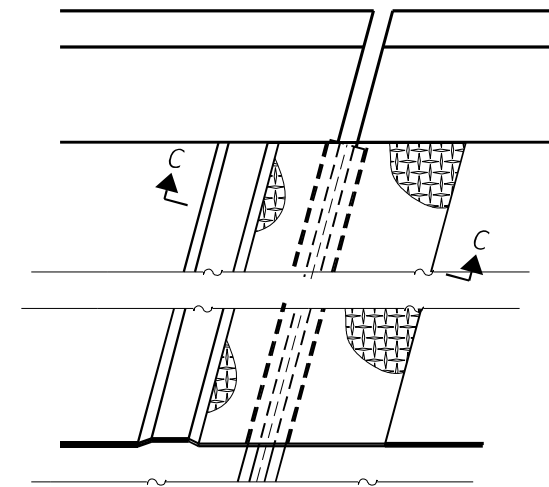
SECTION AT RAISED SIDEWALK - EAST SIDE

(Looking South)

*** Prior to grinding

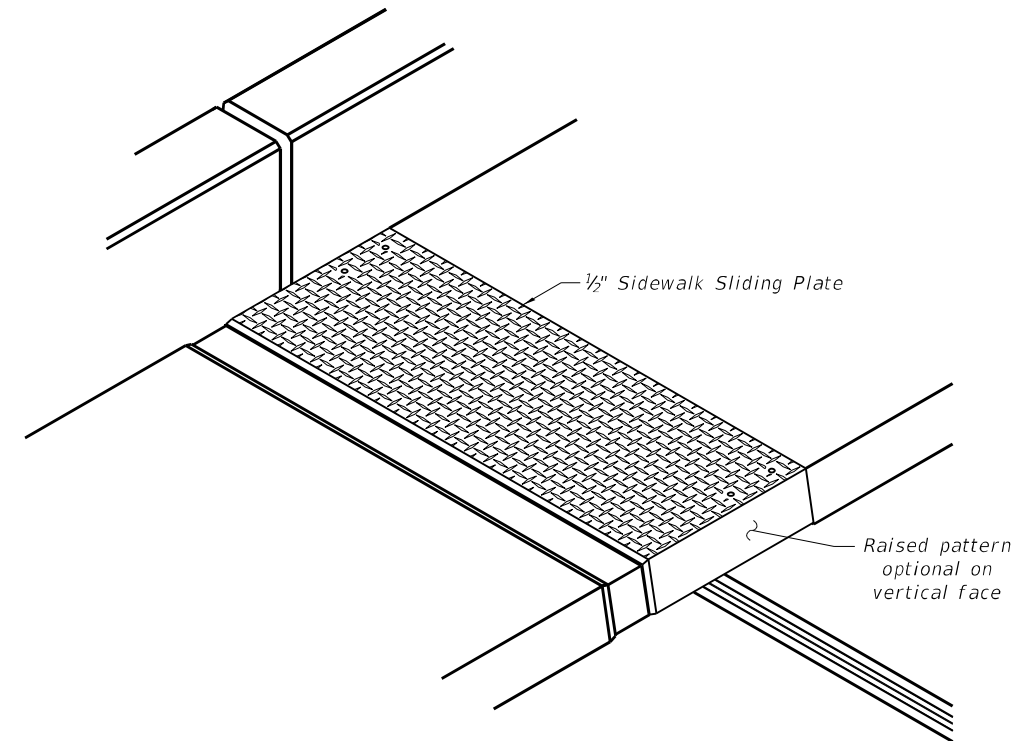


SECTION C-C



PLAN AT RAISED SIDEWALK

(For skews $\leq 30^\circ$)



TRIMETRIC VIEW

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of $\frac{1}{4}$ ". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the $4\frac{1}{2}$ " maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be $\frac{3}{16}$ " and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.

Cost of sidewalk sliding plates, embedded plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required.

(Sheet 2 of 2)

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FILE: 4400161.dwg
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CADD: 10. CADD03_SHEETS-Structure-900_CADD10_CADD03_SHEETS-Structure-900_CADD10.19 Joint_Details.dwg



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

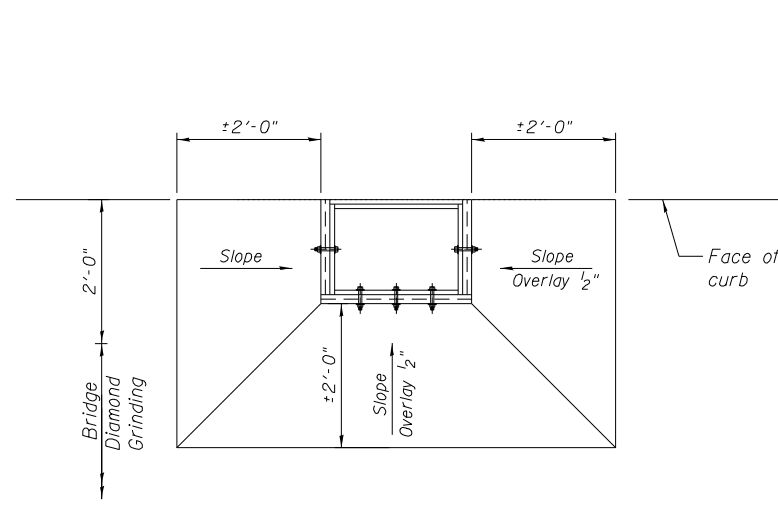
PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 016-0194

SHEET NO. 5B-19 OF 5B-30 SHEETS

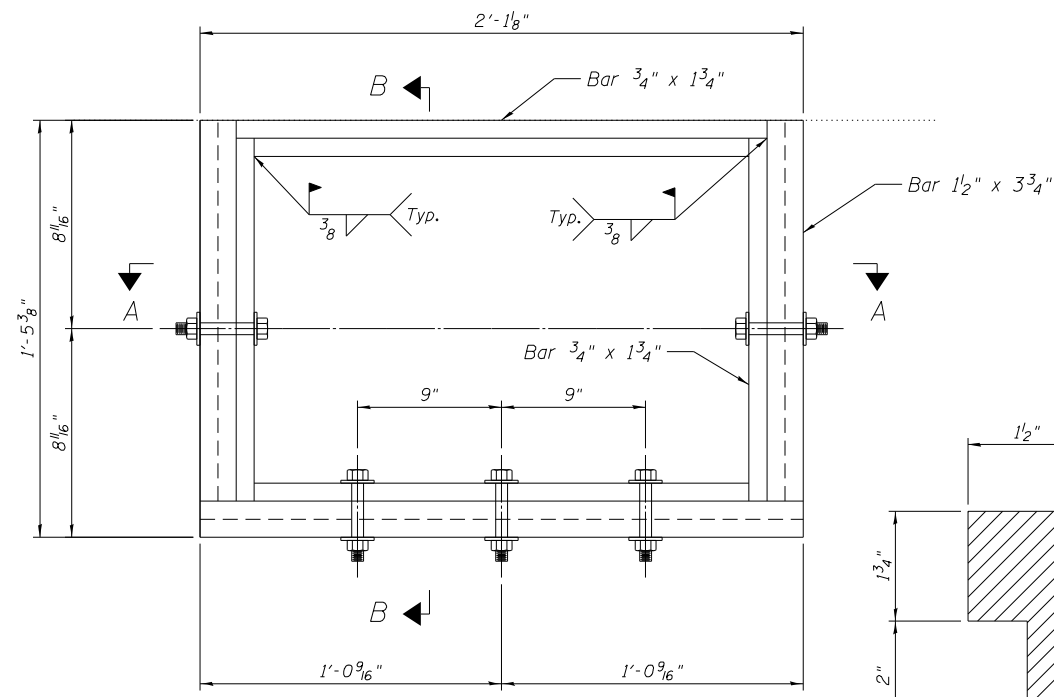
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CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

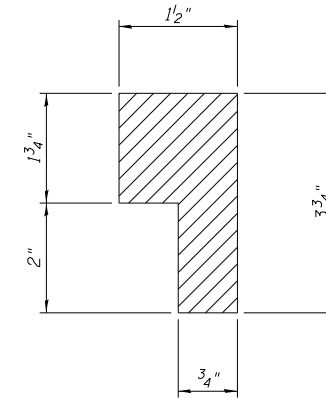
All structural steel shall conform to AASHTO Classification M-270 Gr. 36.
 The adjusting scupper ring shall be galvanized.
 Bolts shall be 1/2"φ, AASHTO M164 Type I, mechanically galvanized.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 The contractor shall ensure that no damage is done to existing grates to be reused.
 Shop plans for proposed adjusting scupper ring shall be submitted for approval prior to fabrication.
 Cost of all labor and materials necessary to remove existing grates, clean existing scuppers, install adjusting scupper rings and reinstalling grates is included in the cost per unit each for Drainage Scuppers To Be Adjusted.
 Existing Scuppers are located near Piers 2 and 7 at east Sidewalk.
 See Sheet SB-22 for details of downspout extension.



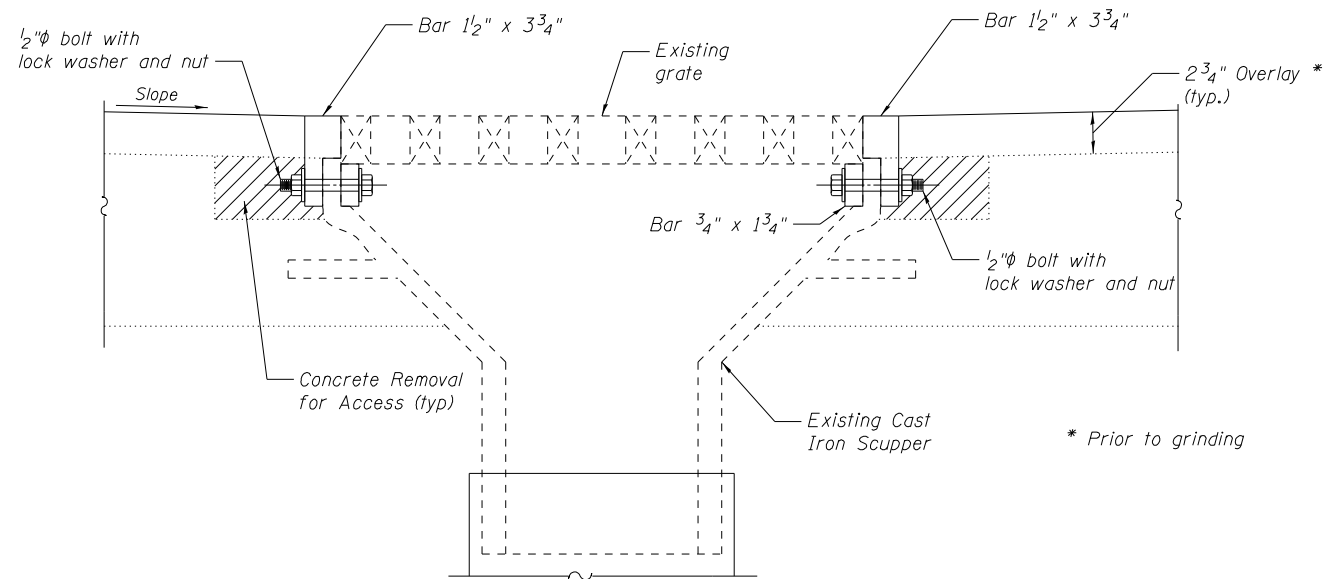
SLOPING PLAN



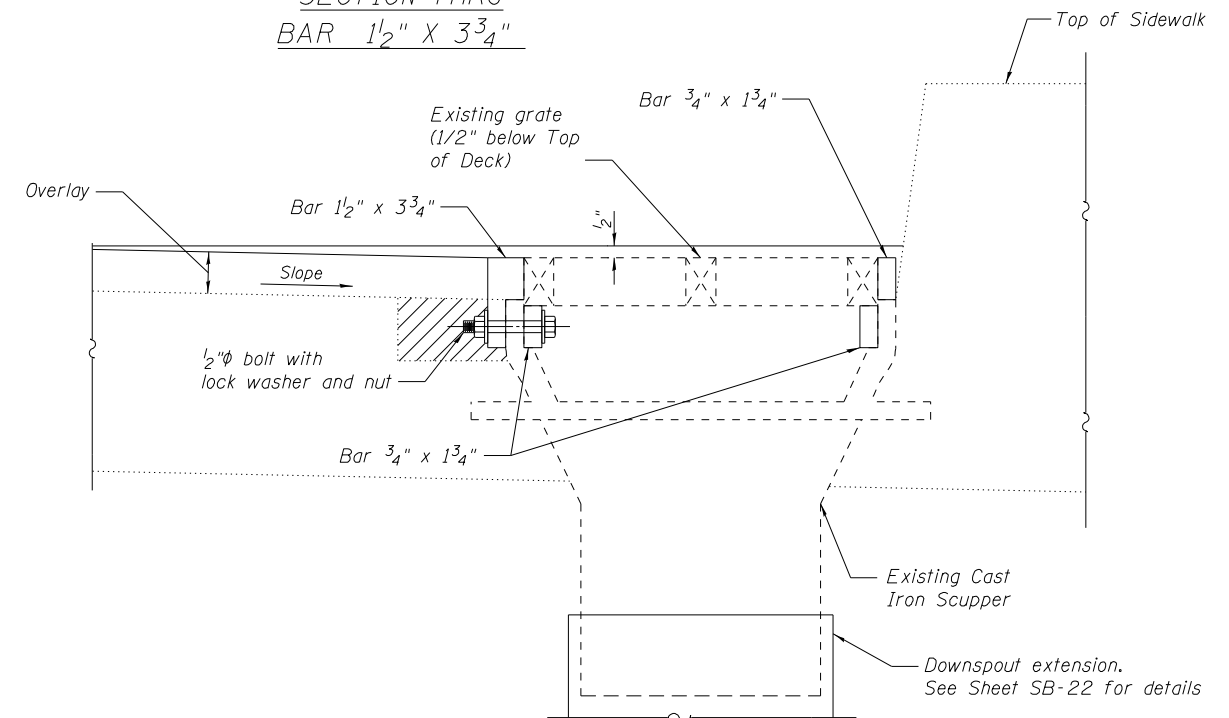
PLAN



SECTION THRU BAR 1 1/2" X 3 3/4"



SECTION A-A



SECTION B-B

BILL OF MATERIAL

Item	Unit	Total
Drainage Scuppers To Be Adjusted	Each	2

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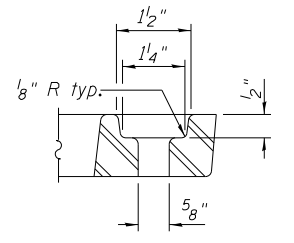
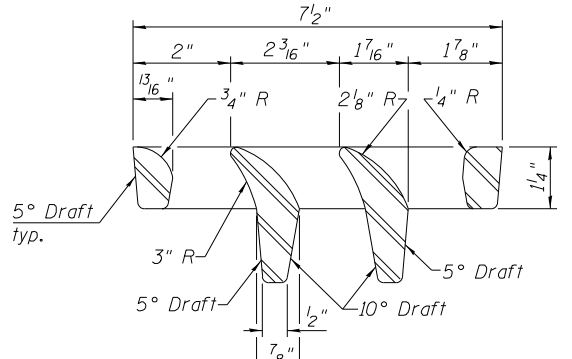
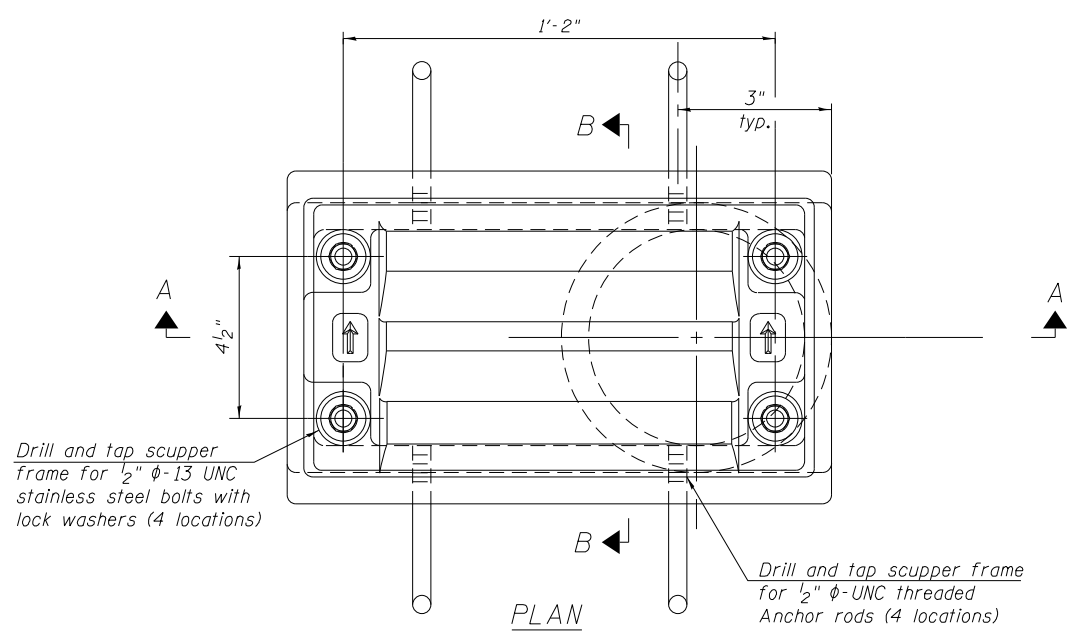
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADJUSTING EXISTING DRAINAGE SCUPPER
 STRUCTURE NO. 016-0194**

SHEET NO. 5B-20 OF 5B-30 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	80
CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT

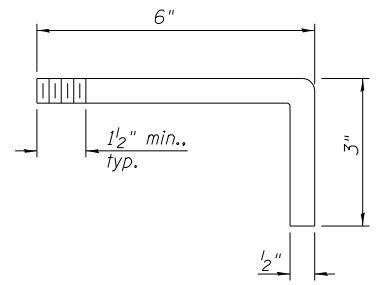
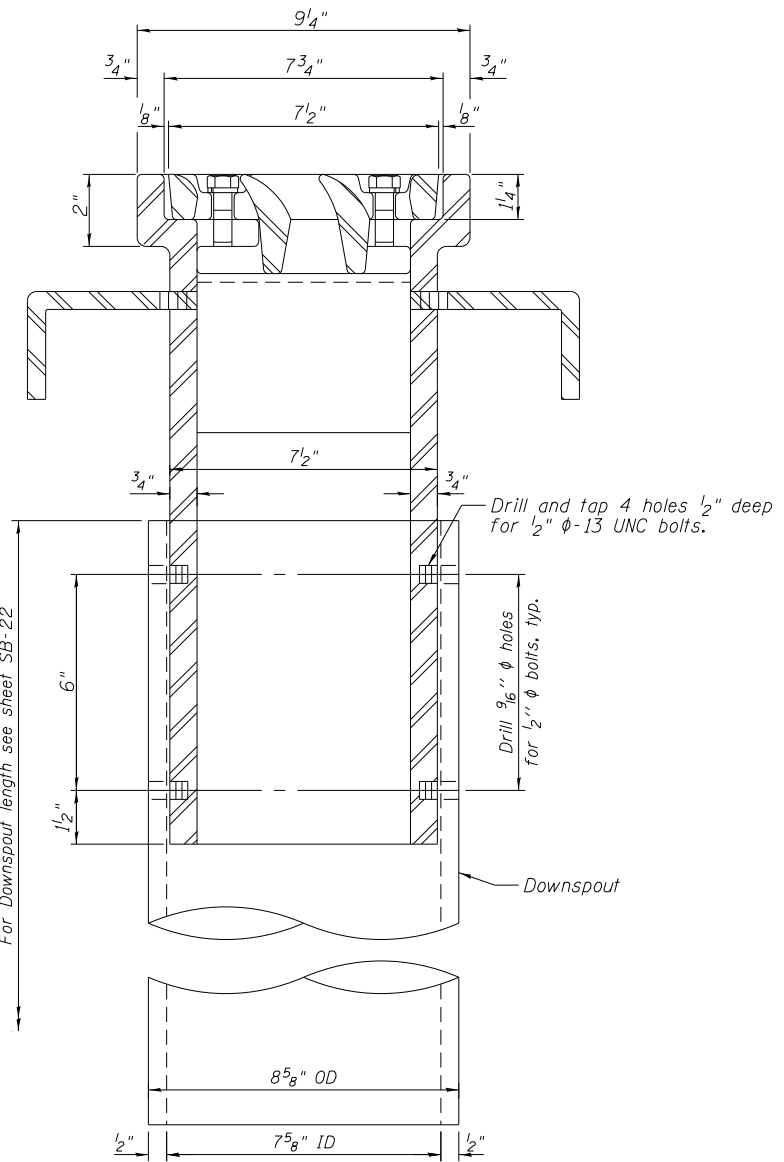
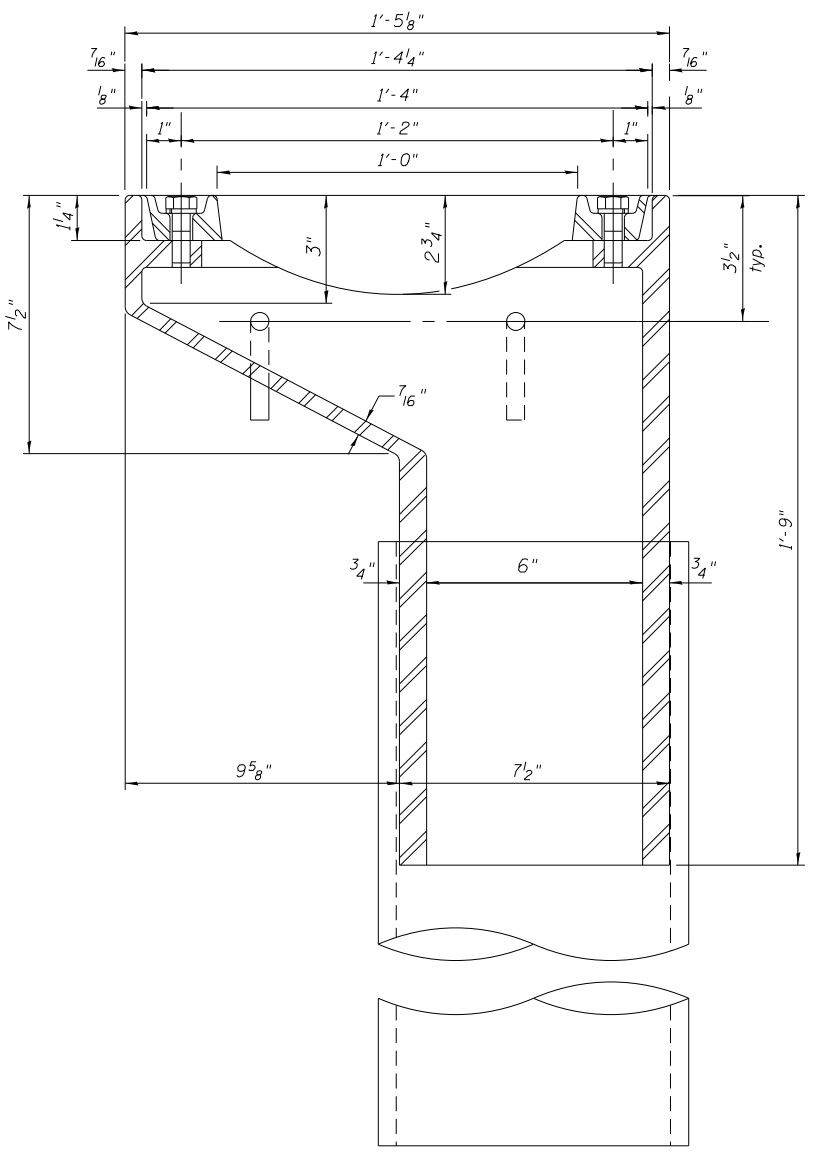


Drill and tap scupper frame for 1/2" φ-13 UNC stainless steel bolts with lock washers (4 locations)

Drill and tap scupper frame for 1/2" φ-UNC threaded Anchor rods (4 locations)

VANE GRATE DETAIL

BOLT HOLE DETAIL



SECTION A-A
See sheet SB-10 for scupper location relative to parapet.

SECTION B-B

ANCHOR ROD DETAIL

Notes:
 All cast iron parts shall be gray iron conforming to the requirements of AASHTO M105, Class 35B and AASHTO M306.
 Bolts, anchor rods, nuts and washers shall be according to ASTM A307 and shall be galvanized according to AASHTO M232. As an alternate, stainless steel may be used.
 Stainless steel hardware shall be according to Article 1006.29(d) of the Standard Specifications.
 Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frames and downspouts; however, the scupper grates shall remain cast iron. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval.
 Structural steel scupper frames and downspouts, when utilized, shall be galvanized according to AASHTO M111.
 As an alternate, fiberglass may be used for downspouts according to ASTM D2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. in lieu of the cast iron or structural steel.
 Exterior surfaces of downspouts and exterior exposed surface of the scupper frame below deck shall be painted to match the finish coat of the exterior steel beams.
 The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.
 Cost of the grate, frame, downspout, anchor rods, nuts and washers including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-11.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scuppers, DS-11	Each	4

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DS-11 (MODIFIED) 1-1-2020



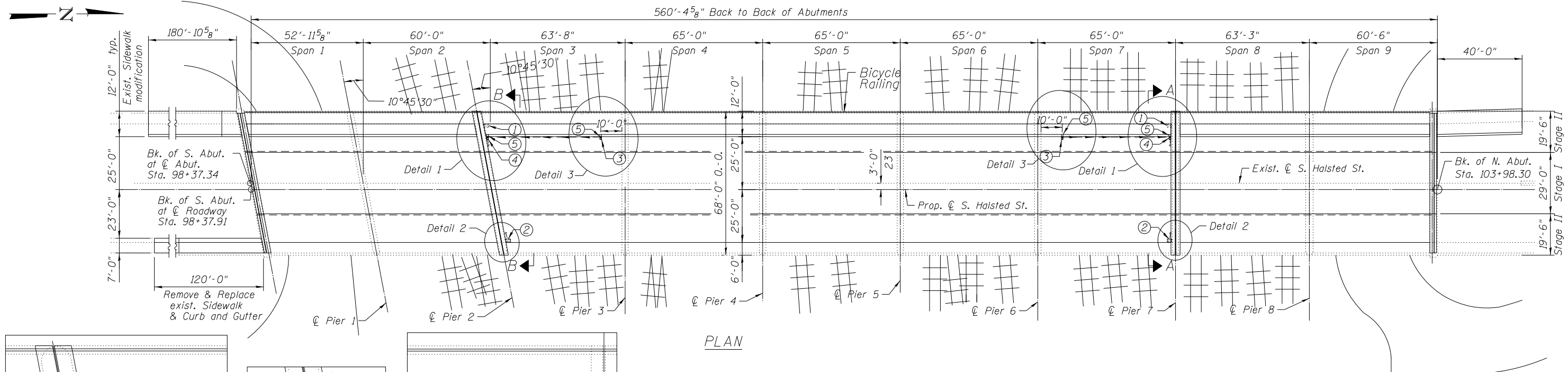
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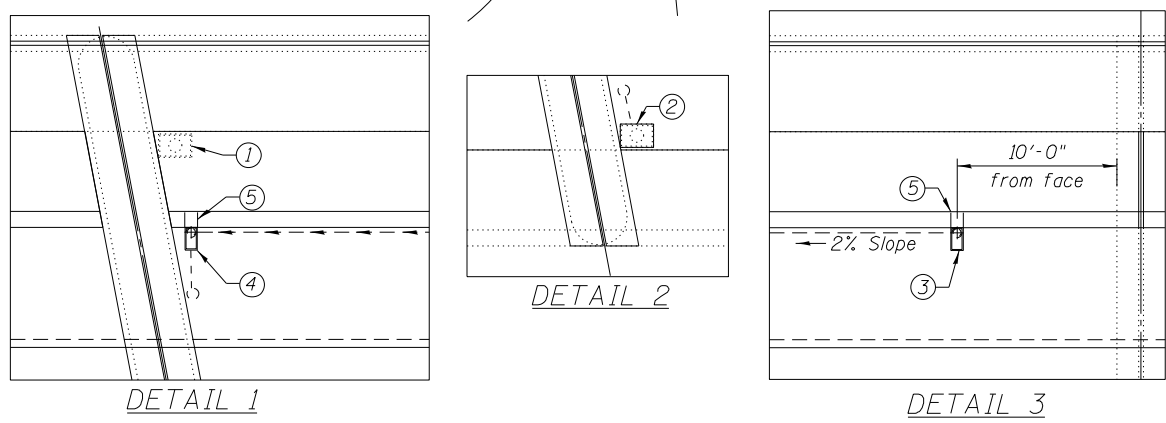
DRAINAGE SCUPPER, DS-11
STRUCTURE NO. 016-0194

SHEET NO. SB-21 OF SB-30 SHEETS

F.A.U. RTE. 3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 81
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				

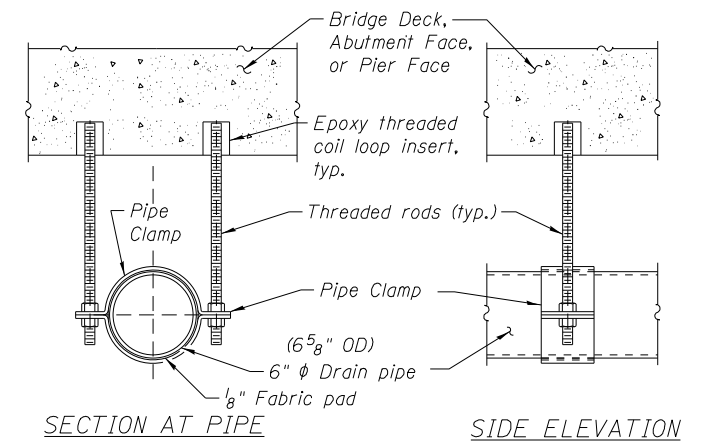
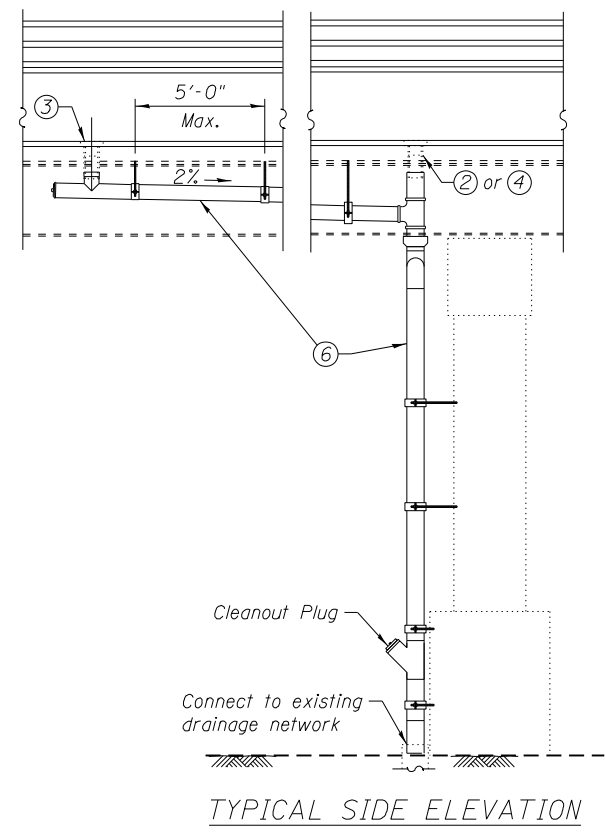
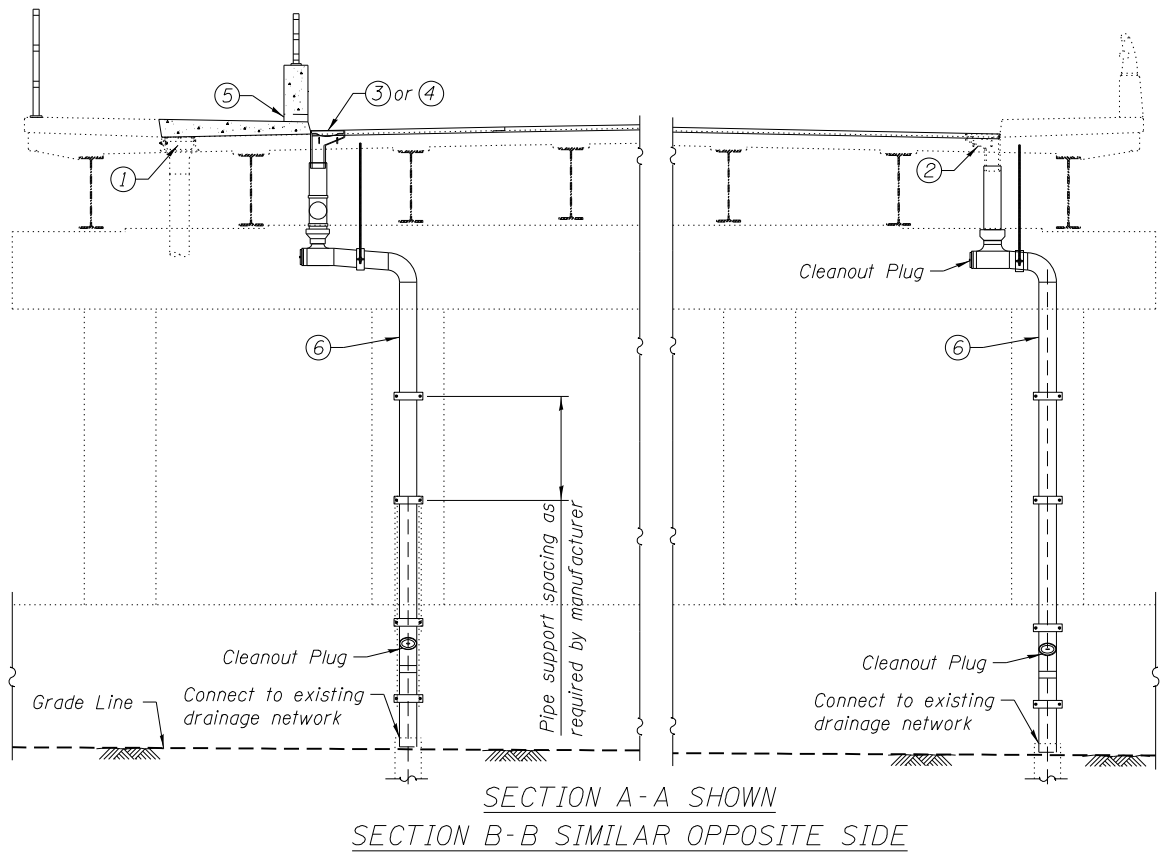


PLAN



LEGEND

- ① Cut downspout to the bottom of deck, Provide steel retainer plate & fill existing scupper with concrete. West side (Typ.) See Sheet SB-10 for additional Section View.
- ② Adjust and reuse Existing Scupper
- ③ New Scupper DS-11 placed into deck 10'-0" from face of pier. See Sheet SB-10 for Full Depth Deck Slab Repair Details.
- ④ New Scupper DS-11 at Piers 2 & 7 West Side. See Sheet SB-21 for drainage scupper details.
- ⑤ Parapet Drains formed by placing 9" x 3" block-out at sidewalk level at each scupper location. See Sheets SB-8 and SB-10 for additional details.
- ⑥ Remove and install new bridge drainage system at Pier 2 and Pier 7. Cost shall be included with Bridge Drainage System.



PIPE SUPPORT DETAIL

BILL OF MATERIAL

Item	Unit	Total
Bridge Drainage System	L. Sum	0.5

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 303 EAST WACKER DRIVE, SUITE 1400 CHICAGO, IL 60601-5276 PHONE: (312) 373-7700 FAX: (312) 373-6800



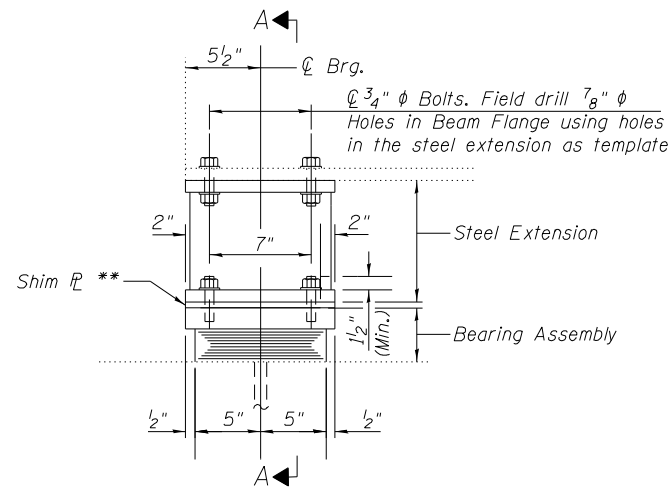
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STATE OF ILLINOIS
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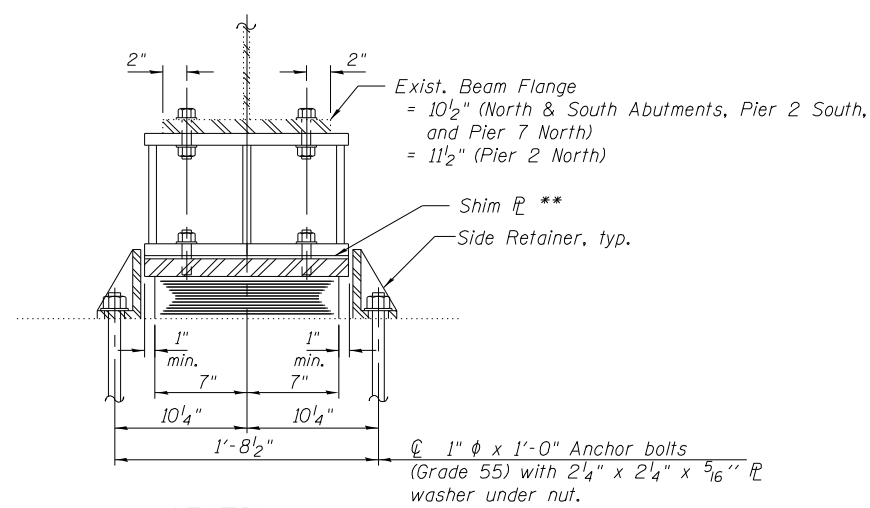
DRAINAGE SYSTEM
STRUCTURE NO. 016-0194

SHEET NO. SB-22 OF SB-30 SHEETS

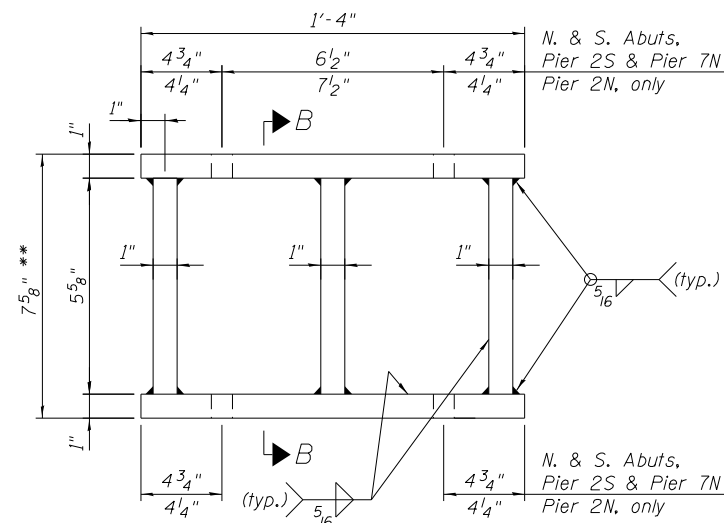
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CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



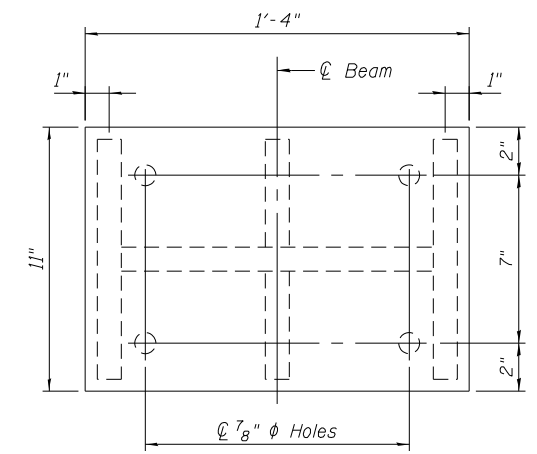
ELEVATION AT ABUT.



SECTION A-A



ELEVATION STEEL EXTENSION
(Looking North or South)

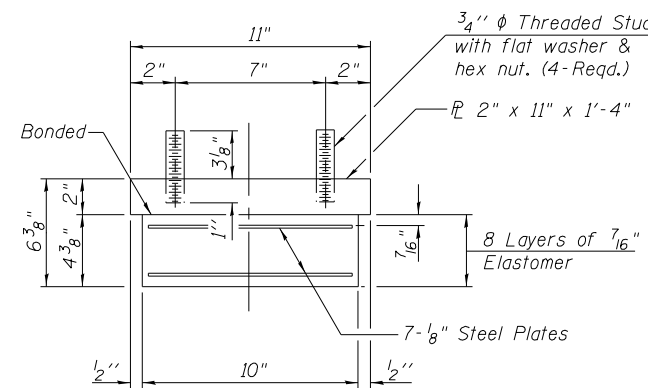


PLAN STEEL EXTENSION

TYPE I ELASTOMERIC EXP. BRG.

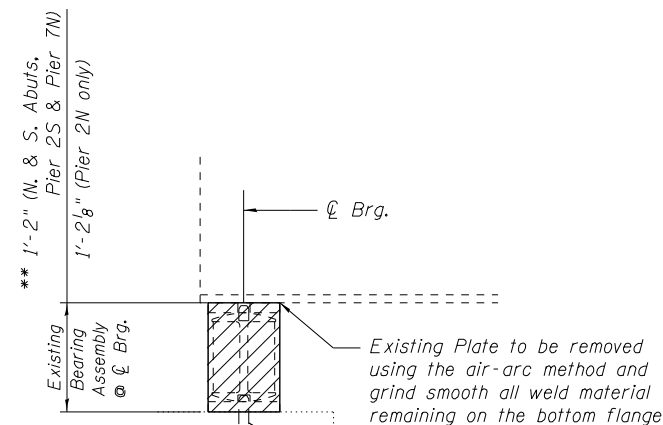
North & South Abutments, Pier 2 South,
Pier 2 North, and Pier 7 North

** Provide one additional 3/8" shim plate for beam line 6 at North Abutment, and Pier 7 North.



BEARING ASSEMBLY

Note:
Shim plates shall not be placed under Bearing Assembly.



EXISTING BEARING
REMOVAL DETAIL

Note:
Prior to jacking, the Contractor shall verify that hold-down rods installed at North and South abutments during construction have been removed according to original details.

Notes:

Existing bearing height (from top of the bearing seat to bottom of girder flange) information obtained from available existing plans is used to compute proposed steel extension height. Prior to ordering any material, the Contractor shall verify in the field all bearing heights and shim thickness dimensions.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Cost of side retainers and stainless steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.

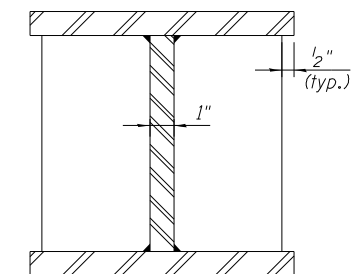
Two 1/8" in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details. Cost included with Elastomeric Bearing Assembly, Type I.

The structural steel plates of the Bearing Assembly shall conform to the requirements of AASHTO M 270 Grade 50.

Existing bearing at beam line 6 at Pier 7 North and N. Abut. has additional 3/8" shim plate. Cost of this shim plate is included in Furnishing and Erecting Structural Steel.

New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.

Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.

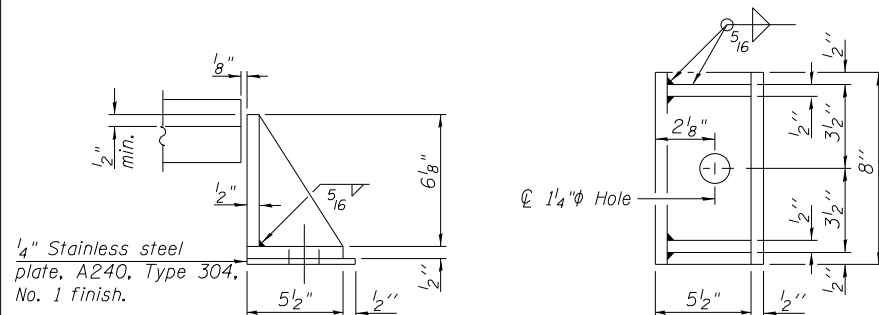


SECTION B-B

REACTION TABLE (KIPS)

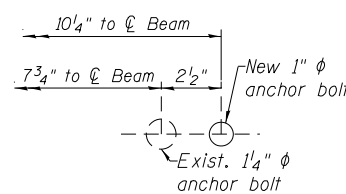
Type	S. Abut.	Pier 2S	Pier 2N	Pier 7N	N. Abut.
R DL	53.3	62.0	65.7	65.4	60.2
R LL	40.7	43.0	43.2	43.3	42.4
R IMP	11.6	11.6	11.5	11.6	11.6
R Total	105.6	116.6	120.4	120.3	114.2

Note:
Minimum Jack Capacity 75 Tons.



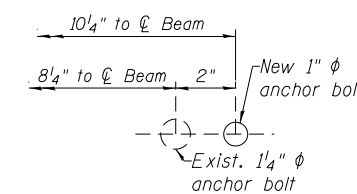
SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



LOCATIONS OF ANCHOR BOLTS

North & South Abutments,
Pier 2 South and Pier 7 North



LOCATIONS OF ANCHOR BOLTS

Pier 2 North, only

(Sheet 1 of 2)

**TYPE I BEARING DETAILS
STRUCTURE NO. 016-0194**

SHEET NO. SB-23 OF SB-30 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W48				

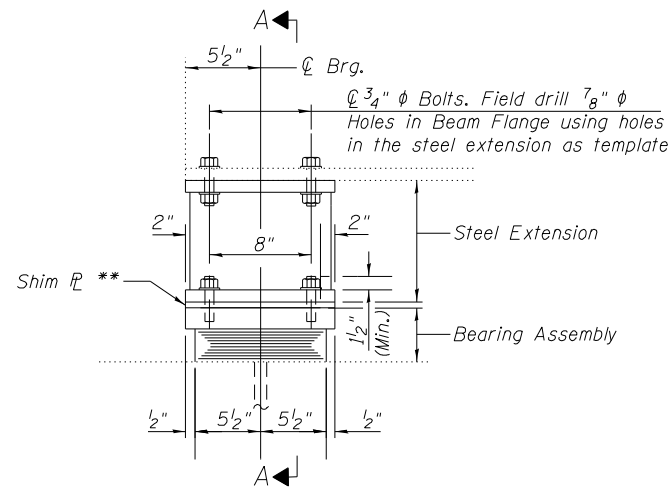
ILLINOIS FED. AID PROJECT

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

USER NAME	DESIGNED	REVISIONS
ken_joy	MCC	-
	ATB	-
	MCC	-
	ATB	-



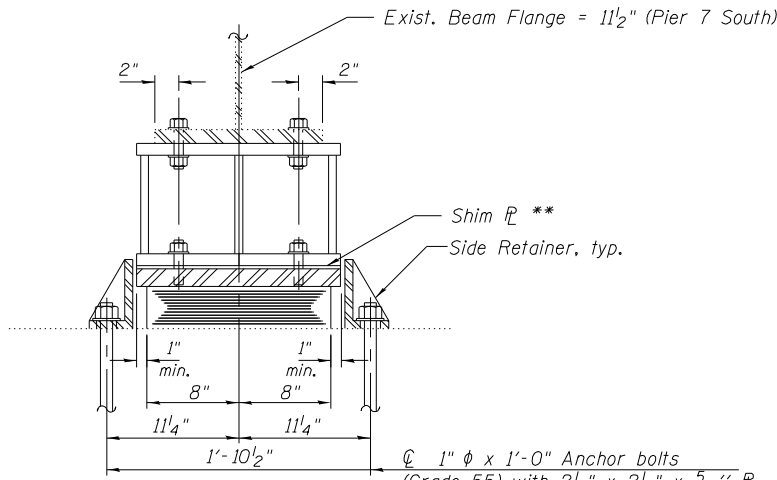
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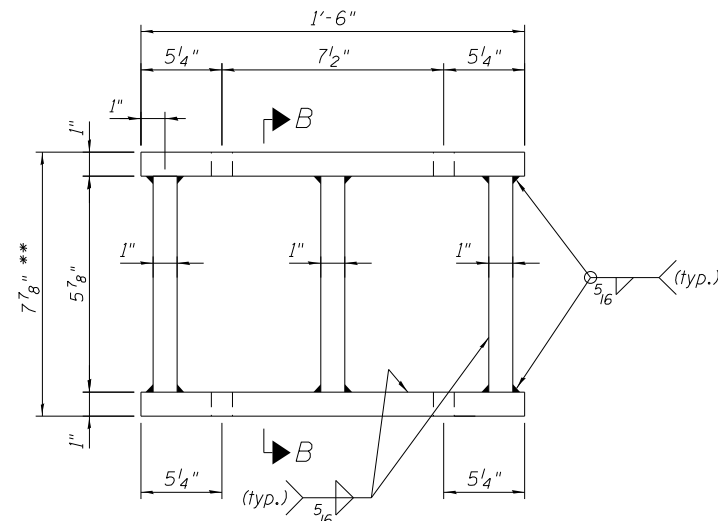
ELEVATION AT ABUT.

TYPE I ELASTOMERIC EXP. BRG.
Pier 7 South

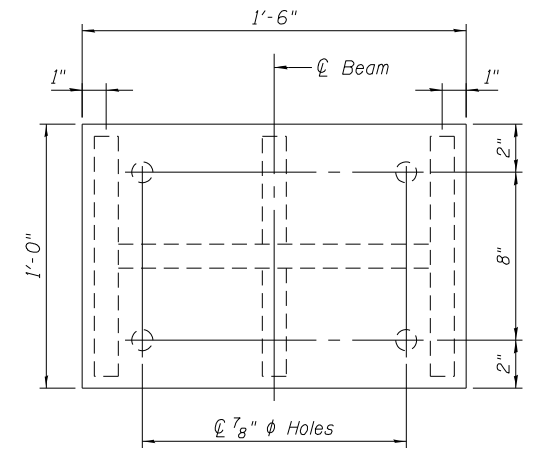
** Provide one additional 3/8" shim plate for beam line 6 at Pier 7 South.



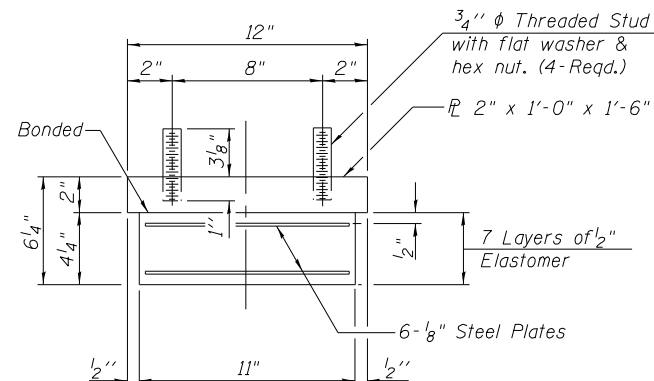
SECTION A-A



ELEVATION STEEL EXTENSION
(Looking North or South)

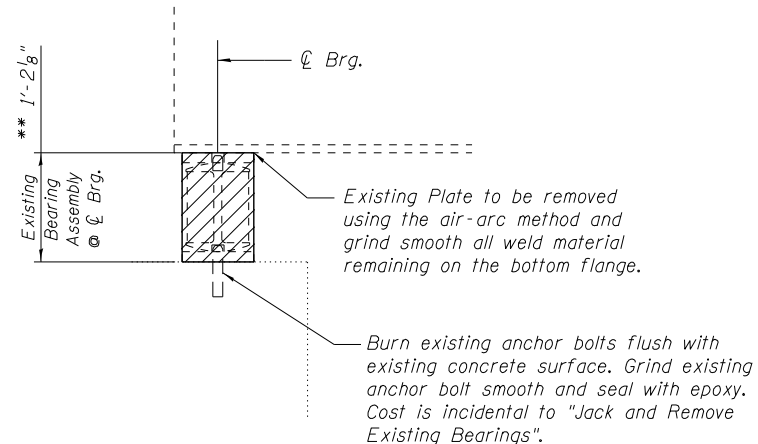


PLAN STEEL EXTENSION



BEARING ASSEMBLY

Note:
Shim plates shall not be placed under Bearing Assembly.



EXISTING BEARING
REMOVAL DETAIL

Note:
Prior to jacking, the Contractor shall verify that hold-down rods installed at North and South abutments during construction have been removed according to original details.

Notes:

Existing bearing height (from top of the bearing seat to bottom of girder flange) information obtained from available existing plans is used to compute proposed steel extension height. Prior to ordering any material, the Contractor shall verify in the field all bearing heights and shim thickness dimensions.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Cost of side retainers and stainless steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I.

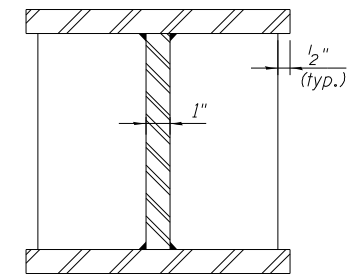
Two 1/8" in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details. Cost included with Elastomeric Bearing Assembly, Type I.

The structural steel plates of the Bearing Assembly shall conform to the requirements of AASHTO M 270 Grade 50.

Existing bearing at beam line 6 at Pier 7 South has additional 3/8" shim plate. Cost of this shim plate is included in Furnishing and Erecting Structural Steel.

New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.

Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.



SECTION B-B

REACTION TABLE (KIPS)

Type	Pier 7S
R DL	67.4
R LL	43.3
R IMP	11.5
R Total	122.2

Note:
Minimum Jack Capacity 75 Tons.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	10
Anchor Bolts, 1"	Each	20
Furnishing and Erecting Structural Steel	Pound	2,230

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 303 EAST WACKER DRIVE, SUITE 1400
 CHICAGO, IL 60601-5276
 PHONE: (312) 373-7700 FAX: (312) 373-6800



USER NAME = ken.joy
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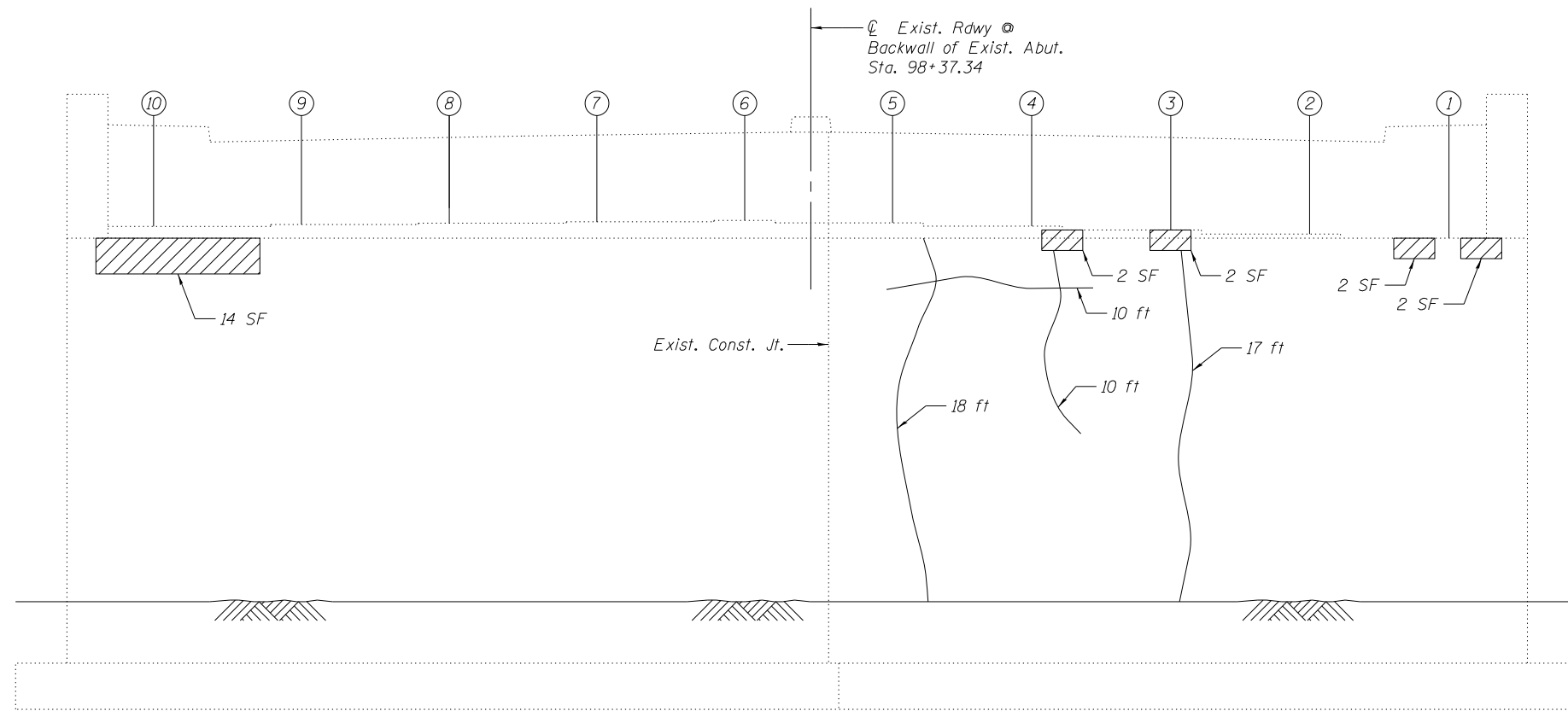
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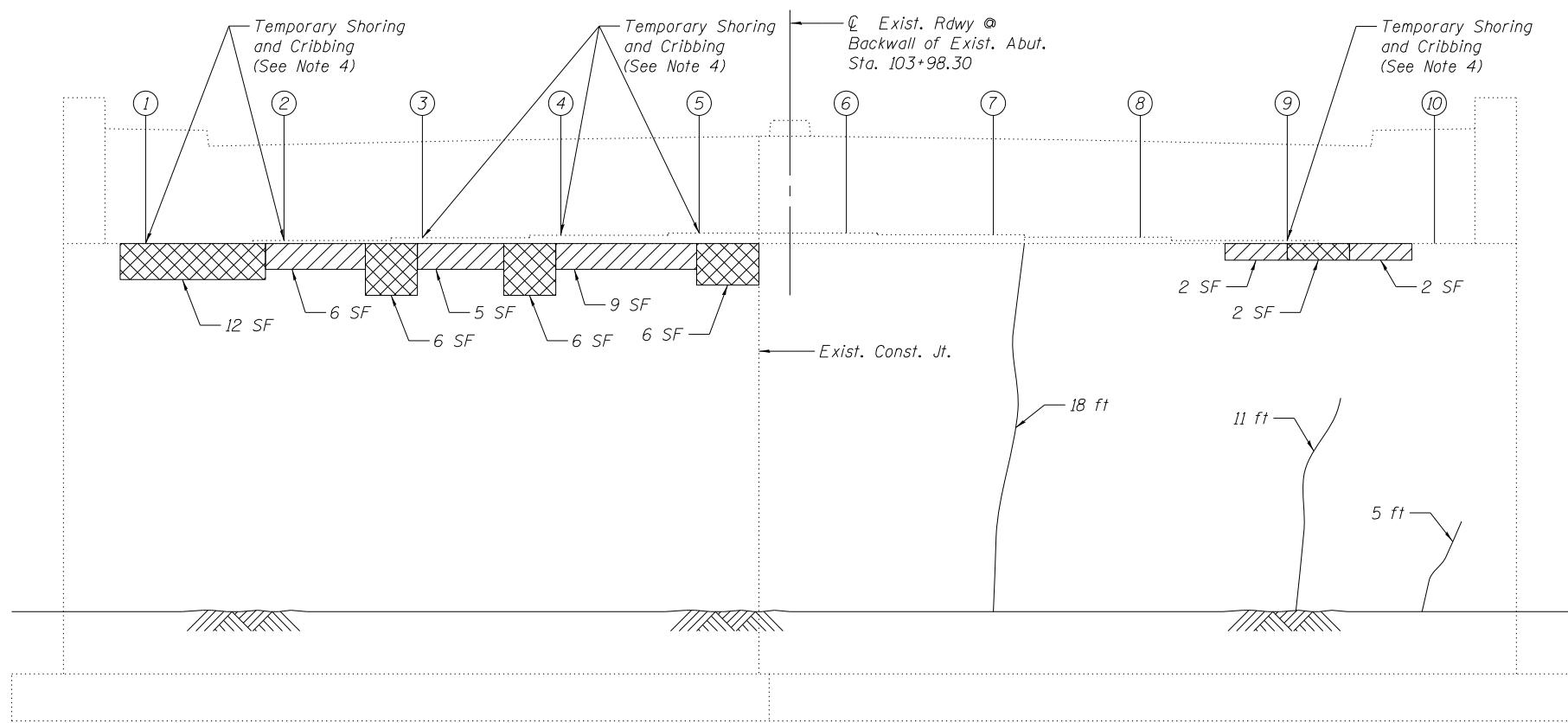
TYPE I BEARING DETAILS
STRUCTURE NO. 016-0194

SHEET NO. SB-24 OF SB-30 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	84
CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



SOUTH ABUTMENT



NORTH ABUTMENT

NOTES:

1. Contractor shall be responsible for removing and reattaching any utility that falls within any substructure repair.
2. Substructure repairs shall be performed when no live load is present over the area of repair work.
3. Concrete repair areas shown based on visual inspection. The actual sizes and locations of repairs shall be determined by the Engineer.
4. Girder lines 1, 2, 3, 4, 5, and 9 shall be temporarily supported off the North Abutment during the concrete repairs below the bearing seats. The temporary supports shall be paid for as "Temporary Shoring and Cribbing." The existing bearing removal at these locations shall not be paid for separately but shall be included in the unit cost of "Temporary Shoring and Cribbing".
5. For bearing reactions, see Sheet SB-23 and SB-24.

LEGEND

- Structural Repair Of Concrete (Depth equal to or less than 5 inches)
- Structural Repair Of Concrete (Depth greater than 5 inches)
- Epoxy Crack Injection

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	46
Structural Repair of Concrete (Depth Greater than 5 inches)	Sq. Ft.	32
Epoxy Crack Injection	Foot	89
Temporary Shoring and Cribbing	Each	6

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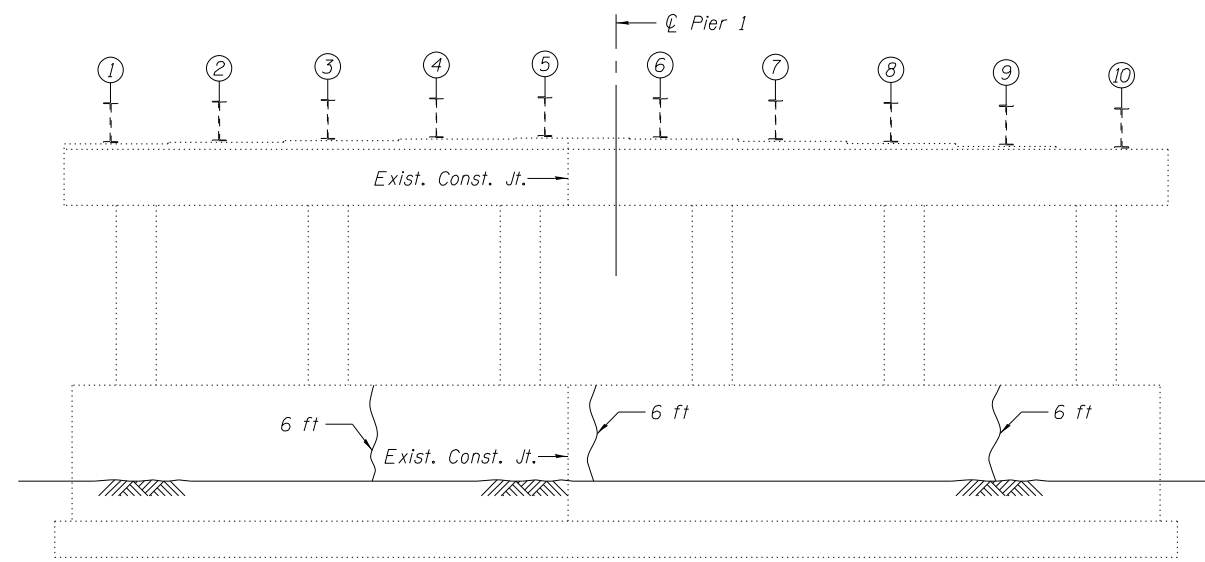
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ABUTMENT REPAIRS
STRUCTURE NO. 016-0194

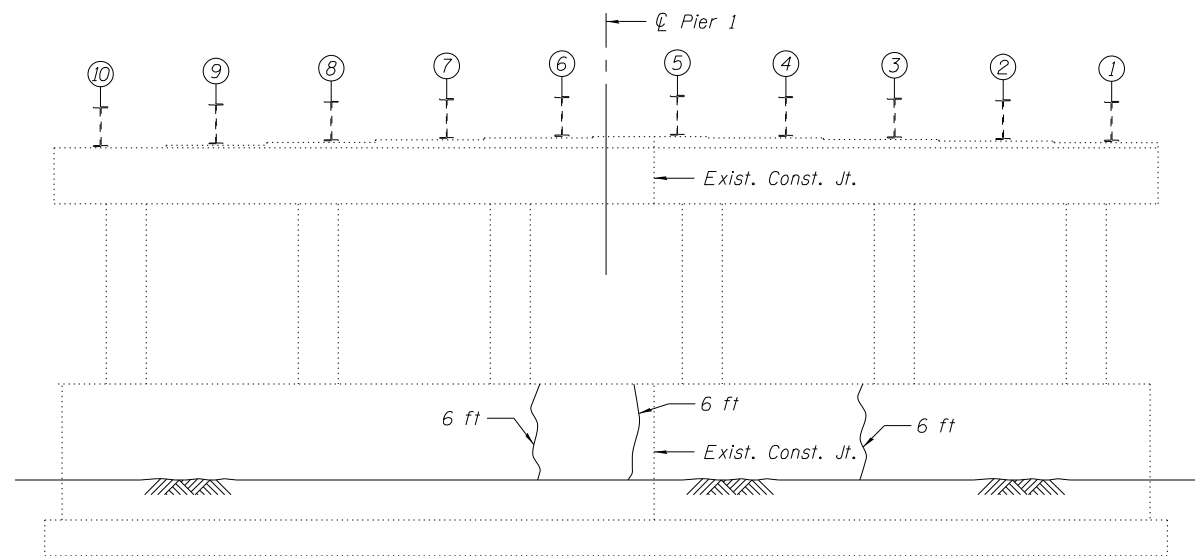
SHEET NO. SB-25 OF 5B-30 SHEETS

F.A.U. R.T.L.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W48				

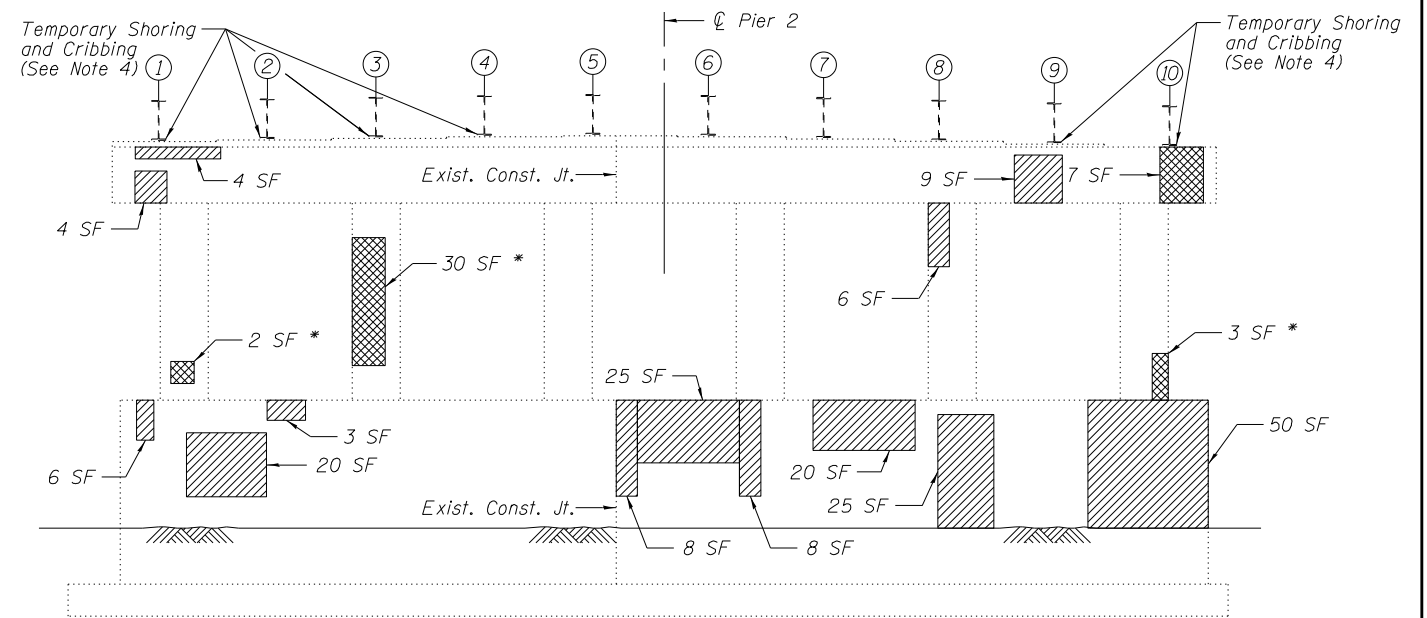
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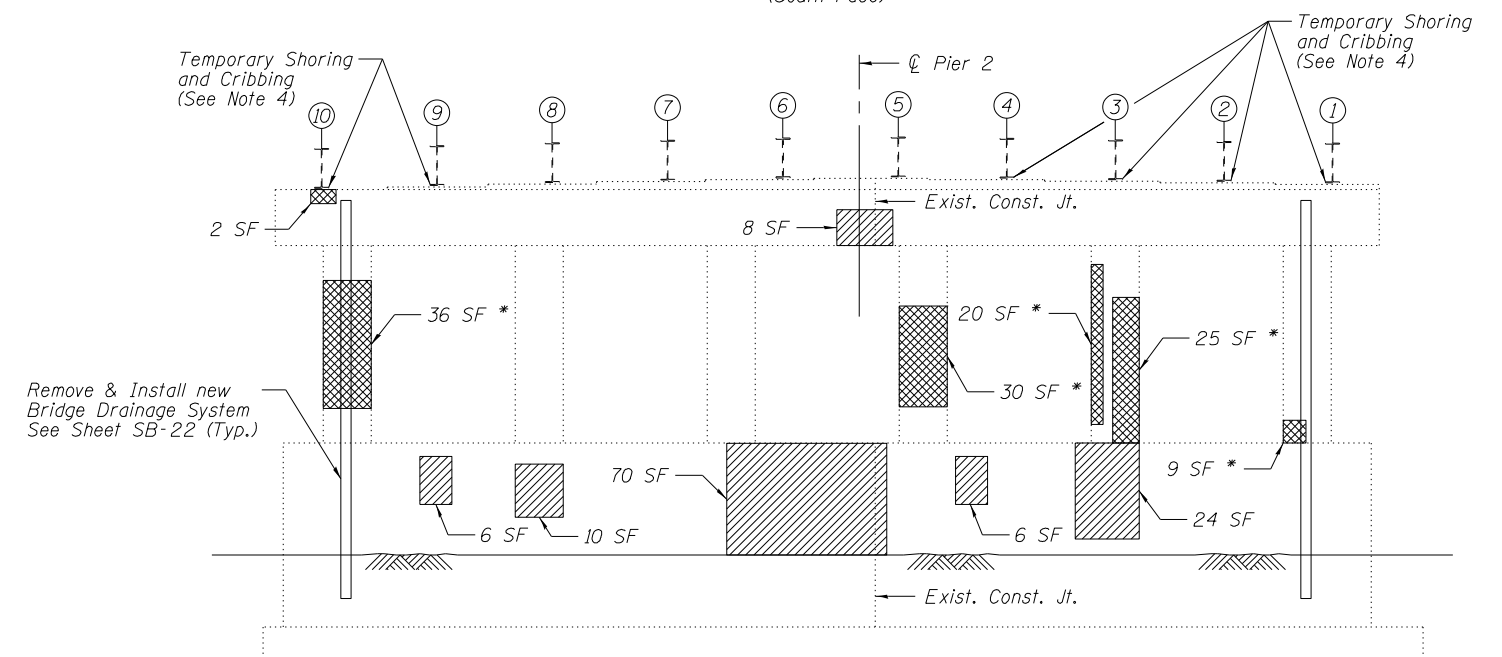
PIER 1
(South Face)



PIER 1
(North Face)



PIER 2
(South Face)



PIER 2
(North Face)

NOTES:

- Contractor shall be responsible for removing and reattaching any utility that falls within any substructure repair.
- Substructure repairs shall be performed when no live load is present over the area of repair work.
- Concrete repair areas shown based on visual inspection. The actual sizes and locations of repairs shall be determined by the Engineer.
- Girder lines 1, 2, 3, 4, 9 and 10 shall be temporarily supported off the pier during the concrete repairs to the pier cap and columns below. The temporary supports shall be paid for as "Temporary Shoring and Cribbing." The existing bearing removal at these locations shall not be paid for separately but shall be included in the unit cost of "Temporary Shoring and Cribbing".
- For bearing reactions, see Sheet SB-23 and SB-24.

* Fiber Wrap shall be applied to the circumference of the pier column after structural repair of concrete. See Special Provisions.

LEGEND

- Structural Repair Of Concrete (Depth equal to or less than 5 inches)
- Structural Repair Of Concrete (Depth greater than 5 inches)
- Epoxy Crack Injection

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	312
Structural Repair of Concrete (Depth Greater than 5 inches)	Sq. Ft.	164
Epoxy Crack Injection	Foot	36
Fiber Wrap	Sq. Ft.	288
Temporary Shoring and Cribbing	Each	12
Acrylic Coating	Sq. Yd.	32

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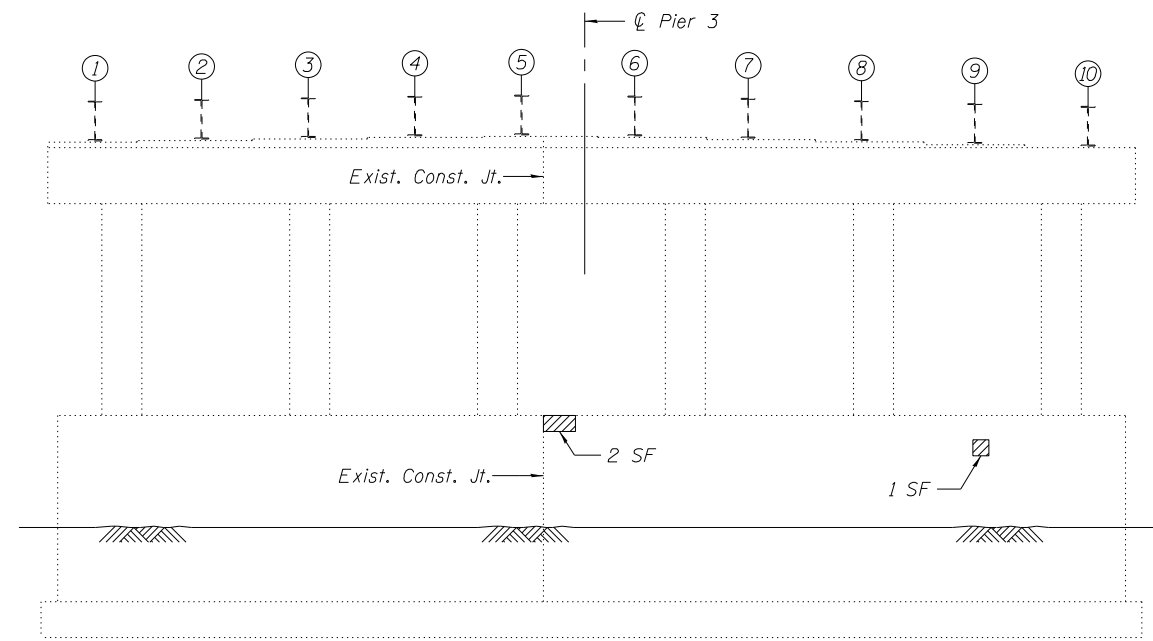
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

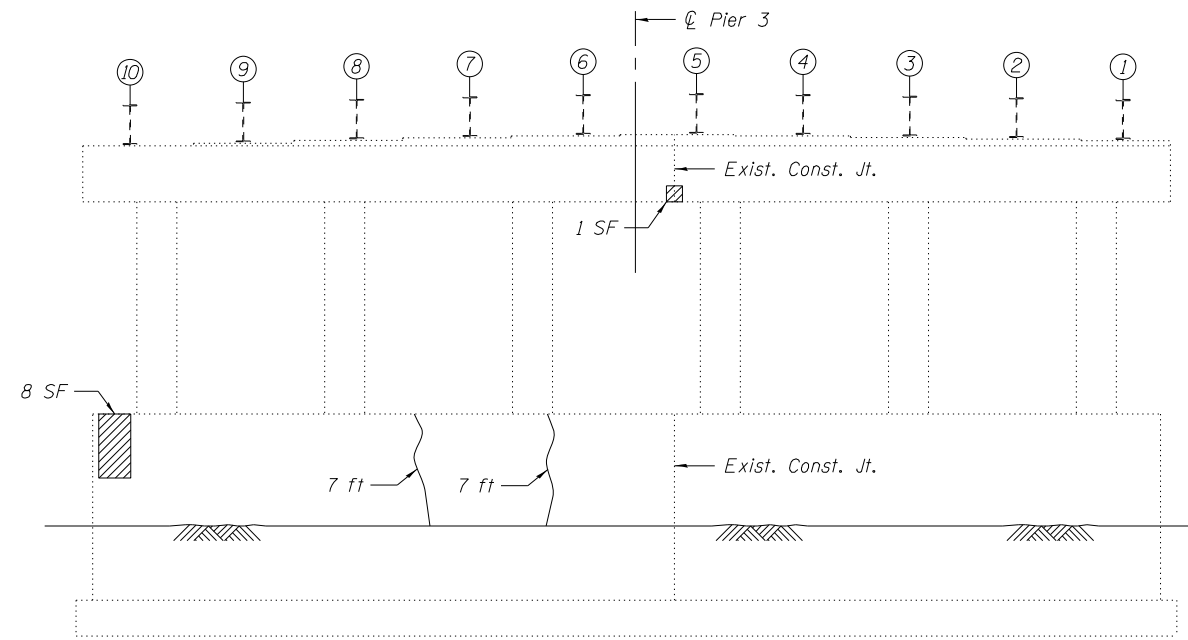
**PIERS 1 & 2 REPAIRS
STRUCTURE NO. 016-019A**

SHEET NO. SB-26 OF SB-30 SHEETS

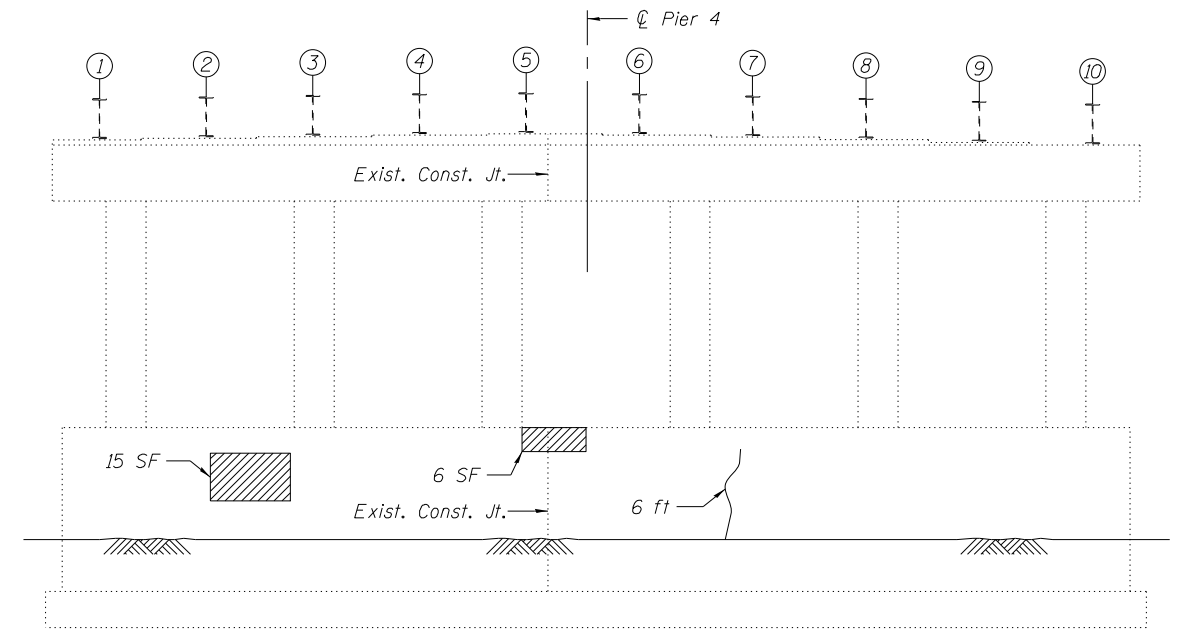
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CONTRACT NO. 62W48				
ILLINOIS FED. AID PROJECT				



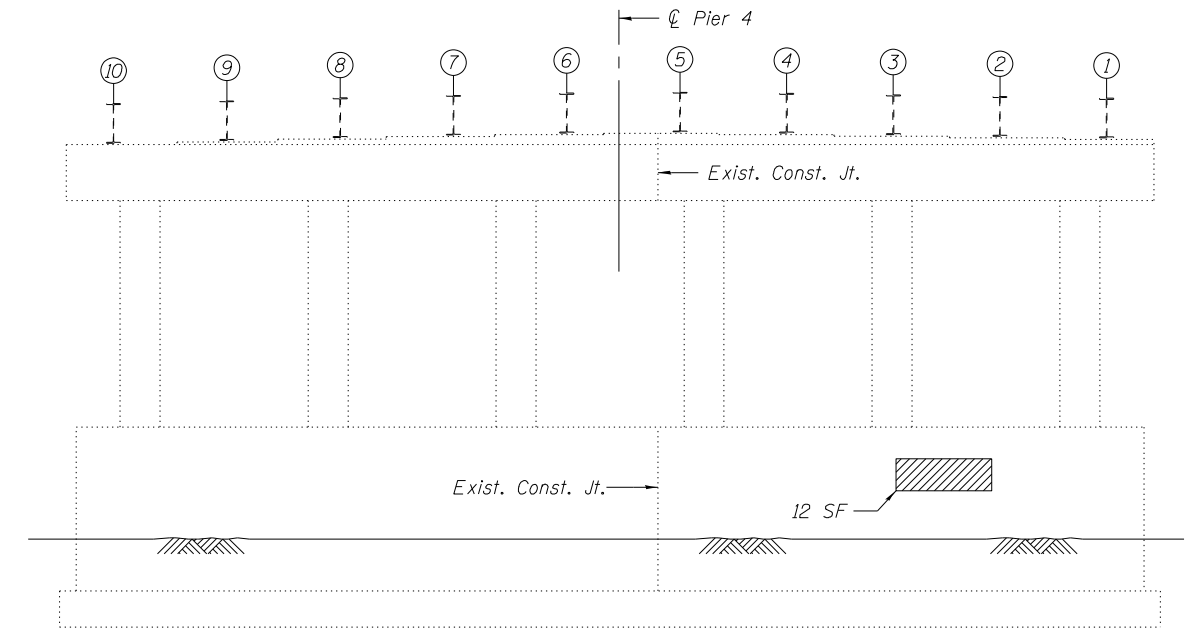
PIER 3
(South Face)



PIER 3
(North Face)



PIER 4
(South Face)



PIER 4
(North Face)

NOTES:

- Contractor shall be responsible for removing and reattaching any utility that falls within any substructure repair.
- Concrete repair areas shown based on visual inspection. The actual sizes and locations of repairs shall be determined by the Engineer.
- Substructure repairs shall be performed when no live load is present over the area of repair work.

LEGEND

- Structural Repair Of Concrete (Depth equal to or less than 5 inches)
- Epoxy Crack Injection

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	45
Epoxy Crack Injection	Foot	20

MODEL: \\MODEL\NAME
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 AECOM, USA
 303 EAST WACKER DRIVE, SUITE 1400
 CHICAGO, IL 60601-5276
 PHONE: (312) 373-7700 FAX: (312) 373-6800



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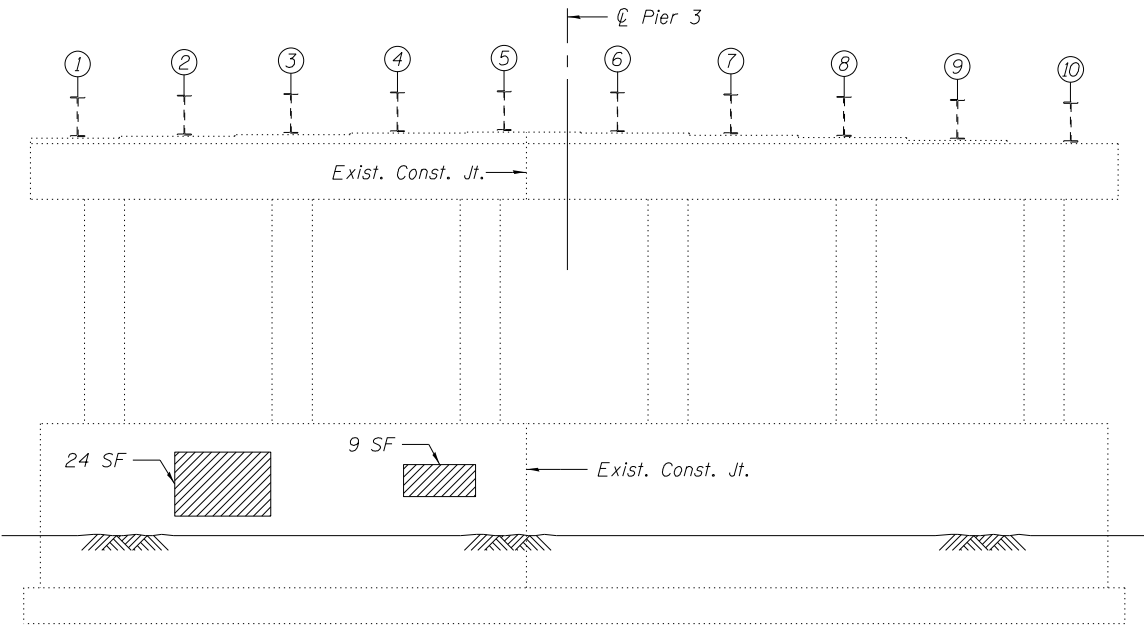
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIERS 3 & 4 REPAIRS
STRUCTURE NO. 016-0194

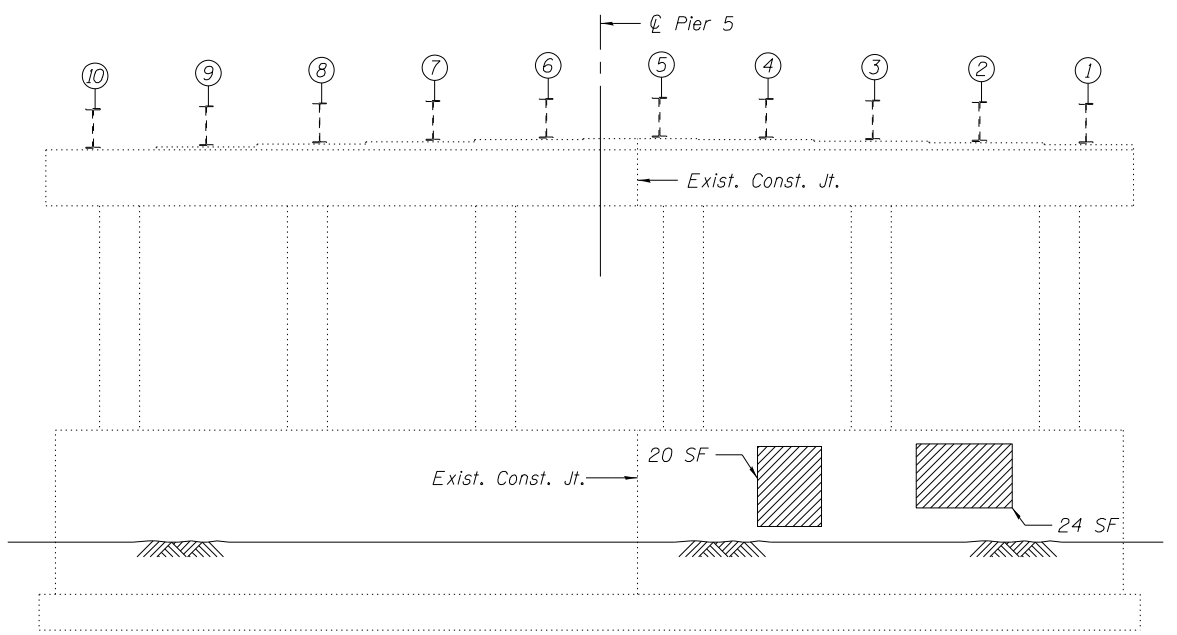
SHEET NO. SB-27 OF SB-30 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W48				

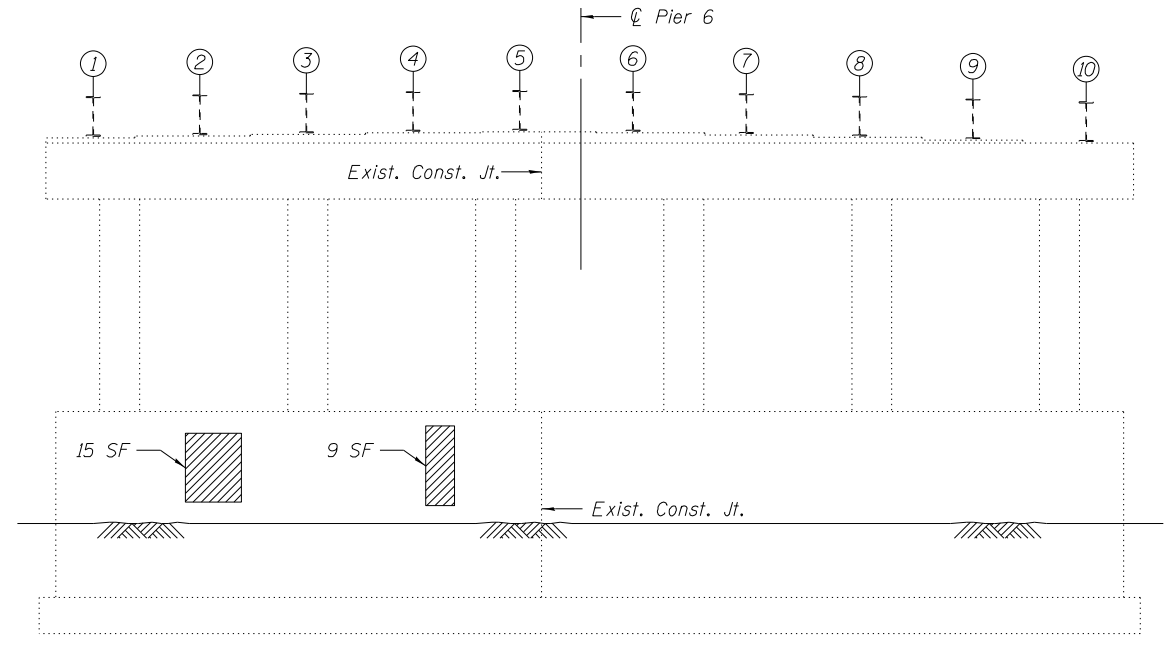
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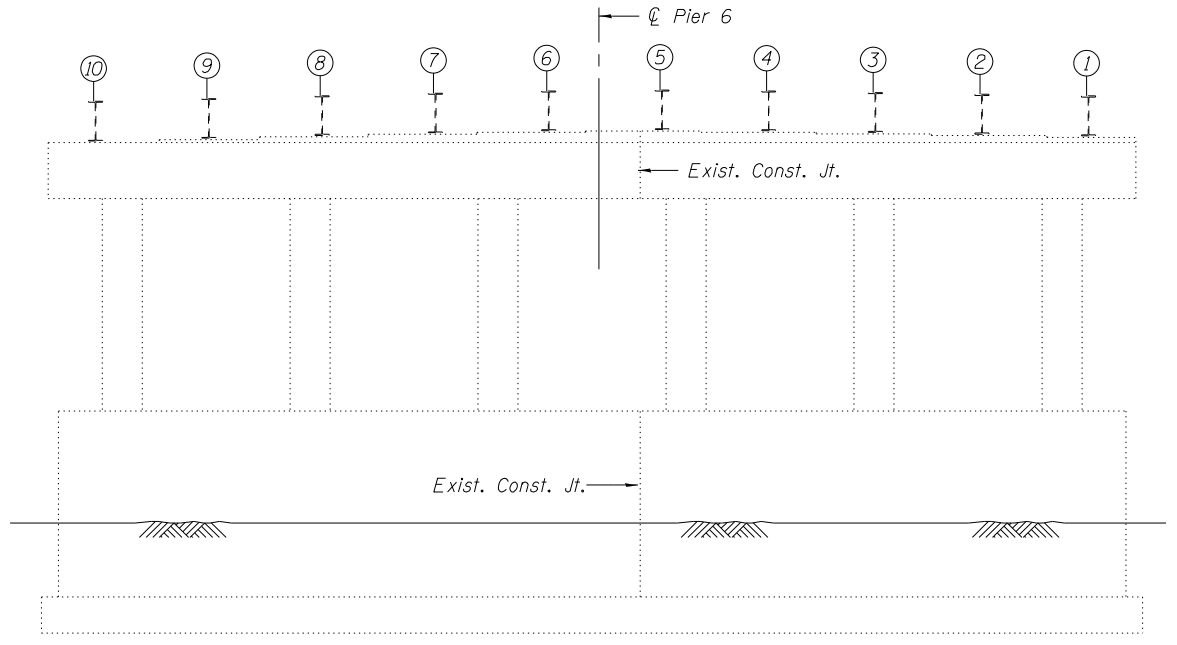
PIER 5
(South Face)



PIER 5
(North Face)



PIER 6
(South Face)

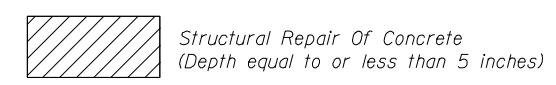


PIER 6
(North Face)

NOTES:

- Contractor shall be responsible for removing and reattaching any utility that falls within any substructure repair.
- Concrete repair areas shown based on visual inspection. The actual sizes and locations of repairs shall be determined by the Engineer.
- Substructure repairs shall be performed when no live load is present over the area of repair work.

LEGEND



BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	101

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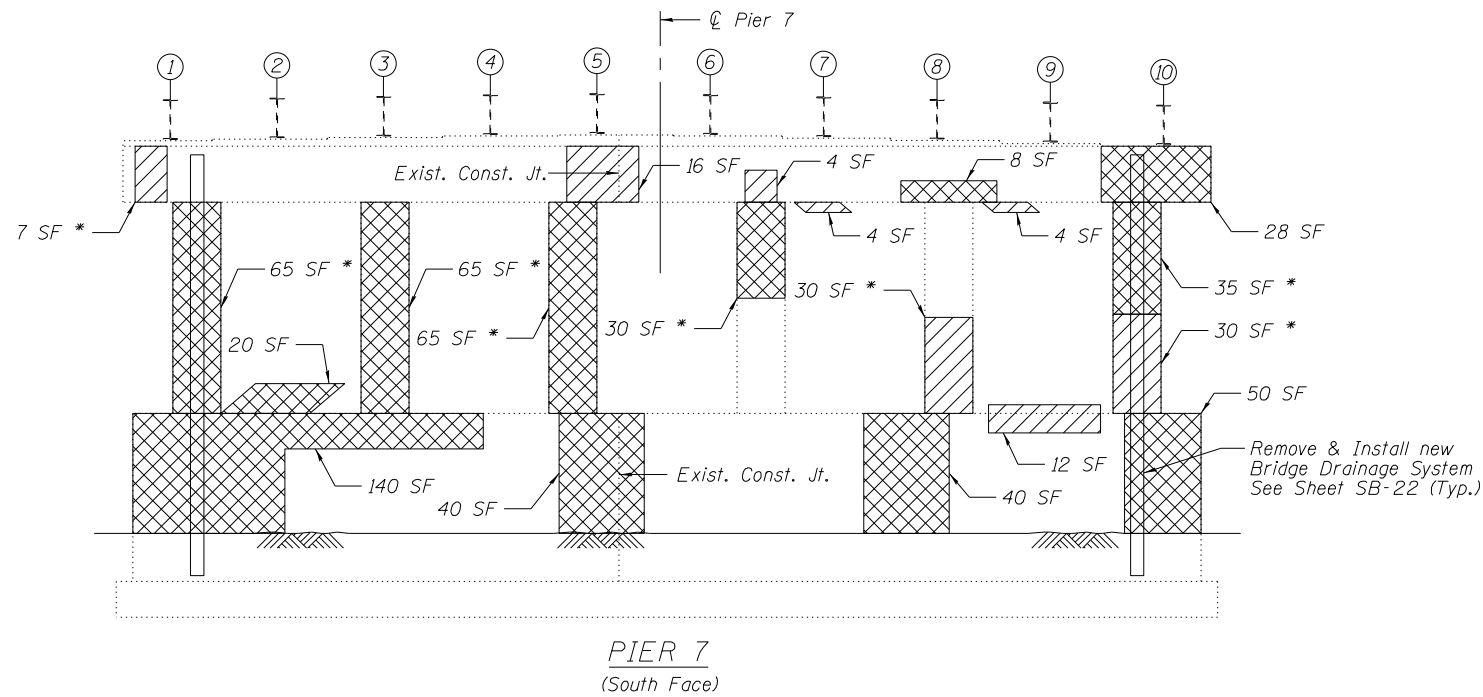
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PIERS 5 & 6 REPAIRS
STRUCTURE NO. 016-0194

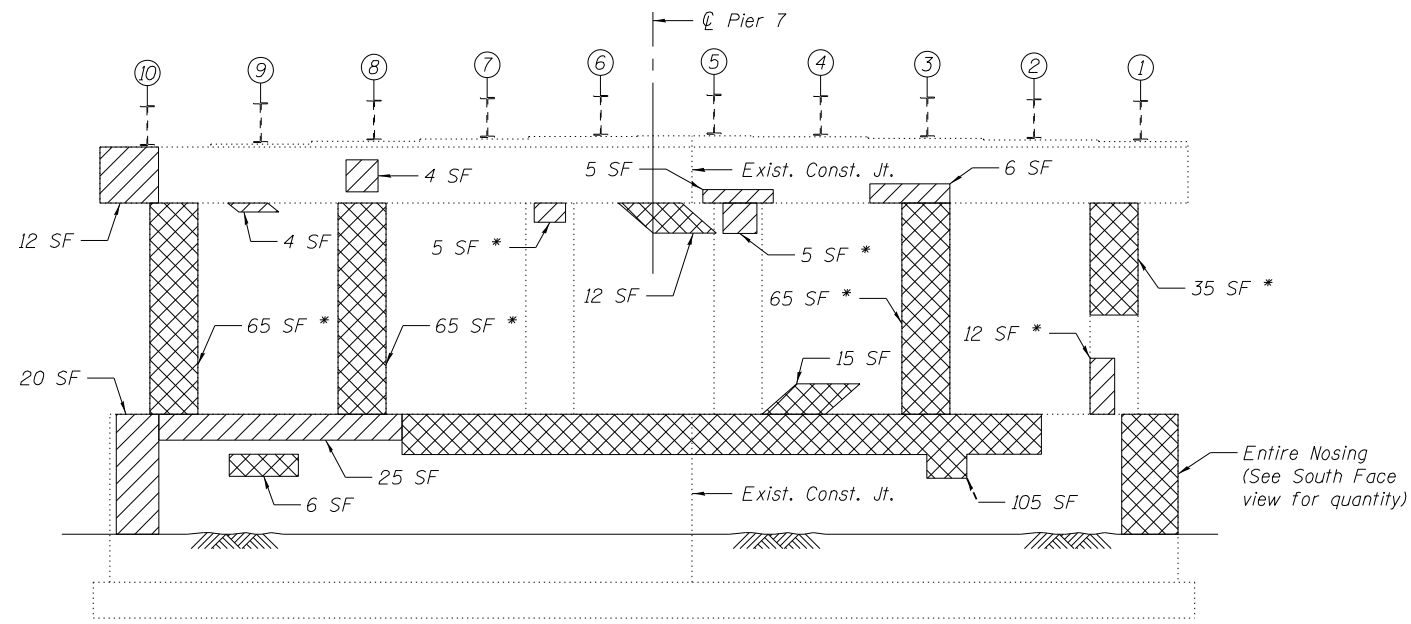
SHEET NO. SB-28 OF SB-30 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	88
CONTRACT NO. 62W48				

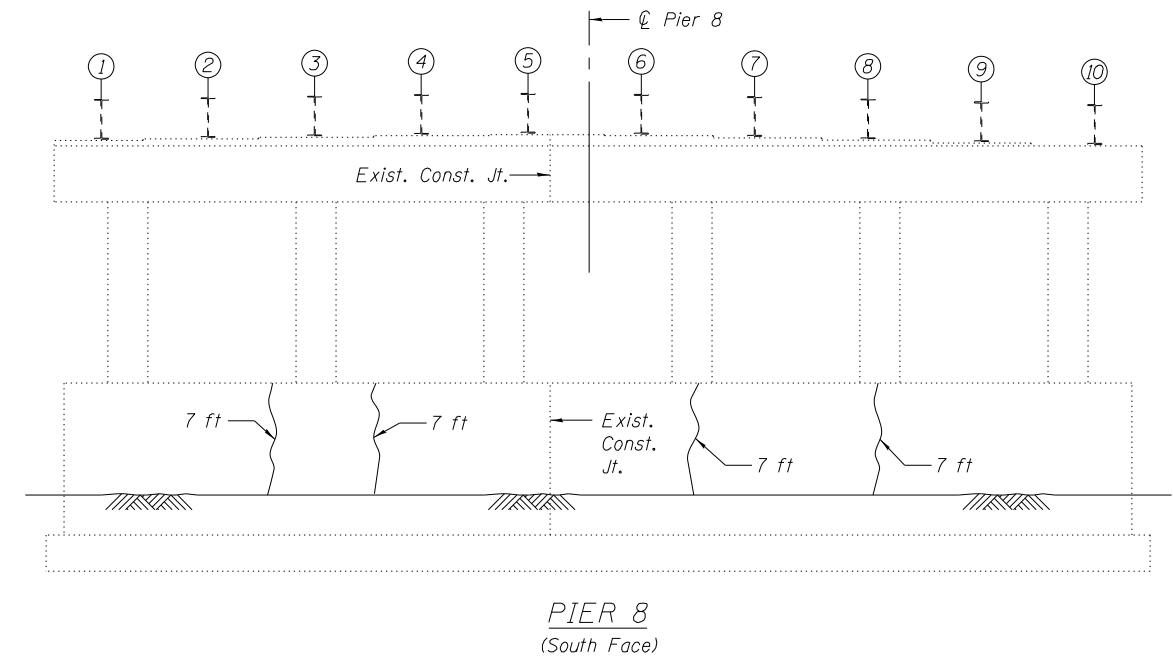
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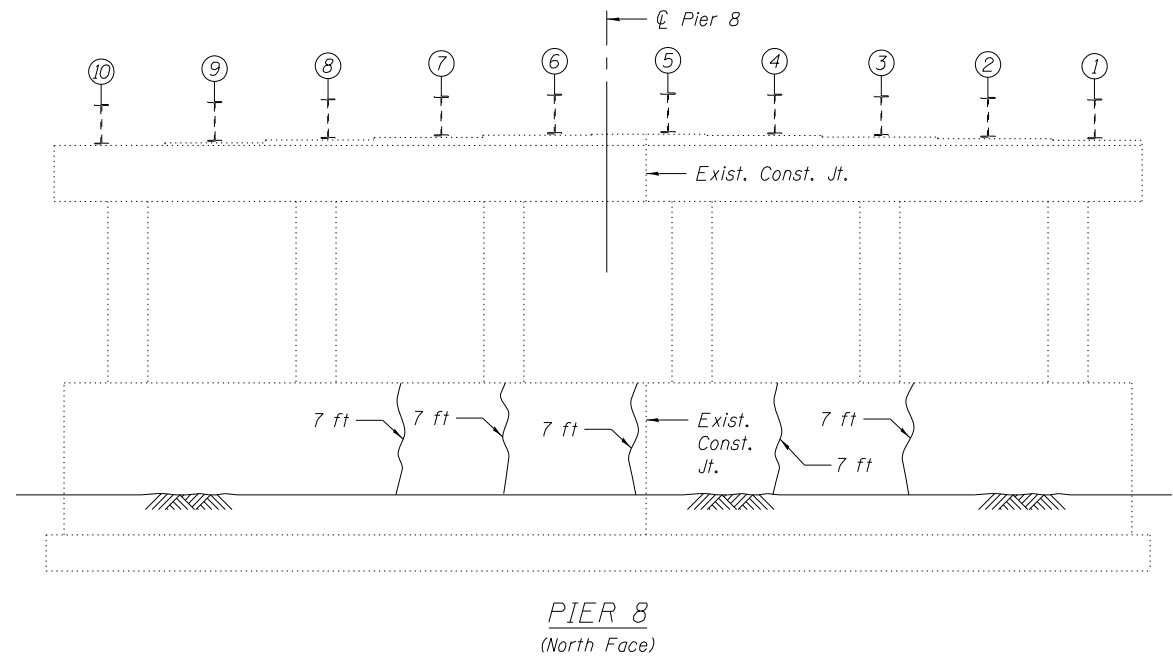
PIER 7
(South Face)



PIER 7
(North Face)



PIER 8
(South Face)



PIER 8
(North Face)

NOTES:

1. Contractor shall be responsible for removing and reattaching any utility that falls within any substructure repair.
2. Substructure repairs shall be performed when no live load is present over the area of repair work.
3. Concrete repair areas shown based on visual inspection. The actual sizes and locations of repairs shall be determined by the Engineer.
4. All girder lines shall be temporarily supported off of Pier 7 during the concrete repairs to the pier cap and columns below. The temporary supports shall be paid for as "Temporary Shoring and Cribbing." The existing bearing removal at these locations shall not be paid for separately but shall be included in the unit cost of "Temporary Shoring and Cribbing".
5. For bearing reactions, see Sheet SB-23 and SB-24.
6. Overhead concrete repairs shall be performed using Shotcrete. See Special Provisions.

* Fiber Wrap shall be applied to the circumference of the pier column after structural repair of concrete. See Special Provisions.

LEGEND

- Structural Repair Of Concrete (Depth equal to or less than 5 inches)
- Structural Repair Of Concrete (Depth greater than 5 inches)
- Epoxy Crack Injection

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	205
Structural Repair of Concrete (Depth Greater than 5 inches)	Sq. Ft.	934
Epoxy Crack Injection	Foot	63
Fiber Wrap	Sq. Ft.	682
Temporary Shoring and Cribbing	Each	20
Acrylic Coating	Sq. Yd.	76

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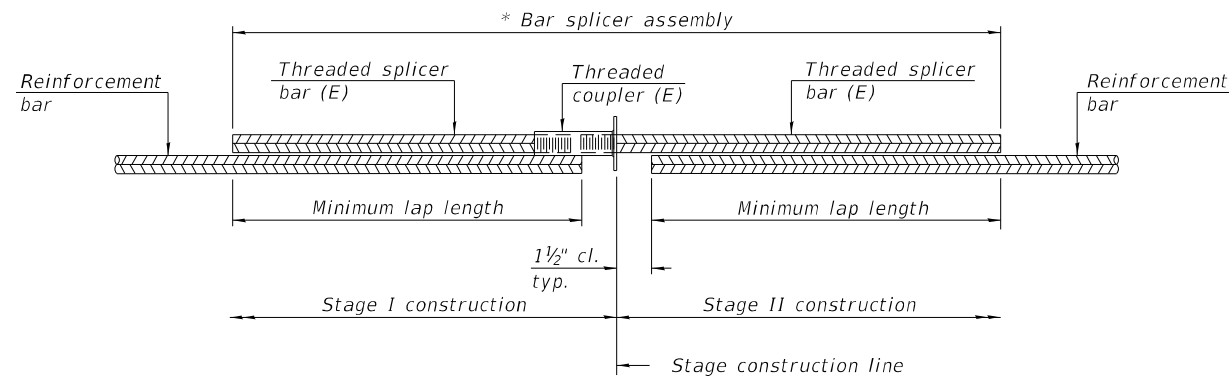
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DEPARTMENT OF TRANSPORTATION**

**PIERS 7 & 8 REPAIRS
STRUCTURE NO. 016-0194**

SHEET NO. SB-29 OF SB-30 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W48				

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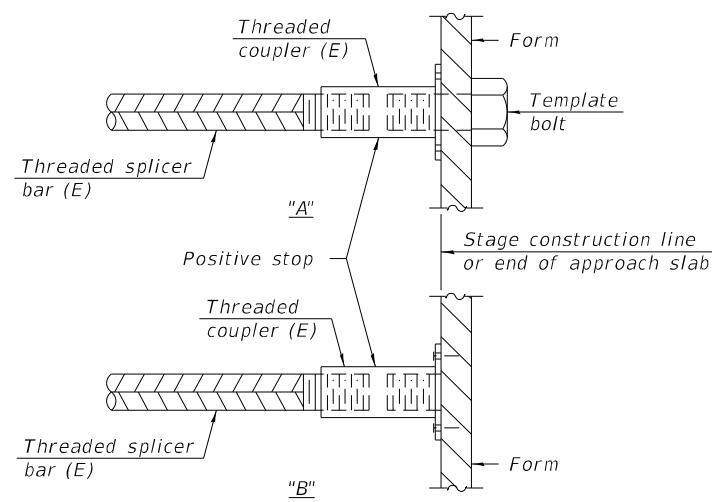


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

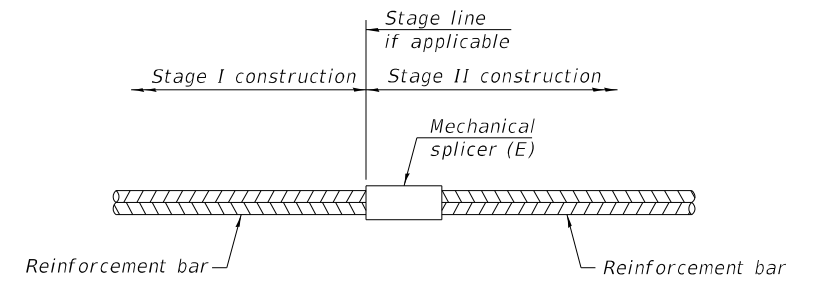
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
South Abutment	#6	8	3'-7"
South Abutment	#5	18	3'-6"
North Abutment	#6	8	3'-7"
North Abutment	#5	18	3'-6"
Pier 2	#5	40	3'-6"
Pier 7	#5	40	3'-6"
Beam Encasement	#5	72	3'-0"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: I:\MODEL\NAME: BSD-1
 FILE: I:\MODEL\NAME: BSD-1.dwg
 USER: ken.joy
 DATE: 4/2/2024
 PROJECT: 15-00131-01-BR
 SHEETS: 109 OF 90
 CONTRACT NO. 62W48
 ILLINOIS FED. AID PROJECT

BSD-1

1-1-2020



USER NAME = ken.joy	DESIGNED - NPP	REVISD -
PLOT SCALE = N.T.S.	CHECKED - MCC	REVISD -
PLOT DATE = 4/2/2024	DRAWN - NPP	REVISD -
	CHECKED - MCC	REVISD -

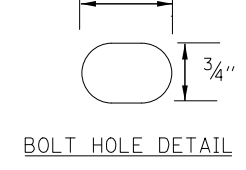
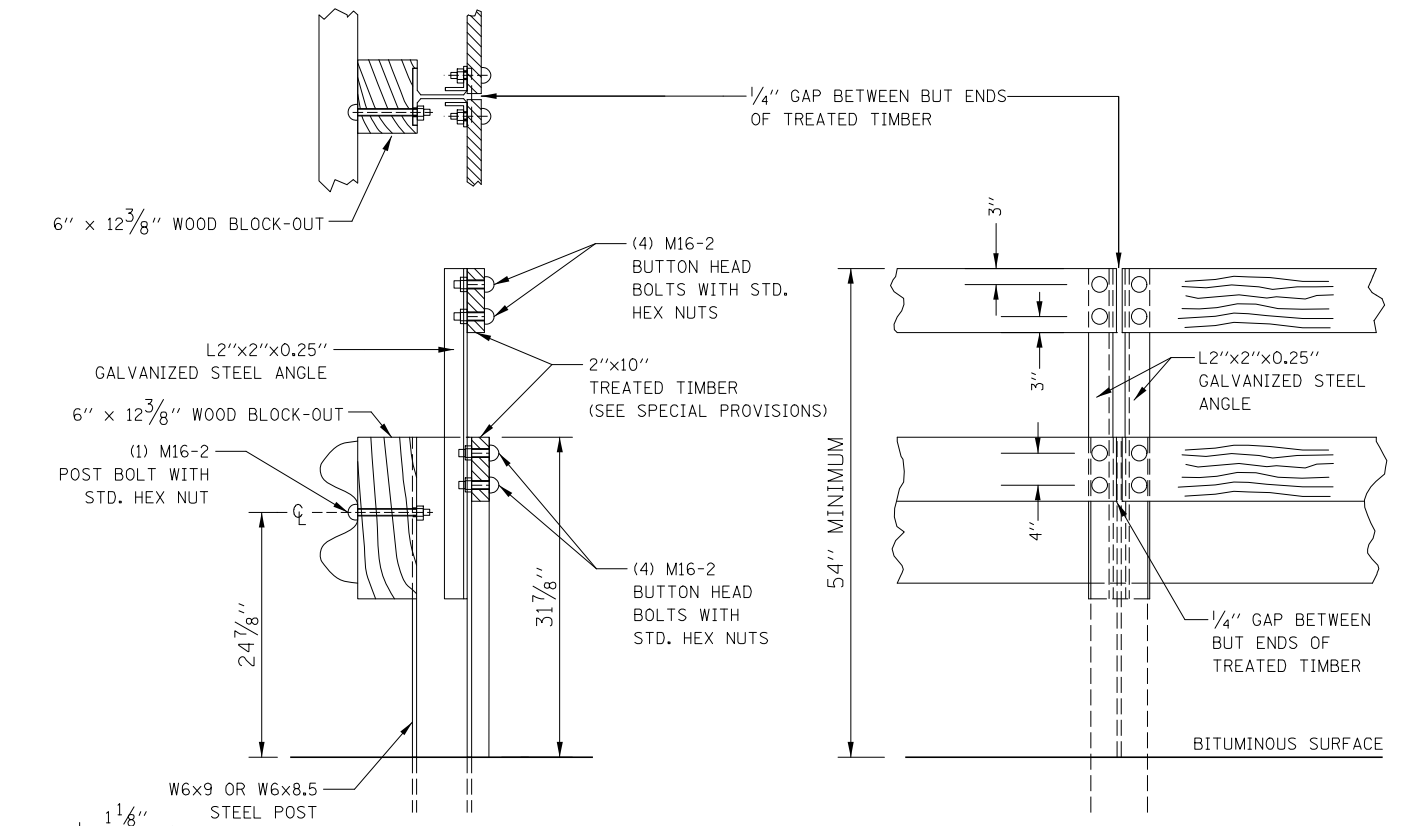
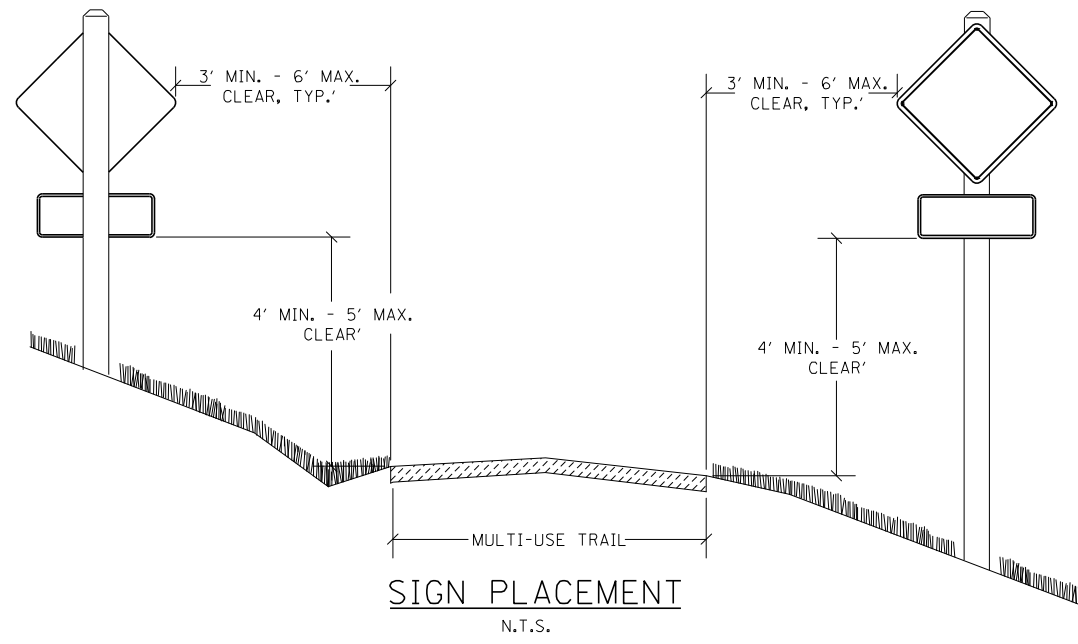
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 016-0194**

SHEET NO. SB-30 OF SB-30 SHEETS

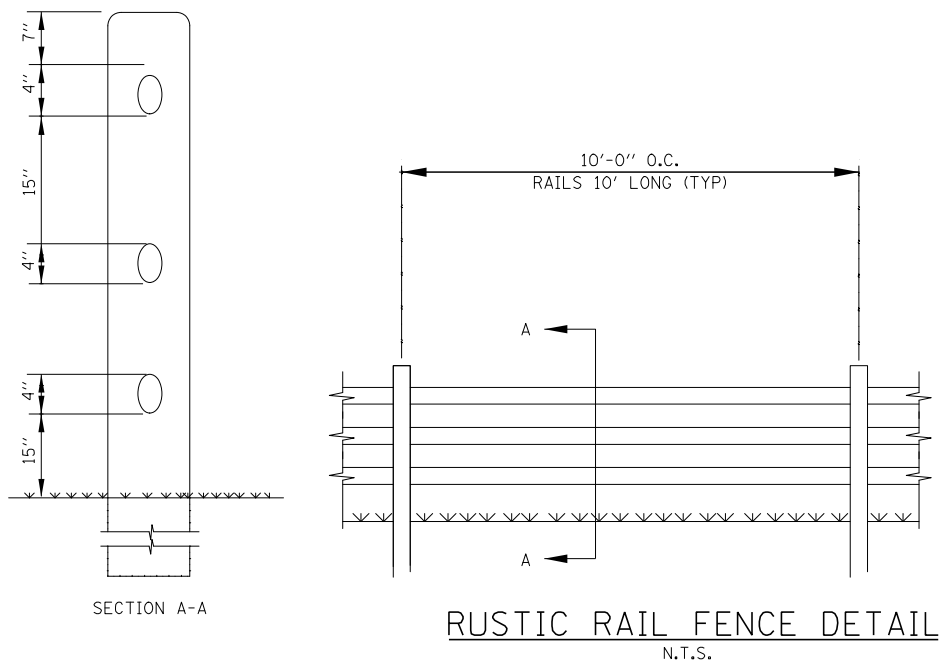
FAU- RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	90
CONTRACT NO. 62W48				

ILLINOIS FED. AID PROJECT



DUE TO EXPANSION AND CONTRACTION OF THE TREATED TIMBER, THE LOCATION OF THE BOLTS WITH RESPECT TO THE SLOTTED BOLT HOLES IN THE TREATED TIMBER WILL BE DEPENDANT ON THE AMBIENT TEMPERATURE AT PLACEMENT. CONSULT THE MANUFACTURER FOR BOLT HOLE LOCATION IN THE TREATED TIMBER AND BOLT LOCATION IN THE SLOTTED BOLT HOLE.

RUB RAIL DETAIL
BIKE PATH GUARDRAIL ADJUSTMENT



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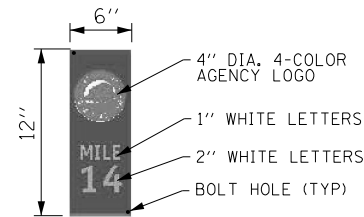
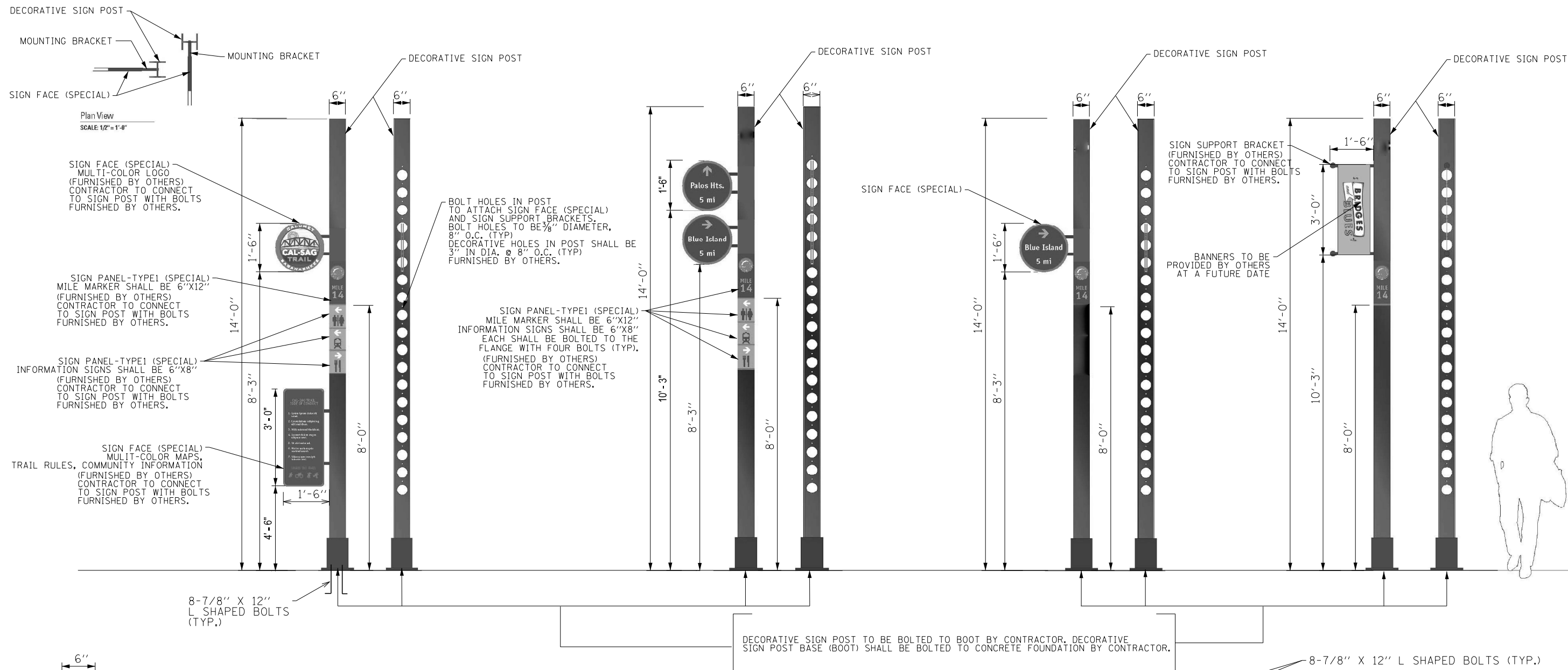
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	DATE - 04/02/2024	REVISED -

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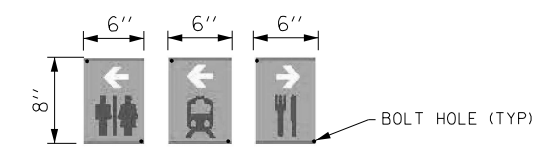
ROADWAY DETAIL
MISCELLANEOUS

SHEET NO. OF SHEETS STA. TO STA.

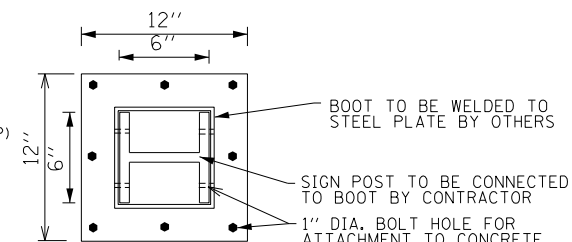
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CONTRACT NO. 62W48				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



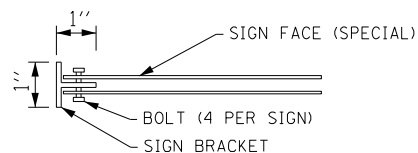
MILE MARKER
SIGN PANEL - TYPE 1
NOT TO SCALE



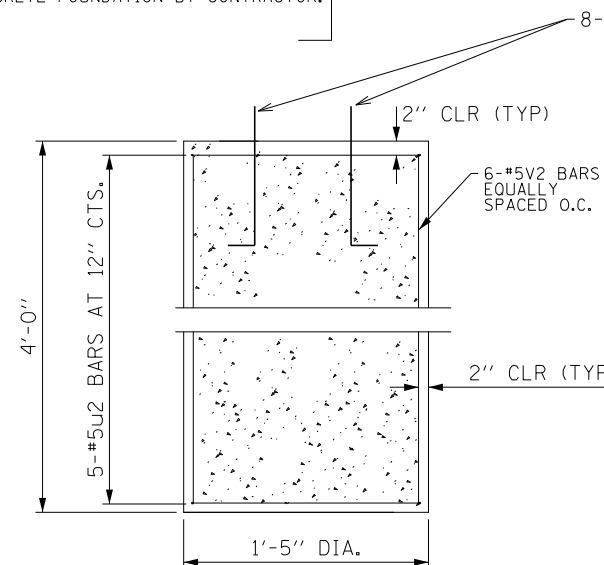
4-COLOR INFORMATION
SIGN PANEL - TYPE 1
NOT TO SCALE



PLAN VIEW
BASE PLATE DETAIL
NOT TO SCALE



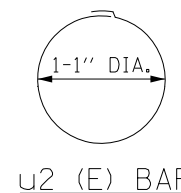
PLAN VIEW
MOUNTING BRACKET DETAIL
NOT TO SCALE



MONUMENT TYPE A FOUNDATION DETAIL
FOR DECORATIVE SIGN AND POST (DIRECT POST)
NOT TO SCALE

BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH	SHAPE
v2(E)	6	#5	3'-8"	—
u2(E)	5	#5	3'-11"	○
CONCRETE STRUCTURE			Cu. Yd.	0.2
REINFORCEMENT BARS, EPOXY COATED			POUND	60



NOTE:
DECORATIVE SIGN POSTS AND ALL ASSOCIATED AND APPLICABLE SIGN PANEL-LOGOS, SIGN PANELS, SIGN SUPPORT BRACKETS, MOUNTING BRACKETS, AND BASE PLATES ARE FURNISHED BY OTHERS FOR THIS CONTRACT.
CONTRACTOR TO COORDINATE THE PROCUREMENT OF FURNISHED ITEMS WITH THE ENGINEER AND THE VILLAGE OF RIVERDALE.



USER NAME = ken.moy	DESIGNED - KLM	REVISED -
PLOT SCALE = 4.0000' / in.	DRAWN - KLM	REVISED -
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	DATE - 04/02/2024	REVISED -

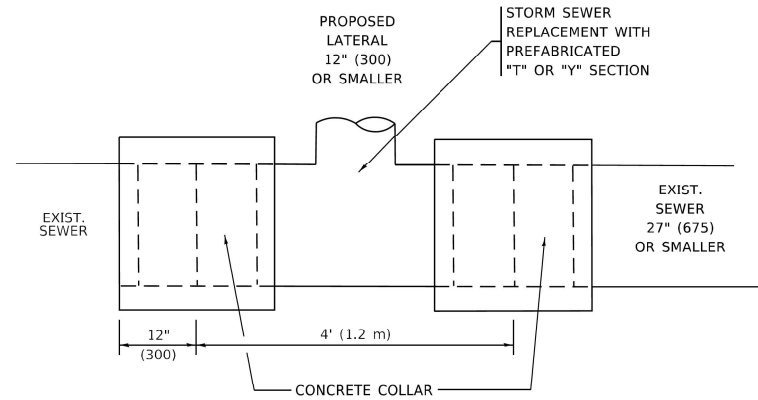
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAIL
DECORATIVE SIGN AND POST ASSEMBLY DETAILS

SHEET NO. OF SHEETS STA. TO STA.

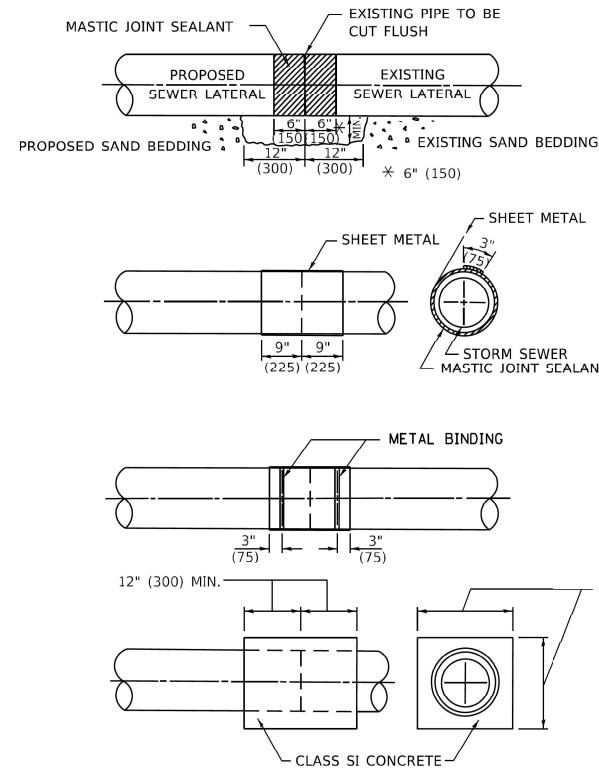
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	92
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

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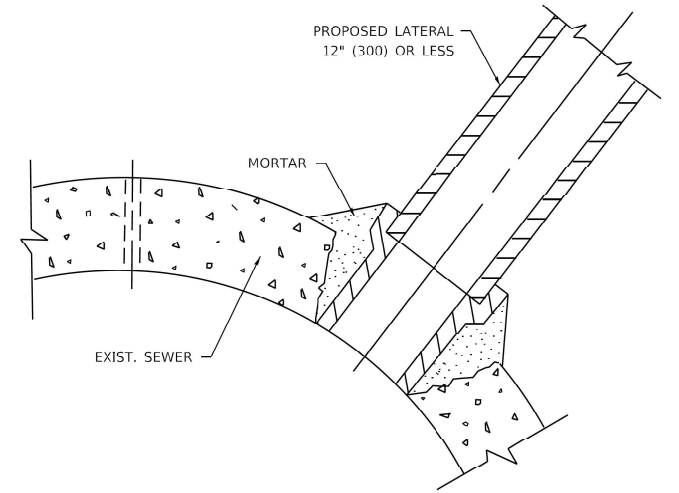
DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



DETAIL "B"

CLASS SI CONCRETE COLLAR



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.

NOTES:

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

- CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.
- CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

- TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.
- REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.
- TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.
- CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

* ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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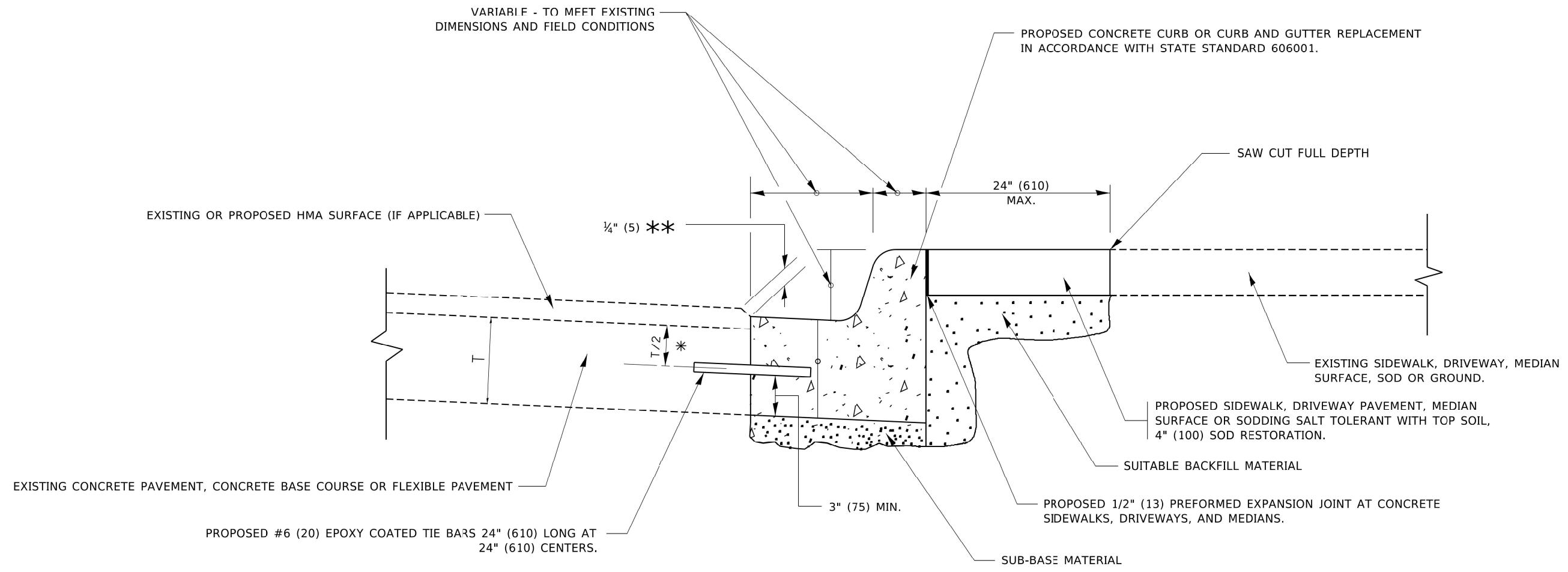


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PLOT DATE = 4/2/2024	CHECKED - BMC	REVISED -
	DATE - 04/02/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

D1 STANDARD - DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	93
SCALE: NONE		SHEET NO. OF SHEETS		STA. TO STA.
				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

CONTRACT NO. 62W48



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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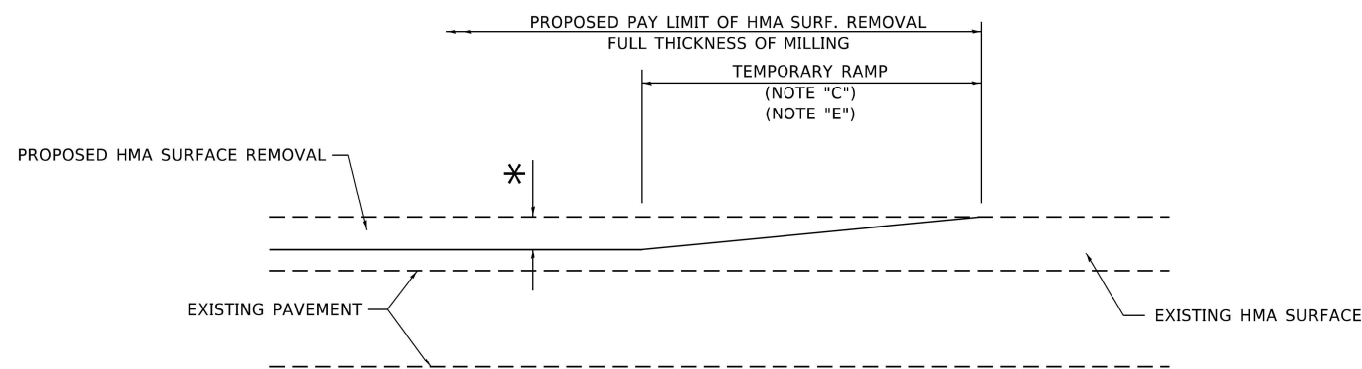
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PLOT DATE = 4/2/2024	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**D1 STANDARD - CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

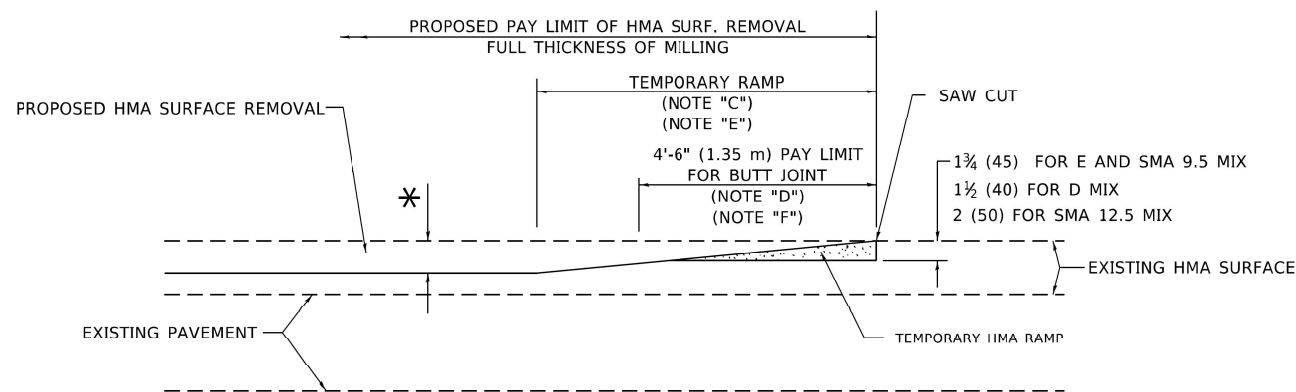
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3730	15-00131-01-BR	COOK	109	94
BD-24		CONTRACT NO. 62W48		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

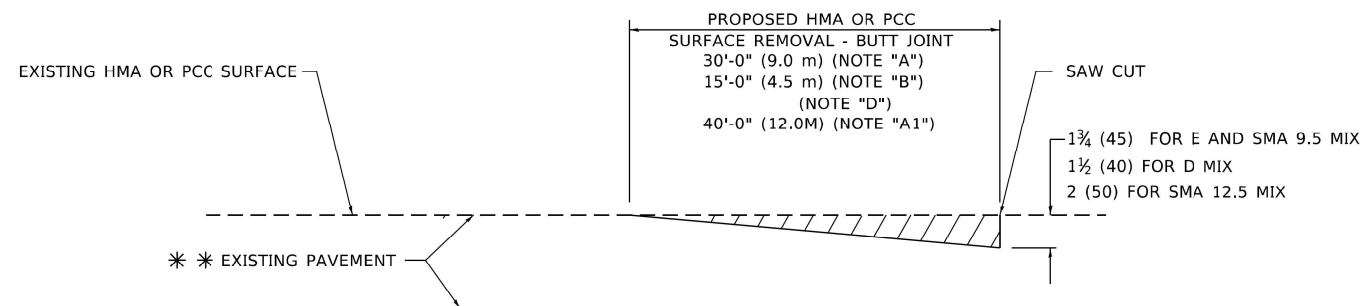


HMA CONSTRUCTED TEMPORARY RAMP

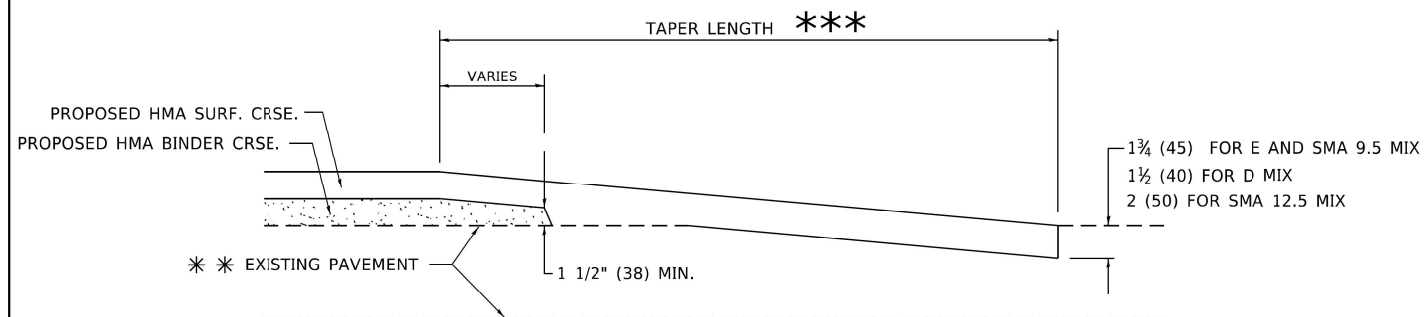
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

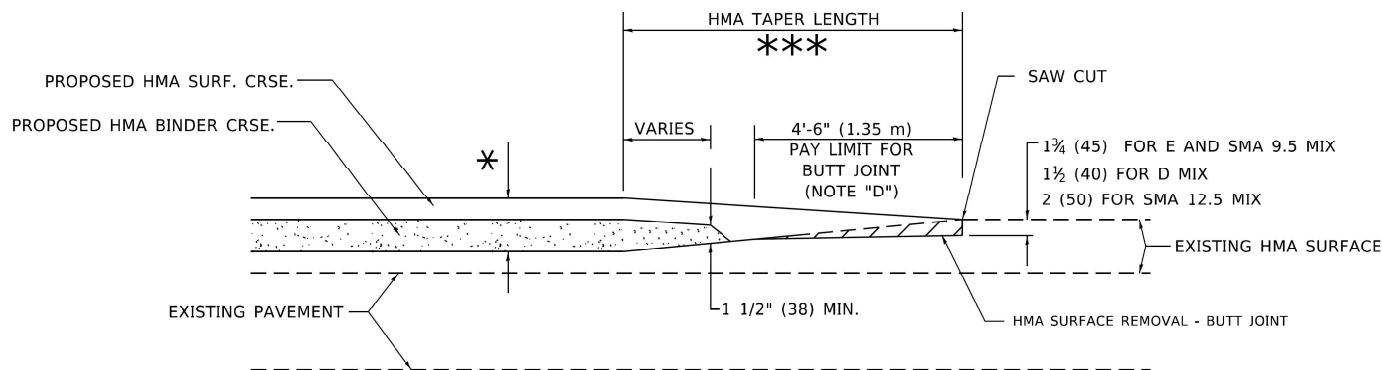
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3" - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.06 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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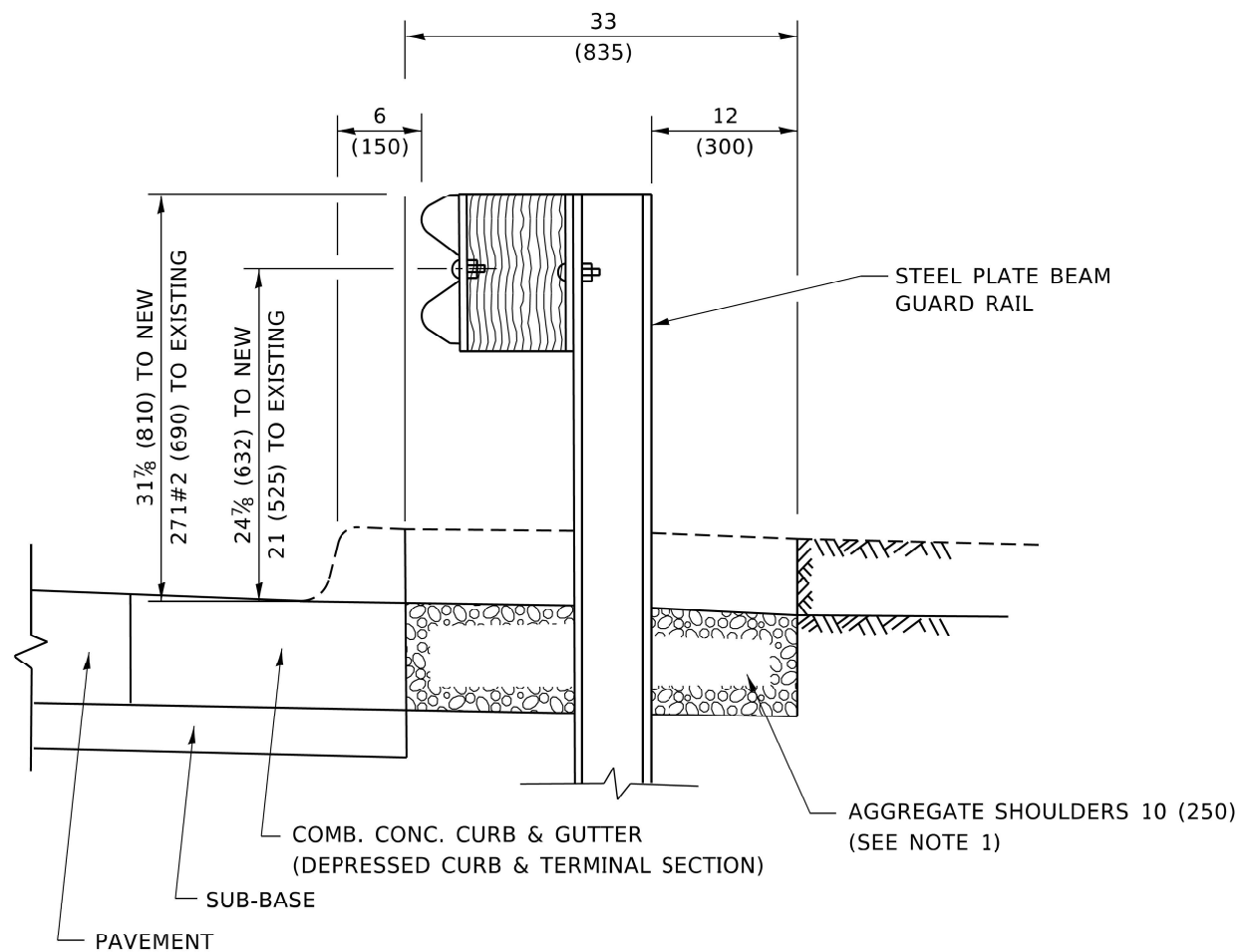
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PLOT DATE = 4/2/2024	DATE - 04/02/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

D1 STANDARD - BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD-32		CONTRACT NO. 62W48		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



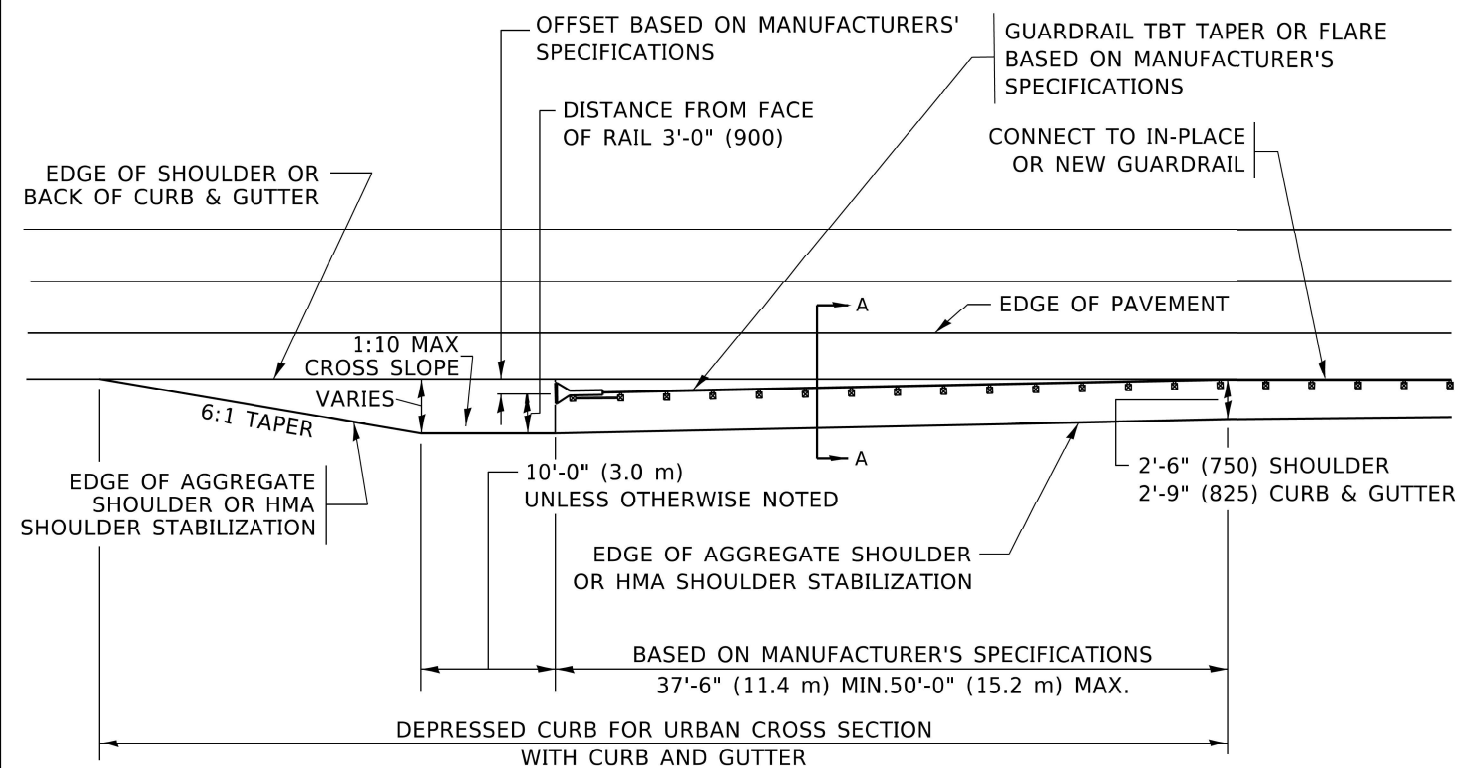
SECTION A-A

NOTES:

1. THE AGGREGATE SHOULDER, 10 (250) OR HMA SHOULDER, 6 (150) (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE EXISTING GUARDRAIL HEIGHT SHALL TRANSITION TO MATCH THE NEW TERMINAL END SECTION AND SHALL BE PAID FOR AS VERTICAL ADJUSTMENT OF EXISTING GUADRAIL.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER**

[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



**DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

AGGREGATE SHOULDER, 10 (250) WILL BE PAID ACCORDING TO SECTION 481.

HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID ACCORDING TO SECTION 482.

COMB. CONC. C&G, STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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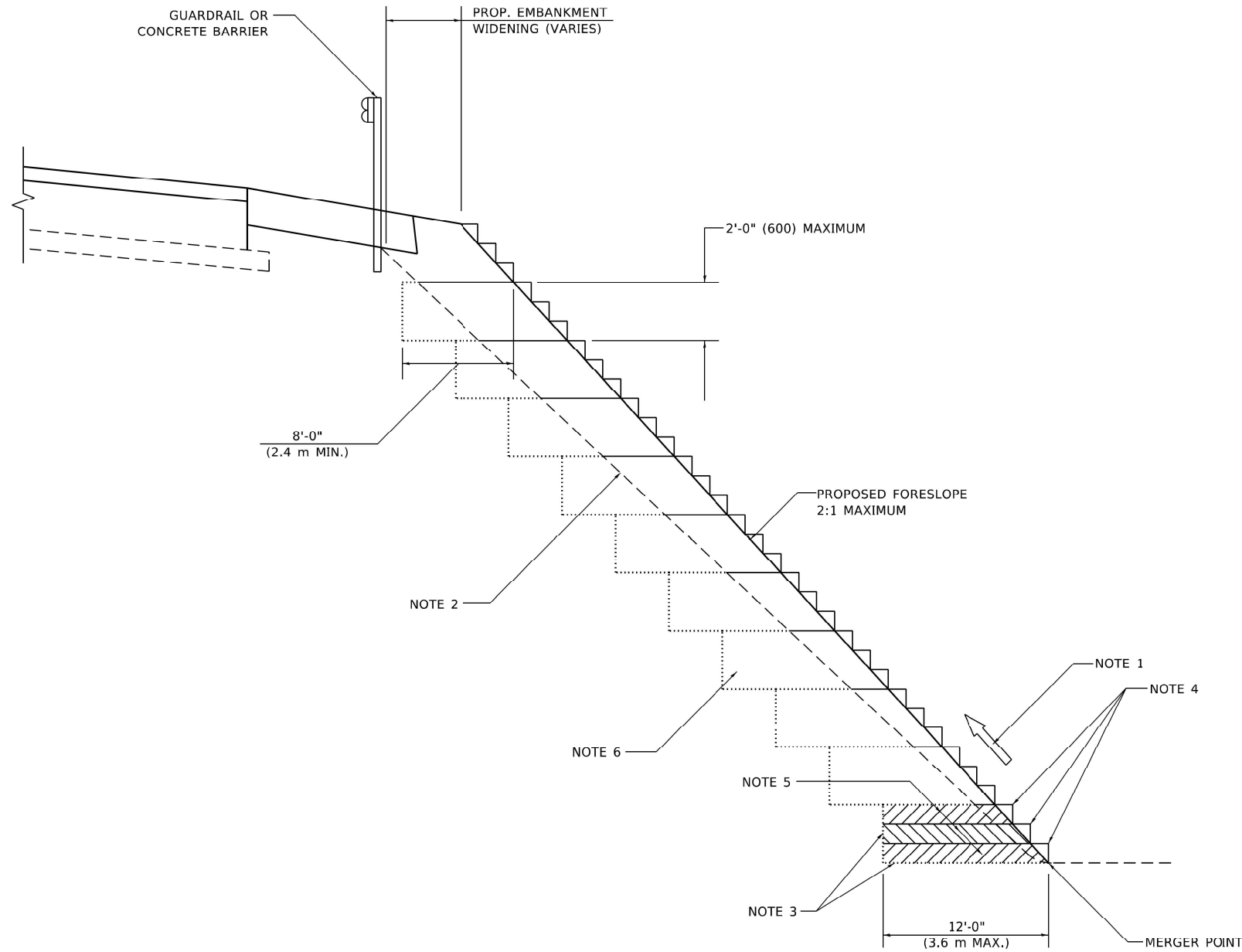
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PLOT DATE = 4/2/2024	DATE - 04/02/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

D1 STANDARD - DETAILS FOR DEPRESSED CURB & GUTTER
AND SHOULDER TREATMENT AT TBT TY. 1 SPL.

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	96
BD-34			CONTRACT NO. 62W48	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

GENERAL NOTES

1. CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
2. EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
3. BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
4. TRIM TO FINAL SLOPE.
5. EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.

BASIS OF PAYMENT

1. EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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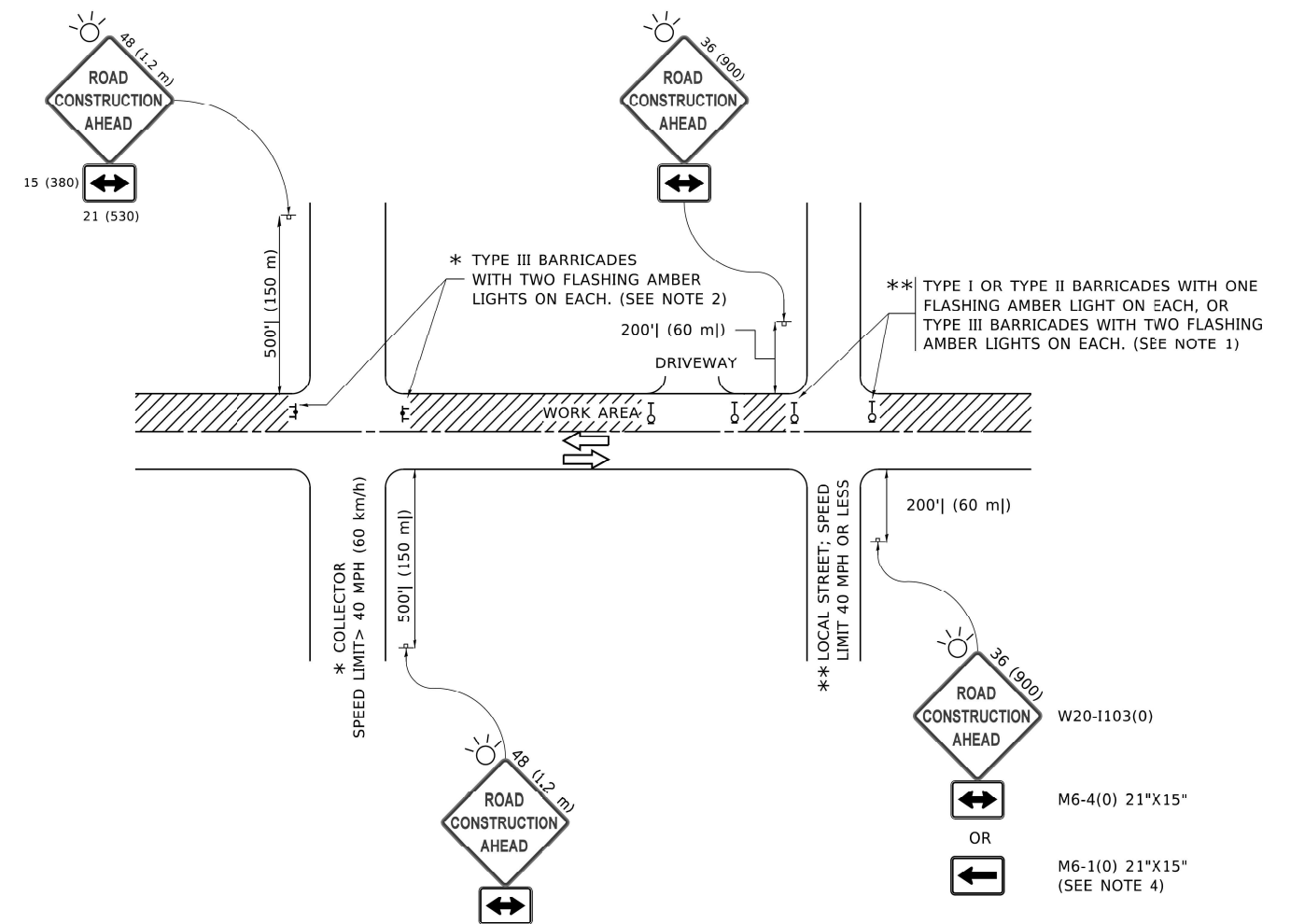
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PLOT DATE = 4/2/2024	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**D1 STANDARD - BENCHING DETAIL
FOR EMBANKMENT WIDENING**

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	97
	BD-51		CONTRACT NO. 62W48	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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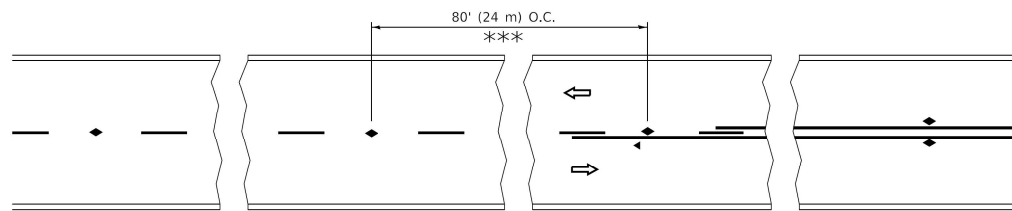
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PLOT SCALE = 2.0000' / in.	DRAWN - KLM	REVISED -
PLOT DATE = 4/2/2024	CHECKED - BMC	REVISED -
	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**D1 STANDARD – TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

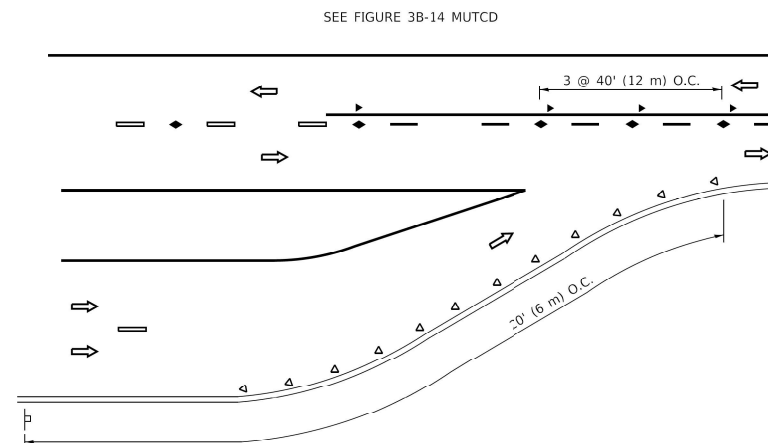
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 62W48	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

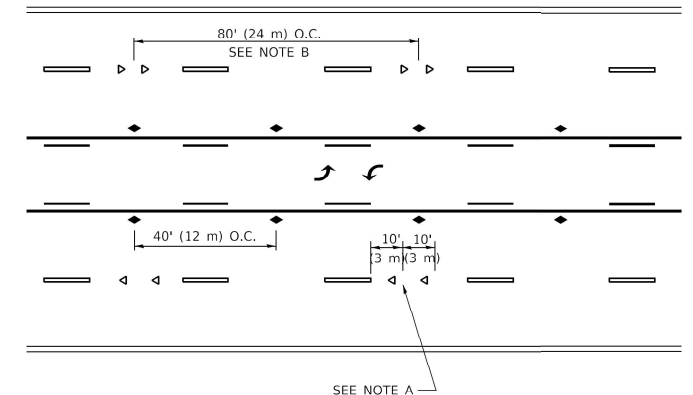


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

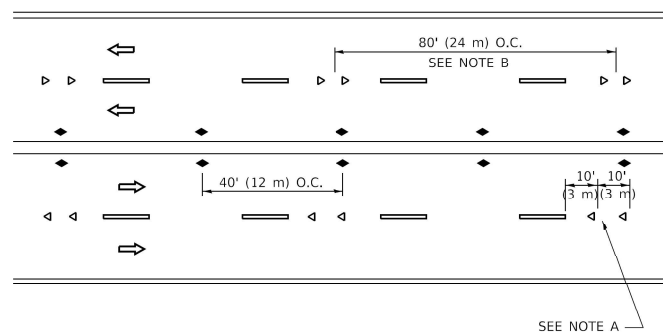
TWO-LANE/TWO-WAY



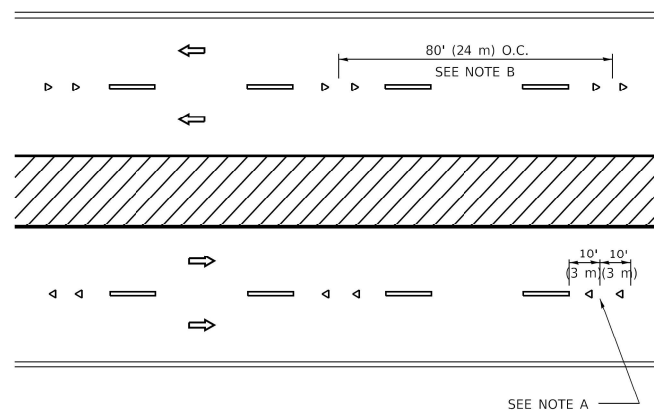
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

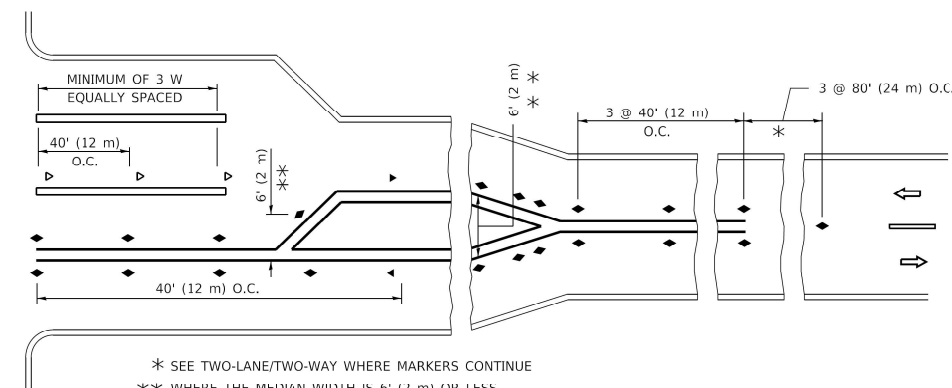
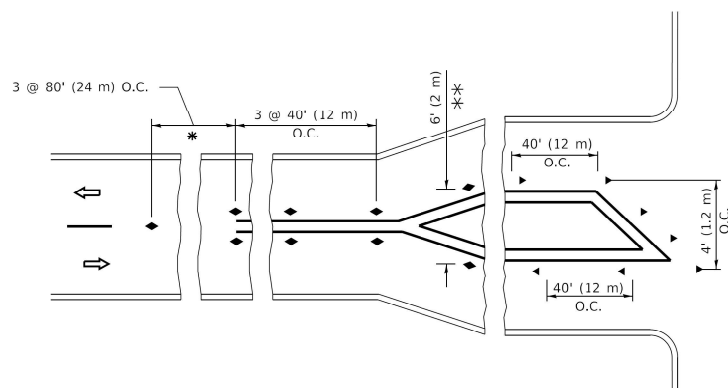
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



TURN LANES

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pwworking\user1\dwg\03232\6067153\5HT_Rdwy_Detail-08.dgn



USER NAME = ken.moy
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 PLOT DATE = 4/2/2024

DESIGNED - KLM
 DRAWN - KLM
 CHECKED - BMC
 DATE - 04/02/2024

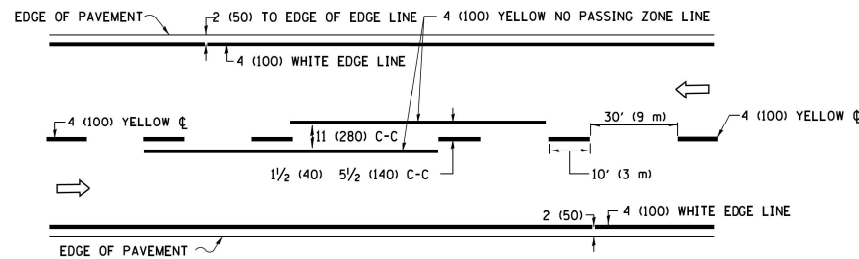
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

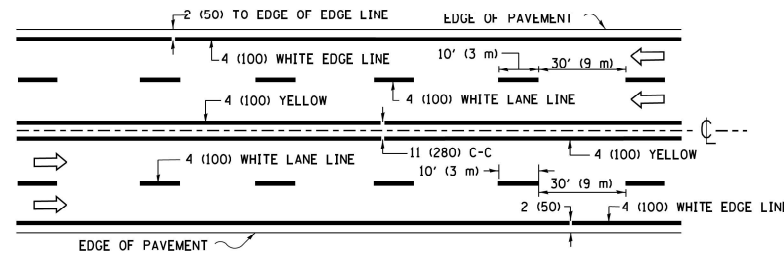
D1 STANDARD - TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

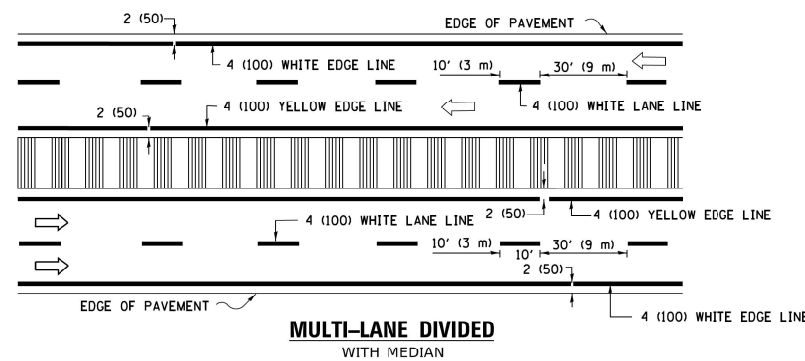
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3730	15-00131-01-BR	COOK	109	99
TC-11		CONTRACT NO. 62W48		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

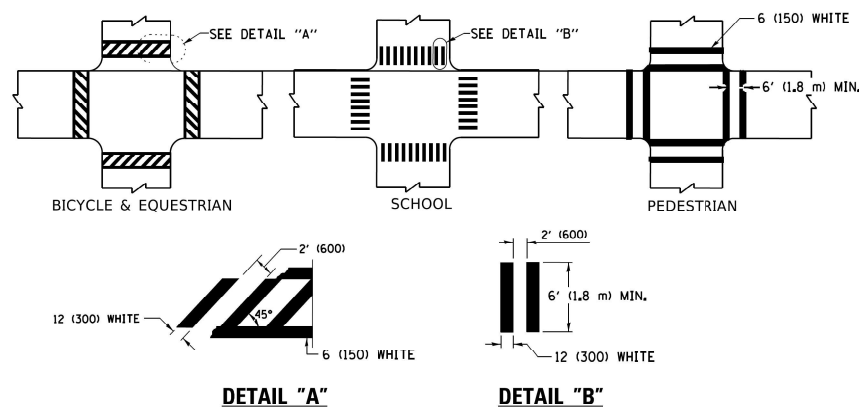


MULTI-LANE UNDIVIDED



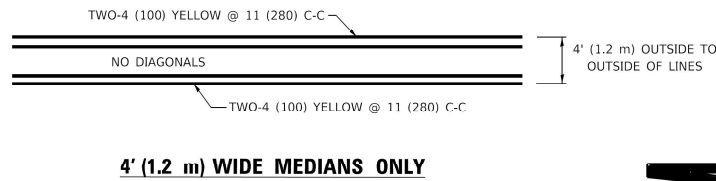
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

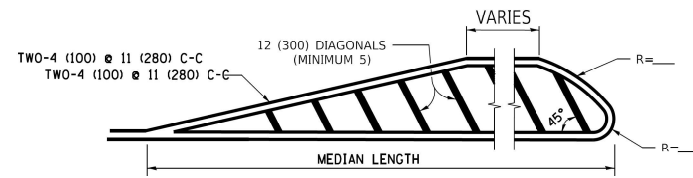


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

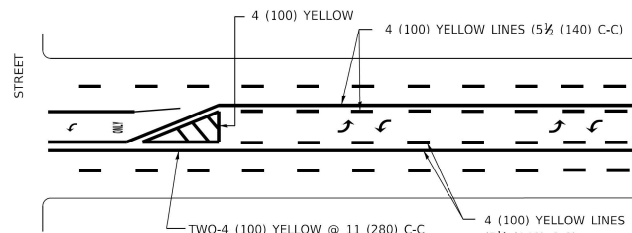


4' (1.2 m) WIDE MEDIANS ONLY



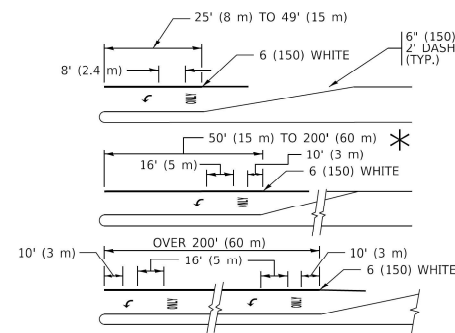
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



MEDIAN WITH TWO-WAY LEFT TURN LANE

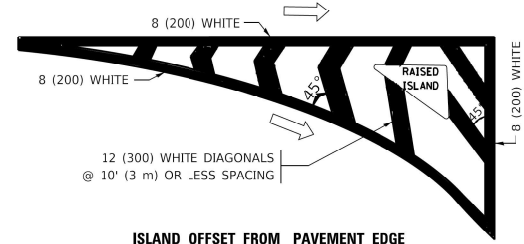
TYPICAL PAINTED MEDIAN MARKING



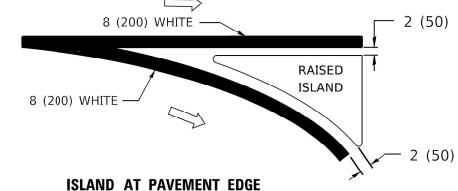
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

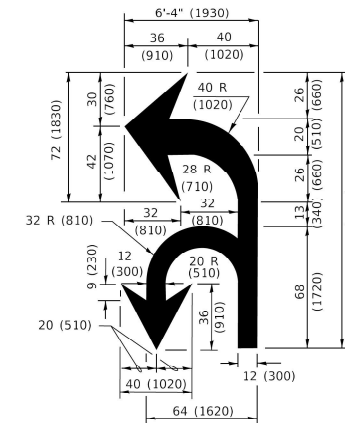


ISLAND OFFSET FROM PAVEMENT EDGE

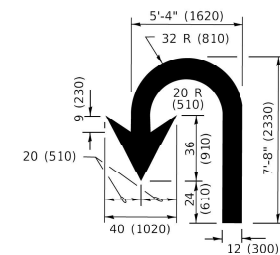


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15' (4.5 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

D1 STANDARD - TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	100
TC-13		CONTRACT NO. 62W48		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

USER NAME = ken.moy	DESIGNED - KLM	REVISED -
PLOT SCALE = 2.0000' / in.	DRAWN - KLM	REVISED -
PLOT DATE = 4/2/2024	CHECKED - BMC	REVISED -
	DATE - 04/02/2024	REVISED -

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

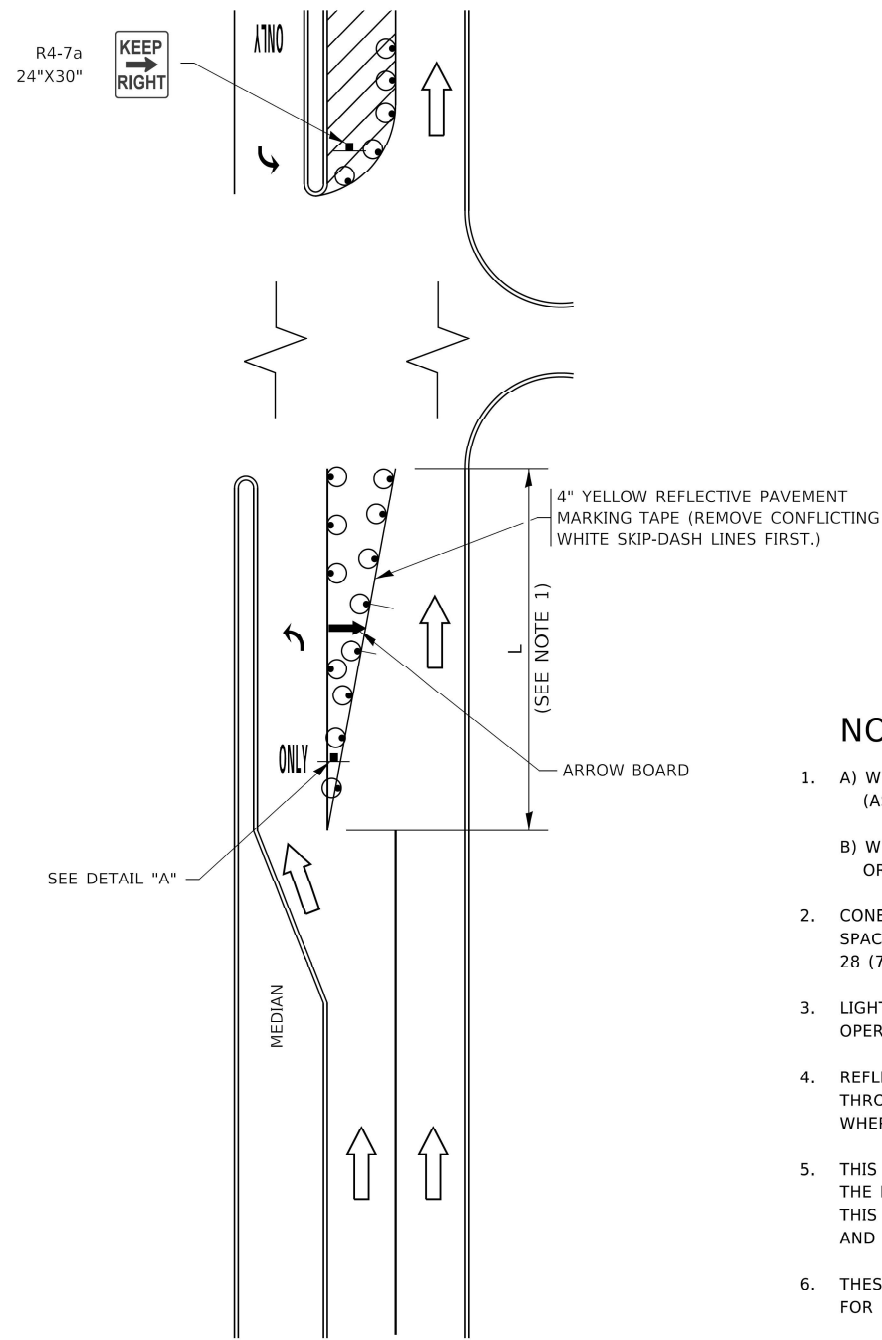


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

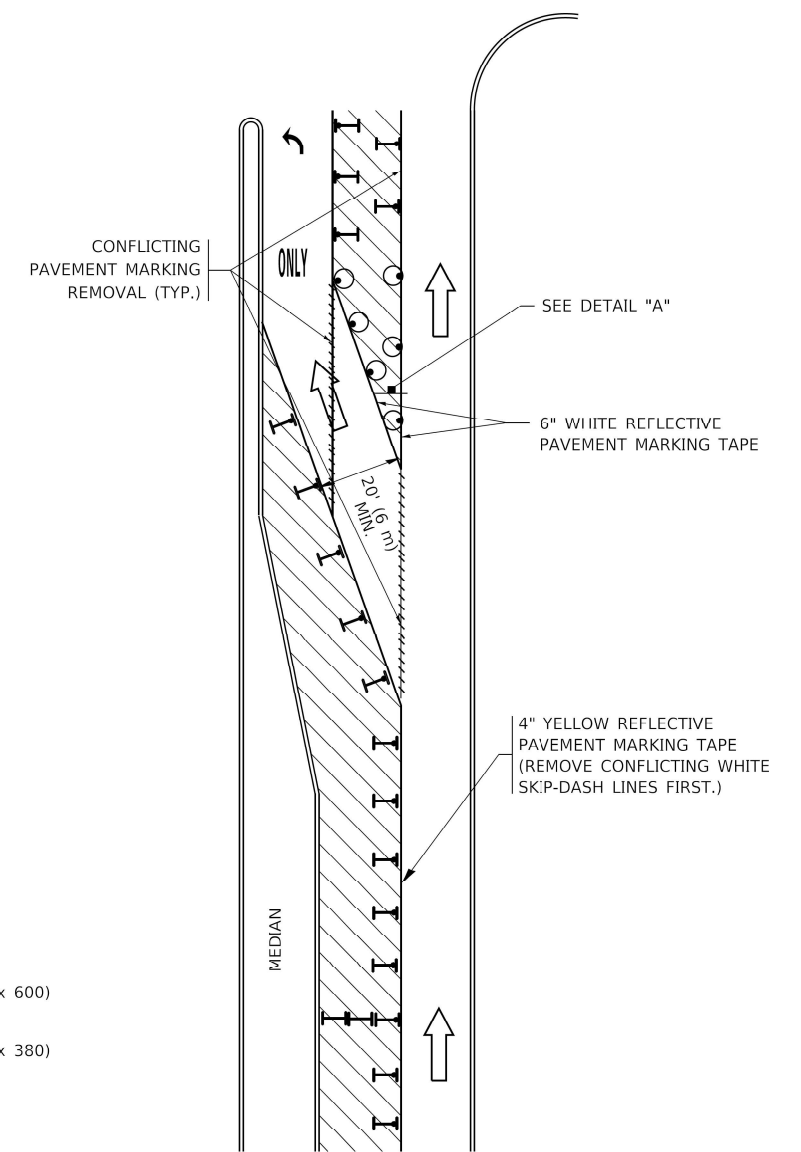


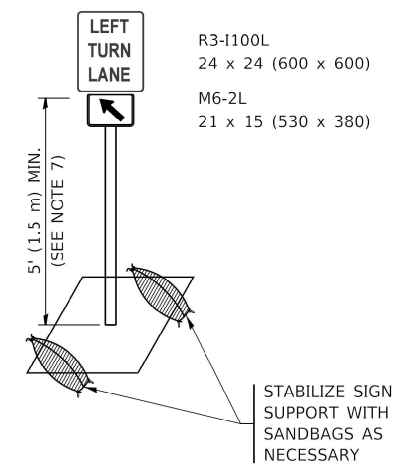
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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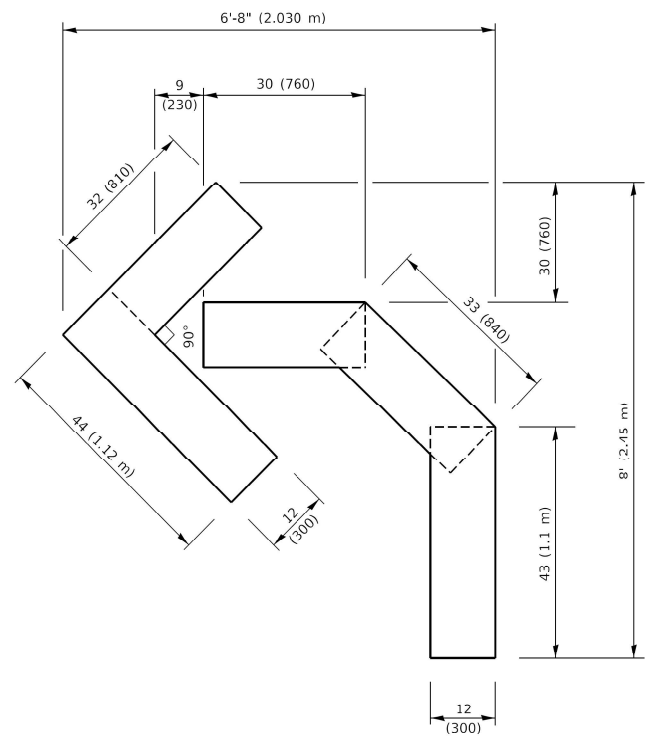


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PLOT DATE = 4/2/2024	CHECKED - BMC	REVISED -
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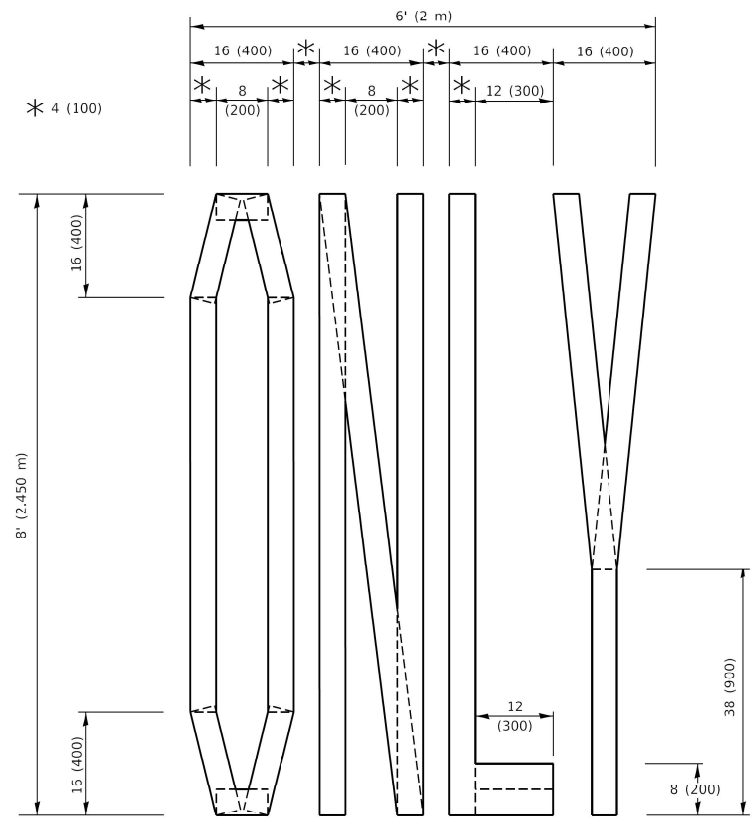
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

D1 STANDARD - TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO.	OF SHEETS	STA. TO STA.

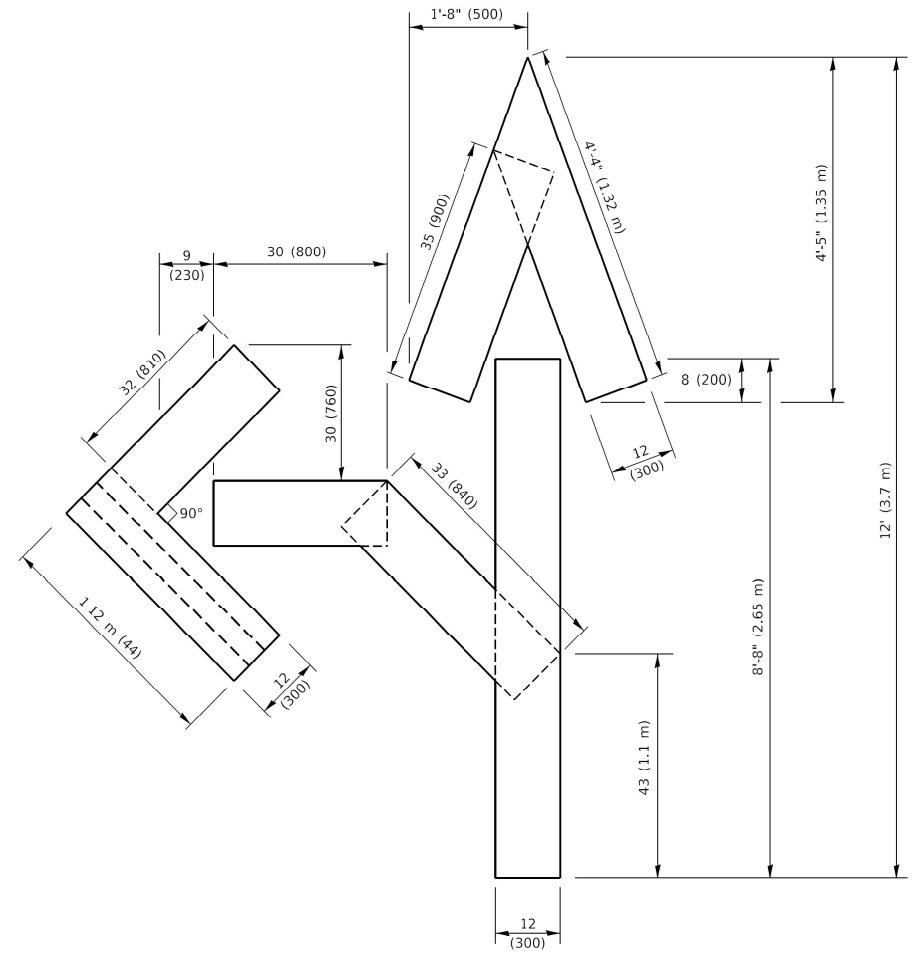
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3730	15-00131-01-BR	COOK	109	101
TC-14			CONTRACT NO. 62W48	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

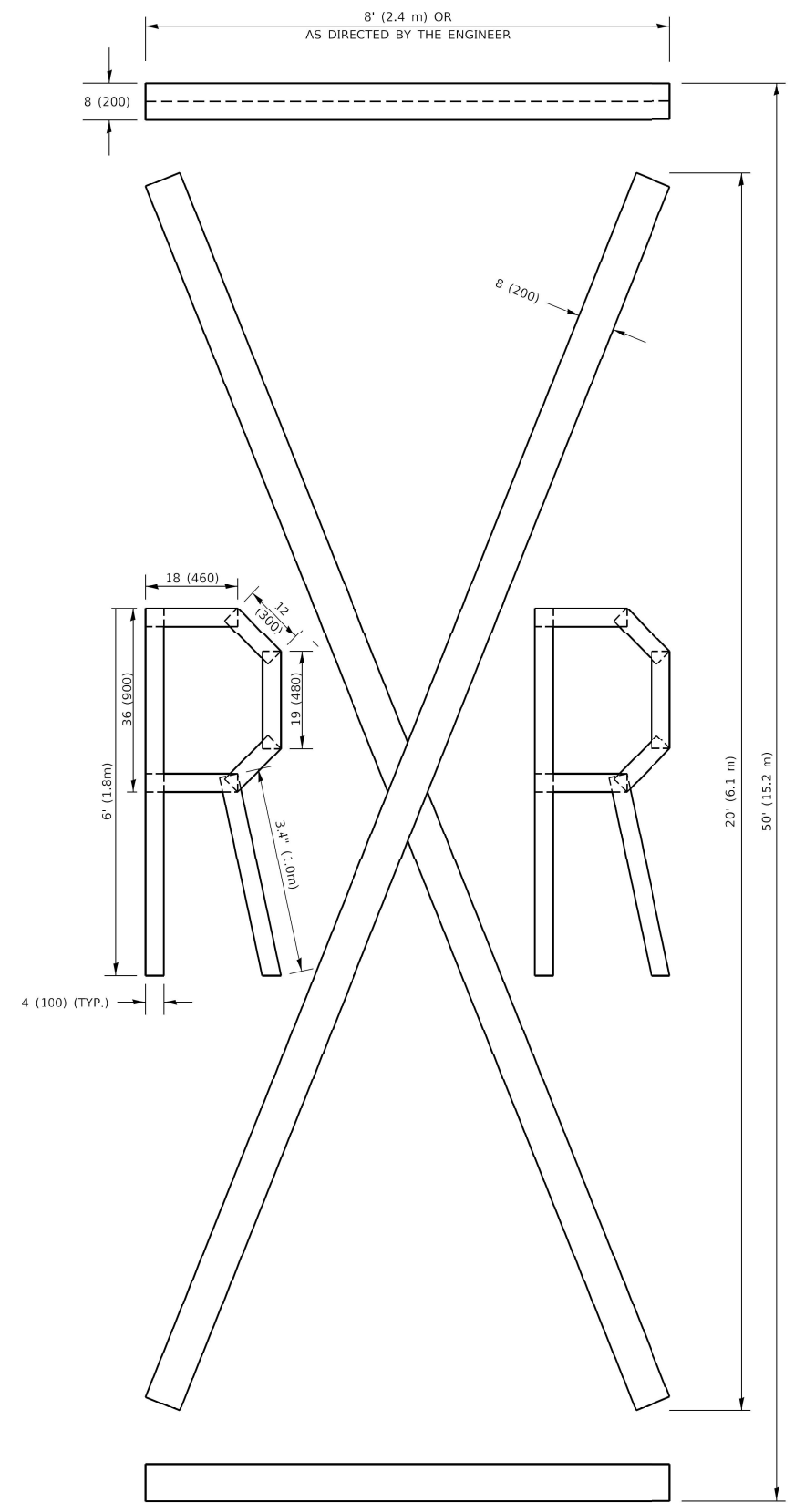


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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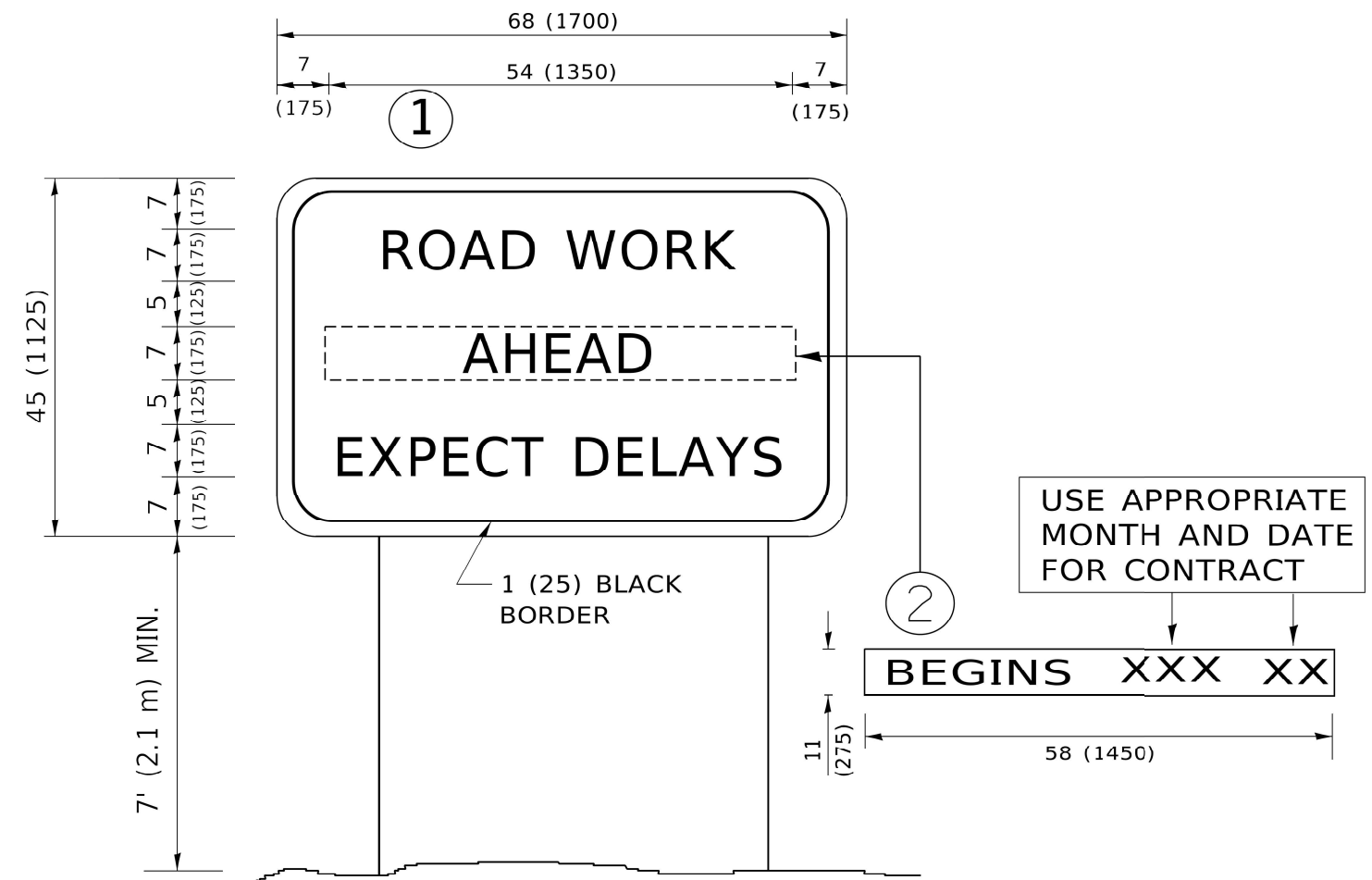
REVISED -
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 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**D1 STANDARD - SHORT TERM PAVEMENT
 MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	102
TC-16			CONTRACT NO. 62W48	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pwworking\us1\alma02322\6067153\5HT_Rdwy_Detail-12.dgn



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	DRAWN - KLM	REVISED -
PLOT SCALE = 2.0000' / in.	CHECKED - BMC	REVISED -
PLOT DATE = 4/2/2024	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**D1 STANDARD - ARTERIAL ROAD
INFORMATION SIGN**

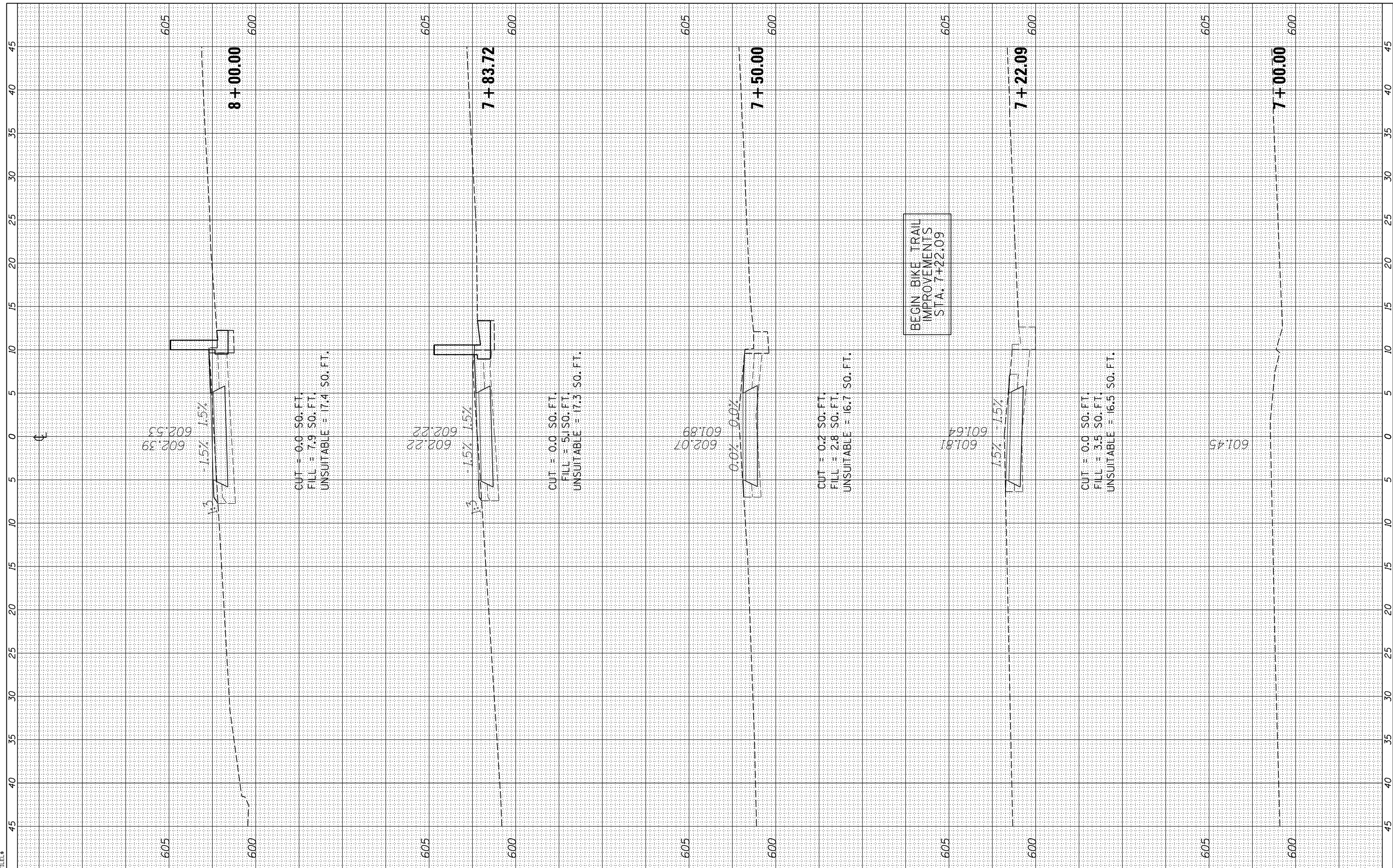
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3730	15-00131-01-BR	COOK	109	103
TC-22			CONTRACT NO. 62W48	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FINL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NOTE BOOK	TEMPLATE AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NOTE BOOK	TEMPLATE AREAS CHECKED		

FILE NAME = #FILEL#



CUT = 0.0 SQ. FT.
 FILL = 7.9 SQ. FT.
 UNSUITABLE = 17.4 SQ. FT.

CUT = 0.0 SQ. FT.
 FILL = 5.1 SQ. FT.
 UNSUITABLE = 17.3 SQ. FT.

CUT = 0.2 SQ. FT.
 FILL = 2.8 SQ. FT.
 UNSUITABLE = 16.7 SQ. FT.

CUT = 0.0 SQ. FT.
 FILL = 3.5 SQ. FT.
 UNSUITABLE = 16.5 SQ. FT.

BEGIN BIKE TRAIL
 IMPROVEMENTS
 STA. 7+22.09



USER NAME = ken.moy	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 4/2/2024	CHECKED -	REVISED -
	DATE = 04/02/2024	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ILLINOIS 1 (HALSTED STREET) BIKE TRAIL AT SN 016-0194
 CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 7+00.00 TO STA. 8+00.00

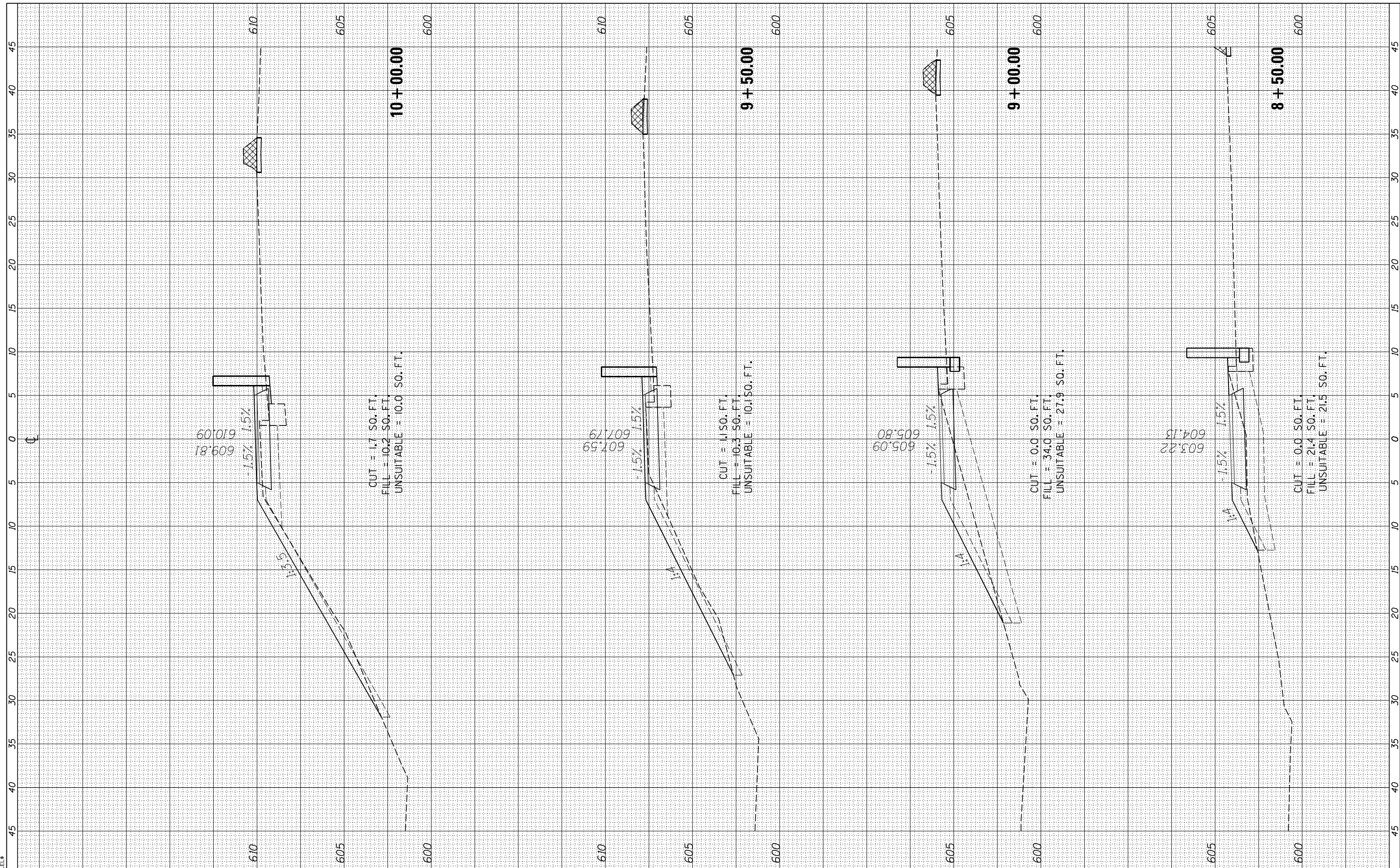
F.A.U. RTE. 3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 104
			CONTRACT NO. 62W48	

ILLINOIS FED. AID PROJECT

FINIAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

FILE NAME = #FILEL#



USER NAME = ken.moy
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE - 04/02/2024

REVISIONS
 REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ILLINOIS 1 (HALSTED STREET) BIKE TRAIL AT SN 016-0194
 CROSS SECTIONS

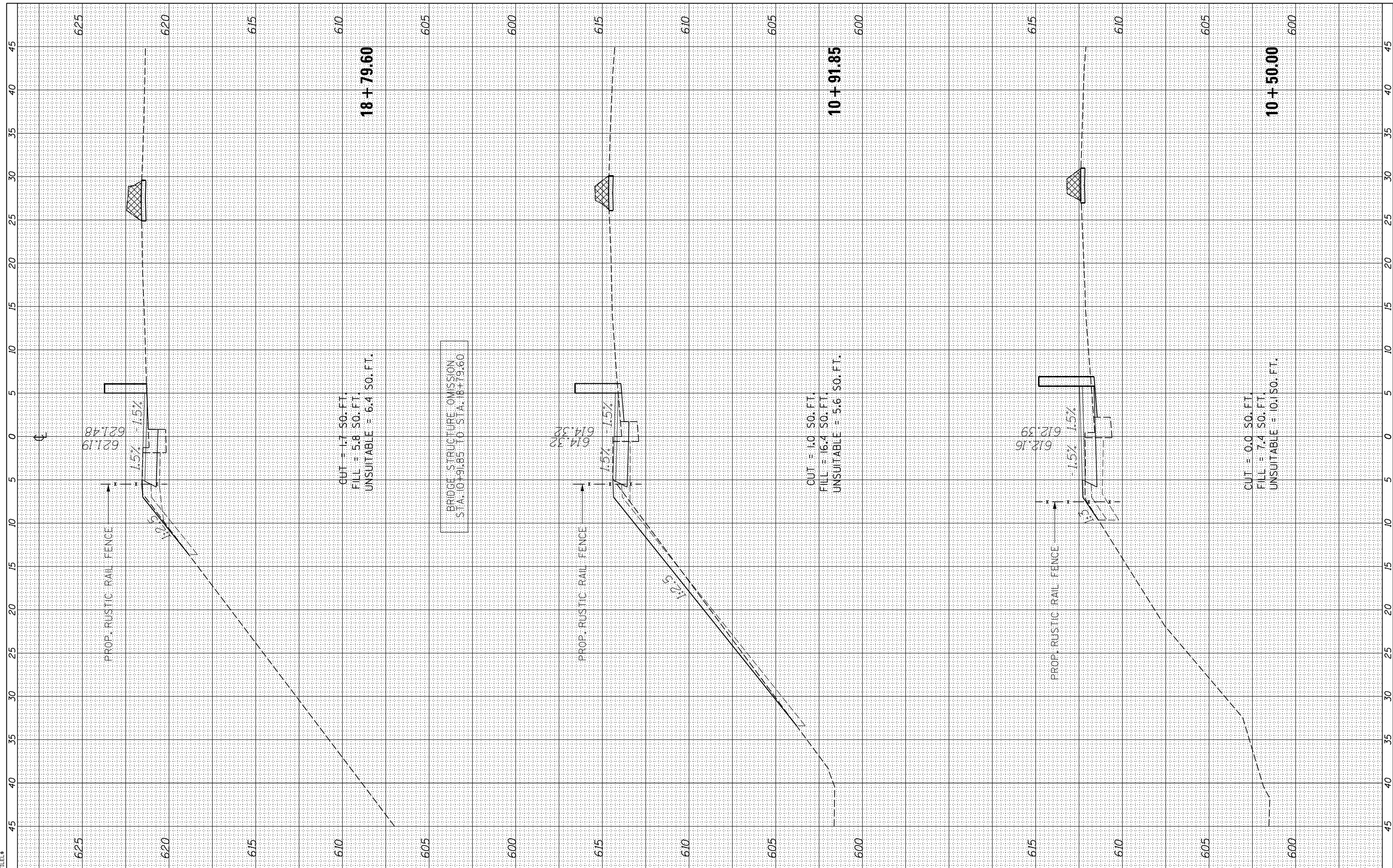
SCALE: SHEET OF SHEETS STA. 8+50.00 TO STA. 10+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	105
CONTRACT NO. 62W48			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

FILE NAME = #FILEL#



USER NAME = ken.moy	DESIGNED -	REVISED -
PLOT SCALE = 10,0000' / 1in.	DRAWN -	REVISED -
PLOT DATE = 4/2/2024	CHECKED -	REVISED -
	DATE = 04/02/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

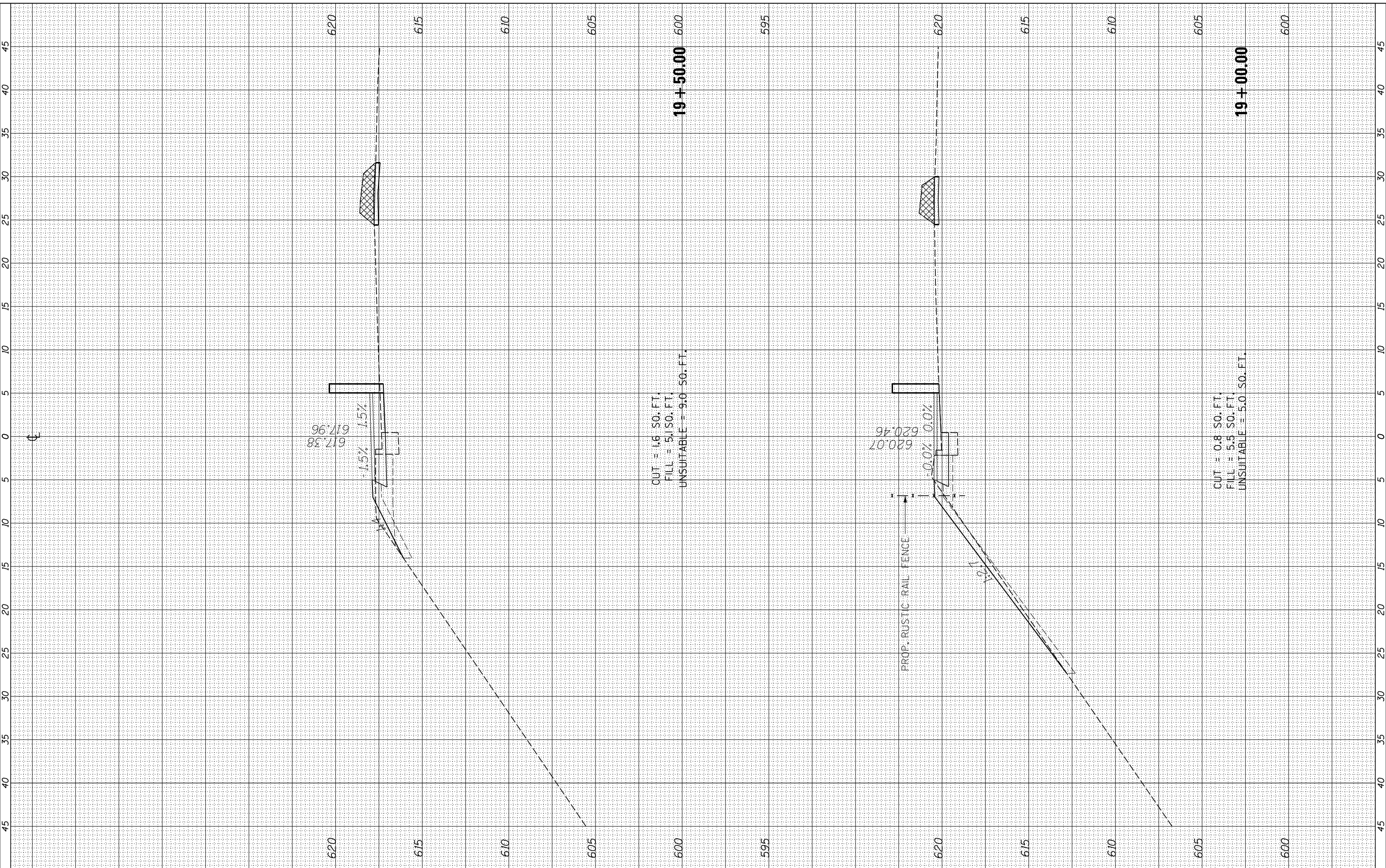
ILLINOIS 1 (HALSTED STREET) BIKE TRAIL AT SN 016-0194
CROSS SECTIONS
SCALE: SHEET OF SHEETS STA. 10+50.00 TO STA. 18+79.58

F.A.U. RTE. 3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 106
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

FINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE

FILE NAME = #FILEL#



USER NAME = ken.moy	DESIGNED -	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 4/2/2024	CHECKED -	REVISED -
	DATE - 04/02/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ILLINOIS 1 (HALSTED STREET) BIKE TRAIL AT SN 016-0194
CROSS SECTIONS**

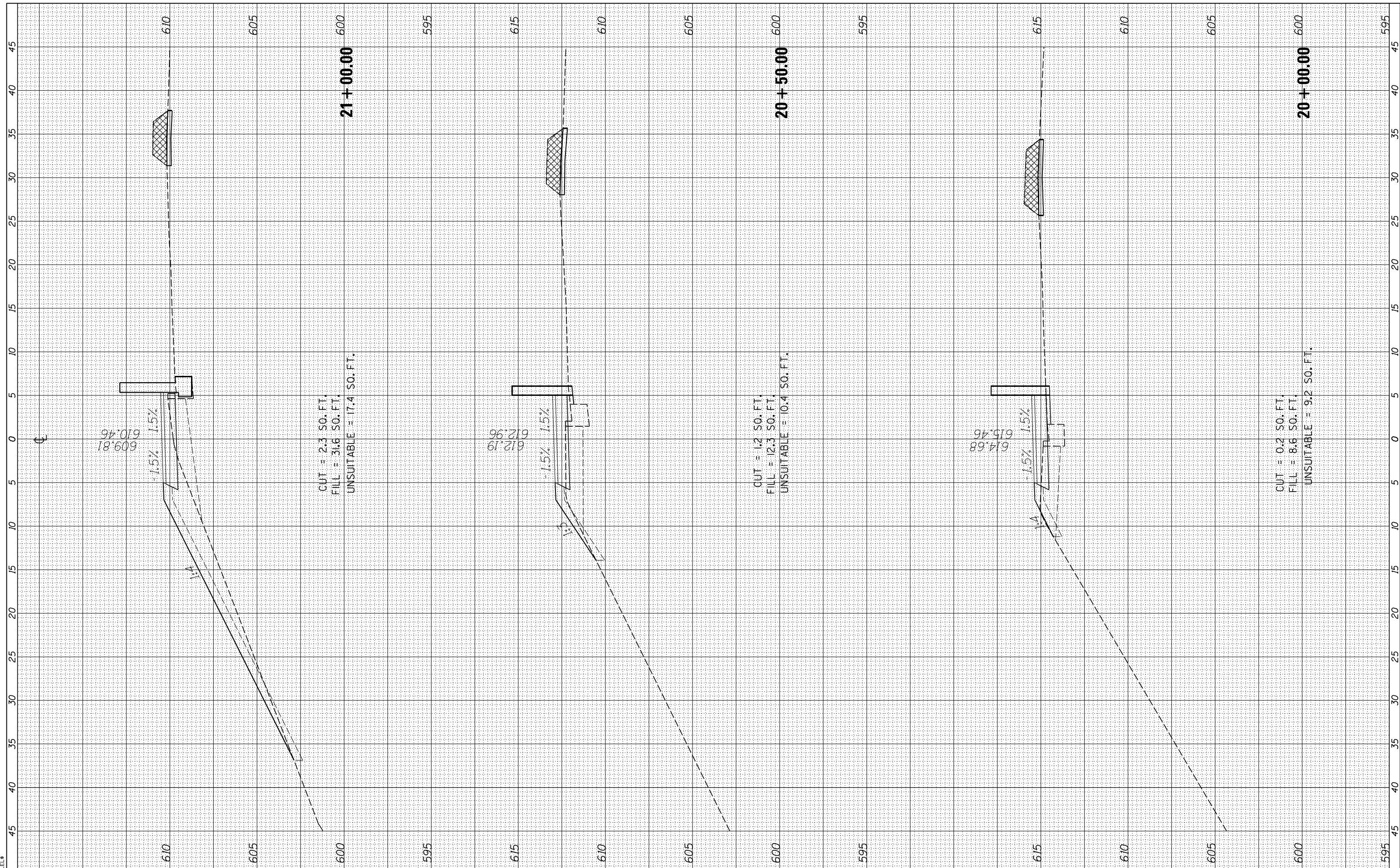
SCALE: SHEET OF SHEETS STA. 19+00.00 TO STA. 19+50.00

F.A.U. RTE. 3730	SECTION 15-00131-01-BR	COUNTY COOK	TOTAL SHEETS 109	SHEET NO. 107
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W48	

FINISH SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

FILE NAME = #FILEL#



USER NAME = ken.moy
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE = 04/02/2024

DESIGNED -
 DRAWN -
 CHECKED -
 DATE = 04/02/2024

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ILLINOIS 1 (HALSTED STREET) BIKE TRAIL AT SN 016-0194
 CROSS SECTIONS

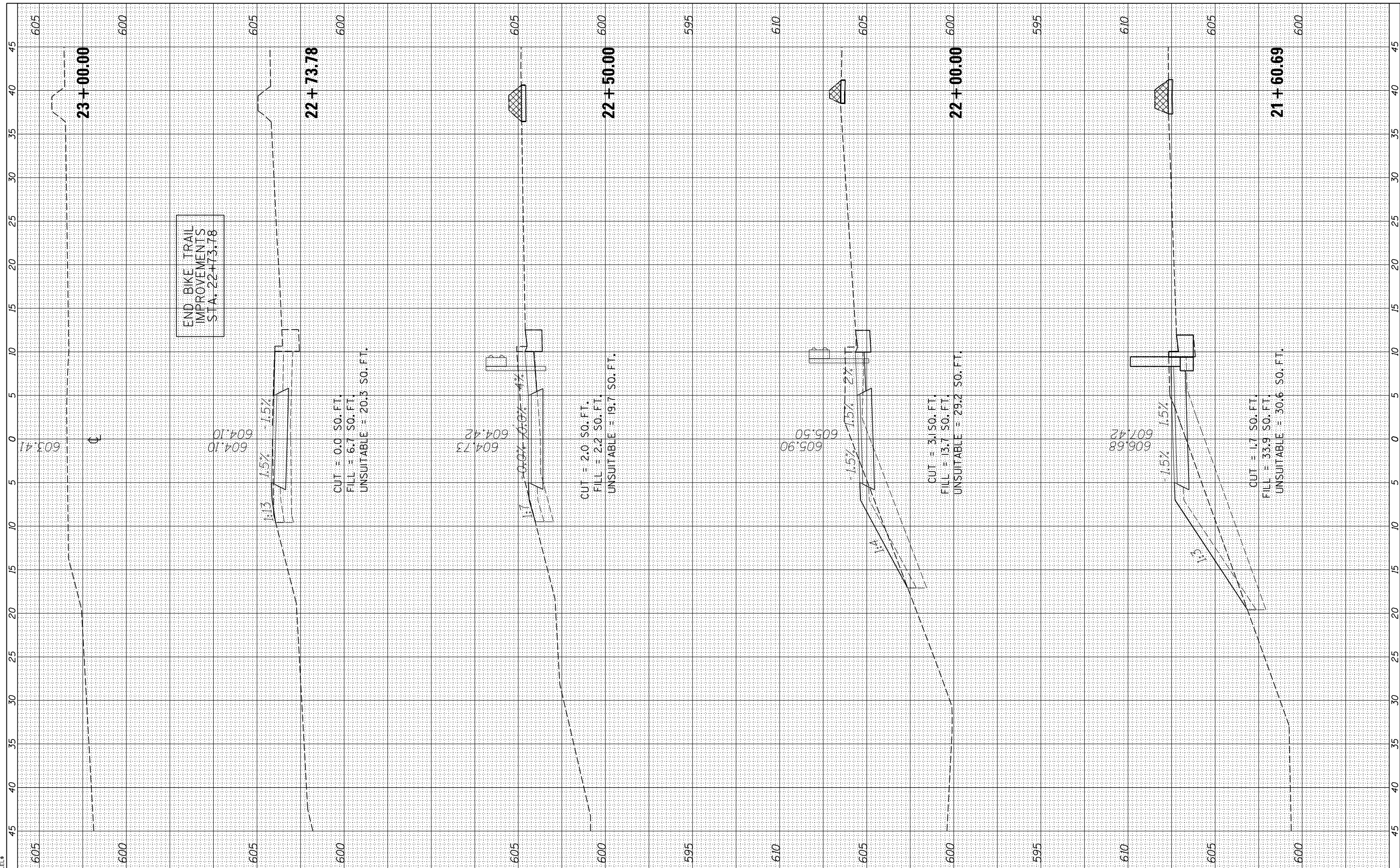
SCALE: SHEET OF SHEETS STA. 20+00.00 TO STA. 21+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	108
CONTRACT NO. 62W48			ILLINOIS FED. AID PROJECT	

FINIAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

FILE NAME = #FILEL#



USER NAME = ken.moy
 PLOT SCALE = 10.0000" / 1"
 PLOT DATE = 4/2/2024

DESIGNED -
 DRAWN -
 CHECKED -
 DATE - 04/02/2024

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ILLINOIS 1 (HALSTED STREET) BIKE TRAIL AT SN 016-0194
 CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 21+60.69 TO STA. 23+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3730	15-00131-01-BR	COOK	109	109
CONTRACT NO. 62W48			ILLINOIS FED. AID PROJECT	