

| F.A.U. RTE. | SECTION     | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-------------|--------|--------------|-----------|
| 1504        | 2007-016-RS | COOK   | 14           | 1         |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

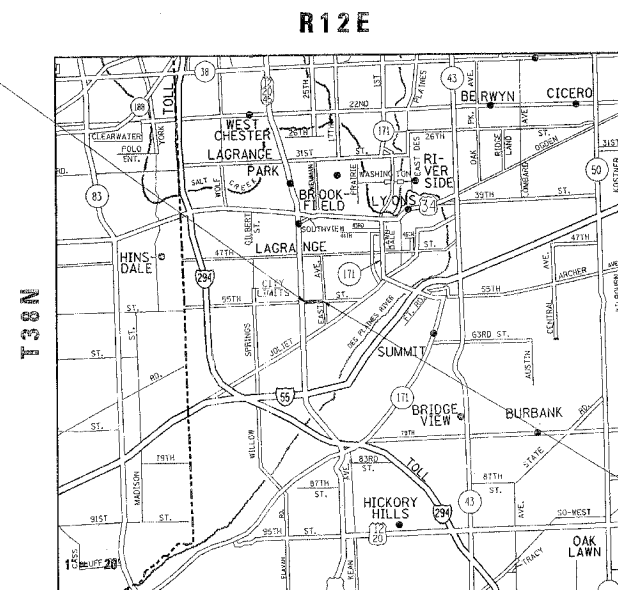
**PROPOSED  
HIGHWAY PLANS**

**F.A.U. ROUTE 1504: 55TH STREET  
US 12/20/45 TO EAST AVENUE/EVERLY AVENUE  
SECTION: 2007-016-RS  
RESURFACING (MAINTENANCE)  
COOK COUNTY  
C-91-256-07**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED IN THE  
VILLAGE OF LAGRANGE

IMPROVEMENT BEGINS  
STA. 101+55

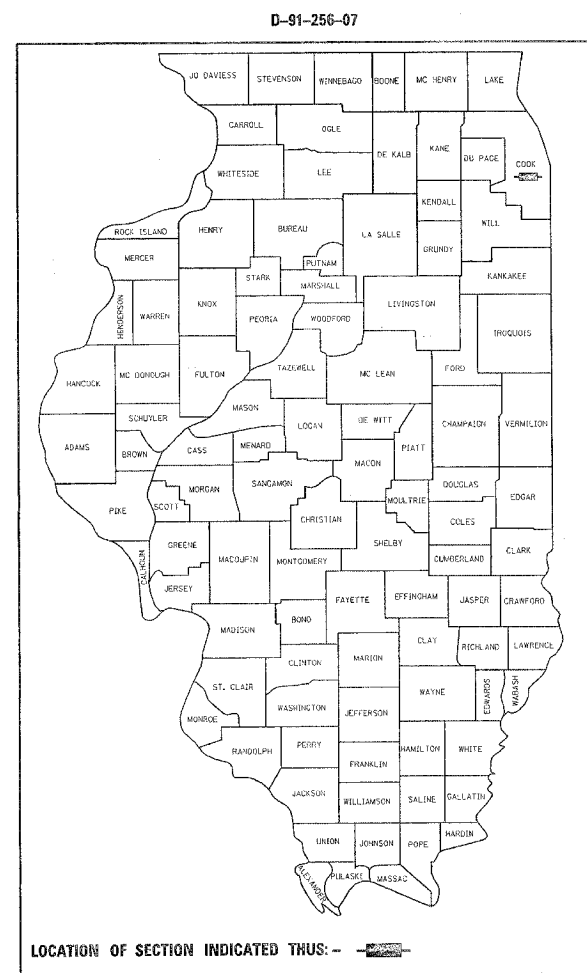


TRAFFIC DATA  
2006 ADT = 23000  
POSTED SPEED LIMIT = 40 MPH

IMPROVEMENT ENDS  
STA. 125+58

LYONS TOWNSHIP

GROSS & NET LENGTH OF IMPROVEMENT = 2,403 FT = 0.455 MILE



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

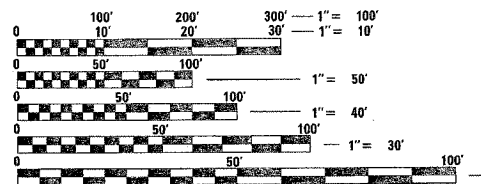
SUBMITTED April 12 2007  
Diane O'Keefe/CD  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 11 2007  
Eric E. Haran/ED  
INTERIM ENGINEER OF DESIGN AND ENVIRONMENT

May 11 2007  
Milton R. Sauer/PE/ED  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

DISTRICT ONE DESIGN PLAN PREPARATION ENGINEER: KEN ENG/LONG TRAN (847) 705-4240



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

CONTRACT NO. 60C70



|                       |                     |             |                 |             |
|-----------------------|---------------------|-------------|-----------------|-------------|
| F.A.U. RTE. 1504      | SECTION 2007-016-RS | COUNTY COOK | TOTAL SHEETS 14 | SHEET NO. 3 |
| FED. ROAD DIST. NO. 1 |                     | ILLINOIS    | HIGHWAY PROJECT |             |

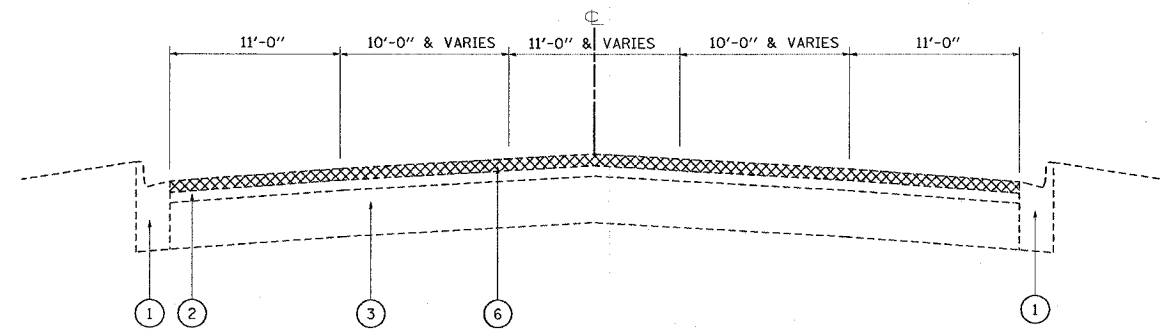
| SUMMARY OF QUANTITIES |  |        | 100% STATE       | CONSTRUCTION TYPE CODE |  |  |  |  |
|-----------------------|--|--------|------------------|------------------------|--|--|--|--|
| CODE NO               | ITEM   | UNIT   | TOTAL QUANTITIES | URBAN                  |  |  |  |  |
|                       |  |        |                  | 1000                   |  |  |  |  |
| 20201006              | GRADING AND SHAPING SHOULDERS                                | UNIT   | 12               | 12                     |  |  |  |  |
| 40600200              | BITUMINOUS MATERIALS (PRIME COAT)                            | TON    | 6                | 6                      |  |  |  |  |
| 40600300              | AGGREGATE (PRIME COAT)                                       | TON    | 30               | 30                     |  |  |  |  |
| 40600400              | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS                   | TON    | 20               | 20                     |  |  |  |  |
| 40600982              | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT                 | SQ YD  | 50               | 50                     |  |  |  |  |
| 40601005              | HOT-MIX ASPHALT REPLACEMENT OVER PATCHES                     | TON    | 100              | 100                    |  |  |  |  |
| 40603595              | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90     | TON    | 1375             | 1375                   |  |  |  |  |
| 44000159              | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"                      | SQ YD  | 14045            | 14045                  |  |  |  |  |
| 44001700              | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT   | 350              | 350                    |  |  |  |  |
| 44002220              | HOT-MIX ASPHALT REMOVAL OVER PATCHES, 5"                     | SQ YD  | 652              | 652                    |  |  |  |  |
| 44201765              | CLASS D PATCHES, TYPE II, 10 INCH                            | SQ YD  | 377              | 377                    |  |  |  |  |
| 44201769              | CLASS D PATCHES, TYPE III, 10 INCH                           | SQ YD  | 206              | 206                    |  |  |  |  |
| 44201771              | CLASS D PATCHES, TYPE IV, 10 INCH                            | SQ YD  | 69               | 69                     |  |  |  |  |
| 48101200              | AGGREGATE SHOULDERS, TYPE B                                  | TON    | 50               | 50                     |  |  |  |  |
| 60250200              | CATCH BASINS TO BE ADJUSTED                                  | EACH   | 5                | 5                      |  |  |  |  |
| 60250400              | CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID  | EACH   | 1                | 1                      |  |  |  |  |
| 60251730              | CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 23 FRAME AND GRATE | EACH   | 2                | 2                      |  |  |  |  |
| 60251740              | CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE | EACH   | 1                | 1                      |  |  |  |  |
| 67000400              | ENGINEER'S FIELD OFFICE, TYPE A                              | CAL MO | 3                | 3                      |  |  |  |  |
| 67100100              | MOBILIZATION   | L SUM  | 1                | 1                      |  |  |  |  |
| 70102625              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701606              | L SUM  | 1                | 1                      |  |  |  |  |
| 70102635              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701              | L SUM  | 1                | 1                      |  |  |  |  |
| 70300100              | SHORT-TERM PAVEMENT MARKING                                  | FOOT   | 1000             | 1000                   |  |  |  |  |
| 70300210              | TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS             | SQ FT  | 72               | 72                     |  |  |  |  |

| SUMMARY OF QUANTITIES |  |       | 100% STATE       | CONSTRUCTION TYPE CODE |  |  |  |  |
|-----------------------|--|-------|------------------|------------------------|--|--|--|--|
| CODE NO               | ITEM   | UNIT  | TOTAL QUANTITIES | URBAN                  |  |  |  |  |
|                       |  |       |                  | 1000                   |  |  |  |  |
| 70300220              | TEMPORARY PAVEMENT MARKING - LINE 4"                       | FOOT  | 6622             | 6622                   |  |  |  |  |
| 70300240              | TEMPORARY PAVEMENT MARKING - LINE 6"                       | FOOT  | 225              | 225                    |  |  |  |  |
| 70300260              | TEMPORARY PAVEMENT MARKING - LINE 12"                      | FOOT  | 60               | 60                     |  |  |  |  |
| 70300280              | TEMPORARY PAVEMENT MARKING - LINE 24"                      | FOOT  | 80               | 80                     |  |  |  |  |
| 70301000              | WORK ZONE PAVEMENT MARKING REMOVAL                         | SQ FT | 750              | 750                    |  |  |  |  |
| * 78000100            | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS       | SQ FT | 72               | 72                     |  |  |  |  |
| * 78000200            | THERMOPLASTIC PAVEMENT MARKING - LINE 4"                   | FOOT  | 6622             | 6622                   |  |  |  |  |
| * 78000400            | THERMOPLASTIC PAVEMENT MARKING - LINE 6"                   | FOOT  | 225              | 225                    |  |  |  |  |
| * 78000600            | THERMOPLASTIC PAVEMENT MARKING - LINE 12"                  | FOOT  | 60               | 60                     |  |  |  |  |
| * 78000650            | THERMOPLASTIC PAVEMENT MARKING - LINE 24"                  | FOOT  | 80               | 80                     |  |  |  |  |
| * 78100100            | RAISED REFLECTIVE PAVEMENT MARKER                          | EACH  | 260              | 260                    |  |  |  |  |
| 78300200              | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL                  | EACH  | 260              | 260                    |  |  |  |  |
| X0322256              | TEMPORARY INFORMATION SIGNING                              | SQ FT | 144              | 144                    |  |  |  |  |
| X4067107              | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON   | 600              | 600                    |  |  |  |  |
| Z0018500              | DRAINAGE STRUCTURES TO BE CLEANED                          | EACH  | 15               | 15                     |  |  |  |  |
| * SPECIALTY ITEMS     |  |       |                  |                        |  |  |  |  |

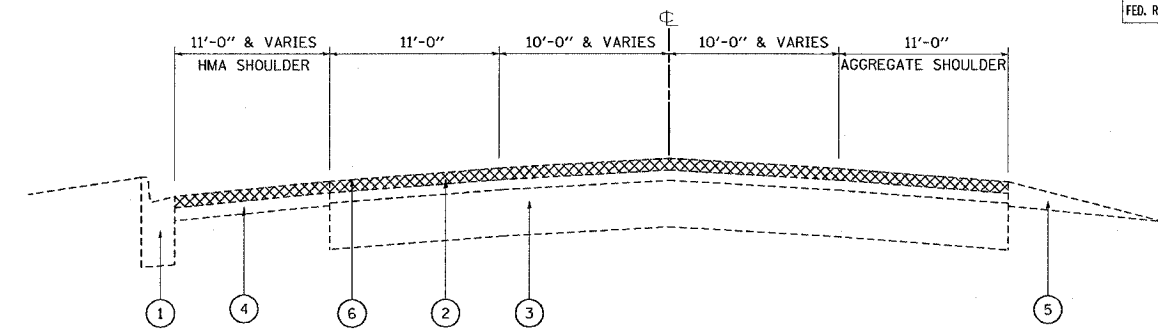
| REVISIONS |      |
|-----------|------|
| NAME      | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION  
SUMMARY OF QUANTITIES

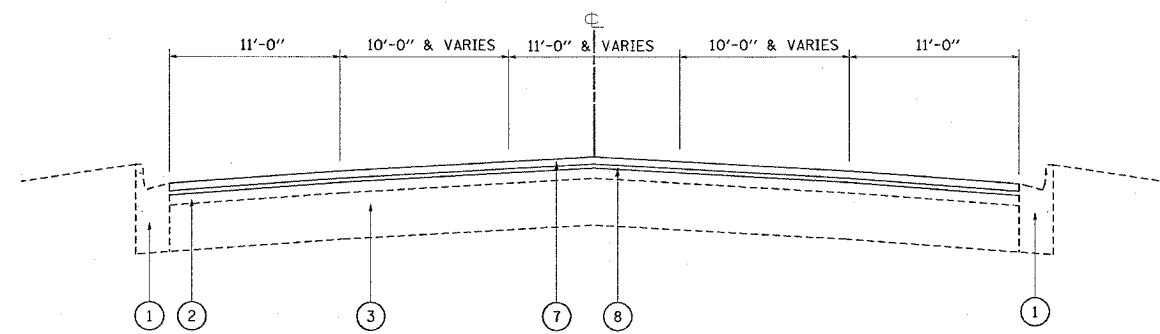
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| F.A.U. RTE.         | SECTION     | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| 1504                | 2007-016-RS | COOK                      | 14           | 4         |
| STA.                |             | TO STA.                   |              |           |
| FED. ROAD DIST. NO. |             | ILLINOIS FED. AID PROJECT |              |           |



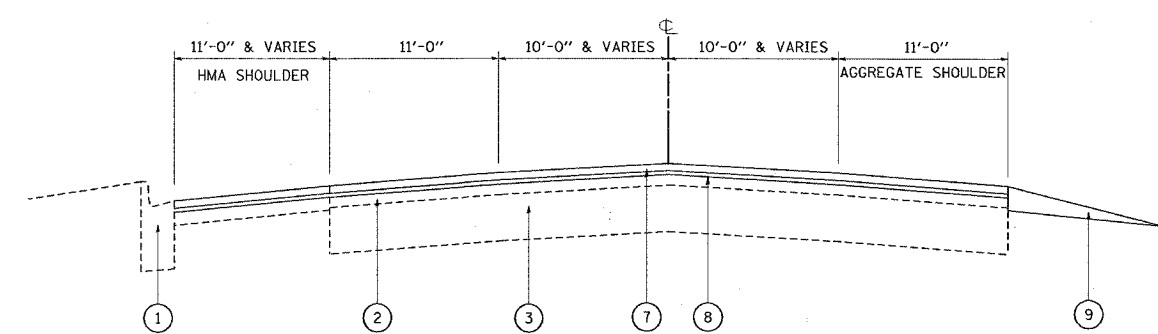
55TH STREET  
EXISTING TYPICAL SECTION  
STA 101+56 TO STA 105+00  
STA 123+00 TO STA 125+58  
LOOKING EAST



55TH STREET  
EXISTING TYPICAL SECTION  
STA 105+00 TO STA 123+00  
LOOKING EAST



55TH STREET  
PROPOSED TYPICAL SECTION  
STA 101+56 TO STA 105+00  
STA 123+00 TO STA 125+58  
LOOKING EAST



55TH STREET  
PROPOSED TYPICAL SECTION  
STA 105+00 TO STA 123+00  
LOOKING EAST

LEGEND

- ① EXISTING B-6.12 COMBINATION CONCRETE CURB AND GUTTER
- ② EXISTING HOT-MIX ASPHALT SURFACE COURSE
- ③ EXISTING CONCRETE SUB-BASE
- ④ EXISTING HOT-MIX ASPHALT SHOULDER
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
- ⑦ PROPOSED POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, MIX F, N90
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50
- ⑨ PROPOSED GRADING AND SHAPING SHOULDER

| HOT MIX ASPHALT MIXTURE REQUIREMENTS                               |                     |             |
|--|---------------------|-------------|
| MIXTURE TYPE   | AC TYPE             | AIR VOIDS   |
| POLYMERIZED HOT MIX ASPHALT SURFACE COURSE, MIX F, N90 (IL-9.5 MM) | SBS/SBR PG 70-22    | 4% @ 90 GYR |
| POLYMERIZE LEVELING BINDER (MACHINE METHOD), IL 4.75, N50          | SBS/SBR PG 76-28/22 | 4% @ 50 GYR |
| CLASS D PATCHES (HMA BINDER IL-19, MM)                             | PG 64-22/58-22 **   | 4% @ 70 GYR |
| HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19.0 MM)               | PG 64-22/58-22 **   | 4% @ 70 GYR |

THE UNIT WEIGHT USED TO CLACULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN  
\*\* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

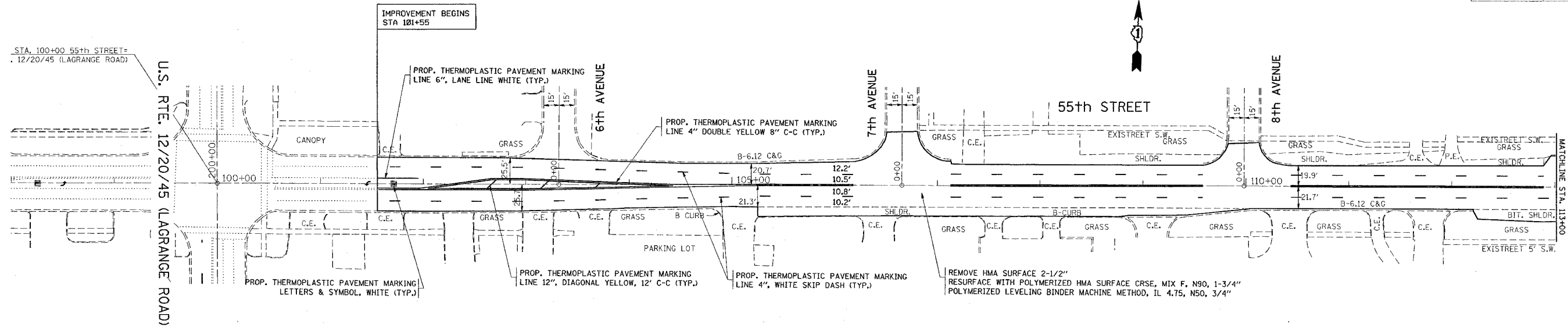
| REVISIONS |      |
|-----------|------|
| NAME      | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION  
U.S. RTE. 12/20/45 (LAGRANGE ROAD)  
@ 55TH STREET  
TYPICAL SECTIONS

SCALE: 1" = 50'  
DATE: 4/16/2007

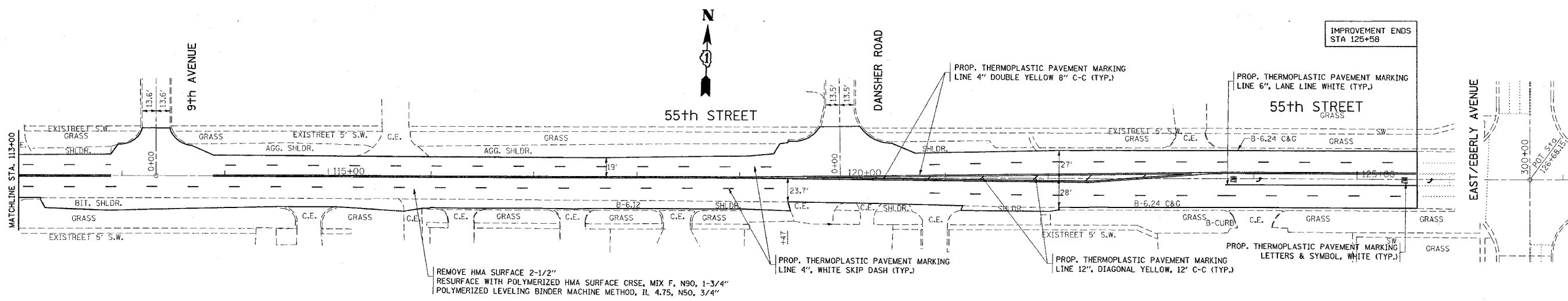
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| F.A.U. RTE.         | SECTION     | COUNTY           | TOTAL SHEETS | SHEET NO. |
| 1504                | 2007-016-RS | COOK             | 14           | 5         |
| STA.                | TO STA.     |                  |              |           |
| FED. ROAD DIST. NO. | ILLINOIS    | FED. AID PROJECT |              |           |



NOTES: ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OR THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL, (TC-13)

ALL RAISED REFLECTIVE PAVEMENT MARDERS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL." (TC-11).



NOTES: ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OR THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL, (TC-13)

ALL RAISED REFLECTIVE PAVEMENT MARDERS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKER DETAIL." (TC-11).

| REVISIONS |      |
|-----------|------|
| NAME      | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

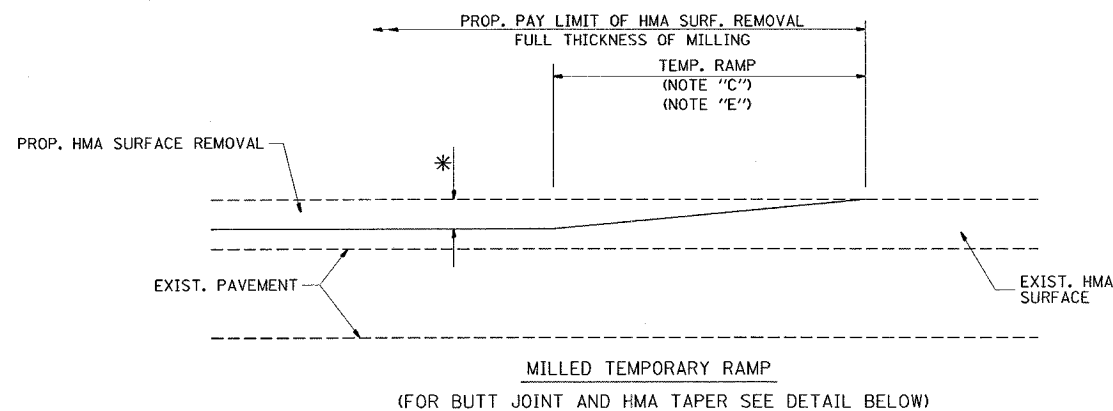
55TH STREET FROM  
U.S. RTE. 12/20/45 (LAGRANGE ROAD  
TO EAST/EBERLY AVENUE  
ROADWAY & PAVEMENT MARKING PLAN

SCALE: 1" = 50'  
DATE: 4/16/2007

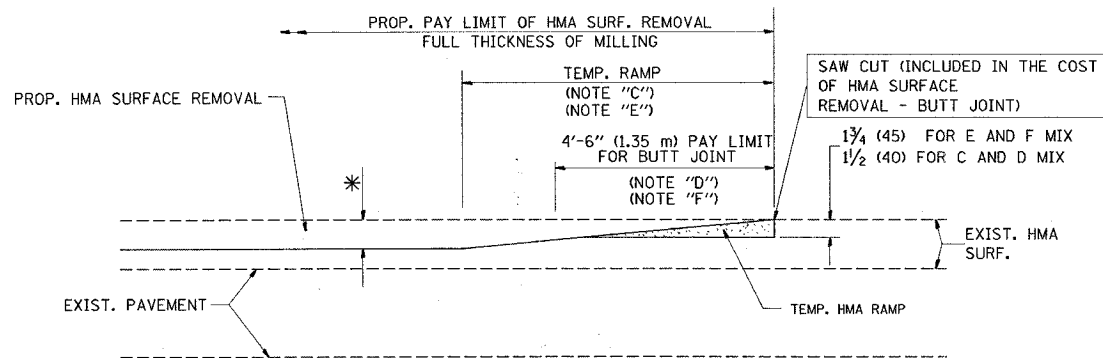
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PLOT SCALE = 50.0000 / IN.  
USER NAME = b.jurish

|                       |         |                           |              |           |
|-----------------------|---------|---------------------------|--------------|-----------|
| F.A. RTE.             | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|                       |         |                           | 14           | 6         |
| STA.                  |         | TO STA.                   |              |           |
| FED. ROAD DIST. NO. 1 |         | ILLINOIS FED. AID PROJECT |              |           |

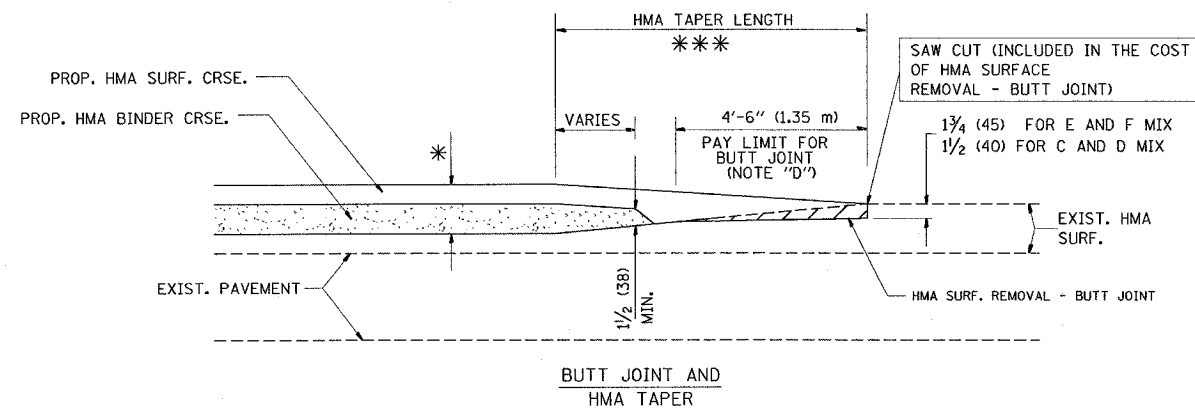


OPTION 1

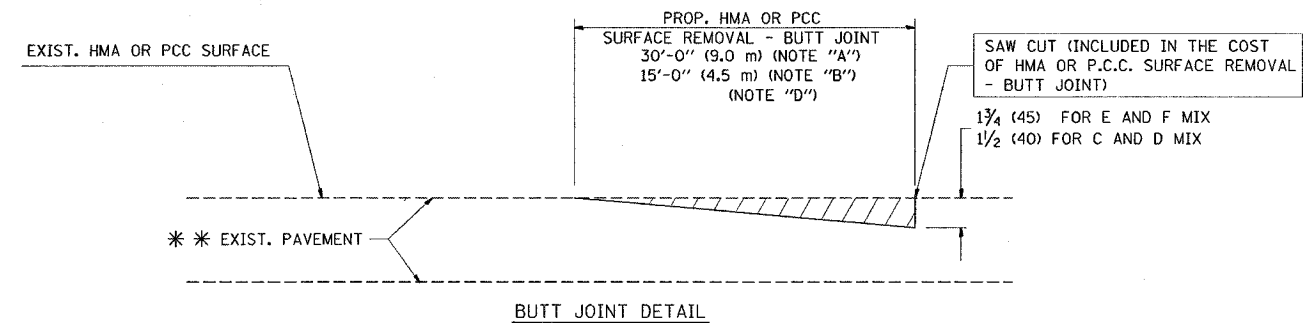


HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

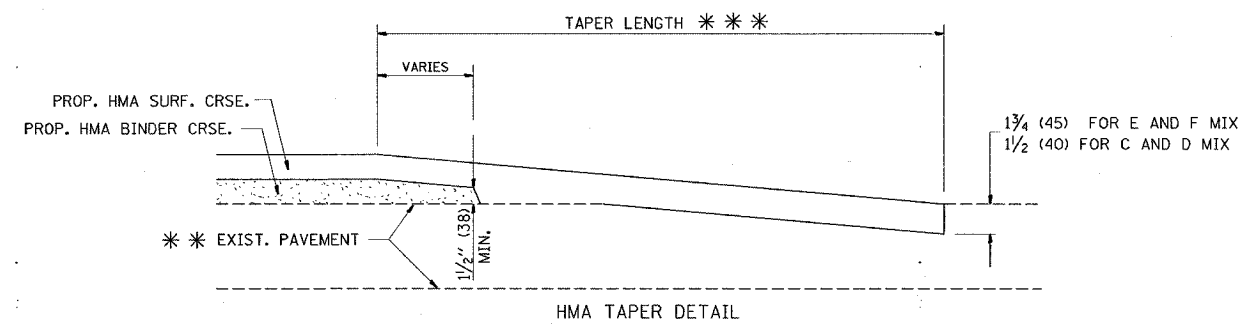
OPTION 2  
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \*\*\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

| REVISIONS  |          |
|------------|----------|
| NAME       | DATE     |
| M. DE YONG | 6-13-90  |
| M. DE YONG | 7-3-90   |
| M. DE YONG | 3-27-92  |
| R. SHAH    | 09/09/94 |
| R. SHAH    | 10/25/94 |
| A. ABBAS   | 03/21/97 |
| M. GOMEZ   | 04/06/00 |
| R. BORO    | 01/01/07 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER  
DETAILS

SCALE: VERT. NONE  
HORIZ. NONE

DRAWN BY  
CHECKED BY

|                       |         |                           |              |           |
|-----------------------|---------|---------------------------|--------------|-----------|
| F.A. RTE.             | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|                       |         |                           | 14           | 7         |
| STA.                  |         | TO STA.                   |              |           |
| FED. ROAD DIST. NO. 1 |         | ILLINOIS FED. AID PROJECT |              |           |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| REVISIONS |          |
|-----------|----------|
| NAME      | DATE     |
| A. HOUSEH | 03/11/94 |
| R. SHAH   | 02/24/95 |
| R. SHAH   | 03/02/95 |
| R. SHAH   | 08/19/96 |
| R. SHAH   | 09/12/96 |
| R. SHAH   | 09/19/96 |
| R. SHAH   | 10/03/96 |
| A. ABBAS  | 03/21/97 |
| M. GOMEZ  | 01/22/01 |
| R. BORO   | 01/01/07 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

**CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT**

SCALE: VERT. NONE  
HORIZ.

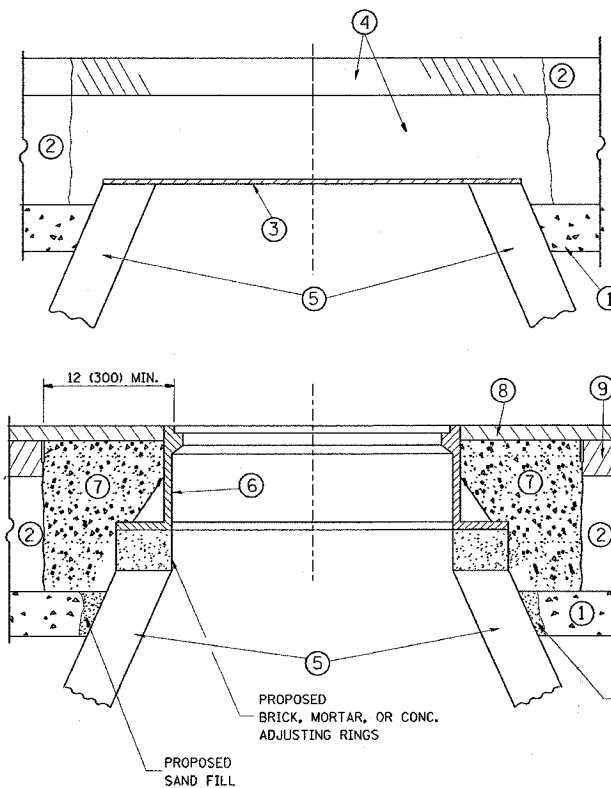
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BD600-06 (BD-24)

**CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT**

|                       |          |                  |              |           |
|-----------------------|----------|------------------|--------------|-----------|
| F.A. RTE.             | SECTION  | COUNTY           | TOTAL SHEETS | SHEET NO. |
|                       |          |                  | 14           | 8         |
| STA.                  | TO STA.  |                  |              |           |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT |              |           |



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| REVISIONS   |          |
|-------------|----------|
| NAME        | DATE     |
| R. SHAH     | 10/25/94 |
| R. SHAH     | 01/30/95 |
| R. SHAH     | 03/10/95 |
| A. ABBAS    | 03/21/97 |
| R. WIEDEMAN | 05/14/04 |
| R. BORO     | 01/01/07 |

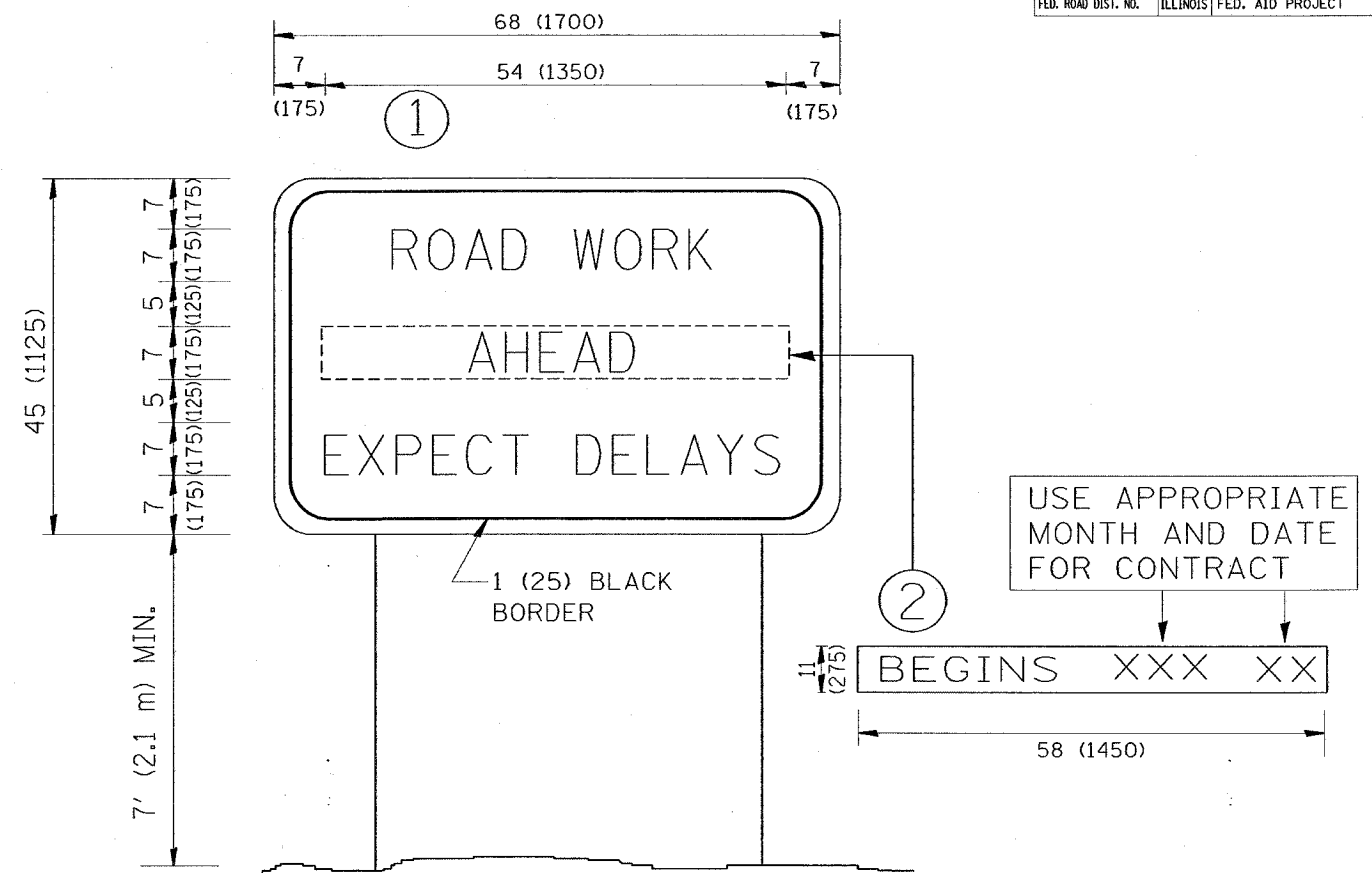
**ILLINOIS DEPARTMENT OF TRANSPORTATION  
DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT  
WITH MILLING**

SCALE: VERT. NONE  
HORIZ.

DRAWN BY  
CHECKED BY



|                     |          |                  |              |           |
|---------------------|----------|------------------|--------------|-----------|
| F.A. RTE.           | SECTION  | COUNTY           | TOTAL SHEETS | SHEET NO. |
|                     |          |                  | 14           | 9         |
| STA.                |          | TO STA.          |              |           |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT |              |           |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| REVISIONS    |          |
|--------------|----------|
| NAME         | DATE     |
| R. MIRS      | 9-15-97  |
| R. MIRS      | 12-11-97 |
| T. RAMMACHER | 2-2-99   |
| C. JUXTUS    | 1-31-07  |
|              |          |
|              |          |
|              |          |
|              |          |

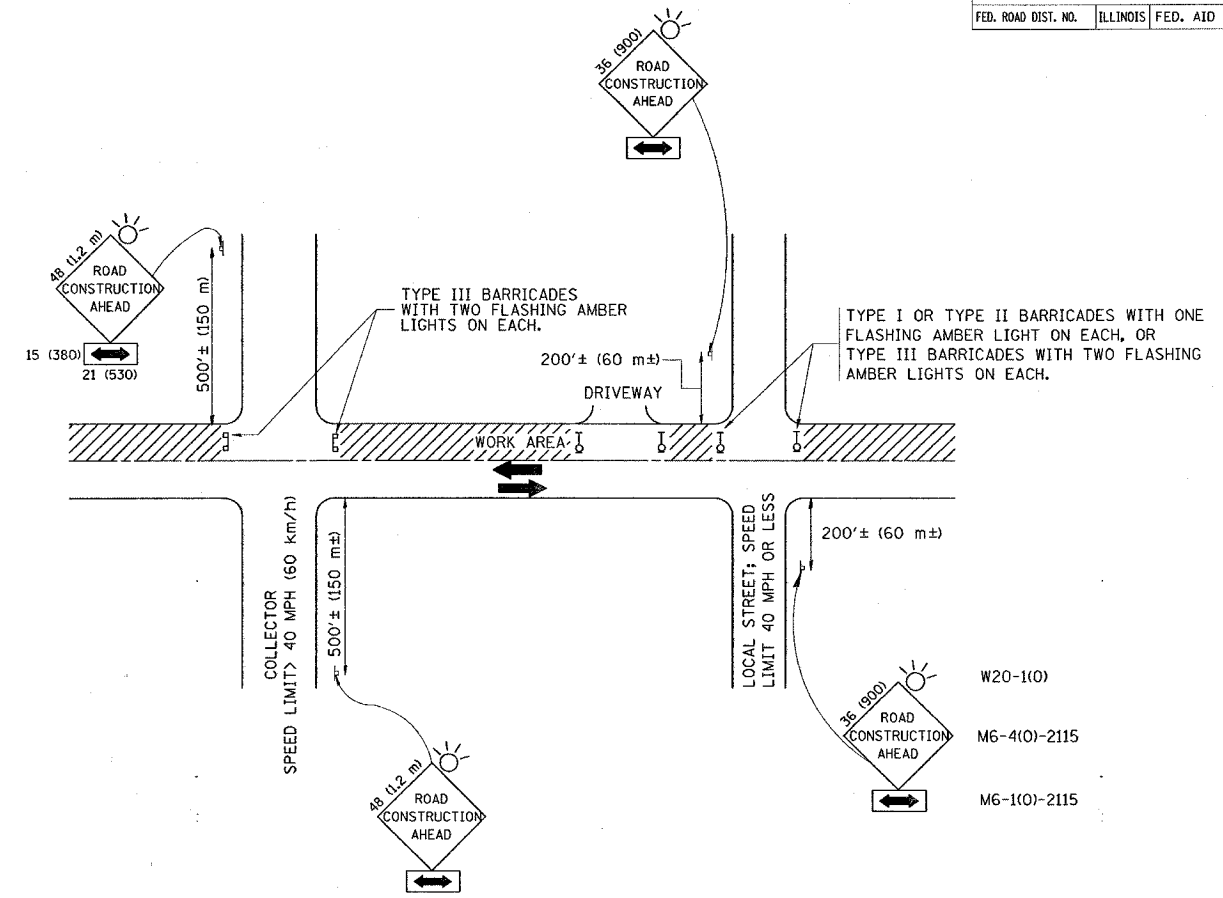
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ARTERIAL ROAD  
 INFORMATION SIGN**

SCALE: NONE

DRAWN BY DESIGN  
 CHECKED BY

TC22

| F.A. RTE.           | SECTION  | COUNTY           | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
|                     |          |                  | 14           | 10        |
| STA.                |          | TO STA.          |              |           |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT |              |           |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

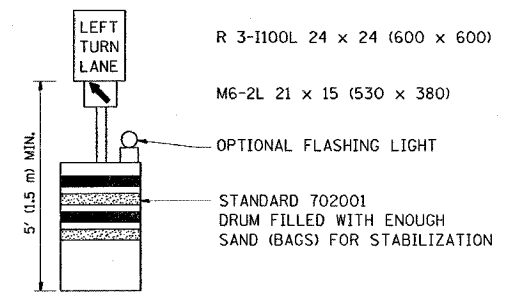
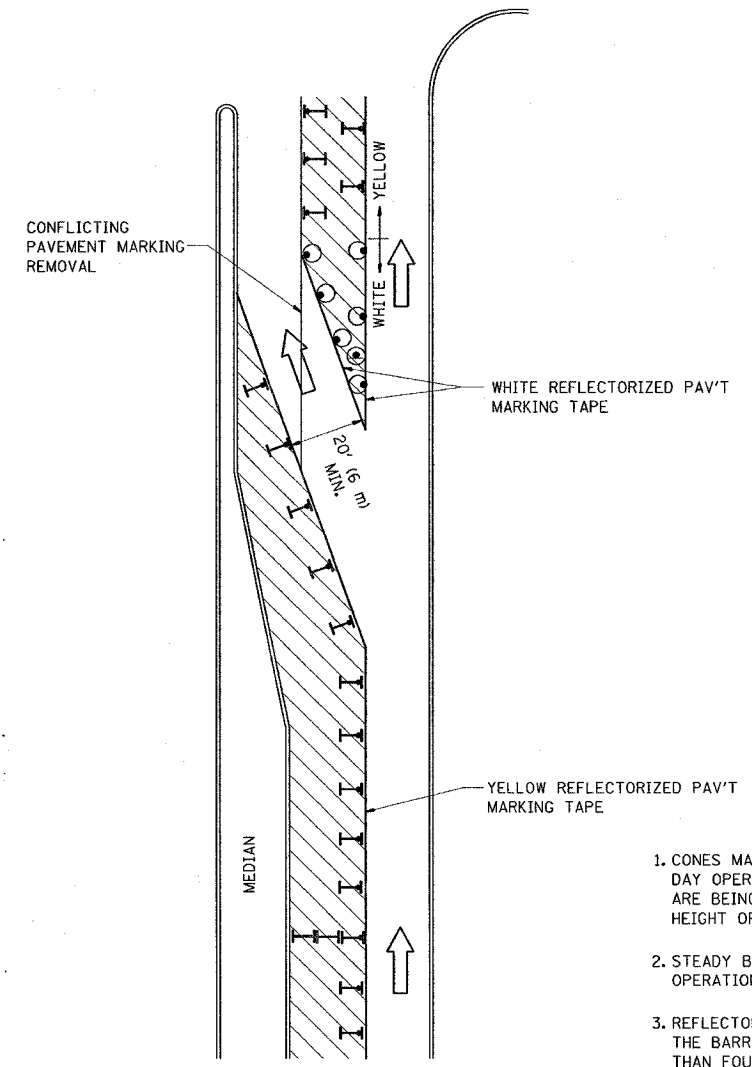
| REVISIONS    |          |
|--------------|----------|
| NAME         | DATE     |
| LHA          | 6/89     |
| T. RAMMACHER | 09/08/94 |
| J. OBERLE    | 10/18/95 |
| A. HOUSEH    | 03/06/96 |
| A. HOUSEH    | 10/15/96 |
| T. RAMMACHER | 01/06/00 |
|              |          |
|              |          |

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TRAFFIC CONTROL AND PROTECTION  
 FOR  
 SIDE ROADS, INTERSECTIONS, AND  
 DRIVEWAYS

SCALE: NONE  
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 CHECKED BY  
 TC-10

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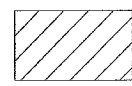





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|---------------------|----------|------------------|--------------|-----------|
| F.A. RTE.           | SECTION  | COUNTY           | TOTAL SHEETS | SHEET NO. |
|                     |          |                  | 14           | 11        |
| STA.                | TO STA.  |                  |              |           |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT |              |           |



**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

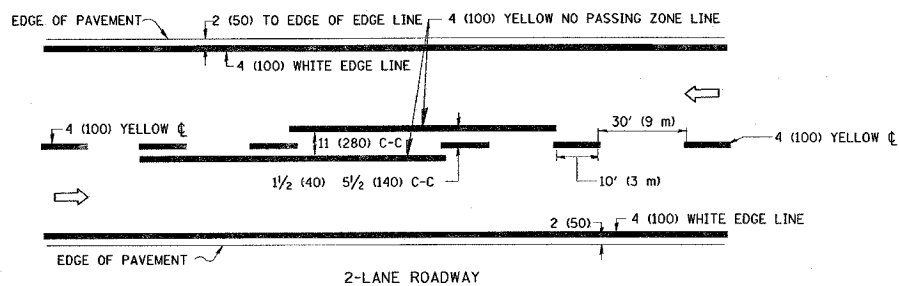
| REVISIONS    |          |
|--------------|----------|
| NAME         | DATE     |
| T. RAMMACHER | 09/08/94 |
| A. HOUSEH    | 11/07/95 |
| A. HOUSEH    | 10/12/96 |
| T. RAMMACHER | 01/06/00 |
|              |          |
|              |          |
|              |          |
|              |          |
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ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TRAFFIC CONTROL AND PROTECTION  
 AT TURN BAYS  
 (TO REMAIN OPEN TO TRAFFIC)**

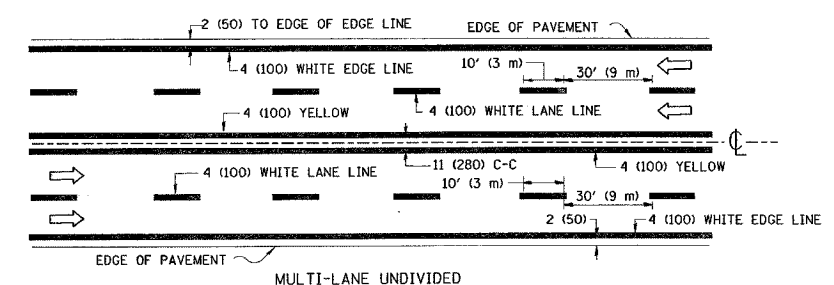
SCALE: NONE

DRAWN BY  
 CHECKED BY LHA  
 TC-14

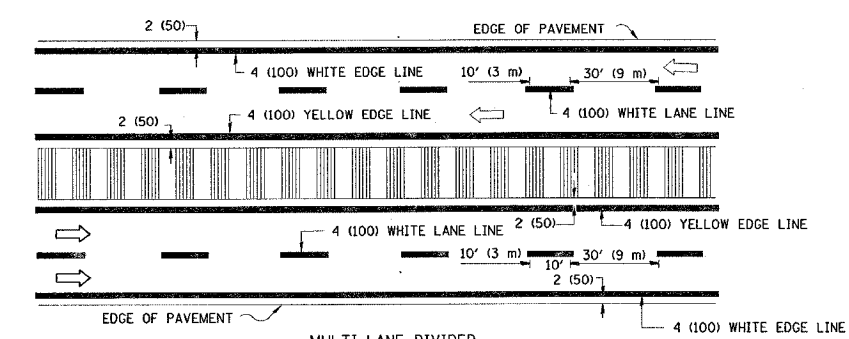
| F.A. RTE.                 | SECTION | COUNTY                          | TOTAL SHEETS | SHEET NO. |
|---------------------------|---------|---------------------------------|--------------|-----------|
|                           |         |                                 | 14           | 12        |
| STA. _____                |         | TO STA. _____                   |              |           |
| FED. ROAD DIST. NO. _____ |         | ILLINOIS FED. AID PROJECT _____ |              |           |



2-LANE ROADWAY



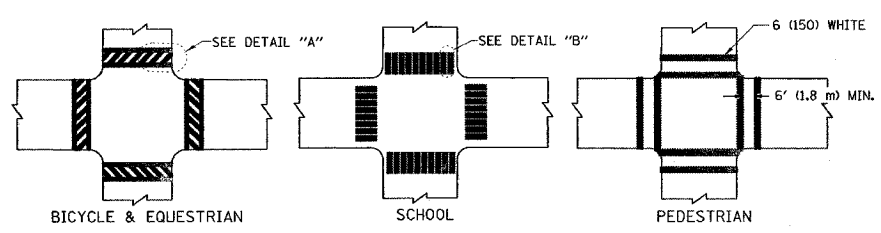
MULTI-LANE UNDIVIDED



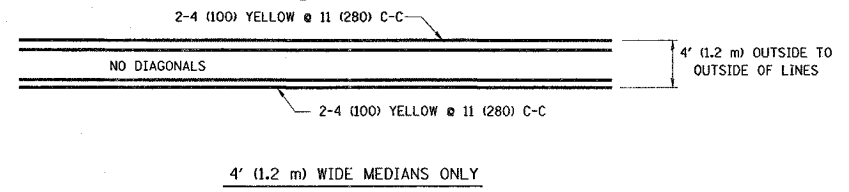
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

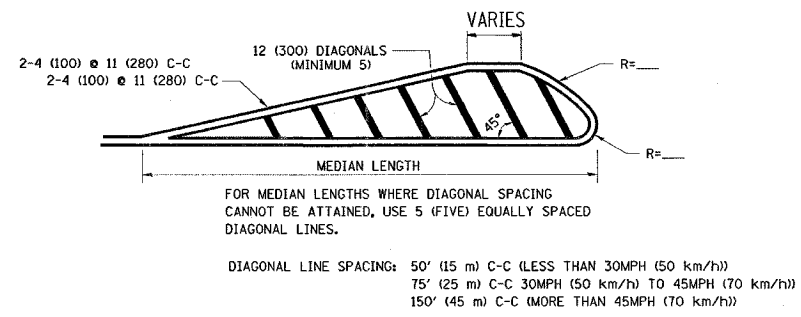
TYPICAL LANE AND EDGE LINE MARKING



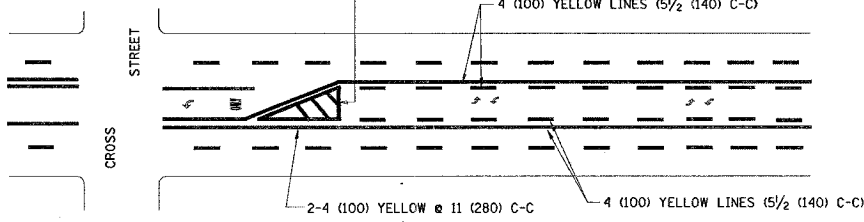
TYPICAL CROSSWALK MARKING



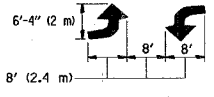
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

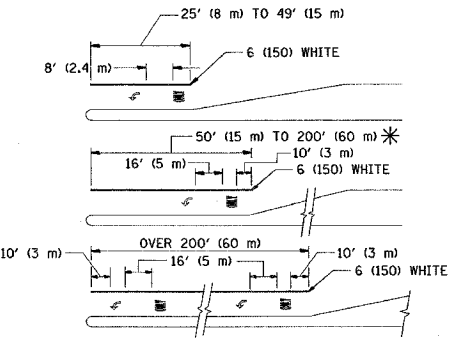


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

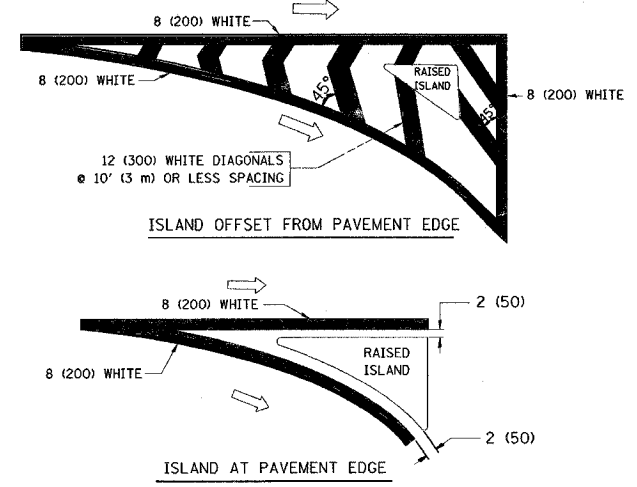


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. \* AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING   | WIDTH OF LINE  | PATTERN                         | COLOR   | SPACING / REMARKS  |
|---|--|---------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT   | 4 (100)  | SKIP-DASH                       | YELLOW  | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT   | 2 @ 4 (100)  | SOLID                           | YELLOW  | 11 (280) C-C   |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS                                      | 4 (100)<br>2 @ 4 (100)   | SOLID<br>SOLID                  | YELLOW<br>YELLOW                                  | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE<br>11 (280) C-C<br>OMIT SKIP-DASH CENTERLINE BETWEEN   |
| LANE LINES  | 4 (100)<br>5 (125) ON FREEWAYS   | SKIP-DASH<br>SKIP-DASH          | WHITE<br>WHITE                                    | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)                                   | SAME AS LINE BEING EXTENDED  | SKIP-DASH                       | SAME AS LINE BEING EXTENDED                       | 2' (600) LINE WITH 6' (1.8 m) SPACE  |
| EDGE LINES  | 4 (100)  | SOLID                           | YELLOW-LEFT<br>WHITE-RIGHT                        | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB  |
| TURN LANE MARKINGS  | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))                                      | SOLID                           | WHITE   | SEE TYPICAL TURN LANE MARKING DETAIL   |
| TWO WAY LEFT TURN MARKING   | 2 @ 4 (100) EACH DIRECTION<br>8' (2.4m) LEFT ARROW   | SKIP-DASH AND SOLID<br>IN PAIRS | YELLOW<br>WHITE                                   | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE<br>SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL             |
| CROSSWALK LINES (PEDESTRIAN)<br>A. DIAGONALS (BIKE & EQUESTRIAN)<br>B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150)<br>12 (300) @ 45°<br>12 (300) @ 90°  | SOLID<br>SOLID<br>SOLID         | WHITE<br>WHITE<br>WHITE                           | NOT LESS THAN 6' (1.8 m) APART<br>2' (600) APART<br>2' (600) APART<br>SEE TYPICAL CROSSWALK MARKING DETAILS.   |
| STOP LINES  | 24 (600)   | SOLID                           | WHITE   | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS   | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°<br>NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID                           | YELLOW; TWO WAY TRAFFIC<br>WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE<br>SEE TYPICAL PAINTED MEDIAN MARKING.  |
| GORE MARKING AND CHANNELIZING LINES   | 8 (200) WITH 12 (300) DIAGONALS @ 45°  | SOLID                           | WHITE   | DIAGONALS:<br>15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h))<br>20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)<br>30' (9 m) C-C (OVER 45MPH (70 km/h))              |
| RAILROAD CROSSING   | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"               | SOLID                           | WHITE   | SEE STATE STANDARD 780001<br>AREA OF:<br>"R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH<br>"X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )                                     |
| SHOULDER DIAGONALS  | 12 (300) @ 45°   | SOLID                           | WHITE - RIGHT<br>YELLOW - LEFT                    | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))<br>75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h))<br>150' (45 m) C-C (OVER 45MPH (70 km/h))                       |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in Inches (millimeters) unless otherwise shown.

| REVISIONS    |          |
|--------------|----------|
| NAME         | DATE     |
| EVERS        | 03-19-90 |
| T. RAMMACHER | 10-27-94 |
| ALEX HOUSEH  | 10-09-96 |
| ALEX HOUSEH  | 10-17-96 |
| T. RAMMACHER | 01-06-00 |

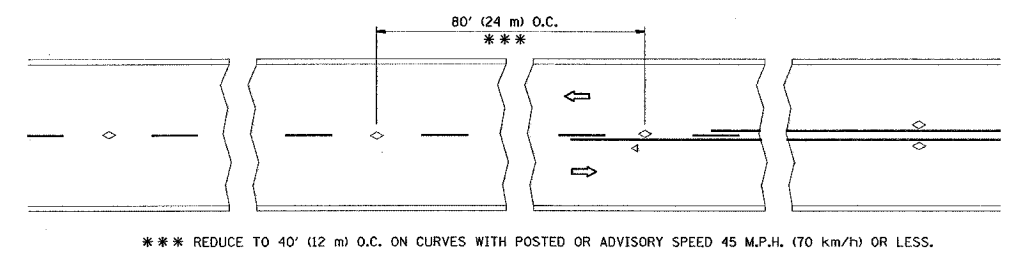
ILLINOIS DEPARTMENT OF TRANSPORTATION  
DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE

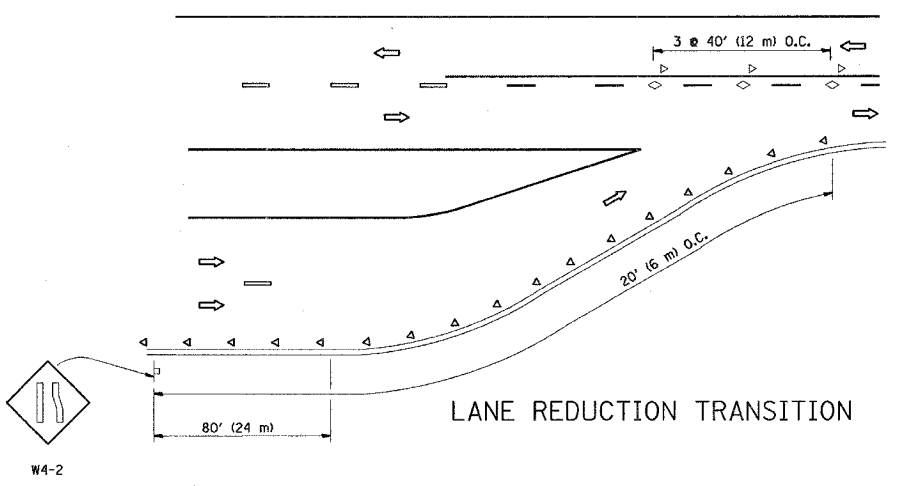
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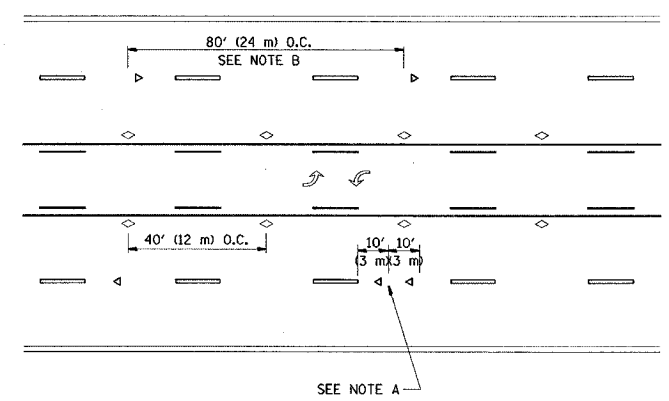
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| F.A. RTE.           | SECTION  | COUNTY           | TOTAL SHEETS | SHEET NO. |
|                     |          |                  | 14           | 13        |
| STA.                |          | TO STA.          |              |           |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT |              |           |



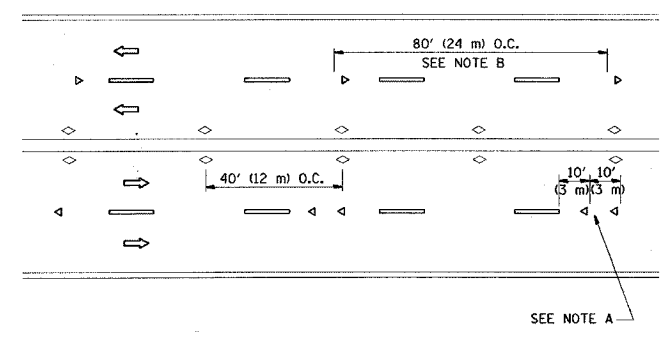
TWO-LANE/TWO-WAY



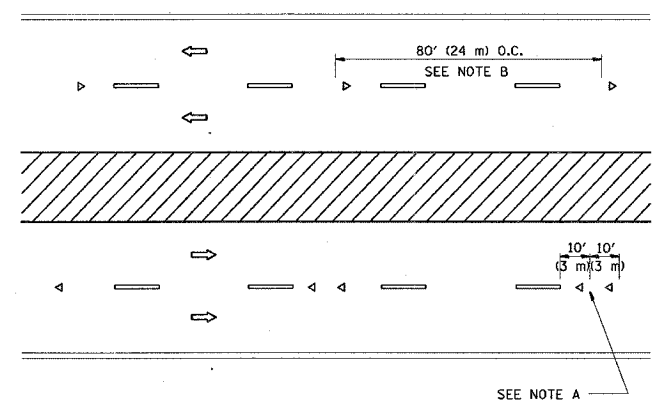
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

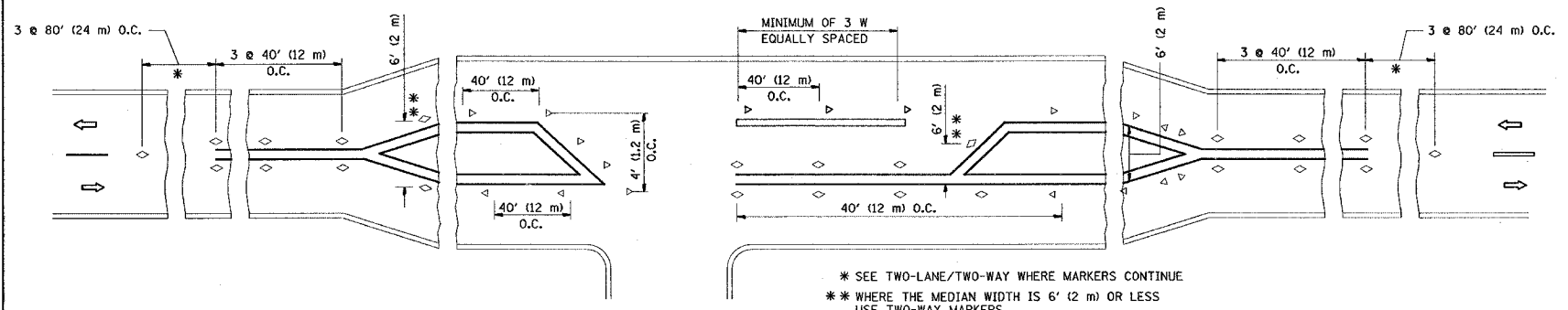
- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◇ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in Inches (millimeters) unless otherwise shown.

| REVISIONS    |          |
|--------------|----------|
| NAME         | DATE     |
| T. RAMMACHER | 09-19-94 |
| T. RAMMACHER | 03-12-99 |
| T. RAMMACHER | 01-06-00 |
|              |          |
|              |          |
|              |          |
|              |          |

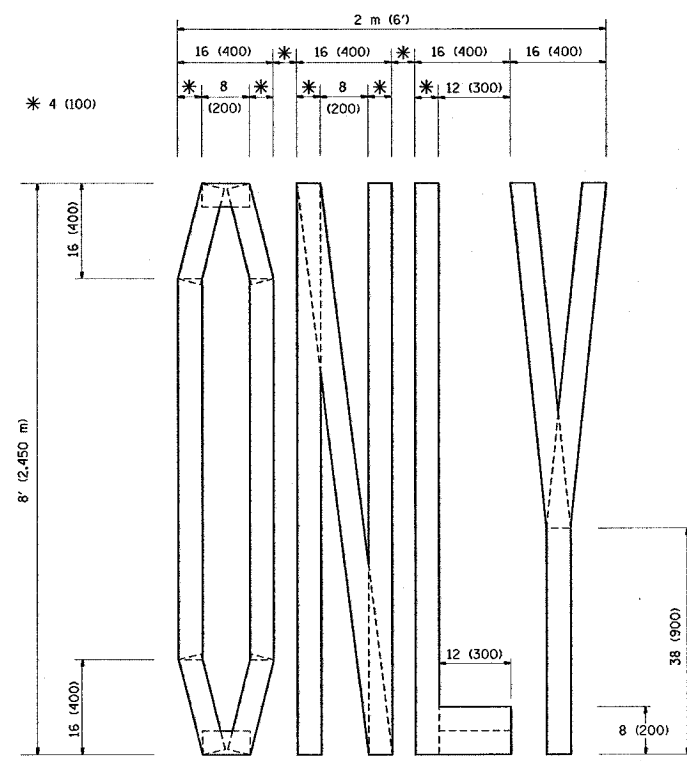
ILLINOIS DEPARTMENT OF TRANSPORTATION  
TYPICAL APPLICATIONS  
RAISED REFLECTIVE PAVEMENT  
MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE

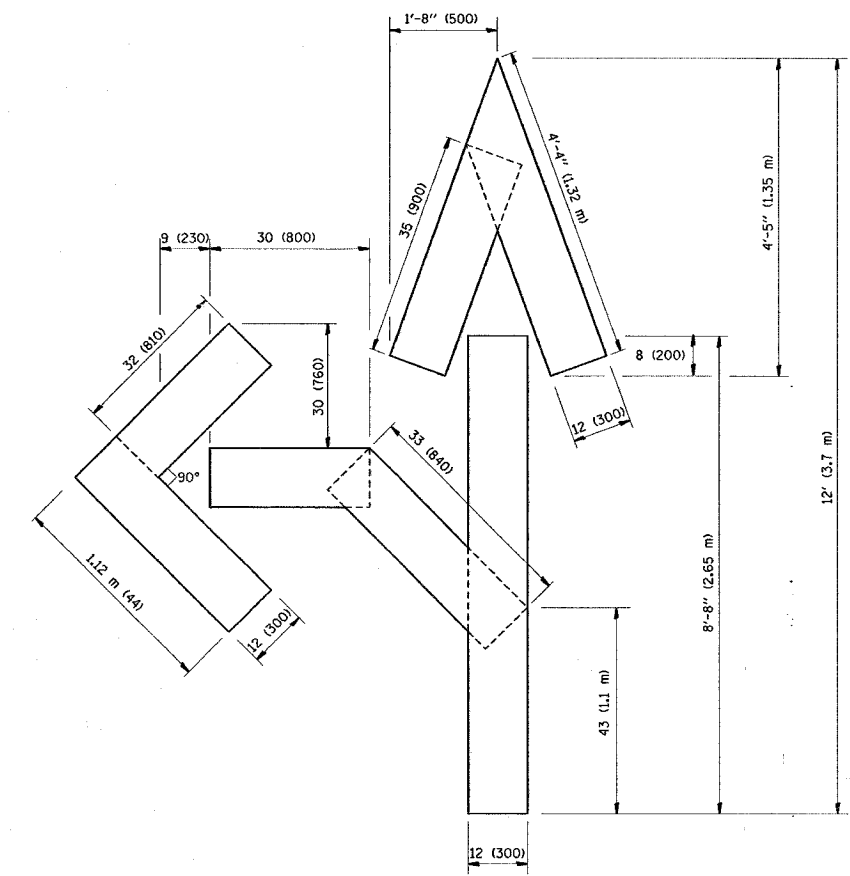
DRAWN BY CADD  
CHECKED BY

PLOT DATE = 4/16/2007  
FILE NAME = \\ntsrsc\coll\dgn  
PLOT SCALE = 80.0000 / IN.  
USER NAME = byunah

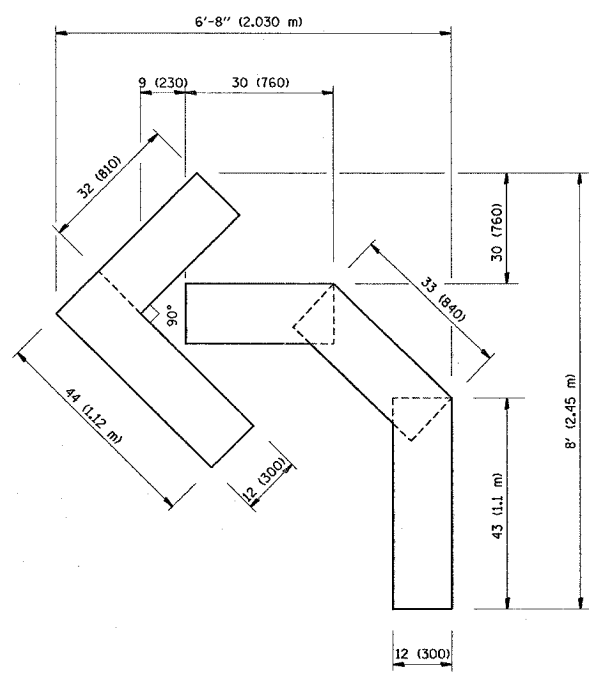
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|---------------------|---------|---------------------------|--------------|-----------|
| F.A. RTE.           | SECTION | COUNTY                    | TOTAL SHEETS | SHEET NO. |
|                     |         |                           | 14           | 14        |
| STA.                |         | TO STA.                   |              |           |
| FED. ROAD DIST. NO. |         | ILLINOIS FED. AID PROJECT |              |           |



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

| REVISIONS    |          |
|--------------|----------|
| NAME         | DATE     |
| T. RAMMACHER | 09/18/94 |
| J. OBERLE    | 06/01/96 |
| T. RAMMACHER | 06/05/96 |
| T. RAMMACHER | 11/04/97 |
| T. RAMMACHER | 03/02/98 |
| E. GOMEZ     | 08/28/00 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING  
 LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

SCALE: NONE

DRAWN BY CADD

CHECKED BY

TC-16