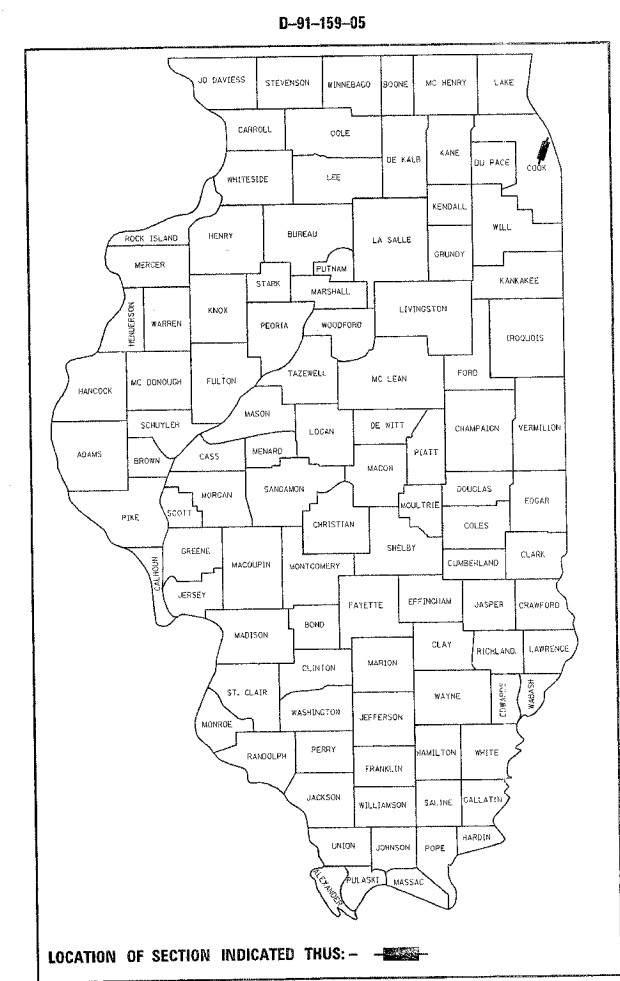


F.A.U. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAU ROUTE 2798: CARPENTER RD.
SECTION: 3055.A-RS-1
LUNT AVE. TO TOUHY AVE.
RESURFACING (MAINTENANCE)
COOK COUNTY
C-91-159-05



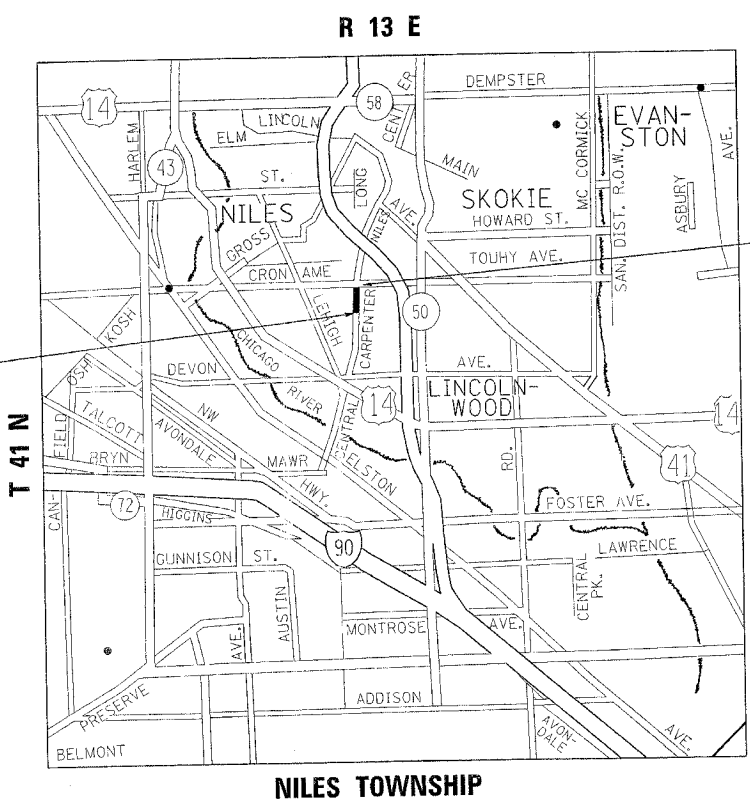
LOCATION OF SECTION INDICATED THUS: - [Symbol] -

FOR INDEX OF SHEETS, SEE SHEET NO. 2

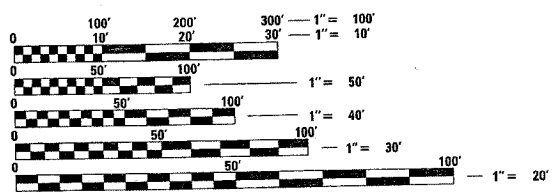
IMPROVEMENT LOCATED IN THE VILLAGE OF SKOKIE.

IMPROVEMENT BEGINS
STATION 51+85

IMPROVEMENT ENDS
STATION 64+20



TRAFFIC DATA
2006 ADT = 21,900
POSTED SPEED = 35 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION)
1-800-892-0123

GROSS & NET LENGTH = 1235 LINEAL FEET = 0.24 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 29 20 07

Diane O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 11, 20 07
Eric E. Horvath
ENGINEER OF DESIGN AND ENVIRONMENT

May 11, 20 07
Milton R. Sees, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

DISTRICT ONE - PLAN PREPARATION ENGINEER: KEN ENG/JP CHANG (847) 705-4432

CONTRACT NO. 62926

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	2
STA. 10+31		TO STA. 63+85		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

INDEX OF SHEETS

DESCRIPTION

LIST OF STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4-5	EXISTING AND PROPOSED TYPICAL SECTIONS
6	ROADWAY AND PAVEMENT MARKING PLANS
7-8	DETECTOR LOOP REPLACEMENT PLANS
9	BUTT JOINT AND HMA TAPER DETAILS
10	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
11	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
12	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
13	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
14	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
15	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
17	ARTERIAL ROAD INFORMATION SIGN
18	DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
19	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)

000001-04	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERN
442201-02	CLASS C AND D PATCHES
606001-03	CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER
701601-04	URBAN LANE CLOSURE, MULTILANE 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-04	URBAN LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN
701701-04	URBAN LANE CLOSURE, MULTILANE, INTERSECTION
701801	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
702001-06	TRAFFIC CONTROL DEVICES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

3 METER (10 FEET) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF SKOKIE

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 40 MM (1 1/2 INCHES) WHERE THE SPEED LIMIT IS 80 KM/H (45 MPH) OR LESS AND 25 MM (1 INCH) WHERE THE SPEED LIMIT IS GREATER THAN 80 KM/H (45 MPH). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 75 MM (3 INCHES) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT MR. WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER @ 773-685-8386 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CARPENTER AVENUE
 (FROM LUNT AVE TO TOUHY AVE)
 INDEX OF SHEETS, LIST OF STANDARD
 AND GENERAL NOTES

SCALE: VERT. NONE
 HORIZ. NONE
 DATE: DRAWN BY YP
 CHECKED BY

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	3
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		URBAN				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	5	5				
40600300	AGGREGATE (PRIME COAT)	TON	20	20				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	3	3				
40600895	CONSTRUCTING TEST STRIP	EACH	2	2				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	137	137				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	450	450				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	862	862				
42001300	PROTECTIVE COAT	SQ YD	65	65				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	9960	9960				
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	110	110				
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	2595	2595				
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	600	600				
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	135	135				
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	1860	1860				
55039700	STORM SEWERS TO BE CLEANED	FOOT	500	500				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	1	1				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	1	1				
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	13	13				
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6				
67100100	MOBILIZATION	L SUM	1	1				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1				
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	2673	2673				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		URBAN				
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	401	401				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	3658	3658				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1735	1735				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	339	339				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	153	153				
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	891	891				
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	401	401				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3658	3658				
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1735	1735				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	339	339				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	153	153				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	171	171				
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	715	715				
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4				
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	431	431				
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	31	31				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				

3/28/2007

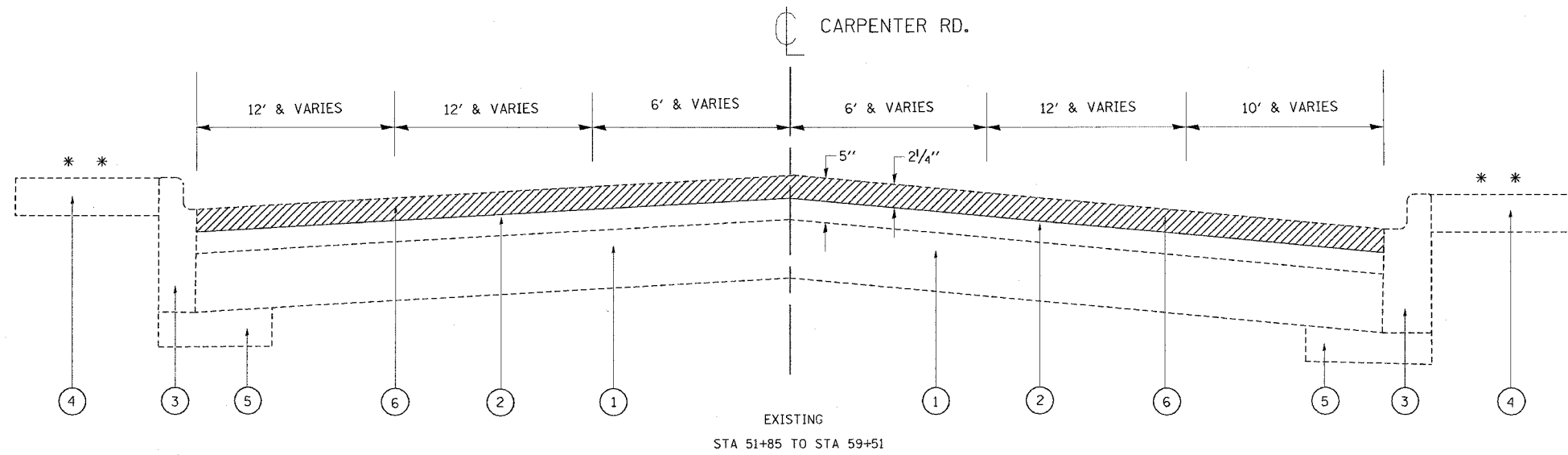
* SPECIALTY ITEMS

Rev.

REVISIONS	
NAME	DATE

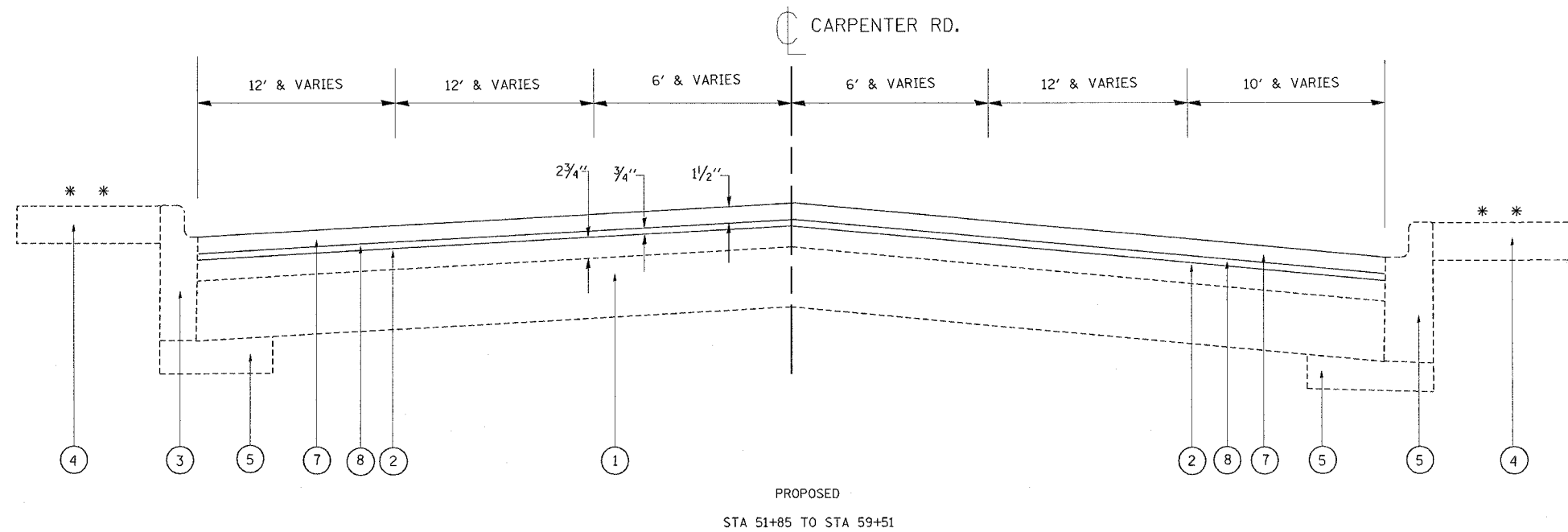
ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	4
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



LEGEND

- ① EXISTING PCC PAVEMENT 12"
- ② EXISTING HOT-MIX ASPHALT SURFACE (+/-) 2 3/4" (AFTER MILLING)
- ③ EXISTING COMB. CONC. CURB AND GUTTER
- ④ EXISTING PCC SIDEWALK
- ⑤ EXISTING SUBBASE GRANULAR MATERIAL TYPE A (4")
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL (2 1/4")
- ⑦ PROPOSED HMA SURFACE COURSE, MIX "D", N70 (1 1/2")
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 (3/4")



* * LOCATIONS VARIES

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE	AC TYPE	AIR VOIDS (%)
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 MM)	PG 64-22	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL- 4.75, N50	SBS/PG 76-28	4% @ 50 GYR.
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 MM)	PG 64-22/58-22	4% @ 70 GYR.
CLASS "D" PATCHES 9", (BINDER, IL-19 MM)	PG 64-22/58-22	4% @ 70 GYR.

NOTE: THE UNIT WEIGHT USED TO CALCULATE HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE 58-22.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

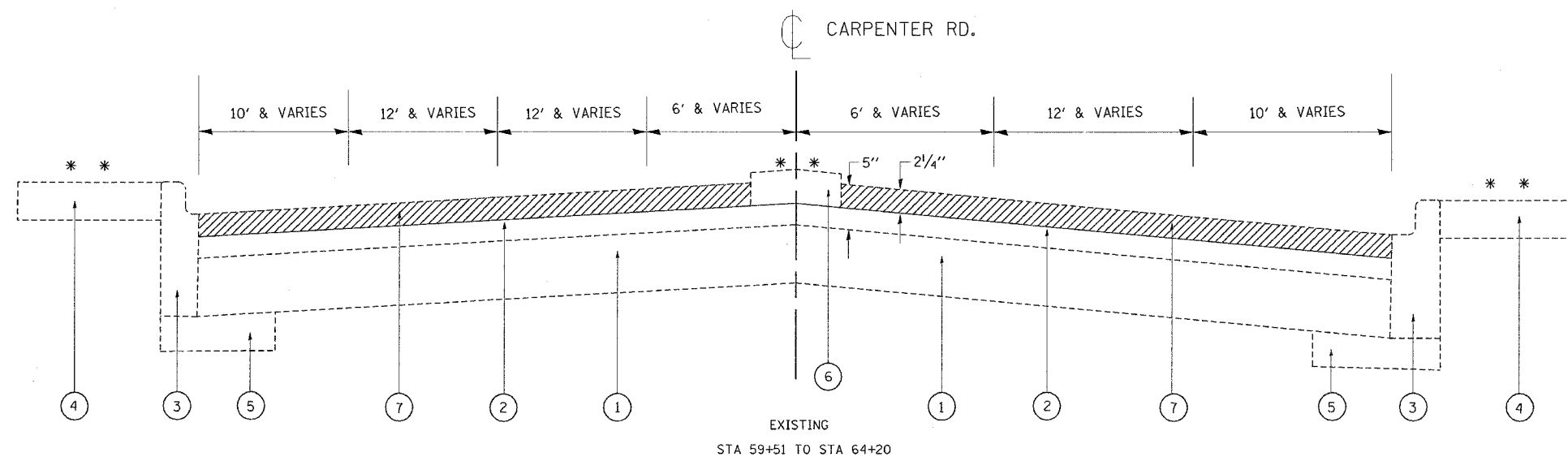
**CARPENTER ROAD
(LUNT AVE. TO TOUHY AVE.)
EXISTING AND PROPOSED
TYPICAL SECTIONS**

SCALE: VERT. NONE
HORIZ. DATE

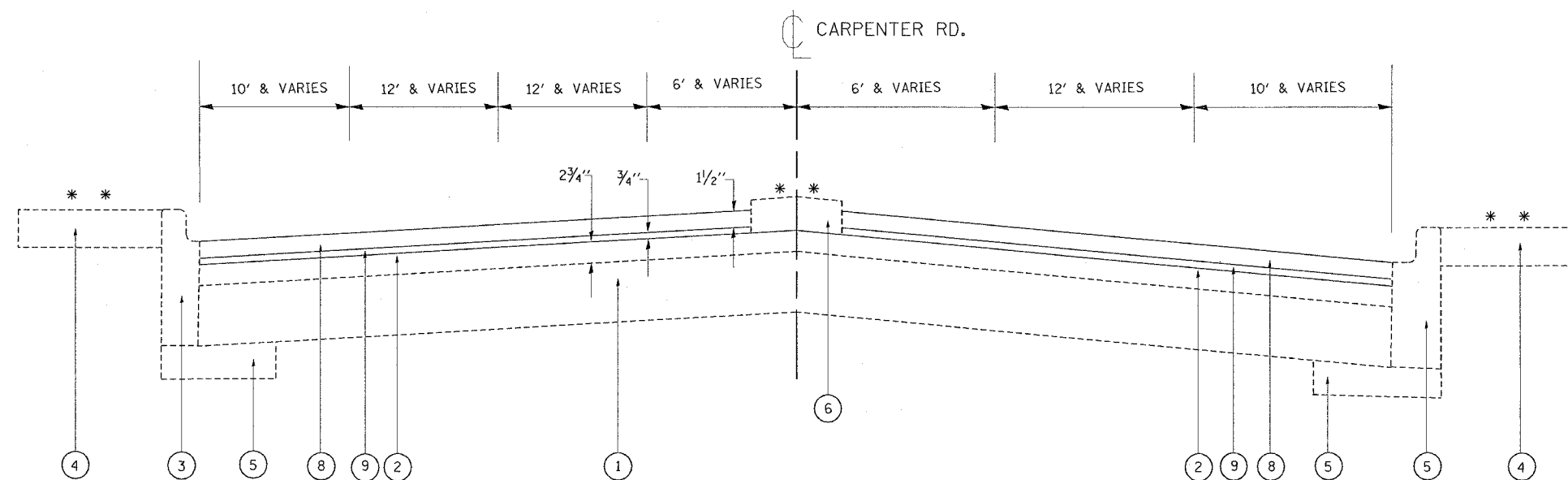
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CHECKED BY

CONTRACT NO.: 62926

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
279B	3055.A-RS-1	COOK	19	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



EXISTING
STA 59+51 TO STA 64+20



PROPOSED
STA 59+51 TO STA 64+20

LEGEND

- ① EXISTING PCC PAVEMENT 12"
- ② EXISTING HOT-MIX ASPHALT SURFACE (+/-) 2 3/4 " (AFTER MILLING)
- ③ EXISTING COMB. CONC. CURB AND GUTTER
- ④ EXISTING PCC SIDEWALK
- ⑤ EXISTING SUBBASE GRANULAR MATERIAL TYPE A (4")
- ⑥ EXISTING 2' PCC RAISED MEDIAN
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL (2 1/4 ")
- ⑧ PROPOSED HMA SURFACE COURSE, MIX "D", N70 (1 1/2 ")
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 (3/4")

* * LOCATIONS VARIES

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**CARPENTER ROAD
(LUNT AVE. TO TOUHY AVE.)
EXISTING AND PROPOSED
TYPICAL SECTIONS**

SCALE: VERT. NONE
HORIZ. NONE
DATE

DRAWN BY JA
CHECKED BY

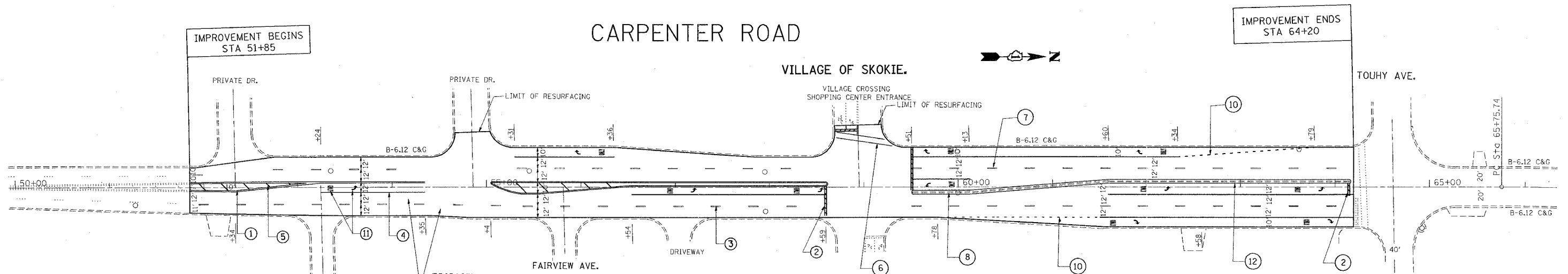
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	6
STA. 50+00		TO STA. 66+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CARPENTER ROAD

VILLAGE OF SKOKIE.

IMPROVEMENT ENDS
STA 64+20

IMPROVEMENT BEGINS
STA 51+85



PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4 "
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX"D", N70, 1 1/2 "
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL- 4.75, N50, 3/4 "

- ① 4" DOUBLE SOLID YELLOW CENTERLINE (11' C-C) (TYP.)
- ② 24" SOLID WHITE - STOP & TRANSVERSE LINES (TYP.)
- ③ 4" SKIP DASH YELLOW CENTERLINE (10' LINE/30' SPACING)
- ④ 6" SOLID WHITE TURN LANE LINE (TYP.)
- ⑤ 12" SOLID YELLOW DIAGONAL LINE (75' C-C OR MINIMUM OF 5)
- ⑥ 6" SOLID WHITE LINE, 2 @ 6' C-C (CROSS WALK LINES) (TYP.)
- ⑦ 4" WHITE SKIP DASH, 10' LINE @ 30' SPACING (TYP.)
- ⑧ 2' PCC RAISED MEDIAN
- ⑨ 4" DOUBLE SOLID YELLOW CENTERLINE (2 @ 11' C-C) (TYP.)
- ⑩ 6" WHITE SKIP DASH, 2' LINE @ 6' SPACING (TYP.)
- ⑪ 8" WHITE, LEFT AND RIGHT TURN ONLY LETTERS AND SYMBOLS (TYP.)
- ⑫ 4" YELLOW, MEDIAN EDGE LINE (TYP.)

PLOT DATE = 4/16/2007
 FILE NAME = c:\prowork\3055a\3055a1.dwg
 USER NAME = qreahy

REVISIONS	
NAME	DATE

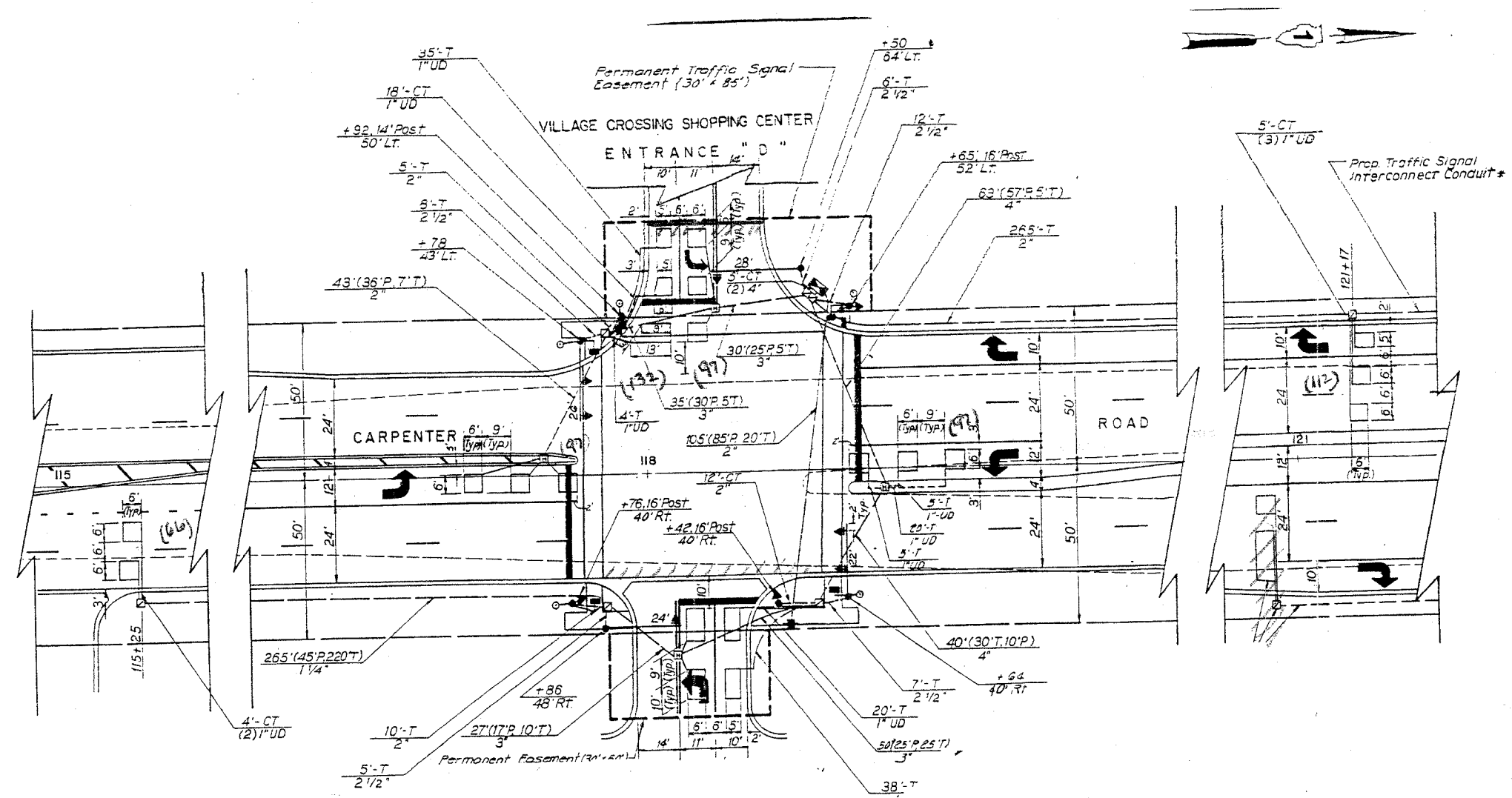
ILLINOIS DEPARTMENT OF TRANSPORTATION

**CARPENTER ROAD
(LUNT AVE. TO TOUHY AVE.)
PROPOSED PLAN AND
PAVEMENT MARKING**

SCALE: VERT. _____
 HORIZ. _____

DATE _____ DRAWN BY _____
 CHECKED BY _____

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	7
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE NO.	QUANTITY	UNIT	ITEM
		EA	DETECTOR LOOP

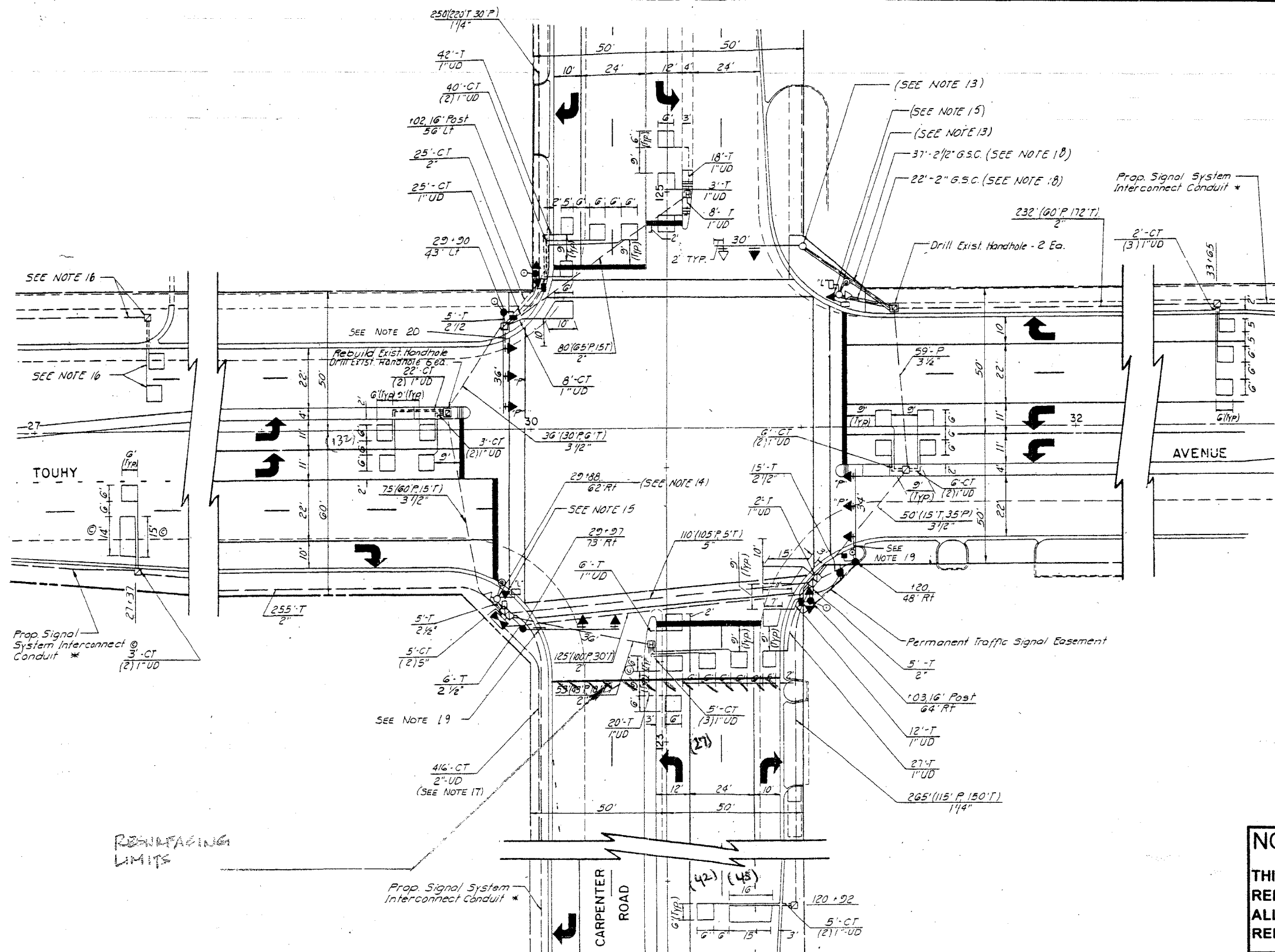
NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS. ALL OTHER INFORMATION SHOWN NOT RELATED AND WILL BE DISREGARDED.



ILLINOIS DEPARTMENT OF TRANSPORTATION
**DETECTOR LOOPS AT:
CARPENTER (CENTRAL AVE.)
& VILLAGE CROSSING C.C.**

SCALE: 1" = 40' DESIGNED BY:

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	COOK	19	8
STA. TO STA.		FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT	



NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	114	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

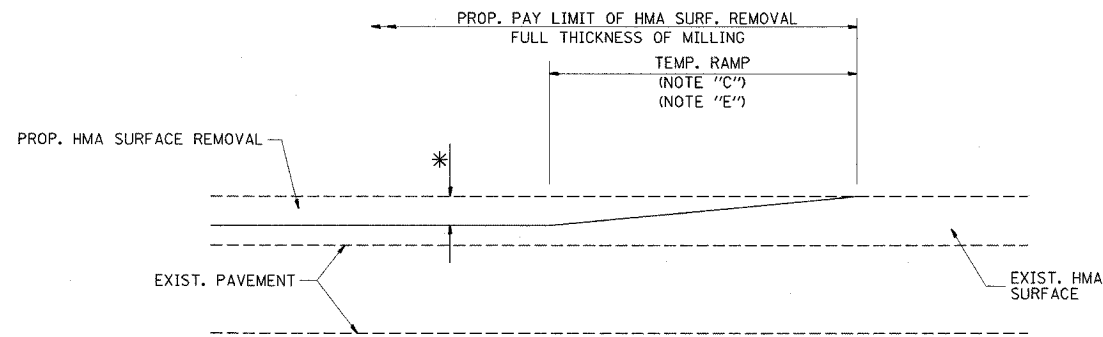
DETECTOR LOOP REPLACEMENT

TOUHY AV. @ CARPENTER ROAD

SCALE: NONE
 DATE: MAR. 2007

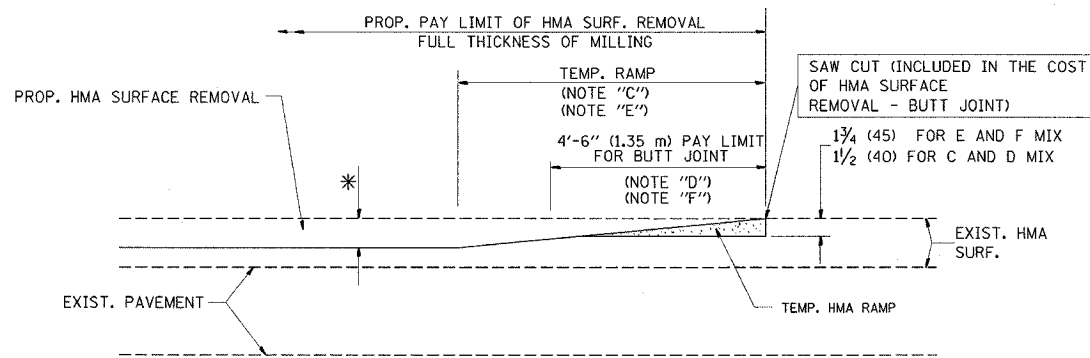
DRAWN BY: J.H.E.
 DESIGNED BY: J.H.E.
 CHECKED BY: D.A.D.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055,A-RS-1	COOK	19	9
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



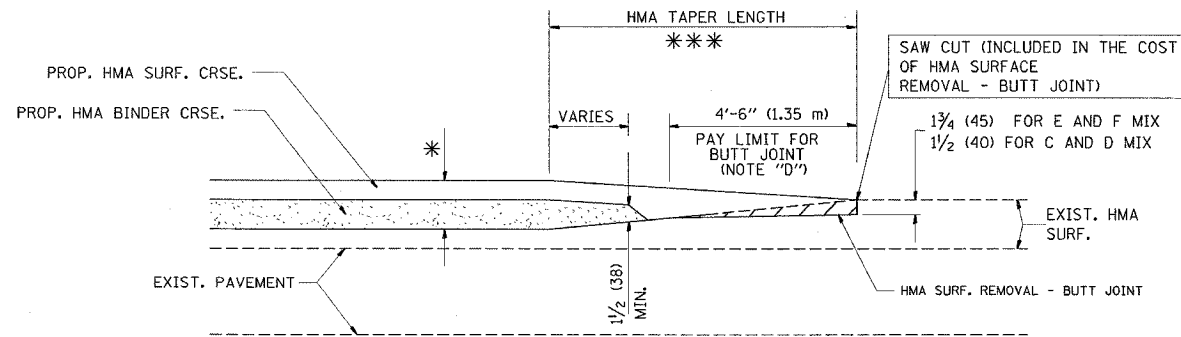
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

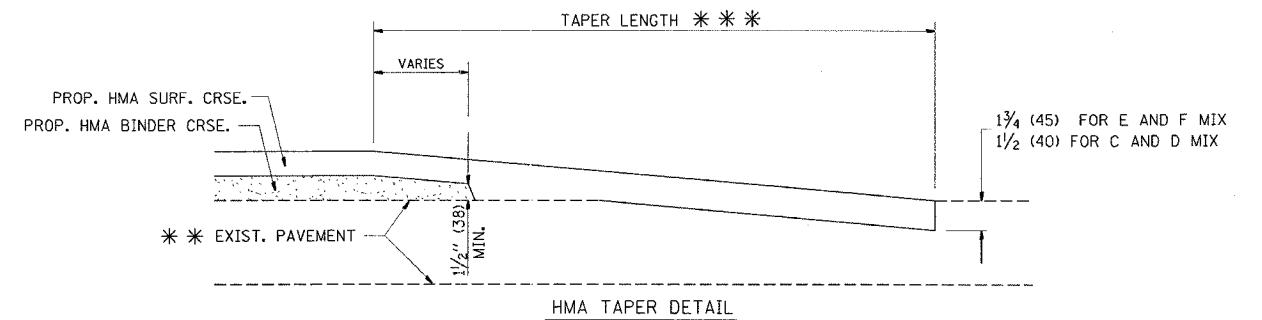
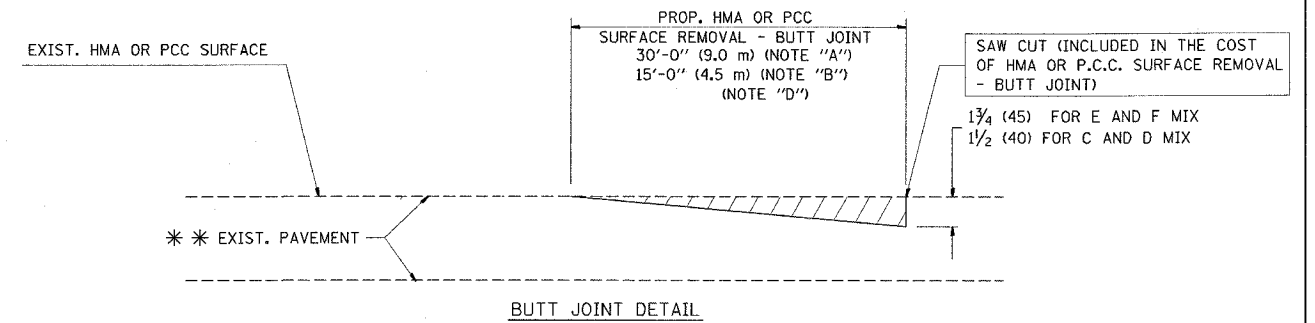


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

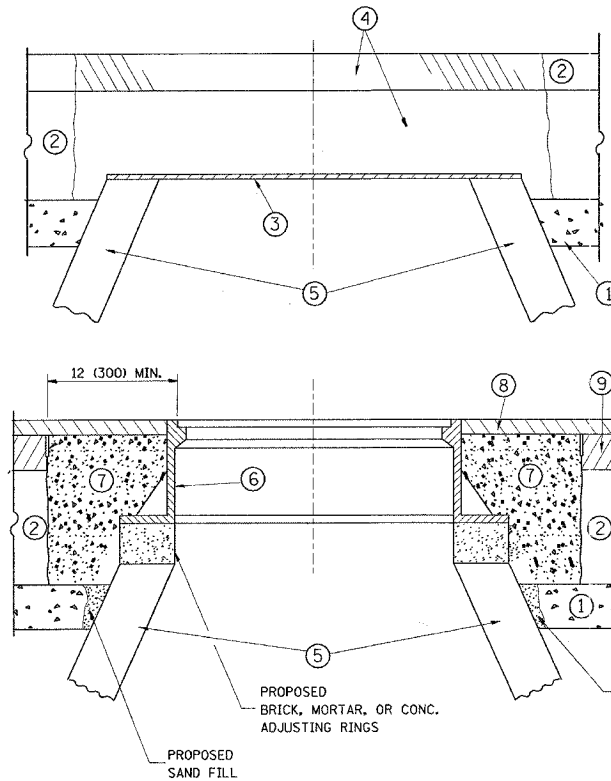
BUTT JOINT AND
HMA TAPER
DETAILS

SCALE: VERT. NONE
HORIZ. NONE
PLOT DATE: 3/28/2007

DRAWN BY
CHECKED BY

BD400-05 (VI-BD32)
REVISION DATE: 01/01/07

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
279B	3055.A-RS-1	COOK	19	10
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

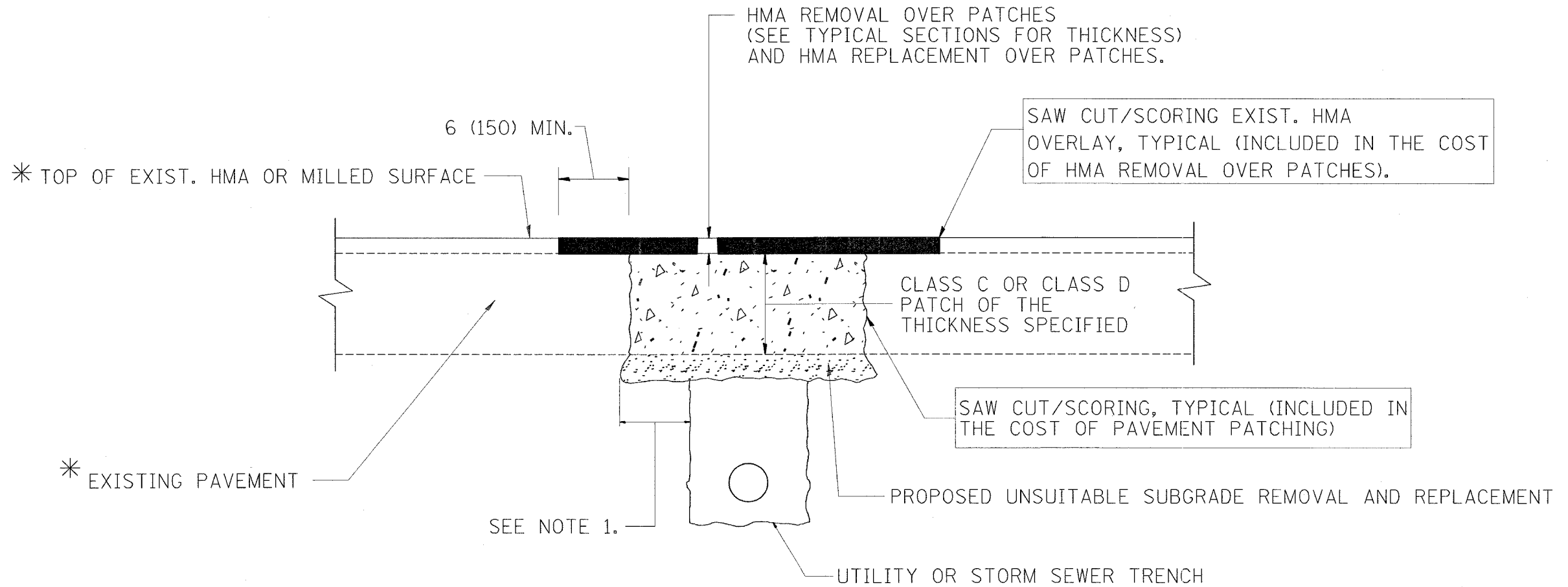
**ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR
FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

SCALE: VERT. NONE
HORIZ.
PLOT DATE: 3/28/2007

DRAWN BY
CHECKED BY

BD600-03 (BD-8)
REVISION DATE: 01/01/07

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	11
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

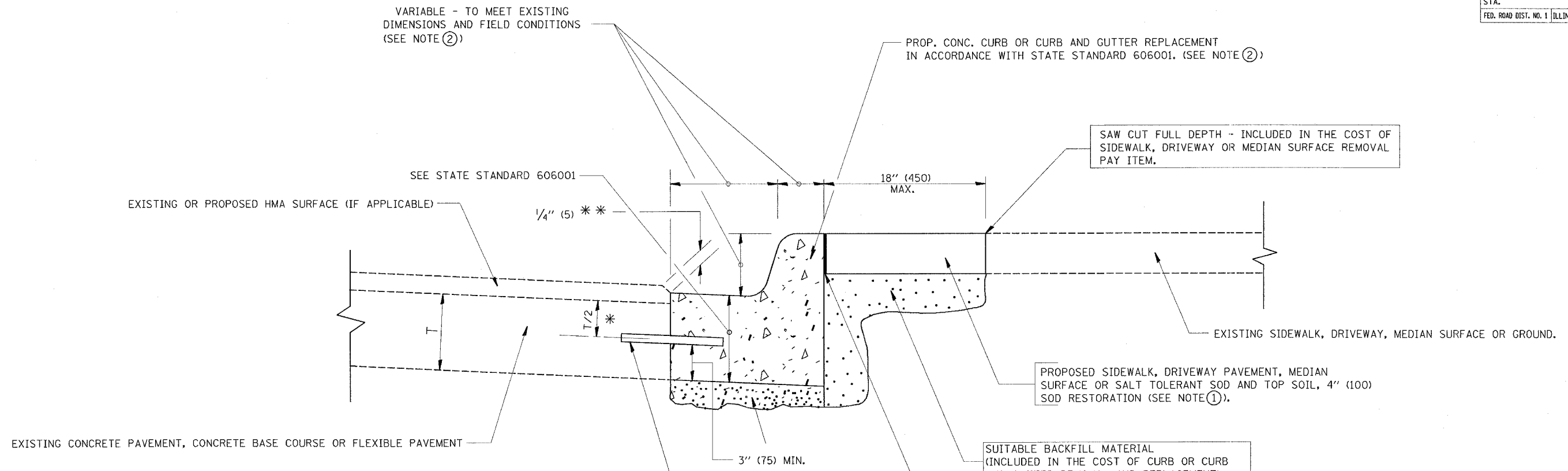
PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: VERT. NONE
HORIZ. NONE
PLOT DATE: 3/28/2007

DRAWN BY
CHECKED BY

BD400-04 (BD-22)
REVISION DATE: 01/01/07

F.A.I.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	12
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
 SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

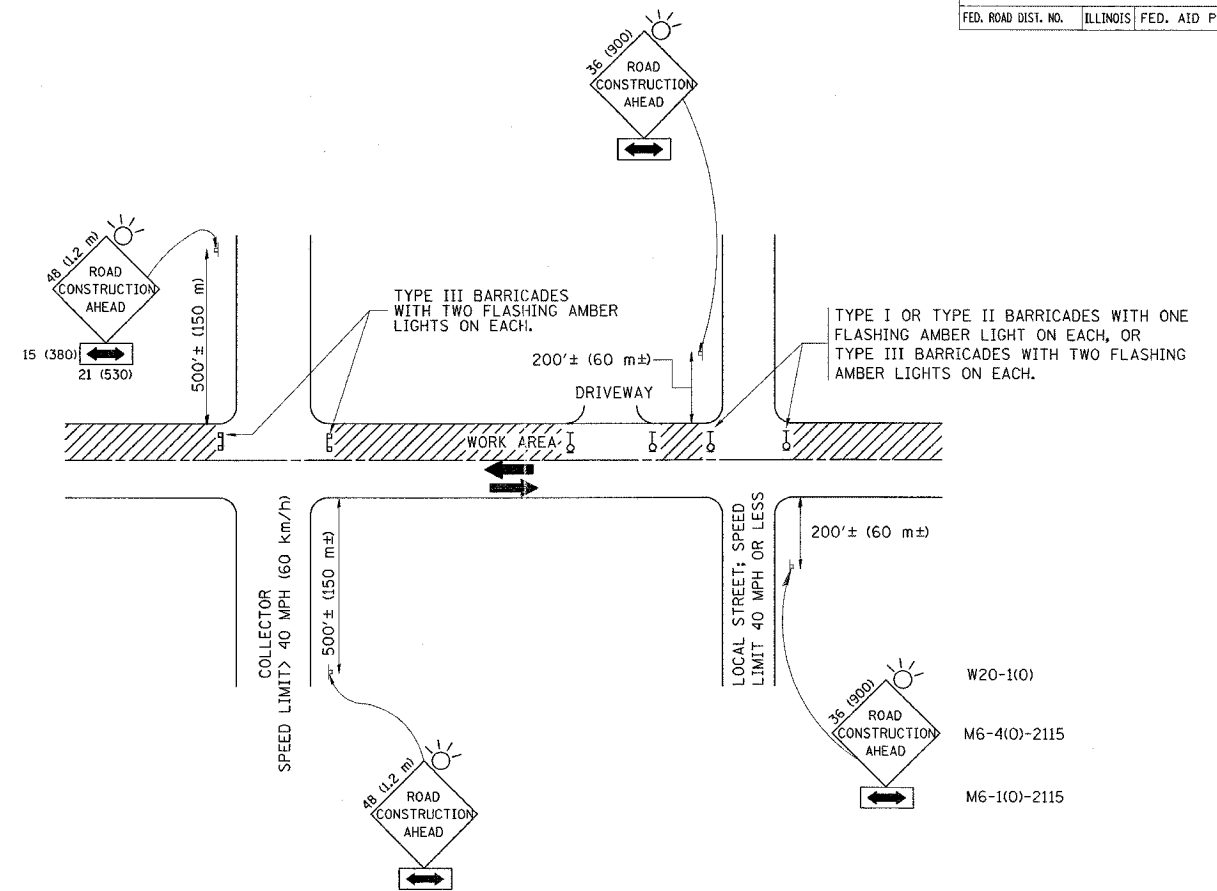
ILLINOIS DEPARTMENT OF TRANSPORTATION
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: VERT. NONE
 HORIZ. BD600-06 (BD-24)
 PLOT DATE: 3/28/2007
 DRAWN BY
 CHECKED BY

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

PLOT DATE = 3/28/2007
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 USER NAME = qurshayh

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	13
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

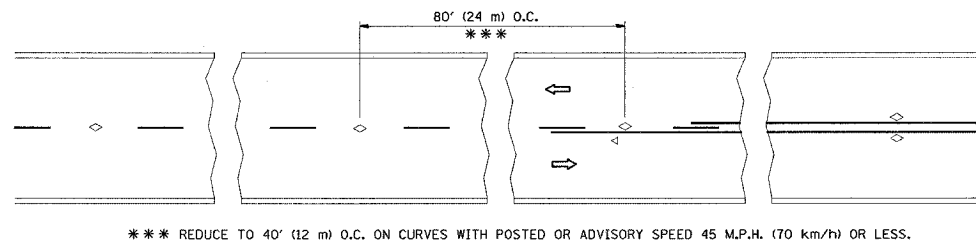
ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: DATE: 3/28/2007
 DRAWN BY: CHECKED BY:

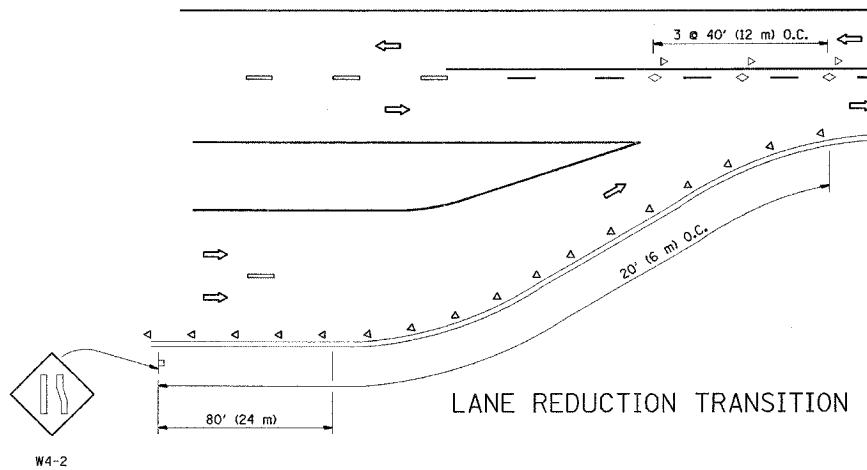
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 REVISION DATE: 02/15/07

PLOT DATE = 3/28/2007
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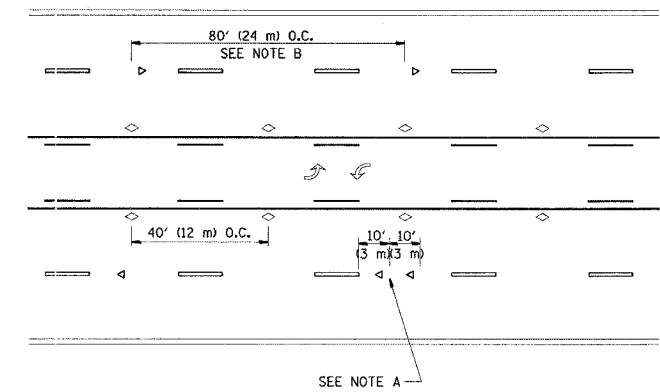
F.A.I.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	14
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



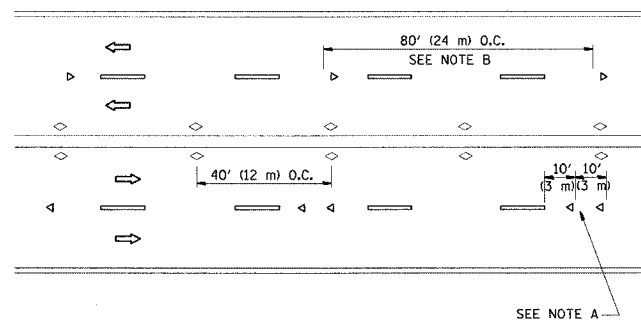
TWO-LANE/TWO-WAY



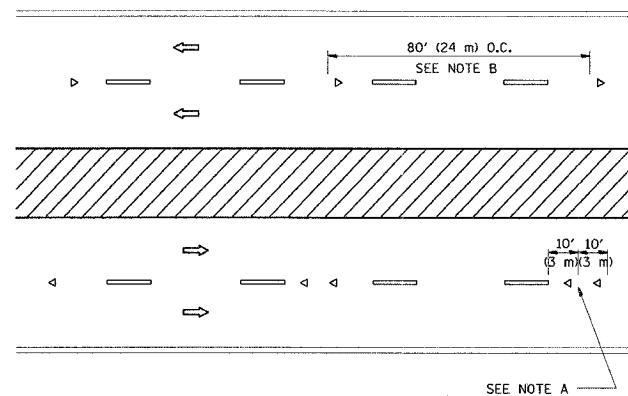
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◇ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in millimeters (Inches) unless otherwise shown.

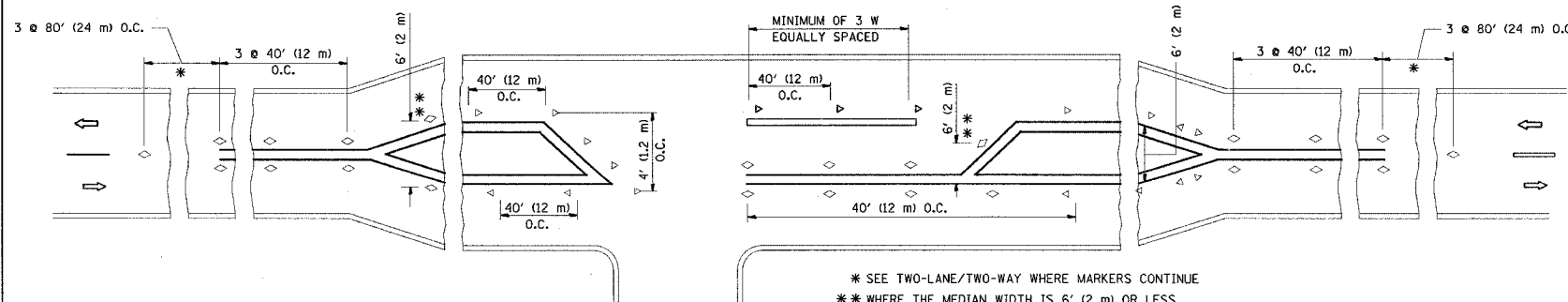
REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT
 MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE
 DATE: 3/28/2007

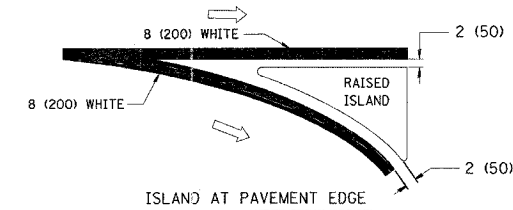
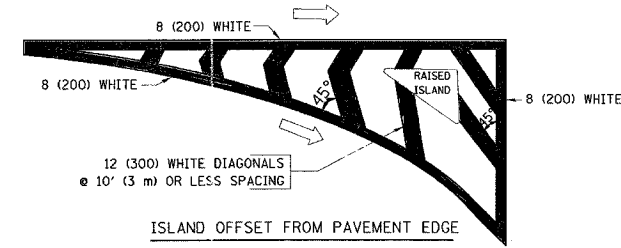
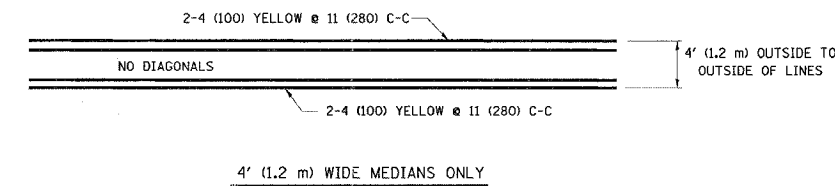
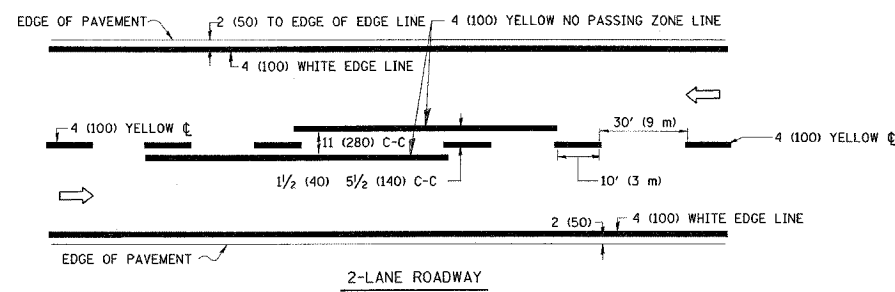
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 CHECKED BY

TC-11
 REVISION DATE: 01/06/00

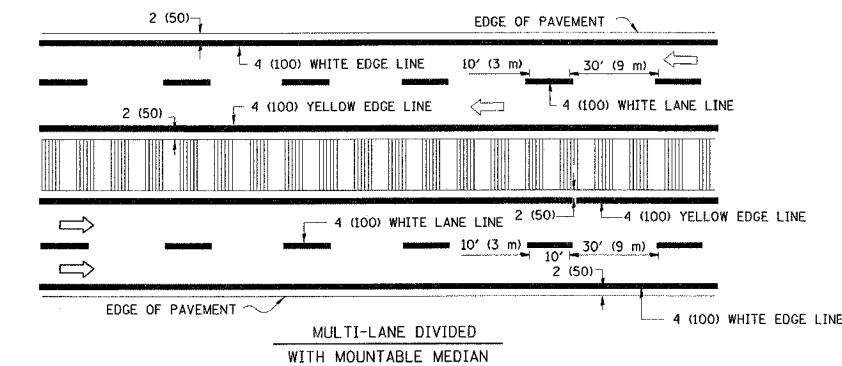
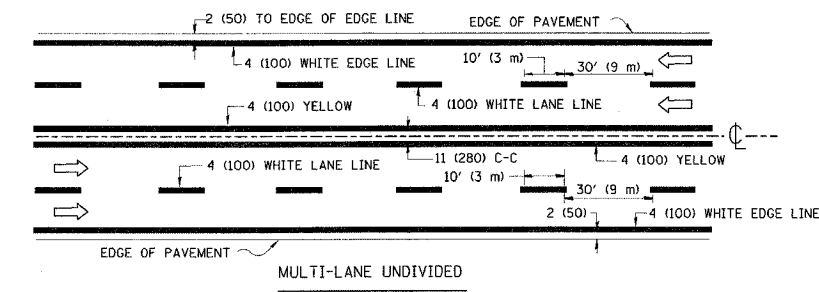


LEFT TURN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	15
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

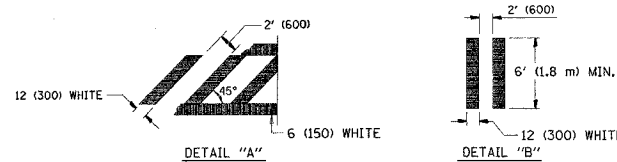
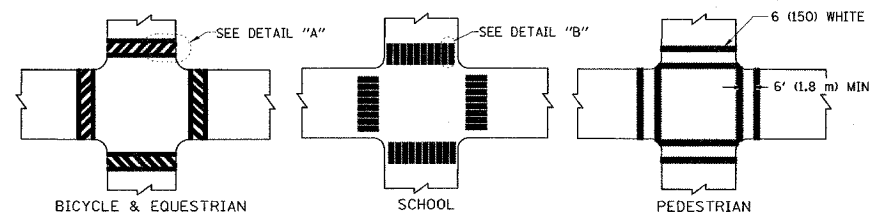


TYPICAL ISLAND MARKING

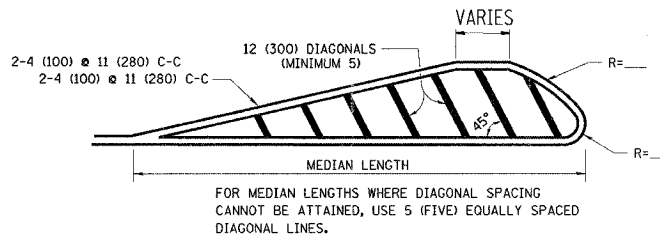


NOTE: MEDIANS WITH BARRIER CURBS DO NOT REQUIRE AN EDGE LINE

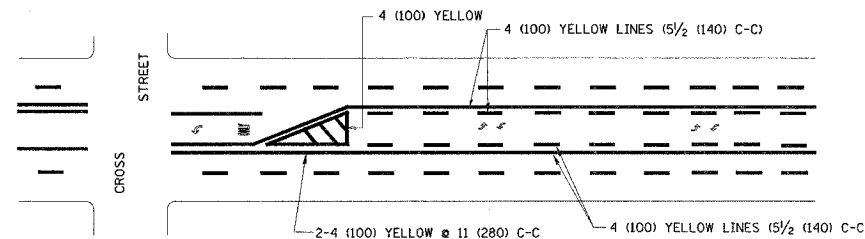
TYPICAL LANE AND EDGE LINE MARKING



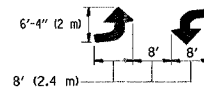
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE

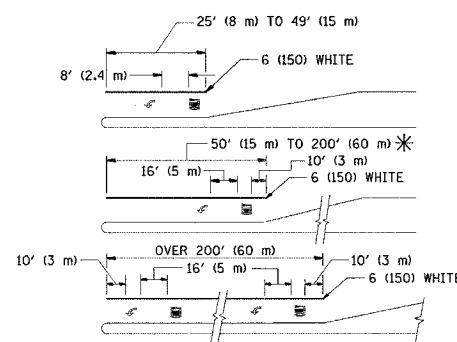


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

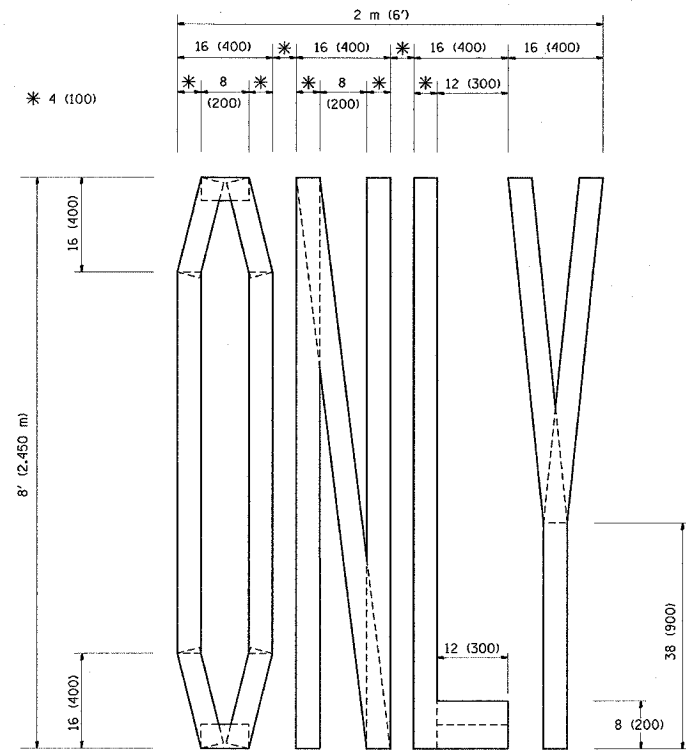
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE
DATE: 3/28/2007

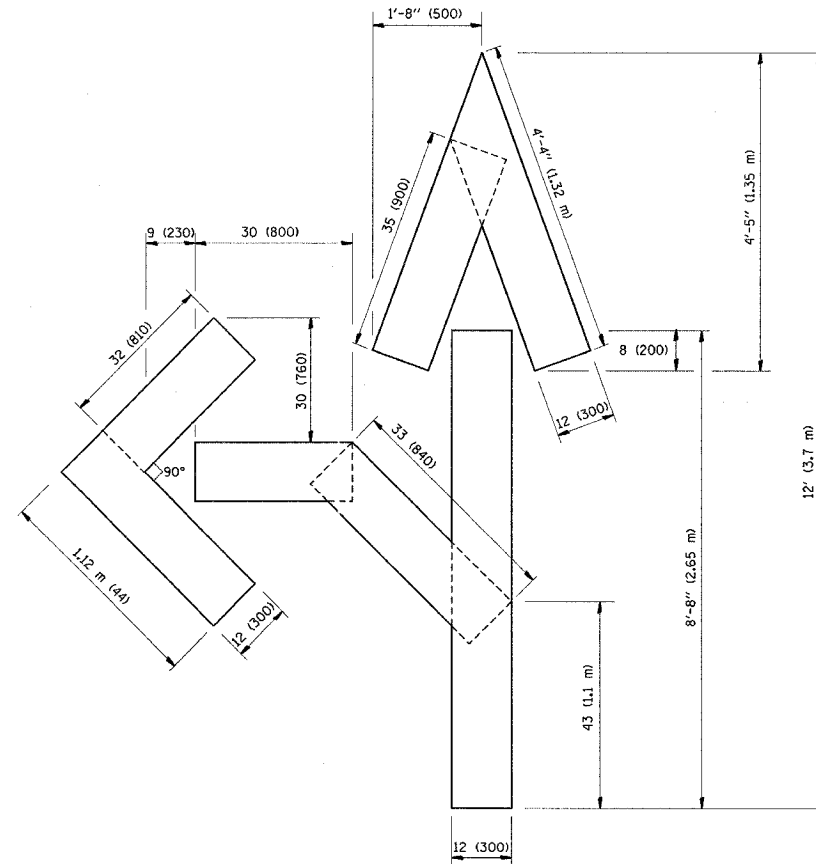
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CHECKED BY

TC-13
REVISION DATE: 01/06/00

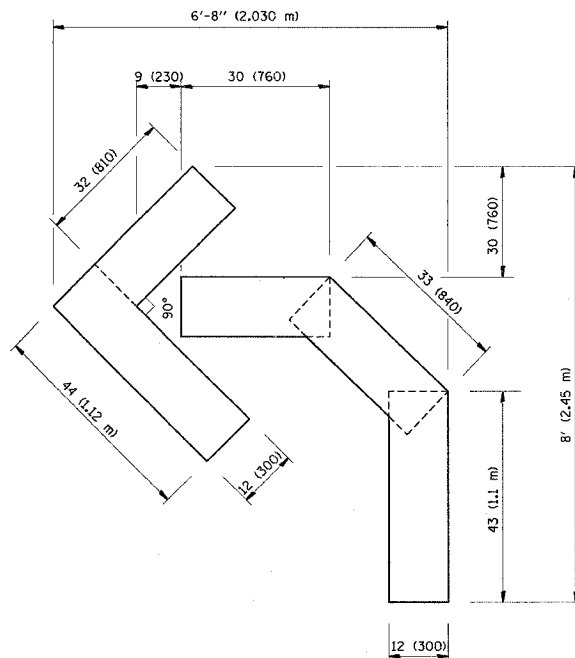
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	16
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in millimeters (Inches) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

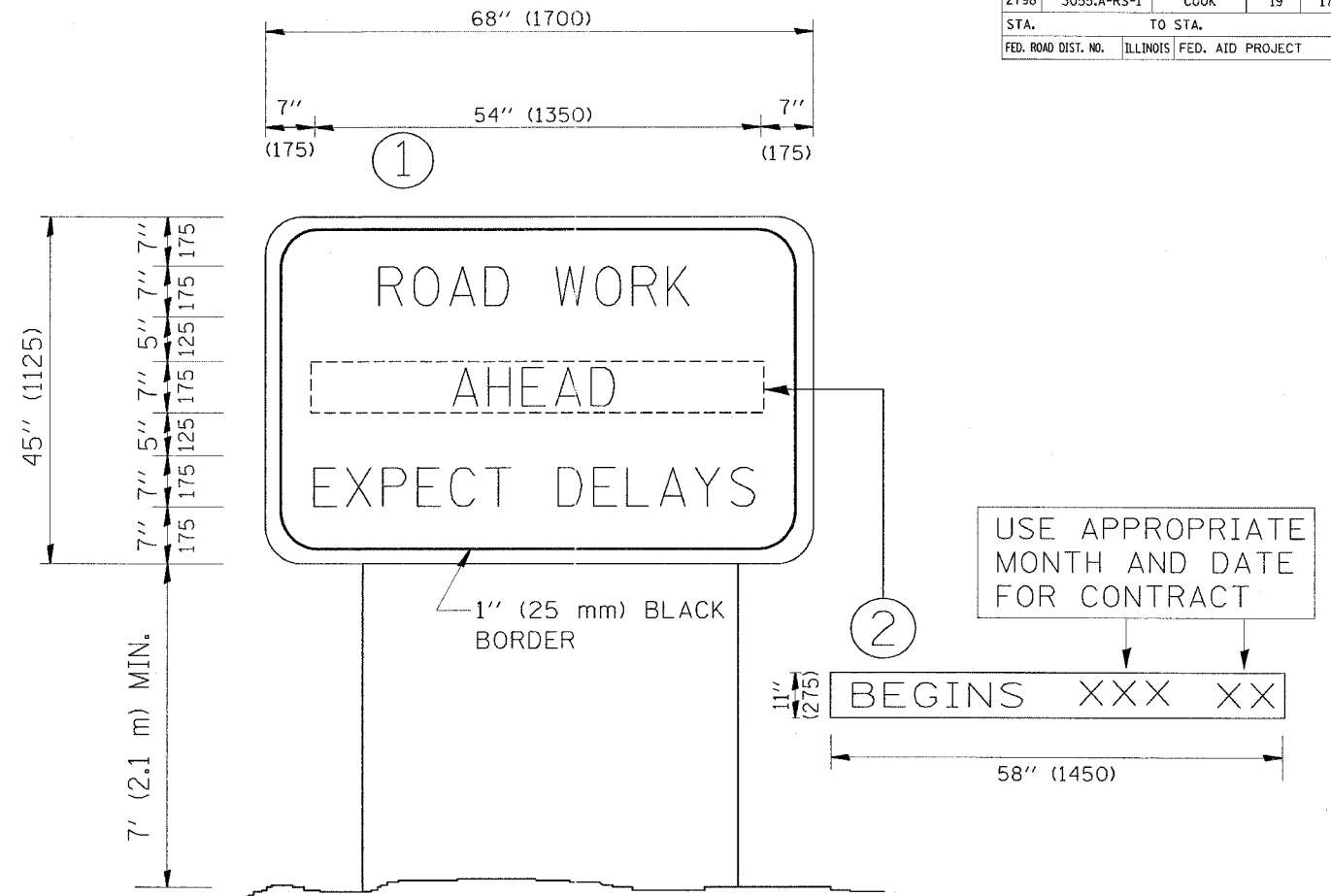
ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE
 DATE: 3/28/2007

DRAWN BY CADD
 CHECKED BY TC-16

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	17
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99
C. JUCIUS	1-31-07

ILLINOIS DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE:
DATE: 3/28/2007

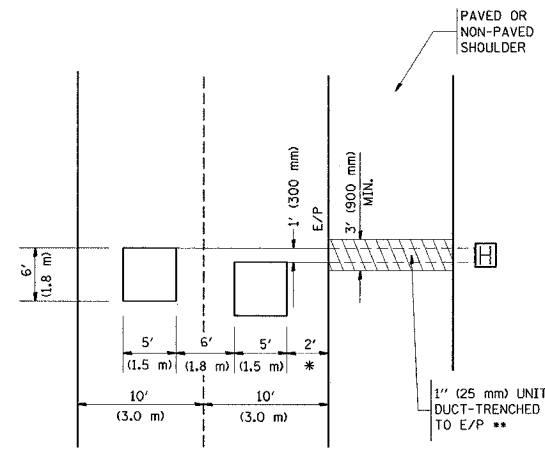
DRAWN BY DESIGN
CHECKED BY

TC22
REVISION DATE: 01/31/07

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	18
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



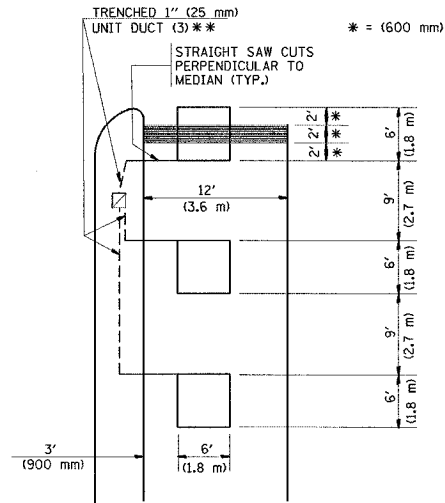
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



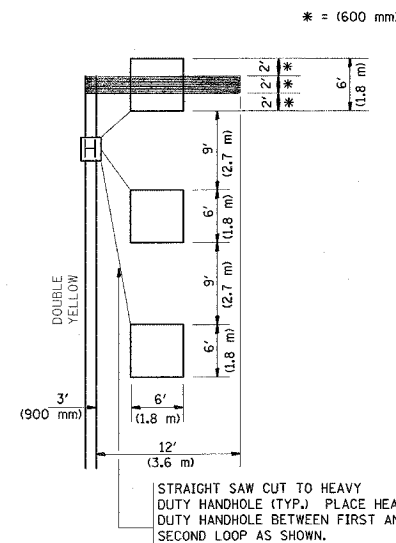
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

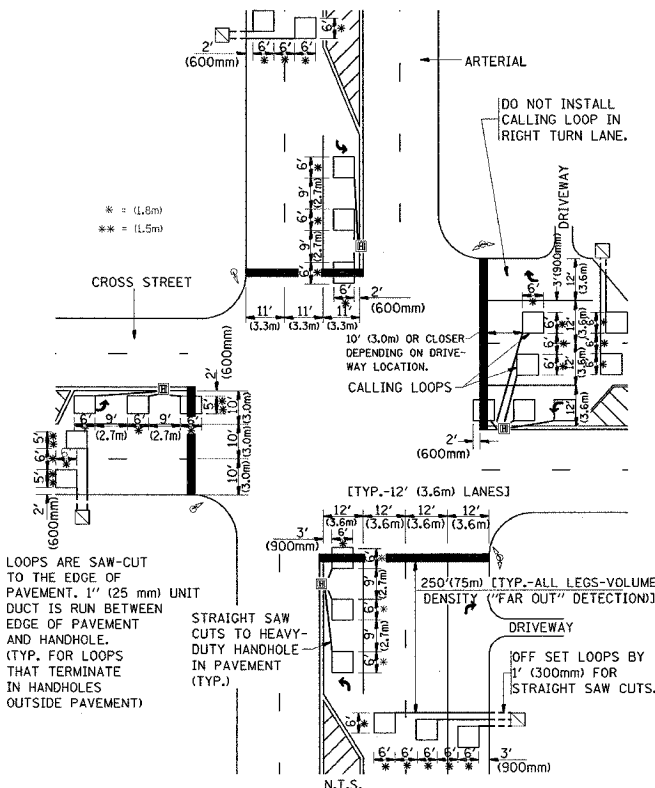


* = (600 mm)

STRAIGHT SAW CUT TO HEAVY DUTY HANDHOLE (TYP.) PLACE HEAVY DUTY HANDHOLE BETWEEN FIRST AND SECOND LOOP AS SHOWN.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)

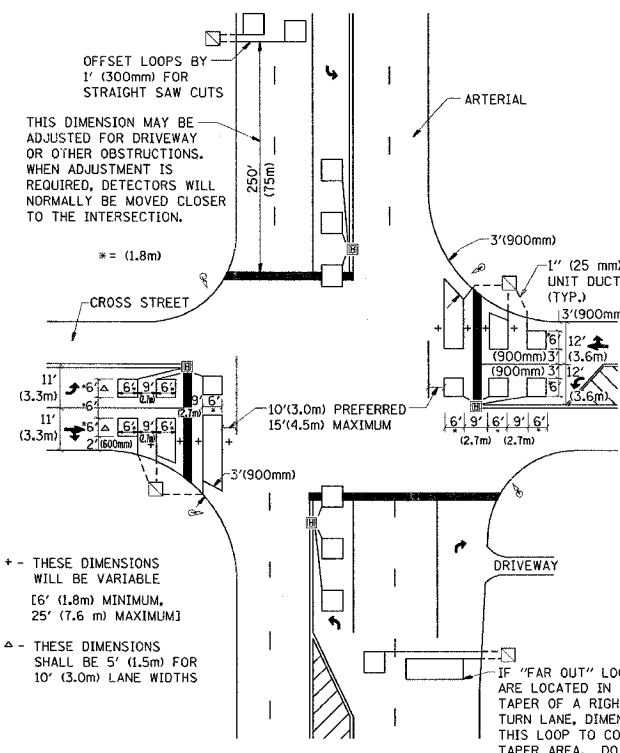


LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

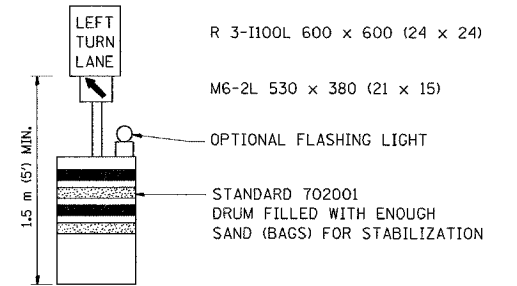
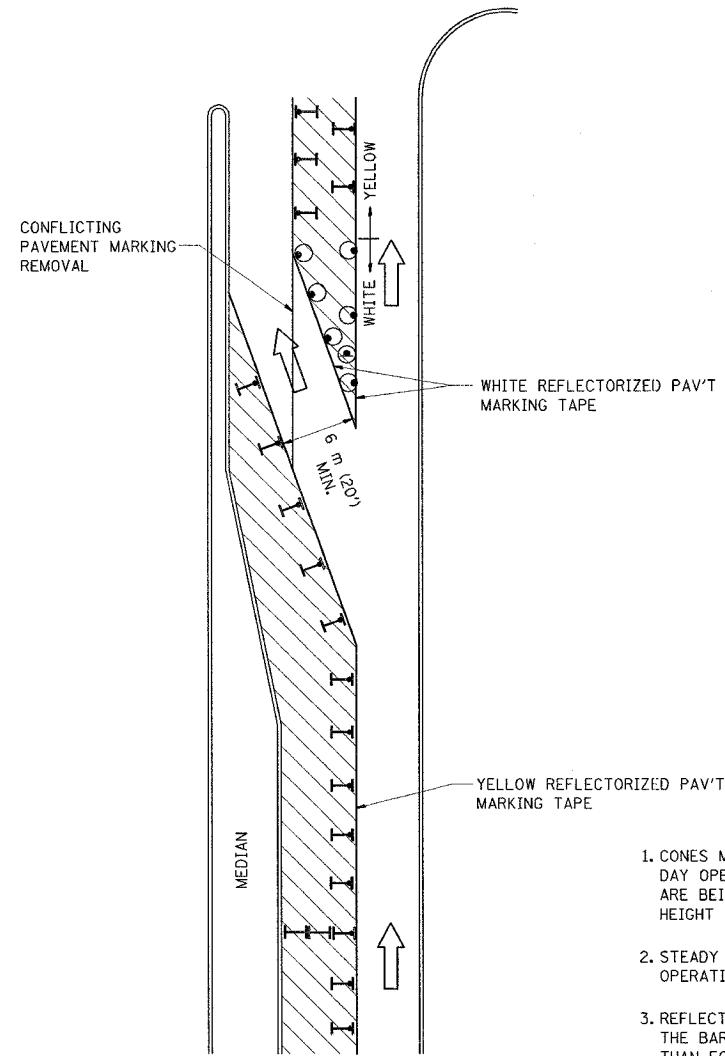
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
NAME	DATE	
		DESIGNED BY DRAWN BY CADD CHECKED BY R.K.F. SCALE: NONE DATE: 3/28/2007

TS07
REVISION DATE:

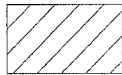





F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	3055.A-RS-1	COOK	19	19
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 710 (28) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 1.5 m (5').
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 600 x 600 (24 x 24) AND M6-2R 530 x 380 (21 x 15) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL AND PROTECTION
 AT TURN BAYS
 (TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE
 DATE: 3/28/2007
 DRAWN BY
 CHECKED BY LHA