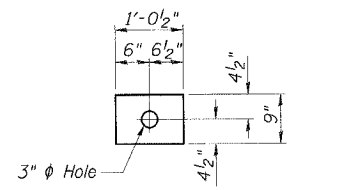


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

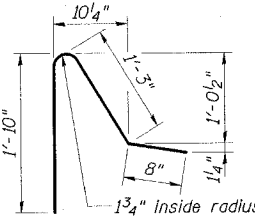
ROUTE NO.	SECTION	COUNTY	SHEETS	SHEET NO.
FAP 328	*	CLAY	61	23
PREL. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	ASD	26 SHEETS

NOTES

- Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270.
- The nominal diameter shall be $\frac{1}{2}$ " and the nominal cross-sectional area shall be 0.153 sq. in.
- Lifting loops shall be 2 - $\frac{1}{2}$ " ϕ - 270 ksi strands, as shown.
- The 1" ϕ rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets that receive transverse tie bar on outside shall be filled with grout after transverse tie assembly is in place.
- Non prestressing steel shall conform to AASHTO M-31 or M-322, Grade 60.
- The bearing seat surfaces shall be adjusted by shimming to assure firm and even bearing. Two $\frac{1}{8}$ " fabric adjusting shims of the dimensions shown shall be provided for each bearing.
- Keyway surfaces shall be cleaned to remove form oil or other bond breaking material prior to shipment of the beams. Cleaning shall be done by sandblasting the keyway areas between top of the beam and the bottom edge of the key.
- Required Release Strength, f'cl, shall be 4,000 p.s.i.
- See Drawing 8 of 26 for location of rail anchors.
- The cut strands at each beam end shall be given two coats of zinc dust spray or paint meeting the requirements of ASTM A780. The zinc dust spray or paint shall be applied before corrosion appears and allowed to dry according to manufacturer's specifications prior to another coat of zinc. This work shall be performed by the producer and included with the cost of the beam.



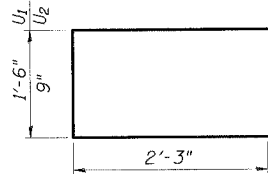
FABRIC ADJUSTING SHIMS
(Fixed shown, Expansion similar without hole)



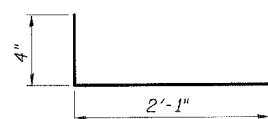
BAR D



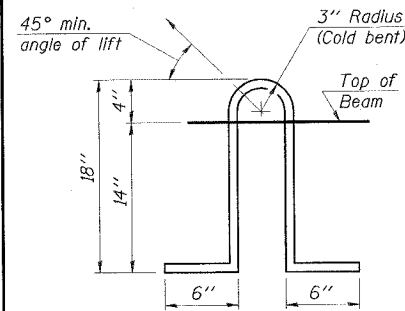
BAR E



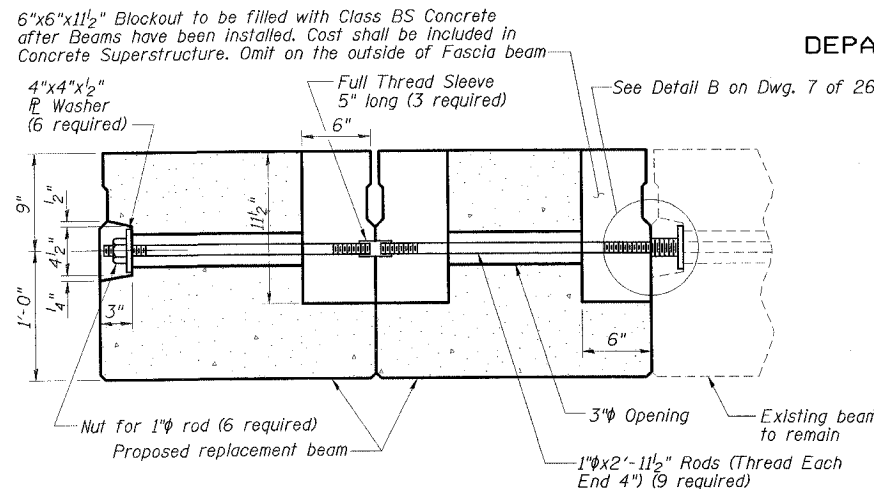
BAR U1 & U2



BAR X

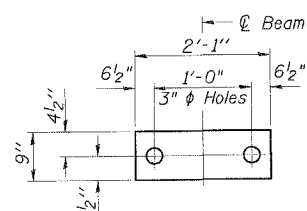


LIFTING LOOP DETAIL

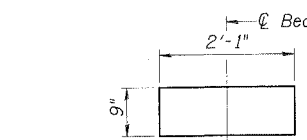


Note:
Existing grout covering existing nuts for transverse tie assemblies shall be removed. Existing rods shall be cut as detailed on Dwg. 5 of 26. Existing rods shall then be removed through holes in exterior beams. Cost included with Removal of Existing PPC Deck Beams.

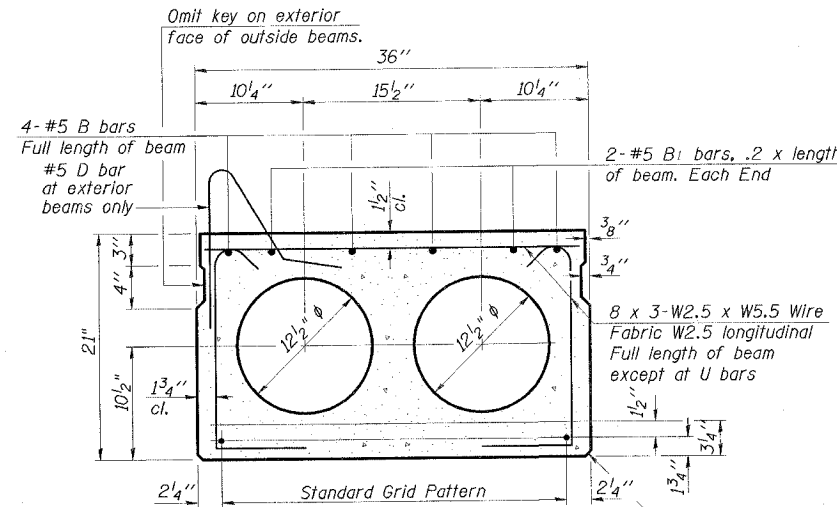
TYPICAL TRANSVERSE TIE ASSEMBLY



FIXED FABRIC BEARING PAD
At Abutments

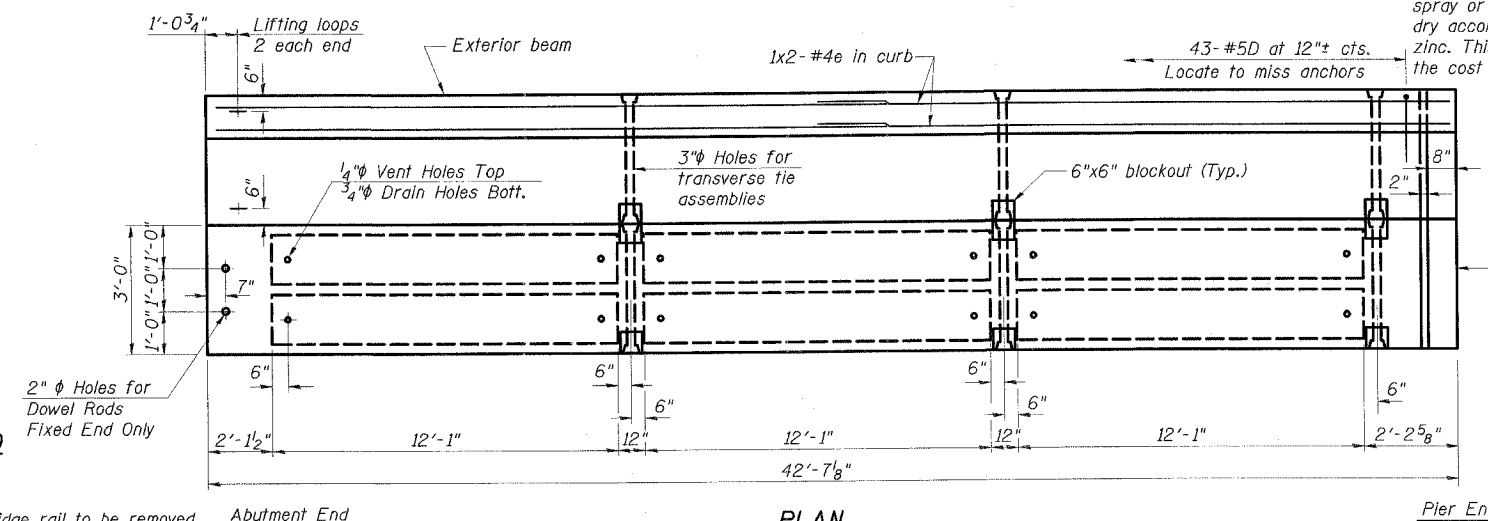


EXPANSION FABRIC BEARING PAD
At Pier

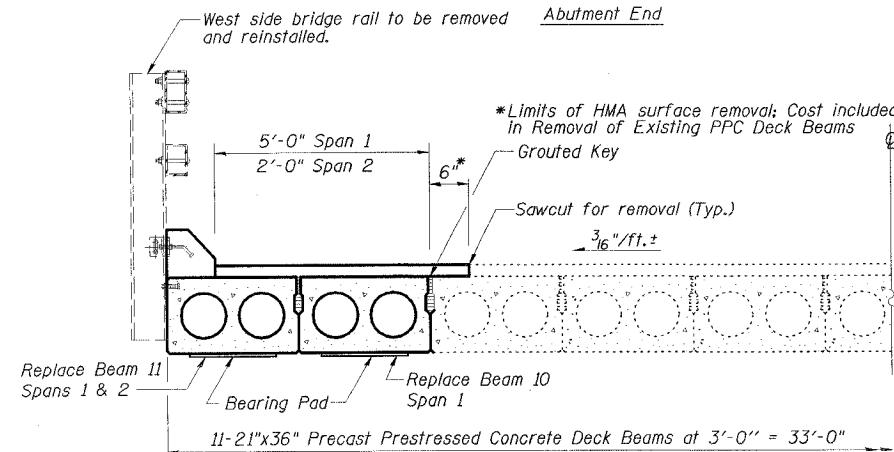


TYPICAL SECTION

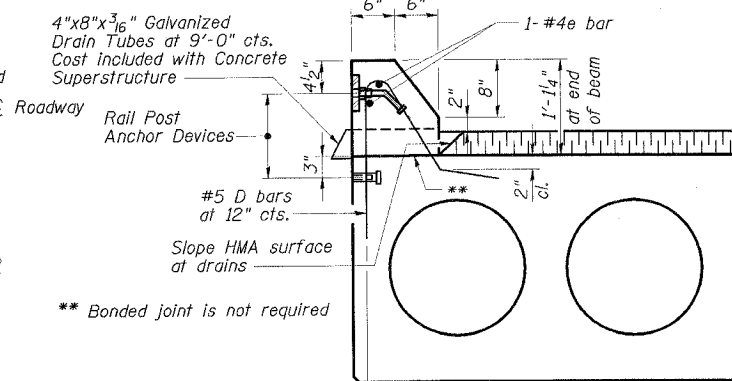
10- $\frac{1}{2}$ " ϕ Strands, Each Strand Stressed to 30,900 Lbs.
6-Strands $1\frac{3}{4}$ " up, 4-Strands $3\frac{1}{4}$ " up



PLAN



HALF CROSS SECTION
(Looking North)

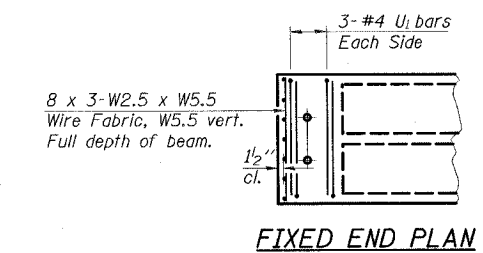


SECTION THRU CURB

Curb shall be poured in the field and paid for as Concrete Superstructure

BILL OF MATERIAL

Item	Unit	Total
Precast Prestressed Conc. Deck Bms. (21" Depth)	Sq. Ft.	384



FIXED END PLAN

DECK BEAM REPLACEMENT DETAILS
US ROUTE 45 OVER
BUCK CREEK
FAP RTE 328-SECTION (6BR-2)B-1
CLAY COUNTY
STATION 1510+13.00
STRUCTURE NO. 013-0041

ESCA
CONSULTANTS, INC.

DESIGNED BY: ELH 01/07
DRAWN BY: CJ 01/07
CHECKED BY: ELH 04/07
APPROVED BY: RDP 04/07