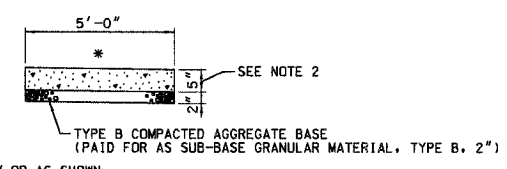


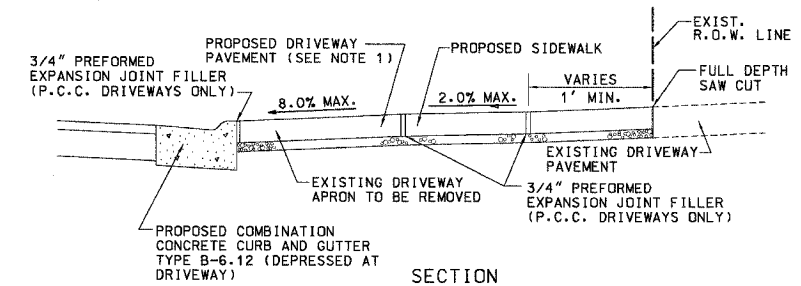
TYPICAL ALLEY RETURN
N.T.S.



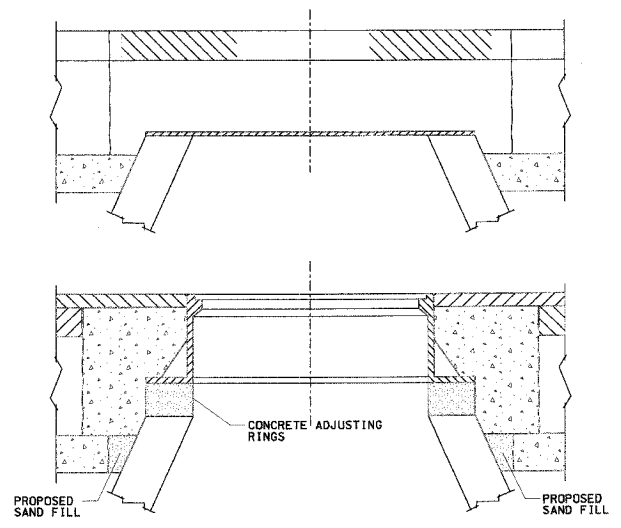
- * CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS
- NOTES:**
- ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE PAID FOR AS P.C.C. SIDEWALK 5 INCH.
 - THICKNESS SHALL BE INCREASED TO 7" WHERE SIDEWALK IS ADJACENT TO A CONCRETE DRIVEWAY. (COST INCIDENTAL)

P.C.C. SIDEWALK 5 INCH
DETAIL

- NOTES:**
- THE PROPOSED DRIVEWAY PAVEMENT SHALL CONSIST OF:
 - 8" PORTLAND CEMENT CONCRETE AND 4" OF AGGREGATE BASE COURSE TYPE B IF THE EXISTING DRIVEWAY IS P.C. CONCRETE, OR
 - 2" HOT-MIX ASPHALT SURFACE COURSE, 6" AGGREGATE BASE COURSE TYPE B IF THE EXISTING DRIVEWAY IS BITUMINOUS OR AGGREGATE.
 - PREFORMED EXPANSION JOINT FILLER SHALL BE CONSIDERED INCIDENTAL TO P.C.C. DRIVEWAY PAVEMENT, OR
 - ALL REQUIRED EARTH EXCAVATION AND DRIVEWAY REMOVAL REQUIRED TO CONSTRUCT DRIVES SHALL BE PAID FOR AS DRIVEWAY PAVEMENT REMOVAL.



SECTION
TYPICAL DRIVEWAY DETAILS
N.T.S.



CONSTRUCTION PROCEDURES

- STAGE I (BEFORE PAVEMENT MILLING)
- REMOVE A MINIMUM OF 12" OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - COVER THE STRUCTURE OPENING WITH A 36" DIAMETER METAL PLATE.
 - BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2" THICK HOT-MIX ASPHALT MATERIAL APPROVED BY THE ENGINEER.
- STAGE II (AFTER PAVEMENT MILLING)
- REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
 - INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE.
- THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602 AND 603 OF THE STANDARD SPECIFICATIONS.

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

STRUCTURE TO BE ADJUSTED (SPECIAL).

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
CONSTRUCTION DETAILS

SCALE:
DATE 2/14/2007

DRAWN BY BEH
CHECKED BY LMF

DATE: _____ BY: _____
 CHECKED: _____
 ALIGNED: _____
 PLOTTED: _____
 PLAN NO. _____

CHRISTOPHER B. BURKE ENGINEERING LTD.
 9575 West Higgins Road, Suite 800
 Rosemont, Illinois 60018
 (847) 823-0500



DATE: _____ BY: _____
 CHECKED: _____
 ALIGNED: _____
 PLOTTED: _____
 PROFILE NO. _____