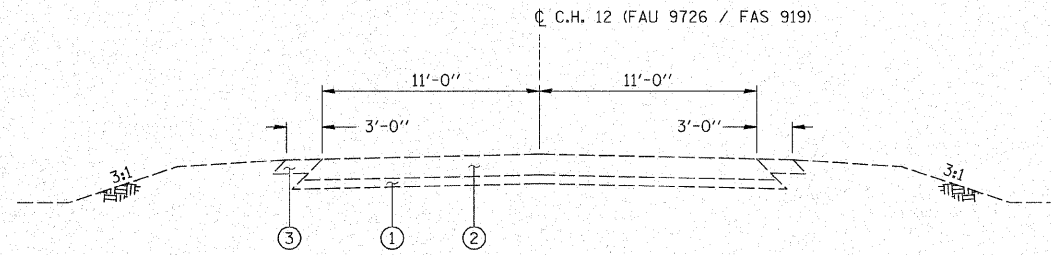
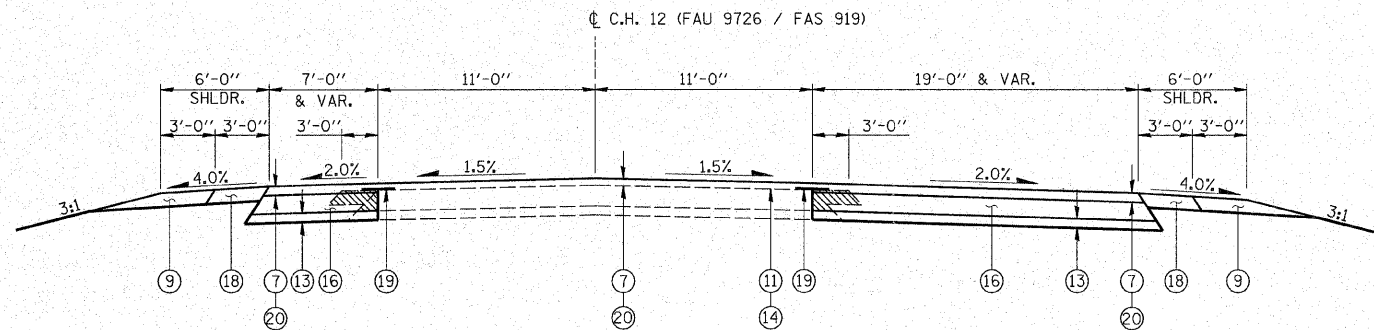


ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 9713	99-02118-00-FP	JACKSON	122	14
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT-	
CONTRACT NO. 99219				



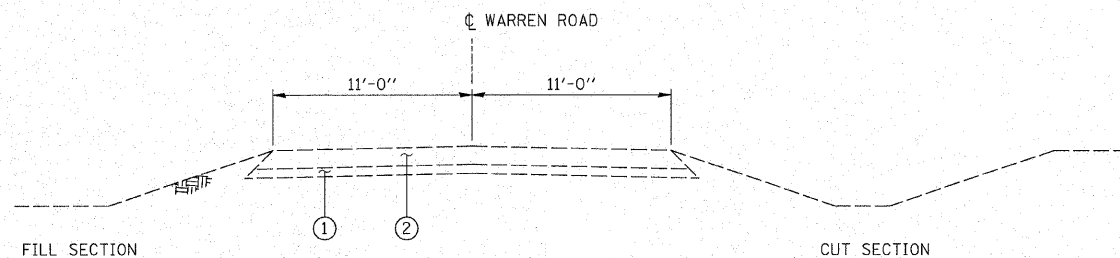
EXISTING TYPICAL CROSS SECTION

C.H. 12 (GIANT CITY ROAD)



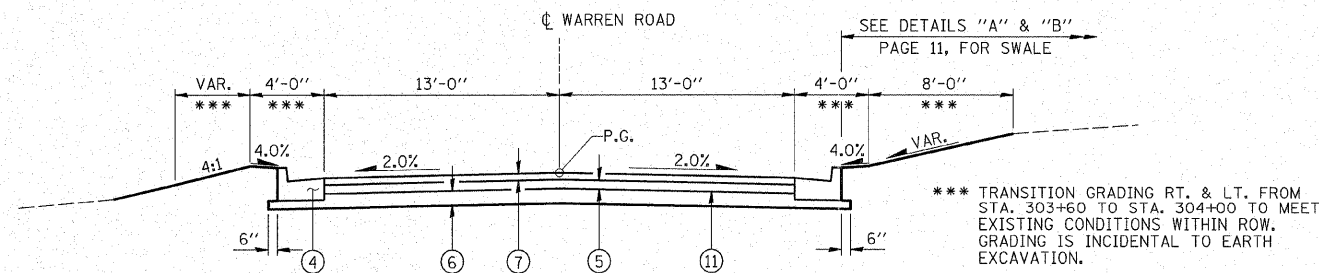
PROPOSED TYPICAL CROSS SECTION

STA. 88+86.23 TO STA. 101+53.78 (GIANT CITY ROAD)



EXISTING TYPICAL CROSS SECTION

(WARREN ROAD)



PROPOSED TYPICAL CROSS SECTION

STA. 300+15.24 TO STA. 303+60.00 (WARREN ROAD)

PAVEMENT DESIGN

GIANT CITY ROAD
 DESIGN PERIOD: 20 YEARS
 STRUCTURAL DESIGN TRAFFIC (S.D.T.) = 8,863 YEAR 2015
 P.V. = 96.86% S.U. = 2.46% M.U. = 0.68%
 CLASS II ROAD
 PERCENT OF S.D.T. IN DESIGN LANE P = 50 S = 50 M = 50
 80,000 LB. LOAD LIMIT
 DESIGN PER BLR MANUAL 37-8
 MINIMUM SOIL SUPPORT: IBR = 2
 TRAFFIC FACTOR = 0.49

PAVEMENT DESIGN

WARREN ROAD
 DESIGN PERIOD: 20 YEARS
 STRUCTURAL DESIGN TRAFFIC (S.D.T.) = 2182 YEAR 2015
 P.V. = 97% S.U. = 2% M.U. = 1%
 CLASS II ROAD
 PERCENT OF S.D.T. IN DESIGN LANE P = 50 S = 50 M = 50
 73,280 LB. LOAD LIMIT
 DESIGN PER BLR MANUAL 37-3
 MINIMUM SOIL SUPPORT: ERI = 2KSI
 TRAFFIC FACTOR = 0.11

LEGEND

- ① EXISTING AGGREGATE BASE
- ② EXISTING BITUMINOUS PAVEMENT
- ③ EXISTING BITUMINOUS AGGREGATE MIXTURE SHOULDER
- ④ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ⑤ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (2 1/4" THICKNESS)
- ⑥ AGGREGATE BASE COURSE, TYPE A (8" THICKNESS)
- ⑦ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (2" THICKNESS)
- ⑧ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (3 3/4" THICKNESS)
- ⑨ AGGREGATE SHOULDERS, TYPE A (6" THICKNESS)
- ⑩ BITUMINOUS MATERIALS (PRIME COAT)
- ⑬ AGGREGATE BASE COURSE, TYPE A (4" THICKNESS)
- ⑭ AGGREGATE (PRIME COAT)
- ⑮ HOT-MIX ASPHALT BASE COURSE, (8" THICKNESS)
- ⑰ HOT-MIX ASPHALT SHOULDER 6"
- ⑱ STRIP REFLECTIVE CRACK CONTROL
- ⑳ LEVELING BINDER (MACHINE METHOD) N70 (VARIABLE DEPTH) (1 1/4" MIN)
- ▨ BITUMINOUS CONCRETE SHOULDER REMOVAL (INCIDENTAL TO EARTH EXCAVATION)
- * S.E. SLOPE OR 4.0% WHICHEVER IS GREATER
- ** ALGEBRAIC DIFFERENCE BETWEEN SHOULDER SLOPE AND SUPERELEVATION RATE NOT TO EXCEED 8.0% WITH A MAXIMUM SLOPE OF 4.0%

Rice, Berry and Associates
 A Division of Hampton,
 Lenzi and Renwick, Inc.
HLR
 Civil & Structural Engineers
 801 S. Durkin Drive
 Springfield, Illinois 62704
 217-546-3400
 Account Number
 03-47-0001-1
 Date: 10-10-06
 DESIGNED: L.F.S. CHECKED: S.W.M. DRAWN: W.J.S.

TYPICAL CROSS SECTIONS
SECTION 99-02118-00-FP
F.A.U. 9713 / EAST PLEASANT HILL ROAD
JACKSON COUNTY