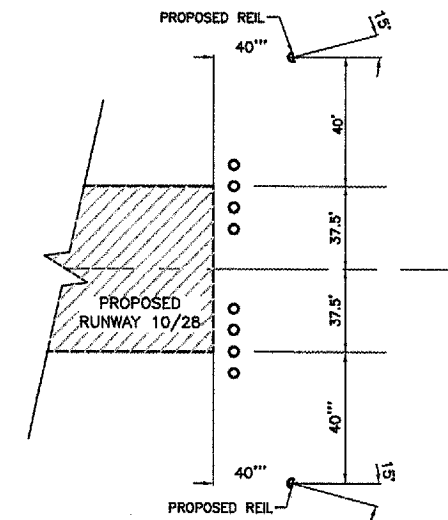


**REIL DETAIL**

NOT TO SCALE

1. THE RELOCATE REIL SYSTEM SHALL BE COMPLETED WITH FLASHHEADS, SPLICE CANS, CURRENT SENSOR, FRANGIBLE FITTINGS AND ALL NECESSARY EQUIPMENT FOR A COMPLETE AND OPERATIONAL SYSTEM. REIL UNIT FAA APPROVED TYPE L-849.
2. THE CONTRACTOR SHALL INSTALL AND ADJUST CURRENT SENSOR AND ISOLATION TRANSFORMER FOR REIL'S TO OPERATE AT 100% BRIGHTNESS OF RUNWAY CIRCUIT ONLY.



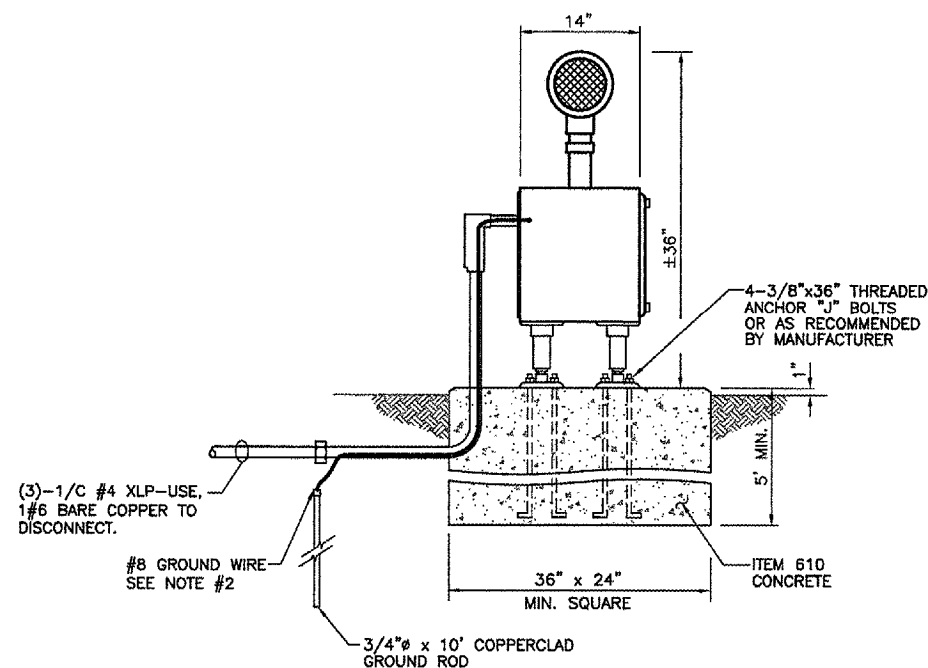
**REVISIONS**

NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS

REMARK RUNWAY 10-28, RUNWAY 28 PAPI  
 RUNWAY 28 REIL RELOCATION  
 AND DETAILS



**REIL DETAIL**

NOT TO SCALE

**REIL NOTES**

1. THE BEAM CENTERLINE OF EACH LIGHT UNIT SHALL BE AIMED 15 DEGREES OUTWARD FROM A LINE PARALLEL TO RUNWAY CENTERLINE AND INCLINED AT AN ANGLE 10 DEGREES ABOVE THE HORIZONTAL. IF THIS ANGLE IS OPERATIONALLY OBJECTABLE, CONTRACTOR SHALL PROVIDE AN OPTICAL BAFFLE AND ORIENT THE BEAM AXIS OF UNIT 10 DEGREES OUTWARD FROM A LINE PARALLEL TO THE RUNWAY CENTERLINE AND INCLINED AT AN ANGLE OF 3 DEGREES ABOVE THE HORIZONTAL AT NO ADDITIONAL COST.
2. ALL CONNECTIONS TO GROUND ROD SHALL BE ONE-SHOT EXOTHERMIC CADWELD OR EQUAL.
3. REIL MASTER AND SLAVE UNITS SHALL BE RELOCATED BY THE CONTRACTOR. ALL OTHER ITEMS INCLUDING SPLICE CAN, GROUNDING, POWER AND CONTROL WIRES, AND CURRENT SENSOR SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR.

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**DPA**  
 DuPage Airport



DESIGN BY:	AB
DRAWN BY:	JRO
CHECKED BY:	AB
APPROVED BY:	DLP
DATE:	04/20/07
JOB No:	07257-04

IL PROJECT: DPA-3695  
 A.I.P. PROJECT: 3-17-0017-B21