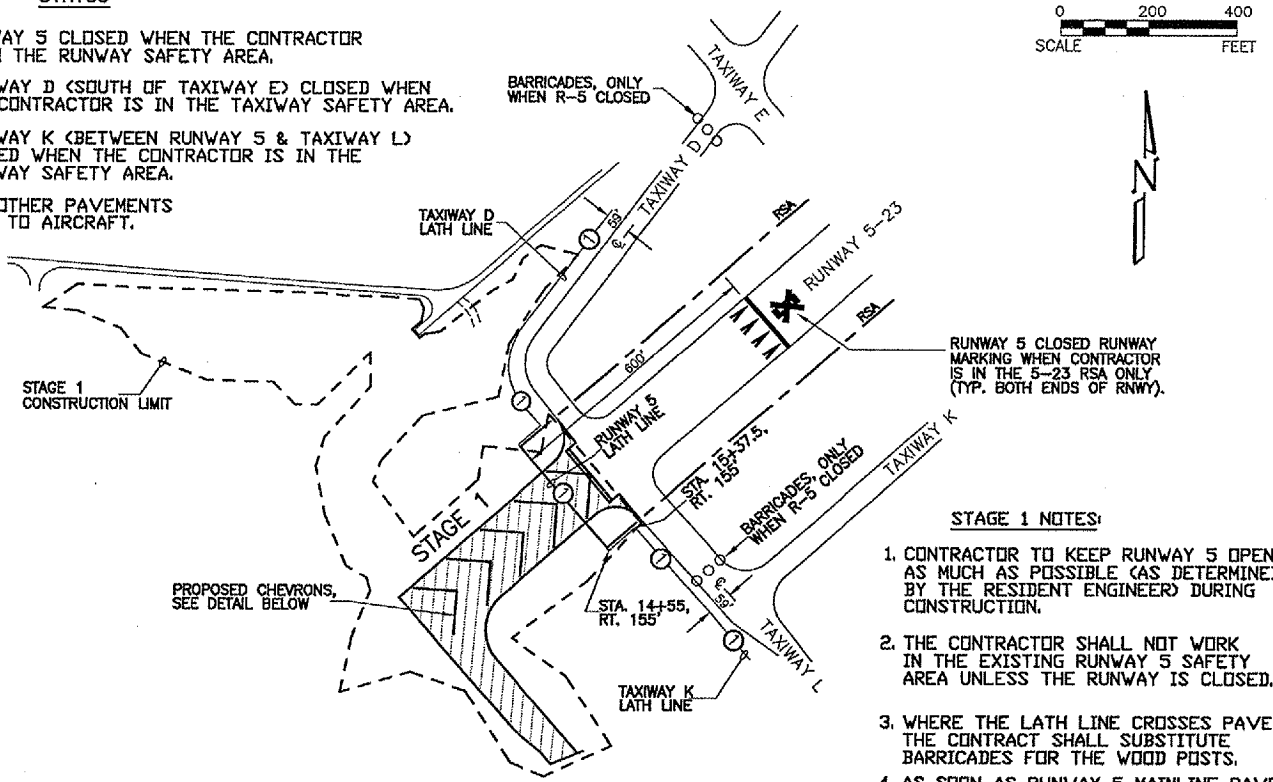


STAGE 1 AIRFIELD STATUS

1. RUNWAY 5 CLOSED WHEN THE CONTRACTOR IS IN THE RUNWAY SAFETY AREA.
2. TAXIWAY D (SOUTH OF TAXIWAY E) CLOSED WHEN THE CONTRACTOR IS IN THE TAXIWAY SAFETY AREA.
3. TAXIWAY K (BETWEEN RUNWAY 5 & TAXIWAY L) CLOSED WHEN THE CONTRACTOR IS IN THE TAXIWAY SAFETY AREA.
4. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.



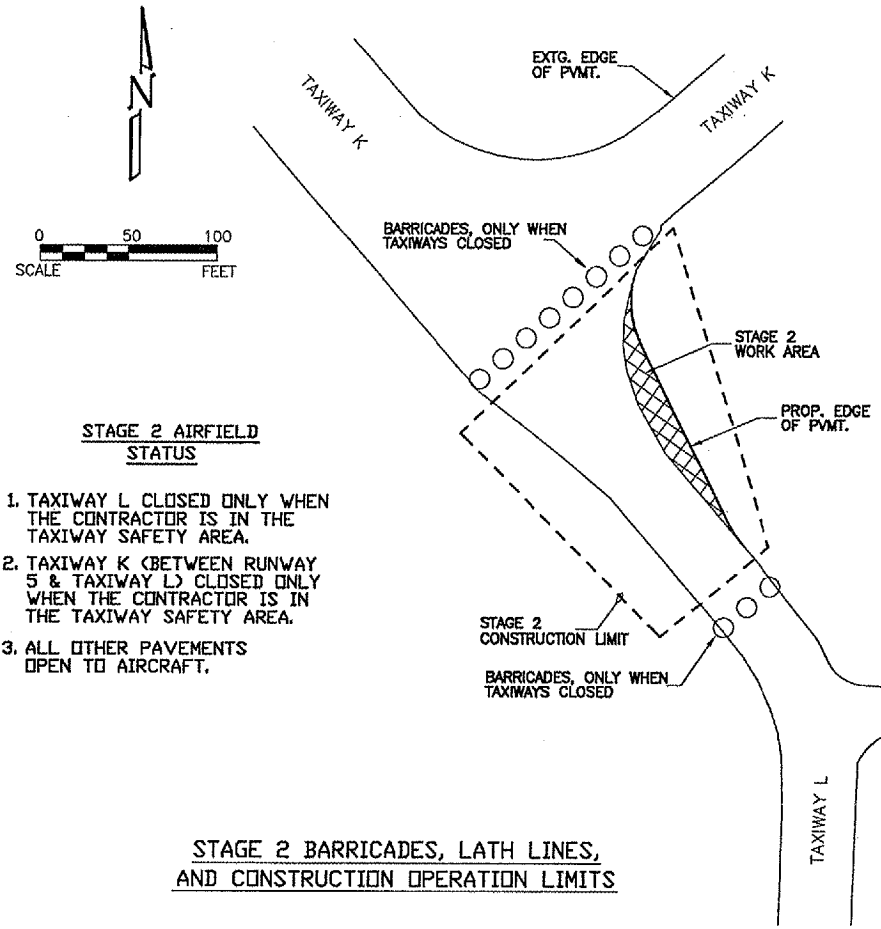
STAGE 1 NOTES:

1. CONTRACTOR TO KEEP RUNWAY 5 OPEN AS MUCH AS POSSIBLE (AS DETERMINED BY THE RESIDENT ENGINEER) DURING CONSTRUCTION.
2. THE CONTRACTOR SHALL NOT WORK IN THE EXISTING RUNWAY 5 SAFETY AREA UNLESS THE RUNWAY IS CLOSED.
3. WHERE THE LATH LINE CROSSES PAVEMENTS, THE CONTRACTOR SHALL SUBSTITUTE BARRICADES FOR THE WOOD POSTS.
4. AS SOON AS RUNWAY 5 MAINLINE PAVEMENT AND TAXIWAY D / K FILLETS INSTALLED, COMPLETE SHOULDER GRADING IN THE SAFETY AREAS AND REOPEN PAVEMENTS TO AIRCRAFT TRAFFIC.

STAGE 1 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 2 AIRFIELD STATUS

1. TAXIWAY L CLOSED ONLY WHEN THE CONTRACTOR IS IN THE TAXIWAY SAFETY AREA.
2. TAXIWAY K (BETWEEN RUNWAY 5 & TAXIWAY L) CLOSED ONLY WHEN THE CONTRACTOR IS IN THE TAXIWAY SAFETY AREA.
3. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.



STAGE 2 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 2 NOTES:

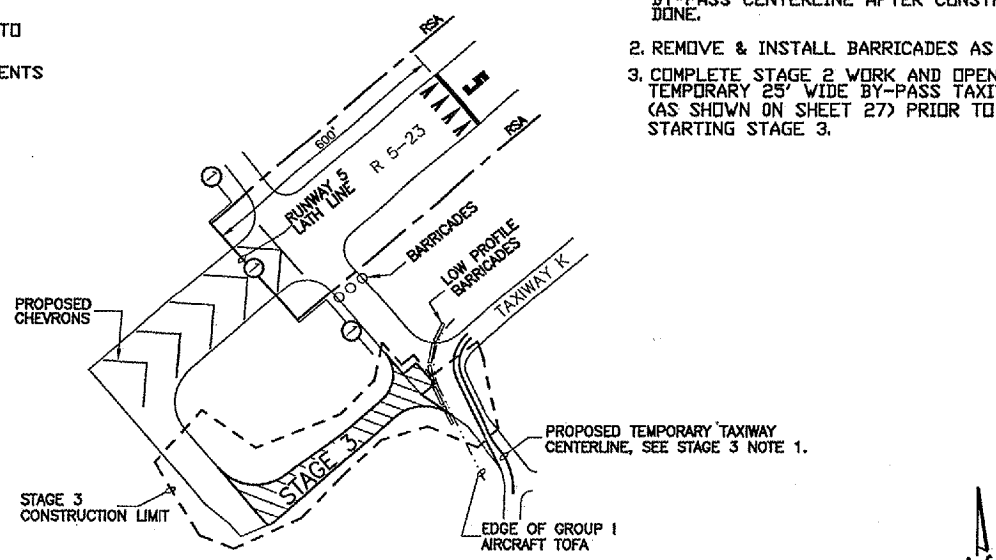
1. THE MAA WILL CLOSE THE TAXIWAYS FOR THE DURATION OF THE DAILY CONTINUOUS WORK PERIODS ONLY. PRIOR TO THE END OF EACH WORK PERIOD, THE CONTRACTOR SHALL REGRADE THE SAFETY AREAS, CLEAN THE PAVEMENT SURFACES TO THE SATISFACTION OF THE RESIDENT ENGINEER AND REOPEN THE TAXIWAYS TO AIRCRAFT TRAFFIC.
2. EARTHWORK, BASE ROCK INSTALLATION, AND BITUMINOUS SURFACE COURSE INSTALLATION SHALL BE ACCOMPLISHED IN ONE SINGLE CONTINUOUS WORK PERIOD. THE CONTRACTOR SHALL MAINTAIN AIRCRAFT ACCESS TO THE GENERAL AVIATION "T" HANGARS AT ALL TIMES WITH THE EXCEPTION OF THE PERIOD OF TIME WHEN BASE ROCK AND BITUMINOUS SURFACE COURSE MATERIALS ARE BEING INSTALLED.
3. PRIOR TO THE END OF EACH WORK PERIOD, THE CONTRACTOR SHALL BACKFILL ALL EXCAVATIONS IN THE TAXIWAY SAFETY AREAS.
4. PRIOR TO THE END OF EACH WORK PERIOD, THE CONTRACTOR SHALL INSTALL TEMPORARY TRANSITION RAMPS ON ALL ROCK BASE COURSE OR BITUMINOUS SURFACE COURSE MATERIALS INSTALLED DURING THE WORK PERIOD. MAXIMUM ALLOWABLE PAVEMENT EDGE DROP OFF IS 1 1/2".
5. INSTALL TEMPORARY ABOVE GROUND JUMPER CABLES TO PROVIDE CIRCUIT CONTINUITY IN THE EXISTING EDGE LIGHT SYSTEM AT ALL TIMES.
6. REMOVE & INSTALL BARRICADES AS REQUIRED.
7. COMPLETE STAGE 2 WORK AND OPEN THE TEMPORARY 25' WIDE BY-PASS TAXIWAY (AS SHOWN ON SHEET 27) PRIOR TO STARTING STAGE 3.

STAGE 3 AIRFIELD STATUS

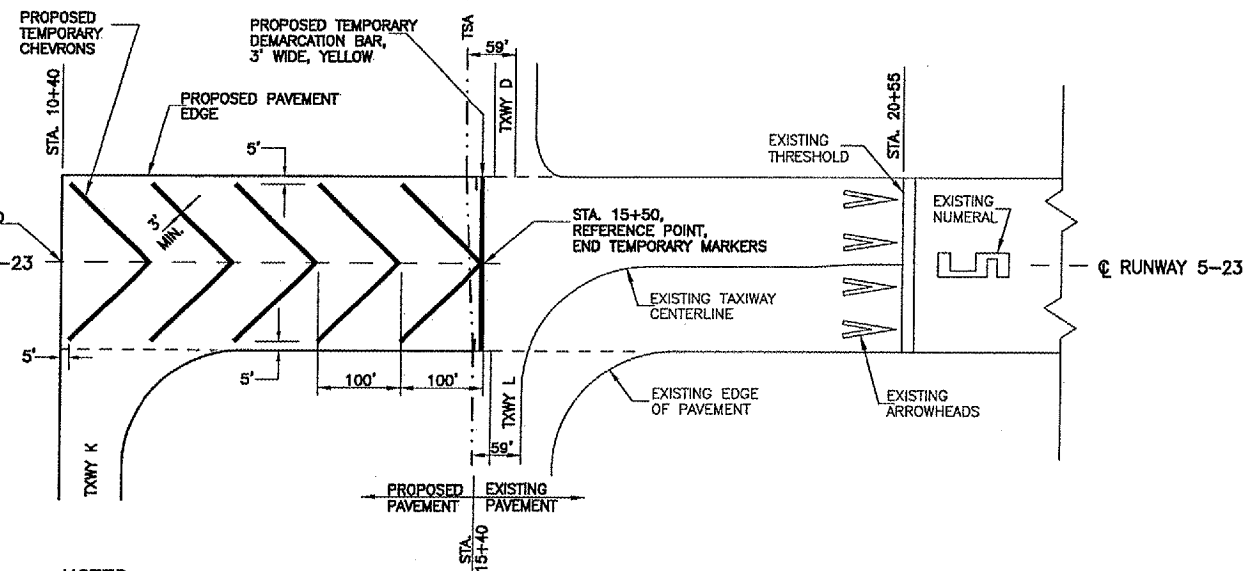
1. TAXIWAY K (BETWEEN RUNWAY 5 & TAXIWAY L) CLOSED.
2. RUNWAY 5 PAVEMENT EXTENSION NOT OPEN TO AIRCRAFT.
3. TAXIWAY L BY-PASS OPEN TO AIRCRAFT.
4. ALL OTHER AIRPORT PAVEMENTS OPEN TO AIRCRAFT.

STAGE 3 NOTES:

1. CENTERLINE OF PROPOSED 25' WIDE TEMPORARY BY-PASS TAXIWAY. TEMPORARY BY-PASS TAXIWAY TO BE IN PLACE PRIOR TO STARTING STAGE 3. SEE SHEET 27 FOR BY-PASS DETAILS. REMOVE BY-PASS CENTERLINE AFTER CONSTRUCTION IS DONE.
2. REMOVE & INSTALL BARRICADES AS REQUIRED.
3. COMPLETE STAGE 2 WORK AND OPEN THE TEMPORARY 25' WIDE BY-PASS TAXIWAY (AS SHOWN ON SHEET 27) PRIOR TO STARTING STAGE 3.



STAGE 3 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS



NOTES:

1. AS SOON AS THE BASE ROCK INSTALLATION IS STARTED, THE CONTRACTOR SHALL IMMEDIATELY INSTALL AND MAINTAIN CHEVRONS AND DEMARCATION BAR ON THE PROPOSED ROCK / PAVEMENT SURFACE AS INDICATED ABOVE. THESE MARKINGS SHALL BE REMOVED AND REINSTALLED AS REQUIRED TO COMPLETE THE PROPOSED WORK AS DIRECTED BY THE RESIDENT ENGINEER. THESE MARKINGS SHALL BE IN PLACE DURING ALL NON-WORKING HOURS, UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. THESE MARKINGS SHALL REMAIN IN PLACE UNTIL THE RUNWAY EXTENSION IS OPENED TO AIRCRAFT TRAFFIC.
2. COLOR OF CHEVRONS SHALL BE AVIATION YELLOW AND INSTALLED AT AN ANGLE OF 45 DEGREES TO THE RUNWAY CENTERLINE. THE CHEVRONS AND DEMARCATION BAR SHALL BE CONSTRUCTED OF ANY SUITABLE, LOCALLY AVAILABLE MATERIALS SUCH AS SNOW FENCE, PLYWOOD, OR OTHER SIMILAR MATERIAL. THEY SHALL BE HELD IN PLACE IN A MANNER LOCALLY DETERMINED TO BE SUITABLE. COSTS FOR SUPPLYING, INSTALLING, AND MAINTAINING THESE MARKINGS SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

TEMPORARY PAVEMENT MARKING PLAN