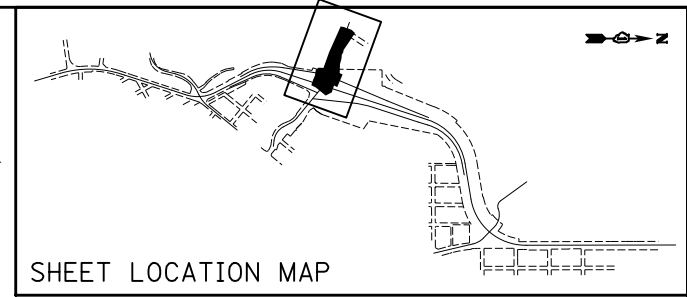


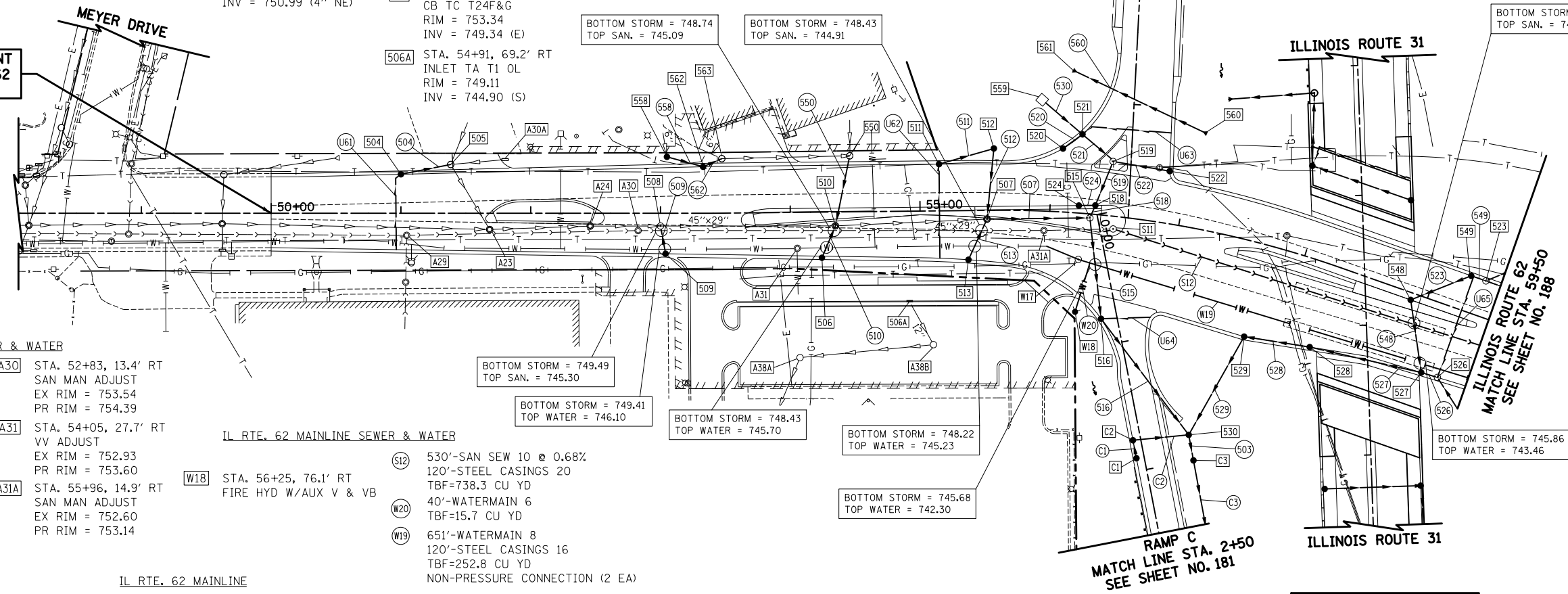
ALGONQUIN ROAD 504 STA. 51+00.00, 30.0' LT CB TC T24F&G RIM = 753.75 INV = 750.60 (E) INV = 750.60 (4" S)	ALGONQUIN ROAD 511 STA. 55+15.00, 36.0' LT CB TC T24F&G RIM = 753.88 INV = 747.60 (E) INV = 750.43 (4" S)	ALGONQUIN ROAD 518 STA. 56+34.55, 7.0' LT CB TA 5 DIA T24F&G RIM = 753.30 INV = 748.20 (NE) INV = 746.95 (SW) INV = 749.10 (W)	ALGONQUIN ROAD 527 STA. 59+14.42, 55.5' RT CB TA 5 DIA T24F&G RIM = 751.50 INV = 745.85 (N) INV = 745.85 (NW) INV = 745.85 (SE)	ALGONQUIN ROAD 549 STA. 59+26.08 27.5' LT CB TA 5 DIA TIF OL RIM = 751.64 INV = 746.05 (W) INV = 746.05 (SE)	RAMP C 516 STA. 0+79.31, 12.3' RT CB TA 5 DIA T24F&G RIM = 751.80 INV = 745.30 (N) INV = 743.42 (S) INV = 748.30 (4" E)	RAMP C C1 STA. 1+90.00, 6.0' RT CB TC T24F&G RIM = 750.69 INV = 745.10 (N)	RAMP D 560 STA. 17+19.52, 57.3' LT PRC FLAR END SEC 30 W/GRATING INV = 749.66 (NW)
506 STA. 54+24.03, 35.2' RT CB TA 4 DIA T24F&G RIM = 753.39 INV = 749.10 (W) INV = 749.10 (NE)	512 STA. 55+57.30, 47.6' LT CB TA 4 DIA T8G RIM = 753.00 INV = 747.40 (W) INV = 747.38 (SW)	558 STA. 53+04.96, 43.5' LT CB TA 4 DIA T8G RIM = 754.22 INV = 750.35 (E) INV = 751.80 EX (N)	528 STA. 58+26.56, 65.5' RT CB TA 5 DIA T24F&G RIM = 751.66 INV = 745.55 (NW) INV = 745.55 (SE)	519 STA. 56+44.65, 41.9' LT MAN TA 5 DIA TIF CL RIM = 754.55 INV = 748.50 (NW) INV = 748.50 (SE) INV = 748.50 (SW)	530 STA. 1+80.00, 37.0' LT MAN TA 9 DIA T8G RIM = 748.50 INV = 742.90 (E) INV = 742.90 (W) INV = 742.90 (NW) INV = 742.87 (S)	C2 STA. 1+75.86, 6.0' RT CB TA 4 DIA T24F&G RIM = 750.67 INV = 745.00 (S) INV = 745.00 (E)	561 STA. 16+76.00, 48.4' RT PRC FLAR END SEC 30 W/GRATING INV = 749.50 (SE)
507 STA. 55+51.51, 6.7' RT MAN TA 7 DIA TIF CL RIM = 753.40 INV = 747.10 (SE) INV = 747.08 (NE) INV = 748.10 (SW) INV = 747.10 EX (NW)	513 STA. 55+36.92, 37.8' RT CB TC T24F&G RIM = 752.80 INV = 748.28 (NE)	562 STA. 53+32.81, 35.8' LT CB TA 4 DIA T24F&G RIM = 754.40 INV = 750.15 (W) INV = 750.15 (S) INV = 751.00 EX (NE)	529 STA. 57+71.00, 72.7' RT CB TA 5 DIA T24F&G RIM = 751.71 INV = 745.31 (W) INV = 745.40 (SE)	522 STA. 56+86.63, 40.1' LT CB TC T24F&G RIM = 753.66 INV = 749.00 (NW)	520 STA. 17+36.66, 53.0' RT CB TC T24F&G RIM = 753.75 INV = 749.40 (E)	C3 STA. 2+00.00, 37.0' LT STC 24" PRECAST CONCRETE STORMCEPTOR RIM = 748.75 INV = 742.75 (N) INV = 742.70 (S)	
508 STA. 53+00.00, 9.8' RT MAN TA 5 DIA TIF CL RIM = 754.41 INV = 749.40 (S) INV = 747.43 EX (SW) INV = 747.41 EX (NE)	515 STA. 56+31.22, 3.0' RT MAN TA 7 DIA TIF CL RIM = 753.25 INV = 745.99 (S) INV = 746.80 (NE) INV = 746.73 (W)	526 STA. 59+27.41, 55.0' RT MAN TA 4 DIA T24F&G RIM = 751.49 INV = 745.87 (S) INV = 745.87 (NW) INV = 750.99 (4" NE)	548 STA. 58+88.20, 5.5' RT CB TA 5 DIA T24F&G RIM = 752.13 INV = 745.95 (E) INV = 745.95 (S)	524 STA. 56+22.00, 5.8' LT CB TC T24F&G RIM = 753.34 INV = 749.34 (E)	521 STA. 17+25.00, 38.5' RT CB TA 5 DIA T24F&G RIM = 753.88 INV = 748.70 (NE) INV = 748.70 (SE) INV = 748.70 (SW) INV = 750.52 (4" E)		
509 STA. 53+03.93, 31.2' RT CB TC T24F&G RIM = 753.97 INV = 749.50 (N)							



EXISTING STRUCTURES

505 STA. 51+38.35, 37.5' LT CB ADJUST EX RIM = 753.47 PR RIM = 753.70 INV = 750.40 EX (S) INV = 750.80 EX (E) INV = 750.40 (W) INV = 750.80 EX (NE)	510 STA. 54+34.70, 10.7' RT MAN ADJUST EX RIM = 753.10 PR RIM = 753.65 INV = 747.20 EX (SE) INV = 747.30 EX (NW) INV = 747.80 (NE) INV = 748.70 (SW)
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**BEGIN IMPROVEMENT
ILLINOIS ROUTE 62
STA. 50+00.00**



EXISTING STRUCTURES

550 STA. 54+46.24, 43.1' LT CB ADJUST EX RIM = 752.93 PR RIM = 753.78 INV = 749.60 EX (NW) INV = 749.50 EX (W) INV = 749.50 (SW)
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EXISTING STRUCTURES

559 STA. 16+91.33, 52.8' RT DETENTION BASIN OUTLET STRUCTURE TO BE MOVED TO STA. 17+00.00, 70.9' RT NEW RIM = 753.65 INV = 748.90 (N & S)
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EXISTING STRUCTURES

563 STA. 53+47.70, 42.0' LT CB ADJUST EX RIM = 753.17 PR RIM = 754.52 INV = 750.1 (E) INV = 750.1 (W) INV = 750.1 EX (NE)

EXISTING STRUCTURES

A23 STA. 51+68, 12.51' RT MAN ADJUST EX RIM = 754.00 PR RIM = 754.40

EXISTING STRUCTURES

A24 STA. 52+46, 9.9' RT MAN ADJUST EX RIM = 753.77 PR RIM = 754.57 INV = 749.50 (S) INV = 747.60 EX (NW) INV = 747.50 EX (SE)

EXISTING STRUCTURES

A30A STA. 51+80, 41.4' LT INLET ADJUST EX RIM = 753.66 PR RIM = 753.60

EXISTING STRUCTURES

A38A STA. 54+06, 111.9' RT. INLET ADJUST EX RIM = 747.89 PR RIM = 747.96

EXISTING STRUCTURES

A38B STA. 55+09, 103.0' LT. INLET ADJUST EX RIM = 747.76 PR RIM = 747.89

**NON NOTATED IMPROVEMENTS
ARE CALLED OFF ON
SHEET NO. 183**

IL RTE. 62 MAINLINE SEWER & WATER

511 STA. 56+50, 9.8' RT MAN SAN 4 DIA TIF CL RIM = 753.08 INV = 743.92 EX (W) INV = 743.92 (E)	A30 STA. 52+83, 13.4' RT SAN MAN ADJUST EX RIM = 753.54 PR RIM = 754.39
A29 STA. 51+04, 17.0' RT SAN MAN ADJUST EX RIM = 753.91 PR RIM = 754.04	A31 STA. 54+05, 27.7' RT VV ADJUST EX RIM = 752.93 PR RIM = 753.60
W17 STA. 56+25, 35.7' RT VV TA 4 DIA TIF CL RIM = 752.70 TOP OF PIPE = 742.30 WATER VALVES 8"	A31A STA. 55+96, 14.9' RT SAN MAN ADJUST EX RIM = 752.60 PR RIM = 753.14

IL RTE. 62 MAINLINE SEWER & WATER

W18 STA. 56+25, 76.1' RT FIRE HYD W/AUX V & VB	512 530'-SAN SEW 10 @ 0.68% 120'-STEEL CASINGS 20 TBF=738.3 CU YD
	W20 40'-WATERMAIN 6 TBF=15.7 CU YD
	W19 651'-WATERMAIN 8 120'-STEEL CASINGS 16 TBF=252.8 CU YD NON-PRESSURE CONNECTION (2 EA)

IL RTE. 62 MAINLINE

504 37'-STORM SEW CL A 1, 12" @ 0.54% TBF=2.0 CU YD	515 73'-SS CL A 2 EQRS 36 @ 0.95% (S45 R29) TBF=61.8 CU YD
507 73'-SS CL A 2 EQRS 36 @ 0.48% (S45 R29) TBF=69.9 CU YD	516 106'-SS CL A 2 EQRS 36 @ 0.49% (S45 R29) TBF=83.7 CU YD
509 20'-STORM SEW CL A 1, 12" @ 0.50% TBF=4.5 CU YD	518 3'-STORM SEW CL A 2, 24" @ 5.00% TBF=2.4 CU YD
510 25'-STORM SEW WM REQ, 12" @ 1.60% TBF=5.4 CU YD	519 35'-STORM SEW CL A 2, 24" @ 0.86% TBF=10.8 CU YD
511 41'-STORM SEW CL A 2, 12" @ 0.49% TBF=3.9 CU YD	520 16'-STORM SEW CL A 1, 12" @ 0.63% TBF=2.4 CU YD
512 50'-STORM SEW CL A 2, 18" @ 0.60% TBF=18.5 CU YD	521 29'-STORM SEW CL A 2, 24" @ 0.69% TBF=7.7 CU YD
513 31'-STORM SEW WM REQ, 12" @ 0.58% TBF=7.3 CU YD	522 42'-STORM SEW CL A 2, 12" @ 1.19% TBF=10.4 CU YD
	523 51'-STORM SEW CL A 2, 30" @ 0.20% TBF=16.7 CU YD

IL RTE. 62 MAINLINE

524 10'-STORM SEW CL A 1, 12" @ 2.40% TBF=1.5 CU YD	550 51'-STORM SEW CL A 2, 18" @ 3.33% TBF=13.1 CU YD
526 9'-STORM SEW CL A 2, 18" @ 0.22% TBF=3.0 CU YD	558 23'-STORM SEW CL A 2, 12" @ 0.87% TBF=0.0 CU YD
527 84'-STORM SEW CL A 2, 36" @ 0.36% TBF=23.7 CU YD	560 103'-STORM SEW CL A 1, 30" @ 0.16% TBF=7.4 CU YD
528 47'-STORM SEW CL A 2, 36" @ 0.32% TBF=25.0 CU YD	562 14'-STORM SEW CL A 1, 12" @ 6.43% TBF=2.3 CU YD
529 78'-STORM SEW CL A 2, 36" @ 3.09% TBF=0.0 CU YD	U61 60'-PIPE UNDERDRAIN, 4"
530 35'-STORM SEW CL A 2, 12" @ 0.57% TBF=0.0 CU YD	U62 73'-PIPE UNDERDRAIN, 4"
548 52'-STORM SEW CL A 2, 30" @ 0.19% TBF=17.1 CU YD	U63 71'-PIPE UNDERDRAIN, 4"
549 7'-STORM SEW WM REQ, 24" @ 0.71% TBF=2.2 CU YD	U64 38'-PIPE UNDERDRAIN, 4"
	U65 83'-PIPE UNDERDRAIN, 4"

IL RTE. 62 MAINLINE

C1 12'-STORM SEW CL A 2, 12" @ 0.83% TBF=4.0 CU YD
C2 39'-STORM SEW CL A 2, 12" @ 5.38% TBF=10.3 CU YD
503 14'-STORM SEW CL A 2, 36" @ 0.86% TBF=0.0 CU YD
C3 214'-STORM SEW CL A 3, 36" @ 0.90% TBF=0.0 CU YD

NOTES:
1. CURB STRUCTURE RIM ELEVATIONS ARE LOCATED AT THE EDGE OF PAVEMENT
2. LOCATION CALL-OFFS AND INVERT ELEVATIONS FOR END SECTIONS ARE AT THE FLARED END OF THE STRUCTURE
3. CONTRACTOR SHALL COORDINATE ALL WATER MAIN WORK WITH THE VILLAGE. ANY WATER MAIN ON ALGONQUIN ROAD SHALL BE CONNECTIONS COMPLETED DURING ONE WATER MAIN SHUTDOWN.

