

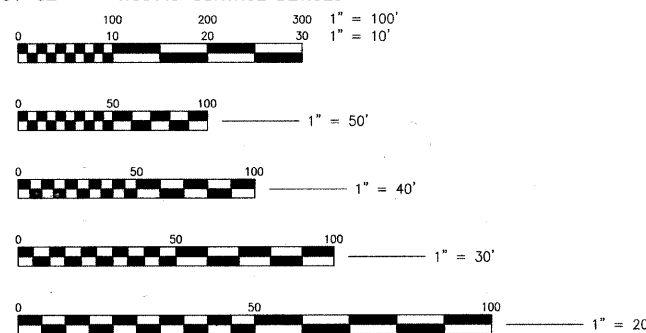
SHEET NO. TITLE

- 1 COVER SHEET, INDEX & LOCATION MAP
- 2 GENERAL NOTES
- 3 SUMMARY OF QUANTITIES
- 4 EXISTING AND PROPOSED TYPICAL SECTIONS
- 5,6, & 7 183RD STREET (FAU ROUTE 1622) EXISTING ROADWAY AND PAVEMENT MARKING PLAN, STA. 113+18 STA. 138+00
- 8 TC-13 IDOT DISTRICT ONE TYPICAL PAVEMENT MARKING DETAIL
- 9 BD 32 IDOT BUTT JOINT & HMA TAPER DETAIL
- 10 TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS,
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- 12 BD-8 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID PROJECT
FAU 1622 (183RD STREET)
SACRAMENTO AVENUE TO FAU 2860 (RIEGEL ROAD)
RESURFACING
SECTION : 10-00132-00-RS
PROJECT NO.: M-9003(985)
VILLAGE OF HOMEWOOD
COOK COUNTY
JOB NO.: C-91-337-12

HIGHWAY STANDARDS

STANDARD NO.	TITLE
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-06	PERPENDICULAR CURB RAMPS
424016-00	MID BLOCK CURB RAMP
442201-03	CLASS C AND D PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
606001-04	CONCRETE CURB TYPE-B AND COMBINATION CONCRETE CURB & GUTTER
701601-07	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSABLE MEDIAN
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED SIZE PLANS, THE ABOVE SCALES SHALL BE USED.

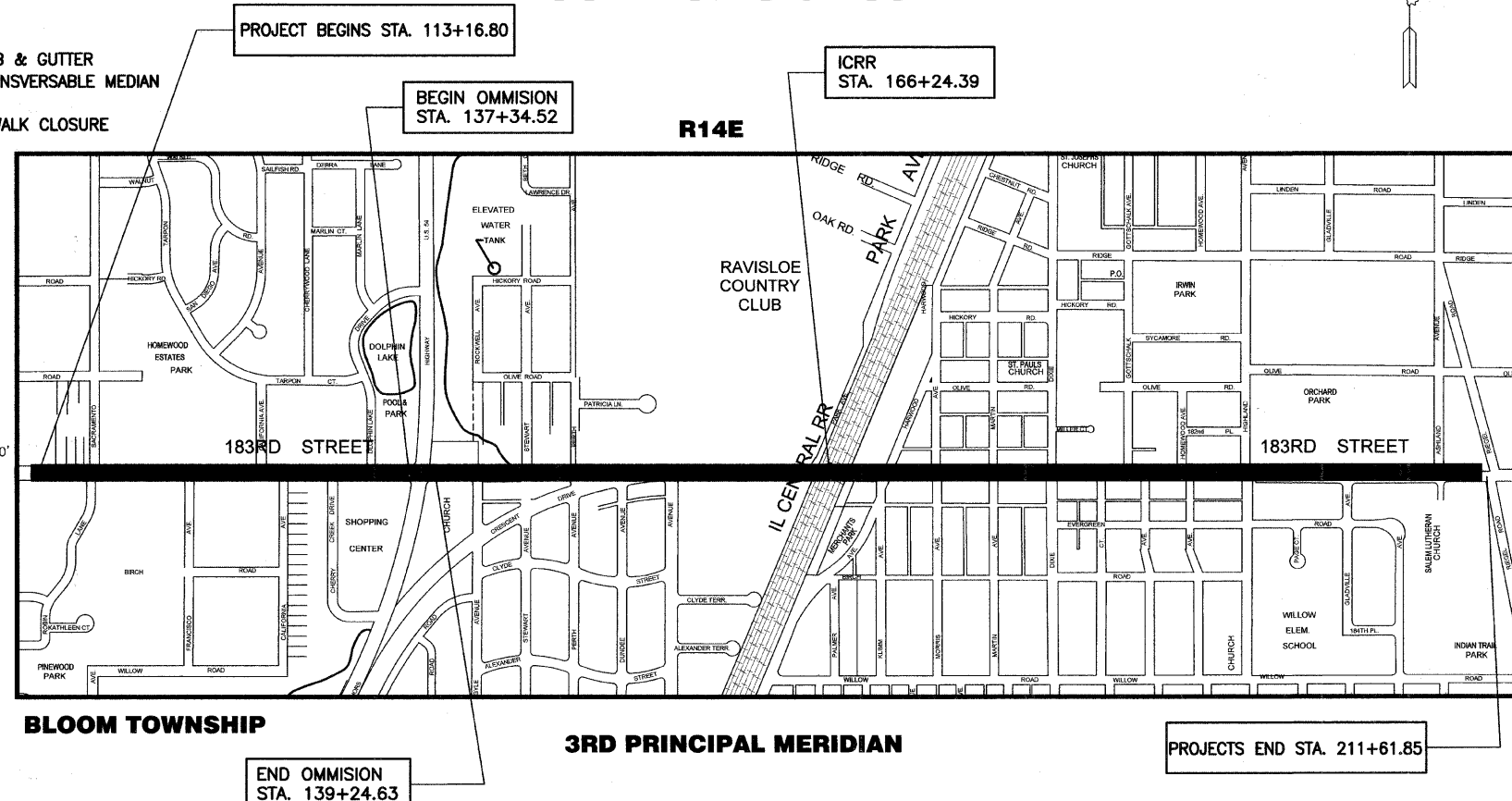
183rd STREET
 POSTED SPEED: 35 MPH
 ADT: 16,500
 MINOR ARTERIAL



OR 811

CALL JULIE (800) 892-0123
 WITH THE FOLLOWING INFORMATION:
 COUNTY = COOK
 48 HOURS BEFORE YOU DIG
 EXCLUDING SAT., SUN. & HOLIDAYS

CONTRACT NO.: 63705



LOCATION MAP
NOT TO SCALE

GROSS LENGTH OF PROJECT = 9,845 LINEAL FEET (1.865 MILES)
 NET LENGTH OF PROJECT = 9,655 LINEAL FEET (1.829 MILES)

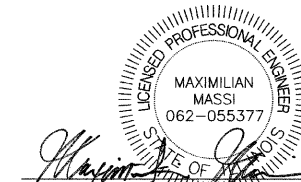
PROJECT IS LOCATED IN THE VILLAGE OF HOMEWOOD



VILLAGE OF HOMEWOOD
 Department of Public Works
 Engineering Division
 17755 S. Ashland Avenue
 Homewood, IL 60445
 (708) 206-3470



INDICATES LOCATION OF IMPROVEMENT



VILLAGE ENGINEER
 " LICENSE EXPIRES 11-30-13 " DATE: 3-30-12


STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	3/30 2012 <i>[Signature]</i> VILLAGE PRESIDENT
PASSED	APRIL 2 2012 <i>[Signature]</i> DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	APRIL 2, 2012 <i>[Signature]</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406, SCHAUMBURG, ILLINOIS

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED JANUARY 1, 2012 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE LATEST EDITION OF THE "ILLINOIS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND THE MANUAL OF TEST PROCEDURES FOR MATERIALS IN EFFECT ON THE DATE OF INVITATION FOR BID.; THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" INDICATED ON THE CHECK SHEET INCLUDED HEREIN AND IN CASE OF CONFLICT WITH ANY PART OR PARTS OF SAID SPECIFICATIONS, THE SAID SPECIAL PROVISIONS SHALL TAKE PRECEDENCE AND GOVERN.
2. ALL REFERENCES TO THE "VILLAGE" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE VILLAGE OF HOMEWOOD.
3. THE FINISHED HMA SURFACE SHALL BE CONSTRUCTED 1/4 - INCH ABOVE THE GUTTER FLAG.
4. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE AND THE ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING THE CONSTRUCTION OPERATION SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 105.07 OF THE "STANDARD SPECIFICATIONS," THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE "STANDARD SPECIFICATIONS."
5. ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE PLANS AND THE IDOT STANDARDS FOR TRAFFIC CONTROL AND PROTECTION.
6. WHERE THE PROPOSED PAVEMENT ABUTS EXISTING PAVEMENT TO REMAIN IN PLACE (BEGIN, END AND LIMITS OF CONSTRUCTION), EXISTING DRIVEWAY PAVEMENT, SIDEWALK TO REMAIN IN PLACE. THE EXISTING PAVEMENT SHALL BE SAW CUT TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING SURFACES. THIS SAW CUT SHALL BE INCLUDED IN THE COST OF THE PAY ITEM "HMA SURFACE REMOVAL - BUTT JOINT".
7. CURB & GUTTER, DRIVEWAY AND SIDEWALK REMOVAL AND REPLACEMENT SHALL BE LIMITED TO WORKING ON ONE SIDE OF THE STREET AT ALL TIMES TO MINIMIZE CONGESTION. NO CURB & GUTTER SHALL BE REMOVED ON FRIDAYS. CONCRETE SHALL HAVE A MINIMUM OF 72 HOURS CURING TIME AND DRIVEWAYS SHALL BE ACCESSIBLE PRIOR TO REMOVING EXISTING CURB ON THE OTHER SIDE OF THE STREET.
8. THE CONTRACTOR SHALL ADJUST ALL VALVE BOXES WITHIN THE PAVEMENT AREA BY DIGGING THE TOP PIECE SUCH THAT IT MAY BE TURNED TO THE FINISHED PAVEMENT GRADE. CAST IRON INSERTS MAY BE USED ONLY IF THEY ARE ADJUSTABLE BY SCREWING INTO THE EXISTING PIECE OR RESTING FIRMLY ON THE EXISTING BOX. MORTAR OR TAR SHALL NOT BE USED TO HOLD AN INSERT IN PLACE.
9. ALL FRAME AND GRATES TO BE REPLACED SHALL REMAIN THE PROPERTY OF THE VILLAGE OF HOMEWOOD AND SHALL BE DELIVERED TO THE PUBLIC WORKS BUILDING BY THE CONTRACTOR. DELIVERY OF OLD FRAMES AND GRATES TO THE VILLAGE OF HOMEWOOD DEPARTMENT OF PUBLIC WORKS SHALL BE INCLUDED IN THE COST OF THE RESPECTIVE STRUCTURE ADJUSTMENTS.
10. THE CONTRACTOR(S) AND THEIR SUBCONTRACTOR(S) SHALL CONTACT J.U.L.I.E. (800) 892-0123 AND THE VILLAGE OF HOMEWOOD/DEPARTMENT OF PUBLIC WORKS (708) 206-3470 BEFORE DIGGING.
11. THE CONTRACTOR SHALL NOTIFY ENGINEER 24 HOURS PRIOR TO BEGINNING WORK SO THAT CARS CAN BE REMOVED FROM DRIVEWAYS. WORK MUST BE PLANNED TO MINIMIZE THE TIME DRIVEWAYS ARE OUT OF SERVICE. WORK SHALL BE SCHEDULED SO THAT DRIVES PULLED ON THE FIRST WORKING DAY ARE FORMED AND POURED BY THE THIRD WORKING DAY. BEYOND THIS 3 DAY TIME IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AT HIS OWN EXPENSE TEMPORARY AGGREGATE FOR ACCESS TO THE DRIVEWAY.
12. DEBRIS REMOVAL - MATERIALS RESULTING FROM THE VARIOUS CONSTRUCTION OPERATIONS SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL REMOVE DEBRIS AND THE CONTRACTOR SHALL BE BILLED ACCORDINGLY.
13. WATER SUPPLY - THE CONTRACTOR CAN OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A WATERING BAN IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR. WATER IS AVAILABLE AT 17755 S. ASHLAND AVENUE, MUNICIPAL SERVICE CENTER FIRE HYDRANT. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER.
14. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/HR) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/HR). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
15. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL PROVIDE TO THE ENGINEER A VIDEO TAPE COVERING FROM RIGHT-OF-WAY TO RIGHT-OF-WAY OF ALL ROADWAYS TO BE IMPROVED. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
16. WHEN REMOVING PAVEMENT, CURB AND GUTTER, SIDEWALK, DRIVEWAY PAVEMENT, PARKING LOT PAVEMENT, AND/OR OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKER WHICH MIGHT DISTURB UNDERGROUND PUBLIC UTILITIES WILL NOT BE PERMITTED.
17. PRIOR TO THE START OF CONSTRUCTION ACTIVITIES, THE ENGINEER AND CONTRACTOR, JOINTLY, SHALL PERFORM A VISUAL INSPECTION OF VILLAGE DRAINAGE AND UTILITY STRUCTURES TO DETERMINE THE AMOUNT OF EXISTING DEBRIS IN EACH STRUCTURE. THE CONTRACTOR WILL BE REQUIRED TO CLEAN THOSE STRUCTURES WITH DEBRIS THAT WERE CLEAN AT THE BEGINNING OF CONSTRUCTION AT NO ADDITIONAL COST TO THE CONTRACT.
18. MILLING OF PAVEMENT SHALL BE DONE SO AS TO NOT DAMAGE THE ADJACENT CURB OR STRUCTURES. REMOVAL ADJACENT TO THESE STRUCTURES SHALL BE ACCOMPLISHED TO THE SATISFACTION OF THE ENGINEER AND MAY REQUIRE HANDWORK.
19. THE CONTRACTOR SHALL PROVIDE TWO (2) SUITABLE TEMPORARY TOILET FACILITIES ALONG THE STREET, UNLESS OTHERWISE APPROVED BY THE ENGINEER, FOR USE OF ALL CONTRACTOR'S PERSONNEL EMPLOYED ON THE WORK, AND SHALL MAINTAIN SAME IN PROPER SANITARY CONDITION. AT COMPLETION, THE FACILITIES SHALL BE REMOVED AND THE PREMISES LEFT CLEAN. THE LOCATION OF THE TEMPORARY TOILETS SHALL BE APPROVED BY THE ENGINEER. COST FOR TOILET FACILITIES WILL NOT BE PAID FOR SEPARATELY BUT SHOULD BE INCLUDED IN THE COST OF MOBILIZATION.
20. THE CONTRACTOR SHALL REMOVE FROM THE PROJECT SITE ALL UNSUITABLE AND SURPLUS EXCAVATED MATERIAL NOT USED OR BACKFILLED. THE WASTE EXCAVATED MATERIAL SHALL NOT BE DEPOSITED IN PUBLIC OR PRIVATE PROPERTY UNLESS THE CONTRACTOR FIRST OBTAINS THE WRITTEN PERMISSION FROM THE PROPERTY OWNER OR ENGINEER.
21. THE CONTRACTOR SHALL KEEP ONE (1) COPY OF ALL SPECIFICATIONS, DRAWINGS, ADDENDA, MODIFICATIONS, AND SHOP DRAWINGS AT THE SITE ON GOOD ORDER AND ANNOTATED TO SHOW ALL CHANGES MADE DURING THE CONSTRUCTION PROCESS. THE FINAL RECORD DRAWINGS SHALL BECOME THE PROPERTY OF THE VILLAGE.
22. THE CONTRACTOR SHALL MAINTAIN TEMPORARY HANDICAP ACCESS TO HOMES DEEMED NECESSARY BY THE ENGINEER BY PROVIDING AND INSTALLING MATERIALS REQUIRED FOR REASONABLE INGRESS AND EGRESS AT ALL TIMES. THE COST OF THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF SIDEWALK REMOVAL.
23. ADD THE FOLLOWING STATEMENT TO THE END OF ARTICLE 105.06: "THE CONTRACTOR SHALL NOT CHANGE HIS SUPERINTENDENT WITHOUT WRITTEN PERMISSION OF THE ENGINEER."
24. NO METAL RINGS SHALL BE USED FOR FRAME ADJUSTMENTS. THE RINGS AND FRAMES SHALL BE SET ON TWO CONCENTRIC RINGS OF CON-SEAL OR PRESS-SEAL "TAR ROPE". MOSTIC SHALL BE APPLIED WITH A TROWEL ON THE OUTSIDE OF THE RINGS, WHILE THE INSIDE SHALL BE PAINTED WITH A FINE CEMENT GROUT. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE STRUCTURE ADJUSTMENTS.
25. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURBS, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR "CRC" PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.
26. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FRO REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, AND GUTTER AND MEDIAN, AND CHAIR SUPPORT FOR "CRC" PAVEMENT, SHALL BE EPOXY COATER, UNLESS NOTED ON THE PLAN.

	VILLAGE OF HOMEWOOD	USER NAME = -	DESIGNED DJA	REVISED 3-22-12	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	ENGINEERING DIVISION	CHECKED MM	REVISIED -	1622			10-00132-00-RS	COOK	13	2	
	PLOT SCALE = -	DATE 01-26-2012	REVISIED -	CONTRACT NO. 63705			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
	PLOT DATE = 03-22-2012	REVISIED -	SCALE: NONE	SHEET NO.			OF SHEETS	STA.	TO STA.		

SUMMARY OF QUANTITIES		CONSTRUCTION TYPE CODE 0005	
CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	665
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	17
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	17
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	17
25200110	SODDING, SALT TOLERANT	SQ YD	665
25200200	SUPPLEMENTAL WATERING	UNIT	12
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	894
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	11,190
40600300	AGGREGATE (PRIME COAT)	TON	225
40600400	MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS	TON	50
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50	TON	2,326
40600895	CONSTRUCTING TEST STRIP	EACH	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	781
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4,652
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	8,050
42400800	DETECTABLE WARNINGS	SQ FT	616
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	55,380
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	865
44000600	SIDEWALK REMOVAL	SQ FT	8,050
44201676	CLASS D PATCHES, TYPE IV, 2 INCH	SQ YD	983
60260300	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, AND OPEN LID	EACH	2

SUMMARY OF QUANTITIES		CONSTRUCTION TYPE CODE 0005	
CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY
60266600	VALVE BOXES TO BE ADJUSTED	EACH	26
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	35
67100100	MOBILIZATION	L SUM	1
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	10,000
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	505
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	25,926
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	4,367
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	464
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	594
* 78001100	PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	505
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	25,926
* 78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	4,367
* 78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	464
* 78001180	PAINT PAVEMENT MARKING - LINE 24"	FOOT	594
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1087
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	114
X6060052	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL)	FOOT	865
XX006947	HOT-MIX ASPHALT DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT	SQ YD	16
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1

* SPECIALTY ITEM



VILLAGE OF HOMEWOOD
ENGINEERING
DIVISION

USER NAME	DESIGNED	REVISION
	DJA	3-28-12
DRAWN	DJA	REVISION
CHECKED	MM	REVISION
DATE	01-26-2012	REVISION

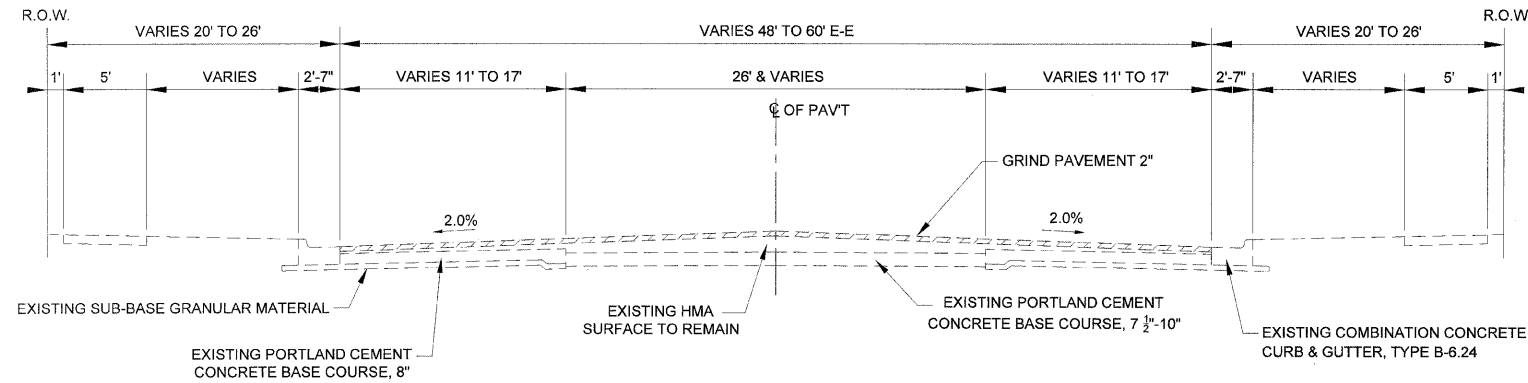
DESIGNED	REVISION
DJA	3-28-12
DRAWN	REVISION
DJA	
CHECKED	REVISION
MM	
DATE	REVISION
01-26-2012	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

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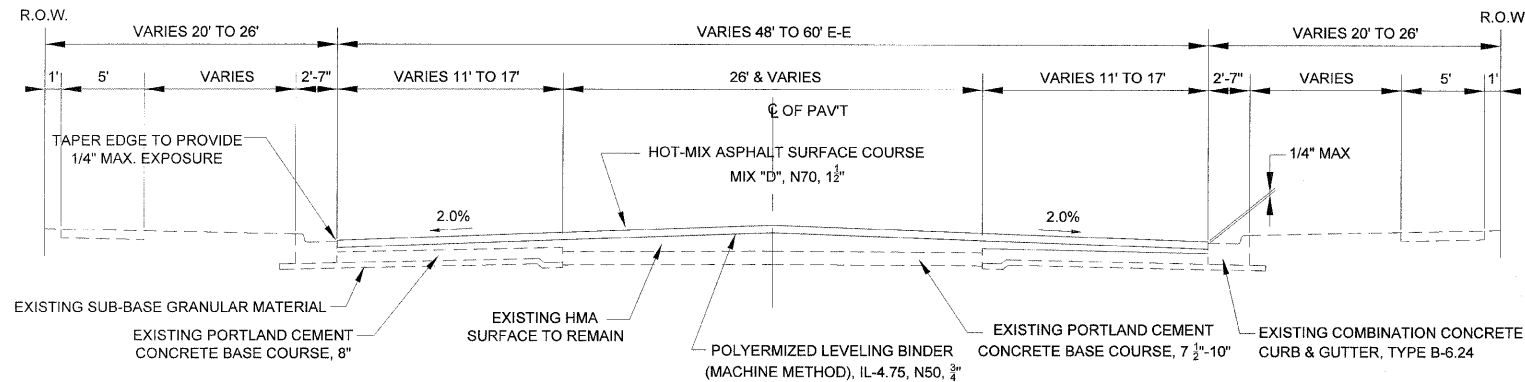
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1622	10-0132-00-RS	COOK	13	3
CONTRACT NO. 63705				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



*** EXISTING TYPICAL SECTION**

**CURB AND GUTTER
FULL WIDTH GRIND
(183rd STREET, STA. 113+16.80 to STA. 211+61.85)**

*** OMISSION LIMITS
FROM STA. 137+34.52
TO STA. 139+24.63**



*** PROPOSED TYPICAL SECTION**

**CURB AND GUTTER
FULL WIDTH GRIND
(183rd STREET, STA. 113+16.80 to STA. 211+61.85)**

*** OMISSION LIMITS
FROM STA. 137+34.52
TO STA. 139+24.63**

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	VOIDS @ Ndes
PAVEMENT RESURFACING	
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	3.5% @ 50 GYR
HOT-MIX ASPHALT SURFACE COURSE MIX "D", N70 (IL-9.5mm), 1 1/2"	4% @ 70 GYR
PATCHING	
CLASS D PATCHES, TYPE IV, 2" HMA BINDER COURSE (IL-19mm)	4% @ 70 GYR
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5mm), 3" (2 LIFTS)	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

EXISTING HMA SURFACE TO REMAIN



VILLAGE OF HOMEWOOD
ENGINEERING
DIVISION

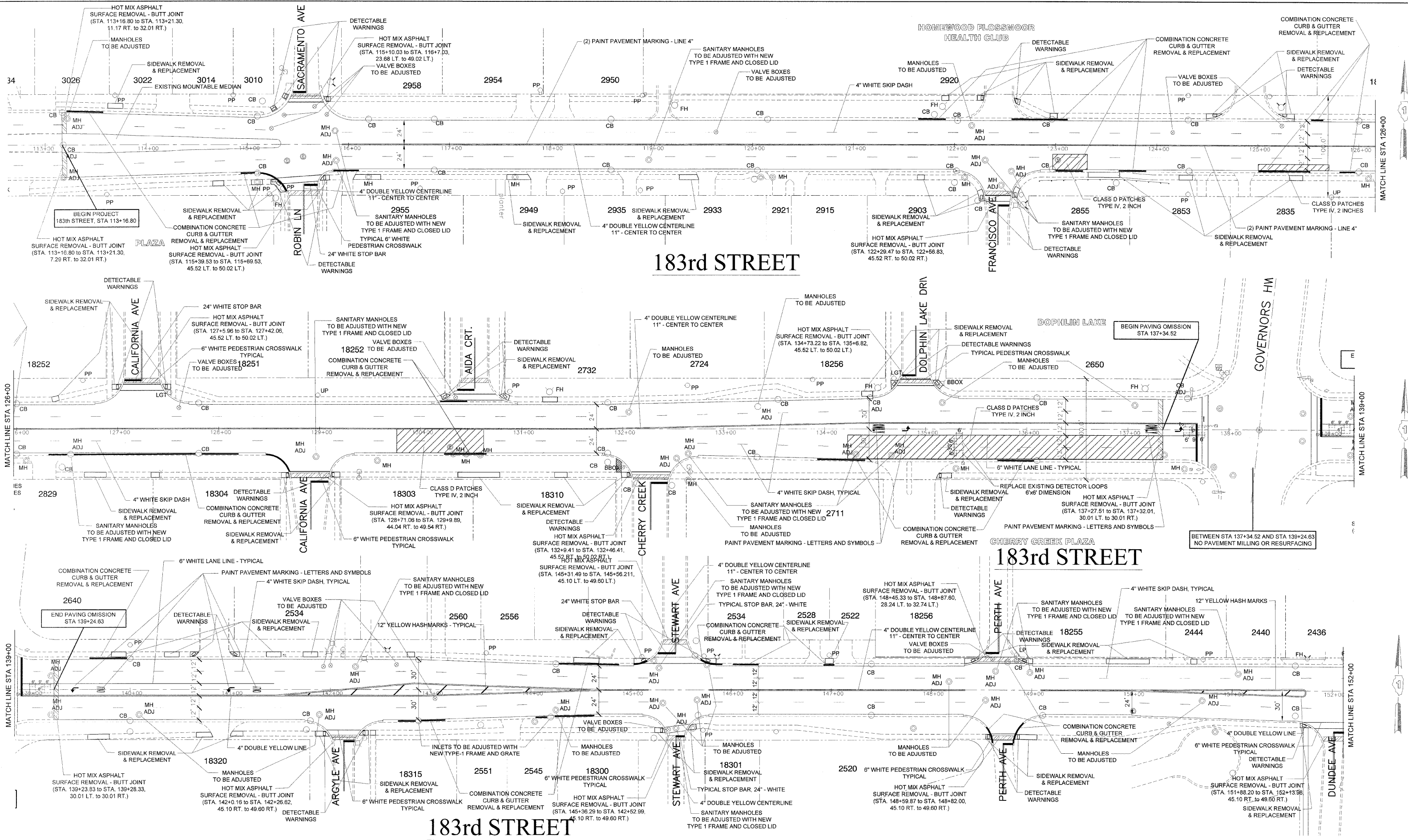
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PLOT SCALE	DRAWN	DJA	REVISED	-
PLOT DATE	CHECKED	MM	REVISED	-
	DATE	01-26-2012	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1622	10-00132-00-RS	COOK	13	4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63705	



VILLAGE OF HOMewood
ENGINEERING
DIVISION

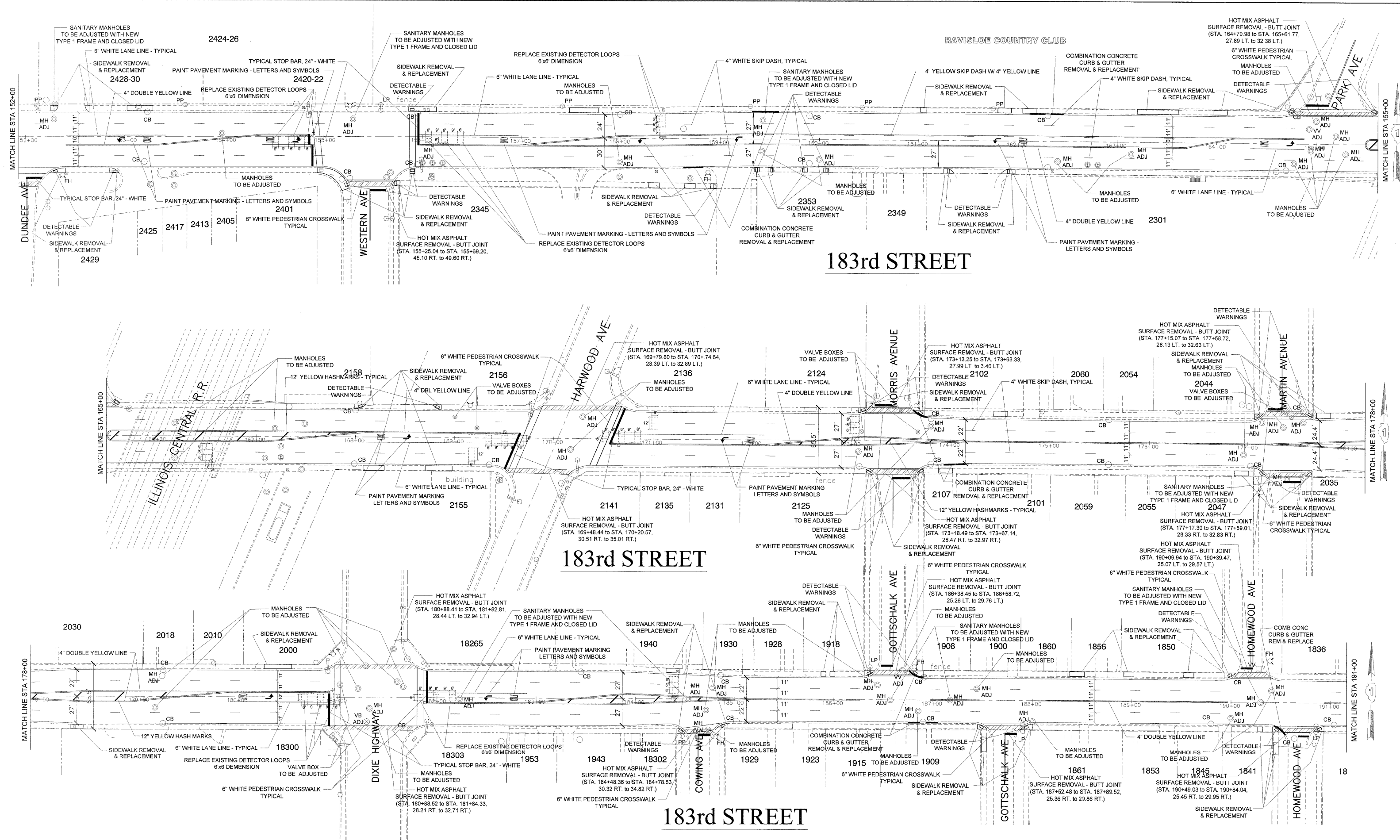
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PLOT SCALE	DRAWN	REVISED
---	DJA	---
PLOT DATE	CHECKED	REVISED
03-22-2012	MM	---
	DATE	REVISED
	01-26-2012	---

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED
ROADWAY AND PAVEMENT MARKING PLAN

SCALE: 1"=40.0' SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1622	10-00132-00-RS	COOK	13	5
CONTRACT NO. 63705				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



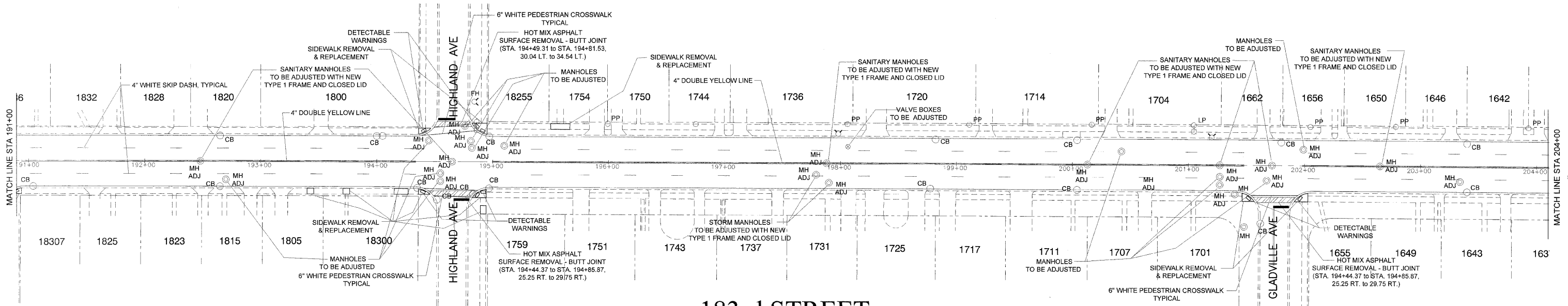
VILLAGE OF HOMEWOOD
ENGINEERING
DIVISION

USER NAME	DESIGNED	REVISED
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PLOT SCALE	DRAWN	REVISED
---	DJA	---
PLOT DATE	CHECKED	REVISED
03-22-2012	MM	---
	DATE	REVISED
	01-26-2012	---

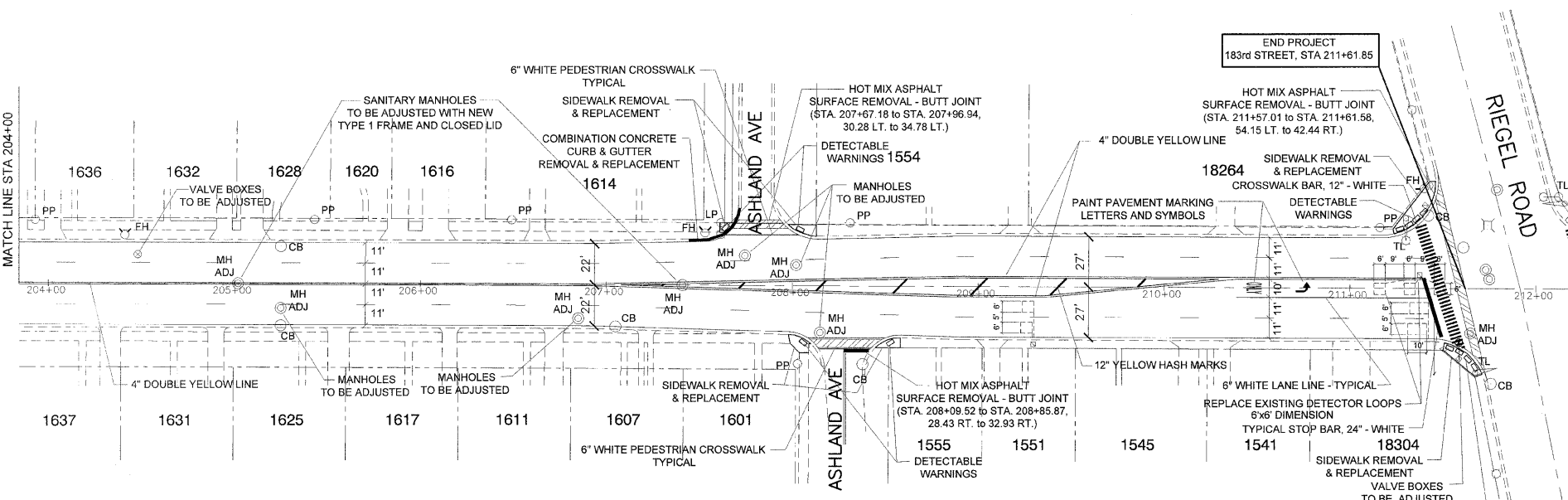
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED
ROADWAY AND PAVEMENT MARKING PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1622	10-00132-00-RS	COOK	13	6
CONTRACT NO. 63705				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



183rd STREET



183rd STREET

VILLAGE OF HOMEWOOD
ENGINEERING
DIVISION

USER NAME = --	DESIGNED DJA	REVISED 3-22-12
PLOT SCALE = --	DRAWN DJA	REVISED -
PLOT DATE = 03-22-2012	CHECKED MM	REVISED -
	DATE 01-26-2012	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

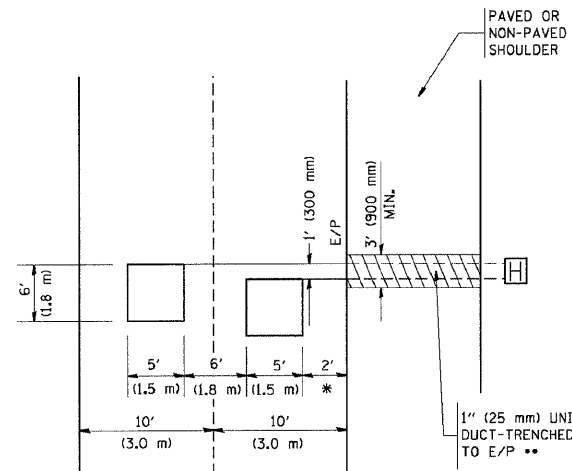
**EXISTING AND PROPOSED
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1"=40.0' SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1622	10-00132-00-RS	COOK	13	7
CONTRACT NO. 63705				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.



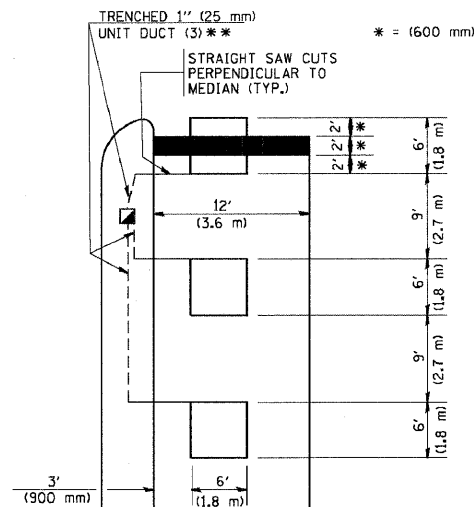
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.

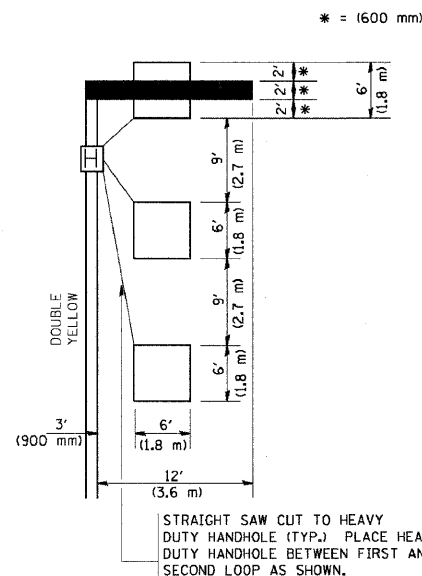


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

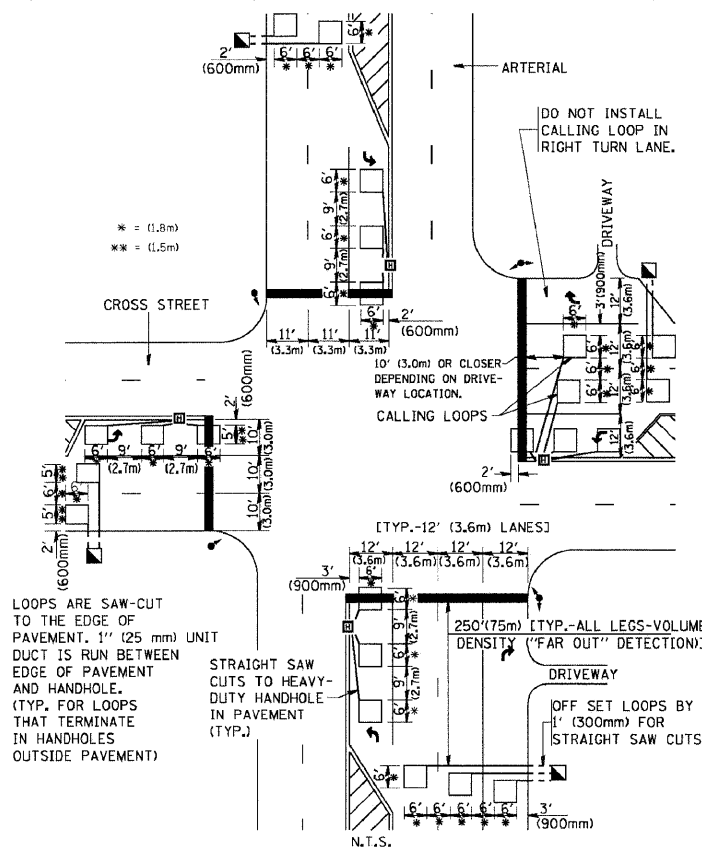
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



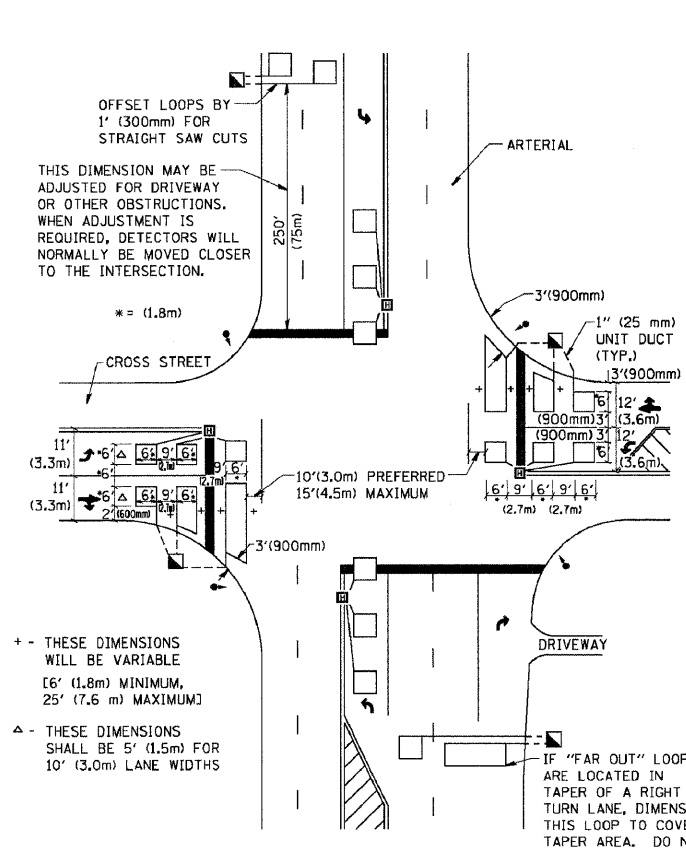
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

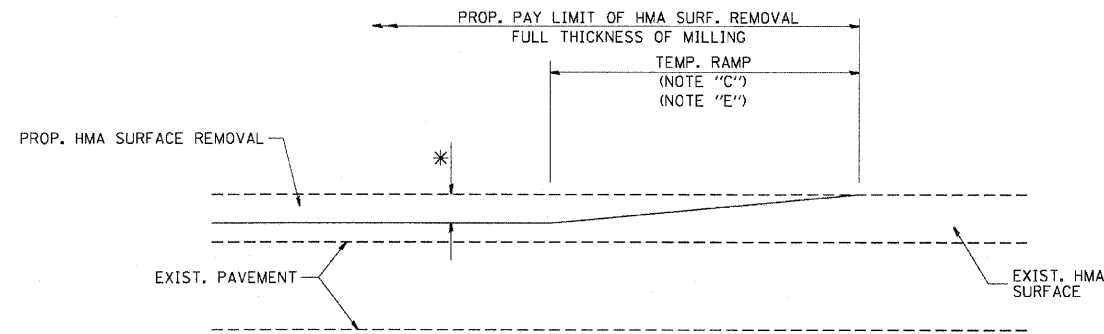
NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

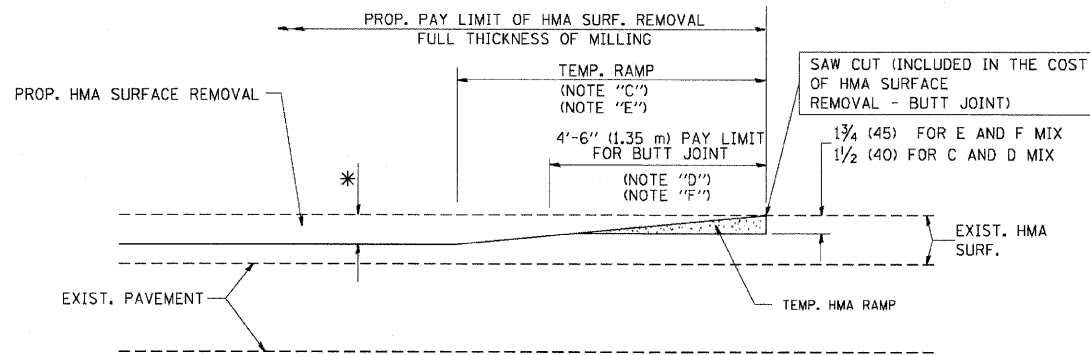
FILE NAME = W:\dststd\22x34\ts07.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A. RTE. 1622	SECTION 10-00132-00-RS	COUNTY COOK	TOTAL SHEETS 13	SHEET NO. 8
	PLOT SCALE = 50:2000 / IN.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TS-07		CONTRACT NO. 63705	
	PLOT DATE = 1/4/2008	CHECKED - R.K.F.	REVISED -		FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT							
		DATE -	REVISED -									

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1622	10-00132-00-RS	COOK	13	9
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

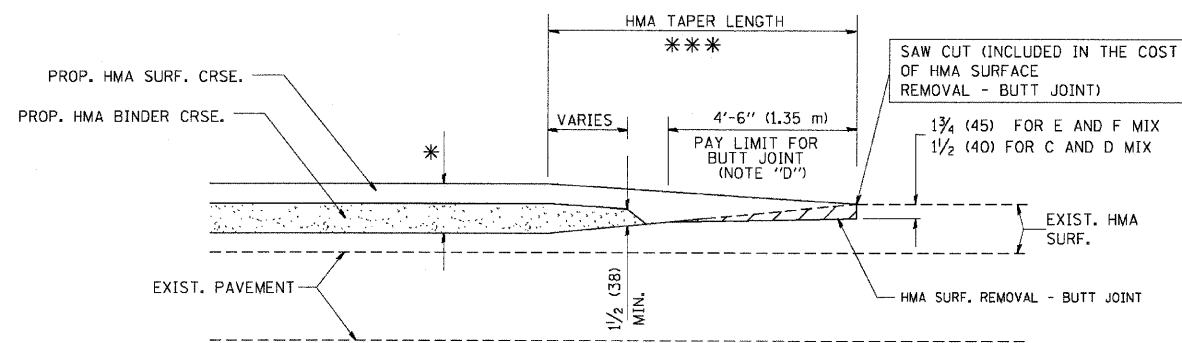
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

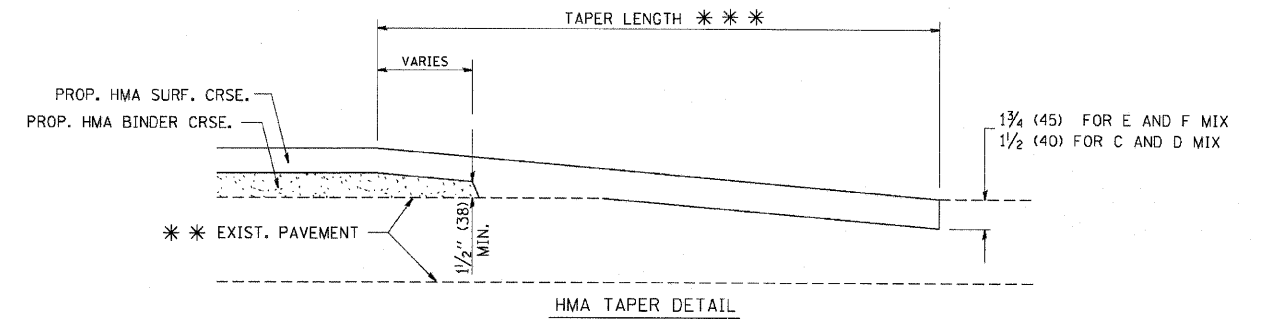
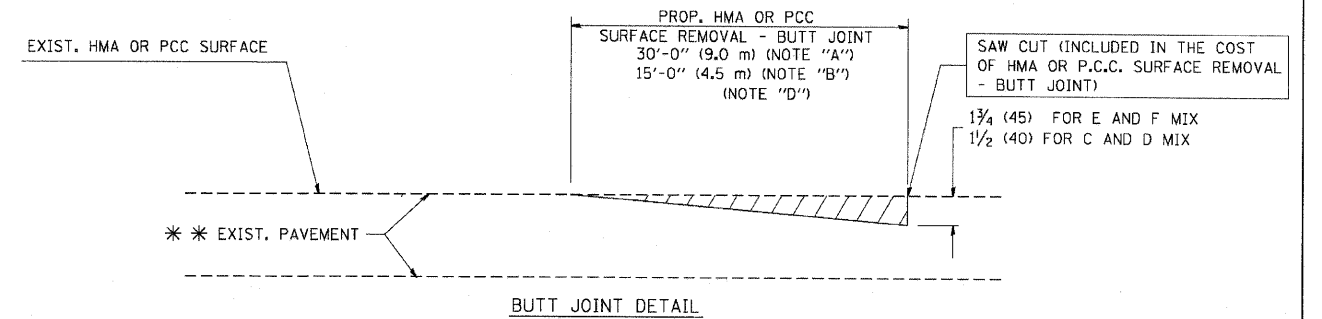
OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT AND
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

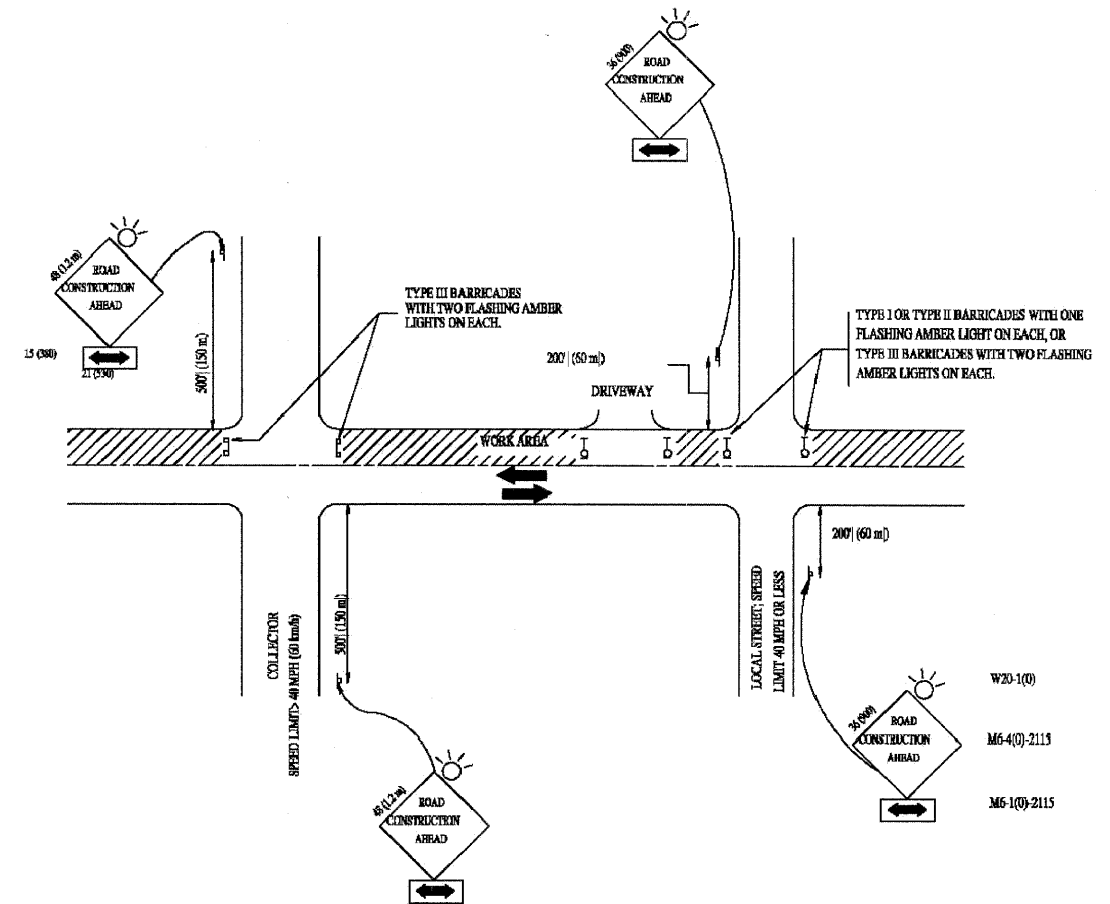
BUTT JOINT AND
HMA TAPER
DETAILS

SCALE: VERT. NONE
HORIZ.

DRAWN BY

CHECKED BY

BD400-05 (VI-BD32)



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER.
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (914x914) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER.
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1220 x 1220) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M5-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M5-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 7019), STD. 7019A OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME =	DESIGNED =	REVISED =
W:\sme\2010\1014\1014.dwg	pp@icad	LMA	J. CHENIER 10-18-95
		DRAWN =	REVISED =
		A. HOUSE 09-06-96	
		CHECKED =	REVISED =
		A. EDWARDS 10-15-96	
		DATE =	REVISED =
		06-99	T. BAUMGARTNER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

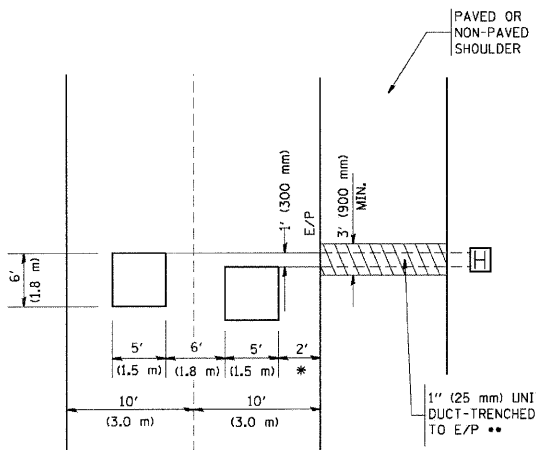
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

EA.U	SECTION	COUNTY		
1622	10-00132-00-RS	COOK	13	10
TC-10		CONTRACT NO. 63705		
TRAFFIC CONTROL	ILLINOIS	TRAFFIC CONTROL		

F.A.I.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1622	10-00132-00-RS	COOK	13	11
STA.	TO STA.			
			FED. AID PROJECT	

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

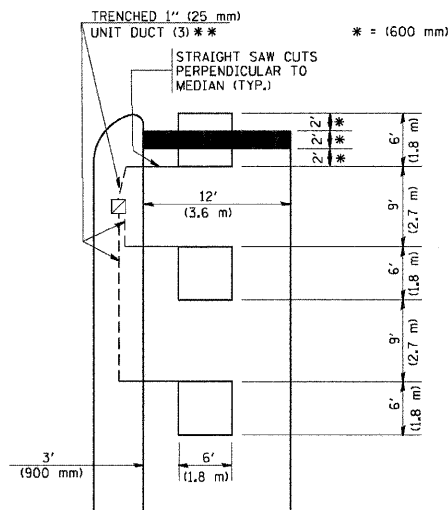


* = (600 mm)

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LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

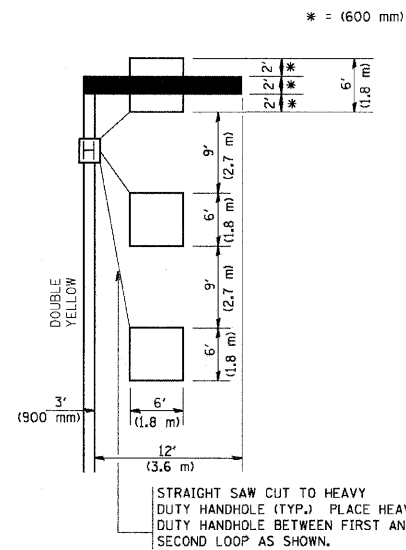
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



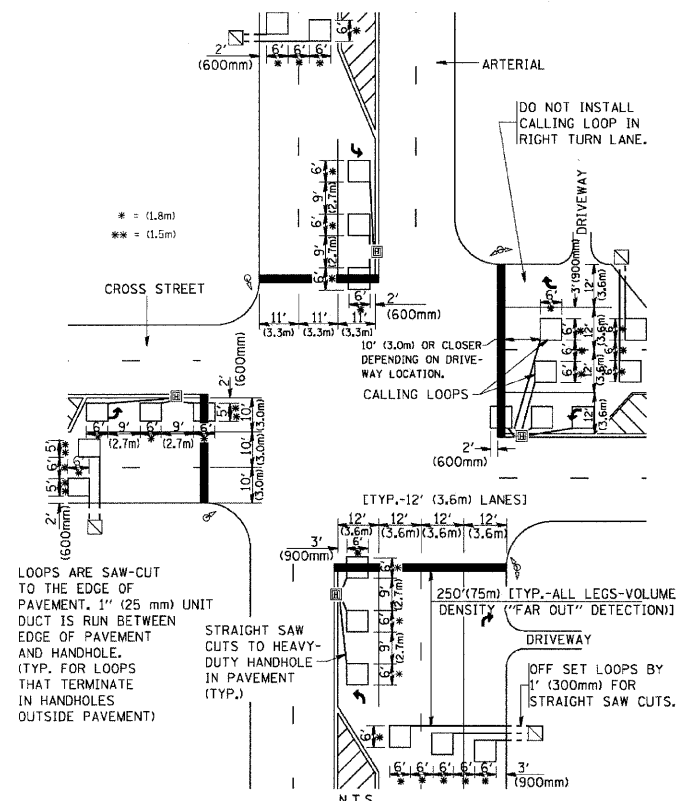
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

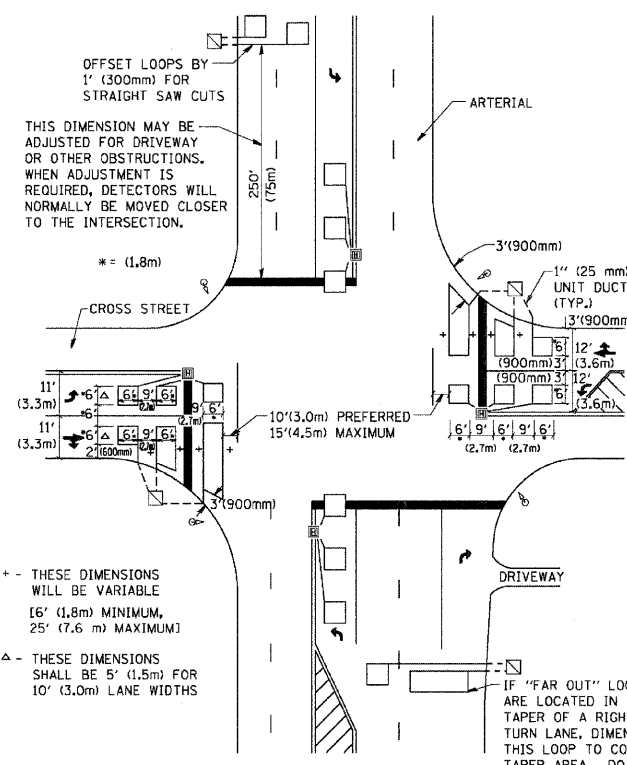
- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
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ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

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THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

REVISIONS	
NAME	DATE

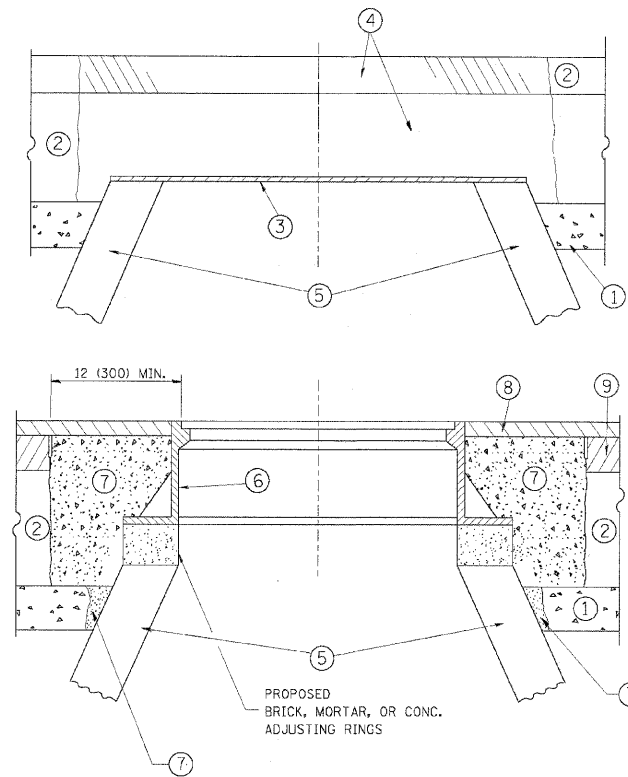
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
DETECTOR LOOP
INSTALLATION DETAILS
FOR ROADWAY RESURFACING

SCALE: NONE
DATE: 2/15/2006

DESIGNED BY
DRAWN BY CADD
CHECKED BY R.K.F.
TS07
REVISION DATE:

PLOT DATE = 2/15/2006
FILE NAME = c:\vstatast\1007.dgn
PLOT SCALE = 48,9999 / IN.
USER NAME = gregtanout

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1622	10-00132-00-RS	COOK	13	12
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07
R. BORO	03/09/11
R. BORO	12/06/11

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DETAILS FOR
 FRAMES AND LIDS ADJUSTMENT
 WITH MILLING

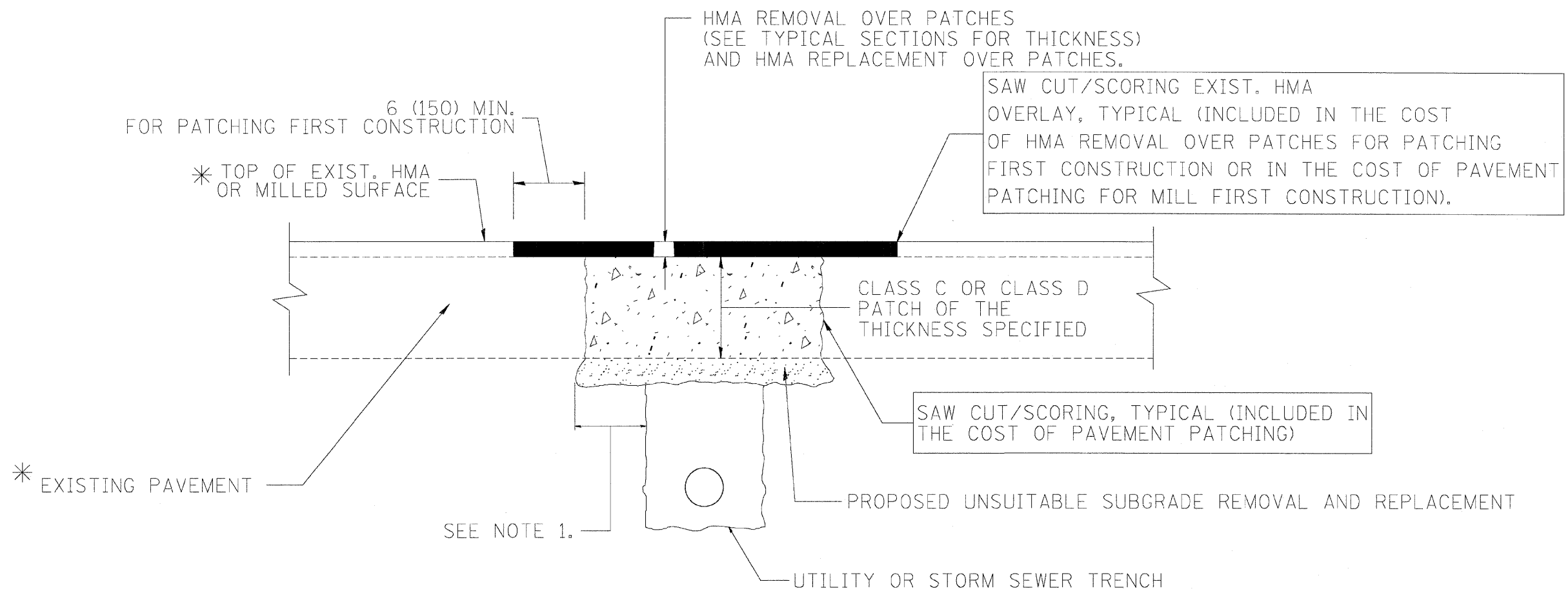
SCALE: VERT. NONE
 HORIZ.

DRAWN BY

CHECKED BY

BD600-03 (BD-8)

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1624	10-00132-00-RS	COOK	13	13
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98
R. BORO	01/01/07
R. BORO	09/04/07
K. ENG	10/27/08

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: VERT. NONE
HORIZ.

DRAWN BY
CHECKED BY
BD400-04 (BD-22)

PLOT DATE = 10/27/2008
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = bboard1