

CONSTRUCTION PLANS FOR ABRAHAM LINCOLN CAPITAL AIRPORT

SPRINGFIELD AIRPORT AUTHORITY SPRINGFIELD, ILLINOIS

IL. PROJ. NO. SPI-4163
AIP PROJ. NO. 3-17-0096-XX

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2

MAY 4, 2012

811 Know what's below. Call before you dig. **J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS** www.illinoiscall.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN INFORMATION

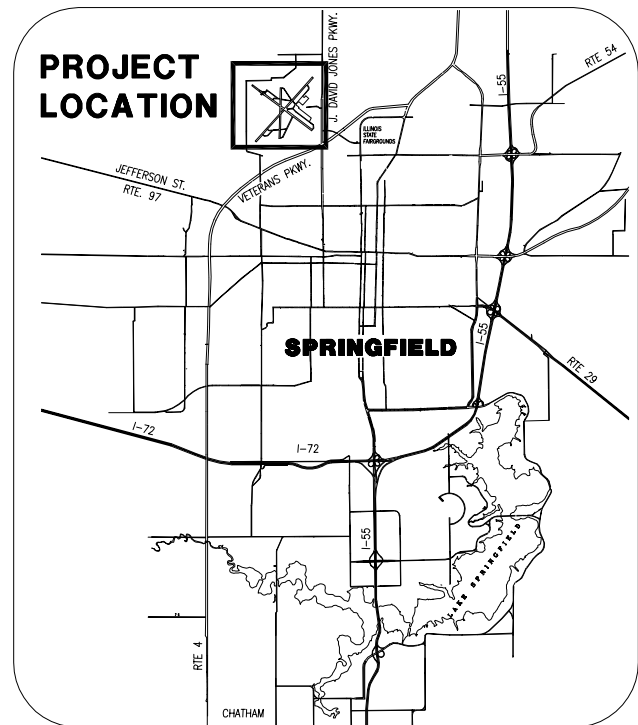
GEOMETRIC CRITERIA
MAIN RAMP:
AIRPLANE DESIGN GROUP II
AIRCRAFT APPROACH CATEGORY B
CRITICAL AIRCRAFT - BEECH 1900

PAVEMENT DESIGN CRITERIA
MAIN RAMP:
17,000# DUAL WHEEL, 5475 ANNUAL DEPARTURES
70,200# DUAL WHEEL, 6 ANNUAL DEPARTURES

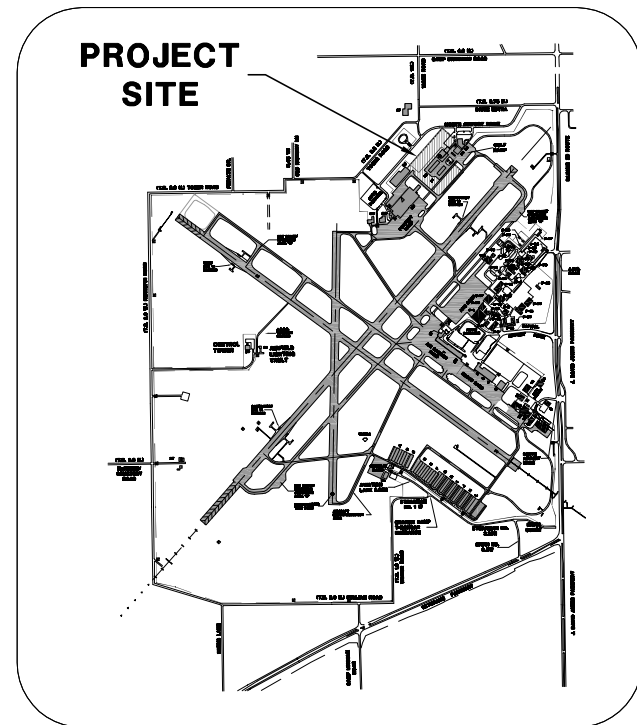
CALL J.U.L.I.E. BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 16 NORTH
RANGE: 5 WEST OF THE 4TH P.M.
SECTION: 9
COUNTY: SANGAMON
CIVIL TOWNSHIP: CAPITAL

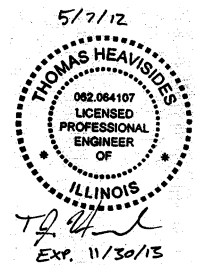
AIR CONTROL FREQUENCY 121.30
GROUND FREQUENCY 121.90
MAXIMUM HEIGHT OF EQUIPMENT = 25'



LOCATION MAP




SITE PLAN




**SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS**

APPROVED *Mark E. Hamner*
EXECUTIVE DIRECTOR OF AVIATION

DATE 5/7/2012

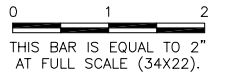
 **CMT**
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY *T. J. ...*
DATE 5/7/12

CMT JOB NUMBER: 09035-05

CA013

REVISIONS		
NUMBER	BY	DATE



SUMMARY OF QUANTITIES			
ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1
AR152480	SHOULDER ADJUSTMENT	S.Y.	655
AR156510	SILT FENCE	L.F.	800
AR162510	CLASS E FENCE 10'	L.F.	70
AR162724	ELECTRIC GATE - 24'	EACH	1
AR162900	REMOVE CLASS E FENCE	L.F.	70
AR162908	REMOVE ELECTRIC GATE	EACH	1
AR163510	TEMPORARY FENCE	L.F.	120
AR201670	CRACK CONTROL FABRIC	S.Y.	1250
AR401610	BITUMINOUS SURFACE COURSE	TON	1650
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	14075
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	830
AR403610	BITUMINOUS BASE COURSE	TON	2470
AR501505	5" PCC PAVEMENT	S.Y.	160
AR603510	BITUMINOUS TACK COAT	GAL	3500
AR605540	CLEAN & SEAL JOINTS	L.F.	3400
AR620510	PAVEMENT MARKING	S.F.	4000
AR620900	PAVEMENT MARKING REMOVAL	S.F.	260
AR754610	PAVED DITCH	L.F.	110
AR754910	REMOVE PAVED DITCH	L.F.	110
AR904510	SODDING	S.Y.	655

INDEX TO SHEETS	
Sheet Number	Sheet Title
01	COVER SHEET
02	INDEX TO SHEETS & SUMMARY OF QUANTITIES
03	SITE PLAN
04	CONSTRUCTION ACTIVITY PLAN 1
05	CONSTRUCTION ACTIVITY PLAN 2
06	CONSTRUCTION ACTIVITY PLAN 3
07	CONSTRUCTION ACTIVITY PLAN DETAILS
08	EXISTING CONDITIONS & REMOVALS
09	SPECIALCONDITIONS AND CONSIDERATIONS -1
10	SPECIALCONDITIONS AND CONSIDERATIONS - 2
11	EXISTING PAVEMENT STRUCTURE
12	PAVEMENT REMOVAL PLAN
13	PROPOSED IMPROVEMENTS
14	TYPICAL SECTIONS
15	STAKING PLAN 1
16	STAKING PLAN 2
17	MARKING PLAN - HANGAR TAXIWAYS
18	MARKING & MISCELLANEOUS DETAILS
19	FENCE PLAN
20	FENCE DETAILS 1
21	FENCE DETAILS 2
22	ELECTRICAL GATE DETAILS 1
23	ELECTRICAL GATE DETAILS 2
24	ELECTRICAL GATE DETAILS 3
25	GATE SIGNS DETAILS
26	TURFING PLAN
27	EROSION CONTROL DETAILS



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 INDEX TO SHEETS & SUMMARY OF QUANTITIES

© Copyright CMT, Inc.



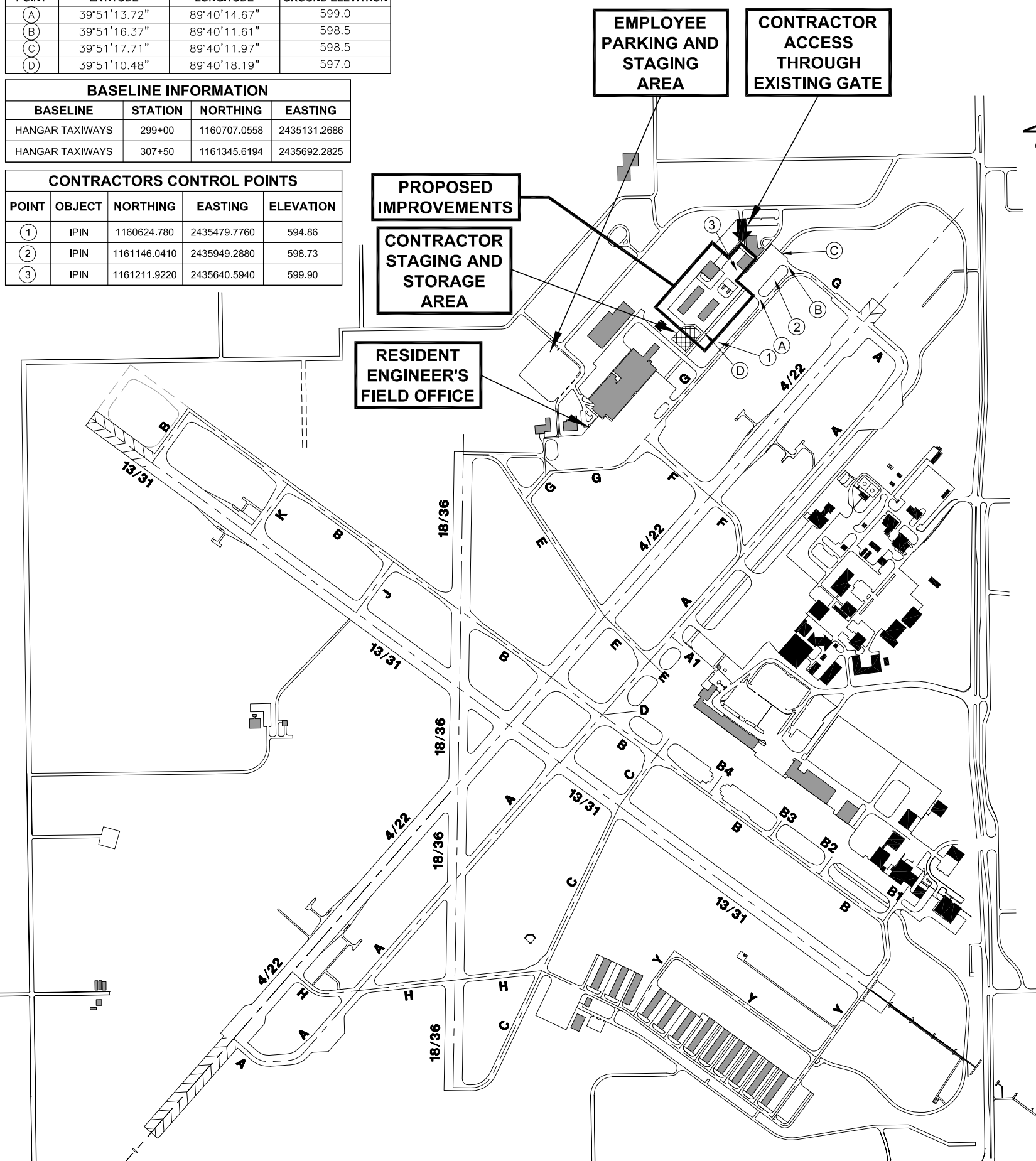
DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL. PROJ. NO. SPI-4163 AIP PROJ. NO. 3-17-0096-XX	
SHEET 02 OF 27 SHEETS	

GROUND CONTROL FREQUENCY - 121.90
 AIR CONTROL FREQUENCY - 121.30
 MAXIMUM HEIGHT OF EQUIPMENT - 25'

CRITICAL POINTS			
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION
(A)	39°51'13.72"	89°40'14.67"	599.0
(B)	39°51'16.37"	89°40'11.61"	598.5
(C)	39°51'17.71"	89°40'11.97"	598.5
(D)	39°51'10.48"	89°40'18.19"	597.0

BASELINE INFORMATION			
BASELINE	STATION	NORTHING	EASTING
HANGAR TAXIWAYS	299+00	1160707.0558	2435131.2686
HANGAR TAXIWAYS	307+50	1161345.6194	2435692.2825

CONTRACTORS CONTROL POINTS				
POINT	OBJECT	NORTHING	EASTING	ELEVATION
①	IPIN	1160624.780	2435479.7760	594.86
②	IPIN	1161146.0410	2435949.2880	598.73
③	IPIN	1161211.9220	2435640.5940	599.90



GENERAL NOTES


- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED SAA/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT DIRECTOR.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- BROKEN ASPHALT AND/OR CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 95' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND TO ORIGINAL CONDITION BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE, SIGNS AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE SAA SECURITY CHIEF.
 - THE CONTRACTOR SHALL USE AN EXISTING GATE FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER, S.A.A. MAINTENANCE SUPERVISOR, AND S.A.A. SECURITY CHIEF. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER. ONLY AUTHORIZED PERSONNEL ALLOWED ACCESS.
 - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES.

K:\Springfield\0903505\Draw\Sheets\PHASE 2
 FILE: SITE PLAN.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/9/2012 1:36 PM
 SPI-BASE-1

CA013


REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).


SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2

SITE PLAN

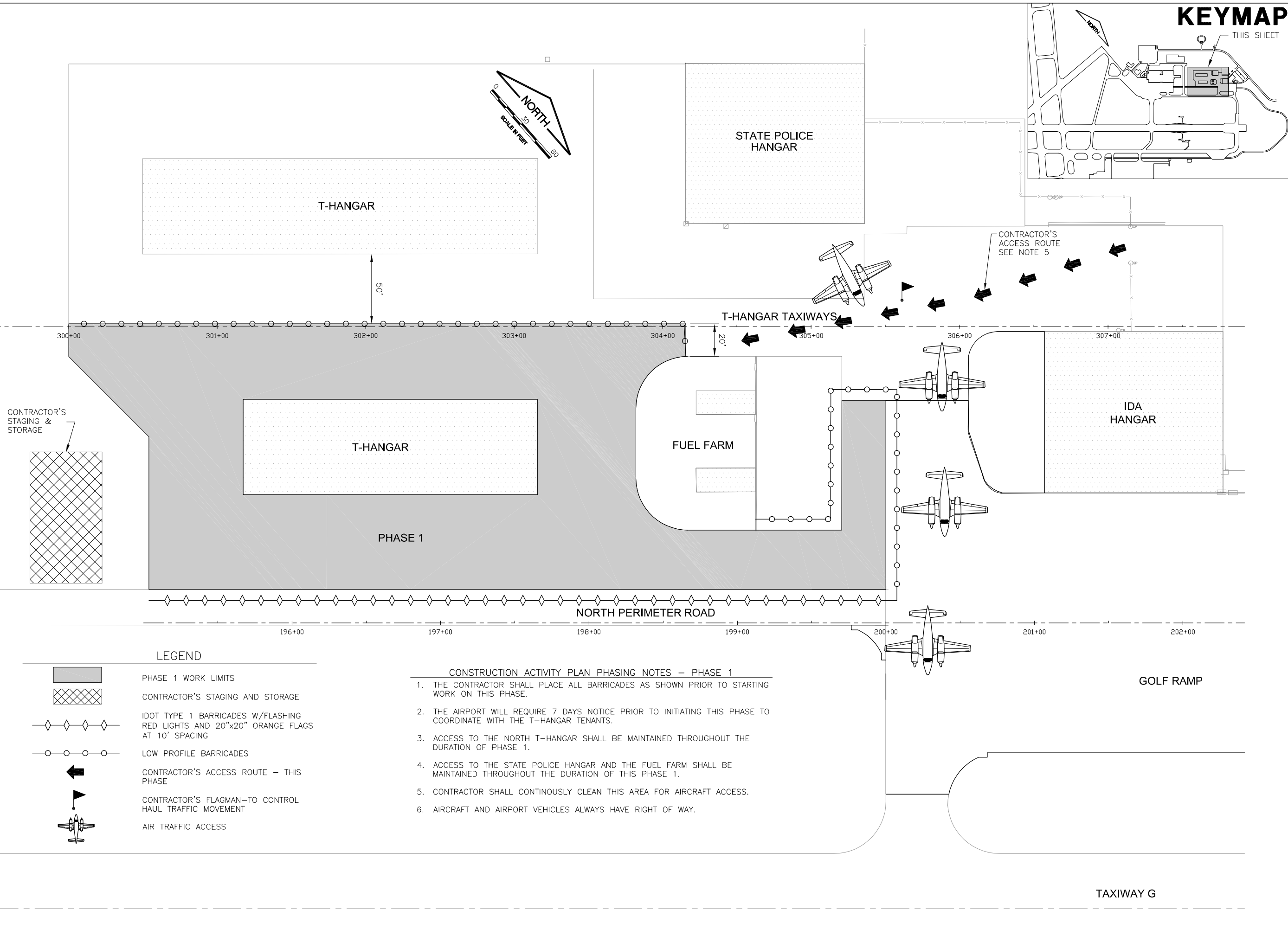
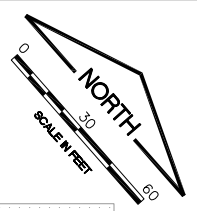
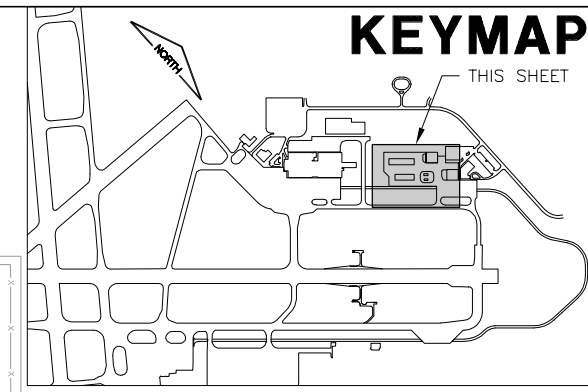
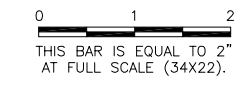

GMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY: RLV/TJH
 DRAWN BY: CMT
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: MAY 4, 2012
 JOB No: 09035-05
 IL. PROJ. NO. SPI-4163
 AIP PROJ. NO. 3-17-0096-XX
 SHEET 03 OF 27 SHEETS

KEYMAP

K:\Springfield\0903505\Draw\Sheets\PHASE 2
 FILE: CONACT4.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/9/2012 1:36 PM
 SPI-BASE-1
 BASE_EXISTGEO
 BASE_PROPGEO
 BASE_EXIST_JOINT
 BASE_EXISTTOPO
 SPIBASE_EXISTGEO
 SPI-KEYMAP
 SPI_Align
CA013

REVISIONS		
NUMBER	BY	DATE



LEGEND

- PHASE 1 WORK LIMITS
- CONTRACTOR'S STAGING AND STORAGE
- IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS AND 20"x20" ORANGE FLAGS AT 10' SPACING
- LOW PROFILE BARRICADES
- CONTRACTOR'S ACCESS ROUTE - THIS PHASE
- CONTRACTOR'S FLAGMAN-TO CONTROL HAUL TRAFFIC MOVEMENT
- AIR TRAFFIC ACCESS

CONSTRUCTION ACTIVITY PLAN PHASING NOTES - PHASE 1

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON THIS PHASE.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING THIS PHASE TO COORDINATE WITH THE T-HANGAR TENANTS.
3. ACCESS TO THE NORTH T-HANGAR SHALL BE MAINTAINED THROUGHOUT THE DURATION OF PHASE 1.
4. ACCESS TO THE STATE POLICE HANGAR AND THE FUEL FARM SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THIS PHASE 1.
5. CONTRACTOR SHALL CONTINUOUSLY CLEAN THIS AREA FOR AIRCRAFT ACCESS.
6. AIRCRAFT AND AIRPORT VEHICLES ALWAYS HAVE RIGHT OF WAY.

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

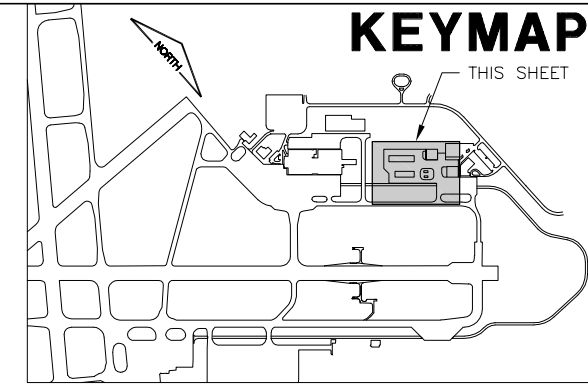
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
CONSTRUCTION ACTIVITY PLAN 1

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

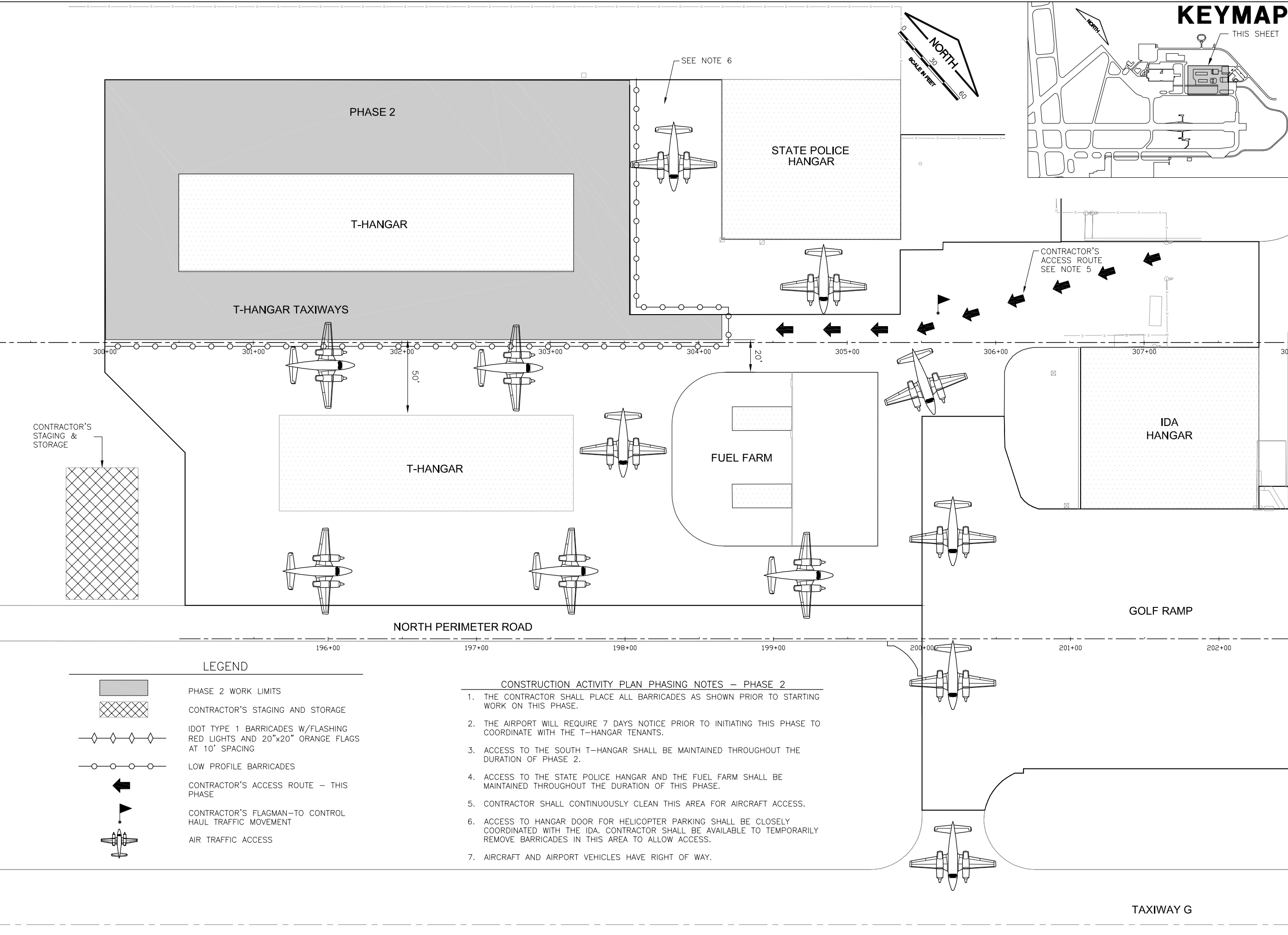
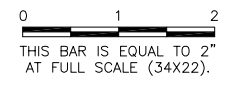
DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL. PROJ. NO. SPI-4163 AIP PROJ. NO. 3-17-0096-XX	
SHEET 04 OF 27 SHEETS	

KEYMAP



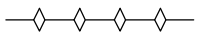
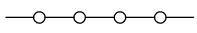





K:\Springfield\0903505\Draw\Sheets\PHASE 2
 FILE: CONACT2_PHS2.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/9/2012 1:36 PM
 SPI-BASE-1
 BASE_EXISTGEO
 BASE_PROPCEO
 BASE_EXIST_JOINT
 BASE_EXISTTOPO
 SPIBASE_EXISTGEO
 SPI-KEYMAP
 SPI_Align
CA013

REVISIONS		
NUMBER	BY	DATE

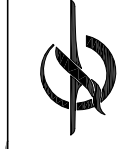


LEGEND

-  PHASE 2 WORK LIMITS
-  CONTRACTOR'S STAGING AND STORAGE
-  IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS AND 20"x20" ORANGE FLAGS AT 10' SPACING
-  LOW PROFILE BARRICADES
-  CONTRACTOR'S ACCESS ROUTE - THIS PHASE
-  CONTRACTOR'S FLAGMAN-TO CONTROL HAUL TRAFFIC MOVEMENT
-  AIR TRAFFIC ACCESS

CONSTRUCTION ACTIVITY PLAN PHASING NOTES - PHASE 2

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON THIS PHASE.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING THIS PHASE TO COORDINATE WITH THE T-HANGAR TENANTS.
3. ACCESS TO THE SOUTH T-HANGAR SHALL BE MAINTAINED THROUGHOUT THE DURATION OF PHASE 2.
4. ACCESS TO THE STATE POLICE HANGAR AND THE FUEL FARM SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THIS PHASE.
5. CONTRACTOR SHALL CONTINUOUSLY CLEAN THIS AREA FOR AIRCRAFT ACCESS.
6. ACCESS TO HANGAR DOOR FOR HELICOPTER PARKING SHALL BE CLOSELY COORDINATED WITH THE IDA. CONTRACTOR SHALL BE AVAILABLE TO TEMPORARILY REMOVE BARRICADES IN THIS AREA TO ALLOW ACCESS.
7. AIRCRAFT AND AIRPORT VEHICLES HAVE RIGHT OF WAY.



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
CONSTRUCTION ACTIVITY PLAN 2

© Copyright CMT, Inc.

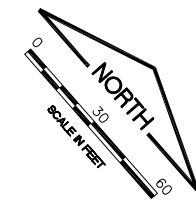
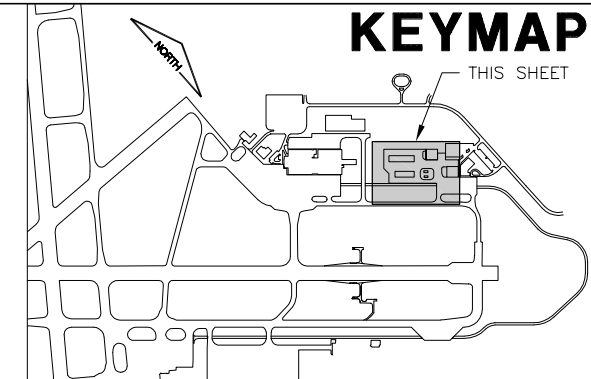
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



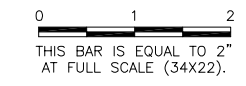
DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL. PROJ. NO. SPI-4163 AIP PROJ. NO. 3-17-0096-XX	
SHEET 05 OF 27 SHEETS	

KEYMAP

K:\Springfield\0903505\Draw\Sheets\PHASE 2
 FILE: CONACT2_PHS3.dwg
 UPDATE BY: TJ Heavisesides
 PLOT DATE: 5/9/2012 1:37 PM
 SPI-BASE-1
 BASE_EXISTGEO
 BASE_PROPGEO
 BASE_EXIST_JOINT
 BASE_EXISTTOPO
 SPIBASE_EXISTGEO
 SPI-KEYMAP
 EGPTBASE_SURVEY
CA013



REVISIONS		
NUMBER	BY	DATE



SEE NOTE 4

STATE POLICE HANGAR

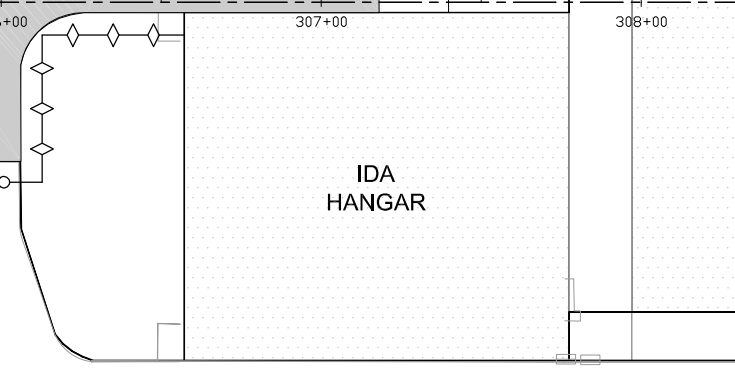
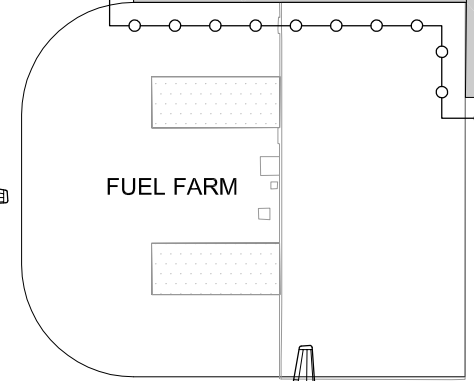
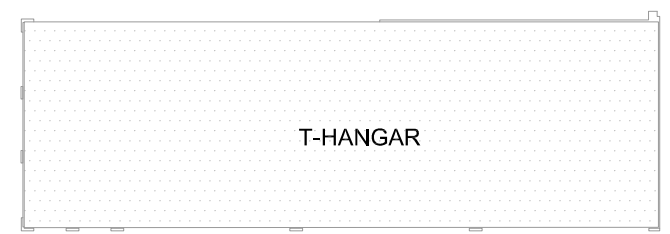
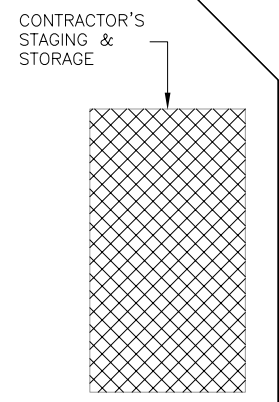
T-HANGAR

T-HANGAR TAXIWAYS

CONTRACTOR'S ACCESS

SEE NOTES 5 AND 6

300+00 301+00 302+00 303+00 304+00 305+00 306+00 307+00 308+00





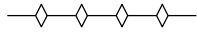
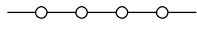
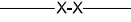



NORTH PERIMETER ROAD

GOLF RAMP

196+00 197+00 198+00 199+00 200+00 201+00 202+00

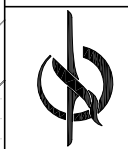
LEGEND

-  PHASE 3 WORK LIMITS
-  JOINT CLEANING & SEALING
-  CONTRACTOR'S STAGING AND STORAGE
-  CONTRACTOR'S ACCESS - THIS PHASE
-  IDOT TYPE 1 BARRICADES W/FLASHING RED LIGHTS AND 20"x20" ORANGE FLAGS AT 10' SPACING
-  LOW PROFILE BARRICADES
-  TEMPORARY FENCE
-  AIR TRAFFIC ACCESS

CONSTRUCTION ACTIVITY PLAN PHASING NOTES - PHASE 3

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON THIS PHASE.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING THIS PHASE TO COORDINATE WITH THE T-HANGAR TENANTS.
3. ACCESS TO THE STATE POLICE HANGAR AND THE FUEL FARM SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE PROJECT.
4. THE WORK IN PHASE 3 SHALL BE CLOSELY COORDINATED WITH THE AIRPORT AND THE TENANTS OF THIS HANGAR. IT IS INTENDED TO MAINTAIN CONTINUOUS ACCESS TO THIS HANGAR THROUGHOUT ALL PHASES. ANY NECESSARY CLOSURES SHALL BE COORDINATED WITH THE AIRPORT AND BE EXPEDITED TO MINIMIZE CLOSURES.
5. TEMPORARY FENCE SHALL BE INSTALLED PRIOR TO THE REMOVAL OF THE EXISTING FENCE. THE EXISTING FENCE AND GATE SHALL NOT HAVE BEEN REMOVED OR MODIFIED DURING THE FIRST TWO PHASES AND ACCESS TO TENANTS, IDA PERSONNEL AND AIRPORT PERSONNEL SHALL BE CONTINUOUS THROUGHOUT EACH PHASE. ACCESS THROUGH THE TEMPORARY FENCE SHALL BE MONITORED BY A COMPETENT SECURITY GUARD DURING CONSTRUCTION HOURS WHEN THE GATE IS LEFT OPEN. AFTER CONSTRUCTION HOURS, THE TEMPORARY ACCESS SHALL BE CLOSED AND SECURED. THE CONTRACTOR SHALL COORDINATE LOCKING THE GATE WITH THE AIRPORT AND IDA TO REDUCE IMPACTS ON THE CURRENT TENANTS. THE CONTRACTOR SHALL BE REQUIRED TO SUPPLY ADDITIONAL PADLOCKS AND KEYS UPON REQUEST BY THE AIRPORT.
6. THE AIRPORT PERIMETER SHALL REMAIN SECURED AT ALL TIMES. GAPS IN THE PERIMETER FENCE SHALL NOT BE ALLOWED.
7. AIRCRAFT AND AIRPORT VEHICLES ALWAYS HAVE RIGHT OF WAY.

TAXIWAY G



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

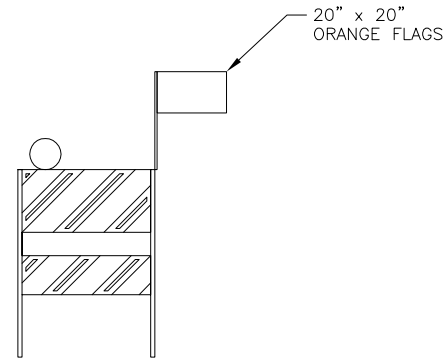
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 CONSTRUCTION ACTIVITY PLAN 3

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO. SPI-4163	
AIP PROJ. NO. 3-17-0096-XX	
SHEET 06 OF 27 SHEETS	



FLASHER BARRICADE DETAIL-IDOT TYPE 1

N.T.S.

FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 10' INTERVALS.

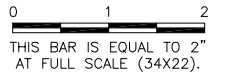
GENERAL NOTES

1. ALL CONSTRUCTION BARRICADES SHALL BE IN PLACE PRIOR TO INITIATING EACH PHASE.
2. ACCESS TO THE T-HANGAR TAXIWAYS AND TO TAXIWAY GOLF SHALL BE CONTINUOUS THROUGHOUT THE PROJECT. THE CONTRACTOR SHALL MAINTAIN ALL MINIMUM DISTANCES SHOWN IN THE CONSTRUCTION ACTIVITY PLANS OR AS REQUIRED FOR THE CRITICAL OPERATING AIRCRAFT THROUGH THESE AREAS.
3. CONSTRUCTION BARRICADES SHALL BE SET AT THE LIMITS OF EACH PHASE'S WORK AREA. DURING PAVING OPERATIONS, BARRICADES MAY BE PLACED UP TO 30' BEYOND THE WORK LIMIT TO ALLOW ROOM FOR PAVING OPERATIONS. OFFSETTING THE BARRICADES TO THIS LOCATION SHALL BE COORDINATED WITH THE AIRPORT IN ADVANCE. IN THE EVENT OF A CONFLICT BETWEEN CONSTRUCTION OPERATIONS AND TAXIING AIRCRAFT, THE CONTRACTOR SHALL CEASE OPERATIONS AND RETURN THE BARRICADES TO THE EDGE OF THE WORKING LIMITS. ALL BARRICADES SHALL BE LOCATED AT THE EDGE OF THE WORKING LIMITS AT THE END OF EACH WORKING DAY.
4. ACTUAL WORK LIMITS/PAVING LIMITS MAY BE REVISED WITH THE AIRPORT'S APPROVAL. ALL MINIMUM DISTANCES SHALL BE MAINTAINED, AIR TRAFFIC ACCESS SHALL REMAIN UNAFFECTED, AND THE PROJECT SCHEDULE SHALL REMAIN THE SAME.

CA013

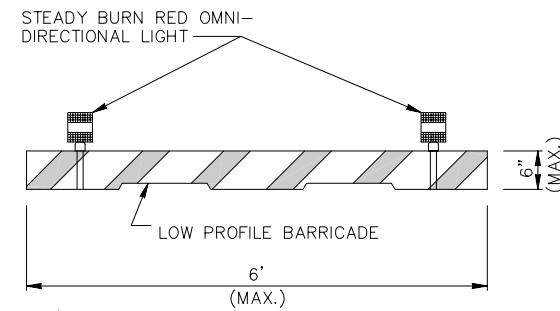
REVISIONS

NUMBER	BY	DATE

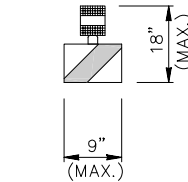


SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

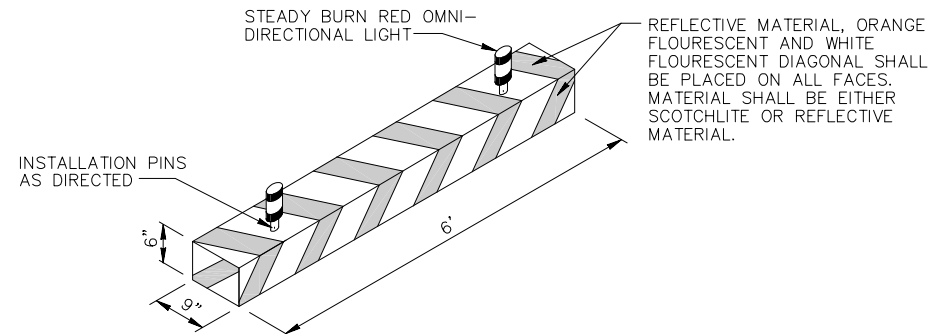
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 CONSTRUCTION ACTIVITY PLAN DETAILS



FRONT ELEVATION



SIDE ELEVATION



ISOMETRIC

BEAM BARRICADE DETAILS

BEAM BARRICADE NOTES

- 1.) BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- 2.) BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- 3.) PLACE AT 10' INTERVALS.

LOW PROFILE BARRICADE DETAILS

N.T.S.

© Copyright CMT, Inc.

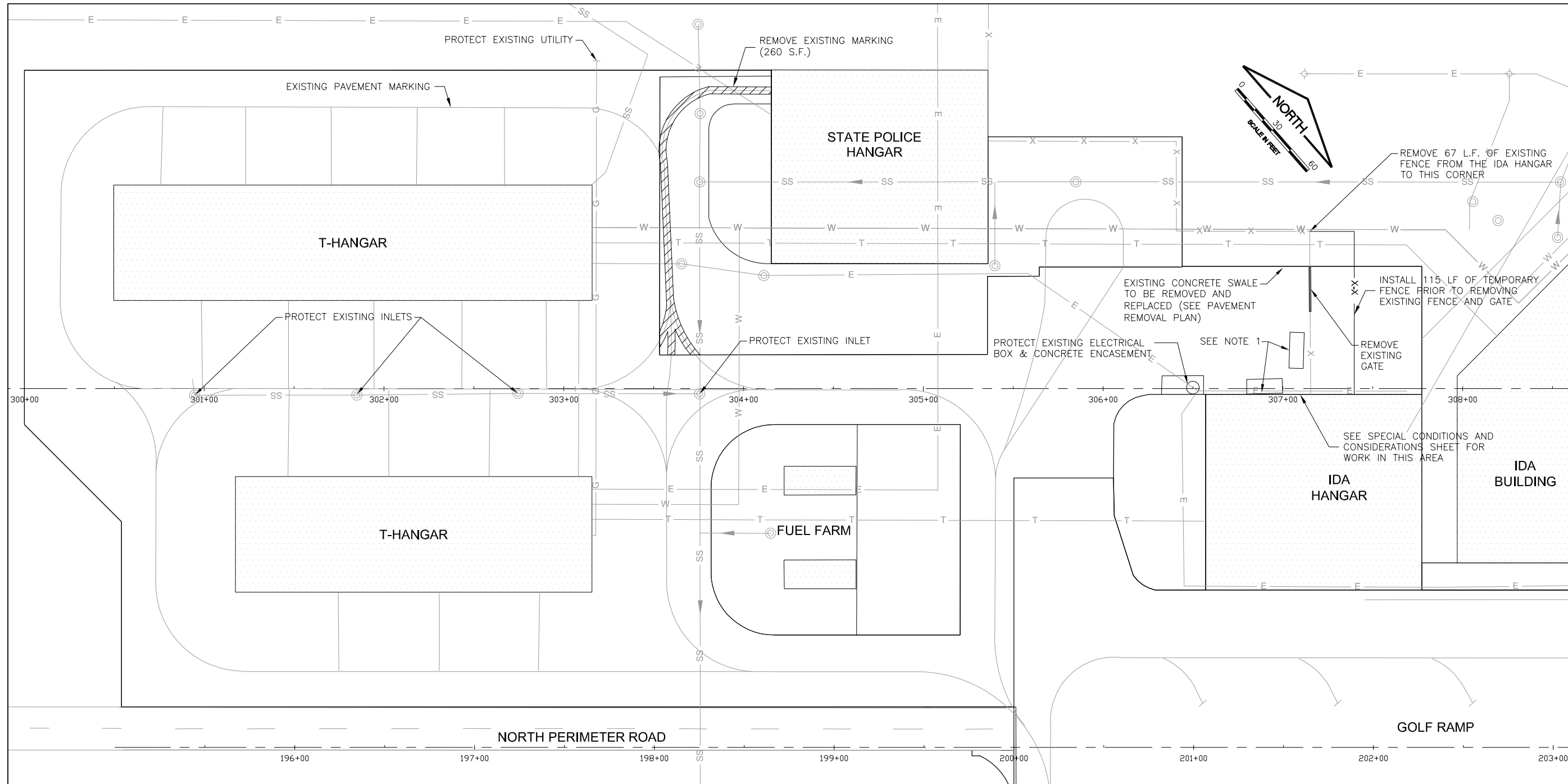
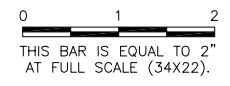
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	07 OF 27 SHEETS

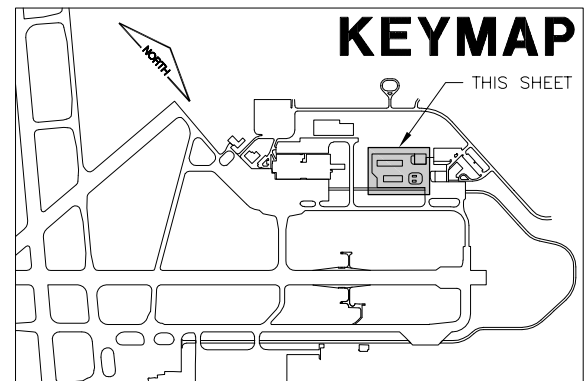
CA013

REVISIONS		
NUMBER	BY	DATE



**SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

**REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 EXISTING CONDITIONS & REMOVALS**



THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

LEGEND			
	EXISTING PAVEMENT		EXIST. TELEPHONE LINE
	NEW PAVEMENT		EXIST. OVERHEAD LIGHT
	EXIST. ELECTRICAL CABLE		EXIST. HANDHOLE
	EXIST. FIBER-OPTIC CABLE		EXIST. DUCT MARKER
	EXIST. STORM SEWER		EXIST. INLET
	EXIST. UNDERDRAIN		EXIST. MANHOLE
	EXIST. FENCE		EXIST. SIGN
	EXISTING GAS LINE		TEMPORARY FENCE
	EXISTING PAVEMENT MARKING		PAVEMENT MARKING REMOVAL

- NOTES**
- STORAGE BINS TO BE TEMPORARILY RELOCATED BY THE CONTRACTOR AT THE DIRECTION OF THE DIVISION OF AERONAUTICS. THE CONTRACTOR SHALL NOTIFY THE DIVISION 7 DAYS PRIOR TO ANY WORK BEING COMPLETED IN THIS AREA. THE COST TO RELOCATE THESE ITEMS SHALL BE INCIDENTAL TO THE AIRPORT.
 - TEMPORARY FENCE SHALL BE INSTALLED SECURELY TO PREVENT UNAUTHORIZED ACCESS. TEMPORARY FENCE SHALL ALSO BE INSTALLED TO ACCOMMODATE CONSTRUCTION AND TENANT VEHICULAR ACCESS BY USE OF PADLOCKS.

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	08 OF 27 SHEETS



PHOTO 1 - ADJACENT T-HANGARS

NOTES

1. PRIOR TO THE COMMENCEMENT OF ANY PAVEMENT DEMOLITION, THE CONTRACTOR SHALL DOCUMENT, IN THE PRESENCE OF THE RESIDENT ENGINEER, THE EXISTING CONDITIONS OF THE SITE INCLUDING ALL ADJACENT CONDITIONS. AT THE END OF CONSTRUCTION, ANY ITEMS SAID TO HAVE BEEN DAMAGED BY THE CONTRACTOR'S OPERATION SHALL BE REPAIRED BY HIM/HER AT NO ADDITIONAL COST TO THE CONTRACT.



PHOTO 2 - ADJACENT T-HANGARS



PHOTO 3 - IDA HANGAR (NORTH SIDE)

CA013

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
SPECIAL CONDITIONS AND CONSIDERATIONS -1

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05

IL. PROJ. NO. SPI-4163
 AIP PROJ. NO. 3-17-0096-XX



PHOTO 4 - ADJACENT T-HANGAR



PHOTO 5 - ADJACENT T-HANGAR



PHOTO 6 - ADJACENT T-HANGAR

CA013

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 SPECIALCONDITIONS AND CONSIDERATIONS - 2

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

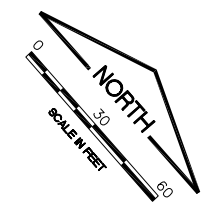


DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05

IL. PROJ. NO. SPI-4163
 AIP PROJ. NO. 3-17-0096-XX

NOTES

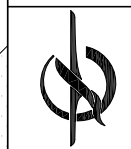
1. PRIOR TO THE COMMENCEMENT OF ANY PAVEMENT DEMOLITION, THE CONTRACTOR SHALL DOCUMENT, IN THE PRESENCE OF THE RESIDENT ENGINEER, THE EXISTING CONDITIONS OF THE SITE INCLUDING ALL ADJACENT CONDITIONS. AT THE END OF CONSTRUCTION, ANY ITEMS SAID TO HAVE BEEN DAMAGED BY THE CONTRACTOR'S OPERATION SHALL BE REPAIRED BY HIM/HER AT NO ADDITIONAL COST TO THE CONTRACT.



CA013

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

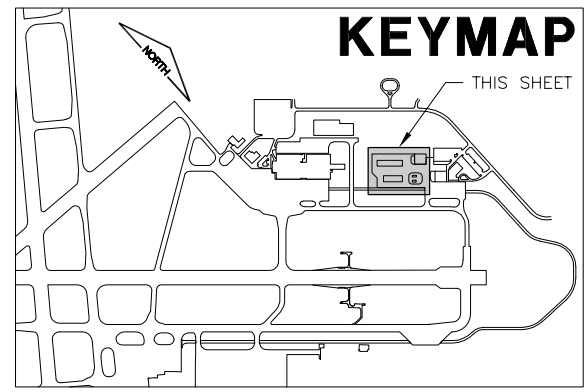
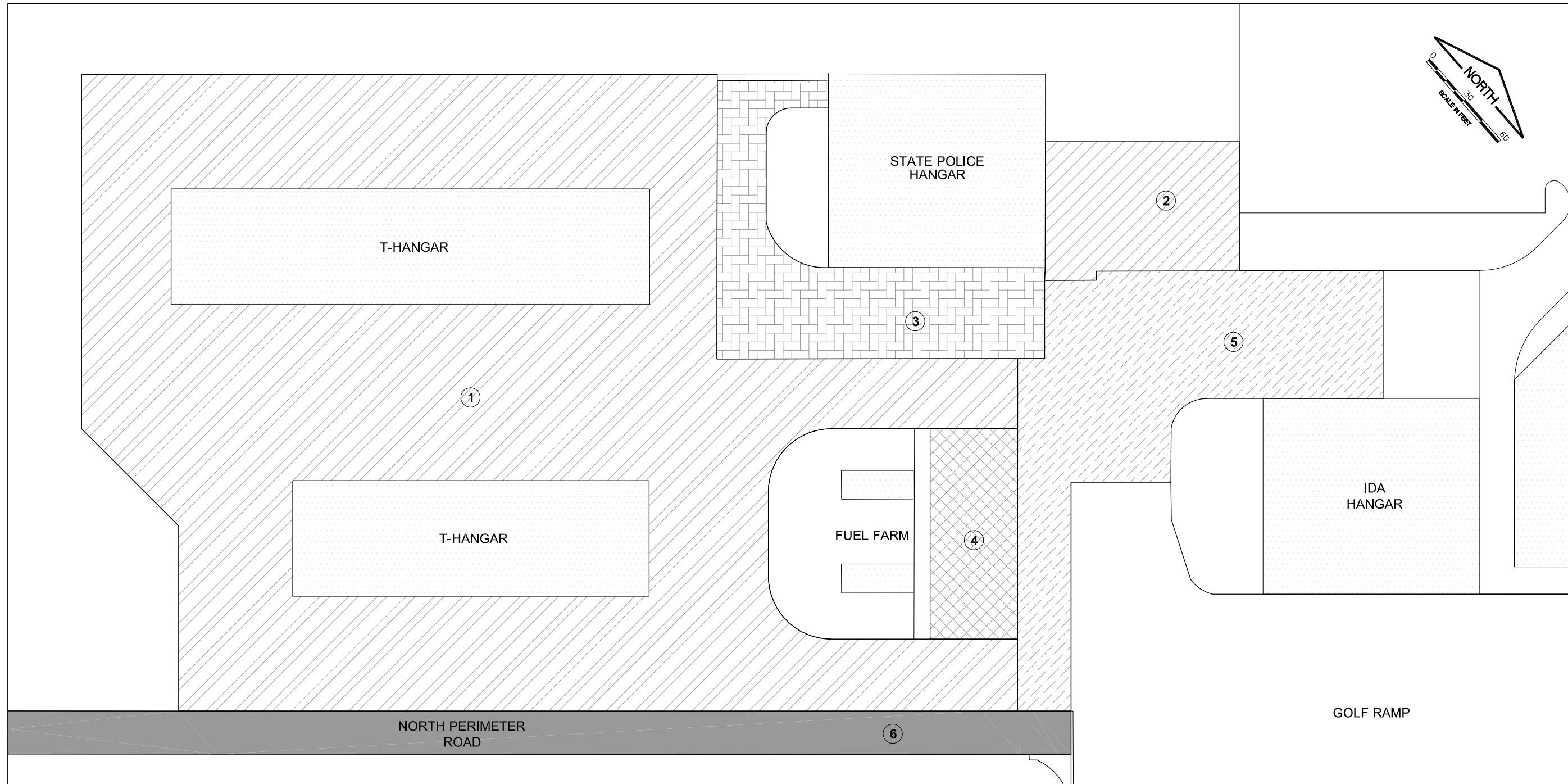
**REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 EXISTING PAVEMENT STRUCTURE**

© Copyright CMT, Inc.


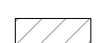


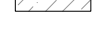

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL. PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	11 OF 27 SHEETS

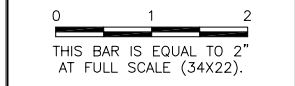


LEGEND

-  ① VARIABLE BITUMINOUS SURFACE COURSE (401)
2" BITUMINOUS SURFACE COURSE (401)
7" BITUMINOUS BASE COURSE (403)
-  ② PCC PAVEMENT - UNKNOWN THICKNESS (501)
-  ③ PCC PAVEMENT - UNKNOWN THICKNESS (501)
-  ④ PCC PAVEMENT - UNKNOWN THICKNESS (501)
-  ⑤ VARIABLE DEPTH BITUMINOUS SURFACE COURSE (401)
2" BITUMINOUS SURFACE COURSE (401)
9" BITUMINOUS BASE COURSE (403)
-  ⑥ 1 1/2" BITUMINOUS SURFACE COURSE (401)
2 1/2" BITUMINOUS BASE COURSE (403)
6" CRUSHED AGGREGATE BASE COURSE (209)

CA013

REVISIONS		
NUMBER	BY	DATE

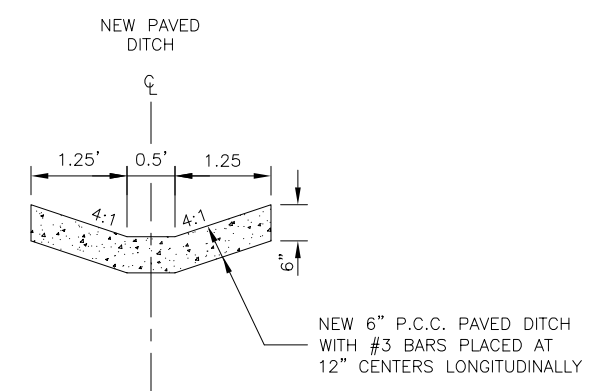
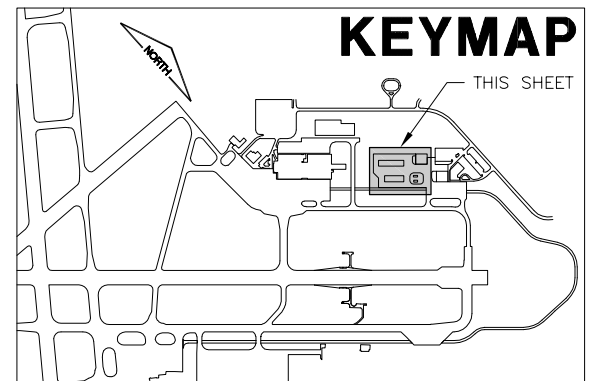
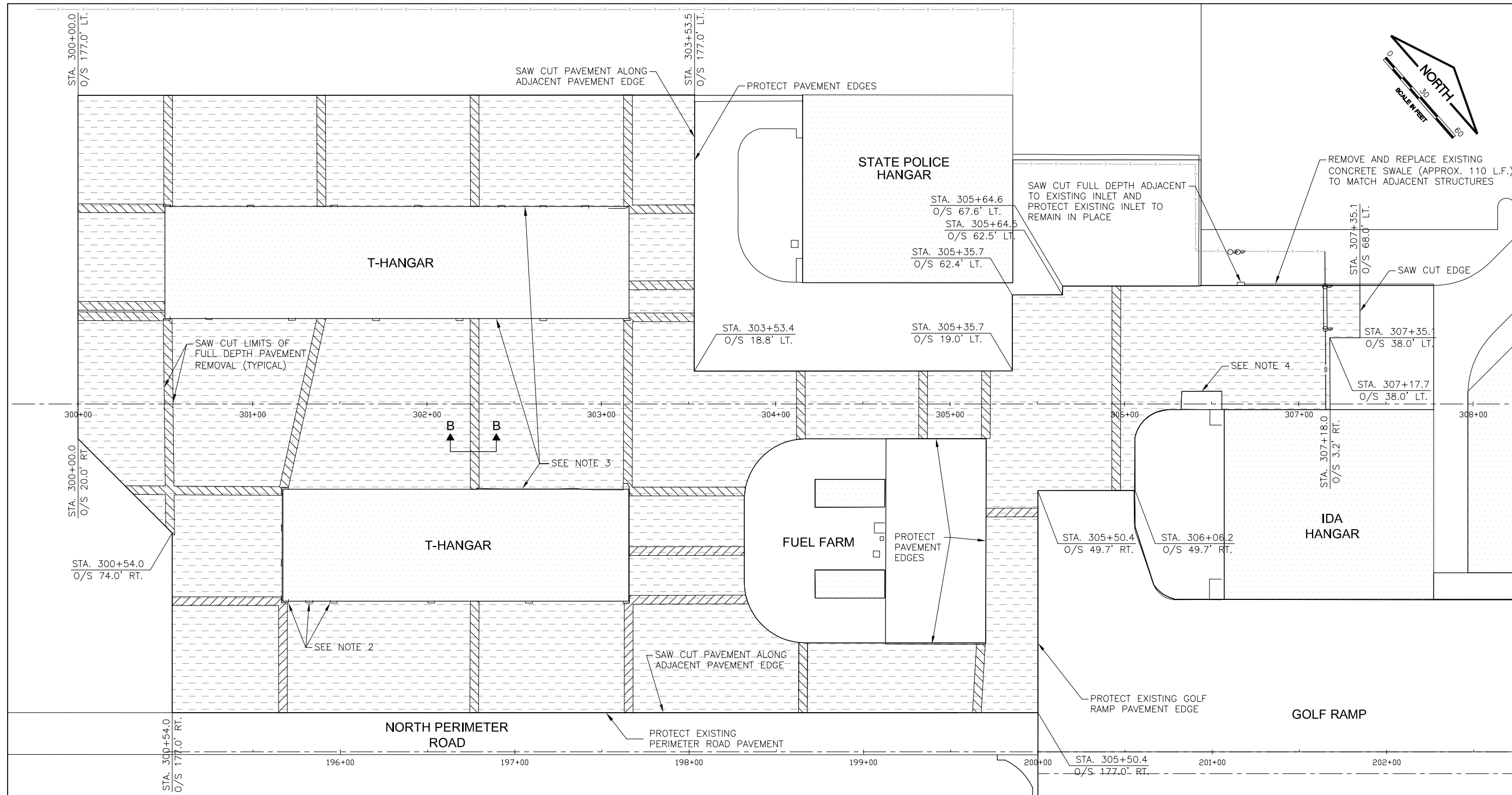


**SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

**REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 PAVEMENT REMOVAL PLAN**

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL. PROJ. NO. SPI-4163 AIP PROJ. NO. 3-17-0096-XX	
SHEET 12 OF 27 SHEETS	



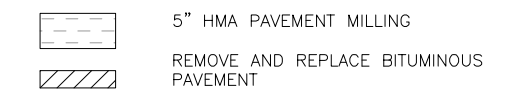
NEW PAVED DITCH TYPICAL SECTION

N.T.S.

NOTES

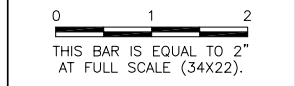
1. WHEN REMOVING PAVEMENT ALONG AN EXISTING PAVEMENT STRUCTURE TO REMAIN IN PLACE, THE CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE THE PAVEMENT. DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY HIM AT HIS/HER EXPENSE.
2. EXISTING CONCRETE FOUNDATION TO REMAIN IN PLACE. CONTRACTOR SHALL USE CAUTION WHEN REMOVING PAVEMENT AROUND THESE FOUNDATIONS.
3. THE EXISTING PAVEMENT EXTENDS UNDER THE T-HANGAR DOORS. THE CONTRACTOR SHALL CLOSELY COORDINATE PAVEMENT REMOVAL WITH THE AIRPORT TO SCHEDULE THE HANGAR DOORS TO BE OPENED WHEN REMOVING PAVEMENT OR PAVING NEAR THE T-HANGAR DOORS.
4. PROTECT EXISTING PCC PAVEMENT AROUND ELECTRICAL JUNCTION BOX.
5. FULL DEPTH CRACK REPAIR TO BE COMPLETED AT THE LOCATIONS SHOWN UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. SEE TYPICAL SECTIONS SHEET FOR FULL DEPTH CRACK REPAIR DETAIL.
6. SAW CUTTING SHALL BE PERFORMED TO PROTECT EXISTING ADJACENT PAVEMENT STRUCTURES TO REMAIN IN PLACE AND SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAVEMENT REMOVAL PAY ITEM.

LEGEND



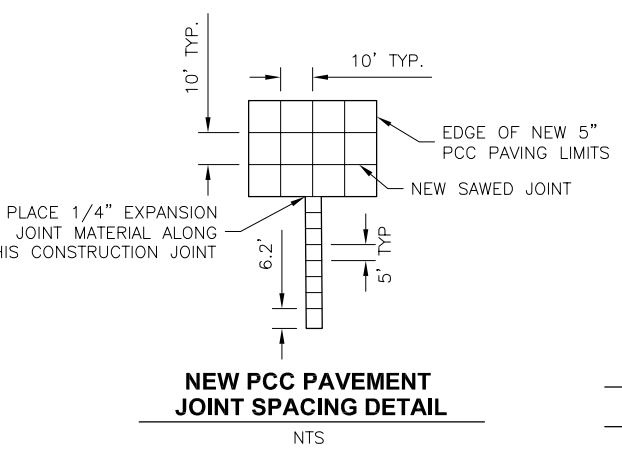
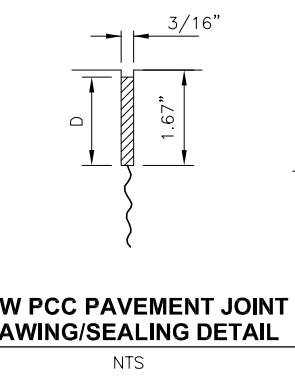
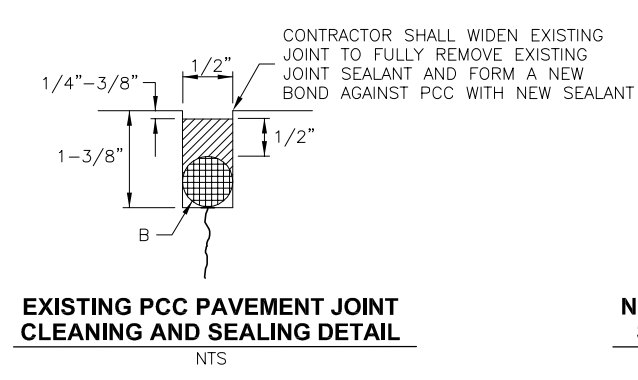
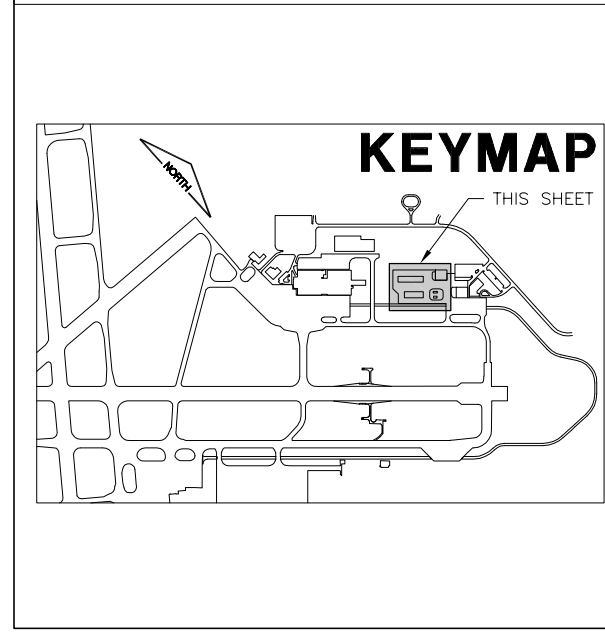
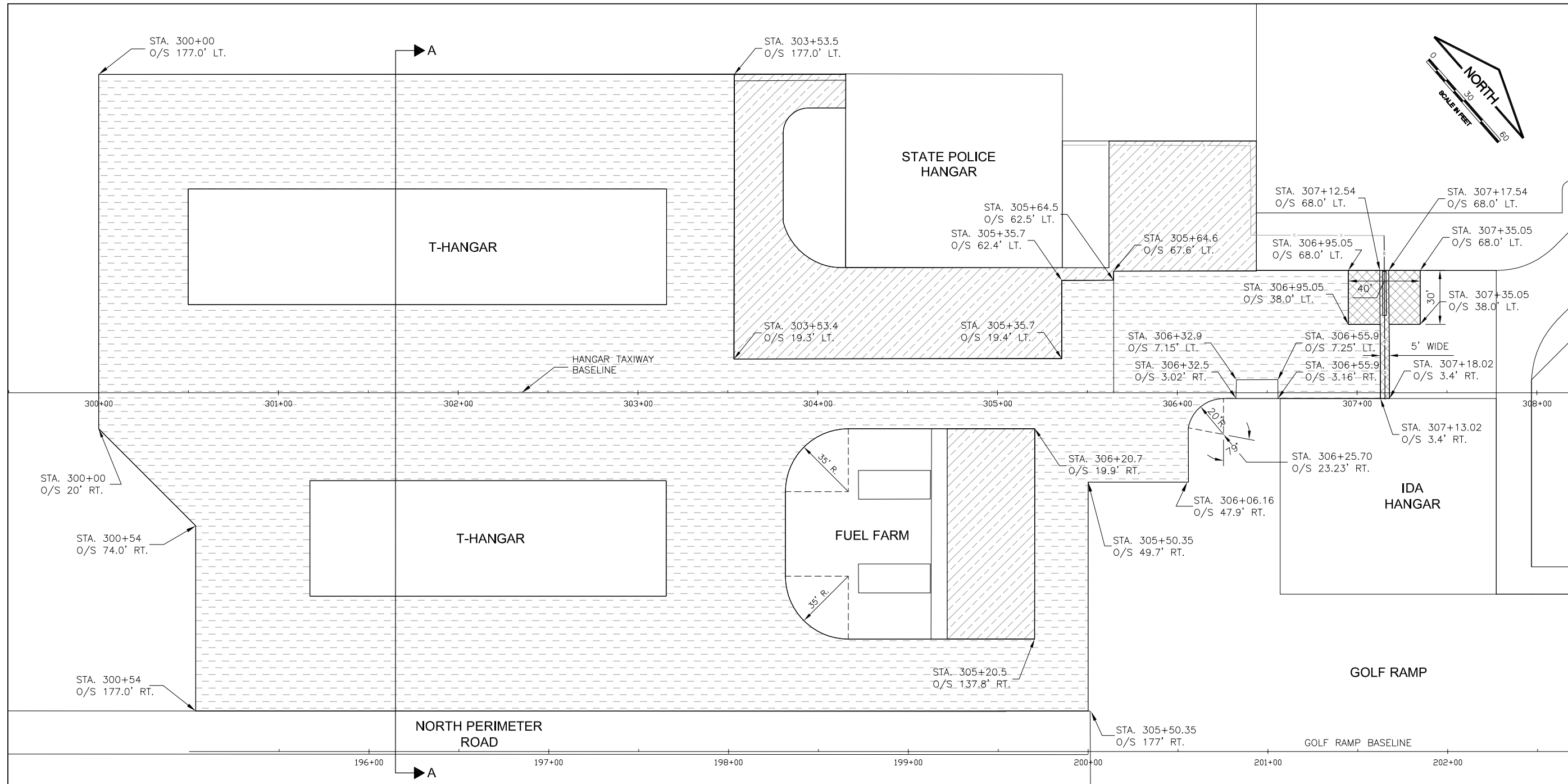
CA013

REVISIONS		
NUMBER	BY	DATE



**SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

**REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 PROPOSED IMPROVEMENTS**



LEGEND

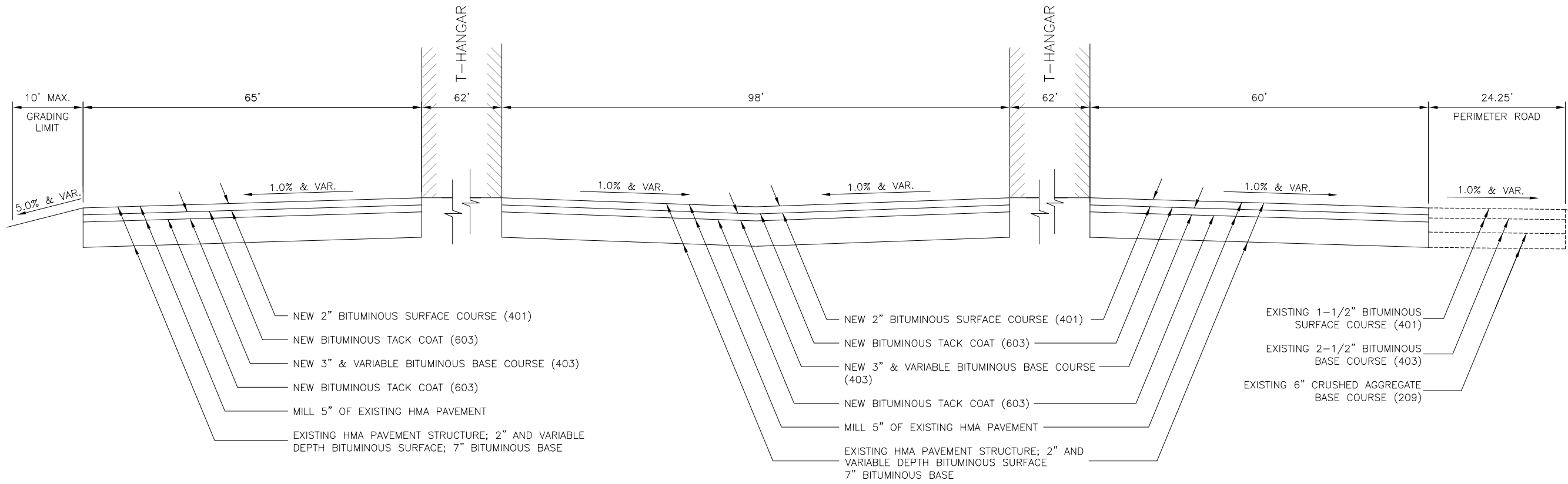
	MILL 5" AND VARIABLE DEPTH OF EXISTING BITUMINOUS PAVEMENT
	NEW 2" BITUMINOUS SURFACE COURSE (401)
	NEW 3" AND VARIABLE BITUMINOUS BASE COURSE (403)
	NEW CLEAN AND SEAL JOINTS (605)
	MILL 5" BITUMINOUS PAVEMENT
	NEW 5" PCC PAVEMENT (SEE JOINTING DETAIL - THIS SHEET)
	NEW ELECTRIC GATE
	NEW 10' FENCE
	EXISTING FENCE

© Copyright CMT, Inc.

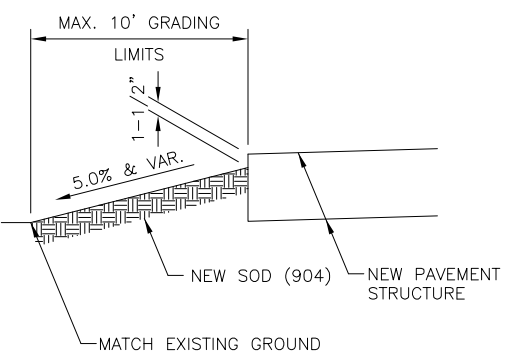
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX

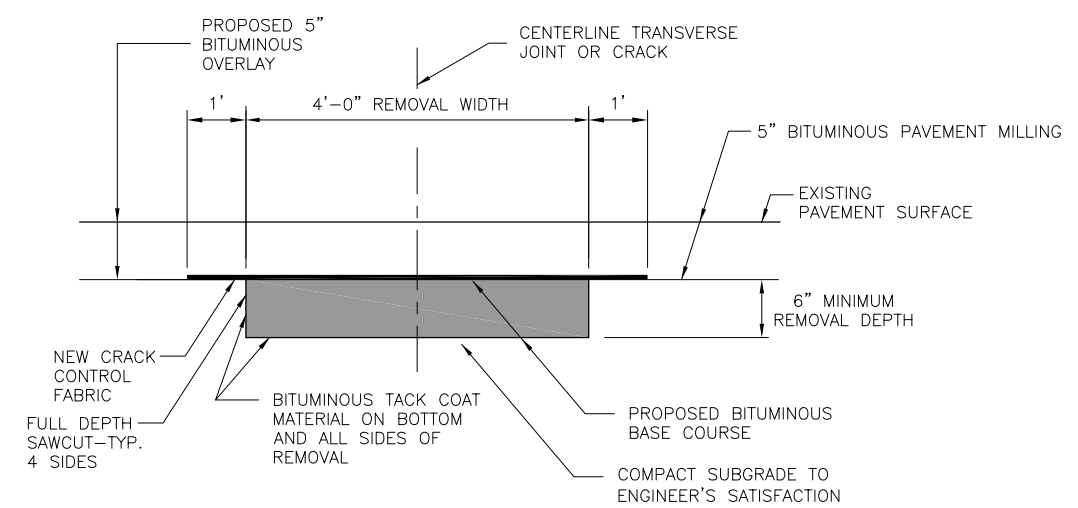
SHEET 13 OF 27 SHEETS



SECTION A-A
 T-HANGAR TAXIWAY TYPICAL SECTION
 NTS



EDGE OF PAVEMENT DETAIL
 NTS



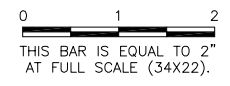
SECTION B-B
 REMOVE AND REPLACE BITUMINOUS PAVEMENT
 N.T.S.

REMOVE AND REPLACE BITUMINOUS PAVEMENT NOTES

1. THE REMOVAL WIDTH SHALL BE 4'-0" AND FULL DEPTH.
2. REMOVAL SHALL BE CENTERED ON THE EXISTING JOINT OR CRACK.
3. FULL DEPTH CRACK REPAIR SHALL BE USED TO REPAIR ALL ASPHALT PAVEMENT JOINTS WITHIN THE LIMITS OF THE PROJECT ACCORDING TO THE DETAIL ON THIS DRAWING AND AT THE LOCATIONS DESIGNATED BY THE RESIDENT ENGINEER.
4. BITUMINOUS TACK COAT SHALL BE PLACED BETWEEN ALL LIFTS.
5. FULL DEPTH SAW CUT, PAVEMENT REMOVAL, SUBGRADE PREPARATION, BITUMINOUS TACK COAT, AND BITUMINOUS BASE COURSE SHALL ALL BE CONSIDERED INCIDENTAL TO THE REMOVE AND REPLACE BITUMINOUS PAVEMENT PAY ITEM.

CA013

REVISIONS		
NUMBER	BY	DATE



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

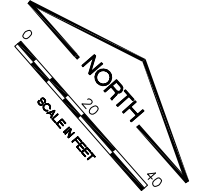
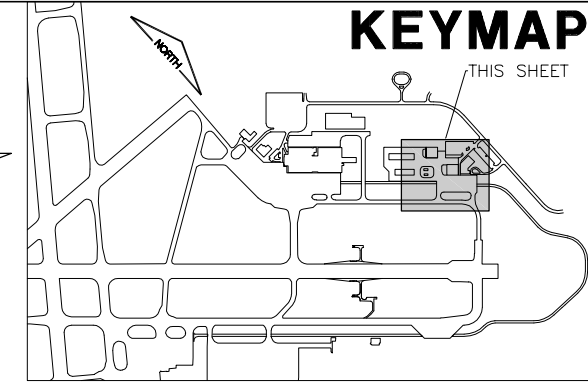
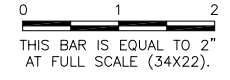
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 TYPICAL SECTIONS

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	14 OF 27 SHEETS

CA013

REVISIONS		
NUMBER	BY	DATE

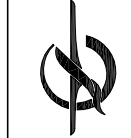


LEGEND

- 600.00 NEW ELEVATION
- 600.00 EXISTING ELEVATION
- 600 NEW CONTOUR
- EXISTING INLET
- EXISTING MANHOLE

NOTES

- CONTRACTOR SHALL MILL EXISTING BITUMINOUS PAVEMENT TO A CONSTANT DEPTH OF 5". THE BITUMINOUS BASE COURSE SHALL BE 3" AND VARIABLE BASED ON THE NEW VS. EXISTING SPOT ELEVATION. MAXIMUM PAYMENT FOR THE BITUMINOUS BASE COURSE WILL BE 103% OF THE CONTRACT QUANTITY WITH ANY OVERAGE TO BE CONSIDERED AT THE CONTRACTOR'S EXPENSE. BITUMINOUS SURFACE COURSE SHALL BE A CONSTANT 2" THICKNESS.



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

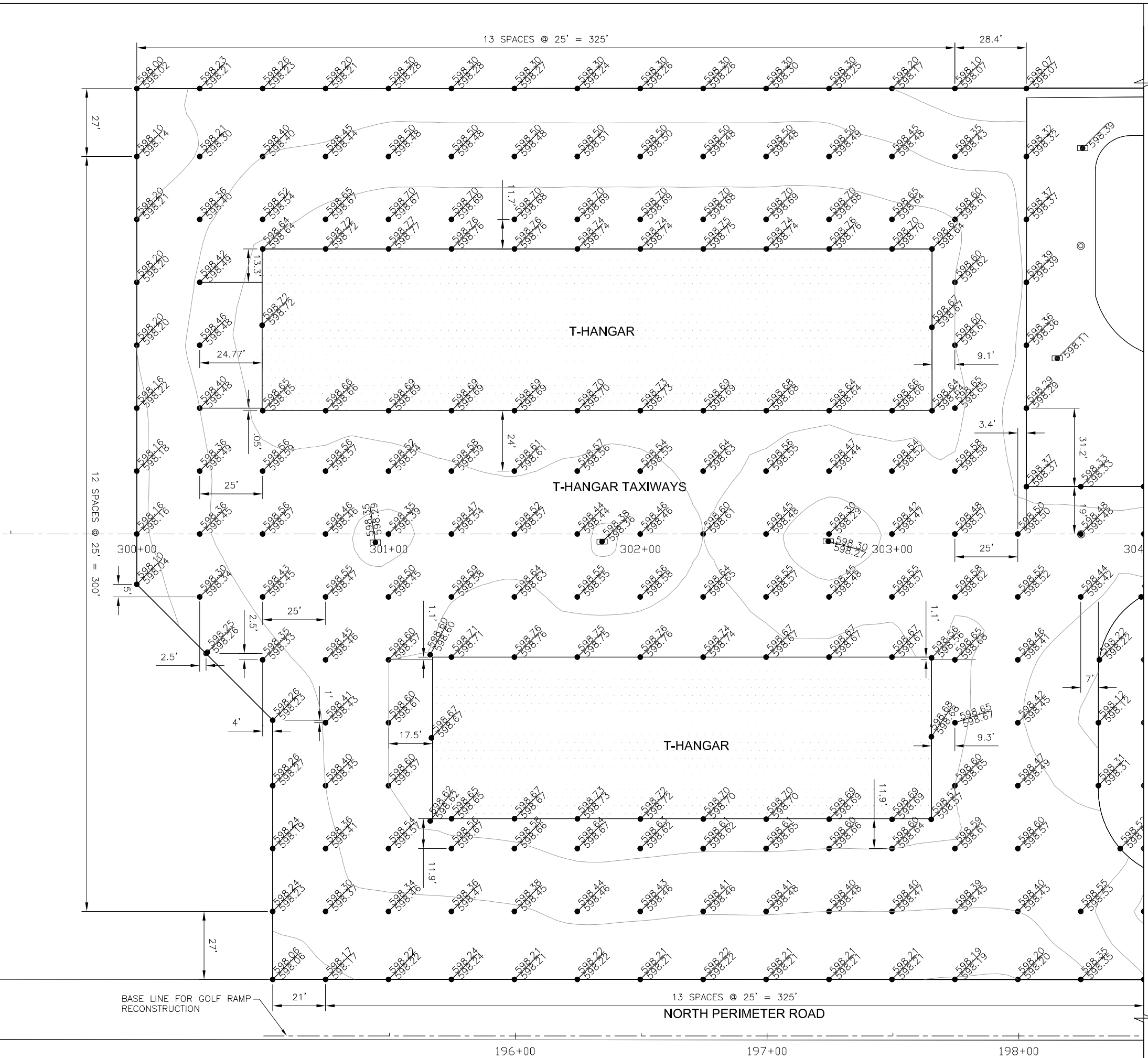
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
STAKING PLAN 1

© Copyright CMT, Inc.

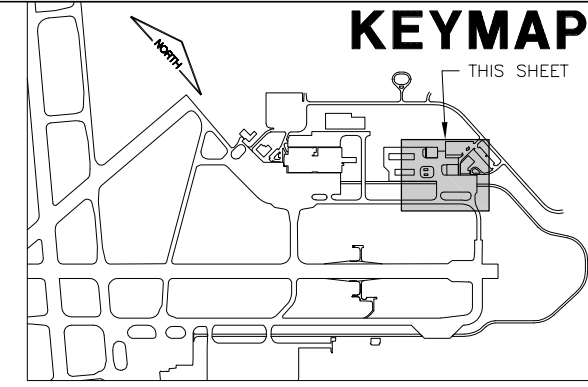
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL. PROJ. NO. SPI-4163 AIP PROJ. NO. 3-17-0096-XX	
SHEET 15 OF 27 SHEETS	



KEYMAP

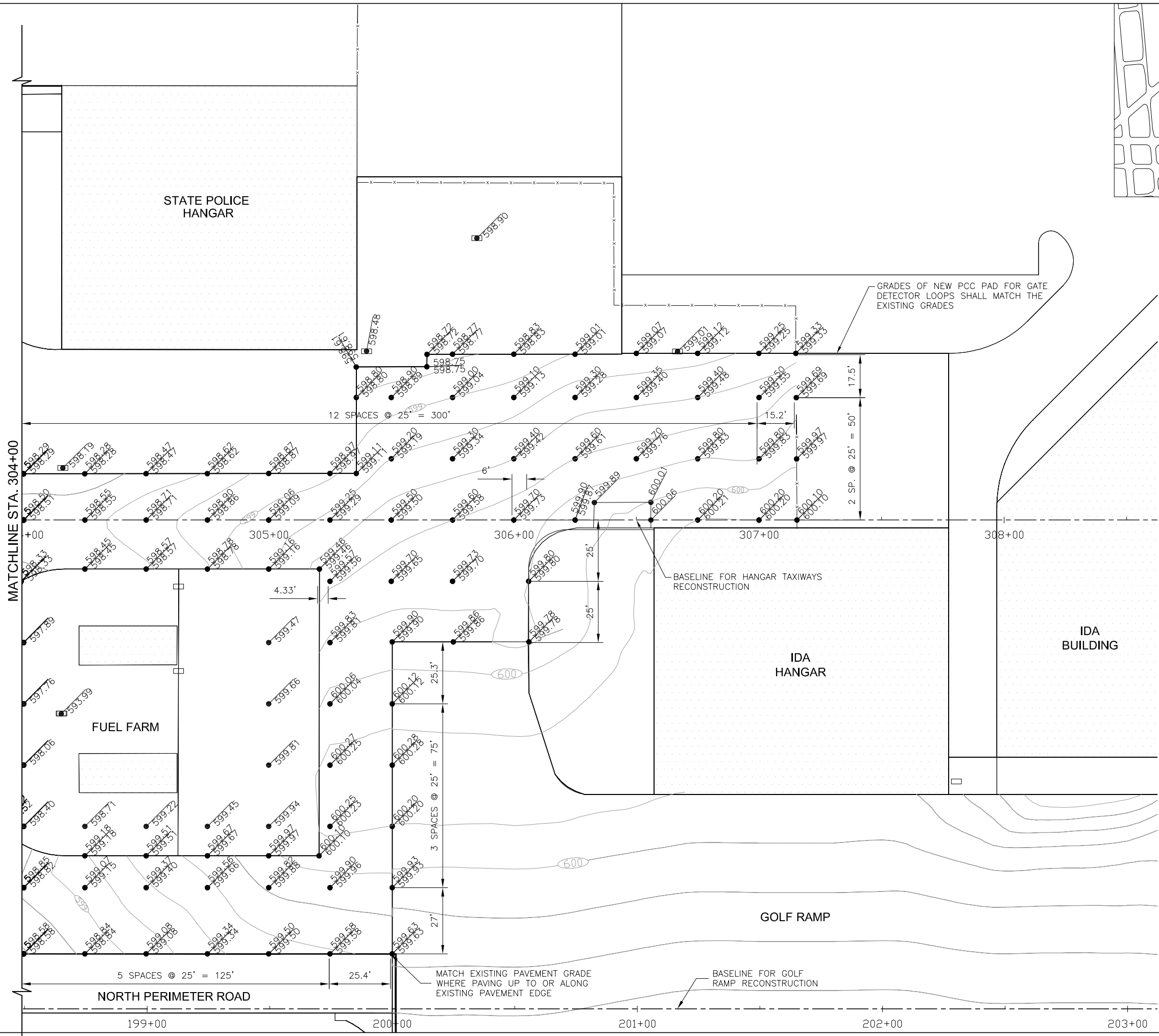
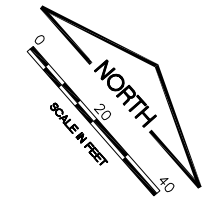
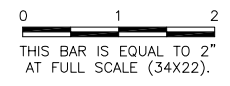


K:\Springfield\0903505\Draw\Sheets\PHASE 2
 FILE: STAKPLAN2-PH2.dwg
 UPDATE BY: TJ Heavysides
 PLOT DATE: 5/9/2012 1:38 PM
 BASE_EXISTGEO
 BASE_PROJGEO
 SPI-BASE-1
 SPI-KEYMAP
 EGPTBASE_SURVEY
 SPL_Contour
 SPL_Align

CA013

REVISIONS

NUMBER	BY	DATE

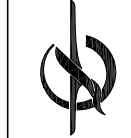


LEGEND

- NEW ELEVATION
- EXISTING ELEVATION
- NEW CONTOUR
- EXISTING INLET
- EXISTING MANHOLE

NOTES

- CONTRACTOR SHALL MILL EXISTING BITUMINOUS PAVEMENT TO A CONSTANT DEPTH OF 5". THE BITUMINOUS BASE COURSE SHALL BE 3" AND VARIABLE BASED ON THE NEW VS. EXISTING SPOT ELEVATION. MAXIMUM PAYMENT FOR THE BITUMINOUS BASE COURSE WILL BE 103% OF THE CONTRACT QUANTITY WITH ANY OVERAGE TO BE CONSIDERED AT THE CONTRACTOR'S EXPENSE. BITUMINOUS SURFACE COURSE SHALL BE A CONSTANT 2" THICKNESS.



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

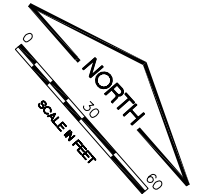
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
STAKING PLAN 2

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

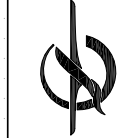
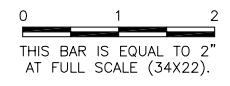


DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	16 OF 27 SHEETS



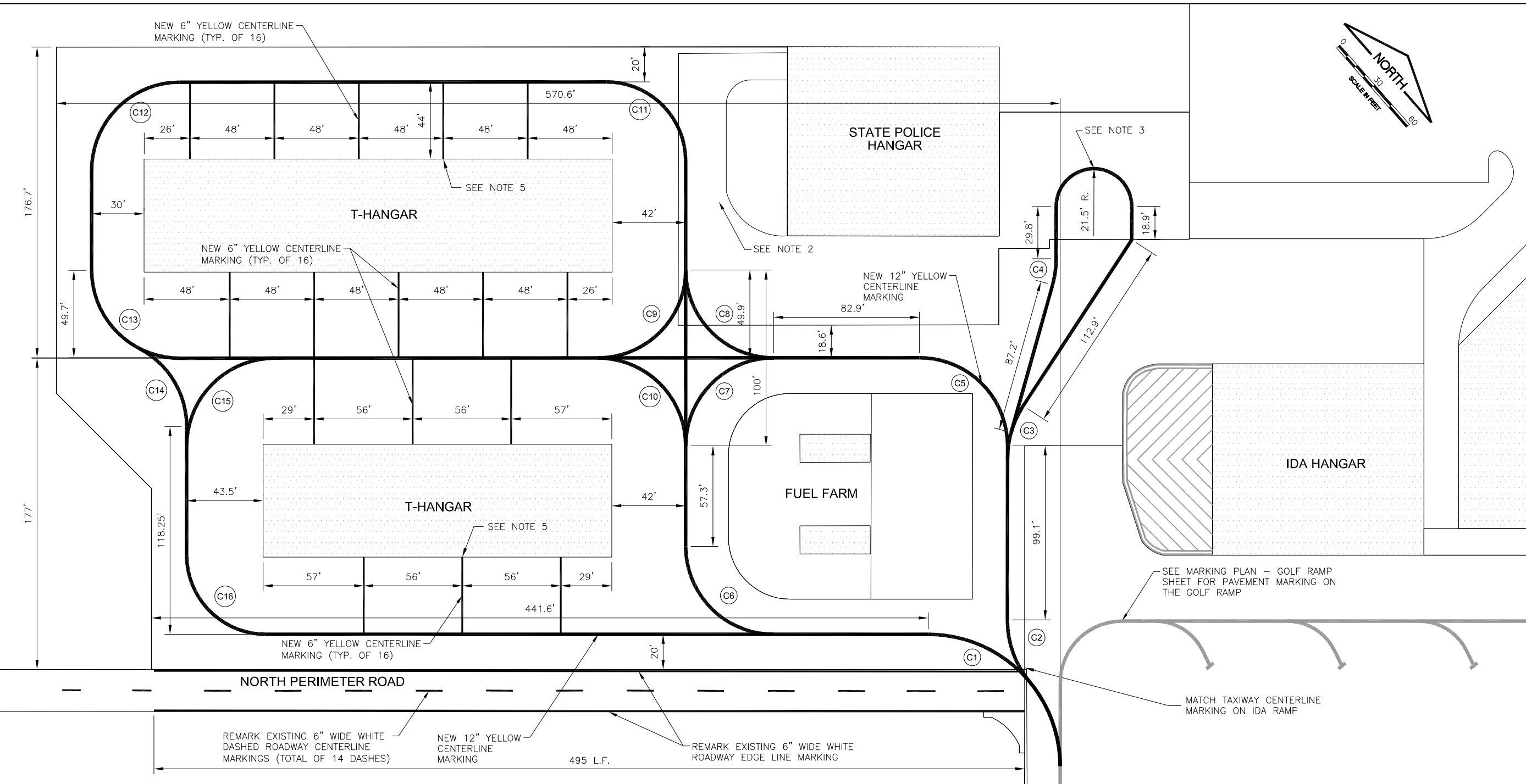
CA013

REVISIONS		
NUMBER	BY	DATE



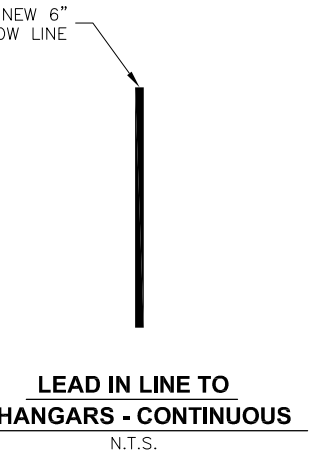
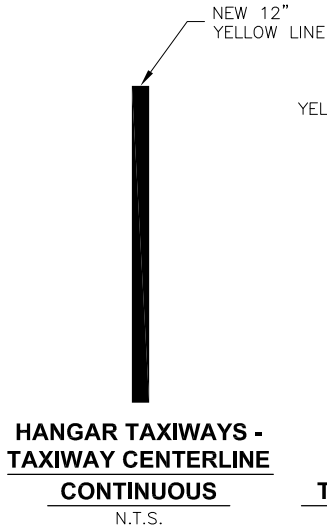
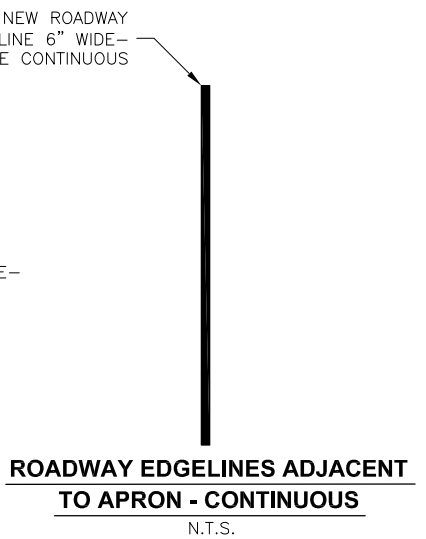
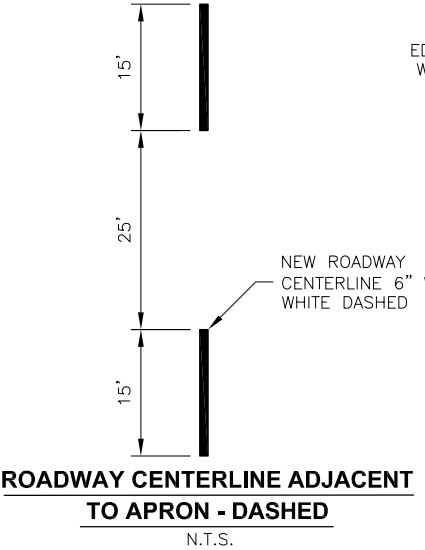
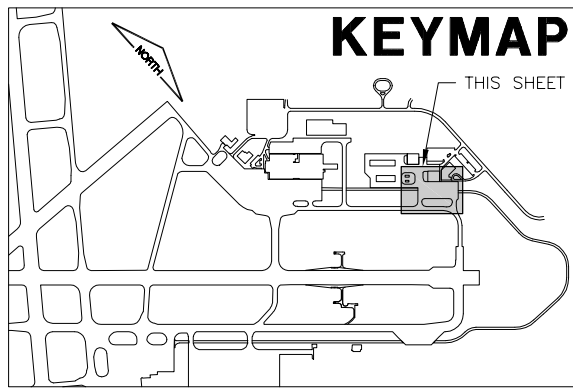
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
MARKING PLAN - HANGAR TAXIWAYS



LEGEND

- EXISTING MARKING
- NEW MARKING
- CURVE INFORMATION (SEE MARKING AND MISCELLANEOUS DETAILS)



NOTES

1. MARKINGS FROM THE T-HANGAR TAXIWAYS SHALL TIE INTO EXISTING MARKINGS ON THE GOLF RAMP.
2. EXISTING MARKING ON THE "STATE POLICE HANGAR'S" PCC PAVEMENT AS NOTED SHALL BE REMOVED (254 S.F.) AND NEW MARKINGS SHALL BE COMPLETED AS SHOWN. SEE EXISTING CONDITIONS AND REMOVAL SHEET FOR MARKING REMOVAL LOCATION.
3. THE PORTION OF THE "TEARDROP" MARKING ON THE STATE POLICE HANGAR'S PCC PAVEMENT SHALL BE REMARKED IN IT'S CURRENT LOCATION. THE NEW MARKINGS SHALL TIE INTO THE EXISTING.
4. TAXIWAY CENTERLINE MARKINGS FOR THE NEW HANGAR PAVEMENT SHALL BE SOLID YELLOW, TO THE WIDTH SHOWN, AND WITHOUT A BLACK BACKGROUND.
5. CONTRACTOR SHALL CORE 6" HOLE IN NEW PAVEMENT AT THE END OF T-HANGAR LEAD IN LINE IN THE CENTER OF THE BI-FOLD DOORS (TYPICAL OF 16 DOORS). ACTUAL LOCATION SHALL BE DETERMINED IN FIELD BASED ON INDIVIDUAL DOOR LATCHES. CORES SHALL BE CONSIDERED INCIDENTAL TO THE PAVING OPERATION.

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET 17 OF 27 SHEETS	

CA013

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

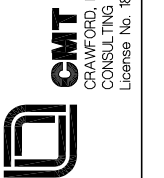
HANGAR TAXIWAYS - MARKING COORDINATES						
CURVE	POINT	NORTHING	EASTING	STATION	OFFSET	CURVE ELEMENTS
C1	PC	1161057.64	2435748.21	305+70.6	232.1' RT	RADIUS= 75.0'
	RP	1161001.36	2435698.64	304+95.6	232.0' RT	LENGTH= 117.90'
	PT	1161050.86	2435642.30	304+95.6	157.0' RT	DELTA= 90°04'07.74"
C2	PC	1161076.33	2435700.69	305+53.2	184.0' RT	RADIUS= 55.0'
	RP	1161131.31	2435702.18	305+95.5	148.9' RT	LENGTH= 38.21'
	PT	1161090.03	2435665.84	305+40.5	148.8' RT	DELTA= 39°48'22.19"
C3	PC	1161150.66	2435596.98	305+40.6	57.1' RT	RADIUS= 55.0'
	RP	1161191.94	2435633.32	305+95.6	57.1' RT	LENGTH= 31.75'
	PT	1161177.19	2435580.34	305+49.6	27.1' RT	DELTA= 33°04'19.37"
C4	PC	1161246.44	2435529.85	305+68.3	56.6' LT	RADIUS= 55.0'
	RP	1161205.10	2435493.57	305+13.3	56.5' LT	LENGTH= 15.07'
	PT	1161235.08	2435539.68	305+66.2	41.7' LT	DELTA= 15°42'11.96"
C5	PC	1161155.51	2435591.48	305+40.6	49.7' RT	RADIUS= 50.0'
	RP	1161117.98	2435558.43	304+90.6	49.7' RT	LENGTH= 78.63'
	PT	1161150.96	2435520.85	304+90.6	0.3' LT	DELTA= 90°06'02.29"
C6	PC	1160984.84	2435584.30	304+07.7	157.0' RT	RADIUS= 50.0'
	RP	1161017.84	2435546.74	304+07.7	107.0' RT	LENGTH= 78.5'
	PT	1160980.28	2435513.74	303+57.7	107.0' RT	DELTA= 90°00'00.00"
C7	PC	1161018.07	2435470.73	303+57.7	49.7' RT	RADIUS= 50.0'
	RP	1161055.63	2435503.73	304+07.7	49.7' RT	LENGTH= 78.51'
	PT	1161088.61	2435466.14	304+07.7	0.3' LT	DELTA= 89°57'50.29"
C8	PC	1161088.66	2435466.19	304+07.7	0.3' LT	RADIUS= 50.0'
	RP	1161121.64	2435428.60	304+07.7	50.3' LT	LENGTH= 78.57'
	PT	1161084.07	2435395.60	303+57.7	50.3' LT	DELTA= 90°02'09.71"
C9	PC	1161013.49	2435400.23	303+07.7	0.2' LT	RADIUS= 50.0'
	RP	1161046.47	2435362.65	303+07.7	50.2' LT	LENGTH= 78.51'
	PT	1161084.03	2435395.65	303+57.7	50.2' LT	DELTA= 89°57'50.29"
C10	PC	1161013.45	2435400.19	303+07.7	0.2' LT	RADIUS= 50.0'
	RP	1160980.47	2435437.77	303+07.7	49.8' RT	LENGTH= 78.57'
	PT	1161018.03	2435470.77	303+57.7	49.8' RT	DELTA= 90°02'09.71"
C11	PC	1161124.96	2435349.07	303+57.7	112.2' LT	RADIUS= 45.0'
	RP	1161091.15	2435319.37	303+12.7	112.2' LT	LENGTH= 70.71'
	PT	1161120.83	2435285.54	303+12.7	157.2' LT	DELTA= 90°02'09.71"
C12	PC	1160938.40	2435125.47	300+70.0	157.0' LT	RADIUS= 50.0'
	RP	1160905.42	2435163.05	300+70.0	107.0' LT	LENGTH= 78.51'
	PT	1160867.86	2435130.05	300+20.0	107.0' LT	DELTA= 89°57'50.29"
C13	PC	1160830.24	2435172.87	300+20.0	50.0' LT	RADIUS= 50.0'
	RP	1160867.80	2435205.88	300+70.0	50.0' LT	LENGTH= 78.57'
	PT	1160834.82	2435243.46	300+70.0	0.0' LT	DELTA= 90°02'09.71"
C14	PC	1160821.22	2435224.05	300+47.0	5.6' LT	RADIUS= 50.0'
	RP	1160774.64	2435242.22	300+24.0	38.7' RT	LENGTH= 54.64'
	PT	1160812.20	2435275.22	300+74.0	38.7' RT	DELTA= 62°36'46.41"
C15	PC	1160804.82	2435283.61	300+74.0	49.9' RT	RADIUS= 50.0'
	RP	1160842.39	2435316.62	301+24.0	49.9' RT	LENGTH= 78.51'
	PT	1160875.36	2435279.03	301+24.0	0.1' LT	DELTA= 89°57'50.29"
C16	PC	1160763.85	2435330.25	300+74.0	112.0' RT	RADIUS= 45.0'
	RP	1160797.66	2435359.95	301+19.0	112.0' RT	LENGTH= 70.69'
	PT	1160767.96	2435393.76	301+19.0	157.0' RT	DELTA= 90°00'00.00"



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 MARKING & MISCELLANEOUS DETAILS

© Copyright CMT, Inc.



CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY: RLV/TJH

DRAWN BY: CMT

CHECKED BY: RLV

APPROVED BY: RLV

DATE: MAY 4, 2012

JOB No: 09035-05

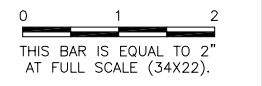
IL PROJ. NO. SPI-4163
 AIP PROJ. NO. 3-17-0096-XX

SHEET 18 OF 27 SHEETS

CA013

REVISIONS

NUMBER	BY	DATE



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

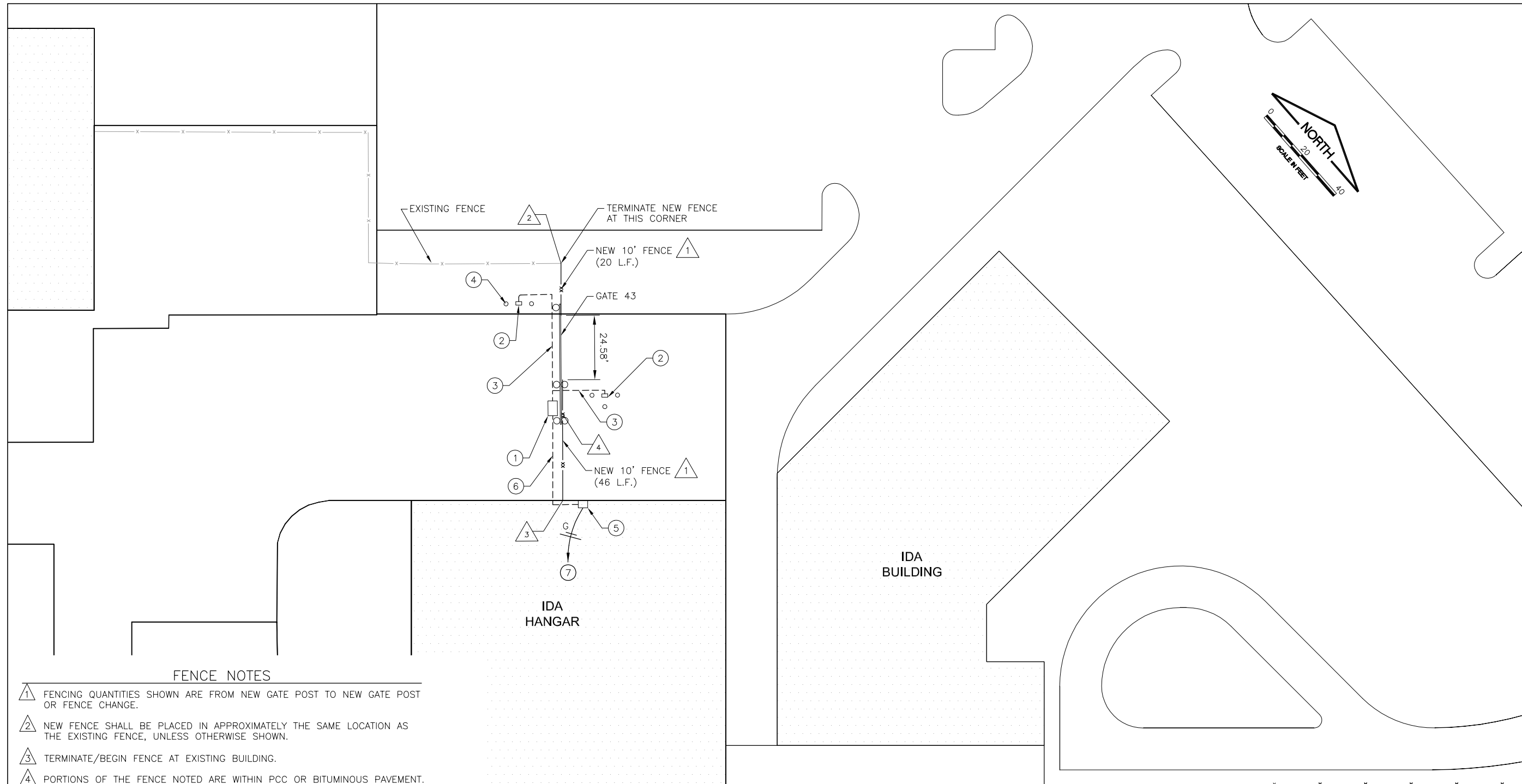
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
FENCE PLAN

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET 19 OF 27 SHEETS	

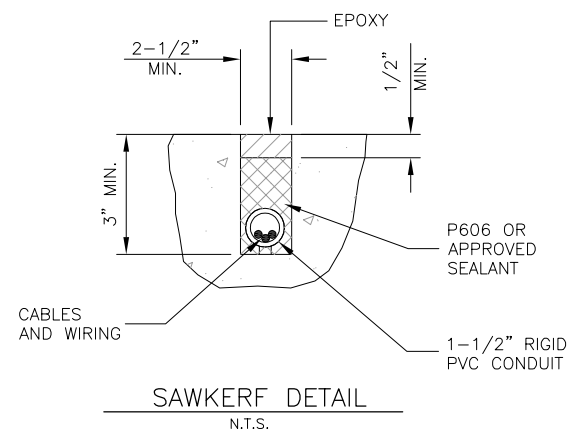


FENCE NOTES

- 1 FENCING QUANTITIES SHOWN ARE FROM NEW GATE POST TO NEW GATE POST OR FENCE CHANGE.
- 2 NEW FENCE SHALL BE PLACED IN APPROXIMATELY THE SAME LOCATION AS THE EXISTING FENCE, UNLESS OTHERWISE SHOWN.
- 3 TERMINATE/BEGIN FENCE AT EXISTING BUILDING.
- 4 PORTIONS OF THE FENCE NOTED ARE WITHIN PCC OR BITUMINOUS PAVEMENT.

GATE NOTES

- 1 CANTILEVER SLIDE GATE, LINEAR/OSCO MODEL HSLG-1, 1/2 HP, 120V, SINGLE PHASE, OR EQUIVALENT, SUITABLE FOR A MAXIMUM GATE WEIGHT OF 1700 LBS. SEE SPECIFICATIONS FOR ADDITIONAL INFORMATION.
- 2 NEW EXIT READER. SEE SPECIFICATIONS FOR ADDITIONAL INFORMATION.
- 3 6-CONDUCTOR #18 SHIELDED CABLE IN 1" CONDUIT. CONTRACTOR SHALL VERIFY WITH CARD READER MANUFACTURER.
- 4 48" TALL, 6" DIAMETER, CONCRETE-FILLED STEEL POST, EMBEDDED 48" INTO A 12" DIAMETER CONCRETE FOUNDATION. ABOVE GRADE PORTION OF POST TO BE PAINTED WITH MINIMUM OF TWO COATS BRIGHT YELLOW EPOXY PAINT. (TYPICAL OF FIVE.)
- 5 NEW GATE CONTROLLER, AAS PROACCESS 200, OR EQUIVALENT, WALL-MOUNTED INSIDE HANGAR WHERE DIRECTED. SEE SPECIFICATIONS FOR ADDITIONAL INFORMATION.
- 6 TWO 6- CONDUCTOR #18 SHIELDED CABLE (CONTRACTOR TO VERIFY) AND TWO #14 (GATE OPEN CONTROL) IN 1-1/2" CONDUIT.
- 7 TWO #12 THWN (120V), ONE #12 GND IN 3/4" CONDUIT TO PANELBOARD IN IDA HANGAR.

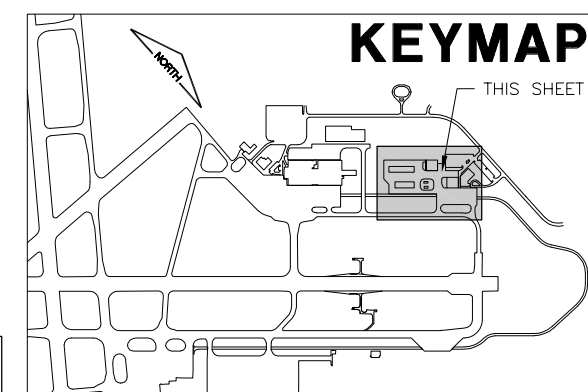


NOTE:
 NEW CONDUITS SHALL BE INSTALLED IN SAWKERF IN NEW BASEMENT

LEGEND

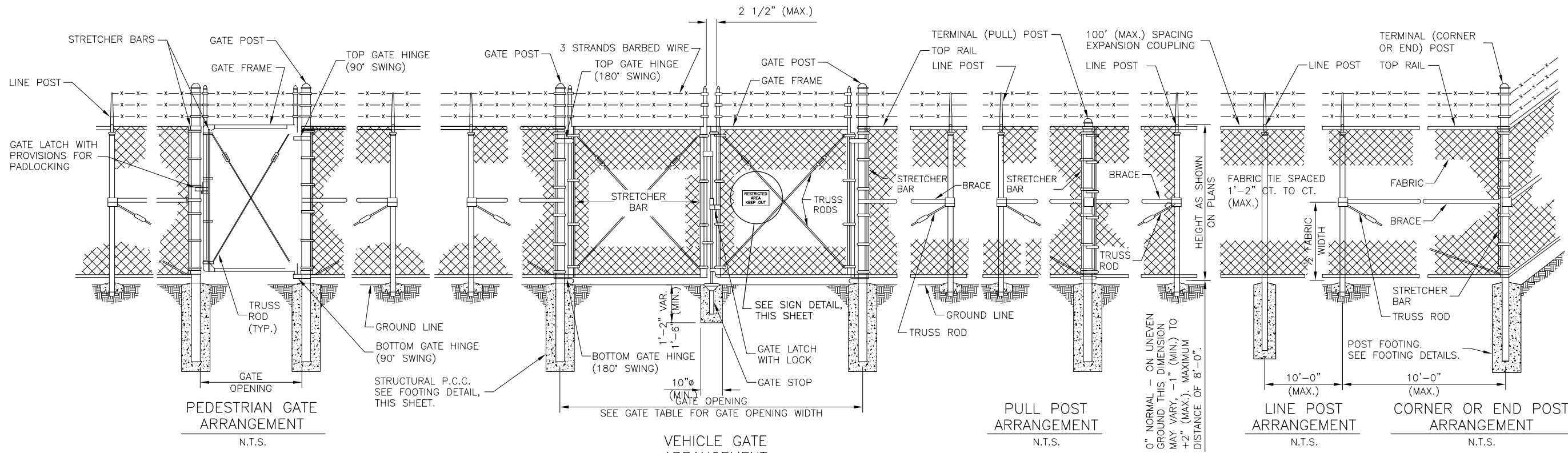
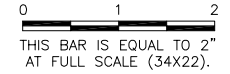
- xx — xx — NEW 10' FENCE
- - - - - EXISTING FENCE
- [Symbol] — NEW ELECTRIC GATE

ALL SPLICES SHALL BE MADE USING TWIST-ON CONNECTORS PRE-FILLED WITH SILICONE-BASED SEALANT TO PROTECT AGAINST MOISTURE AND CORROSION.

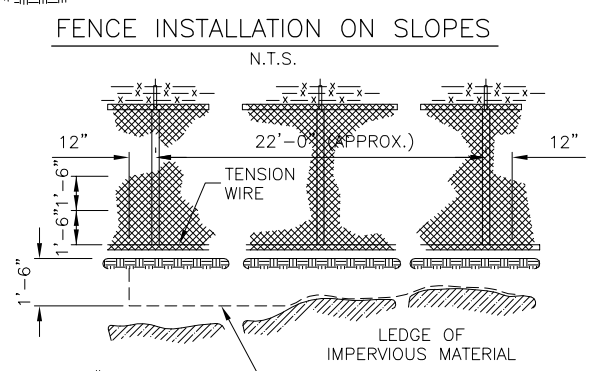
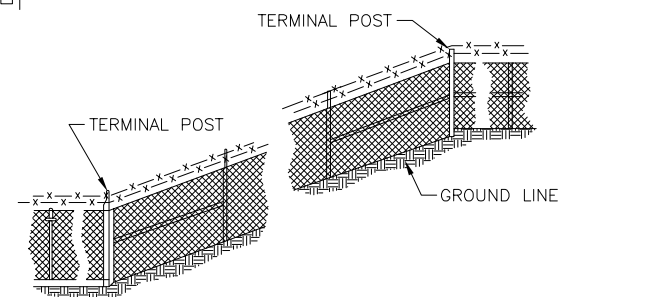
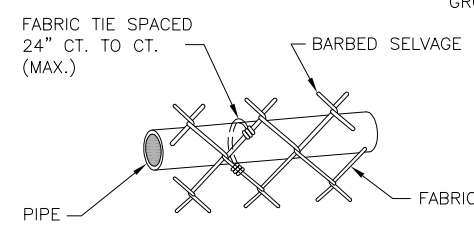
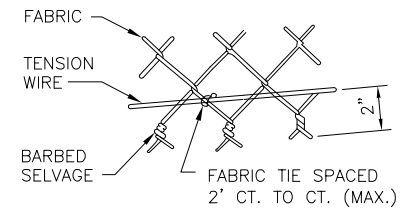
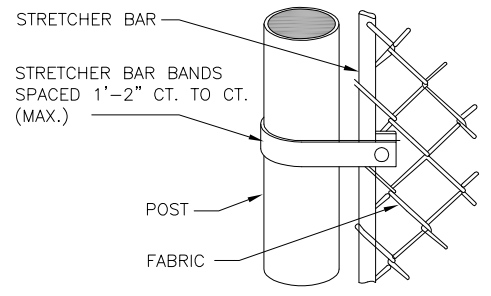
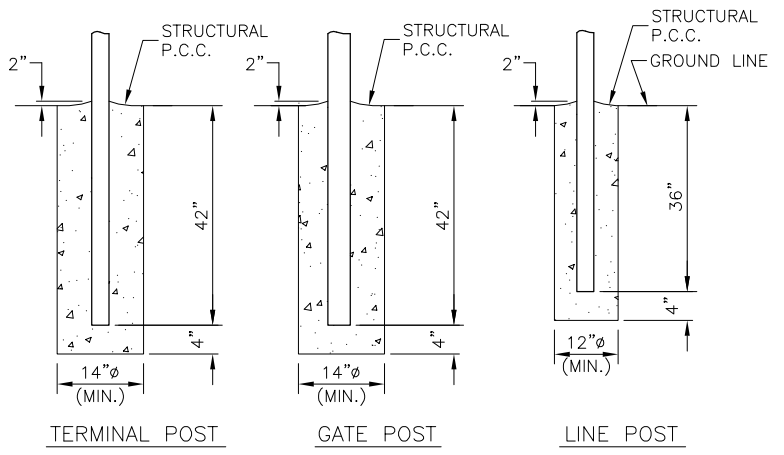


CA013

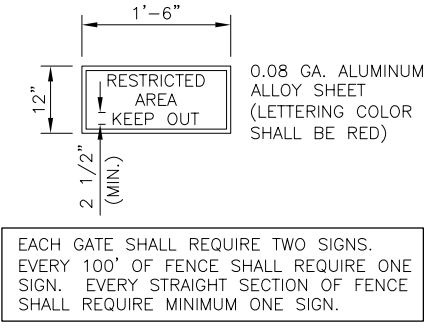
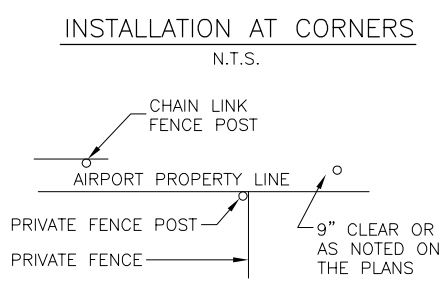
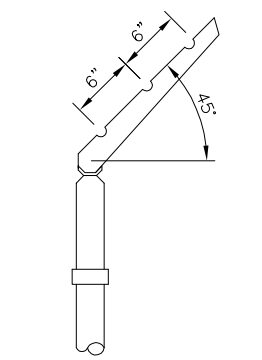
REVISIONS		
NUMBER	BY	DATE



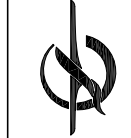
PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THEY SHALL BE PLACED AT 660 FT. INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320 FT. AND GREATER THAN 660 FT.



WHERE FENCE LINE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE. WHERE ANGLE IS LESS THAN 15° AND EXISTING CONDITIONS REQUIRE A TERMINAL POST, THEY SHALL BE PLACED AS DIRECTED BY THE ENGINEER.



- NOTES**
1. CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 1000 FT. EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
 2. FENCE UNDER POWER LINES SHALL BE GROUNDED BY 3 GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSINGS.
 3. THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF IMPERVIOUS EARTH STRUCTURES.
 4. THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.
 5. ALL PROPOSED CLASS E FENCE SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 664001-01.



**SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

**REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 FENCE DETAILS 1**

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	20 OF 27 SHEETS

CA013

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

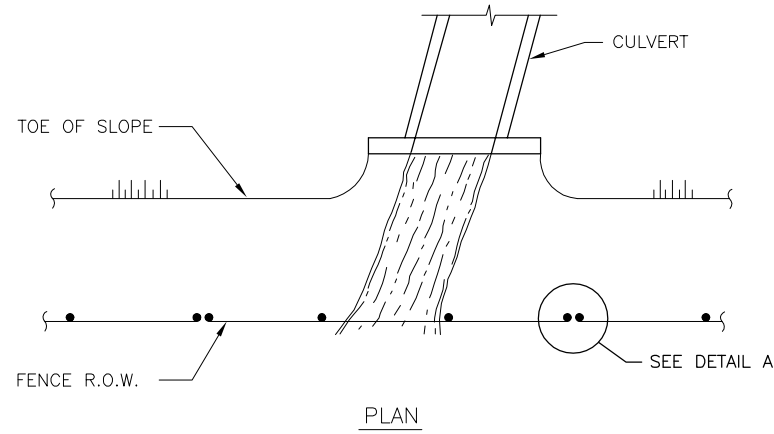
REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
FENCE DETAILS 2

© Copyright CMT, Inc.

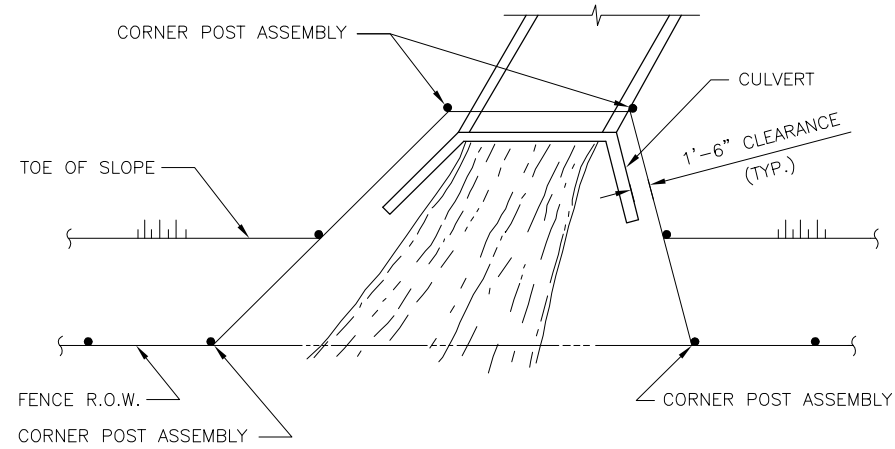
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



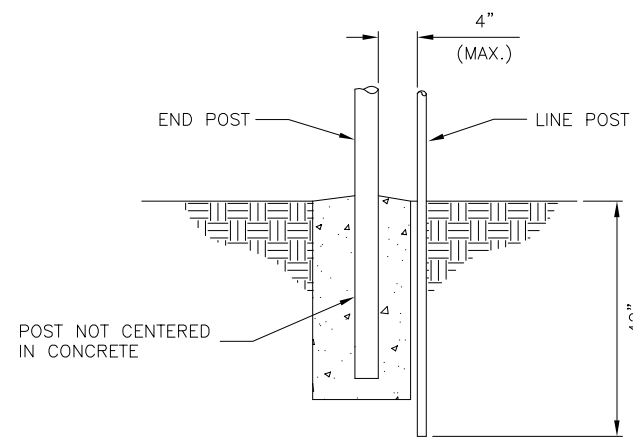
DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	21 OF 27 SHEETS



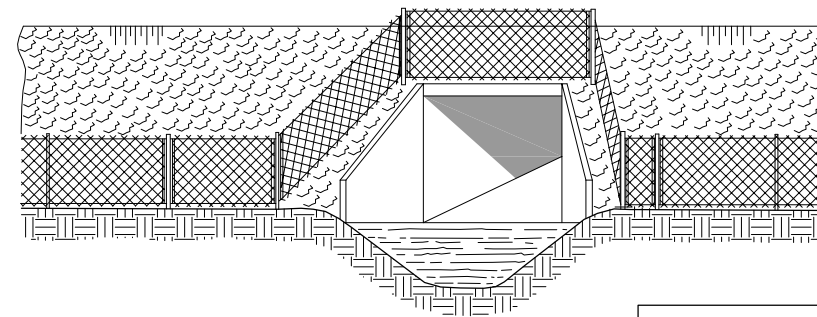
PLAN



PLAN



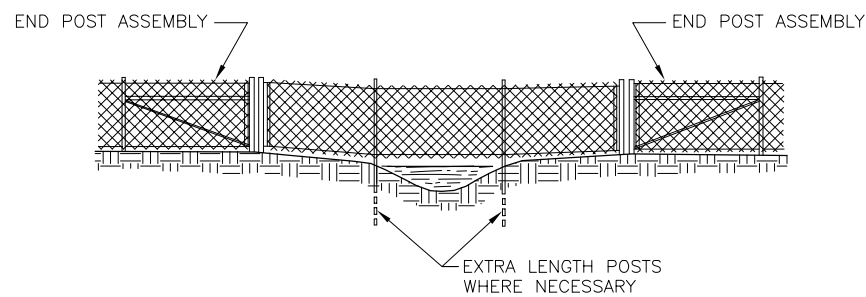
DETAIL A



ELEVATION

WHEN THE WIDTH OF THE CULVERT MAKES IT NECESSARY TO ANCHOR A POST TO THE TOP OF THE CULVERT, A CAST IRON SHOE OR OTHER DEVICE APPROVED BY THE ENGINEER SHALL BE USED.

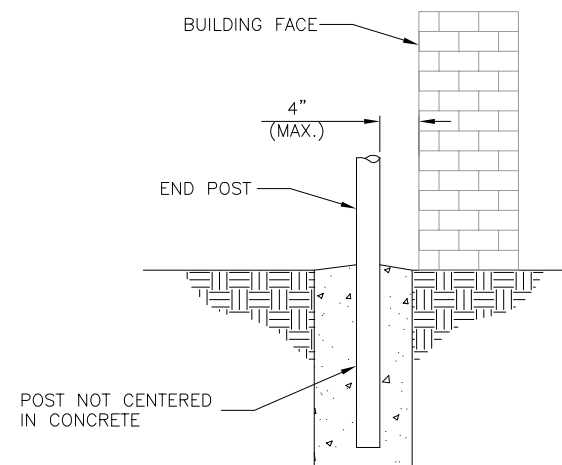
FENCE INSTALLATION AROUND HEADWALL DETAILS
 N.T.S.



ELEVATION

THE CHAIN LINK FABRIC SHALL BE REPLACED BY BARBED WIRE STRANDS AT 12" MAXIMUM CENTERS BETWEEN THE DOUBLE POSTS SHOWN ON DETAIL A WHEN SHOWN ON THE PLANS.

FENCE INSTALLATION OVER STREAM DETAILS
 N.T.S.



FENCE INSTALLATION AT BUILDING FACE
 N.T.S.

POST TYPE	FENCE POST SHAPE, SIZE AND WEIGHT					
	FENCE POST MATERIAL					
	ROUND STEEL PIPE SCH. 40		ROUND STEEL PIPE SS 40		ROLL FORMED C SECTION	
	DIAMETER	WEIGHT	DIAMETER	WEIGHT	SIZE	WEIGHT
CORNER, END, PULL	4.0" O.D.	9.11 LB/FT	4.0" O.D.	6.57 LB/LF	3.25" X 2.5"	4.50 LB/FT
LINE POST	2.875" O.D.	5.79 LB/FT	2.875" O.D.	4.64 LB/LF	3.25" X 2.5"	4.50 LB/FT
TOP RAIL	1.66" O.D.	2.77 LB/FT	1.66" O.D.	1.84 LF/LF	1.625" X 1.25"	1.35 LB/FT
GATE POST	6.25" O.D.	18.97 LB/FT	N/A	N/A	N/A	N/A

CA013

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34x22).



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2

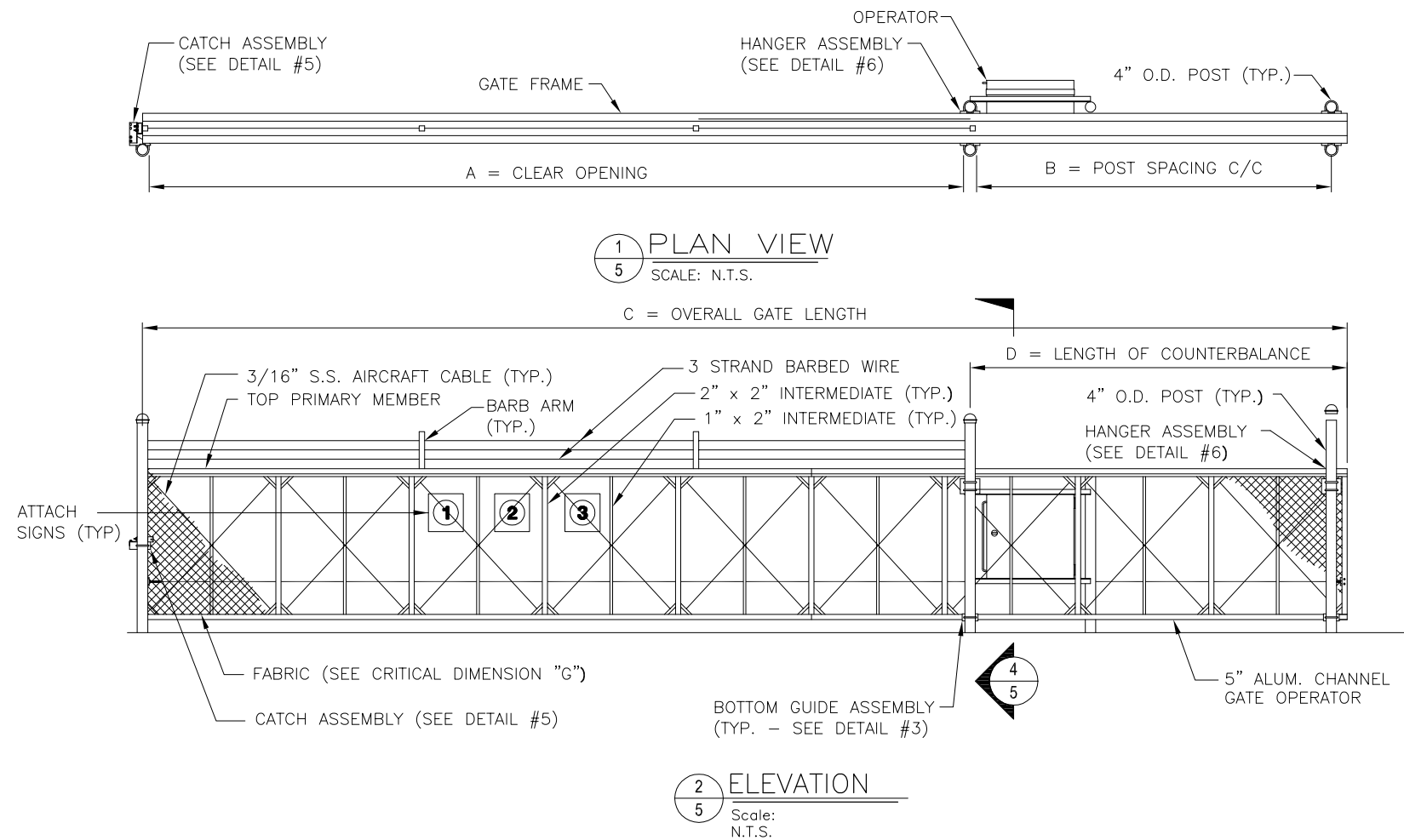
ELECTRICAL GATE DETAILS 1

© Copyright CMT, Inc.



CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	22 OF 27 SHEETS



CANTILEVER SLIDE GATE CRITICAL DIMENSION CHART		
		GATE #27
A	CLEAR OPENING	24'-0"
B	COUNTERBALANCE POST SPACING C/C	11'-1"
C	OVERALL GATE LENGTH	36'-0"
D	COUNTERBALANCE LENGTH	12'-0"
E	NOMINAL GATE HEIGHT	8'-0"
F	POST HEIGHT	9'-6"
G	FABRIC HEIGHT	7'-0"
H	NOMINAL WEIGHT	594 LBS

SIGNS					
CONTRACTOR SHALL FURNISH AND INSTALL SIGNS AS INDICATED IN TABLE BELOW					
SIGN	DIMENSIONS	BACKGROUND COLOR	LETTERING		TEXT
			COLOR	HEIGHT	
1	24' x 18"	WHITE	RED	1.5"	FAA REGULATIONS REQUIRE THAT AFTER ENTERING OR EXITING THROUGH THIS SECURITY GATE YOU ARE REQUIRED TO WAIT UNTIL THE GATE CLOSES BEFORE PROCEEDING. FINES OR PENALTIES WILL BE IMPOSED.
2	18" x 12"	WHITE	RED	4"	GATE #27
3	SEE WARNING SIGN DETAIL				

NOTES
 1) SIGNS #1 AND #3 SHALL BE INSTALLED ON BOTH SIDES OF GATE. SIGN #2 SHALL BE INSTALLED ON "LAND" SIDE OF GATE.

NOTES

- CANTILEVERED GATE SHALL BE SUFFICIENTLY RIGID TO WITHSTAND FLEXING OR BENDING DURING WINDY CONDITIONS. CONTRACTOR SHALL PROVIDE STIFFENERS, STRUCTURAL SHAPES IN EXCESS OF THE MINIMUM SPECIFIED DIMENSIONS OR ADDITIONAL ROLLERS AND POSTS SUFFICIENT TO PREVENT DISPLACEMENT OF THE GATE BY WIND OR BY UNAUTHORIZED PERSONNEL.
- CONTRACTOR SHALL PROVIDE AND INSTALL GATE AS COMPLETE WORKING UNIT. THE GATE WORK SHALL INCLUDE, BUT NOT BE LIMITED TO: GATE, OPERATOR, DIGITAL CODE CONTROL AND POWER CABLES, CONDUIT, TRENCHING, CIRCUIT BREAKERS, AND ALL CONNECTIONS, LABOR AND MATERIALS NECESSARY TO COMPLETE OPERATION.
- LOCATION OF THE GATE OPERATOR SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- PIPE BOLLARDS SHALL BE INSTALLED AT LOCATIONS SHOWN IN PLAN VIEW.
- THE FABRIC TYPE AND FINISH OF THE GATE, MATCH WITH PROPOSED FENCE OR AS DIRECTED BY THE ENGINEER.
- ALL SLIDING GATES SHALL HAVE ALL ROLLERS ENCLOSED IN STEEL OR PLASTIC SHROUDS TO PREVENT ACCIDENTAL INJURY.

UL 235 COMPLIANCE NOTES

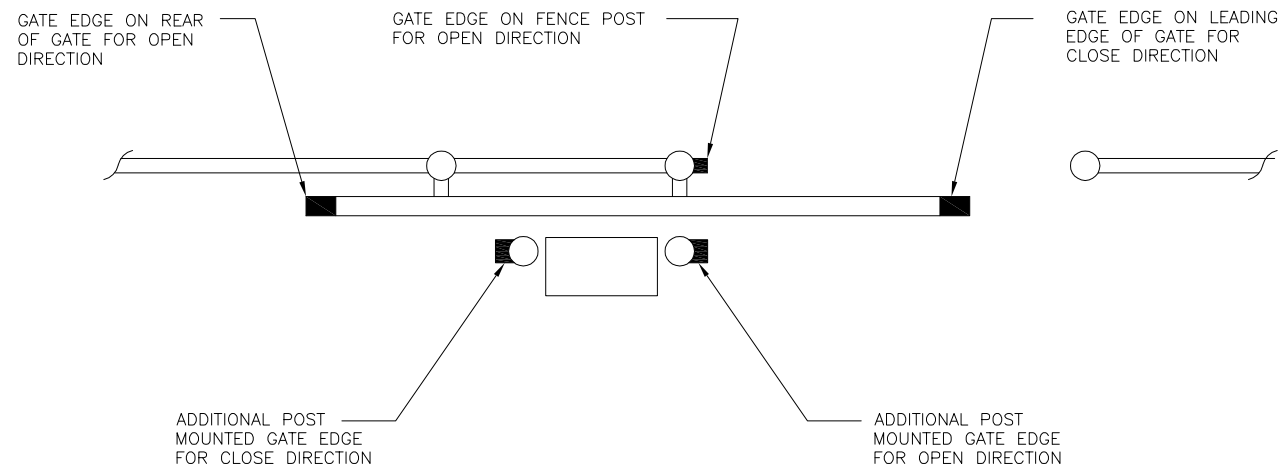
GATE INSTALLATION SHALL COMPLY WITH ALL REQUIREMENTS OF UL 235, INCLUDING, BUT NOT LIMITED TO:

- ALL OPENINGS OF THE SLIDE GATE ARE GUARDED OR SCREENED FROM THE BOTTOM OF THE GATE TO A MINIMUM OF 4 FEET ABOVE GROUND TO OPENINGS ANYWHERE IN THE GATE, AND IN THAT PORTION OF THE ADJACENT FENCE THAT THE GATE COVERS IN THE OPEN POSITION.
- ALL EXPOSED PINCH POINTS ARE ELIMINATED OR GUARDED AND GUARDING IS SUPPLIED FOR ALL EXPOSED ROLLERS.
- A WARNING SIGN (SEE DETAIL, THIS SHEET) MUST BE AFFIXED TO BOTH SIDES OF THE SLIDE GATE.
- FOR ADDITIONAL UL 235 REQUIREMENTS FOR THIS GATE INSTALLATION, SEE GATE OPERATOR DETAILS SHEET.



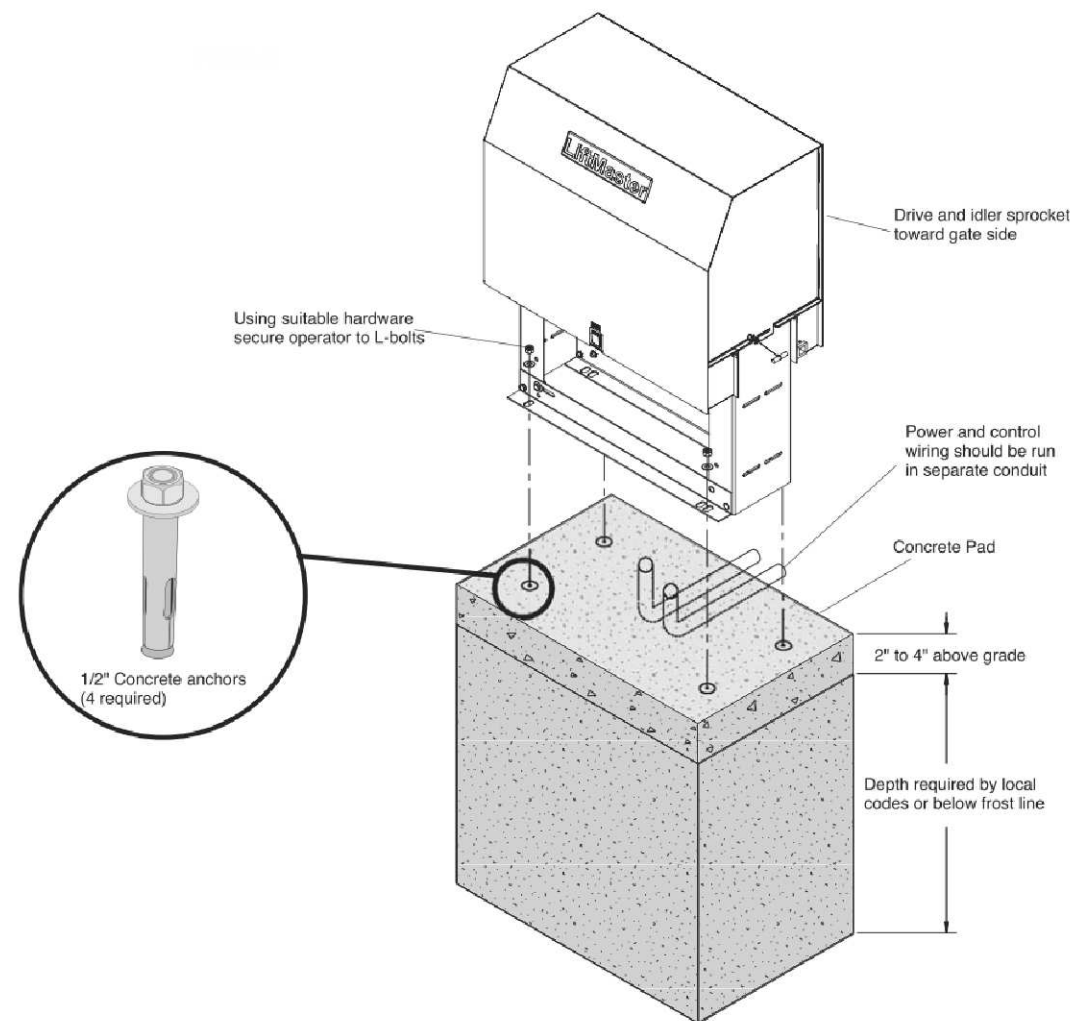
WARNING SIGN DETAIL

LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET IS GENERIC. EQUIPMENT LOCATIONS SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.

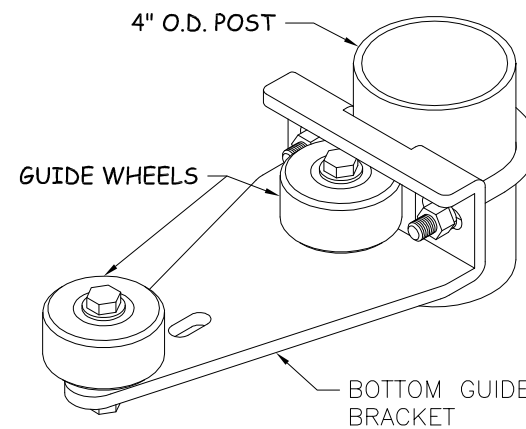


SECONDARY GATE OPERATOR ENTRAPMENT PROTECTION CONTACT SENSORS

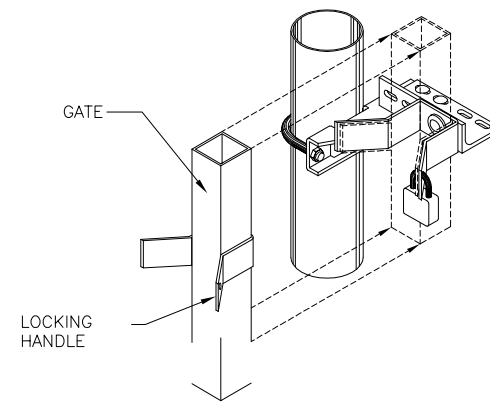
N.T.S.



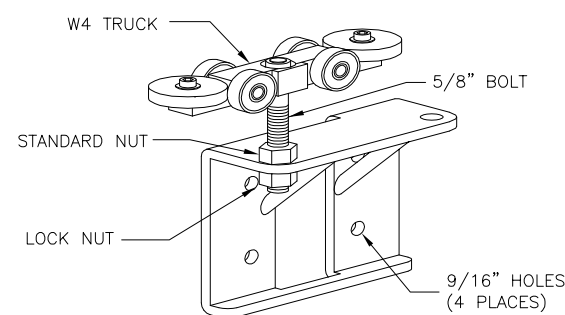
GATE OPERATOR FOUNDATION



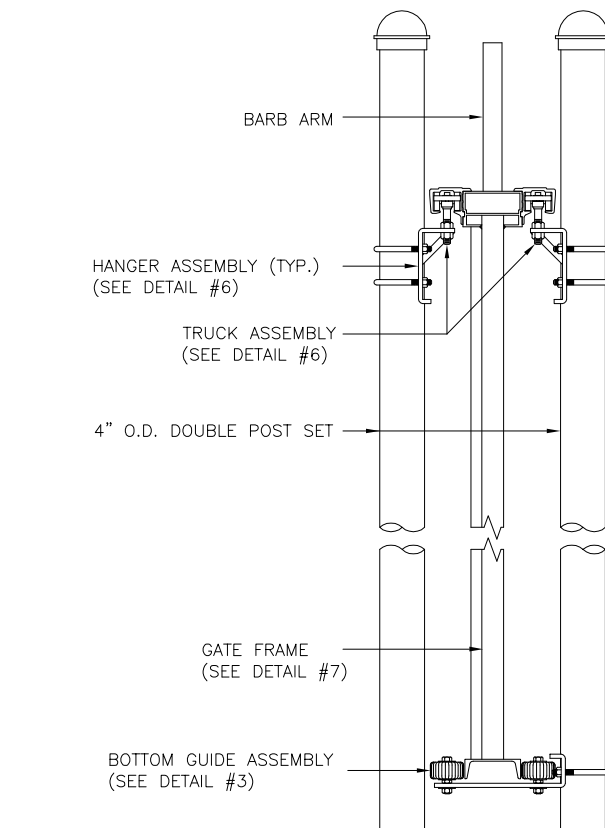
3 5 BOTTOM GUIDE
N.T.S.



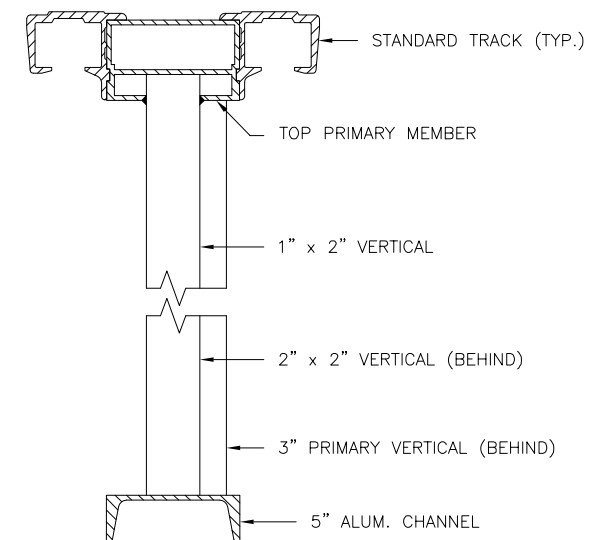
5 5 CATCH ASSEMBLY
N.T.S.



6 5 GATE HANGER ASSEMBLY
N.T.S.



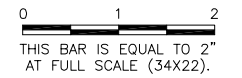
4 5 ASSEMBLY SECTION
N.T.S.



7 5 GATE FRAME SECTION
N.T.S.

CA013

REVISIONS		
NUMBER	BY	DATE



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 ELECTRICAL GATE DETAILS 2

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

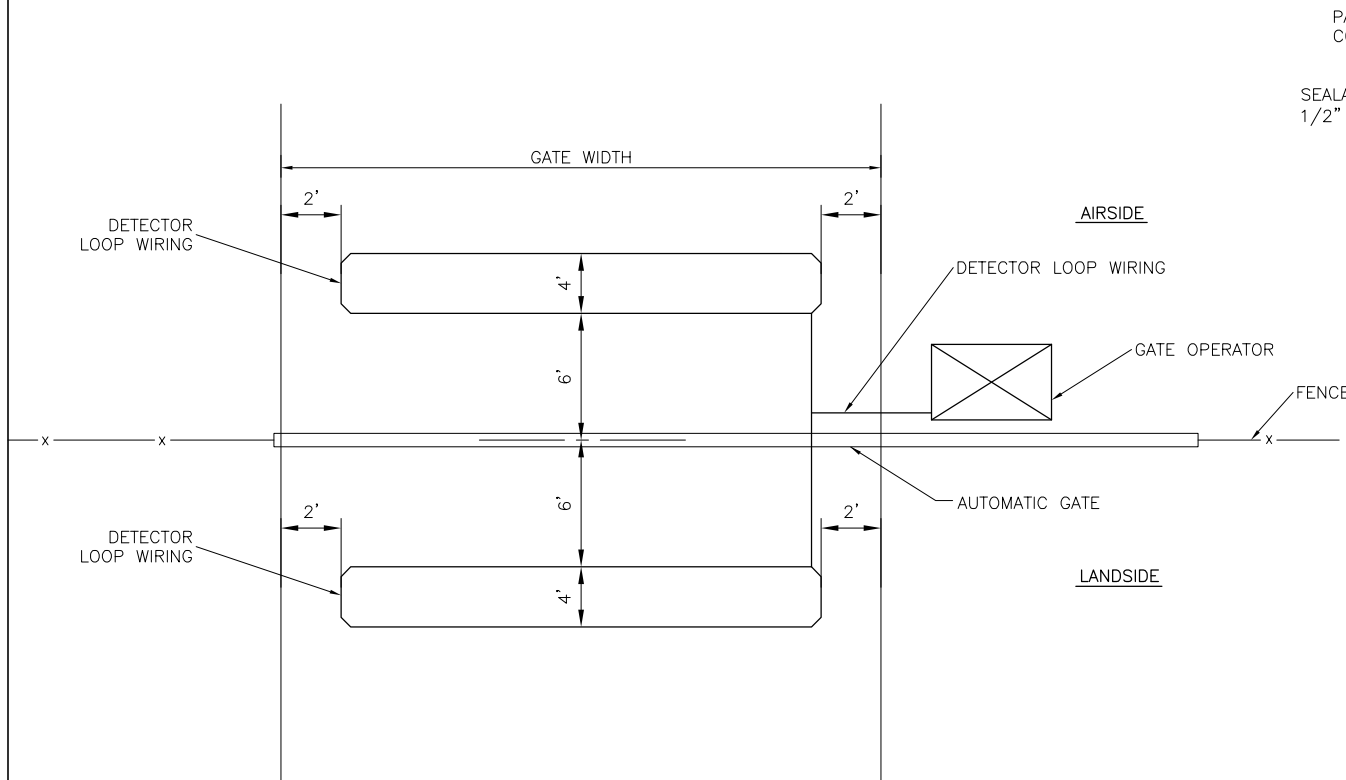


DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	23 OF 27 SHEETS

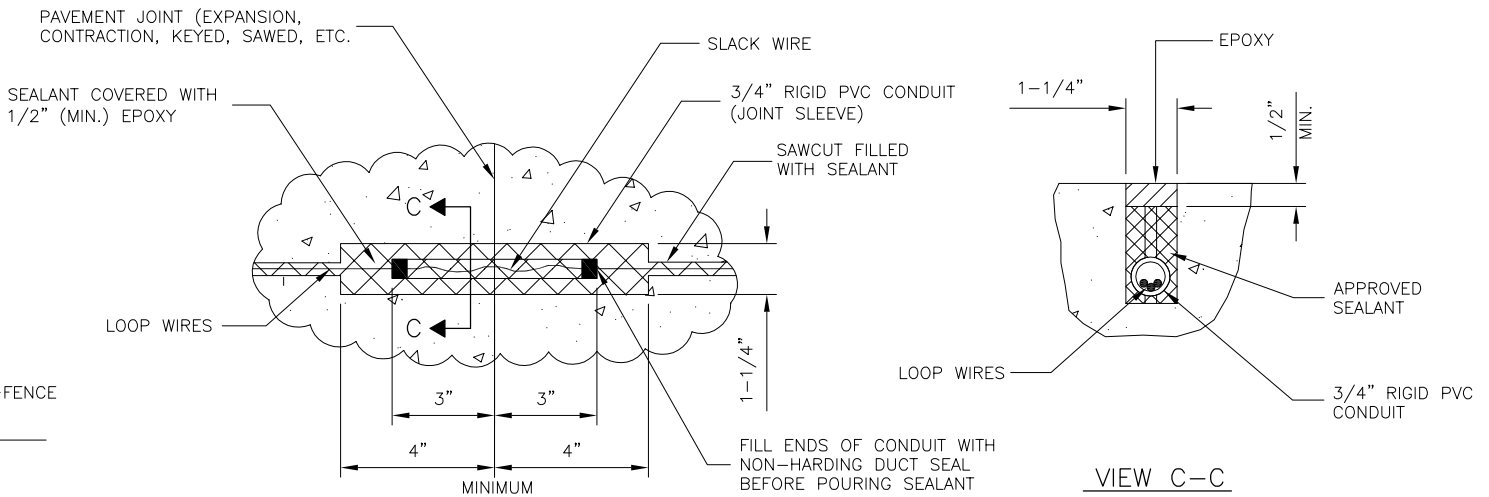
CA013

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



TYP. GATE PLAN
 N.T.S.



JOINT CROSSING DETAIL
 N.T.S.

NOTES

1. LOOP LEADS ARE LIMITED TO 100 FEET.
2. LOOP LEADS MUST HAVE FOUR (4) TWISTS PER FOOT.
3. LOOP AND LOOP LEADS MUST BE LOCATED, AT LEAST, 18" FROM ANY ELECTRICAL POWER SERVICE OR STEEL REINFORCEMENT.
4. LOOP LEADS MUST BE IN SEPARATE CONDUIT BETWEEN LOOP AND DETECTOR. THEY MUST NOT SHARE CONDUIT WITH OTHER WIRING OR LEADS FROM OTHER LOOPS.
5. WIRE SHALL BE #16 THHN SINGLE CONDUCTOR STRANDED WIRE.
6. ALL WIRE SHALL BE CONTINUOUS WITHOUT SPLICING.

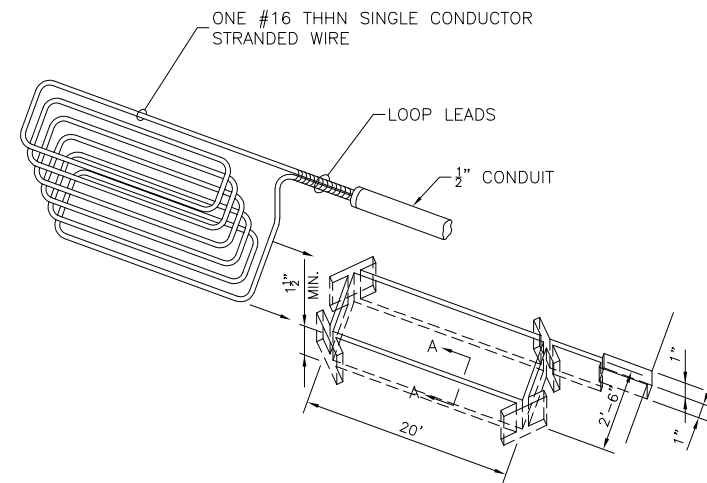
CAUTION:

- DO NOT SPLICE WIRE.
- DO NOT FRACTURE WIRE INSULATION. LOOPS SHORTED TO GROUND WILL CAUSE DETECTOR MALFUNCTION. WHEN PLACING WIRE IN THE SLOT. DO NOT USE SCREWDRIVER OR OTHER SHARP TOOLS.

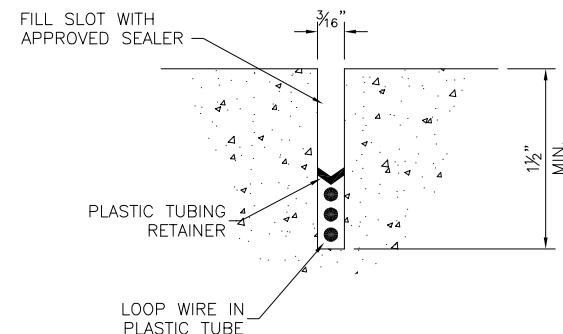
TYPICAL LAYOUT FOR LOOP:

- SAW SLOT 3/16" WIDE x 1-1/2" DEEP. MAKE RECTANGULAR SHAPE TO SPECIFIED LOOP DIMENSIONS PLUS SLOT FOR LEAD CONDUIT.
- GROUT WITH NO. 202 WEATHERBAN SEALANT (A PRODUCT OF 3M CO.) OR APPROVED EQUIVALENT ... (EXAMPLE: DE WITTS NO. 99 BLACK MASTIC CAULK).

NOTE:
 LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET ARE GENERIC. EQUIPMENT LOCATION SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.



PERSPECTIVE VIEW OF
 DETECTOR LOOP SAW SLOT



SECTION A-A

DETECTOR LOOP DETAILS
 N.T.S.

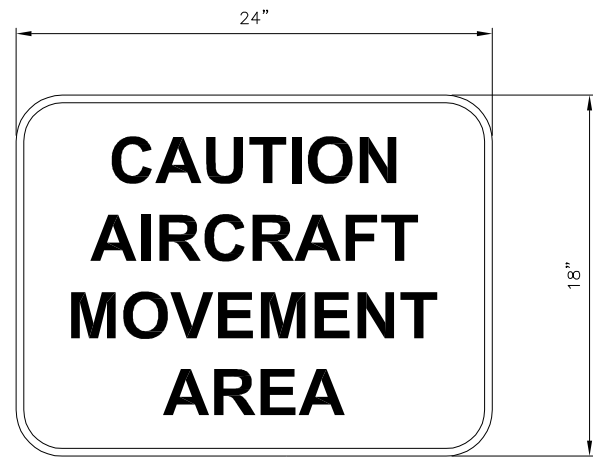
SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2

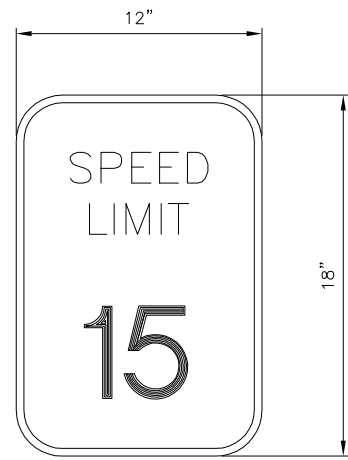
ELECTRICAL GATE DETAILS 3

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

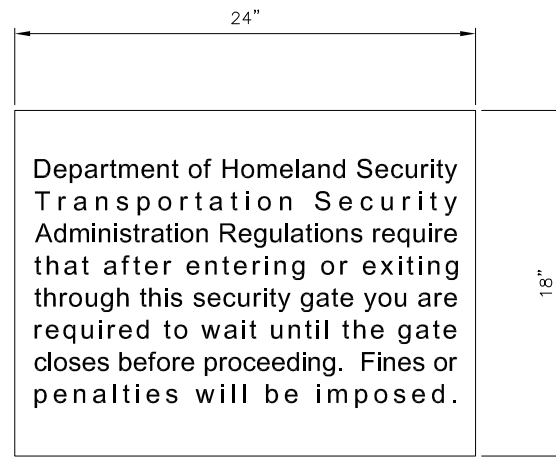
DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	24 OF 27 SHEETS



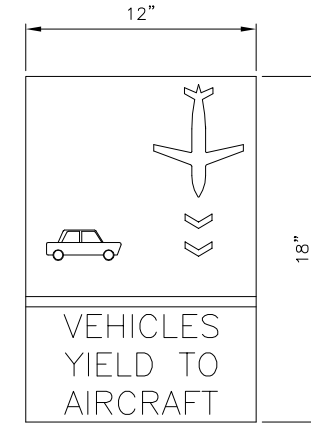
CAUTION SIGN



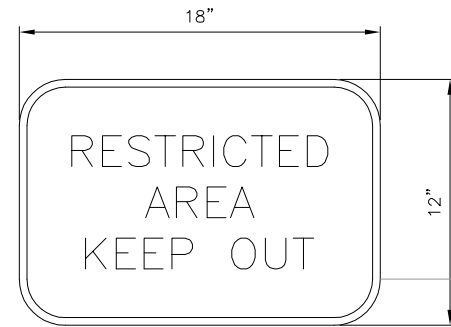
SPEED LIMIT SIGN



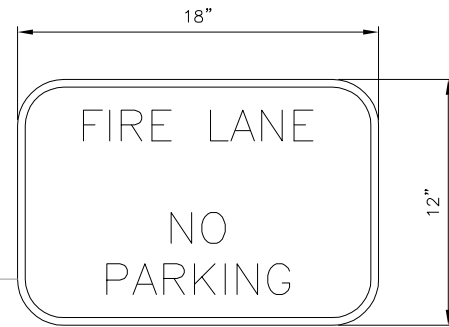
TSA SIGN



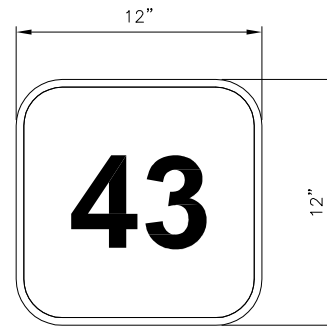
YIELD SIGN



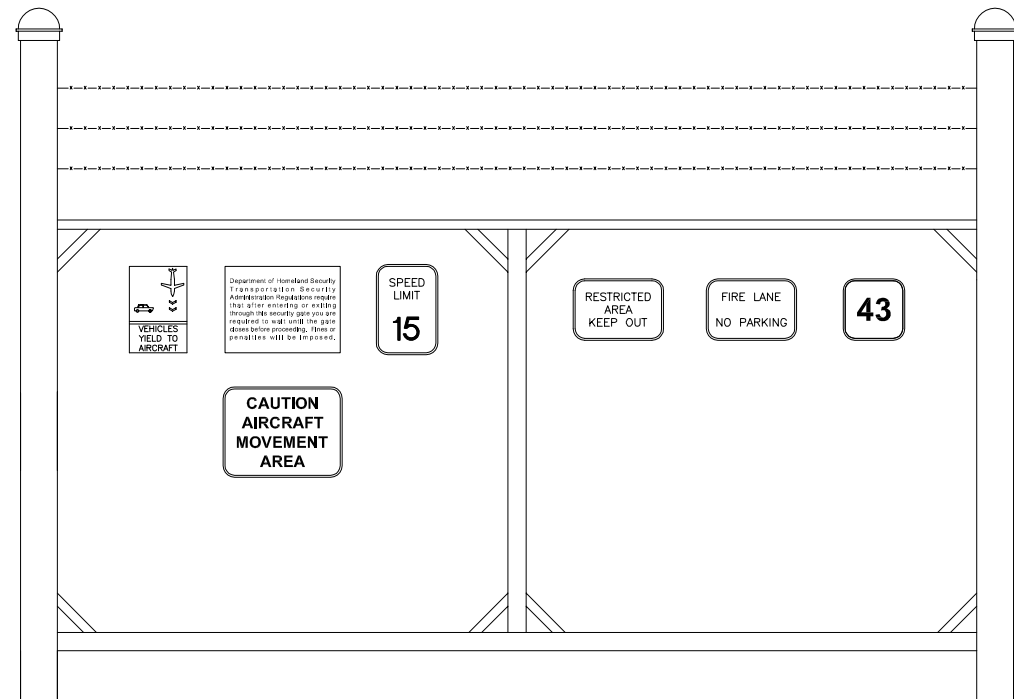
KEEP OUT SIGN



FIRE LANE SIGN



GATE NUMBER SIGN



ELECTRIC VEHICLE GATE

NOTES:

1. SEE PROPOSED GATE PLAN FOR TYPES OF SIGNS REQUIRED AT EACH GATE.
2. GATE NUMBER AND RESTRICTED AREA SIGNS SHALL BE INSTALLED ON ALL NEW GATES. COST TO PROVIDE AND INSTALL SIGNS SHALL BE INCIDENTAL TO THE COST OF THE ASSOCIATED GATE. COORDINATE WITH AIRPORT ON NUMBER.
3. CONTRACTOR SHALL PROVIDE SHOP DRAWINGS OF EACH SIGN. SHOP DRAWINGS SHALL MEET THE APPROVAL OF THE AIRPORT AND ENGINEER PRIOR TO MANUFACTURE AND INSTALLATION.
4. LETTERING COLOR SHALL BE RED ON WHITE BACKGROUND.
5. EVERY 100' OF FENCE SHALL REQUIRE ONE RESTRICTED AREA SIGN. EVERY STRAIGHT SECTION OF FENCE SHALL REQUIRE MINIMUM ONE RESTRICTED AREA SIGN.
6. SEE ELECTRIC GATE DETAIL 1 FOR ADDITIONAL GATE RELATED SIGNS.
7. COSTS TO PROVIDE AND INSTALL SIGNS SHALL BE INCIDENTAL TO CONTRACT PAY ITEMS.
8. ALL SIGNS SHALL BE FASTENED TO THE INSIDE OF THE FENCE.

CA013

REVISIONS

NUMBER	BY	DATE

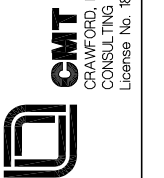
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2
 GATE SIGNS DETAILS

© Copyright CMT, Inc.



DESIGN BY: RLV/TJH

DRAWN BY: CMT

CHECKED BY: RLV

APPROVED BY: RLV

DATE: MAY 4, 2012

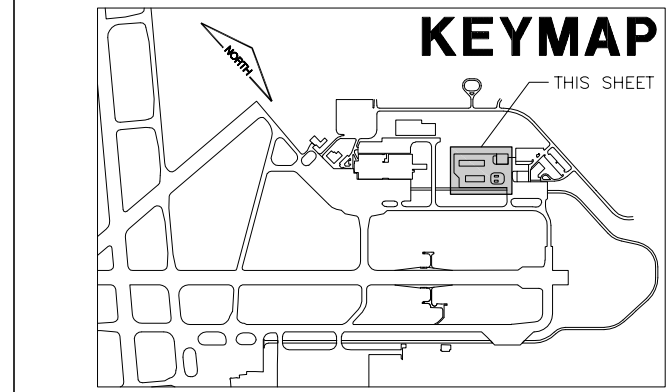
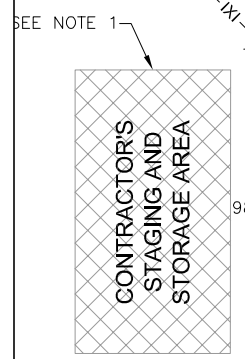
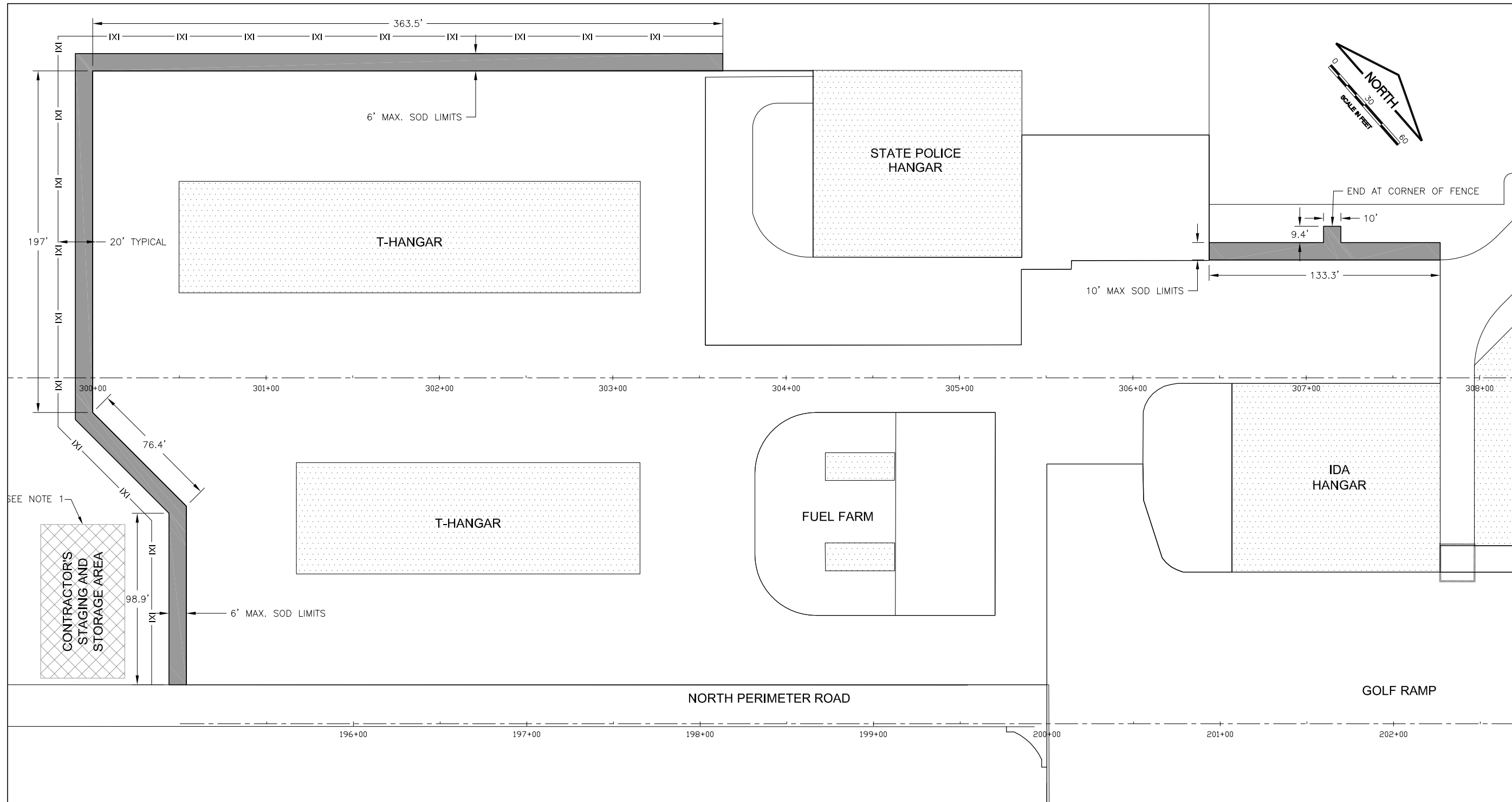
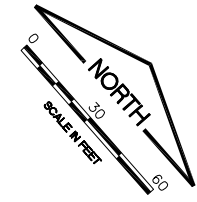
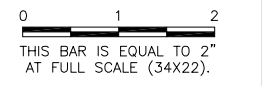
JOB No: 09035-05

IL PROJ. NO. SPI-4163
 AIP PROJ. NO. 3-17-0096-XX

SHEET 25 OF 27 SHEETS

CA013


REVISIONS		
NUMBER	BY	DATE



- NOTES**
1. CONTRACTOR'S STAGING AND STORAGE AREA SHALL BE SEEDED AND MULCHED AT THE CONTRACTOR'S EXPENSE TO RESTORE TURF TO ITS ORIGINAL CONDITION.
 2. ANY EXISTING TURF DISTURBED BY THE CONTRACTOR'S OPERATIONS OUTSIDE OF THE GRADING LIMITS SHALL BE SEEDED AND MULCHED IN ACCORDANCE WITH ITEMS 901 & 908 AT THE EXPENSE OF THE CONTRACTOR.

LEGEND

	SHOULDER ADJUSTMENT AND SOD LIMITS
	NEW SILT FENCE



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2

TURFING PLAN

© Copyright CMT, Inc.

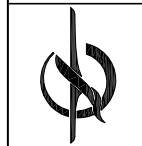
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	26 OF 27 SHEETS

CA013

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

REHABILITATE GOLF RAMP & HANGAR TAXIWAYS - PHASE 2

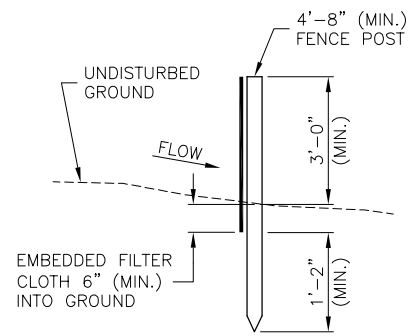
EROSION CONTROL DETAILS

© Copyright CMT, Inc.

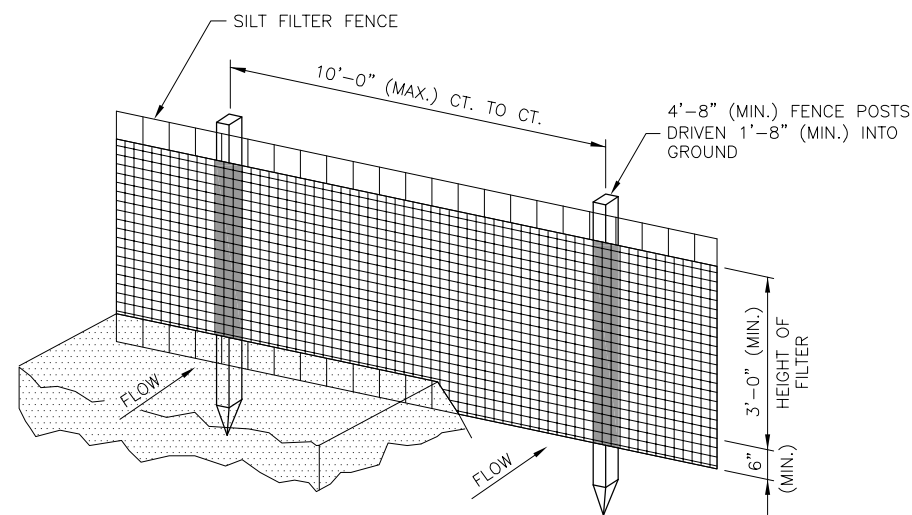
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY:	RLV/TJH
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MAY 4, 2012
JOB No:	09035-05
IL PROJ. NO.	SPI-4163
AIP PROJ. NO.	3-17-0096-XX
SHEET	27 OF 27 SHEETS



SECTION



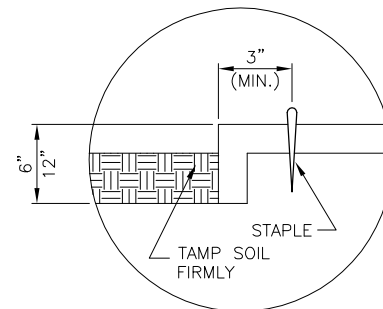
PERSPECTIVE VIEW

EROSION CONTROL FABRIC FENCE DETAILS

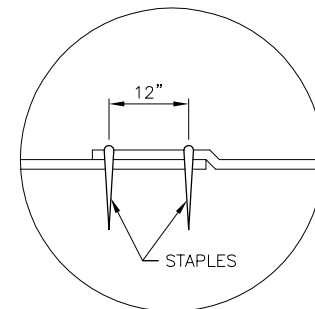
N.T.S.

EROSION CONTROL FABRIC FENCE NOTES

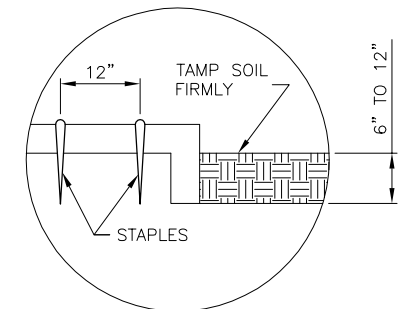
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



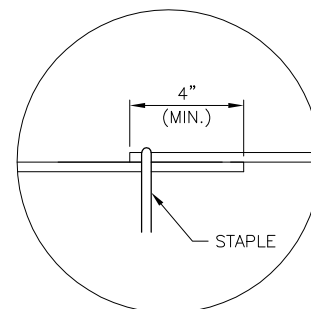
DETAIL 1 - TERMINAL FOLD



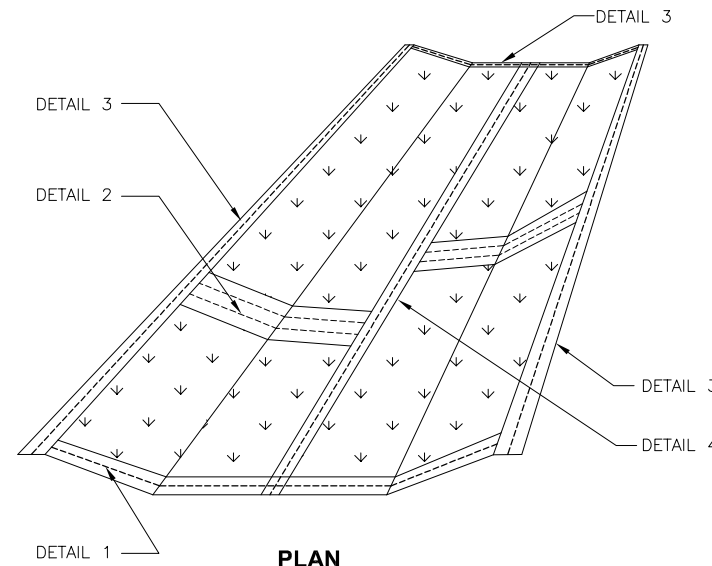
DETAIL 2 - JUNCTION SLOT



DETAIL 3 - ANCHOR SLOT



DETAIL 4 - LAP JOINT



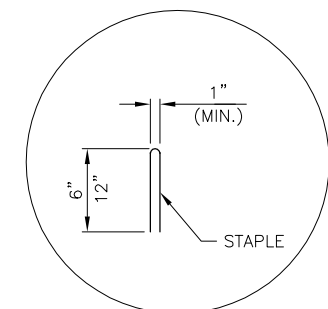
PLAN

EXCELSIOR BLANKET DETAILS

N.T.S.

EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINAL ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



DETAIL 5 - STAPLE DETAIL