

CONSTRUCTION PLANS FOR ABRAHAM LINCOLN CAPITAL AIRPORT

SPRINGFIELD AIRPORT AUTHORITY SPRINGFIELD, ILLINOIS

**IL PROJECT NO. SPI-4156
 AIP PROJECT NO. 3-17-0096-XX**

EXTEND TAXIWAY Y, CONSTRUCT ARFF RESPONSE ROAD, REALIGN PERIMETER ROAD & ASSOCIATED IMPROVEMENTS

APRIL 27, 2012

811 Know what's below. Call before you dig.
 J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN INFORMATION

GEOMETRIC CRITERIA

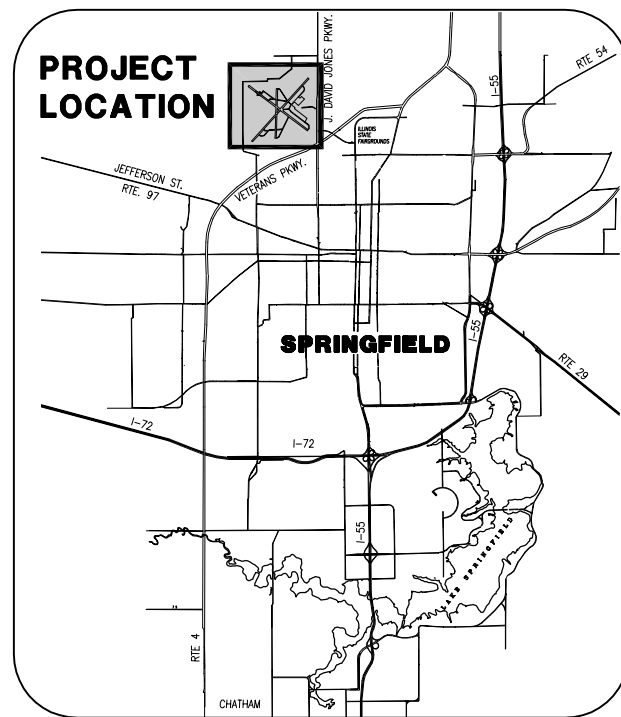
AIRFIELD
 AIRPLANE DESIGN GROUP II

ARFF ROADWAY
 LOW SPEED URBAN STREET
 45 MPH DESIGN SPEED

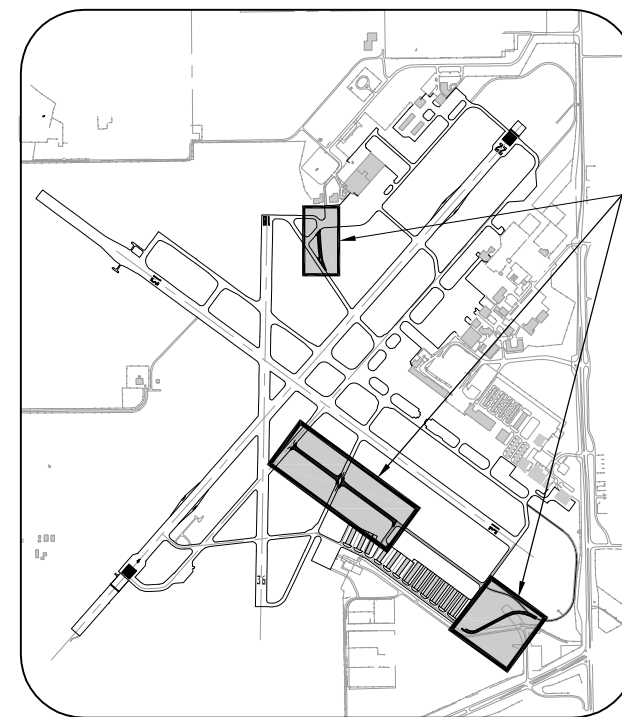
PERIMETER ROADWAY
 LOW SPEED URBAN STREET
 30 MPH DESIGN SPEED

CALL J.U.L.I.E. BEFORE EXCAVATING
 1-800-892-0123

TOWNSHIP: 16 NORTH
 RANGE: 5 WEST OF THE 4TH P.M.
 SECTION: 8, 16, & 17
 COUNTY: SANGAMON
 CIVIL TOWNSHIP: CAPITAL



LOCATION MAP



SITE PLAN

PROJECT SITE






SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

APPROVED *Mark Stammen*
 EXECUTIVE DIRECTOR OF AVIATION

DATE 5/7/2012

 **GMT**
 CRAWFORD MURPHY & TILLY, INC.
 CONSULTING ENGINEERS

■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY *Mark Stammen*
 DATE 5/7/12

CMT JOB NUMBER: 110350400

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 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
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EXTEND TAXIWAY Y
 INDEX TO SHEETS

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

| SUMMARY OF QUANTITIES | | | |
|-----------------------|---------------------------------|------|----------|
| ITEM NO. | ITEM DESCRIPTION | UNIT | QUANTITY |
| AR108158 | 1/C #8 5 KV UG CABLE IN UD | LF | 18260 |
| AR108706 | 1/C #6 COUNTER POISE | LF | 9405 |
| AR109210 | VAULT MODIFICATIONS | LS | 1 |
| AR109321 | 10 KW REGULATOR, STYLE 1 | EA | 1 |
| AR110014 | 4" DIRECTIONAL BORE | LF | 1235 |
| AR110504 | 4-WAY CONCRETE ENCASED DUCT | LF | 285 |
| AR110550 | SPLIT DUCT | LF | 367 |
| AR110967 | RELOCATE ELECTRICAL MANHOLE | EA | 1 |
| AR125415 | MITL-BASE MOUNTED | EA | 18 |
| AR125416 | MITL-BASE MOUNTED-LED | EA | 119 |
| AR125420 | TAXIWAY LIGHT IN PAVEMENT | EA | 1 |
| AR125441 | TAXI GUIDANCE SIGN, 1 CHARACTER | EA | 2 |
| AR125442 | TAXI GUIDANCE SIGN, 2 CHARACTER | EA | 1 |
| AR125443 | TAXI GUIDANCE SIGN, 3 CHARACTER | EA | 3 |
| AR125444 | TAXI GUIDANCE SIGN, 4 CHARACTER | EA | 7 |
| AR125446 | TAXI GUIDANCE SIGN, 6 CHARACTER | EA | 4 |
| AR125901 | REMOVE STAKE MOUNTED LIGHT | EA | 62 |
| AR125902 | REMOVE BASE MOUNTED LIGHT | EA | 25 |
| AR125904 | REMOVE TAXI GUIDANCE SIGN | EA | 5 |
| AR125906 | REMOVE SPLICE CAN | EA | 3 |
| AR150510 | ENGINEER'S FIELD OFFICE | LS | 1 |
| AR151450 | CLEARING AND GRUBBING | AC | 1 |
| AR152410 | UNCLASSIFIED EXCAVATION | CY | 9816 |
| AR155540 | BY-PRODUCT LIME | TON | 180 |
| AR155608 | SOIL PROCESSING - 8" | SY | 11150 |
| AR156510 | SILT FENCE | LF | 2550 |
| AR156513 | SEPARATION FABRIC | SY | 215 |
| AR156520 | INLET PROTECTION | EA | 9 |
| AR162900 | REMOVE CLASS E FENCE | LF | 635 |
| AR208540 | OVERSIZED AGGREGATE | TON | 85 |
| AR209600 | GEOTEXTILE FABRIC | SY | 130 |
| AR209604 | CRUSHED AGG. BASE COURSE - 4" | SY | 1836 |
| AR209606 | CRUSHED AGG. BASE COURSE - 6" | SY | 8180 |
| AR209612 | CRUSHED AGG. BASE COURSE - 12" | SY | 5069 |
| AR401610 | BITUMINOUS SURFACE COURSE | TON | 1369 |

| | | | |
|----------|---------------------------------|-----|-------|
| AR401655 | BUTT JOINT CONSTRUCTION | SY | 587 |
| AR401900 | REMOVE BITUMINOUS PAVEMENT | SY | 3541 |
| AR403610 | BITUMINOUS BASE COURSE | TON | 2825 |
| AR403630 | BITUMINOUS BASE TEST SECTION | EA | 1 |
| AR501510 | 10" PCC PAVEMENT | SY | 1836 |
| AR501530 | PCC TEST BATCH | EA | 1 |
| AR501900 | REMOVE PCC PAVEMENT | SY | 118 |
| AR602510 | BITUMINOUS PRIME COAT | GAL | 5373 |
| AR603510 | BITUMINOUS TACK COAT | GAL | 2520 |
| AR620510 | PAVEMENT MARKING | SF | 57000 |
| AR620900 | PAVEMENT MARKING REMOVAL | SF | 1425 |
| AR701518 | 18" RCP, CLASS IV | LF | 579 |
| AR701530 | 30" RCP, CLASS IV | LF | 582 |
| AR701542 | 42" RCP, CLASS IV | LF | 1451 |
| AR701900 | REMOVE PIPE | LF | 2033 |
| AR705524 | 4" PERFORATED UNDERDRAIN W/SOCK | LF | 1862 |
| AR705544 | 4" NON PERFORATED UNDERDRAIN | LF | 75 |
| AR705635 | UNDERDRAIN COLLECTION STRUCTURE | EA | 1 |
| AR705640 | UNDERDRAIN CLEANOUT | EA | 5 |
| AR751410 | INLET | EA | 4 |
| AR751415 | INLET - SPECIAL | EA | 1 |
| AR751426 | INLET - 6'x6' | EA | 4 |
| AR751427 | INLET - 7'x7' | EA | 1 |
| AR751900 | REMOVE INLET | EA | 5 |
| AR754610 | PAVED DITCH | LF | 575 |
| AR800234 | DEMOLISH STRUCTURE | LS | 1 |
| AR800241 | CLASS E FENCE - 10' W/2' BURIED | LF | 643 |
| AR800250 | 2 - 1/C #8 5KV UG CABLE IN UD | LF | 3310 |
| AR800293 | DUCT MARKER - IN PAVEMENT | EA | 30 |
| AR800308 | EXPLORATORY EXCAVATION | EA | 5 |
| AR800317 | PAVEMENT SENSOR | EA | 2 |
| AR800397 | TEMPORARY FENCE | LS | 1 |
| AR801233 | SEMI-FLUSH MITL IN TURF | EA | 1 |
| AR801236 | LIGHTING CONTROL MODIFICATIONS | LS | 1 |
| AR901510 | SEEDING | AC | 13 |
| AR904510 | SODDING | SY | 1855 |
| AR908510 | MULCHING | AC | 12 |
| AR908520 | EXCELSIOR BLANKET | SY | 6410 |
| AR910915 | REMOVE ROADWAY SIGN | EA | 1 |
| AR910200 | ROADWAY SIGN | EA | 3 |



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
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EXTEND TAXIWAY Y

SUMMARY OF QUANTITIES

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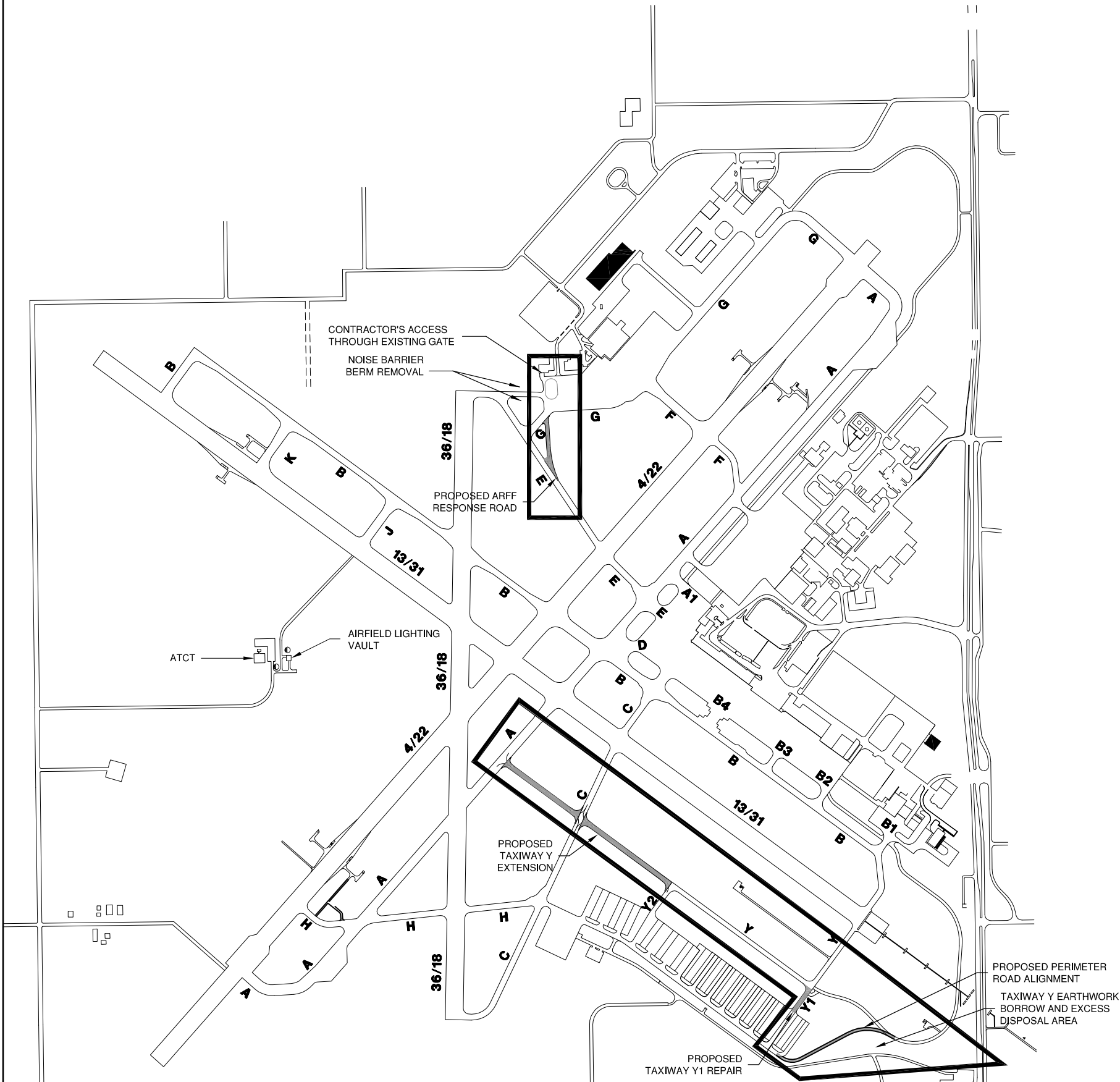
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 APPROVED BY: **RLV**
 DATE: APRIL 27, 2012
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SPI-4156
 3-17-0096-XX

GROUND CONTROL FREQUENCY - 121.90
 AIR CONTROL FREQUENCY - 121.30
 MAXIMUM HEIGHT OF EQUIPMENT - 25'

LEGEND

-  CONTRACTOR'S ACCESS
-  WORK AREA LIMITS



GENERAL NOTES

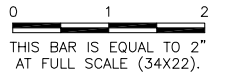
1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED SAA/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
4. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
5. WASTE ASPHALT AND CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 95' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS UNLESS OTHERWISE NOTED.
7. ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
9. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
10. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
11. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
 - C. THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER, S.A.A. MAINTENANCE SUPERVISOR, AND S.A.A. SECURITY CHIEF. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT.
 - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE APPLICABLE CITY, COUNTY, TOWNSHIP, OR I.D.O.T. JURISDICTION.
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED BY THE AIRPORT.
 - G. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE UTILIZED OR CROSSED BY CONSTRUCTION VEHICLES.
12. EXCESS EARTHWORK DISPOSAL SHALL BE AS FOLLOWS:
 - A. EXCESS EARTHWORK SHALL BE DISPOSED AT THE LOCATION SHOWN, AS DIRECTED BY THE AIRPORT.
 - B. EXCESS EARTHWORK SHALL BE NEATLY STOCKPILED IN MULTIPLE LAYERS TO MINIMIZE AREA USED.
13. EARTHWORK BORROW SITES SHALL BE AS FOLLOWS:
 - A. THE EARTHWORK BORROW SITES SHALL BE AT THE LOCATIONS SHOWN AS DIRECTED BY THE AIRPORT.
 - B. THE EARTHWORK BORROW SITES SHALL BE NEATLY EXCAVATED WITH SLOPES NO STEEPER THAN THE EXISTING AND RE-GRADED TO DRAIN INTO EXISTING DRAINAGE FEATURES.
 - C. THE EARTHWORK BORROW SITES SHALL BE SEEDED AND MULCHED IN ACCORDANCE WITH THE SPECIFICATIONS. COST FOR THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
14. WORK SHALL BE DONE IN ACCORDANCE WITH FAA ADVISORY CIRCULAR AC 150/5370-2, LATEST EDITION, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.

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 PLOT DATE: 5/10/2012 8:47 AM
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EXTEND TAXIWAY Y

AIRPORT SITE PLAN

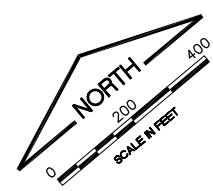
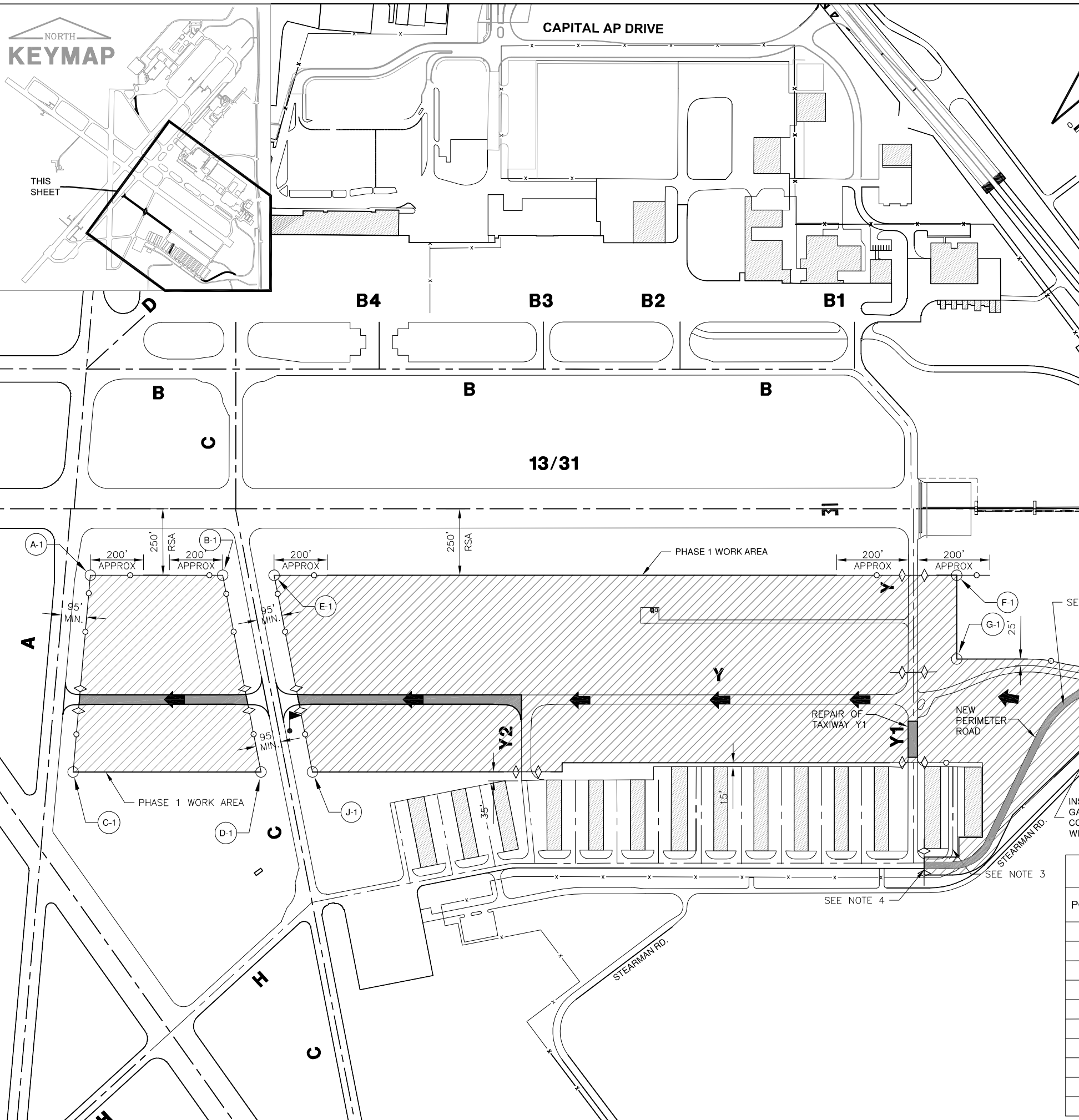
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LEGEND

- WORK AREA
- NEW PAVEMENT
- BARRICADES LINE - SEE DETAIL
- PROPOSED CONSTRUCTION SET BACK LINE - SEE DETAIL
- FLAGMAN - IN CONTACT WITH AIR TRAFFIC CONTROL TOWER (ATCT)
- CONTRACTOR'S ACCESS

PHASE 1 NOTES:

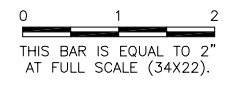
1. ALL CONSTRUCTION BARRICADES AND SETBACK LINES SHALL BE INSTALLED AS SHOWN PRIOR TO STARTING WORK IN THIS PHASE.
2. NO WORK SHALL BE COMPLETED WITHIN THE TAXIWAY A OR TAXIWAY C SAFETY AREA DURING THIS PHASE AND BOTH TAXIWAYS WILL REMAIN OPEN AT ALL TIMES.
3. WORK IN THIS AREA SHALL BE COORDINATED WITH THE TENANTS AND THE AIRPORT.
4. BARRICADES SHALL BE PLACED IN THIS AREA AS DIRECTED BY THE AIRPORT.
5. THE CONTRACTOR SHALL TAKE CARE TO ROUTE HIS CONSTRUCTION TRAFFIC AND HAUL TRUCKS IN SUCH A MANNER AS TO AVOID DAMAGE TO THE SUBGRADE OF THE NEW ALIGNMENT OF THE PERIMETER ROAD PRIOR TO ITS CONSTRUCTION AND SHALL TAKE CARE NOT TO DAMAGE THE NEW SURFACE OF THE PERIMETER ROAD AFTER ITS CONSTRUCTION. ANY DAMAGE TO EITHER SHALL BE REPAIRED TO THE SATISFACTION OF THE AIRPORT AND THE ENGINEER.
6. THE CONTRACTOR SHALL INSTALL AT THE LOCATION DESIGNATED AND PER THE AIRPORT'S DIRECTION, A TEMPORARY ACCESS POINT THROUGH THE PERIMETER FENCE. THIS ACCESS POINT SHALL CUT OPEN THE EXISTING PERIMETER FENCE AND INVOLVE THE INSTALLATION A TEMPORARY, OPERATIONAL GATE THAT IS CAPABLE OF BEING LOCKED AND SECURED.
7. ALL TEMPORARY GATES AND FENCES SHALL AT ALL TIMES KEEP THE AIRPORT SECURE. UNATTENDED OPENINGS IN THE FENCE WILL NOT BE ALLOWED UNDER ANY CIRCUMSTANCES.
8. THE CONTRACTOR SHALL RETURN ALL FENCES AND ACCESS POINTS TO THEIR ORIGINAL CONDITION AND TO THE SATISFACTION OF THE AIRPORT AND THE ENGINEER AT THE COMPLETION OF THE PROJECT. THIS SHALL BE DONE AT NO ADDITIONAL COST TO THE CONTRACT.

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 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/8/2012 9:02 PM
 SPI-BASE CURRENT GEOMETRY
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 KEYMAP

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SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

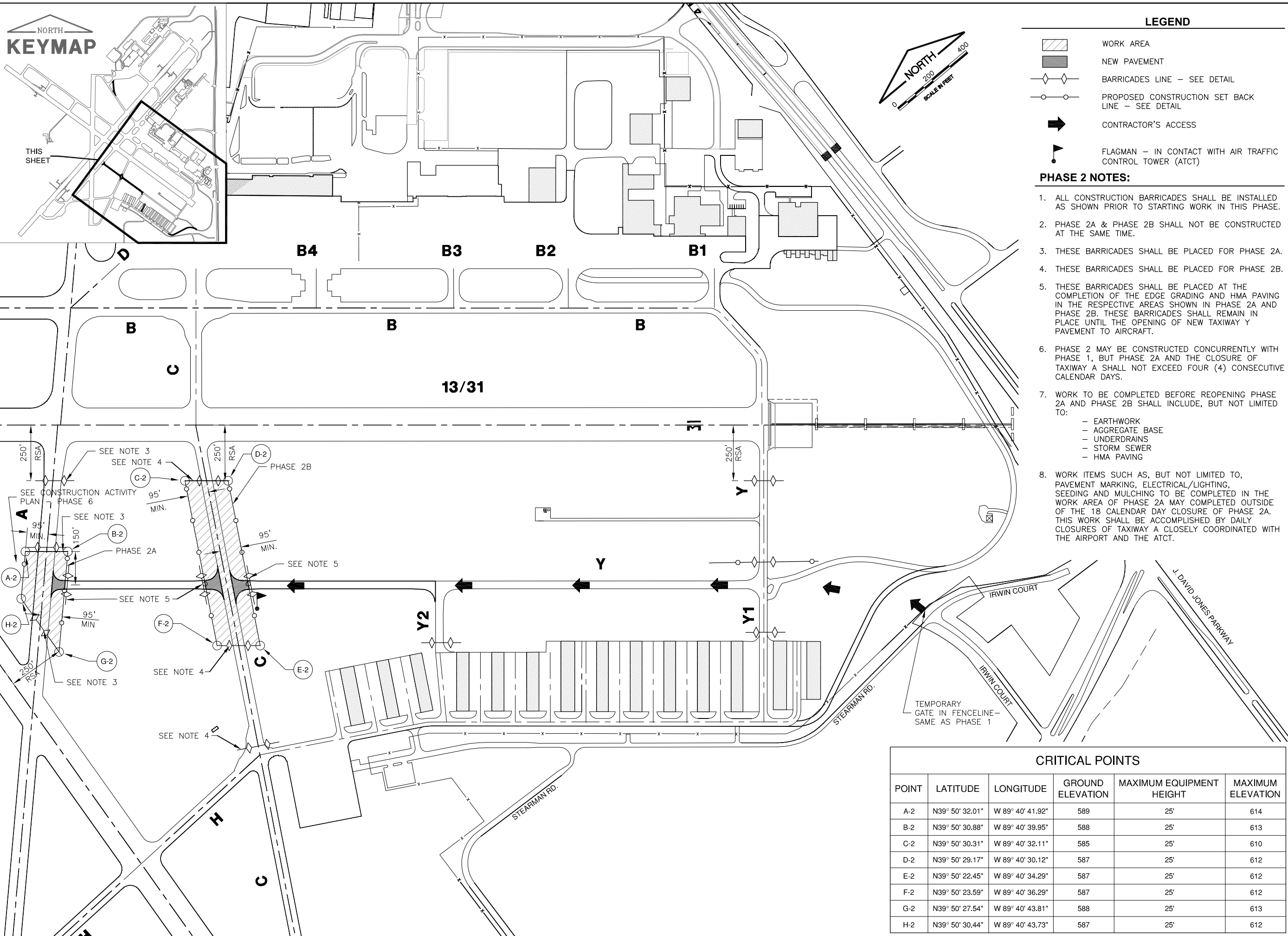
**EXTEND TAXIWAY Y
 CONSTRUCTION ACTIVITY PLAN - PHASE 1**

CRITICAL POINTS

| POINT | LATITUDE | LONGITUDE | GROUND ELEVATION | MAXIMUM EQUIPMENT HEIGHT | MAXIMUM ELEVATION |
|-------|-----------------|------------------|------------------|--------------------------|-------------------|
| A-1 | N39° 50' 33.24" | W 89° 40' 37.23" | 587 | 25' | 614 |
| B-1 | N39° 50' 30.31" | W 89° 40' 32.11" | 585 | 25' | 610 |
| C-1 | N39° 50' 27.75" | W 89° 40' 43.56" | 588 | 25' | 613 |
| D-1 | N39° 50' 23.59" | W 89° 40' 36.29" | 587 | 25' | 612 |
| E-1 | N39° 50' 29.17" | W 89° 40' 30.12" | 587 | 25' | 612 |
| F-1 | N39° 50' 14.03" | W 89° 40' 03.69" | 579 | 25' | 604 |
| G-1 | N39° 50' 11.53" | W 89° 40' 06.10" | 570 | 25' | 595 |
| H-1 | N39° 50' 07.31" | W 89° 39' 56.04" | 562 | 25' | 587 |
| I-1 | N39° 50' 05.49" | W 89° 39' 52.95" | 542 | 25' | 567 |
| J-1 | N39° 50' 22.45" | W 89° 40' 34.29" | 587 | 25' | 612 |

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 DRAWN BY: CMT
 CHECKED BY: **RLV**
 APPROVED BY: **RLV**
 DATE: APRIL 27, 2012
 JOB No: 110350400
 SPI-4156
 3-17-0096-XX
 SHEET 05 OF 106 SHEETS



LEGEND

- WORK AREA
- NEW PAVEMENT
- BARRICADES LINE - SEE DETAIL
- PROPOSED CONSTRUCTION SET BACK LINE - SEE DETAIL
- CONTRACTOR'S ACCESS
- FLAGMAN - IN CONTACT WITH AIR TRAFFIC CONTROL TOWER (ATCT)

PHASE 2 NOTES:

1. ALL CONSTRUCTION BARRICADES SHALL BE INSTALLED AS SHOWN PRIOR TO STARTING WORK IN THIS PHASE.
2. PHASE 2A & PHASE 2B SHALL NOT BE CONSTRUCTED AT THE SAME TIME.
3. THESE BARRICADES SHALL BE PLACED FOR PHASE 2A.
4. THESE BARRICADES SHALL BE PLACED FOR PHASE 2B.
5. THESE BARRICADES SHALL BE PLACED AT THE COMPLETION OF THE EDGE GRADING AND HMA PAVING IN THE RESPECTIVE AREAS SHOWN IN PHASE 2A AND PHASE 2B. THESE BARRICADES SHALL REMAIN IN PLACE UNTIL THE OPENING OF NEW TAXIWAY Y PAVEMENT TO AIRCRAFT.
6. PHASE 2 MAY BE CONSTRUCTED CONCURRENTLY WITH PHASE 1, BUT PHASE 2A AND THE CLOSURE OF TAXIWAY A SHALL NOT EXCEED FOUR (4) CONSECUTIVE CALENDAR DAYS.
7. WORK TO BE COMPLETED BEFORE REOPENING PHASE 2A AND PHASE 2B SHALL INCLUDE, BUT NOT LIMITED TO:
 - EARTHWORK
 - AGGREGATE BASE
 - UNDERDRAINS
 - STORM SEWER
 - HMA PAVING
8. WORK ITEMS SUCH AS, BUT NOT LIMITED TO, PAVEMENT MARKING, ELECTRICAL/LIGHTING, SEEDING AND MULCHING TO BE COMPLETED IN THE WORK AREA OF PHASE 2A MAY COMPLETED OUTSIDE OF THE 18 CALENDAR DAY CLOSURE OF PHASE 2A. THIS WORK SHALL BE ACCOMPLISHED BY DAILY CLOSURES OF TAXIWAY A CLOSELY COORDINATED WITH THE AIRPORT AND THE ATCT.

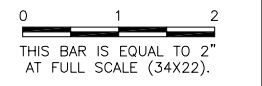
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| A-2 | N39° 50' 32.01" | W 89° 40' 41.92" | 589 | 25' | 614 |
| B-2 | N39° 50' 30.88" | W 89° 40' 39.95" | 588 | 25' | 613 |
| C-2 | N39° 50' 30.31" | W 89° 40' 32.11" | 585 | 25' | 610 |
| D-2 | N39° 50' 29.17" | W 89° 40' 30.12" | 587 | 25' | 612 |
| E-2 | N39° 50' 22.45" | W 89° 40' 34.29" | 587 | 25' | 612 |
| F-2 | N39° 50' 23.59" | W 89° 40' 36.29" | 587 | 25' | 612 |
| G-2 | N39° 50' 27.54" | W 89° 40' 43.81" | 588 | 25' | 613 |
| H-2 | N39° 50' 30.44" | W 89° 40' 43.73" | 587 | 25' | 612 |

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 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/8/2012 9:02 PM
 SPI-BASE CURRENT GEOMETRY
 BASE_PROP_GEO
 KEYMAP

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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

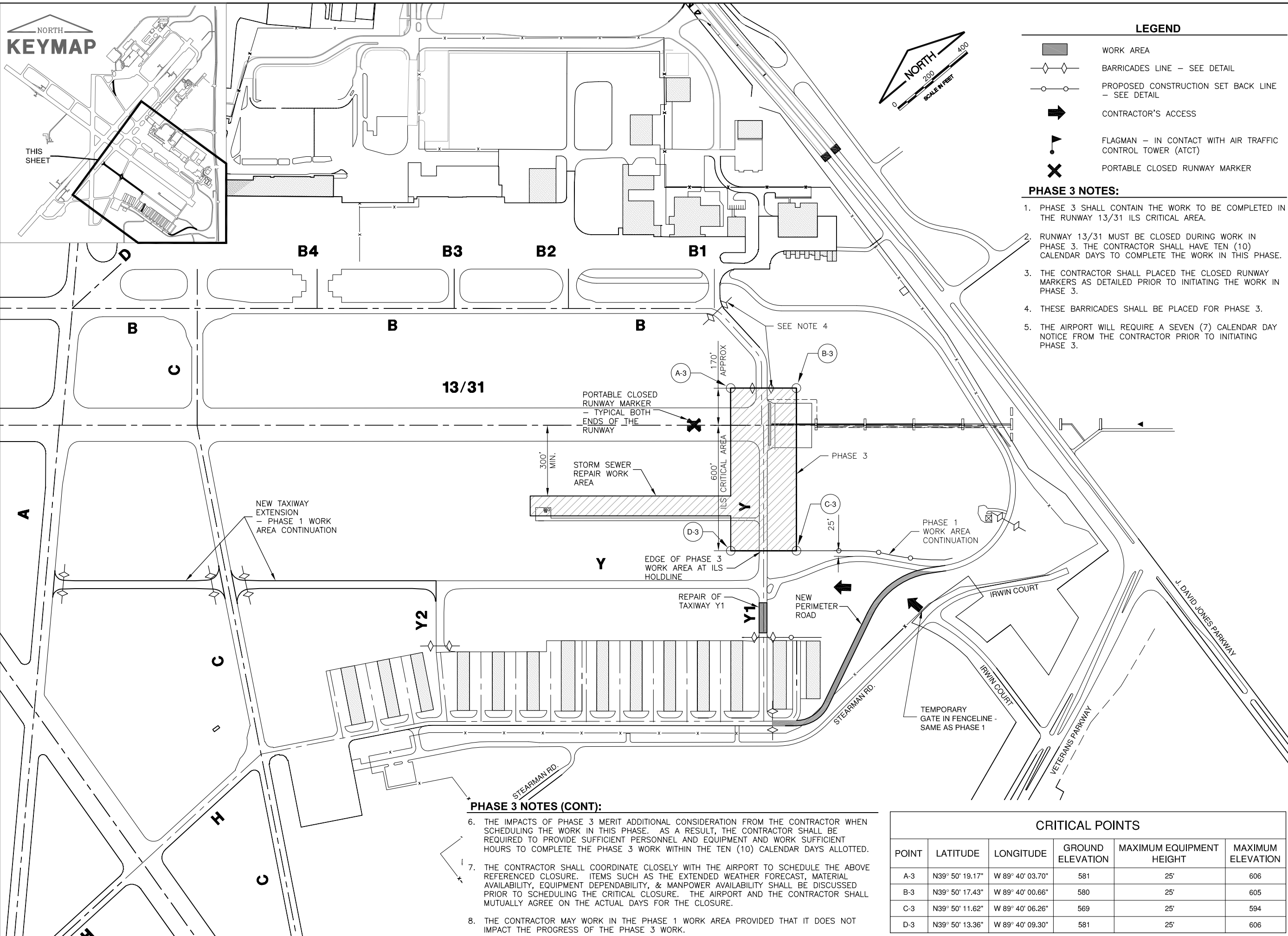
EXTEND TAXIWAY Y
CONSTRUCTION ACTIVITY PLAN - PHASE 2

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 3-17-0096-XX
 SHEET 06 OF 106 SHEETS



LEGEND

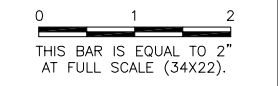
- WORK AREA
- BARRICADES LINE - SEE DETAIL
- PROPOSED CONSTRUCTION SET BACK LINE - SEE DETAIL
- CONTRACTOR'S ACCESS
- FLAGMAN - IN CONTACT WITH AIR TRAFFIC CONTROL TOWER (ATCT)
- PORTABLE CLOSED RUNWAY MARKER

- PHASE 3 NOTES:**
- PHASE 3 SHALL CONTAIN THE WORK TO BE COMPLETED IN THE RUNWAY 13/31 ILS CRITICAL AREA.
 - RUNWAY 13/31 MUST BE CLOSED DURING WORK IN PHASE 3. THE CONTRACTOR SHALL HAVE TEN (10) CALENDAR DAYS TO COMPLETE THE WORK IN THIS PHASE.
 - THE CONTRACTOR SHALL PLACE THE CLOSED RUNWAY MARKERS AS DETAILED PRIOR TO INITIATING THE WORK IN PHASE 3.
 - THESE BARRICADES SHALL BE PLACED FOR PHASE 3.
 - THE AIRPORT WILL REQUIRE A SEVEN (7) CALENDAR DAY NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING PHASE 3.

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 UPDATE BY: T.J. Heavisides
 PLOT DATE: 5/9/2012 2:21 PM
 SPI-BASE CURRENT GEOMETRY
 BASE_PROP_GEO
 KEYMAP

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 SPRINGFIELD, ILLINOIS**

EXTEND TAXIWAY Y

CONSTRUCTION ACTIVITY PLAN - PHASE 3

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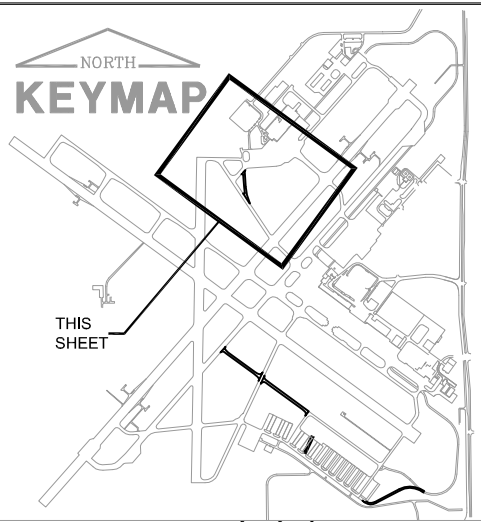
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- PHASE 3 NOTES (CONT):**
- THE IMPACTS OF PHASE 3 MERIT ADDITIONAL CONSIDERATION FROM THE CONTRACTOR WHEN SCHEDULING THE WORK IN THIS PHASE. AS A RESULT, THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE SUFFICIENT PERSONNEL AND EQUIPMENT AND WORK SUFFICIENT HOURS TO COMPLETE THE PHASE 3 WORK WITHIN THE TEN (10) CALENDAR DAYS ALLOTTED.
 - THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT TO SCHEDULE THE ABOVE REFERENCED CLOSURE. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY, & MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THE CRITICAL CLOSURE. THE AIRPORT AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE ACTUAL DAYS FOR THE CLOSURE.
 - THE CONTRACTOR MAY WORK IN THE PHASE 1 WORK AREA PROVIDED THAT IT DOES NOT IMPACT THE PROGRESS OF THE PHASE 3 WORK.

CRITICAL POINTS

| POINT | LATITUDE | LONGITUDE | GROUND ELEVATION | MAXIMUM EQUIPMENT HEIGHT | MAXIMUM ELEVATION |
|-------|-----------------|------------------|------------------|--------------------------|-------------------|
| A-3 | N39° 50' 19.17" | W 89° 40' 03.70" | 581 | 25' | 606 |
| B-3 | N39° 50' 17.43" | W 89° 40' 00.66" | 580 | 25' | 605 |
| C-3 | N39° 50' 11.62" | W 89° 40' 06.26" | 569 | 25' | 594 |
| D-3 | N39° 50' 13.36" | W 89° 40' 09.30" | 581 | 25' | 606 |

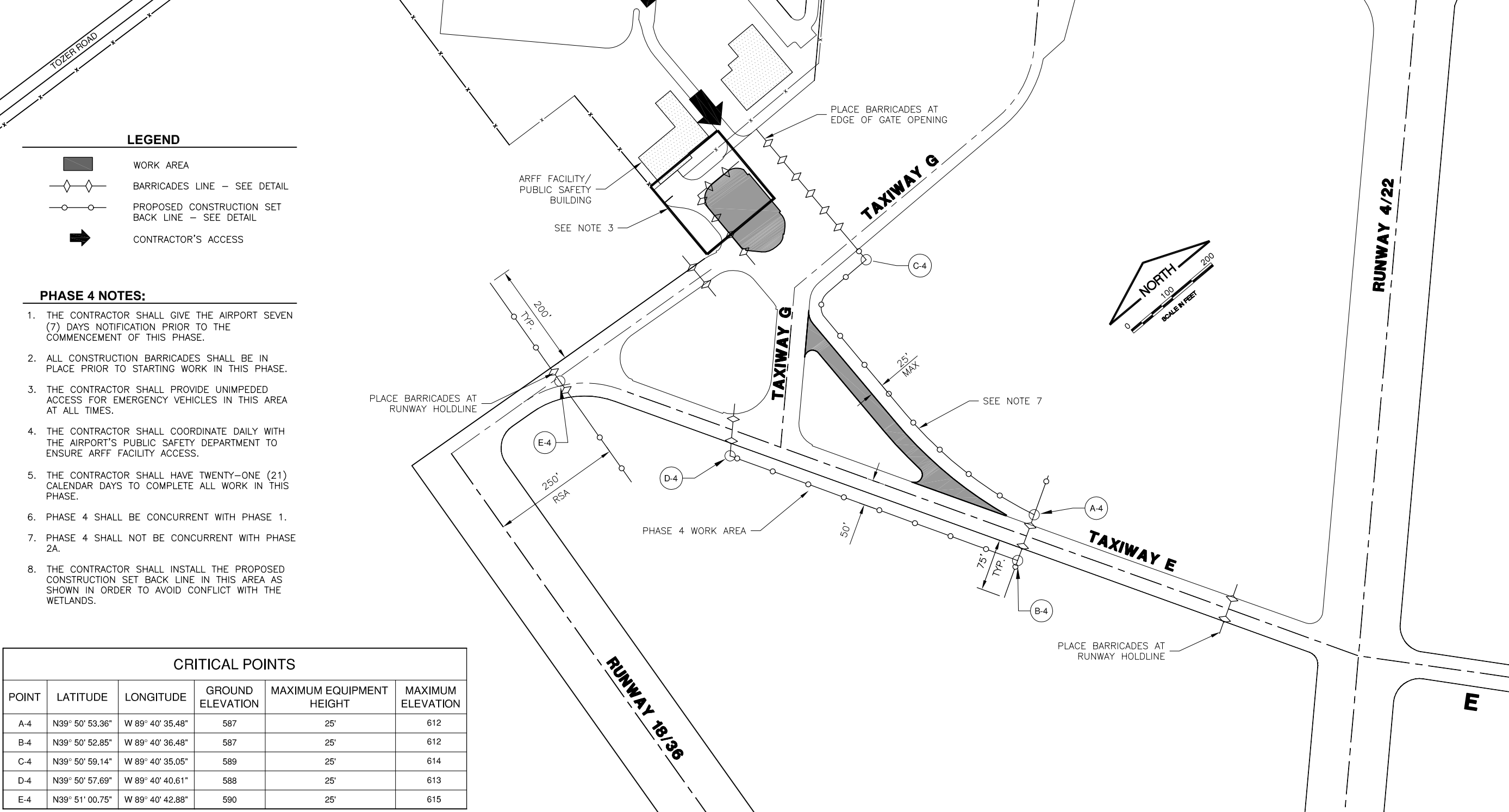
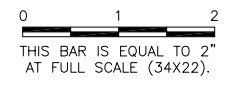
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 DRAWN BY: CMT
 CHECKED BY: **RLV**
 APPROVED BY: **RLV**
 DATE: APRIL 27, 2012
 JOB No: 110350400
 SPI-4156
 3-17-0096-XX
 SHEET 07 OF 106 SHEETS



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 FILE: CONSTRUCTION ACTIVITY PLAN 4.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/8/2012 9:02 PM
 SPI-BASE CURRENT GEOMETRY
 BASE_PROP_GEO
 KEYMAP

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LEGEND

- WORK AREA
- BARRICADES LINE - SEE DETAIL
- PROPOSED CONSTRUCTION SET BACK LINE - SEE DETAIL
- CONTRACTOR'S ACCESS

PHASE 4 NOTES:

1. THE CONTRACTOR SHALL GIVE THE AIRPORT SEVEN (7) DAYS NOTIFICATION PRIOR TO THE COMMENCEMENT OF THIS PHASE.
2. ALL CONSTRUCTION BARRICADES SHALL BE IN PLACE PRIOR TO STARTING WORK IN THIS PHASE.
3. THE CONTRACTOR SHALL PROVIDE UNIMPEDED ACCESS FOR EMERGENCY VEHICLES IN THIS AREA AT ALL TIMES.
4. THE CONTRACTOR SHALL COORDINATE DAILY WITH THE AIRPORT'S PUBLIC SAFETY DEPARTMENT TO ENSURE ARFF FACILITY ACCESS.
5. THE CONTRACTOR SHALL HAVE TWENTY-ONE (21) CALENDAR DAYS TO COMPLETE ALL WORK IN THIS PHASE.
6. PHASE 4 SHALL BE CONCURRENT WITH PHASE 1.
7. PHASE 4 SHALL NOT BE CONCURRENT WITH PHASE 2A.
8. THE CONTRACTOR SHALL INSTALL THE PROPOSED CONSTRUCTION SET BACK LINE IN THIS AREA AS SHOWN IN ORDER TO AVOID CONFLICT WITH THE WETLANDS.

CRITICAL POINTS

| POINT | LATITUDE | LONGITUDE | GROUND ELEVATION | MAXIMUM EQUIPMENT HEIGHT | MAXIMUM ELEVATION |
|-------|-----------------|------------------|------------------|--------------------------|-------------------|
| A-4 | N39° 50' 53.36" | W 89° 40' 35.48" | 587 | 25' | 612 |
| B-4 | N39° 50' 52.85" | W 89° 40' 36.48" | 587 | 25' | 612 |
| C-4 | N39° 50' 59.14" | W 89° 40' 35.05" | 589 | 25' | 614 |
| D-4 | N39° 50' 57.69" | W 89° 40' 40.61" | 588 | 25' | 613 |
| E-4 | N39° 51' 00.75" | W 89° 40' 42.88" | 590 | 25' | 615 |



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**EXTEND TAXIWAY Y
 CONSTRUCTION ACTIVITY PLAN - PHASE 4**

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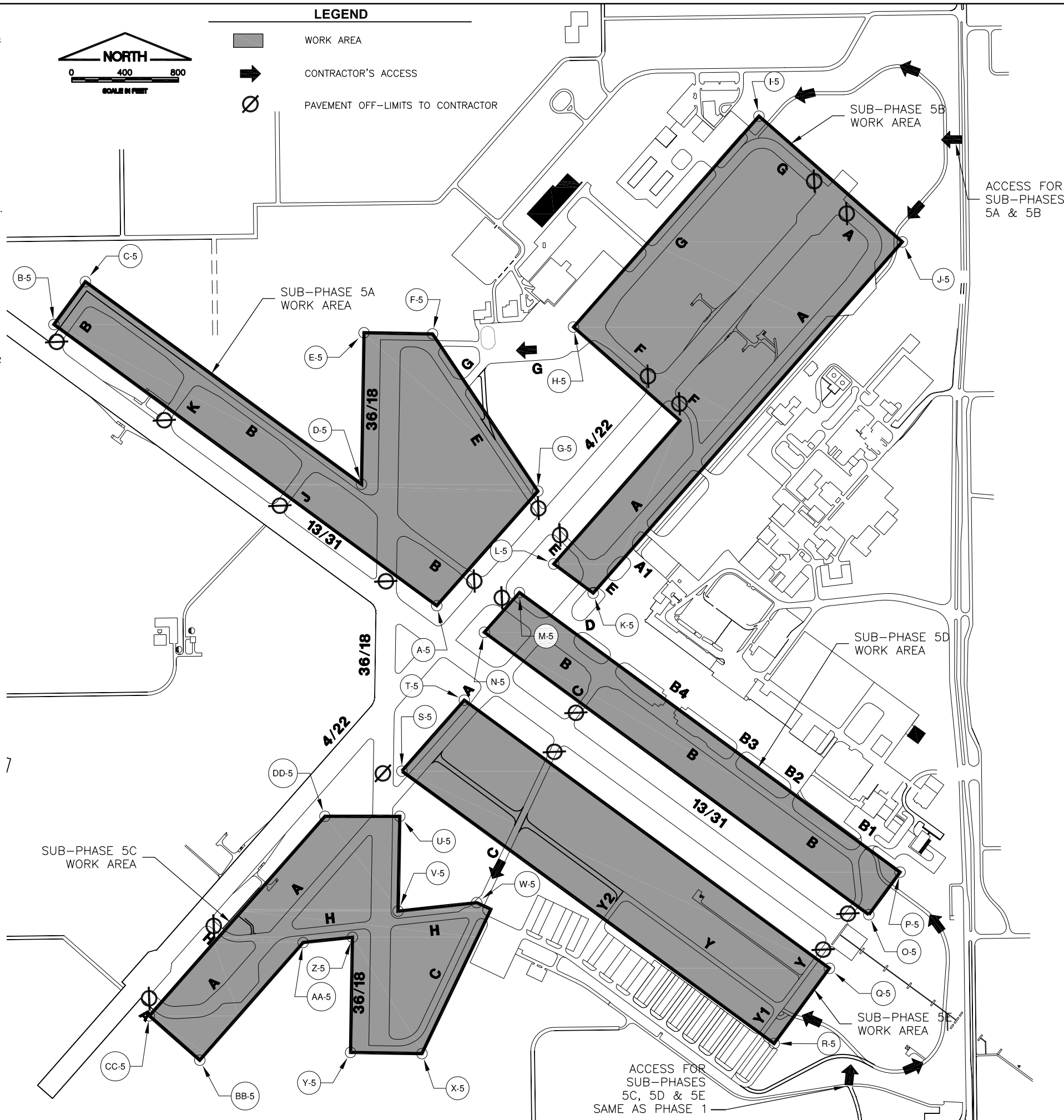
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| DESIGN BY: | RLV/JMW |
| DRAWN BY: | CMT |
| CHECKED BY: | RLV |
| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET 08 OF 106 SHEETS | |

PHASE 5 NOTES:

1. WORK TO BE COMPLETED IN THIS PHASE INCLUDES ALL OF THE PAVEMENT REMARKING INCLUDING THE RUNWAY HOLD LINES, ENHANCED TAXIWAY CENTERLINES AND SURFACE PAINTED HOLDING POSITION SIGNS. FOR MORE INFORMATION SEE MARKING PLAN AND DETAIL SHEETS.
2. THE AIRPORT WILL REQUIRE SEVEN (7) CALENDAR DAY NOTICE PRIOR TO INITIATING PHASE 5.
3. THE CONTRACTOR MAY ONLY WORK WITHIN ONE SUB-PHASE AT A TIME AND ALL WORK TO BE COMPLETED IN THAT SUB-PHASE MUST BE FINISHED BEFORE MOVING TO NEXT SUB-PHASE.
4. THE CONTRACTOR MAY PROGRESS THROUGH THE SUB-PHASES IN ANY ORDER AS LONG AS THE ORDER IS AGREED UPON BY THE AIRPORT AND THE ENGINEER. THE AIRPORT SHALL HAVE THE FINAL AUTHORITY IN THE ORDER OF COMPLETION.
5. THE CONTRACTOR SHALL COORDINATE THE NEXT DAY'S WORK AREA AT THE END OF THE PREVIOUS DAY WITH THE AIRPORT AND THE ENGINEER.
6. THE CONTRACTOR WILL BE REQUIRED TO OBTAIN A SECURITY BADGE AND SHALL MAINTAIN CONSTANT COMMUNICATION WITH THE TOWER IN ORDER TO COMPLETE THE WORK WITHIN PHASE 5.
7. THE CONTRACTOR SHALL HAVE UP TO SIXTEEN (16) BARRICADES AVAILABLE AT ALL TIMES TO BE PLACED AS DIRECTED BY THE AIRPORT DURING TEMPORARY TAXIWAY CLOSURES.
8. THE CONTRACTOR MAY BE REQUIRED TO CLOSE A RUNWAY UPON DIRECTION OF THE AIRPORT SHOULD WORK EXTEND INTO THE RUNWAY SAFETY AREA. THE CONTRACTOR MUST THEN FOLLOW THE GUIDELINES AND REQUIREMENTS FOR CLOSING A RUNWAY SET FORTH IN THE CONSTRUCTION ACTIVITY PLAN NOTES & DETAILS SHEET.
9. THE CONTRACTOR MAY ONLY CLOSE AT MOST AT ONE TIME EITHER RUNWAY 18/36 AND RUNWAY 4/22 OR RUNWAY 18/36 AND RUNWAY 13/31. EITHER RUNWAY 4/22 OR RUNWAY 13/31 SHALL BE OPEN AT ALL TIMES. EFFORT AND CONSTRUCTION METHODS SHOULD BE OPTIMIZED TO MINIMIZE RUNWAY CLOSURES.
10. THE PAVEMENTS DELINEATED "OFF-LIMITS TO CONTRACTOR" SHALL NOT BE USED FOR ACCESS OR MATERIAL ROUTING. THE CONTRACTOR MAY AT NO TIME CROSS AN OPEN RUNWAY AND SHALL UTILIZE PERIMETER ROADS.
11. ALL TAXIWAYS AND ALL RUNWAYS SHALL BE OPEN FOR FULL USAGE AT THE END OF EACH DAY.
12. THE CONTRACTOR MAY COMPLETE THE WORK IN THIS PHASE AT NIGHT PROVIDED THAT THE AIRPORT APPROVES THIS ACTION AND THAT THEY FOLLOW ALL THE SPECIFICATION REQUIREMENTS FOR NIGHT WORK.
13. THE CONTRACTOR WILL BE REQUIRED TO COMPLETE THE WORK IN PHASE 5 PRIOR TO OCTOBER 31, 2012. THIS MAY MEAN COMPLETION OF THIS PHASE WILL BE INDEPENDENT OF THE OTHER PHASES OF THE PROJECT. THE CONTRACTOR SHOULD BE AWARE THAT MULTIPLE MOBILIZATIONS MAY LIKELY BE REQUIRED IN ORDER TO COMPLETE PAVEMENT MARKINGS IN THIS PHASE AND TO COMPLETE THE PAVEMENT MARKINGS IN THE OTHER PHASES AT LATER DATES.

LEGEND

-  WORK AREA
-  CONTRACTOR'S ACCESS
-  PAVEMENT OFF-LIMITS TO CONTRACTOR



CRITICAL POINTS

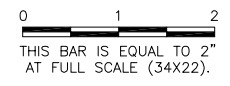
| POINT | LATITUDE | LONGITUDE | GROUND ELEVATION | MAXIMUM EQUIPMENT HEIGHT | MAXIMUM ELEVATION |
|-------|-----------------|------------------|------------------|--------------------------|-------------------|
| A-5 | N39° 50' 41.38" | W 89° 40' 41.74" | 589 | 25' | 614 |
| AA-5 | N39° 50' 16.44" | W 89° 40' 54.78" | 586 | 25' | 611 |
| B-5 | N39° 51' 02.40" | W 89° 41' 18.44" | 583 | 25' | 608 |
| BB-5 | N39° 50' 07.76" | W 89° 41' 04.78" | 585 | 25' | 610 |
| C-5 | N39° 51' 05.56" | W 89° 41' 15.39" | 575 | 25' | 600 |
| CC-5 | N39° 50' 11.07" | W 89° 41' 09.61" | 584 | 25' | 609 |
| D-5 | N39° 50' 50.42" | W 89° 40' 48.92" | 589 | 25' | 614 |
| DD-5 | N39° 50' 25.80" | W 89° 40' 52.62" | 587 | 25' | 612 |
| E-5 | N39° 51' 01.66" | W 89° 40' 48.60" | 588 | 25' | 613 |
| F-5 | N39° 51' 01.55" | W 89° 40' 42.01" | 589 | 25' | 614 |
| G-5 | N39° 50' 49.83" | W 89° 40' 31.95" | 587 | 25' | 612 |
| H-5 | N39° 51' 01.99" | W 89° 40' 28.43" | 591 | 25' | 616 |
| I-5 | N39° 51' 17.57" | W 89° 40' 10.47" | 597 | 25' | 622 |
| J-5 | N39° 51' 08.18" | W 89° 39' 56.75" | 593 | 25' | 618 |
| K-5 | N39° 50' 42.25" | W 89° 40' 26.68" | 587 | 25' | 612 |
| L-5 | N39° 50' 44.43" | W 89° 40' 30.46" | 586 | 25' | 611 |
| M-5 | N39° 50' 42.32" | W 89° 40' 33.78" | 588 | 25' | 613 |
| N-5 | N39° 50' 39.39" | W 89° 40' 37.18" | 590 | 25' | 615 |
| O-5 | N39° 50' 18.28" | W 89° 40' 00.33" | 581 | 25' | 606 |
| P-5 | N39° 50' 21.42" | W 89° 39' 57.30" | 579 | 25' | 604 |
| Q-5 | N39° 50' 14.31" | W 89° 40' 04.16" | 579 | 25' | 604 |
| R-5 | N39° 50' 08.76" | W 89° 40' 09.51" | 575 | 25' | 600 |
| S-5 | N39° 50' 29.12" | W 89° 40' 45.19" | 589 | 25' | 614 |
| T-5 | N39° 50' 34.36" | W 89° 40' 39.14" | 587 | 25' | 612 |
| U-5 | N39° 50' 25.77" | W 89° 40' 45.42" | 588 | 25' | 613 |
| V-5 | N39° 50' 18.73" | W 89° 40' 45.62" | 588 | 25' | 613 |
| W-5 | N39° 50' 19.33" | W 89° 40' 38.05" | 589 | 25' | 614 |
| X-5 | N39° 50' 08.15" | W 89° 40' 43.40" | 585 | 25' | 610 |
| Y-5 | N39° 50' 08.27" | W 89° 40' 50.28" | 583 | 25' | 608 |
| Z-5 | N39° 50' 16.82" | W 89° 40' 50.03" | 586 | 25' | 611 |

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ACCESS FOR SUB-PHASES 5A & 5B



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 SPRINGFIELD, ILLINOIS

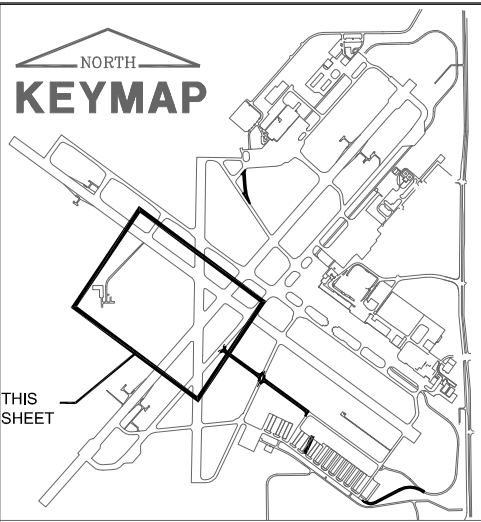
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CONSTRUCTION ACTIVITY PLAN - PHASE 5

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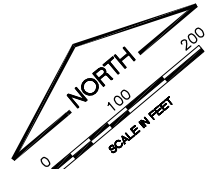
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 APPROVED BY: RLV
 DATE: APRIL 27, 2012
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 3-17-0096-XX



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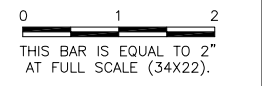
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KEYMAP



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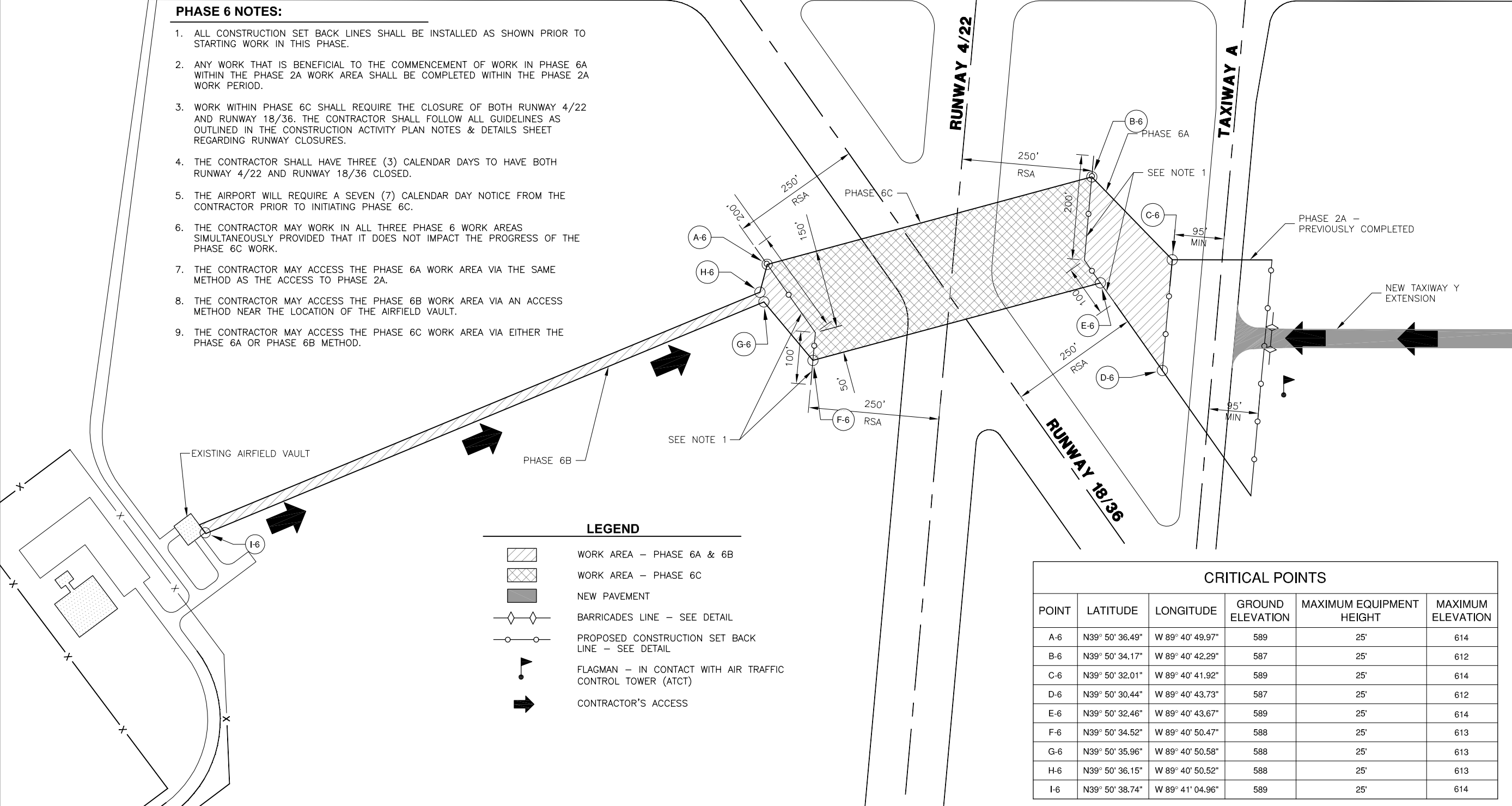
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PHASE 6 NOTES:

1. ALL CONSTRUCTION SET BACK LINES SHALL BE INSTALLED AS SHOWN PRIOR TO STARTING WORK IN THIS PHASE.
2. ANY WORK THAT IS BENEFICIAL TO THE COMMENCEMENT OF WORK IN PHASE 6A WITHIN THE PHASE 2A WORK AREA SHALL BE COMPLETED WITHIN THE PHASE 2A WORK PERIOD.
3. WORK WITHIN PHASE 6C SHALL REQUIRE THE CLOSURE OF BOTH RUNWAY 4/22 AND RUNWAY 18/36. THE CONTRACTOR SHALL FOLLOW ALL GUIDELINES AS OUTLINED IN THE CONSTRUCTION ACTIVITY PLAN NOTES & DETAILS SHEET REGARDING RUNWAY CLOSURES.
4. THE CONTRACTOR SHALL HAVE THREE (3) CALENDAR DAYS TO HAVE BOTH RUNWAY 4/22 AND RUNWAY 18/36 CLOSED.
5. THE AIRPORT WILL REQUIRE A SEVEN (7) CALENDAR DAY NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING PHASE 6C.
6. THE CONTRACTOR MAY WORK IN ALL THREE PHASE 6 WORK AREAS SIMULTANEOUSLY PROVIDED THAT IT DOES NOT IMPACT THE PROGRESS OF THE PHASE 6C WORK.
7. THE CONTRACTOR MAY ACCESS THE PHASE 6A WORK AREA VIA THE SAME METHOD AS THE ACCESS TO PHASE 2A.
8. THE CONTRACTOR MAY ACCESS THE PHASE 6B WORK AREA VIA AN ACCESS METHOD NEAR THE LOCATION OF THE AIRFIELD VAULT.
9. THE CONTRACTOR MAY ACCESS THE PHASE 6C WORK AREA VIA EITHER THE PHASE 6A OR PHASE 6B METHOD.



LEGEND

- WORK AREA - PHASE 6A & 6B
- WORK AREA - PHASE 6C
- NEW PAVEMENT
- BARRICADES LINE - SEE DETAIL
- PROPOSED CONSTRUCTION SET BACK LINE - SEE DETAIL
- FLAGMAN - IN CONTACT WITH AIR TRAFFIC CONTROL TOWER (ATCT)
- CONTRACTOR'S ACCESS

CRITICAL POINTS

| POINT | LATITUDE | LONGITUDE | GROUND ELEVATION | MAXIMUM EQUIPMENT HEIGHT | MAXIMUM ELEVATION |
|-------|-----------------|------------------|------------------|--------------------------|-------------------|
| A-6 | N39° 50' 36.49" | W 89° 40' 49.97" | 589 | 25' | 614 |
| B-6 | N39° 50' 34.17" | W 89° 40' 42.29" | 587 | 25' | 612 |
| C-6 | N39° 50' 32.01" | W 89° 40' 41.92" | 589 | 25' | 614 |
| D-6 | N39° 50' 30.44" | W 89° 40' 43.73" | 587 | 25' | 612 |
| E-6 | N39° 50' 32.46" | W 89° 40' 43.67" | 589 | 25' | 614 |
| F-6 | N39° 50' 34.52" | W 89° 40' 50.47" | 588 | 25' | 613 |
| G-6 | N39° 50' 35.96" | W 89° 40' 50.58" | 588 | 25' | 613 |
| H-6 | N39° 50' 36.15" | W 89° 40' 50.52" | 588 | 25' | 613 |
| I-6 | N39° 50' 38.74" | W 89° 41' 04.96" | 589 | 25' | 614 |

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SPRINGFIELD, ILLINOIS

**EXTEND TAXIWAY Y
CONSTRUCTION ACTIVITY PLAN - PHASE 6**

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DESIGN BY: RLV/JMW
DRAWN BY: CMT
CHECKED BY: **RLV**
APPROVED BY: **RLV**
DATE: APRIL 27, 2012
JOB No: 110350400

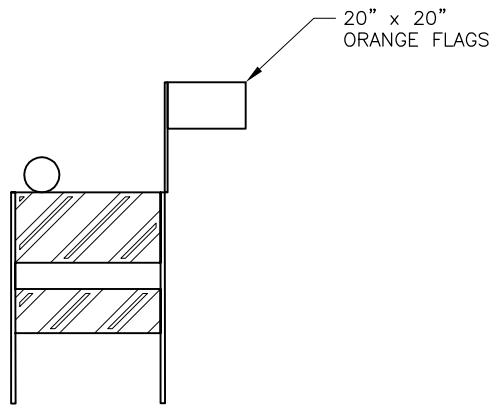
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3-17-0096-XX

PHASING GENERAL NOTES

1. ALL CONSTRUCTION BARRICADES SHALL BE IN PLACE PRIOR TO INITIATING EACH PHASE.
2. THE CONTRACTOR SHALL GIVE THE AIRPORT A MINIMUM 7 DAYS NOTIFICATION PRIOR TO THE INITIATION OF WORK IN EACH PHASE.
3. THE CONTRACTOR SHALL MAINTAIN ALL MINIMUM DISTANCES SHOWN IN THE CONSTRUCTION ACTIVITY PLANS OR AS REQUIRED FOR THE CRITICAL OPERATING AIRCRAFT THROUGH THESE AREAS.
4. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THE CONSTRUCTION SET BACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND WILL NOT BE MEASURED FOR PAYMENT. SETBACK LINE SHALL ALSO BE INSTALLED AROUND WETLAND AREA NEAR ARFF ROAD PREVENTING ANYONE FROM ENTERING THIS AREA.
5. ALL PAVEMENTS OR TURF AREAS UTILIZED BY THE CONTRACTOR FOR AN ACCESS ROUTE, STAGING, OR STORAGE SHALL BE REPAIRED AND RESTORED TO THEIR ORIGINAL CONDITION TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION TO PROTECT, REPAIR, OR RESTORE THESE AREAS SHALL BE MADE.
6. WORK AREA ACCESS MAINTENANCE SHALL BE INCIDENTAL TO THE CONTRACT. MAINTENANCE SHALL CONTINUE AS REQUIRED SUCH THAT DURING ADVERSE WEATHER THE CONTRACTOR SHALL BE ABLE TO MAINTAIN ACCESS TO THE WORK AND NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITES.
7. FOR PHASES INVOLVING CLOSURE OF A TAXIWAY OR RUNWAY FOR A RESTRICTED NUMBER OF CALENDAR DAYS, THE CONTRACTOR SHALL TAKE INTO ACCOUNT CERTAIN SCHEDULING CONSIDERATIONS, SUCH AS, SUFFICIENT NUMBER OF PERSONNEL AND EQUIPMENT AVAILABLE AND THE WORKING OF SUFFICIENT HOURS TO COMPLETE CONSTRUCTION WITHIN THESE PHASES AND TIME RESTRAINTS.
8. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT TO SCHEDULE TAXIWAY AND RUNWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY, & MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THE CRITICAL CLOSURE. THE AIRPORT AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE ACTUAL DAYS FOR ANY CLOSURES.

ROADWAY TRAFFIC CONTROL PLAN

1. THE CONTRACTOR MAY BE REQUIRED TO IMPLEMENT A ROADWAY TRAFFIC CONTROL PLAN WHEN WORKING ALONG STEARMAN ROAD NEAR THE NEW PERIMETER ROAD REALIGNMENT ON THE SOUTHEAST SIDE OF THE AIRPORT.
2. THE REQUIREMENTS FOR THIS PLAN SHALL BE THE SAME AS WHEN WORKING IN THE AREA OF A TYPICAL LOCAL COUNTY OR CITY ROADWAY. I.D.O.T. - BUREAU OF LOCAL ROADS' REQUIREMENTS SHALL GOVERN REGARDING SIGNAGE AND BARRICADING.
3. COSTS TO MAINTAIN THE REQUIRED ROADWAY TRAFFIC CONTROL PLAN SHALL BE INCIDENTAL TO THE CONTRACT AND WILL NOT BE PAID FOR AS A SEPARATE PAY ITEM.

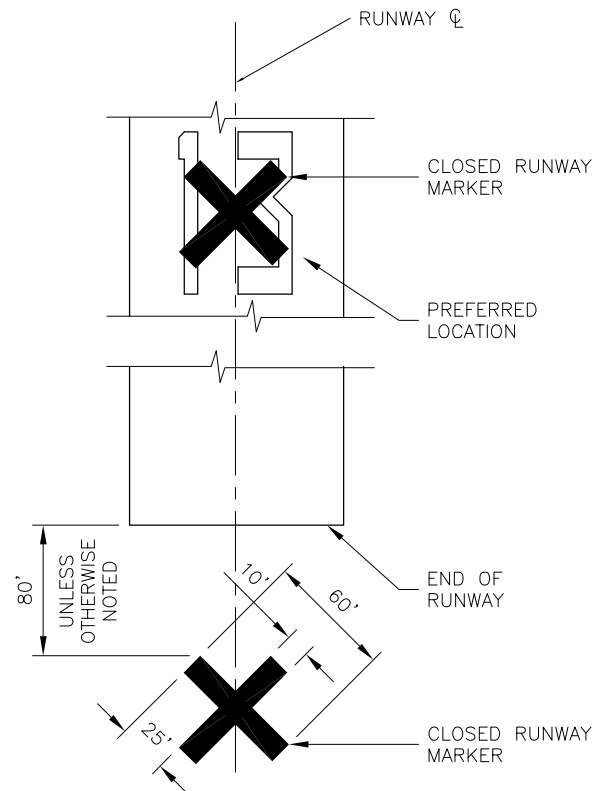


FLASHER BARRICADE DETAIL-IDOT TYPE 1

N.T.S.

FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 10' INTERVALS.

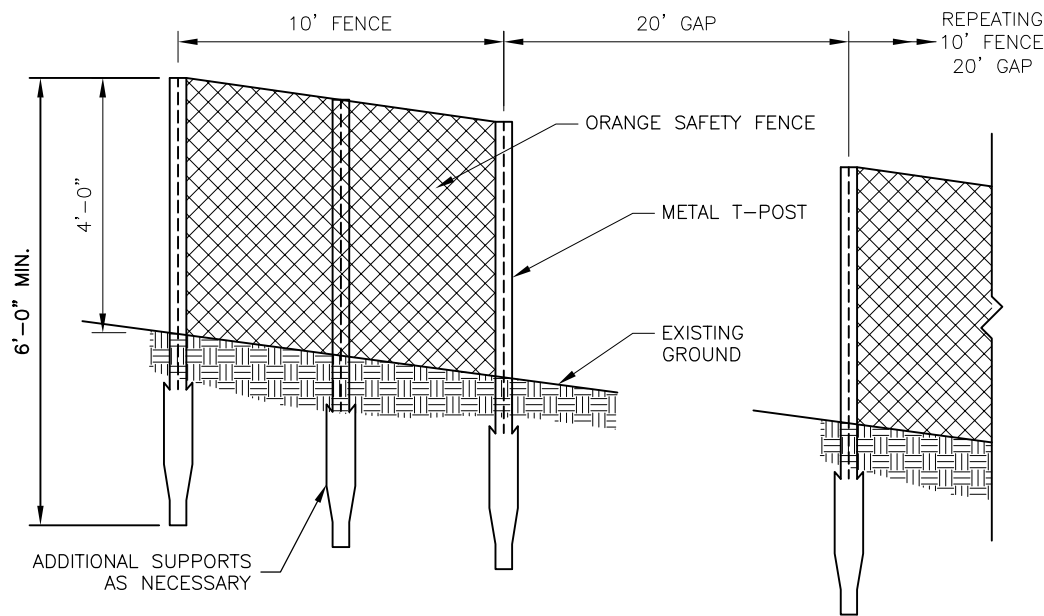


PORTABLE CLOSED RUNWAY MARKER DETAIL & NOTES

N.T.S.

NOTES

1. THE AIRPORT WILL PROVIDE PORTABLE LIGHTED CLOSURE MARKERS FOR THE CONTRACTOR'S USE.
2. THE CONTRACTOR WILL BE REQUIRED TO TOW AND SET UP THE MARKERS AT THE ENDS OF THE RUNWAY WHEN RUNWAY CLOSURES ARE REQUIRED
3. THE CONTRACTOR WILL BE REQUIRED TO PROVIDE REPLACEMENT BULBS, FUEL, LUBRICANTS, AND DAILY/PERIODIC MAINTENANCE INSPECTIONS AS REQUIRED BY THE AIRPORT.
4. THE AIRPORT WILL PERFORM PERIODIC MAINTENANCE, PROVIDE REPLACEMENT PARTS AND PERFORM REPAIRS AS REQUIRED TO KEEP THE EQUIPMENT OPERATIONAL.
5. THE COST OF SET-UP, FUELING, BULBS, INSPECTION, AND REMOVAL OF THE MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.
6. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS. HOWEVER, WHEN WORK IS NECESSARY FOR THAT AREA OR PAVEMENT IS OPEN TO TAXIING AIRCRAFT, THE MARKERS SHALL BE RELOCATED OFF THE PAVEMENT AS SHOWN ON THE DETAIL.
7. UPON COMPLETION OF THE RUNWAY CLOSURE, THE CONTRACTOR WILL BE REQUIRED TO TAKE DOWN & RETURN THE MARKERS TO THEIR STORAGE LOCATION.

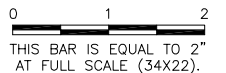


CONSTRUCTION SETBACK LINE DETAIL

N.T.S.

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SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y

CONSTRUCTION ACTIVITY PLAN
 NOTES & DETAILS

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

























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DATE: APRIL 27, 2012





























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




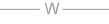
LEGEND - EXISTING CONDITIONS AND REMOVALS 1

| | | | |
|---|--------------------------------------|--|-------------------------------------|
|  | REMOVE BITUMINOUS PAVEMENT |  | EXISTING WATER |
|  | EXISTING STAKE MOUNTED TAXIWAY LIGHT |  | EXISTING SANITARY LINE |
|  | EXISTING BASE MOUNTED TAXIWAY LIGHT |  | EXISTING RUNWAY 13/31 CIRCUIT |
|  | EXISTING RUNWAY GUARD LIGHT |  | EXISTING TAXIWAY A CIRCUIT |
|  | EXISTING HANDHOLE/CLEANOUT |  | EXISTING TAXIWAY C CIRCUIT |
|  | EXISTING ELECTRICAL STRUCTURE |  | EXISTING ABD ELECTRIC CABLE |
|  | EXISTING SPLICE CAN |  | EXISTING TAXIWAY GUIDANCE SIGN |
|  | EXISTING WATER VALVE |  | EXISTING STAKE MOUNTED RUNWAY LIGHT |
|  | EXISTING STORM SEWER PIPE |  | EXISTING BASE MOUNTED RUNWAY LIGHT |
|  | EXISTING DUCT |  | EXISTING TELEPHONE LINE |
|  | EXISTING INLET |  | EXISTING ILS CABLE |
|  | EXISTING FLARED END SECTION |  | REMOVAL |
|  | EXISTING ELECTRICAL LINE |  | EXISTING LIGHT |
|  | EXISTING GAS LINE | | |






























LEGEND - EXISTING CONDITIONS AND REMOVALS 2

| | | | |
|---|---------------------------------------|--|-------------------------------------|
|  | REMOVE & REPLACE PAVEMENT -FULL DEPTH |  | EXISTING GAS LINE |
|  | EXISTING STAKE MOUNTED TAXIWAY LIGHT |  | EXISTING WATER |
|  | EXISTING BASE MOUNTED TAXIWAY LIGHT |  | EXISTING SANITARY LINE |
|  | EXISTING RUNWAY GUARD LIGHT |  | ABANDON WATER LINE |
|  | EXISTING HANDHOLE/CLEANOUT |  | EXISTING RUNWAY 13/31 CIRCUIT |
|  | EXISTING SPLICE CAN |  | EXISTING TAXIWAY Y |
|  | EXISTING FIRE HYDRANT |  | EXISTING ROADWAY SIGN |
|  | EXISTING STORM SEWER PIPE |  | EXISTING TAXIWAY GUIDANCE SIGN |
|  | EXISTING DUCT |  | EXISTING STAKE MOUNTED RUNWAY LIGHT |
|  | EXISTING INLET |  | EXISTING BASE MOUNTED RUNWAY LIGHT |
|  | EXISTING FLARED END SECTION |  | EXISTING ILS CABLE |
|  | EXISTING GAS VALVE |  | REMOVAL |
|  | EXISTING VEGETATION |  | EXISTING ELECTRIC HANDHOLE |
|  | EXISTING ELECTRICAL LINE |  | EXISTING WINDCONE |




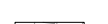



















LEGEND - EXISTING CONDITIONS AND REMOVALS 3

| | | | |
|---|--------------------------------------|--|-----------------------------------|
|  | REMOVE P.C.C. PAVEMENT |  | EXISTING TAXIWAY G HOMERUN CABLES |
|  | EXISTING STAKE MOUNTED TAXIWAY LIGHT |  | EXISTING TAXIWAY E CIRCUIT |
|  | EXISTING BASE MOUNTED TAXIWAY LIGHT |  | EXISTING TAXIWAY G CIRCUIT |
|  | EXISTING STORM SEWER STRUCTURE |  | EXISTING WOOD BARRIER FENCE |
|  | EXISTING ELECTRICAL MANHOLE (4'x4') |  | REMOVAL |
|  | EXISTING STORM SEWER PIPE |  | ADJUST |
|  | EXISTING ELECTRICAL LINE |  | EXISTING DUCT MARKERS |
|  | EXISTING DUCT |  | EXISTING FLARED END SECTION |
|  | EXISTING GAS |  | EXISTING TAXIWAY GUIDANCE SIGN |
|  | EXISTING INLET |  | EXISTING LIGHT |
|  | EXISTING ELECTRICAL OUTLET |  | EXISTING HYDRANT |
|  | EXISTING WATER | | |
|  | EXISTING SANITARY LINE | | |

LEGEND - EXISTING CONDITIONS AND REMOVALS 4

| | | | |
|---|--------------------------------------|---|-----------------------------|
|  | REMOVE PAVEMENT - FULL DEPTH |  | EXISTING SANITARY LINE |
|  | EXISTING STAKE MOUNTED TAXIWAY LIGHT |  | EXISTING GUARDRAIL |
|  | EXISTING BASE MOUNTED TAXIWAY LIGHT |  | EXISTING TAXIWAY Y CIRCUIT |
|  | EXISTING AIRPORT PERIMETER FENCE |  | REMOVAL |
|  | EXISTING FENCE REMOVAL |  | EXISTING VEGETATION |
|  | AIRPORT PROPERTY LINE |  | EXISTING GAS VALVE |
|  | EXISTING STORM SEWER PIPE |  | EXISTING INLET |
|  | EXISTING ELECTRICAL LINE |  | EXISTING SANITARY STRUCTURE |
|  | EXISTING GAS |  | EXISTING CLEANOUT |
|  | NEW TEMPORARY FENCE INSTALLATION |  | EXISTING SIGN |
|  | EXISTING FLARED END SECTION |  | EXISTING FIRE HYDRANT |
|  | EXISTING SPLICE CAN |  | EXISTING WATER VALVE |
|  | EXISTING WATER |  | EXISTING ELECTRIC HANDHOLE |
|  | EXISTING STORM DRAIN |  | SURVEY CONTROL POINT |
|  | ABANDON WATER LINE | | |

LEGEND - EXISTING CONDITIONS AND REMOVALS 5

| | | | |
|---|-------------------------------------|---|---------------------------------|
|  | EXISTING STAKE MOUNTED RUNWAY LIGHT |  | EXISTING ILS/MALSR CABLE |
|  | EXISTING BASE MOUNTED RUNWAY LIGHT |  | EXISTING BROKEN CONCRETE/DEBRIS |
|  | EXISTING RUNWAY GUARD LIGHT |  | EXISTING FLARED END SECTION |
|  | EXISTING HANDHOLE/CLEANOUT |  | EXISTING SIGN |
|  | EXISTING STORM SEWER STRUCTURE |  | EXISTING R.O.W. MARKER |
|  | EXISTING FENCE |  | EXISTING VEGETATION |
|  | EXISTING STORM SEWER PIPE |  | REMOVAL |
|  | EXISTING ELECTRICAL LINE |  | EXISTING POWER POLE |
|  | EXISTING GAS |  | EXISTING LIGHT POLE |
|  | EXISTING WATER |  | EXISTING R.O.W. MARKER |
|  | EXISTING GUARDRAIL |  | SURVEY CONTROL POINT |
|  | EXISTING DRAINAGE SWALE | | |

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

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 AT FULL SCALE (34X22).

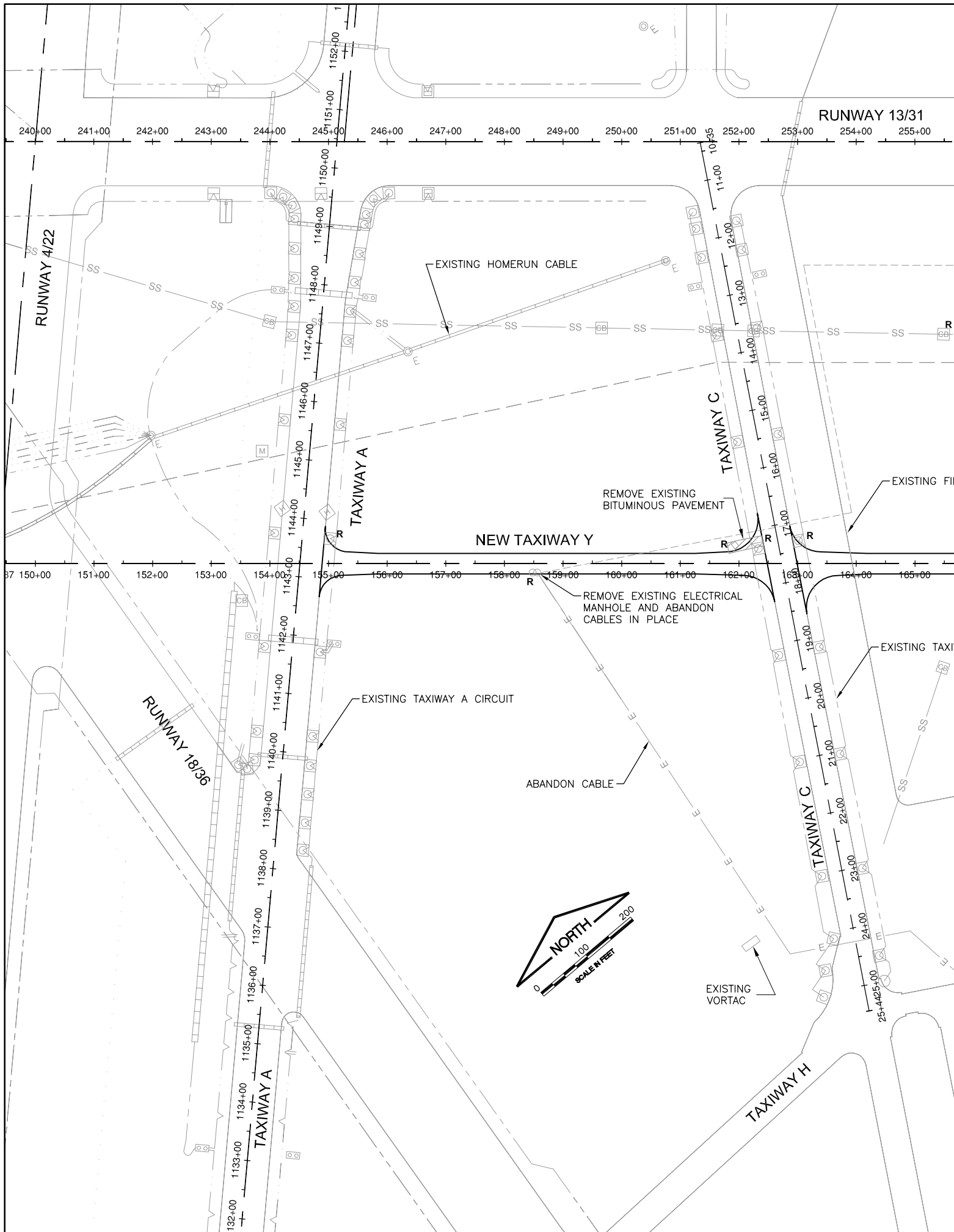

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 SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y

EXISTING CONDITIONS PLAN NOTES
 AND LEGEND

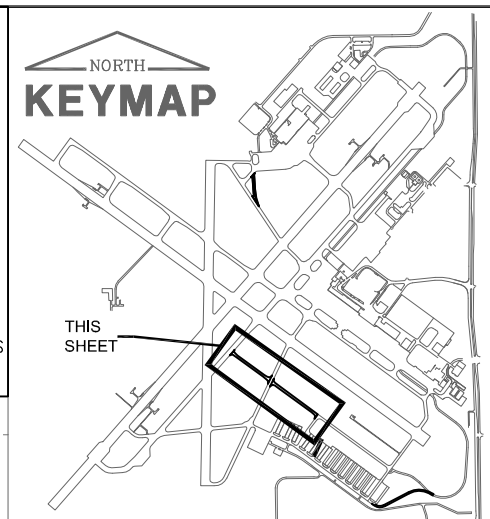
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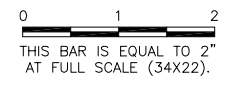
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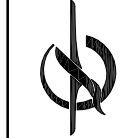
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SEE EXISTING CONDITIONS & REMOVALS 2



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EXTEND TAXIWAY Y

EXISTING CONDITIONS & REMOVALS 1

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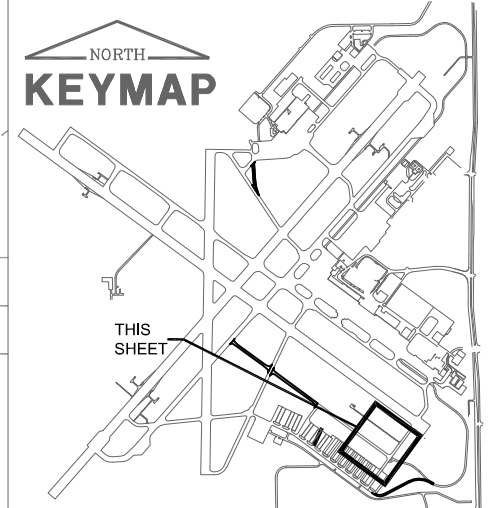
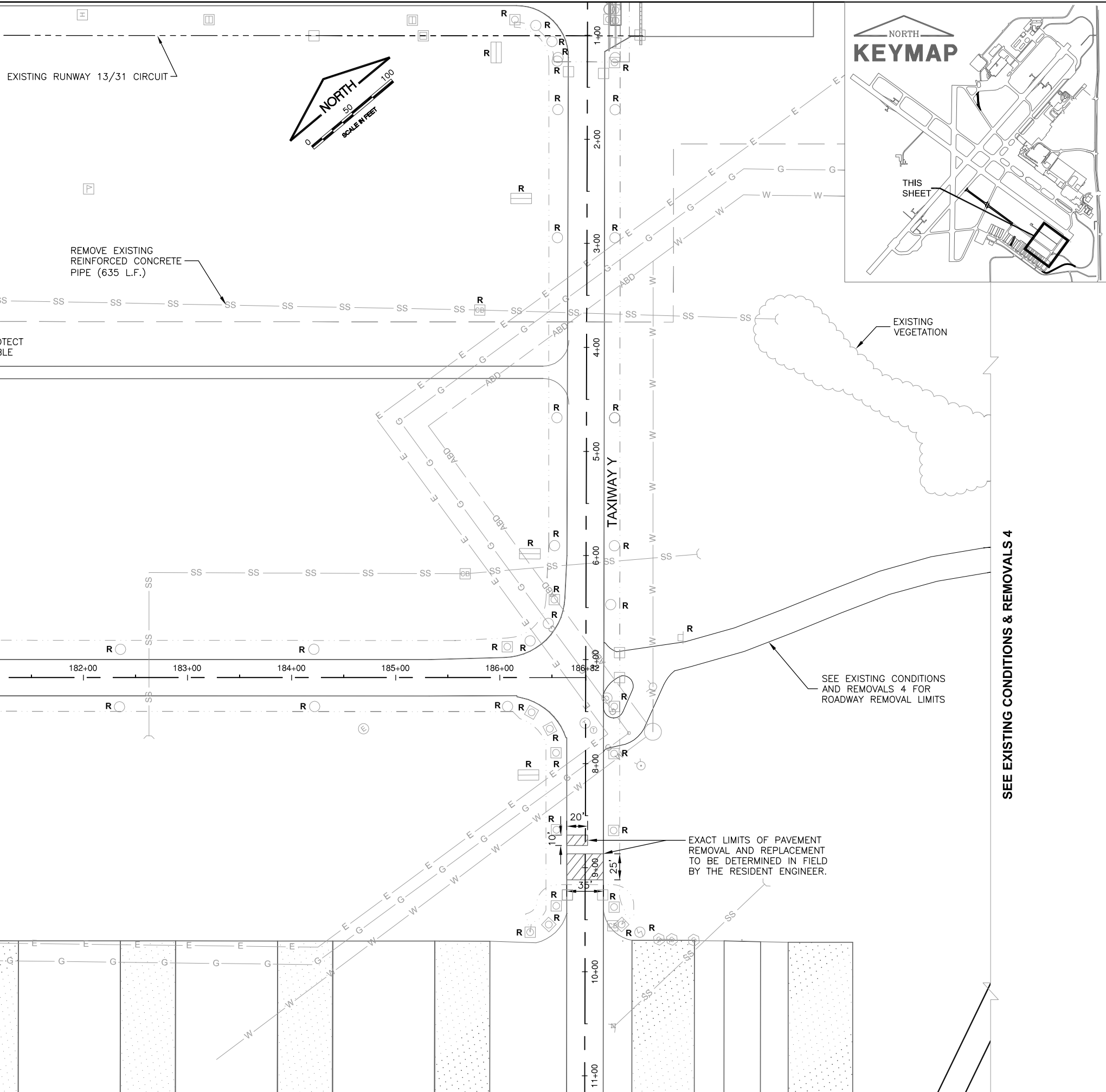


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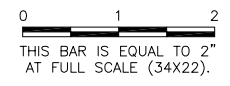
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SEE EXISTING CONDITIONS & REMOVALS 4

EXACT LIMITS OF PAVEMENT
 REMOVAL AND REPLACEMENT
 TO BE DETERMINED IN FIELD
 BY THE RESIDENT ENGINEER.

SEE EXISTING CONDITIONS
 AND REMOVALS 4 FOR
 ROADWAY REMOVAL LIMITS



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EXTEND TAXIWAY Y

EXISTING CONDITIONS & REMOVALS 2

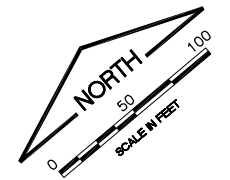
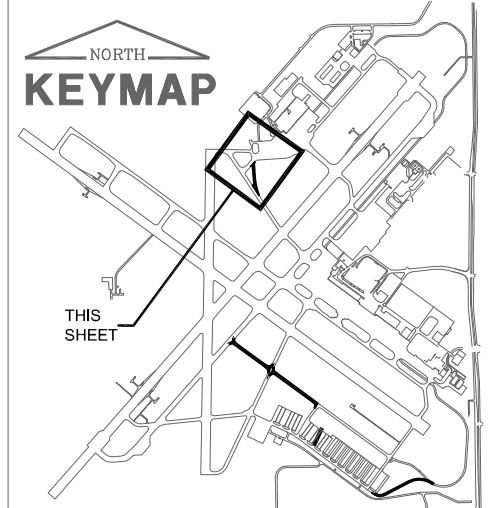
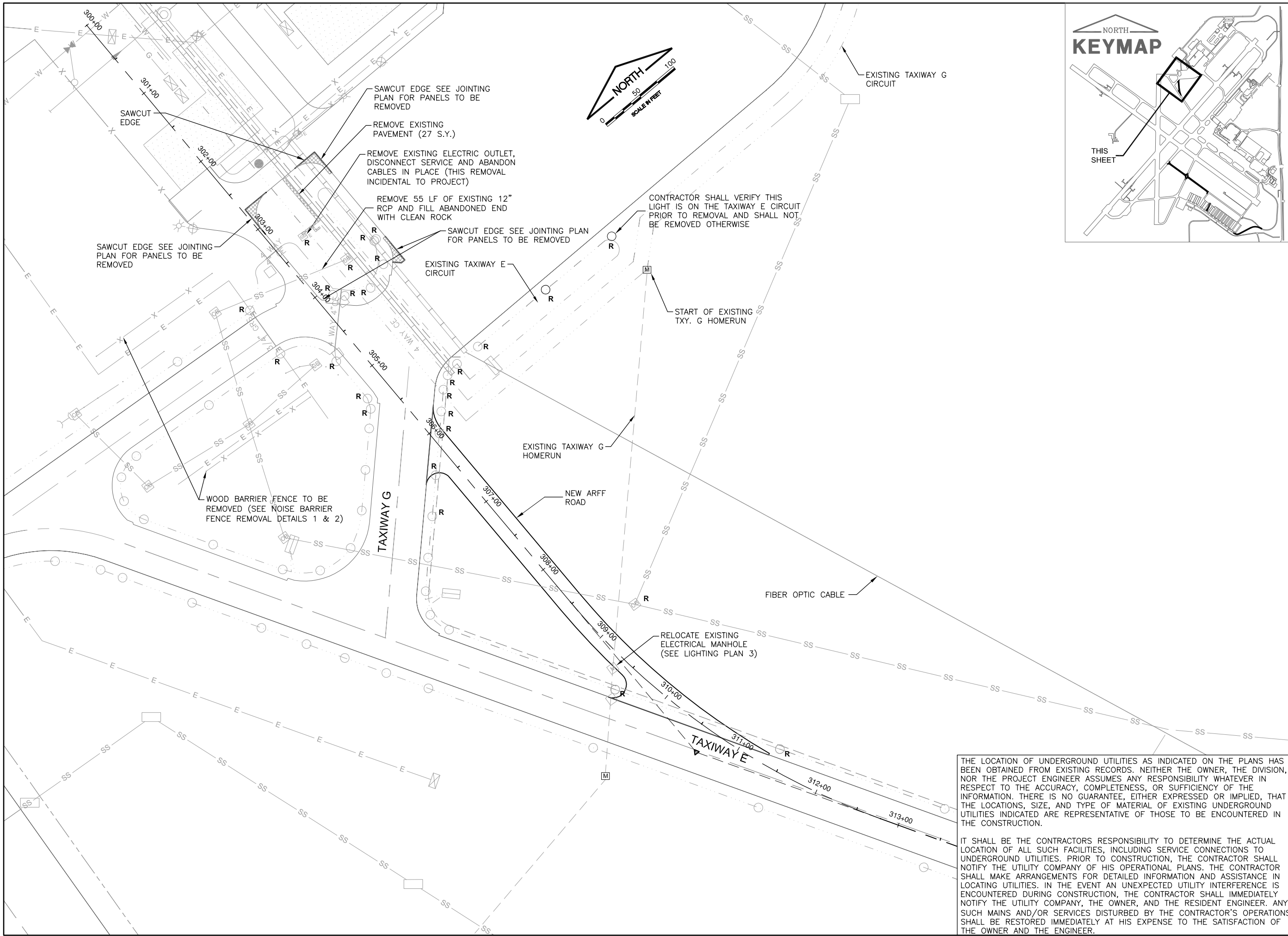
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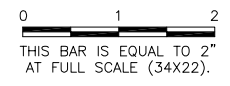

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EXISTING CONDITIONS & REMOVALS 3

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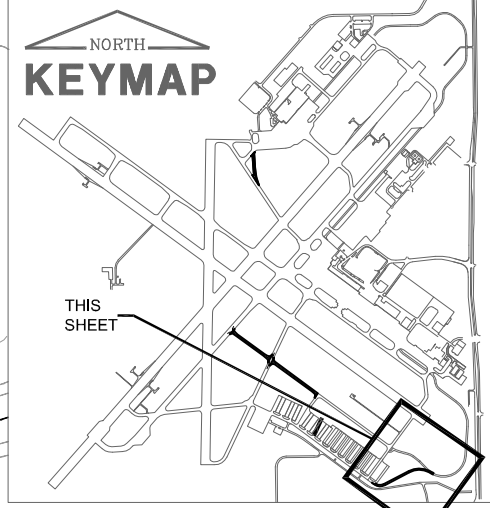
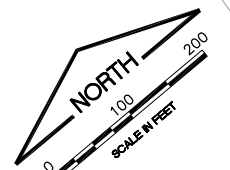
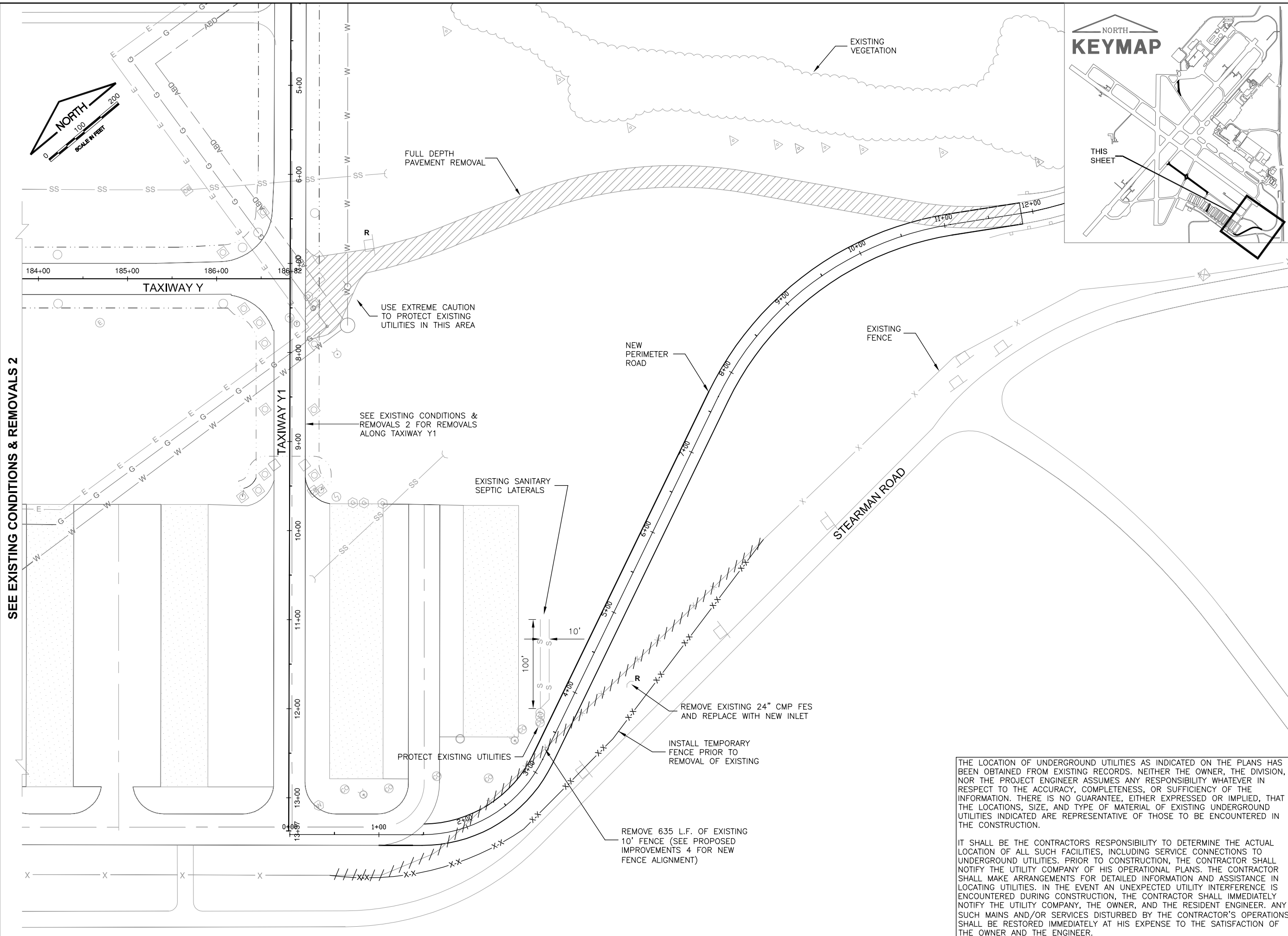


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| DESIGN BY: | RLV/JMW |
| DRAWN BY: | CMT |
| CHECKED BY: | RLV |
| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET 15 OF 106 SHEETS | |

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.



K:\Springfield\1103504\Draw\Sheets
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 PLOT DATE: 5/10/2012 9:04 AM
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 BASE_PROP_GEO
 SPL_UH
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 SPL_1104_ALIGN
 SPI-BASE CURRENT GEOMETRY

CA014

| REVISIONS | | |
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

SEE EXISTING CONDITIONS & REMOVALS 2


TAXIWAY Y

TAXIWAY Y1

STEARMAN ROAD

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y

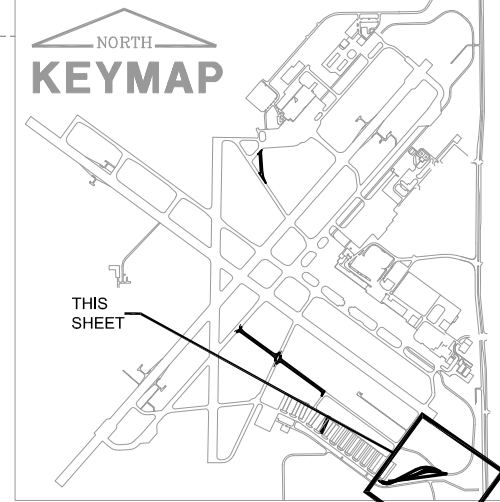
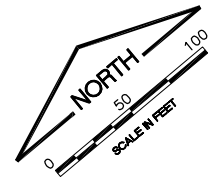
EXISTING CONDITIONS & REMOVALS 4

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| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET 16 OF 106 SHEETS | |



K:\Springfield\1103504\Draw\Sheets
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 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/8/2012 9:05 PM
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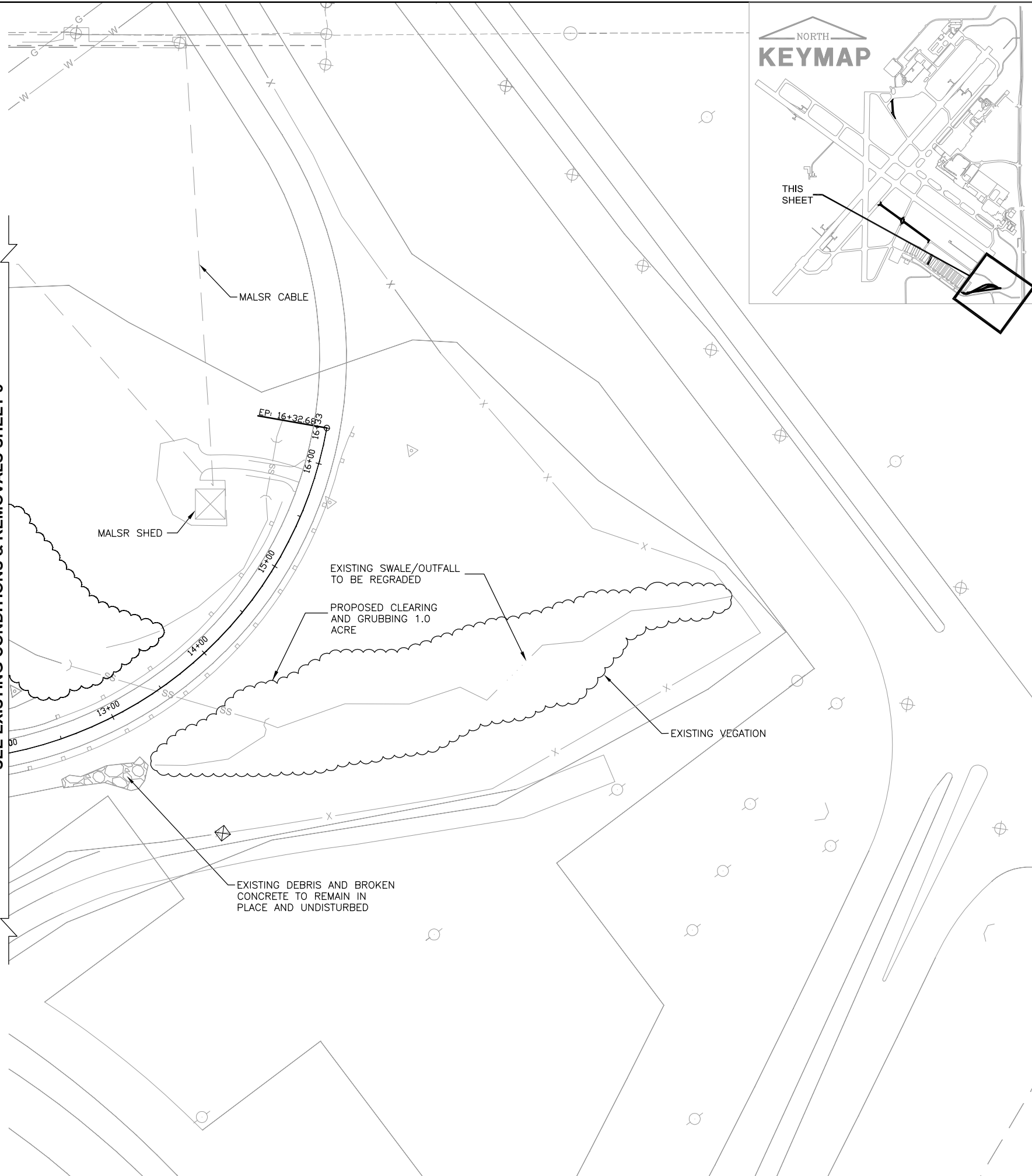
CA014

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| NUMBER | BY | DATE |
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

SEE EXISTING CONDITIONS & REMOVALS SHEET 3



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EXTEND TAXIWAY Y

EXISTING CONDITIONS & REMOVALS 5

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APPROVED BY: RLV

DATE: APRIL 27, 2012

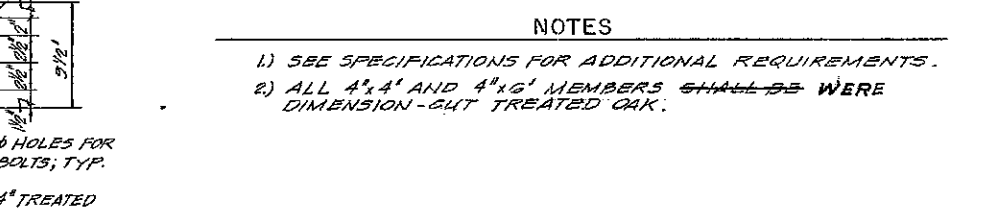
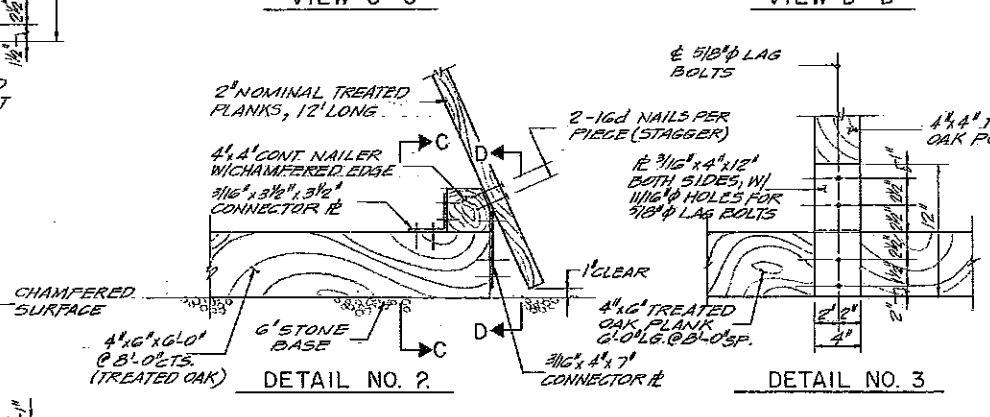
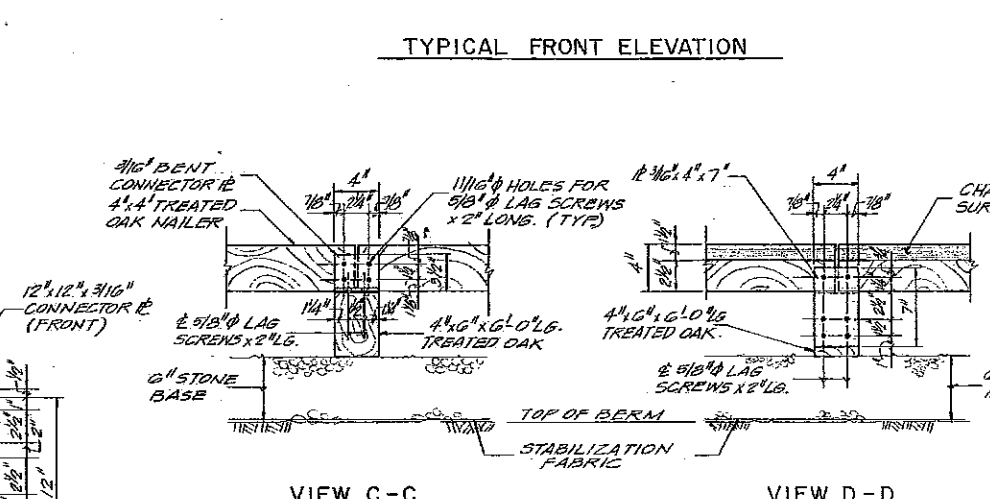
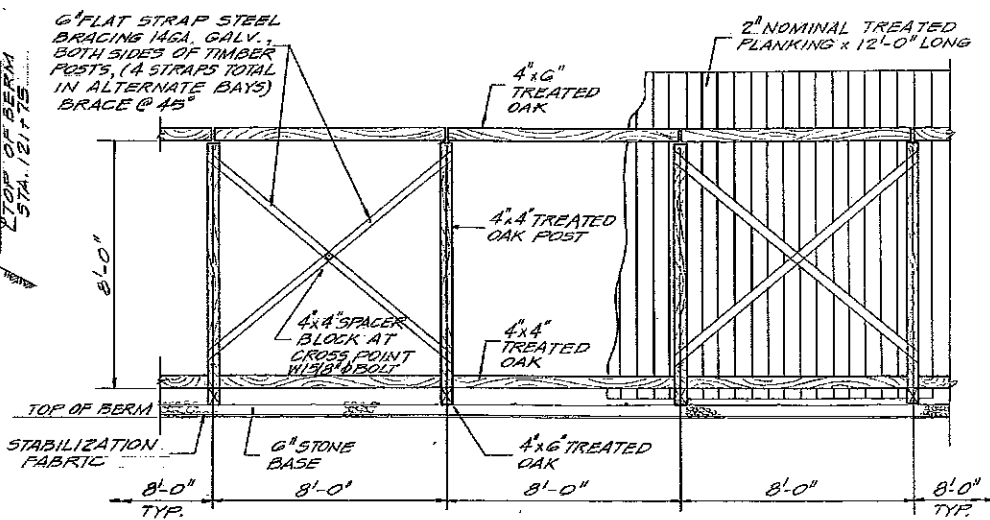
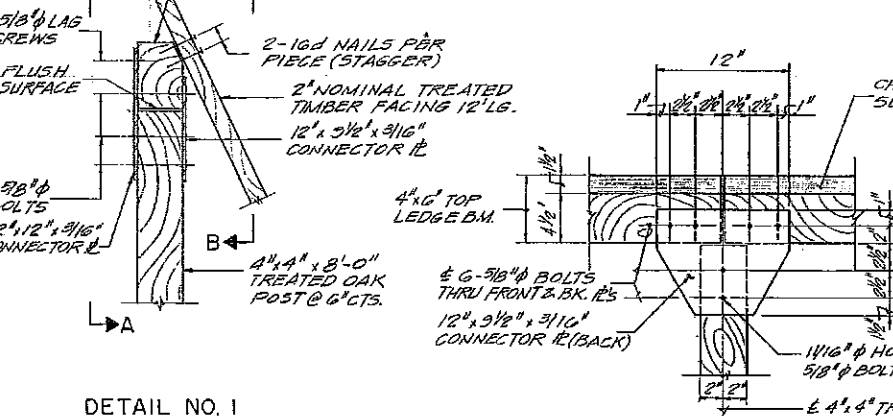
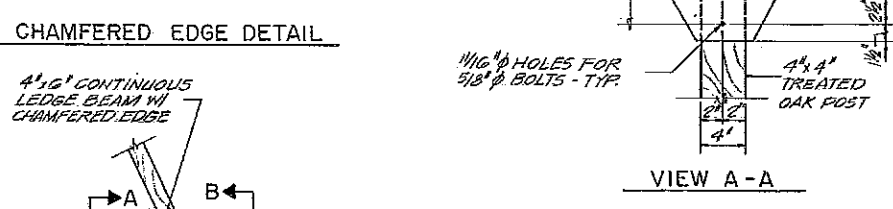
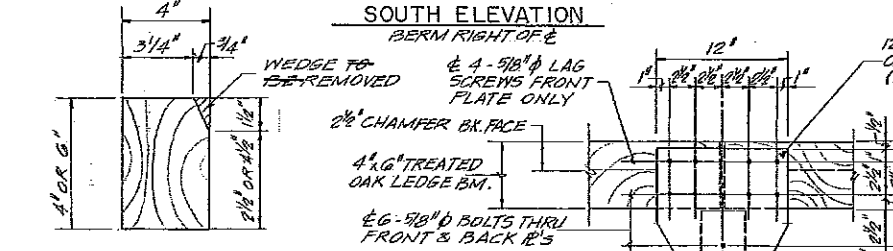
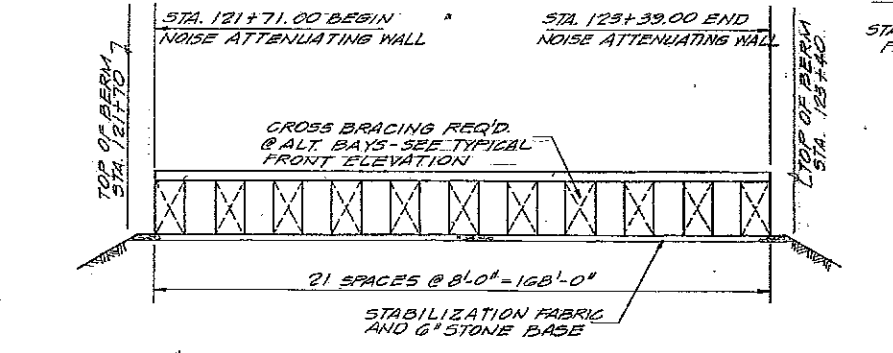
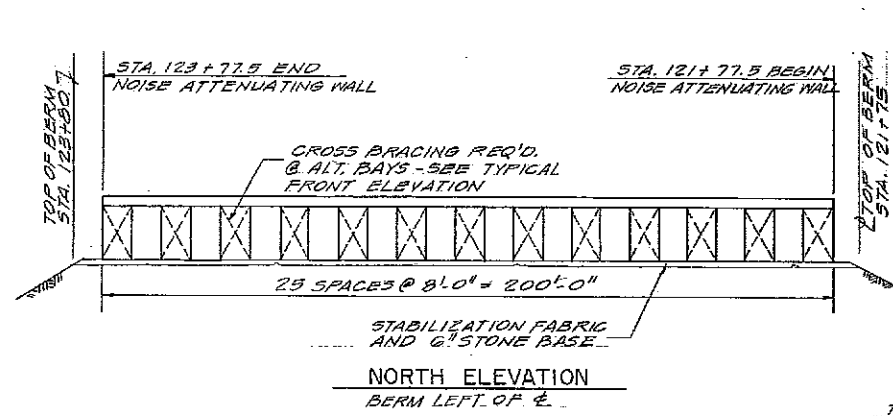
JOB No: 110350400

SPI-4156

3-17-0096-XX

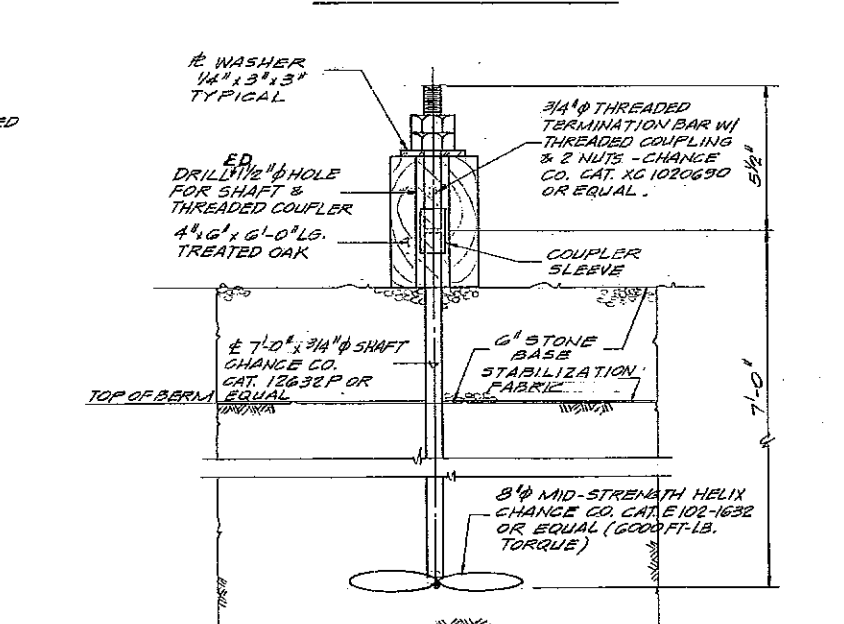
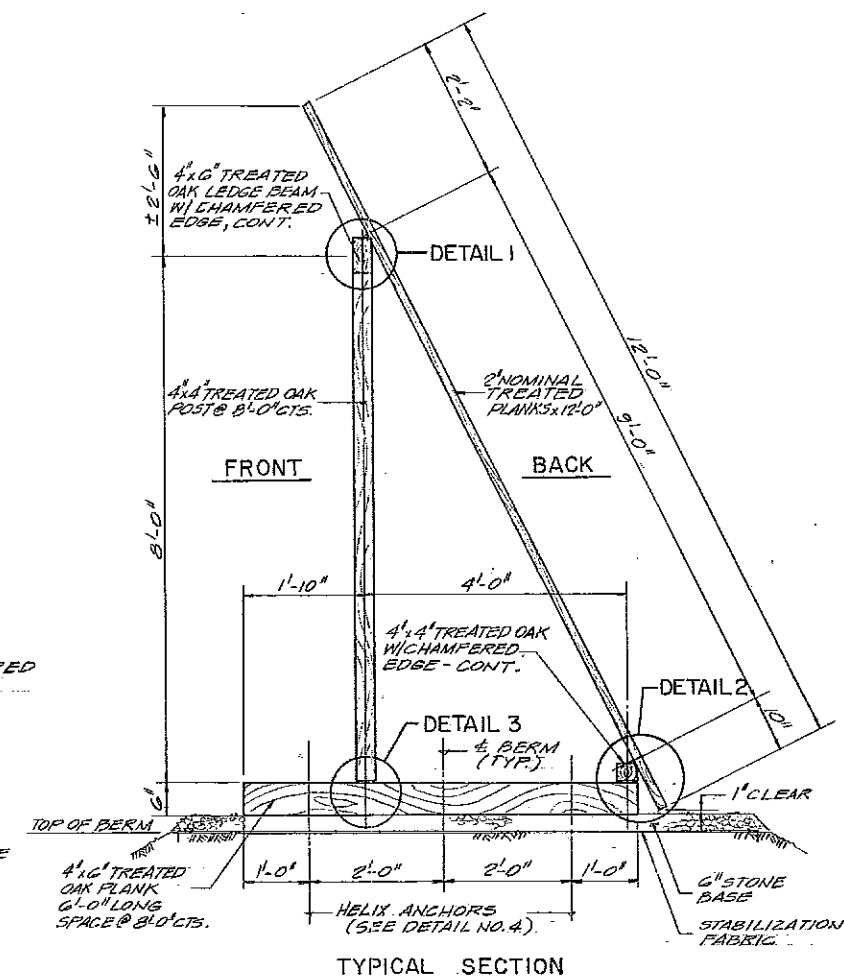
SHEET 17 OF 106 SHEETS

NOTE: REMOVAL OF EXISTING NOISE BARRIER WALLS SHALL INCLUDE THE REMOVAL OF THE EXISTING WALL STRUCTURE, ANCHORS, SUPPORTS, OBSTRUCTION LIGHTING, AND STONE BASE. THE POWER FOR THE OBSTRUCTION LIGHTS ARE TO BE DISCONNECTED AND THE CABLES ABANDONED IN PLACE. THE AREA SHALL BE RE-GRADED SMOOTH.



NOTES

- 1) SEE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
- 2) ALL 4x4 AND 4x6 MEMBERS SHALL BE WERE DIMENSION-GUT TREATED OAK.



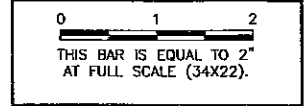
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FOR INFORMATION ONLY

K:\Springfield\1103504\Draw\Sheets
FILE: NOISE BARRIER FENCE REMOVAL
UPDATE BY: TJ Heavysides
PLOT DATE: 5/29/2012 3:24 PM

CA014

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| NUMBER | BY | DATE |
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SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY

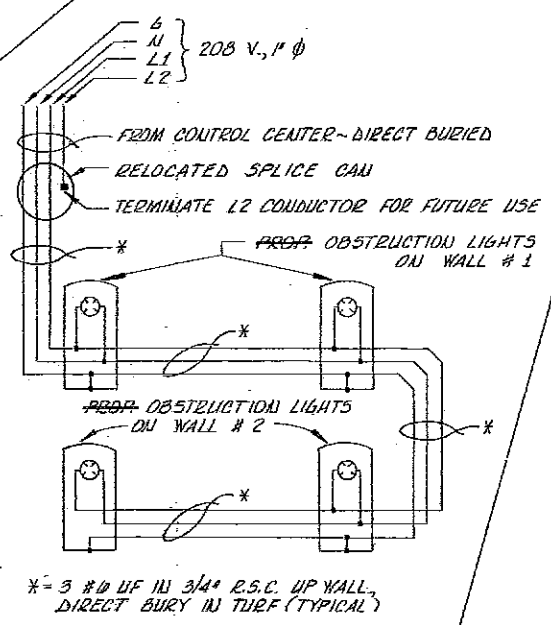
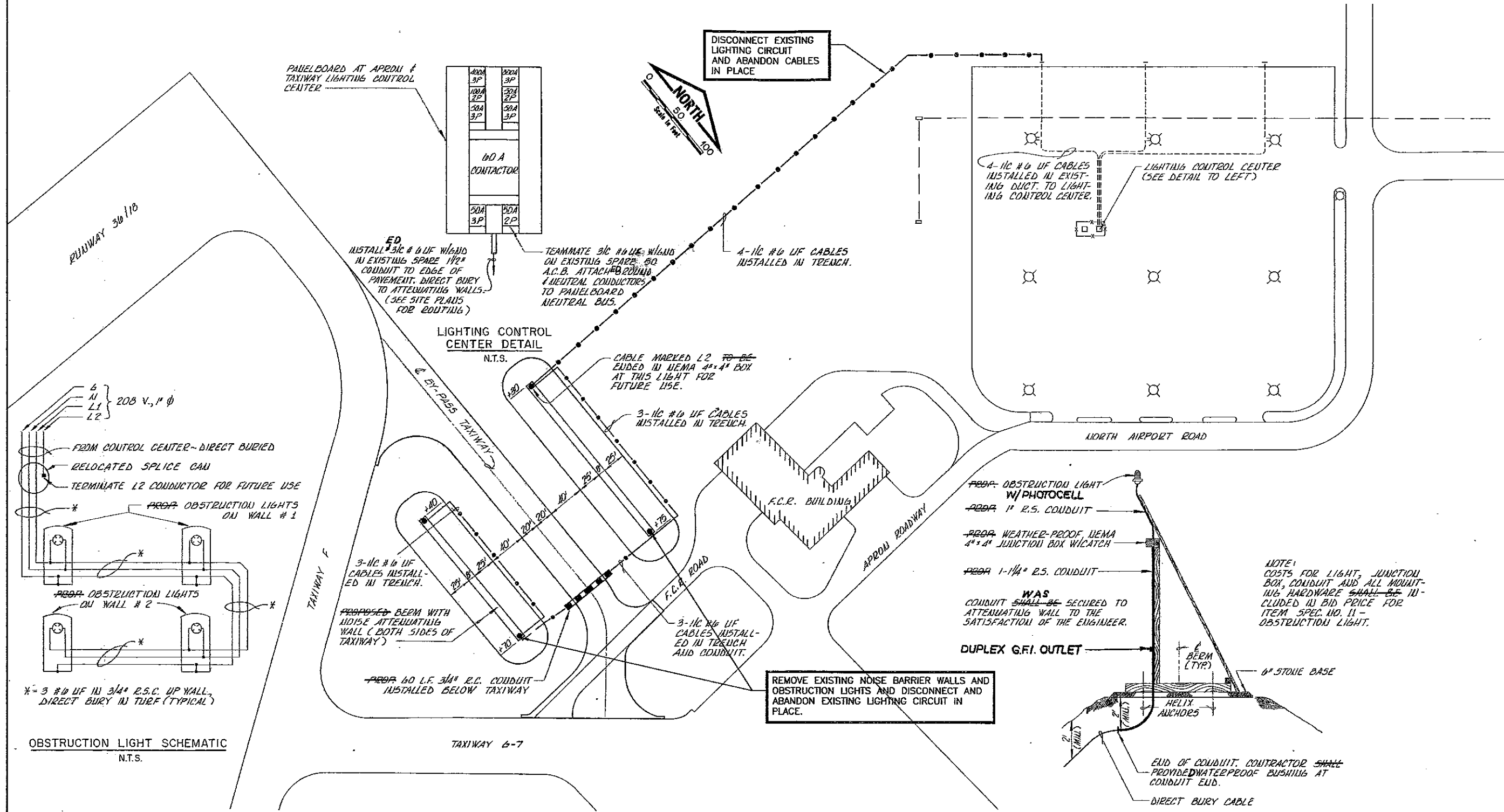
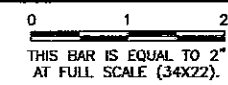
NOISE BARRIER FENCE REMOVAL DETAILS 1

DESIGN BY: RLV/JMW
DRAWN BY: CMT
CHECKED BY: RLV
APPROVED BY: RLV
DATE: APRIL 27, 2012
JOB No: 110350400

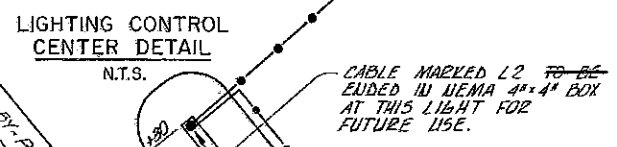
SPI-4156
3-17-0096-XX
SHEET 18 OF 106 SHEETS

CA014

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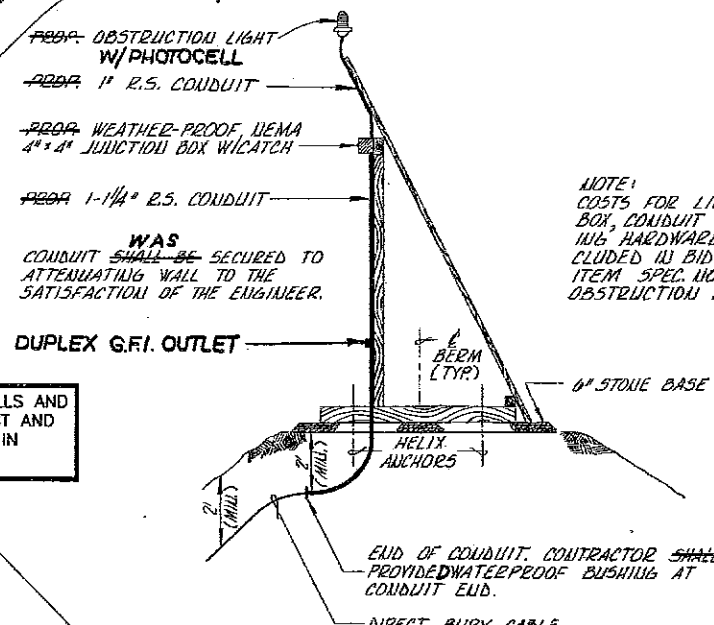
OBSTRUCTION LIGHT SCHEMATIC
N.T.S.



LIGHTING CONTROL CENTER DETAIL
N.T.S.

LEGEND

| | |
|--|----------------------------|
| | PROPOSED LIGHTING CIRCUIT |
| | PROPOSED 3/4" R.S. CONDUIT |
| | PROPOSED OBSTRUCTION LIGHT |



OBSTRUCTION LIGHT AND CONDUIT MOUNTING DETAIL
N.T.S.

FOR INFORMATION ONLY

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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS
 EXTEND TAXIWAY
 NOISE BARRIER FENCE REMOVAL DETAILS 2

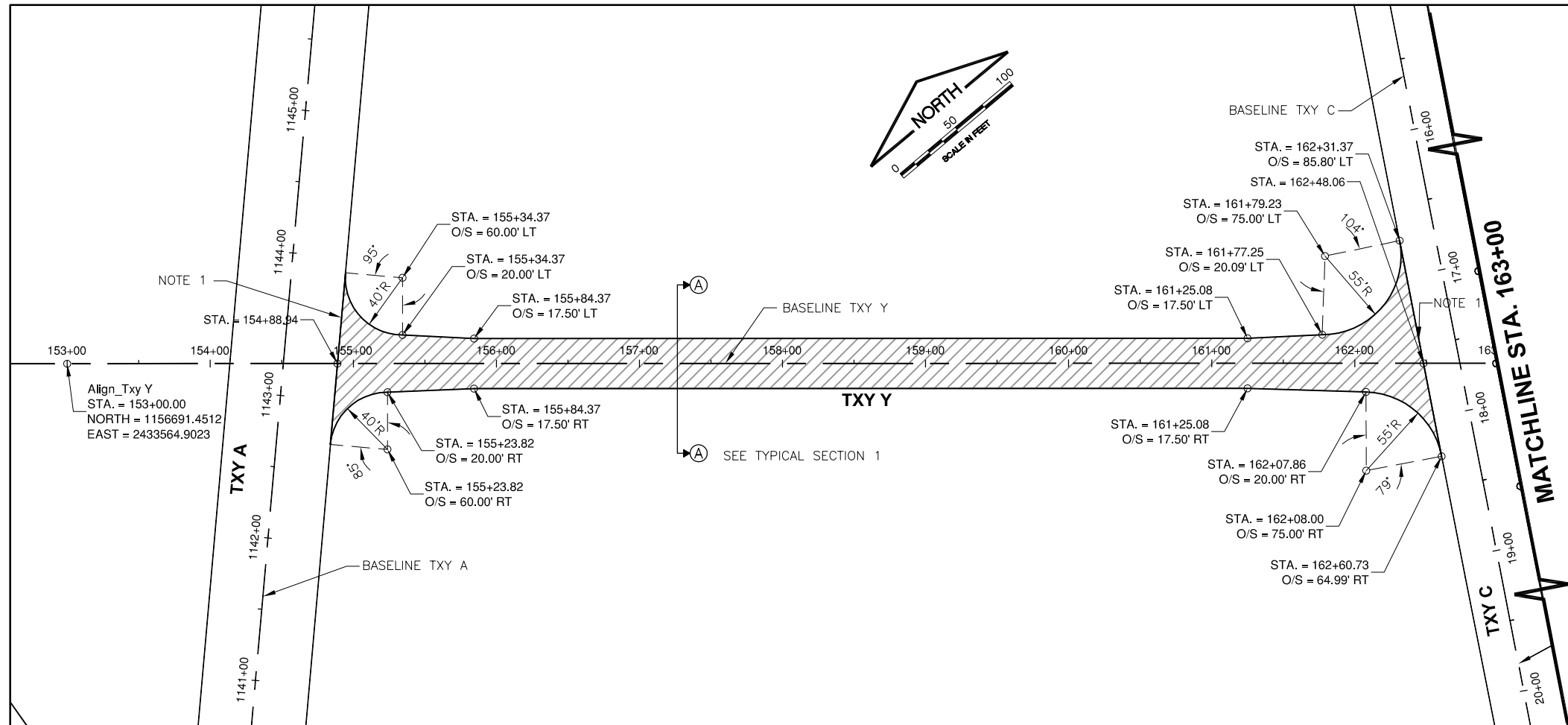
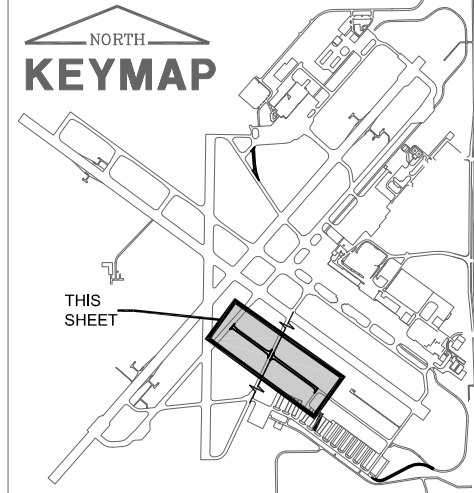
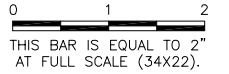
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| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET 19 OF 106 SHEETS | |

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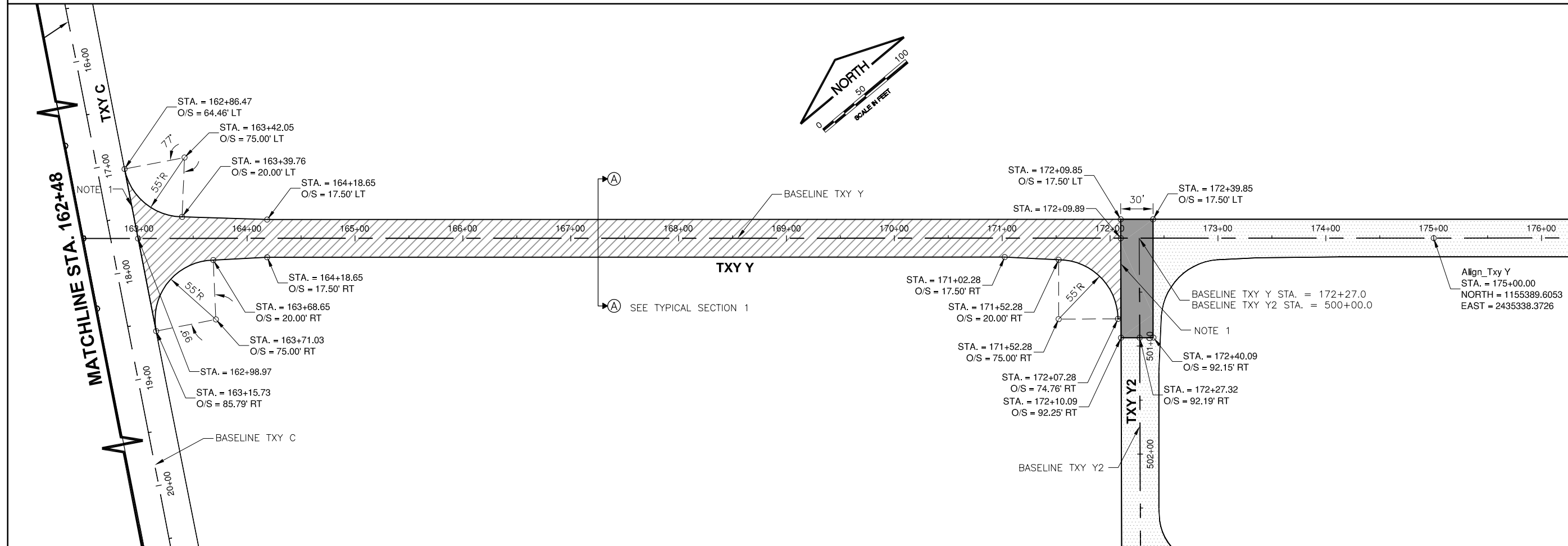
| NUMBER | BY | DATE |
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LEGEND

- NEW 2" BITUMINOUS SURFACE COURSE (401)
- NEW 4" BITUMINOUS BASE COURSE (403)
- NEW 6" CRUSHED AGGREGATE BASE COURSE (209)
- NEW BUTT JOINT CONSTRUCTION (401)
- EXISTING 2" BITUMINOUS SURFACE COURSE (401)
- EXISTING 4" BITUMINOUS BASE COURSE (201)
- EXISTING 6" CRUSHED AGGREGATE BASE COURSE (209)

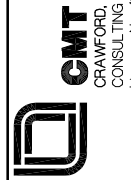
NOTE 1: SAWCUT EDGE FULL DEPTH OF PAVEMENT TO PROVIDE A CLEAN EDGE TO CONSTRUCT NEW PAVEMENT. EDGE SHALL BE TACK COATED PRIOR TO NEW PAVEMENT CONSTRUCTION.



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EXTEND TAXIWAY Y
 PROPOSED IMPROVEMENTS 1

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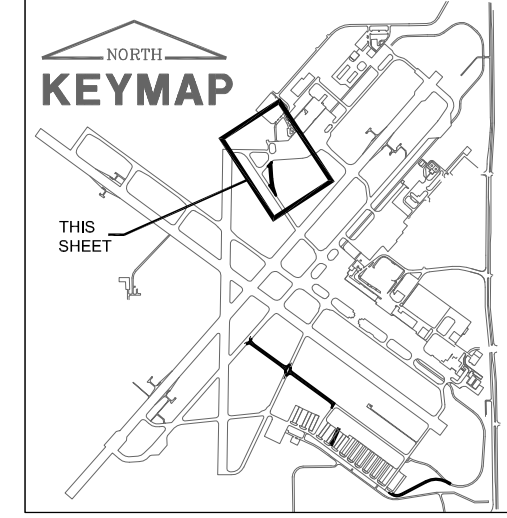
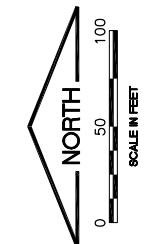
DESIGN BY: RLV/JMW
 DRAWN BY: CMT
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: APRIL 27, 2012
 JOB No: 110350400

SPI-4156
 3-17-0096-XX

LEGEND

| | |
|--|---|
| | NEW 2" BITUMINOUS SURFACE COURSE (401) |
| | NEW 4" BITUMINOUS BASE COURSE (403) |
| | NEW 12" CRUSHED AGGREGATE BASE COURSE (209) |
| | NEW 10" PCC PAVEMENT (501) |
| | NEW 4" CRUSHED AGGREGATE BASE COURSE (209) |

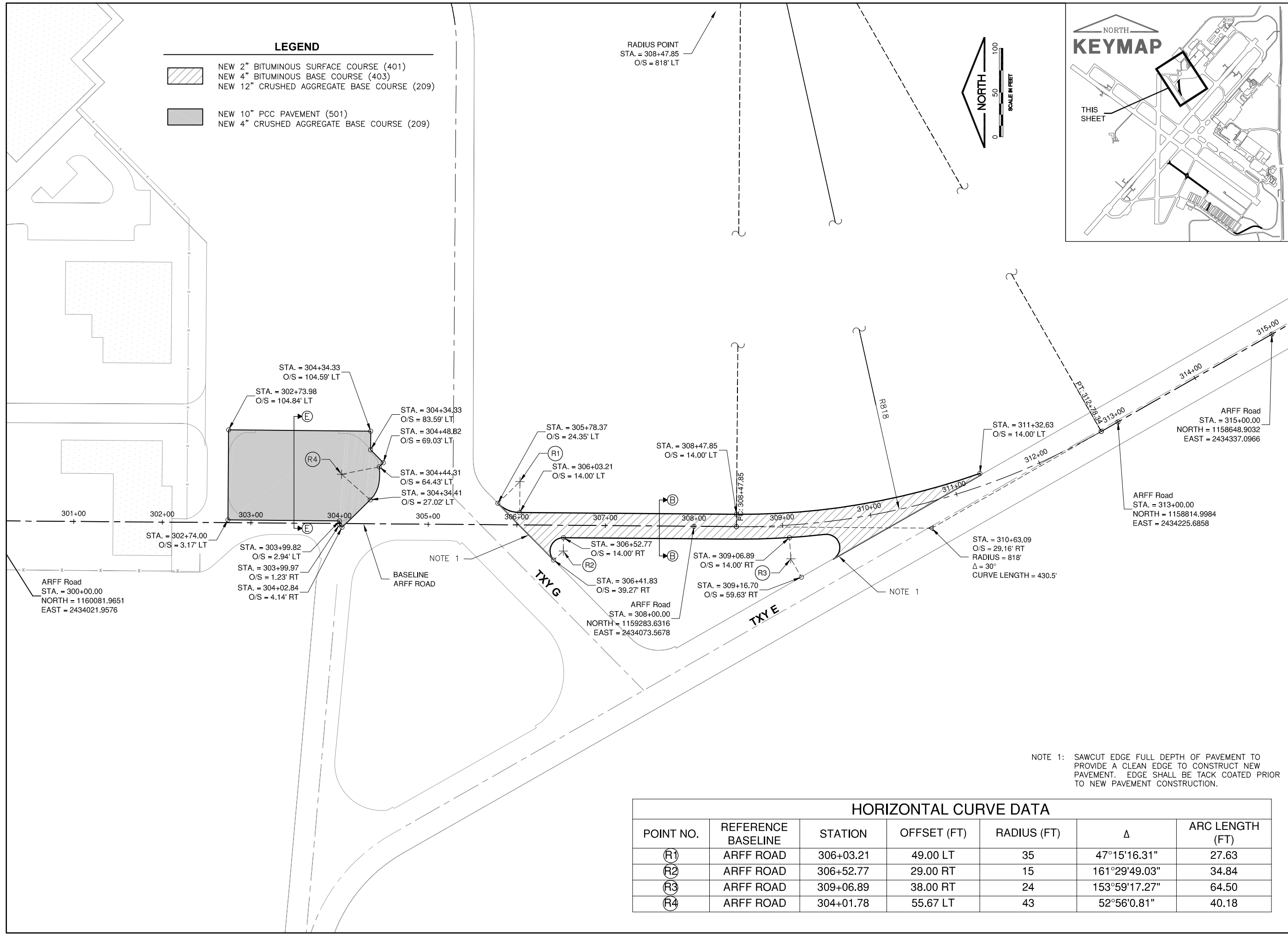
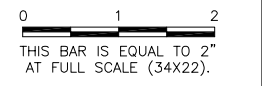

RADIUS POINT
STA. = 308+47.85
O/S = 818' LT



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UPDATE BY: TJ Heavisides
PLOT DATE: 5/8/2012 9:06 PM
SPL-BASE CURRENT GEOMETRY
KEYMAP
1103504-C-SPAL
BASE_PROP_GEO
1103504-V-VF2D

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SPRINGFIELD, ILLINOIS


EXTEND TAXIWAY Y

PROPOSED IMPROVEMENTS 2

NOTE 1: SAWCUT EDGE FULL DEPTH OF PAVEMENT TO PROVIDE A CLEAN EDGE TO CONSTRUCT NEW PAVEMENT. EDGE SHALL BE TACK COATED PRIOR TO NEW PAVEMENT CONSTRUCTION.

| HORIZONTAL CURVE DATA | | | | | | |
|-----------------------|--------------------|-----------|-------------|-------------|---------------|-----------------|
| POINT NO. | REFERENCE BASELINE | STATION | OFFSET (FT) | RADIUS (FT) | Δ | ARC LENGTH (FT) |
| (R1) | ARFF ROAD | 306+03.21 | 49.00 LT | 35 | 47°15'16.31" | 27.63 |
| (R2) | ARFF ROAD | 306+52.77 | 29.00 RT | 15 | 161°29'49.03" | 34.84 |
| (R3) | ARFF ROAD | 309+06.89 | 38.00 RT | 24 | 153°59'17.27" | 64.50 |
| (R4) | ARFF ROAD | 304+01.78 | 55.67 LT | 43 | 52°56'0.81" | 40.18 |

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CHECKED BY: **RLV**
APPROVED BY: **RLV**
DATE: APRIL 27, 2012
JOB No: 110350400

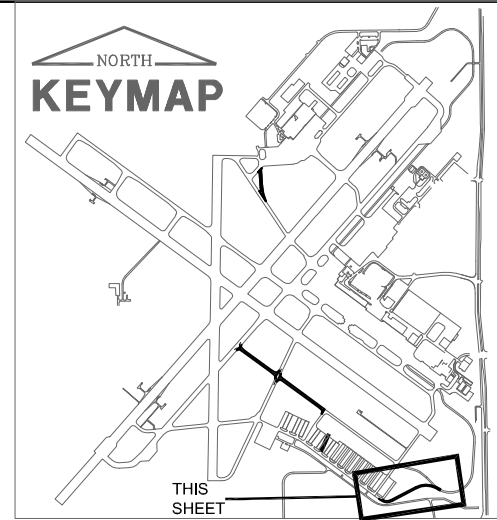
SPI-4156
3-17-0096-XX

SHEET 21 OF 106 SHEETS

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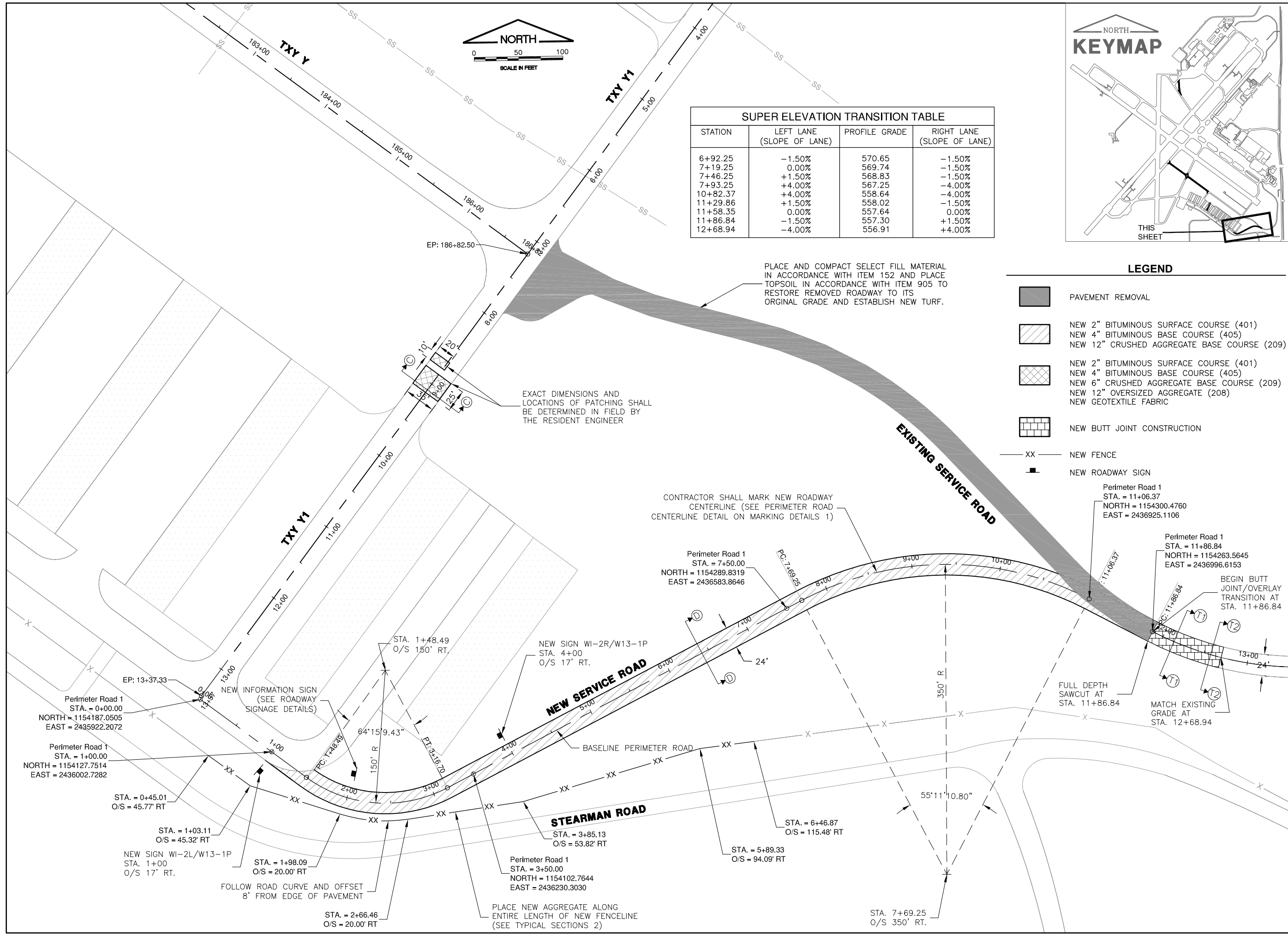
| REVISIONS | | |
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| NUMBER | BY | DATE |
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



| STATION | LEFT LANE (SLOPE OF LANE) | PROFILE GRADE | RIGHT LANE (SLOPE OF LANE) |
|----------|------------------------------|---------------|-------------------------------|
| 6+92.25 | -1.50% | 570.65 | -1.50% |
| 7+19.25 | 0.00% | 569.74 | -1.50% |
| 7+46.25 | +1.50% | 568.83 | -1.50% |
| 7+93.25 | +4.00% | 567.25 | -4.00% |
| 10+82.37 | +4.00% | 558.64 | -4.00% |
| 11+29.86 | +1.50% | 558.02 | -1.50% |
| 11+58.35 | 0.00% | 557.64 | 0.00% |
| 11+86.84 | -1.50% | 557.30 | +1.50% |
| 12+68.94 | -4.00% | 556.91 | +4.00% |

- LEGEND**
- PAVEMENT REMOVAL
 - NEW 2" BITUMINOUS SURFACE COURSE (401)
NEW 4" BITUMINOUS BASE COURSE (405)
NEW 12" CRUSHED AGGREGATE BASE COURSE (209)
 - NEW 2" BITUMINOUS SURFACE COURSE (401)
NEW 4" BITUMINOUS BASE COURSE (405)
NEW 6" CRUSHED AGGREGATE BASE COURSE (209)
NEW 12" OVERSIZED AGGREGATE (208)
NEW GEOTEXTILE FABRIC
 - NEW BUTT JOINT CONSTRUCTION
 - NEW FENCE
 - NEW ROADWAY SIGN



PLACE AND COMPACT SELECT FILL MATERIAL
 IN ACCORDANCE WITH ITEM 152 AND PLACE
 TOPSOIL IN ACCORDANCE WITH ITEM 905 TO
 RESTORE REMOVED ROADWAY TO ITS
 ORIGINAL GRADE AND ESTABLISH NEW TURF.

EXACT DIMENSIONS AND
 LOCATIONS OF PATCHING SHALL
 BE DETERMINED IN FIELD BY
 THE RESIDENT ENGINEER

CONTRACTOR SHALL MARK NEW ROADWAY
 CENTERLINE (SEE PERIMETER ROAD
 CENTERLINE DETAIL ON MARKING DETAILS 1)

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SPRINGFIELD, ILLINOIS
EXTEND TAXIWAY Y
PROPOSED IMPROVEMENTS 3

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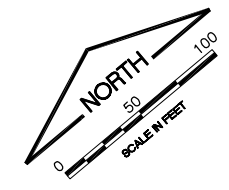
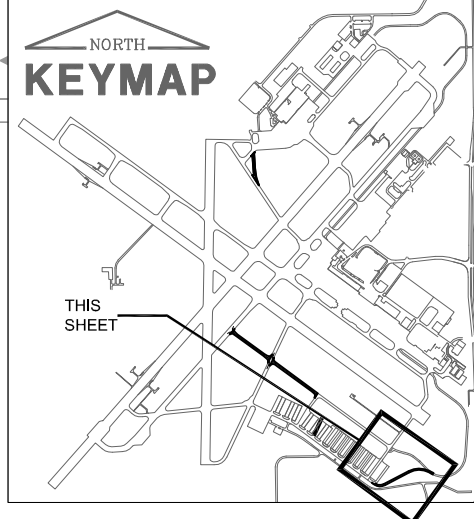
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| SPI-4156 3-17-0096-XX | |
| SHEET 22 OF 106 SHEETS | |

CA014

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

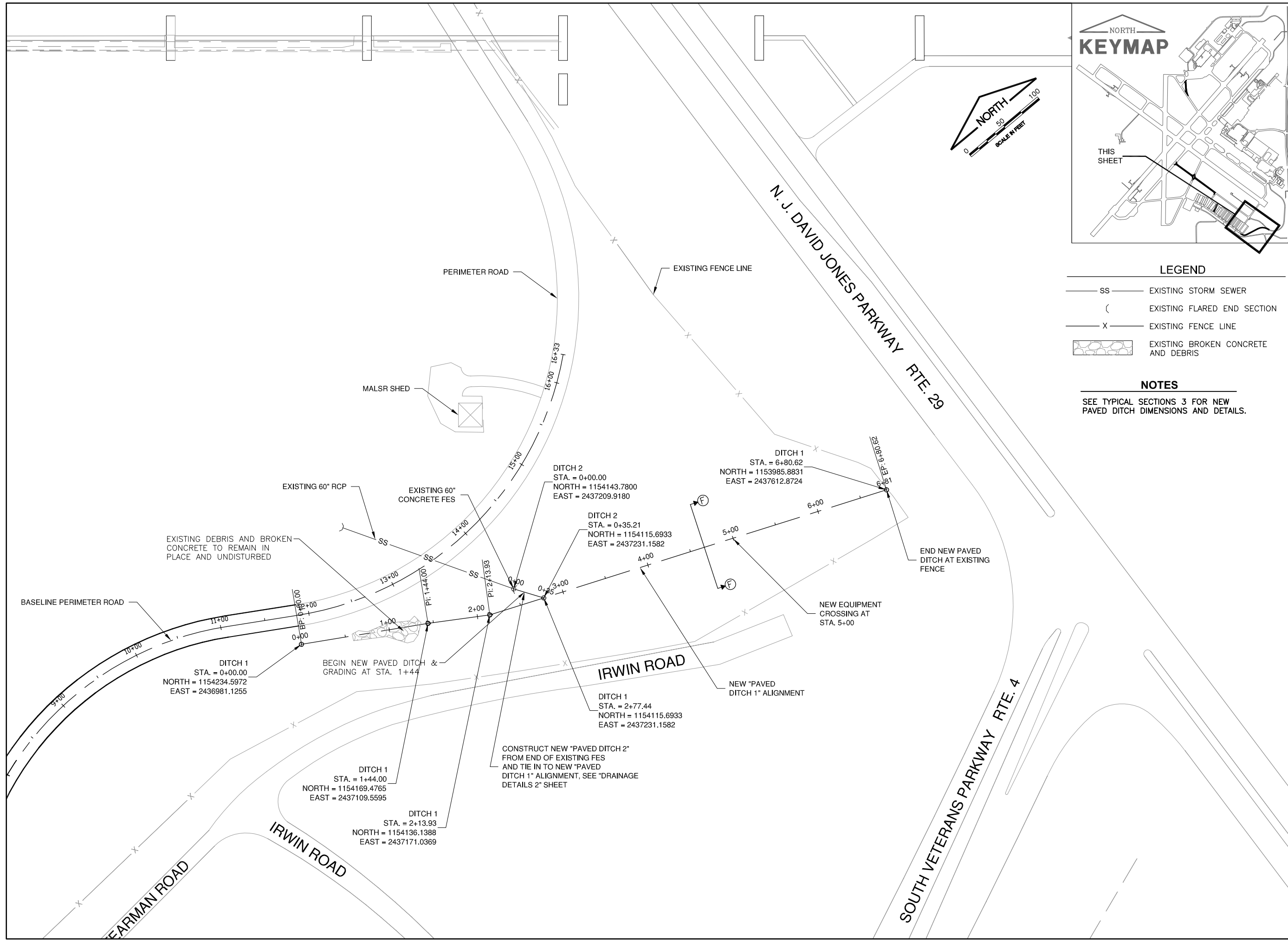



LEGEND

- SS — EXISTING STORM SEWER
- () EXISTING FLARED END SECTION
- X — EXISTING FENCE LINE
- EXISTING BROKEN CONCRETE AND DEBRIS

NOTES

SEE TYPICAL SECTIONS 3 FOR NEW PAVED DITCH DIMENSIONS AND DETAILS.






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SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y

PROPOSED IMPROVEMENTS 4

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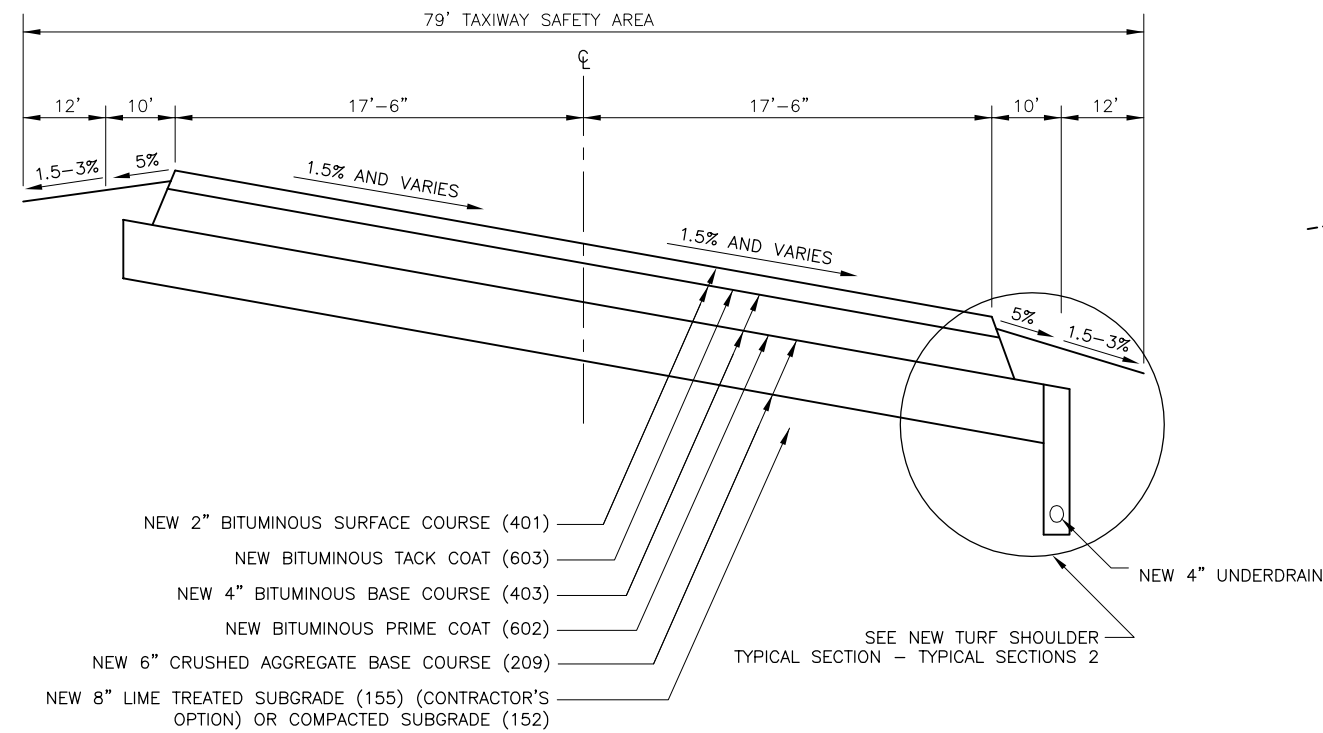
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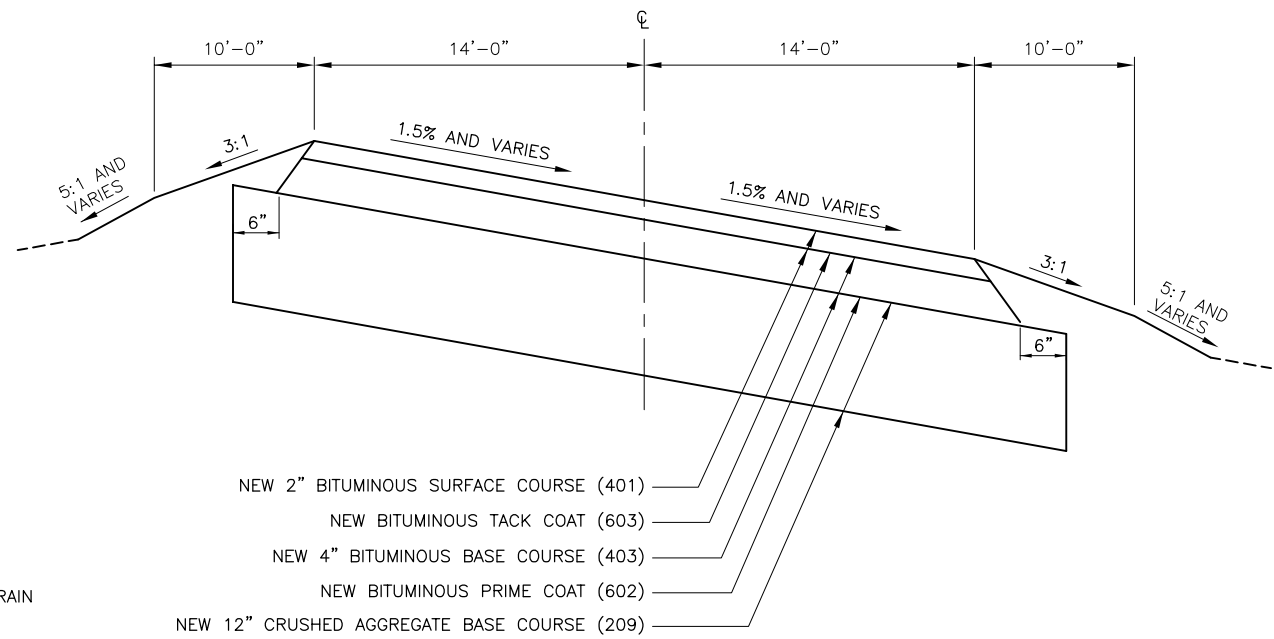
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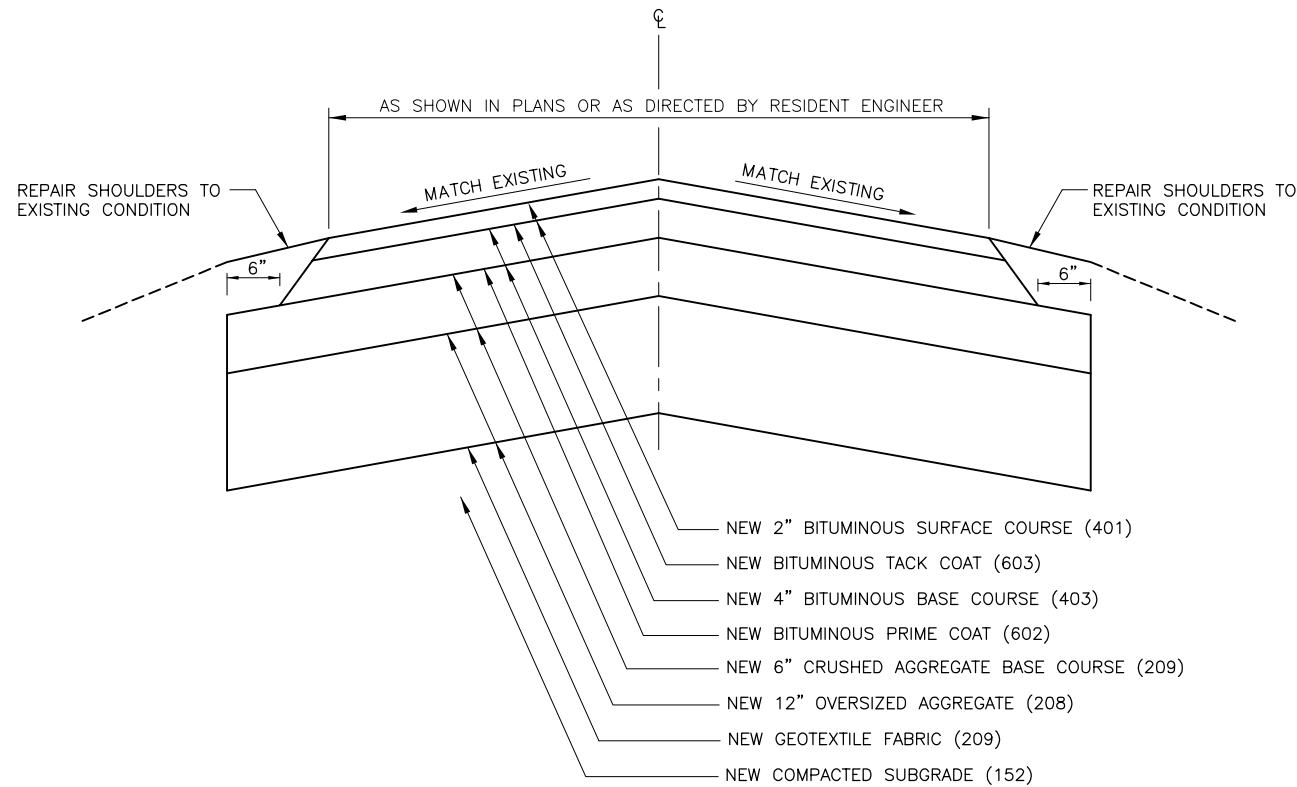
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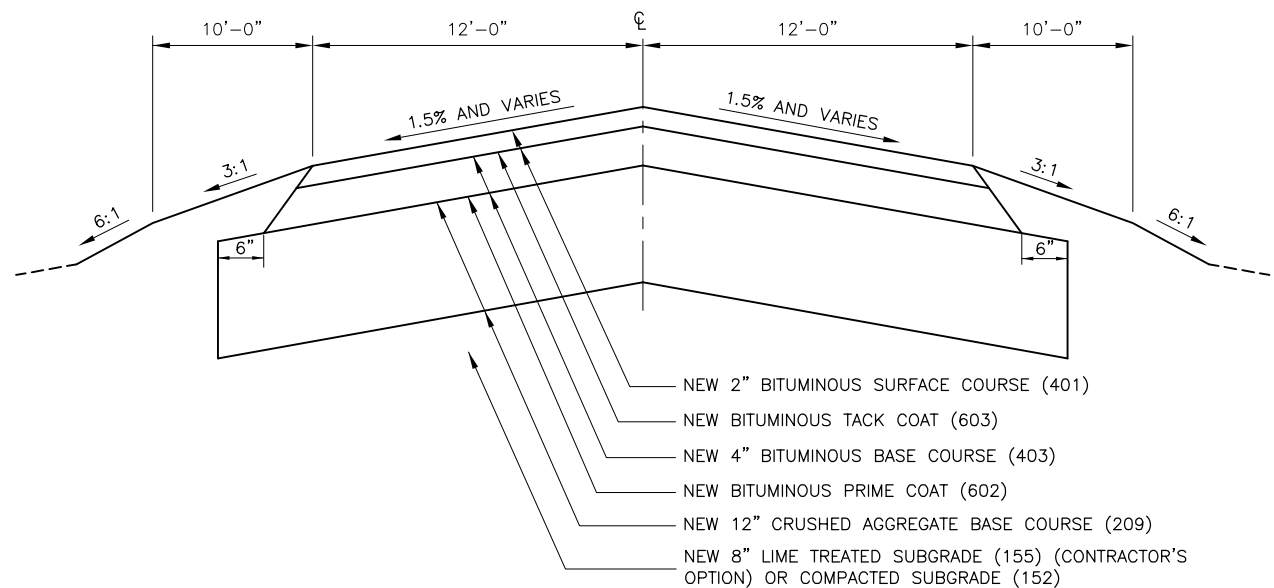
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 N.T.S.



(B) NEW ARFF ROAD
 N.T.S.



(C) TAXIWAY Y1 REPAIR
 N.T.S.



(D) NEW PERIMETER ROAD REALIGNMENT
 N.T.S.



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SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y
TYPICAL SECTIONS 1

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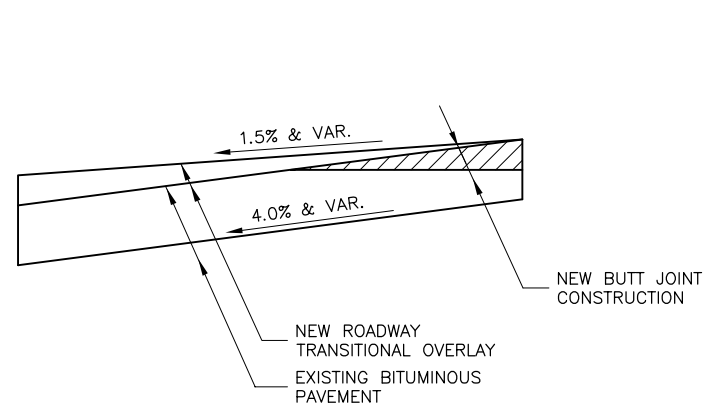
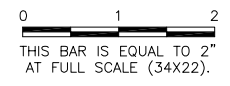
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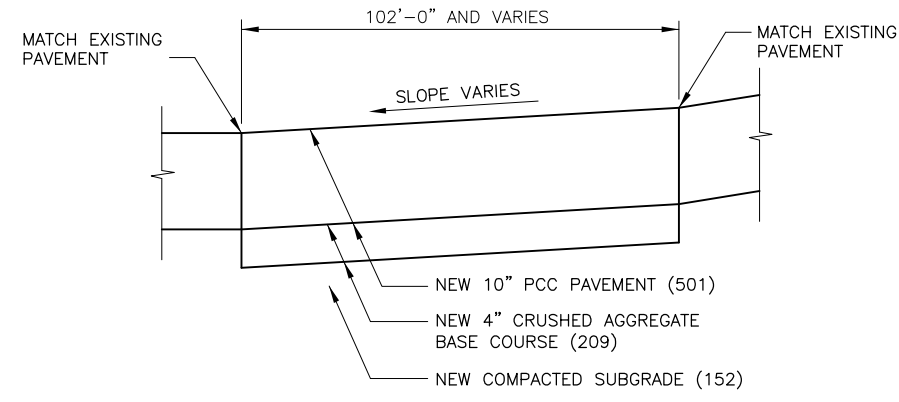
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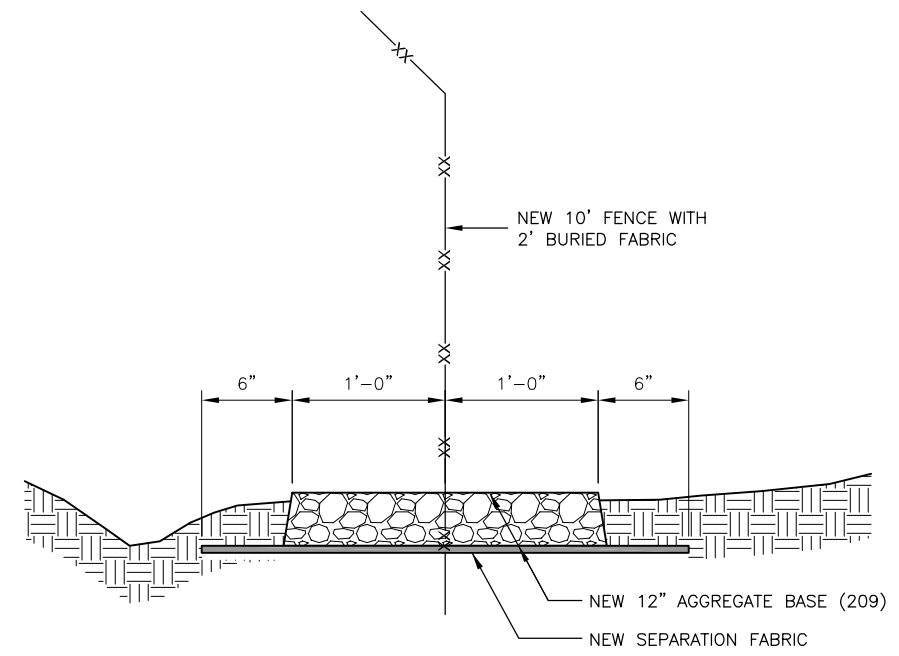
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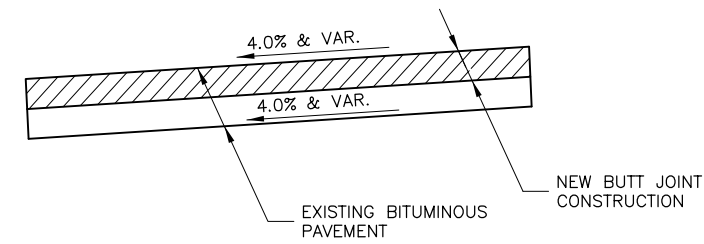
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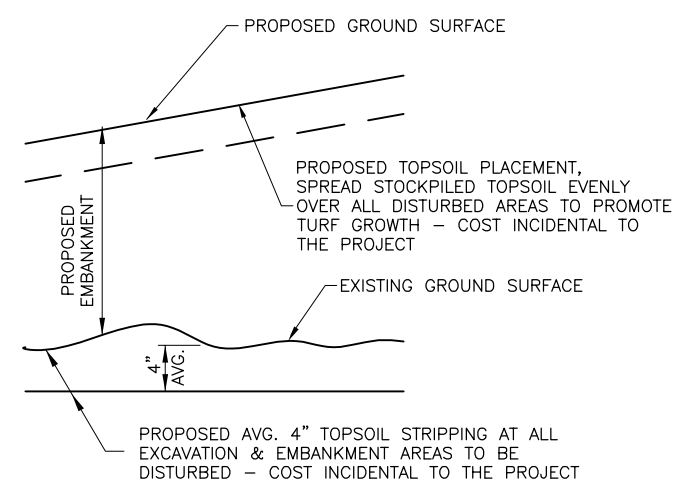
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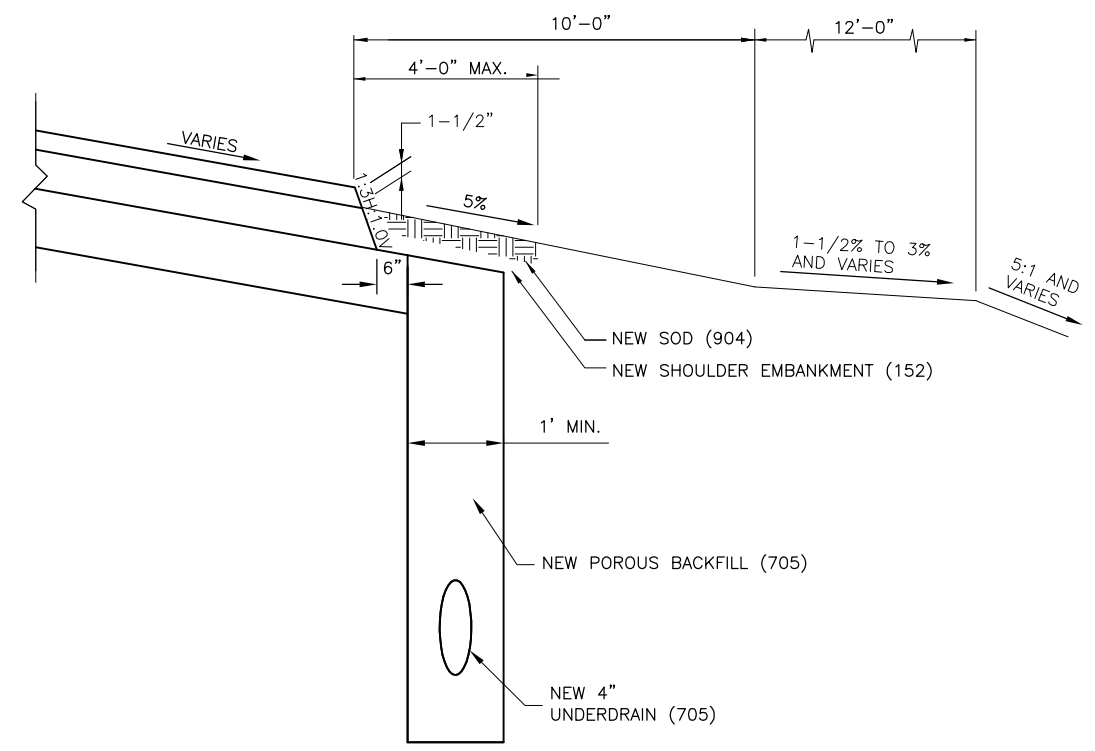
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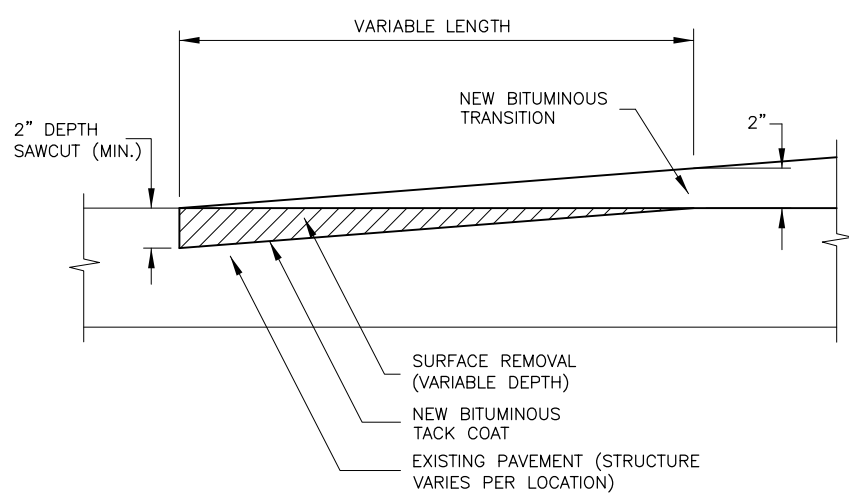
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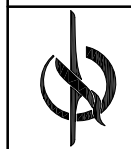
TOPSOIL STRIPPING DETAIL
N.T.S.



NEW TURF SHOULDER - TYPICAL SECTION
N.T.S.



BUTT JOINT DETAIL
N.T.S.



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EXTEND TAXIWAY Y

TYPICAL SECTIONS 2

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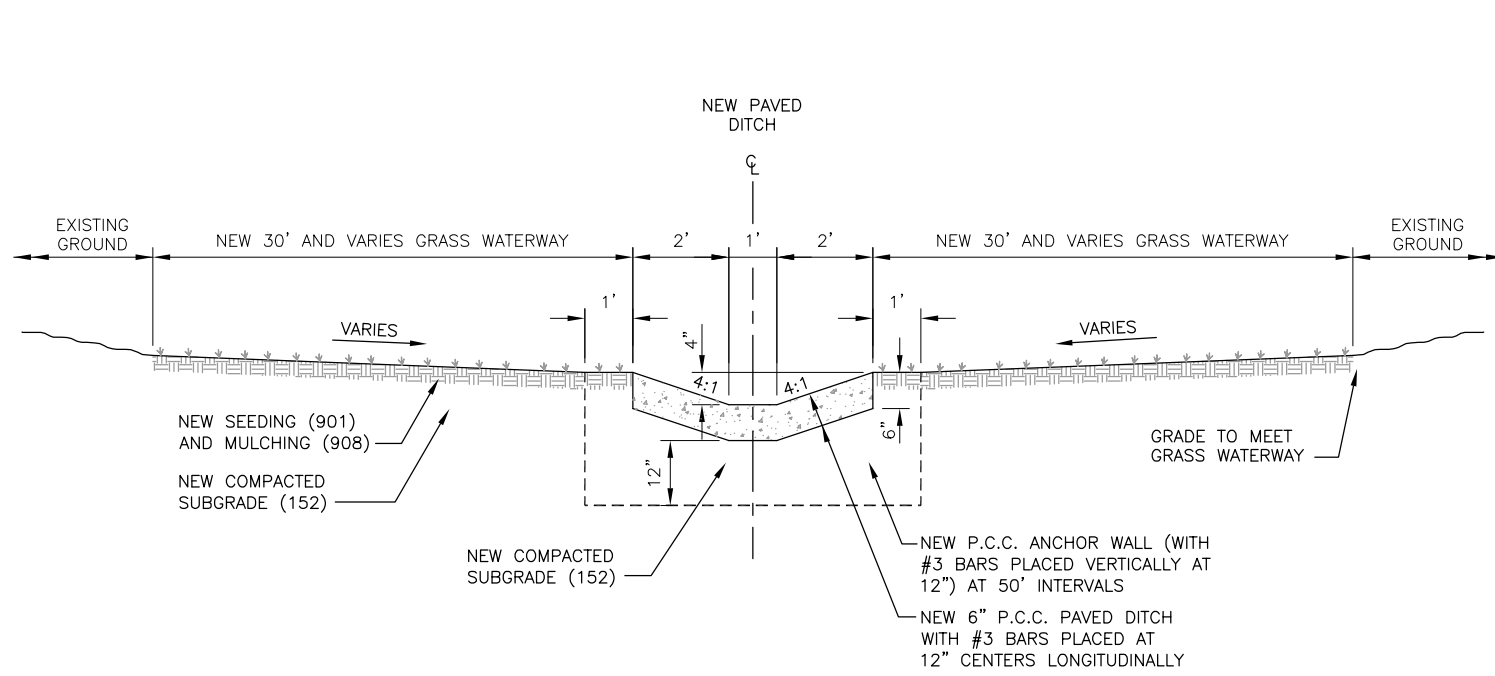
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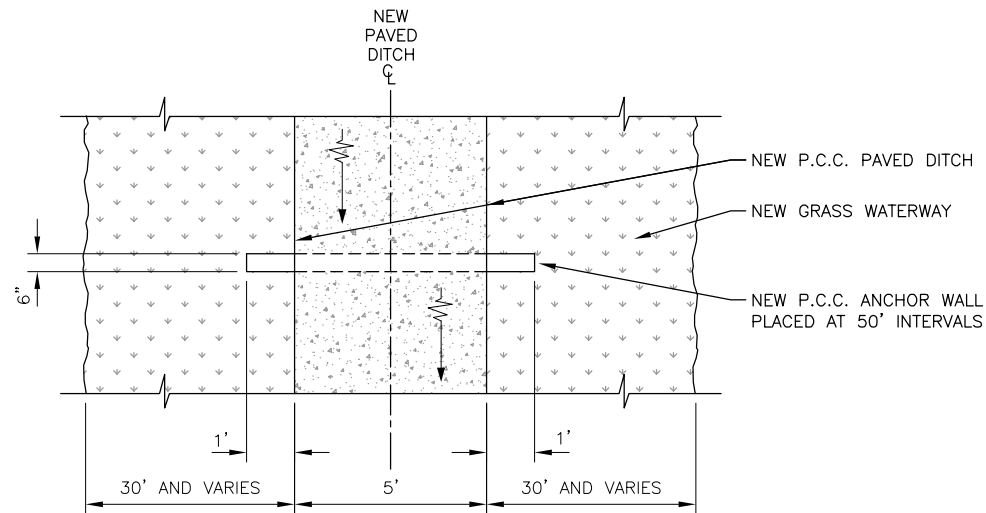
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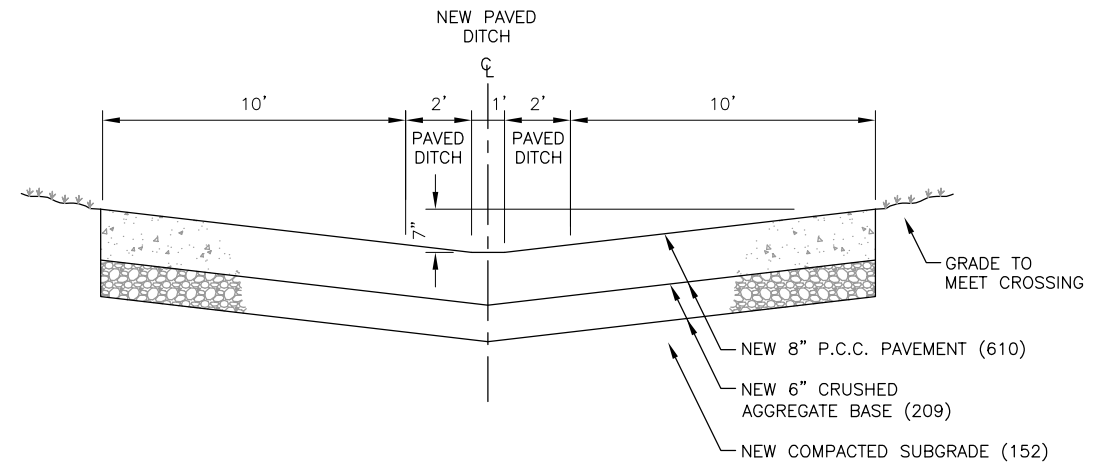
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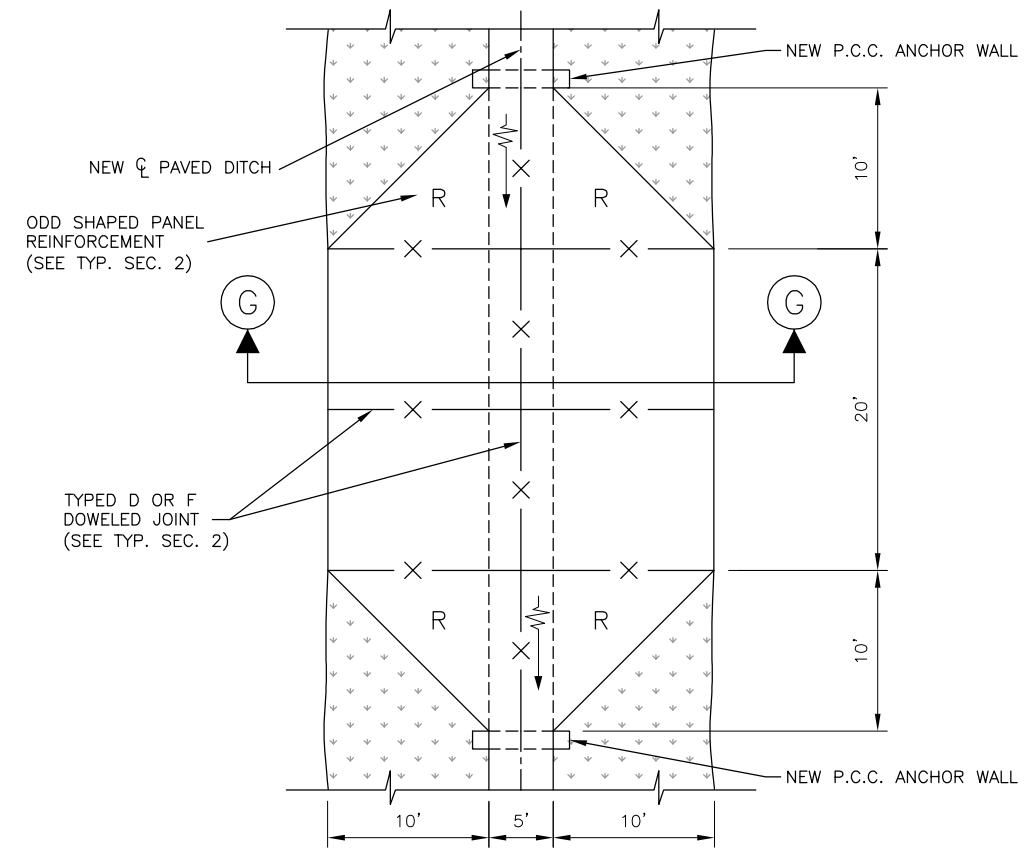
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 TYPICAL SECTION**
 N.T.S.



**PAVED DITCH AND GRASS WATERWAY
 PLAN VIEW**
 N.T.S.



PAVED DITCH EQUIPMENT CROSSING SECTION
 N.T.S.



PAVED DITCH EQUIPMENT CROSSING
 N.T.S.



SPRINGFIELD AIRPORT AUTHORITY
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EXTEND TAXIWAY Y

TYPICAL SECTIONS 3

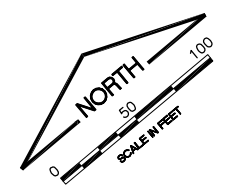
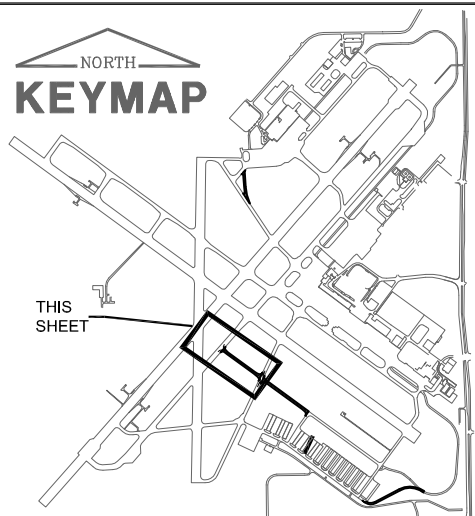
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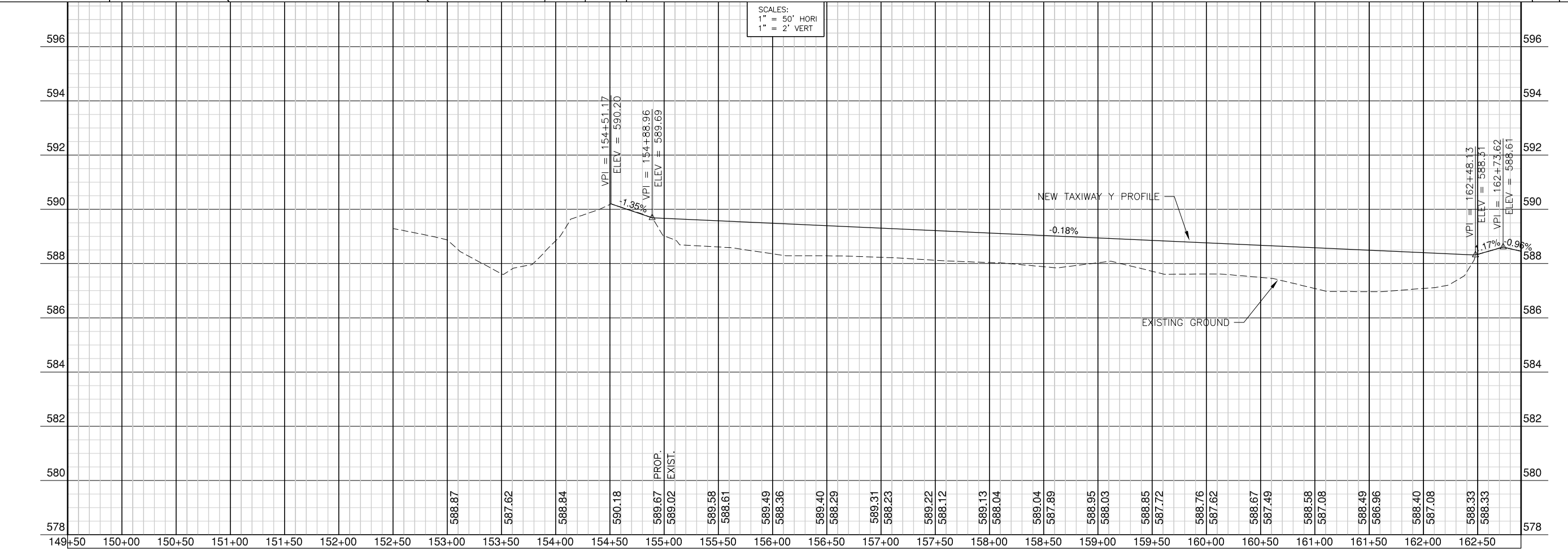
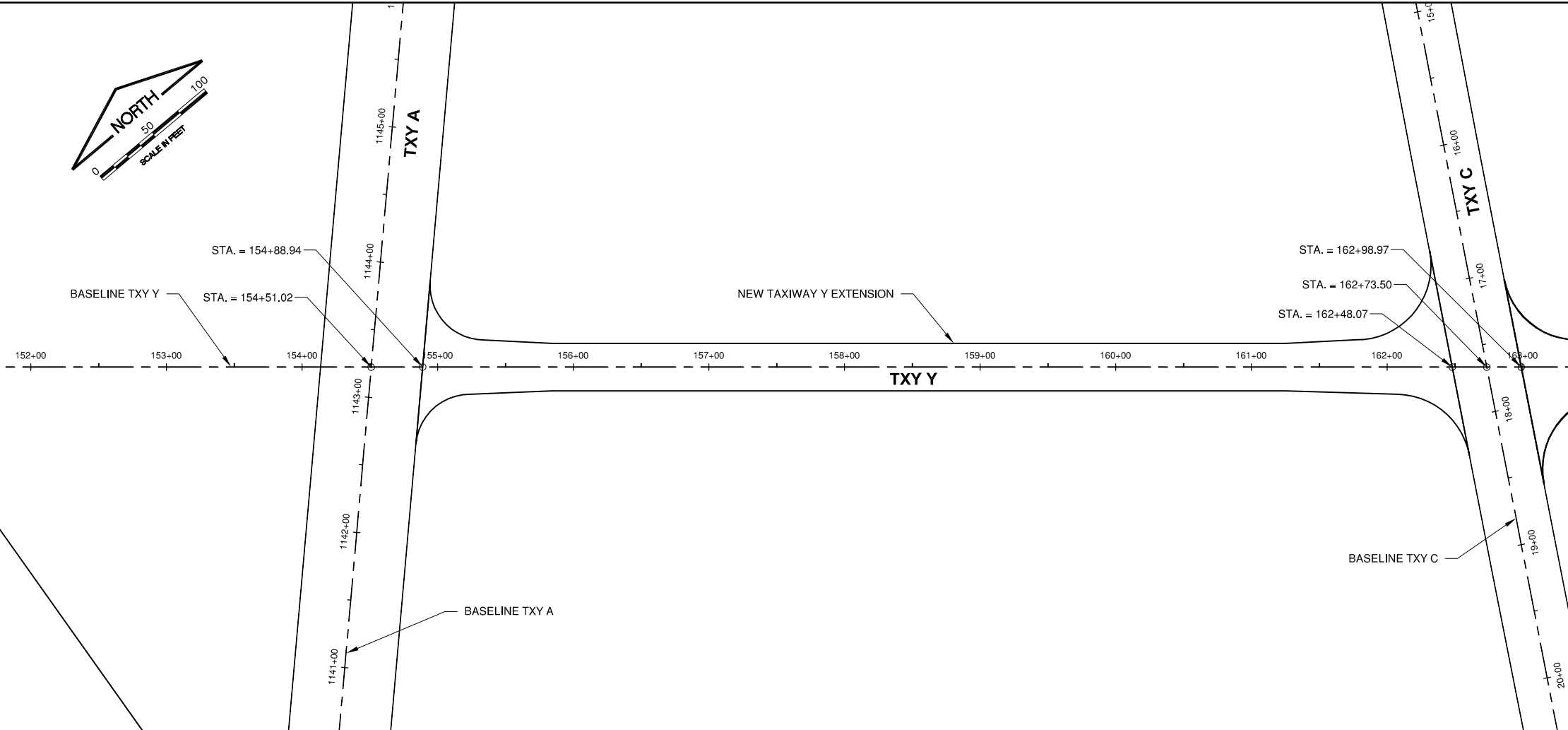
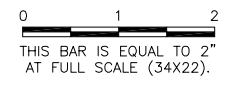
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 KEYMAP
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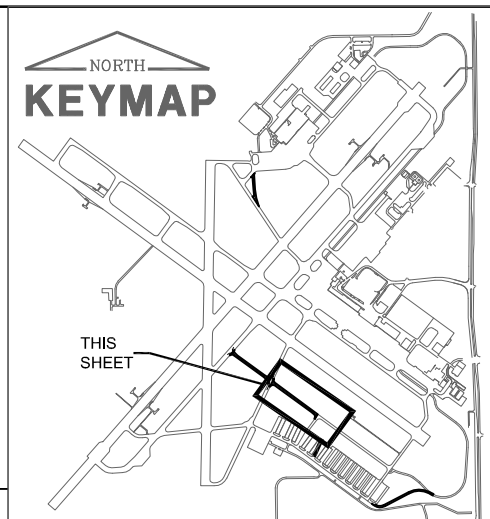
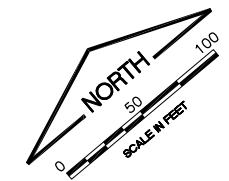
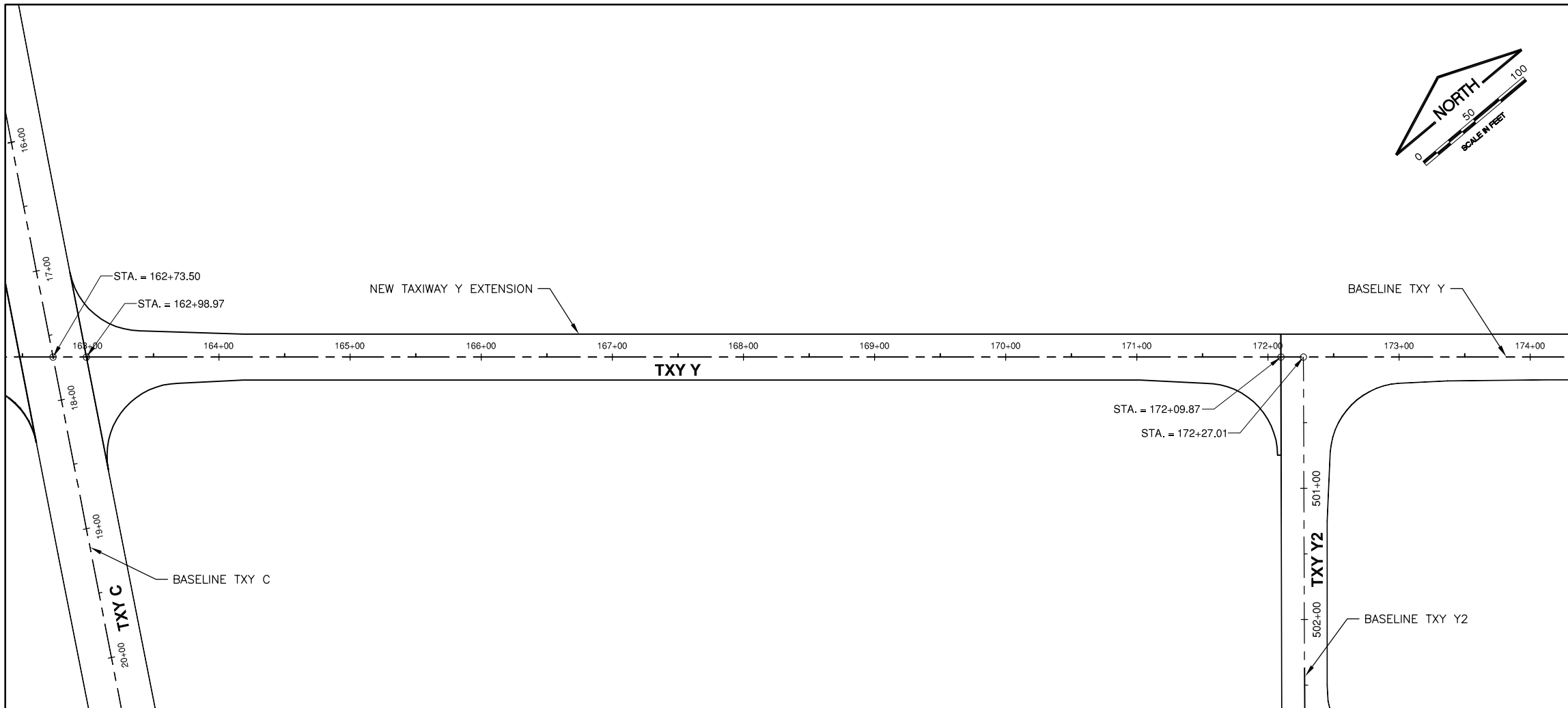
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 1" = 2' VERT

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SPRINGFIELD, ILLINOIS
EXTEND TAXIWAY Y
TXY Y EXTENSION PLAN & PROFILE 1

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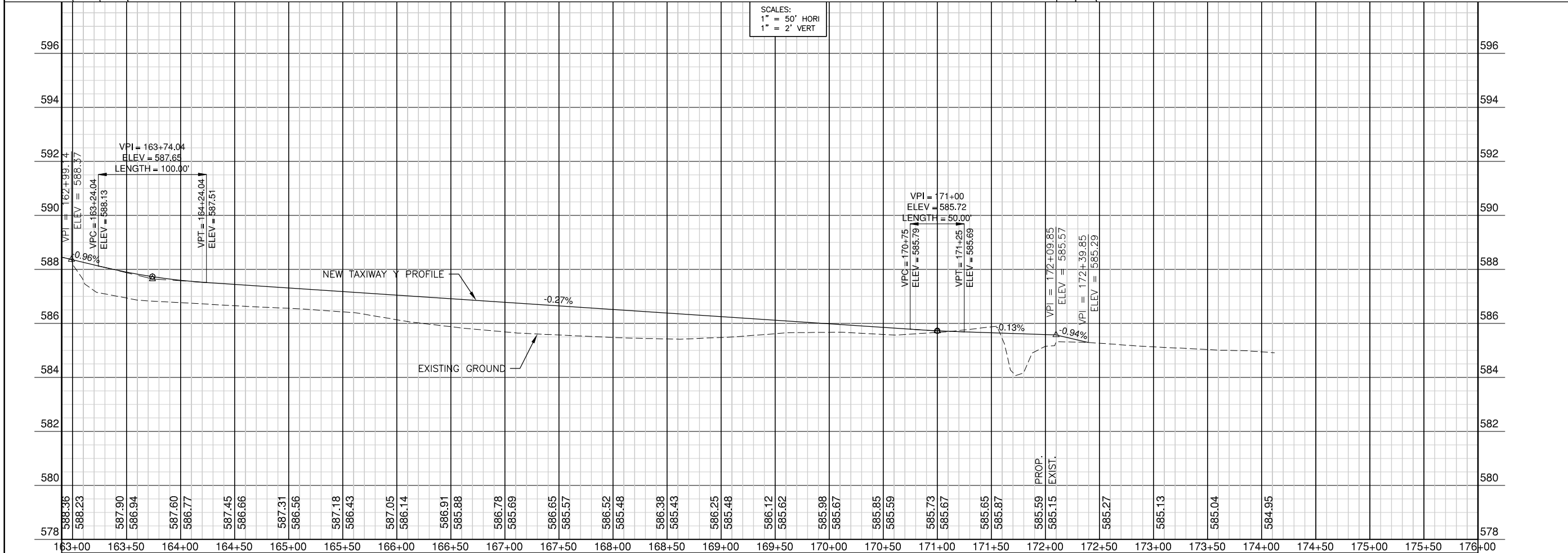
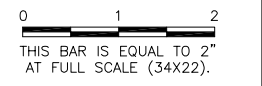
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| JOB No: | 110350400 |
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| SHEET 27 OF 106 SHEETS | |



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SCALES:
 1" = 50' HORI
 1" = 2' VERT

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EXTEND TAXIWAY Y

TXY Y EXTENSION PLAN & PROFILE 2

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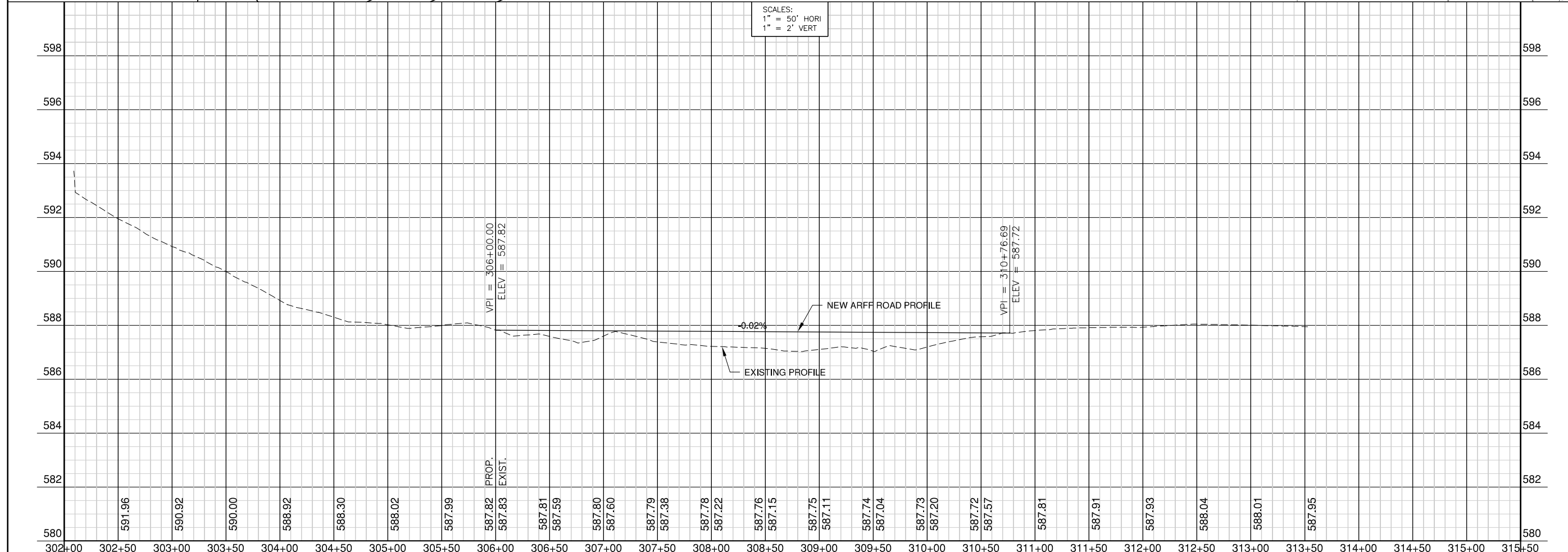
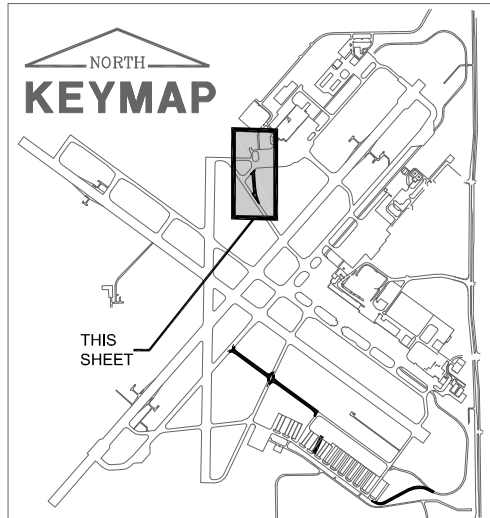
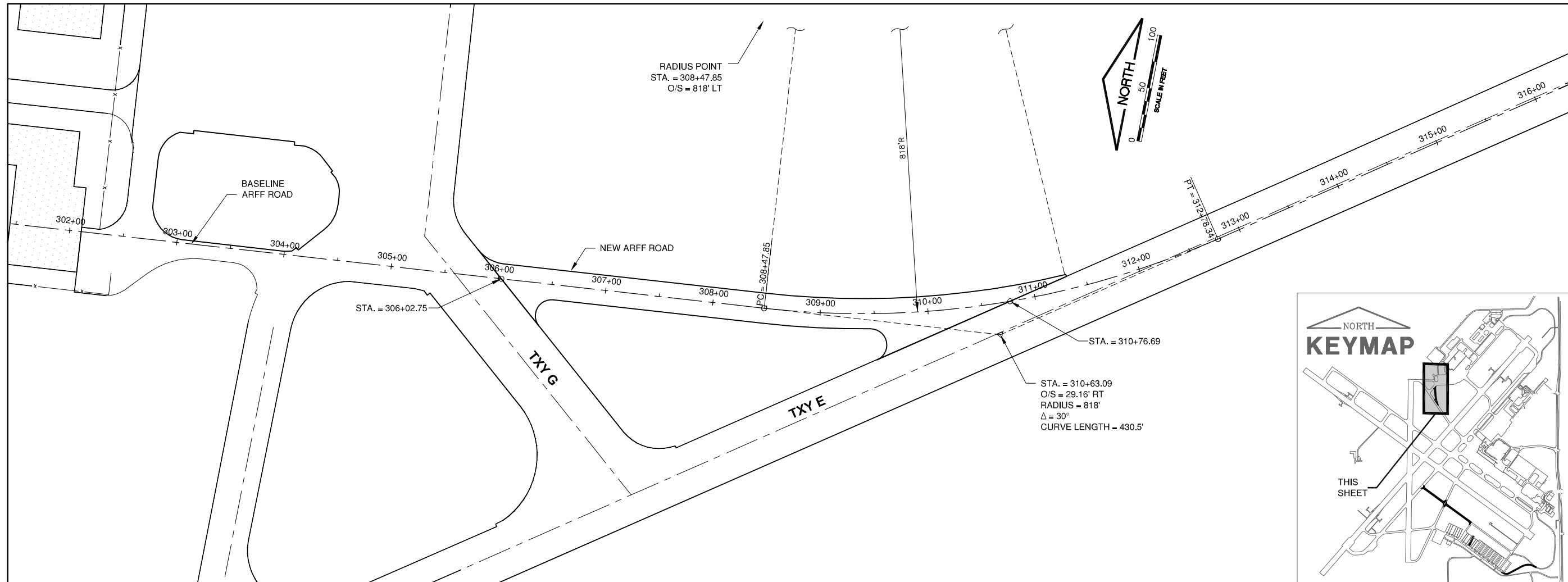
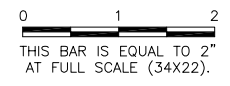


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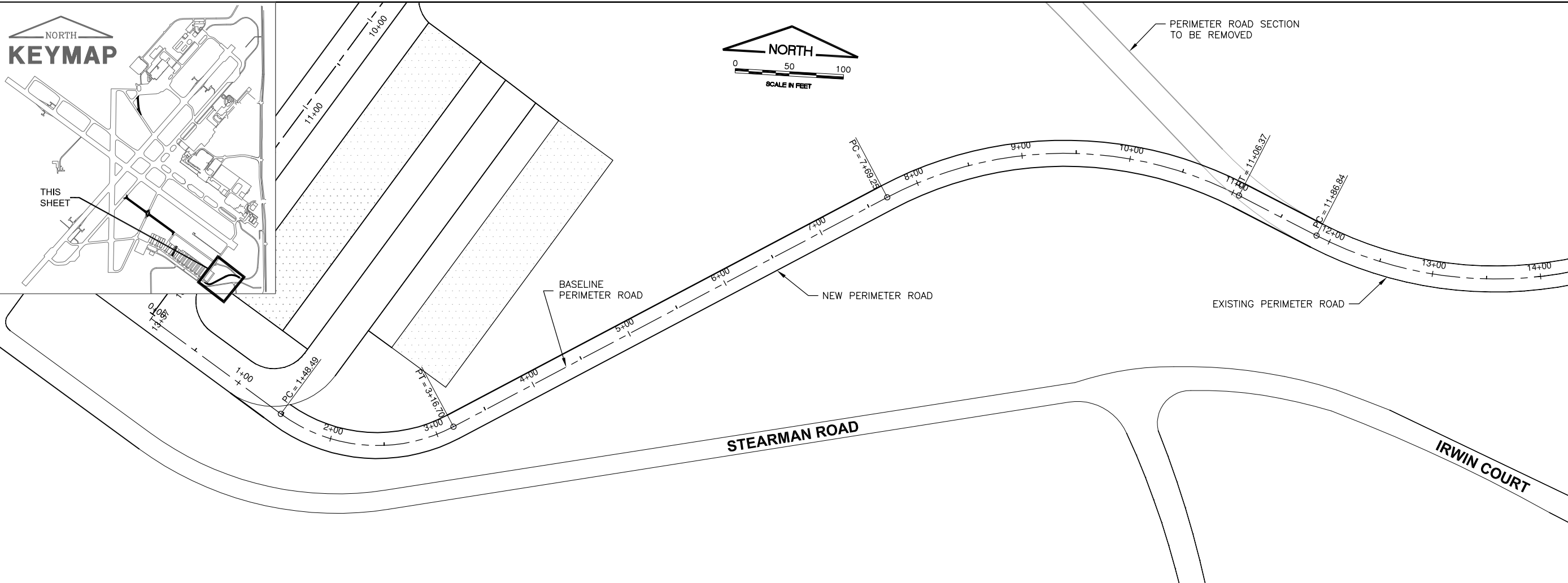
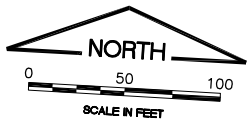
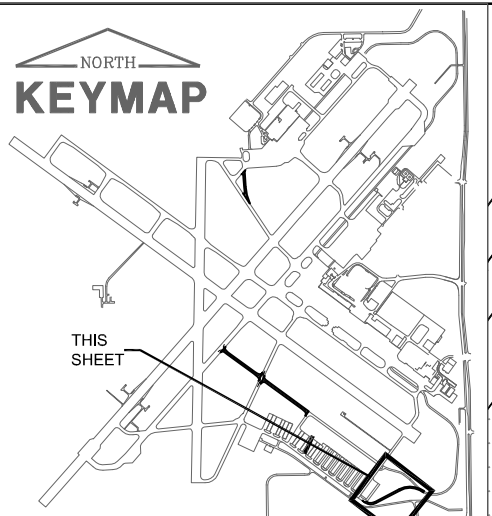
SCALES:
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 1" = 2' VERT

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SPRINGFIELD, ILLINOIS
EXTEND TAXIWAY Y
ARFF ROAD PLAN & PROFILE

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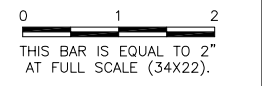
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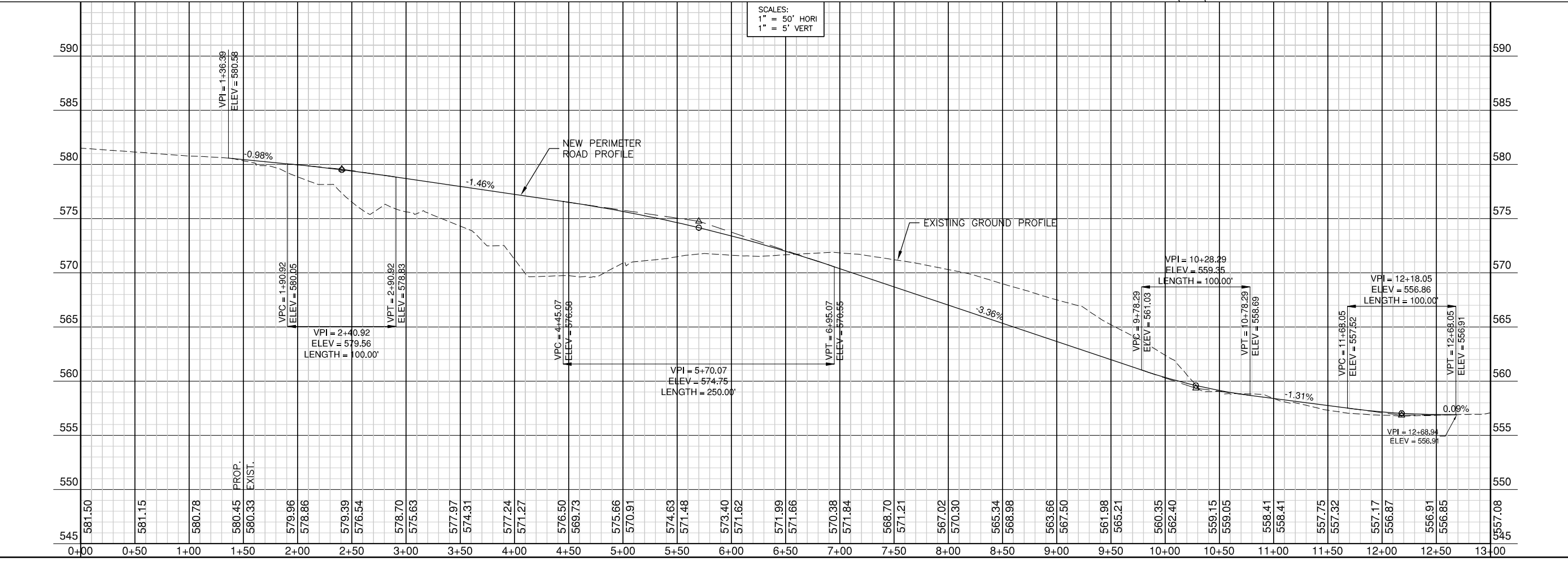
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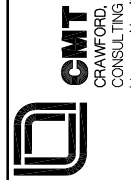
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EXTEND TAXIWAY Y

SE PERIMETER ROAD PLAN & PROFILE



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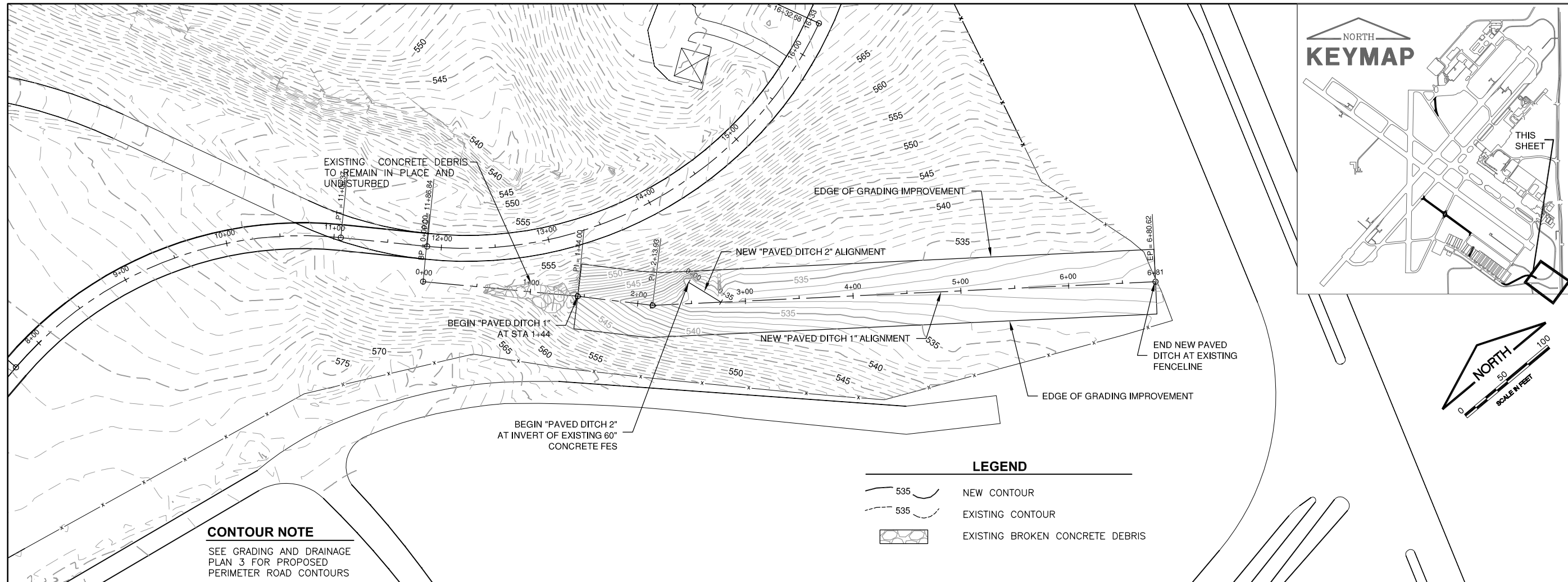
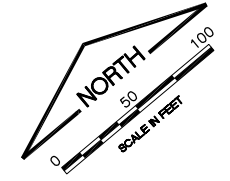
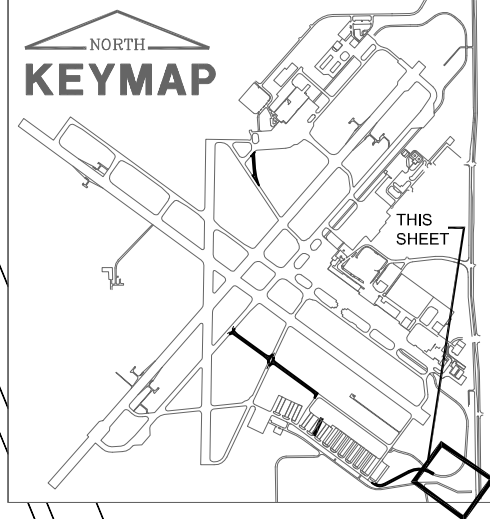
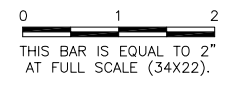
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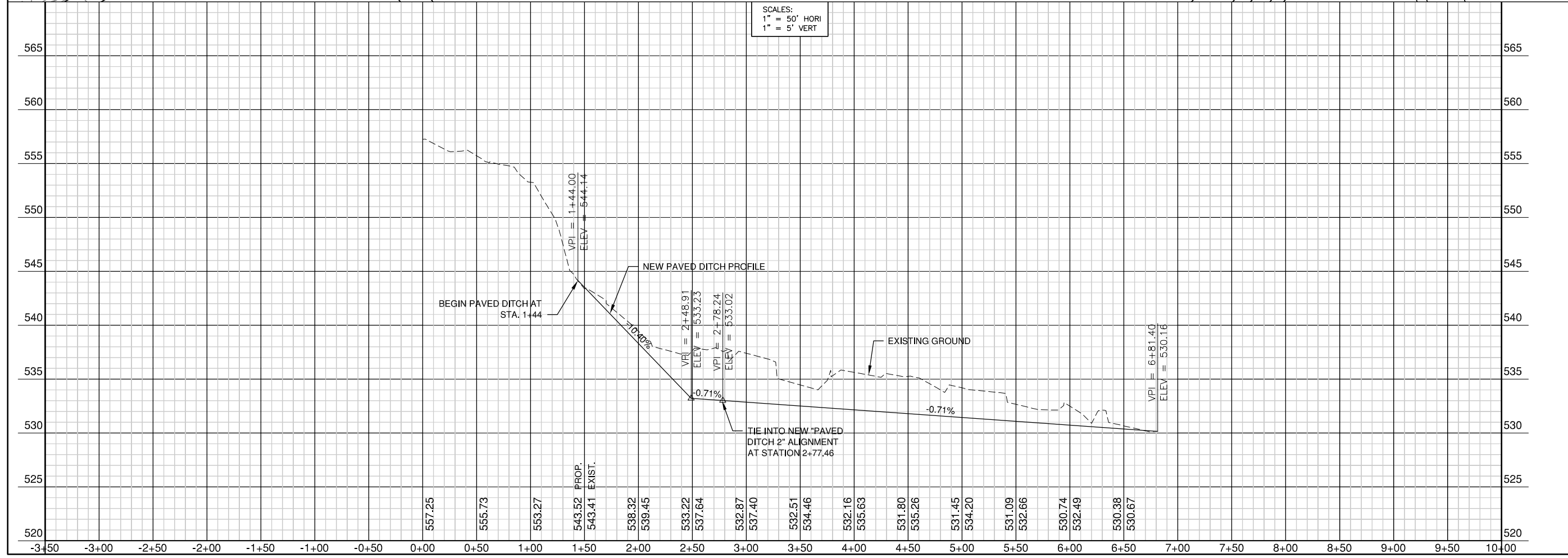
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CONTOUR NOTE
 SEE GRADING AND DRAINAGE
 PLAN 3 FOR PROPOSED
 PERIMETER ROAD CONTOURS

LEGEND

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| | 535 | NEW CONTOUR |
| | 535 | EXISTING CONTOUR |
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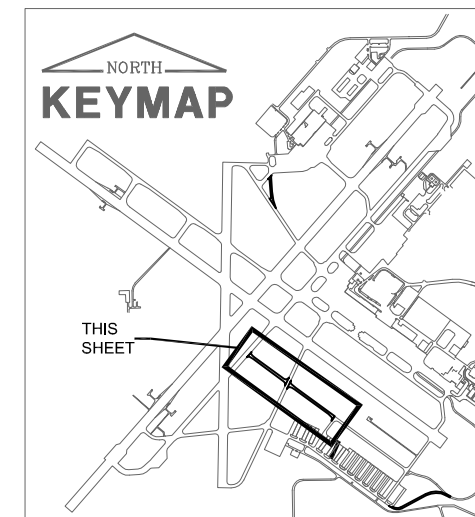
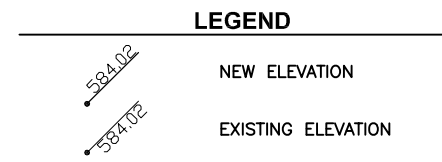
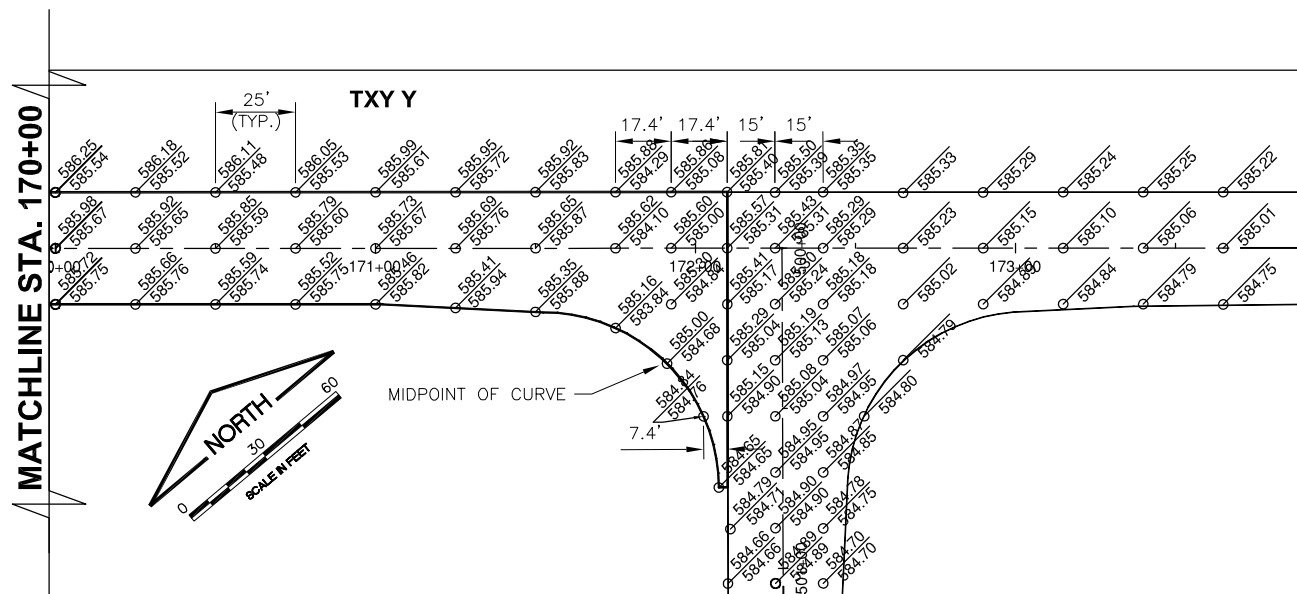
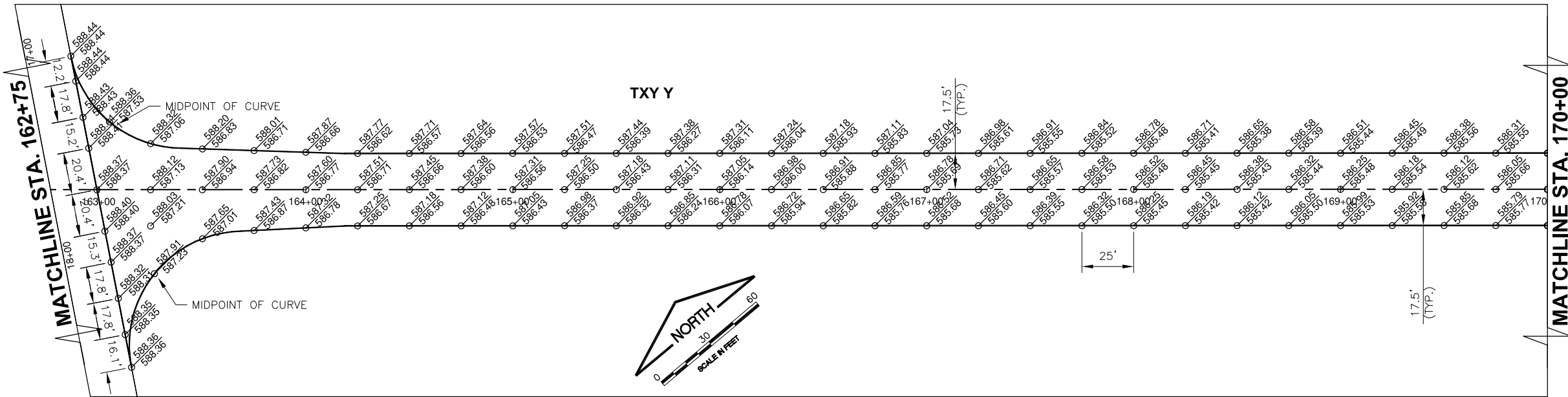
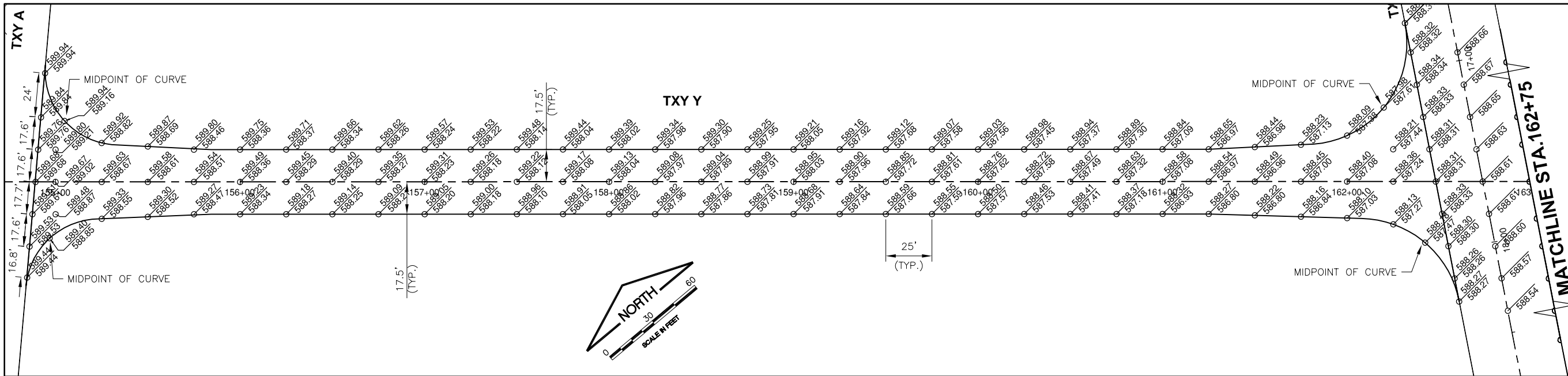


SCALES:
 1" = 50' HORI
 1" = 5' VERT

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SPRINGFIELD, ILLINOIS
EXTEND TAXIWAY Y
DRAINAGE OUTFALL PLAN & PROFILE

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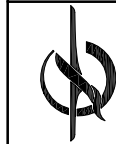
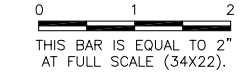
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| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET | 31 OF 106 SHEETS |



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CA014

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SPRINGFIELD, ILLINOIS

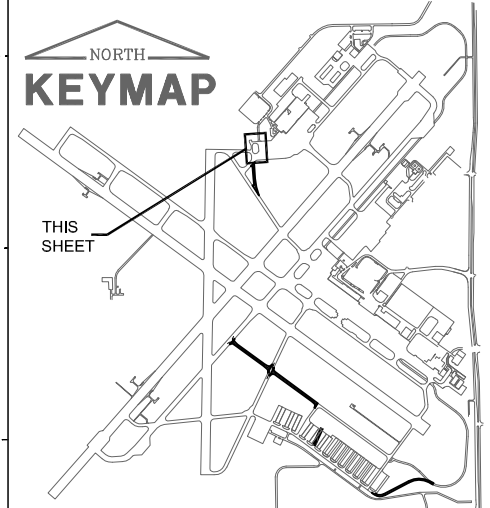
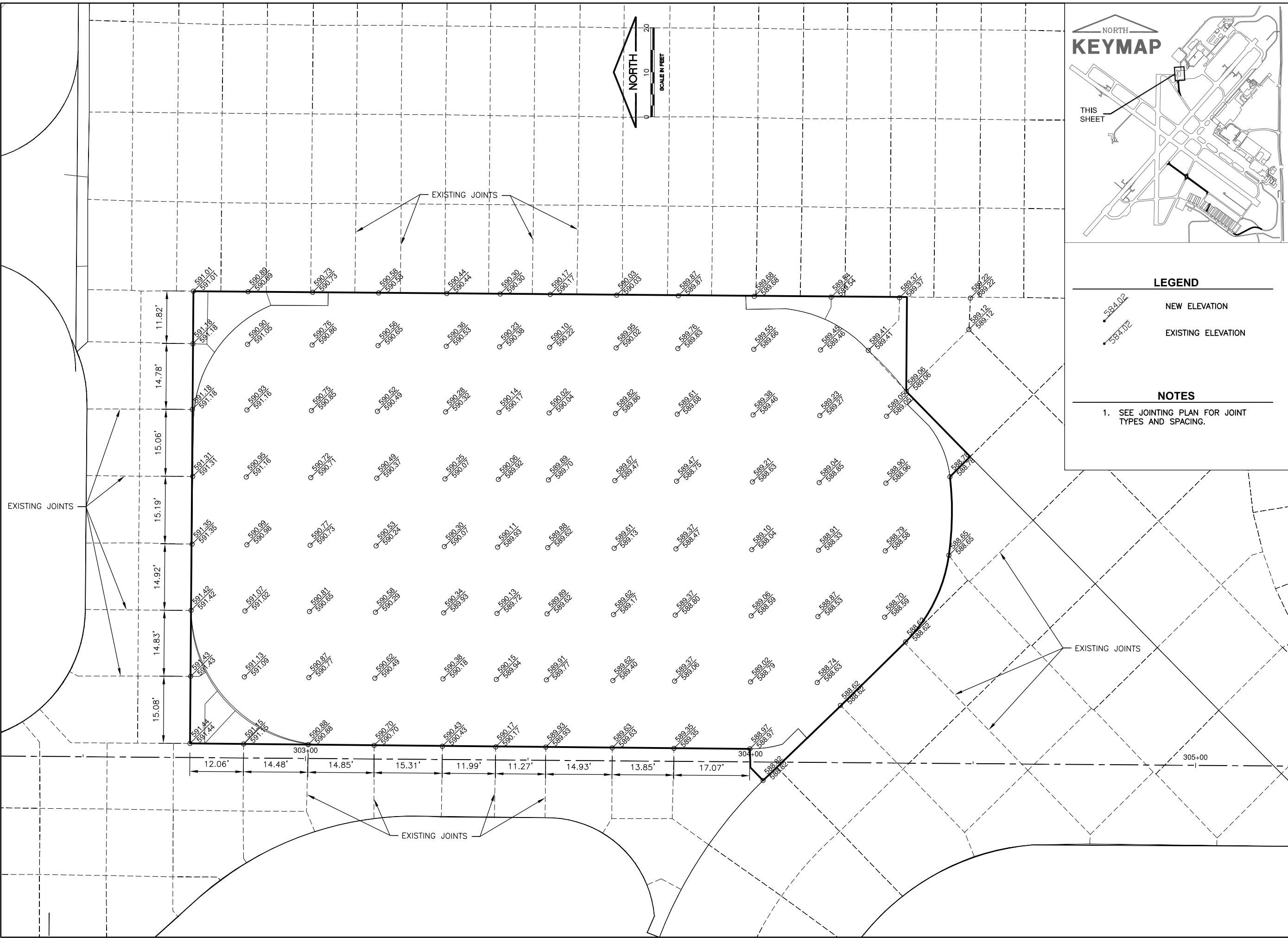
EXTEND TAXIWAY Y
STAKING PLAN 1

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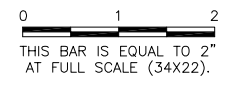
SPI-4156
 3-17-0096-XX



K:\Springfield\1103504\Draw\Sheets
 FILE: STAKING PLAN 2.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/8/2012 9:09 PM
 KEYMAP
 BASE_PROP_GEO
 BASE_PROP_JOINT
 SPI-BASE CURRENT GEOMETRY
 1103504-C-KP00
 1103504-V-VF2D

CA014

| REVISIONS | | |
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| NUMBER | BY | DATE |
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LEGEND

584.02 — NEW ELEVATION
 584.02 — EXISTING ELEVATION

NOTES

1. SEE JOINTING PLAN FOR JOINT TYPES AND SPACING.



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SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y
STAKING PLAN 2

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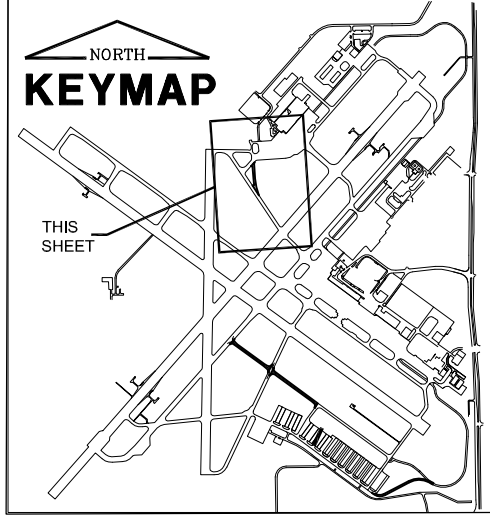
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| DRAWN BY: | CMT |
| CHECKED BY: | RLV |
| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET 33 OF 106 SHEETS | |

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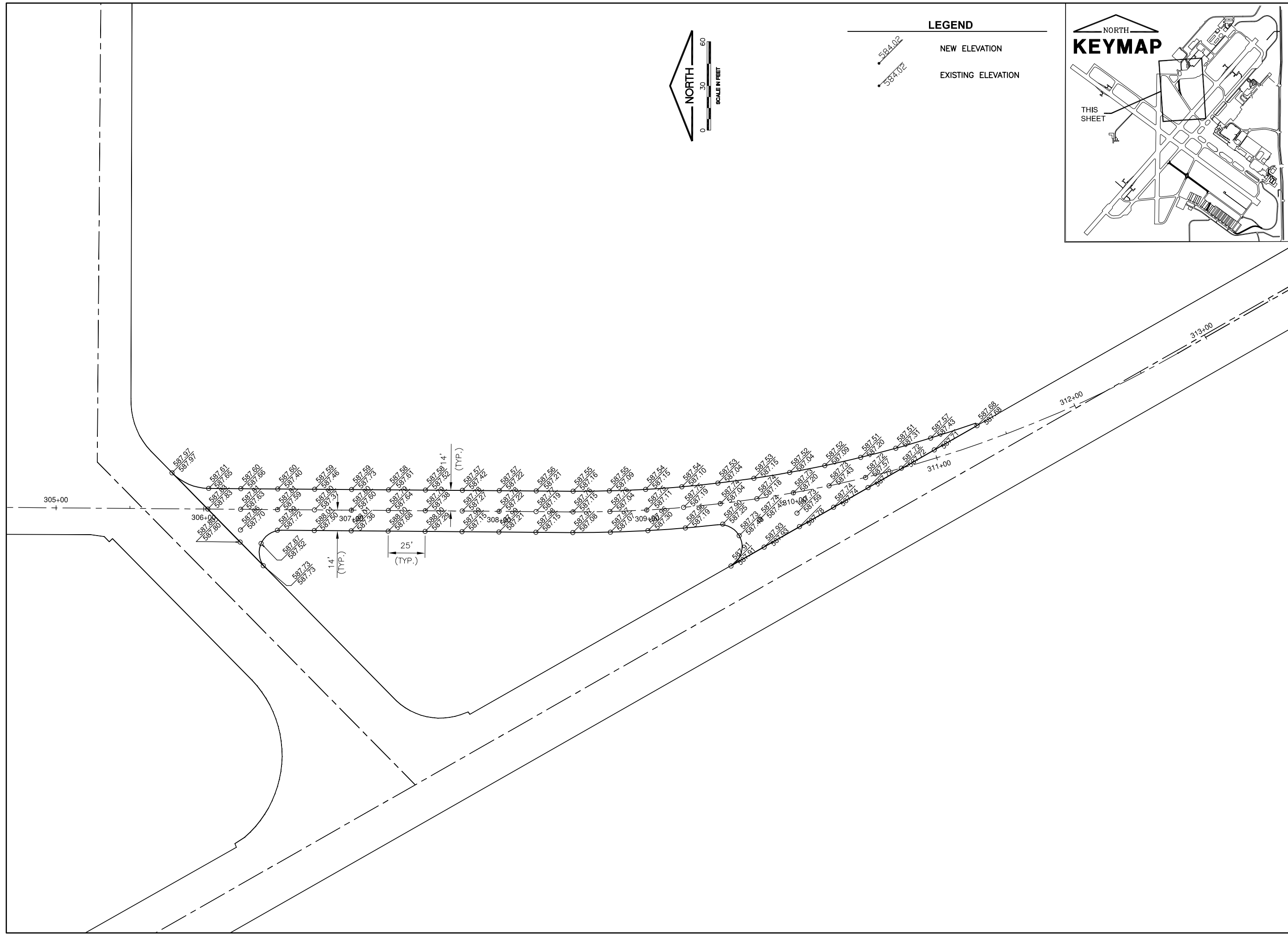
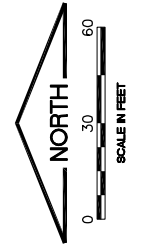
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



LEGEND

- NEW ELEVATION
- EXISTING ELEVATION



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ABRAHAM LINCOLN CAPITAL AIRPORT
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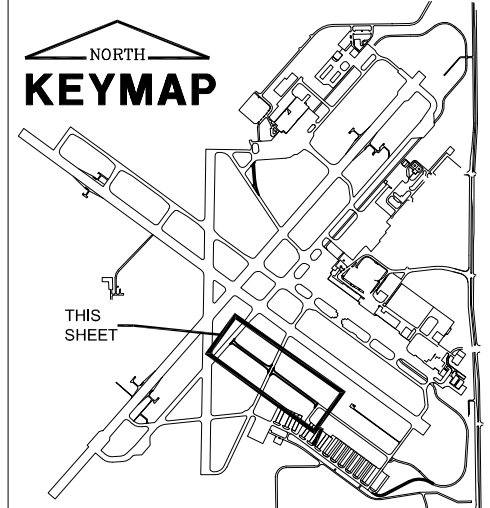
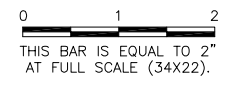


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| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |

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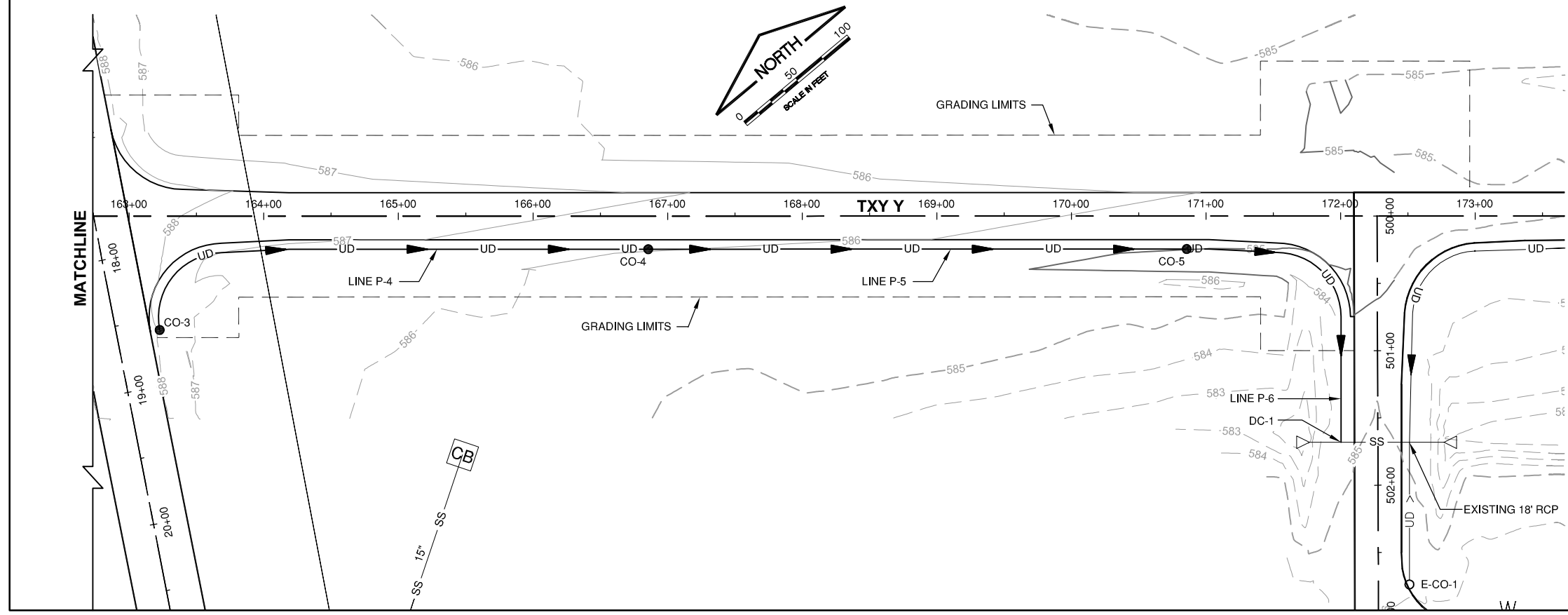
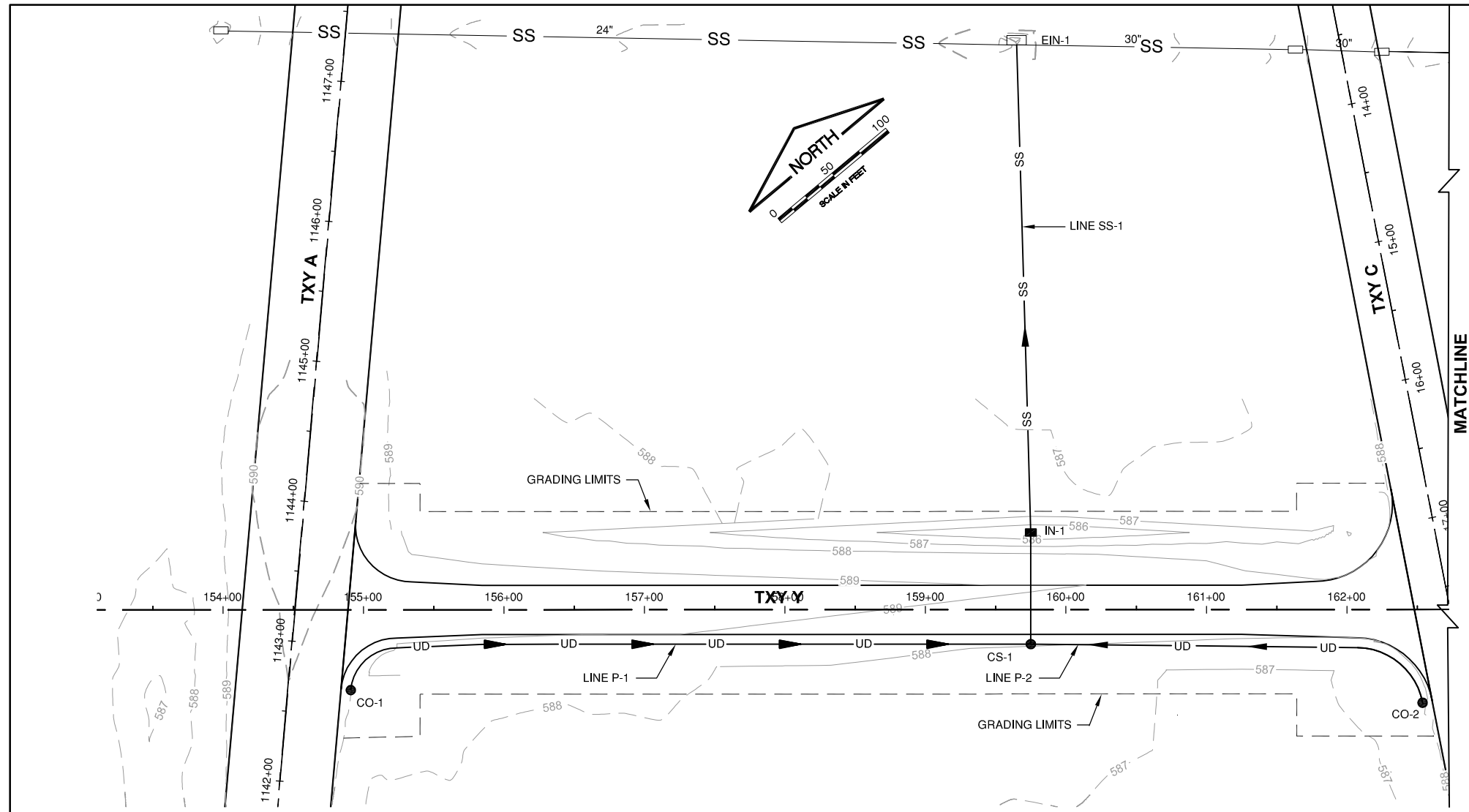
CA014

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LEGEND

- SS — SS — EXISTING STORM SEWER
- SS — SS — NEW STORM SEWER
- EXISTING INLET
- IN-1 NEW INLET
- △ EXISTING FLARED END SECTION (F.E.S.)
- UD —▶ EXISTING UNDERDRAIN
- UD —▶ NEW UNDERDRAIN
- - - - - NEW GRADING LIMITS
- CO-1 NEW CLEANOUT STRUCTURE
- CS-2 NEW COLLECTION STRUCTURE
- E-CO-1 EXISTING CLEANOUT STRUCTURE
- 590 NEW CONTOUR
- 590 EXISTING CONTOUR



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 SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y

GRADING & DRAINAGE PLAN 1

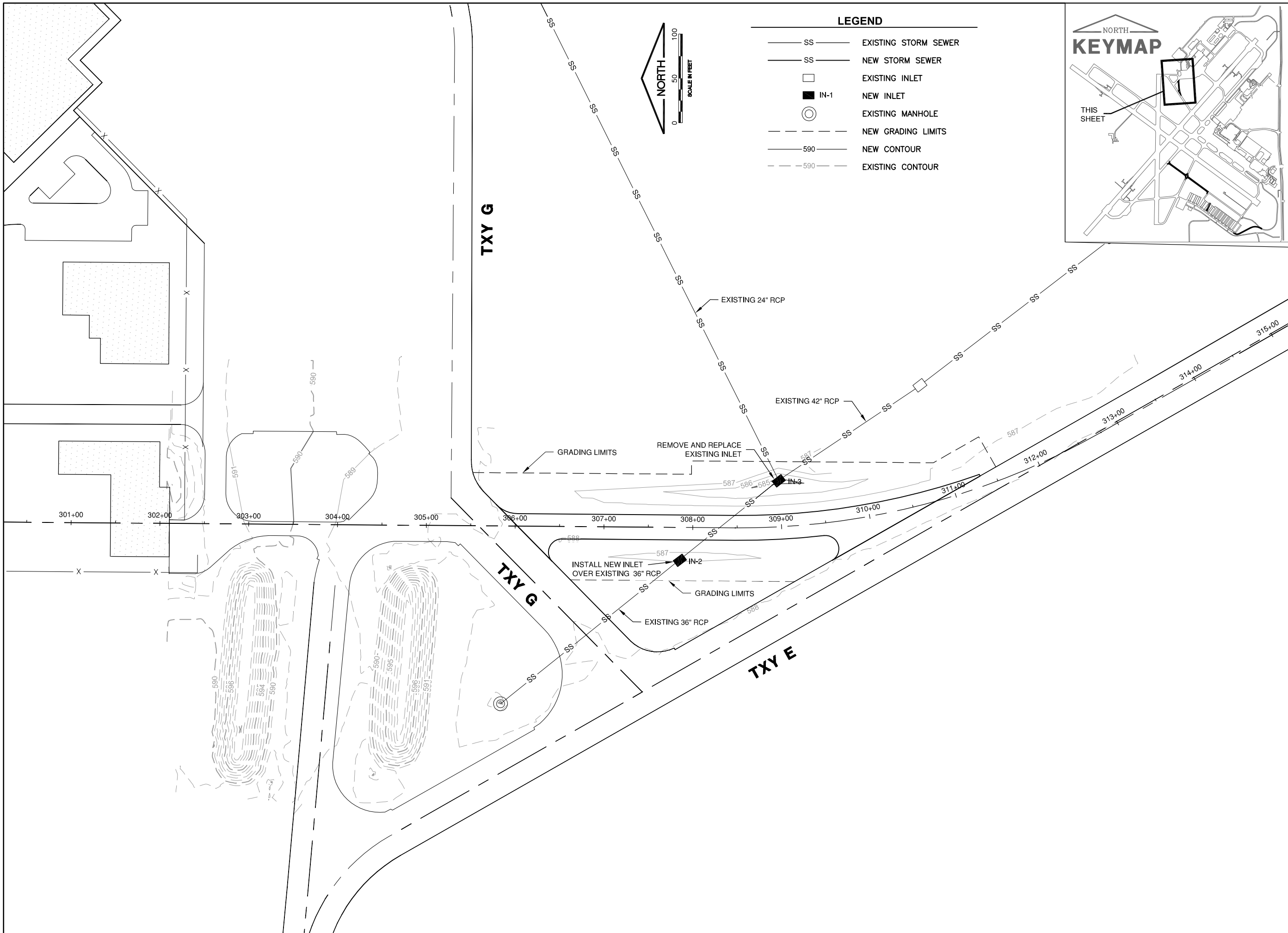
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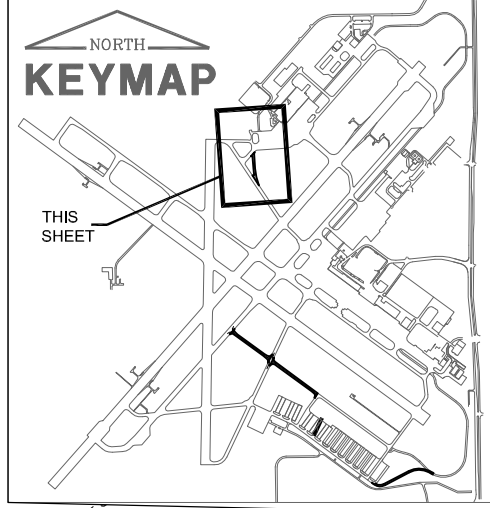
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| APPROVED BY: | RLV |
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LEGEND


- SS ——— EXISTING STORM SEWER
- SS ——— NEW STORM SEWER
- EXISTING INLET
- IN-1 NEW INLET
- EXISTING MANHOLE
- - - - - NEW GRADING LIMITS
- 590— NEW CONTOUR
- - -590- - - EXISTING CONTOUR



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UPDATE BY: TJ Heavisides
PLOT DATE: 5/8/2012 9:10 PM
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SPI-BASE CURRENT GEOMETRY
BASE_PROP_GEO
SPL_UH
BASE_PROP_TURF
1103504-V-VF3D
BASE_EXIST_UTL
CA014

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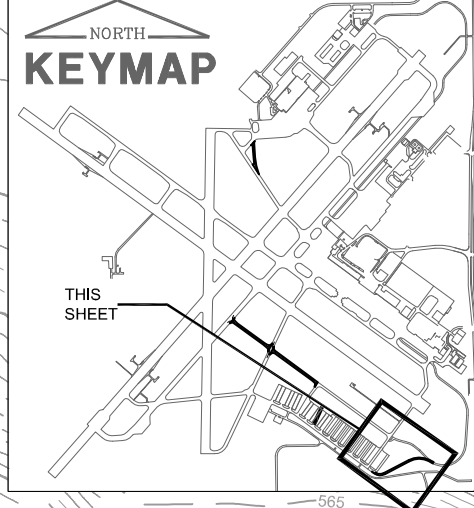
0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).


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SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y
GRADING & DRAINAGE PLAN 2

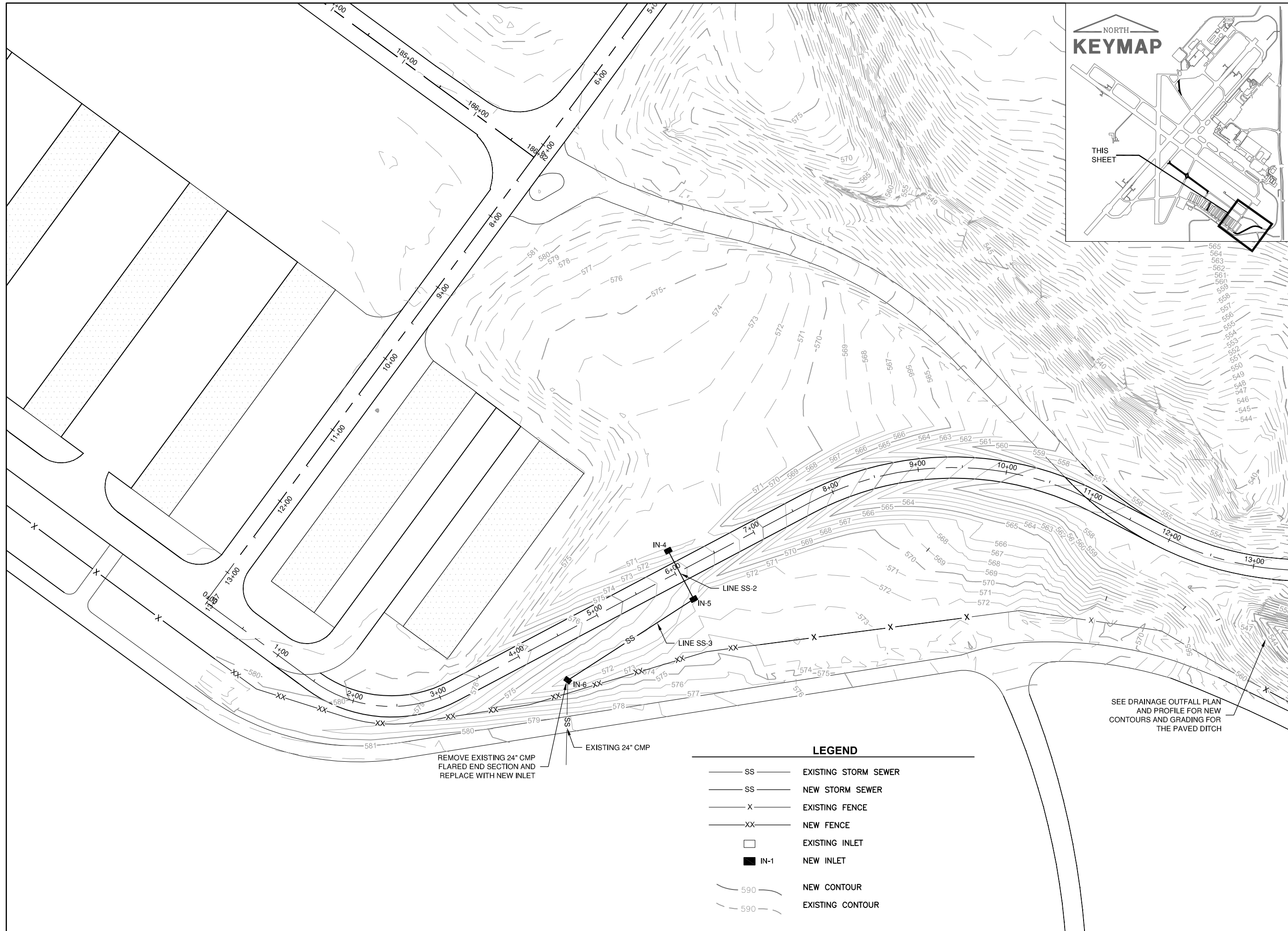
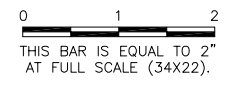
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| SPI-4156 3-17-0096-XX | |
| SHEET 36 OF 106 SHEETS | |



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LEGEND

| | |
|-----------------|----------------------|
| — SS — | EXISTING STORM SEWER |
| — SS — | NEW STORM SEWER |
| X | EXISTING FENCE |
| XX | NEW FENCE |
| □ | EXISTING INLET |
| ■ IN-1 | NEW INLET |
| — 590 — | NEW CONTOUR |
| - - - 590 - - - | EXISTING CONTOUR |

SEE DRAINAGE OUTFALL PLAN
 AND PROFILE FOR NEW
 CONTOURS AND GRADING FOR
 THE PAVED DITCH



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EXTEND TAXIWAY Y

GRADING & DRAINAGE PLAN 3

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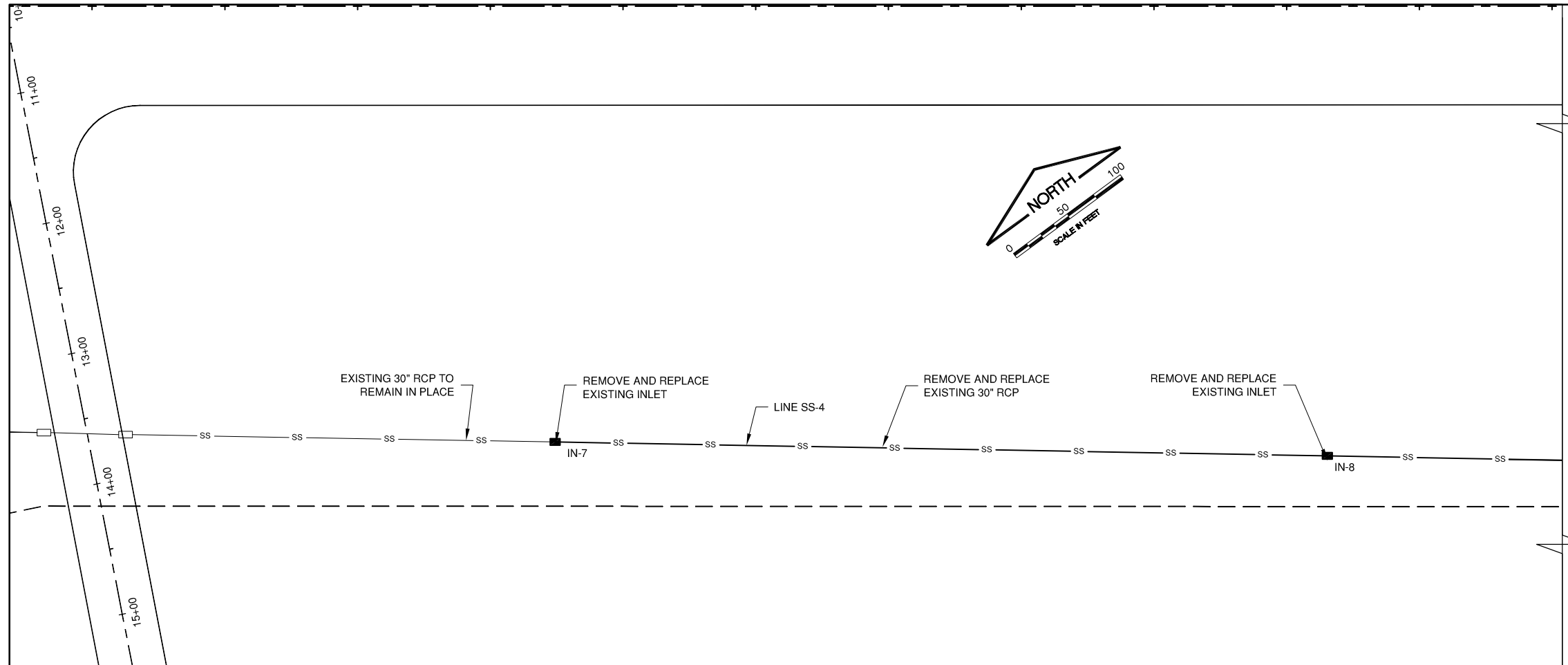
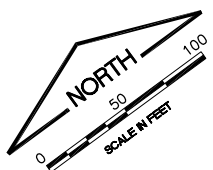
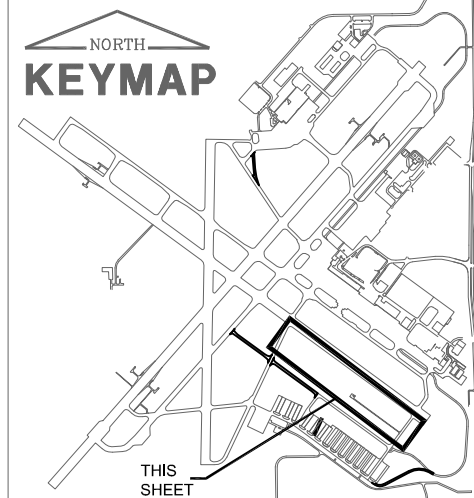
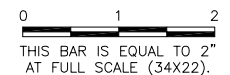


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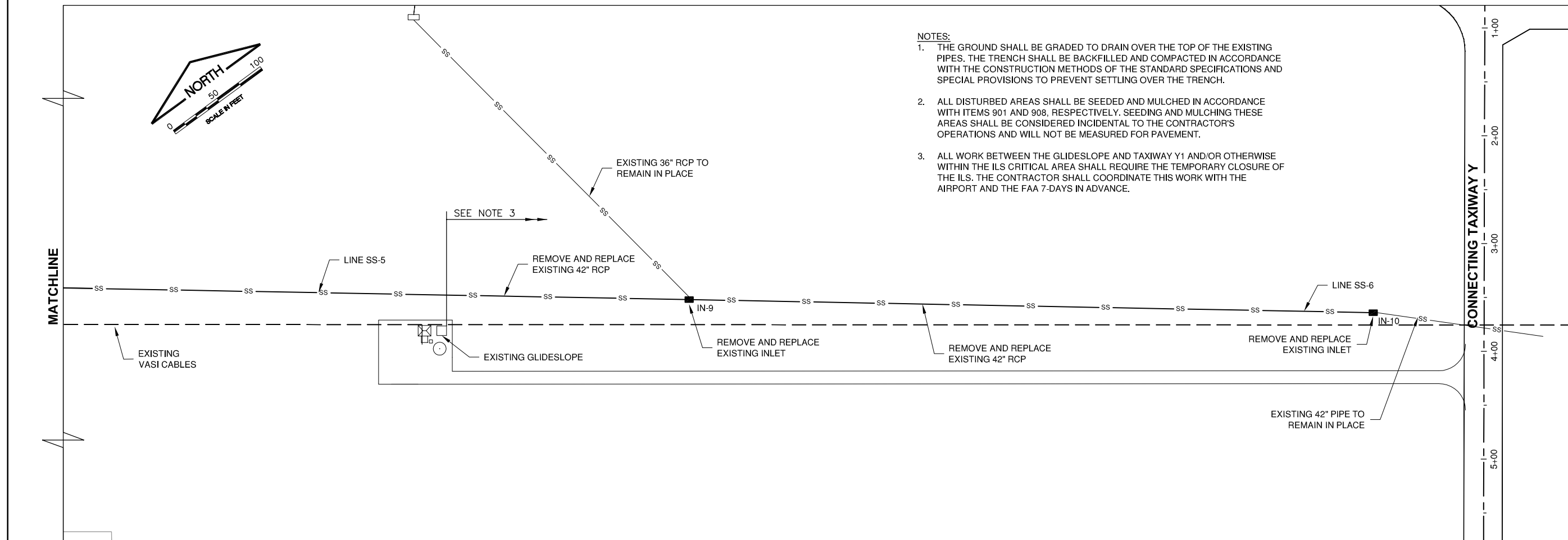
MATCHLINE

LEGEND

- SS — SS — EXISTING STORM SEWER
- SS — SS — NEW STORM SEWER
- EXISTING INLET
- IN-1 NEW INLET

NOTES:

1. THE GROUND SHALL BE GRADED TO DRAIN OVER THE TOP OF THE EXISTING PIPES. THE TRENCH SHALL BE BACKFILLED AND COMPACTED IN ACCORDANCE WITH THE CONSTRUCTION METHODS OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS TO PREVENT SETTLING OVER THE TRENCH.
2. ALL DISTURBED AREAS SHALL BE SEEDED AND MULCHED IN ACCORDANCE WITH ITEMS 901 AND 908, RESPECTIVELY. SEEDING AND MULCHING THESE AREAS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACTOR'S OPERATIONS AND WILL NOT BE MEASURED FOR PAVEMENT.
3. ALL WORK BETWEEN THE GLIDESLOPE AND TAXIWAY Y1 AND/OR OTHERWISE WITHIN THE ILS CRITICAL AREA SHALL REQUIRE THE TEMPORARY CLOSURE OF THE ILS. THE CONTRACTOR SHALL COORDINATE THIS WORK WITH THE AIRPORT AND THE FAA 7-DAYS IN ADVANCE.



MATCHLINE

CONNECTING TAXIWAY Y



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EXTEND TAXIWAY Y

GRADING & DRAINAGE PLAN 4

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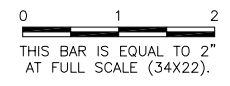
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 SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y
 DRAINAGE DETAILS

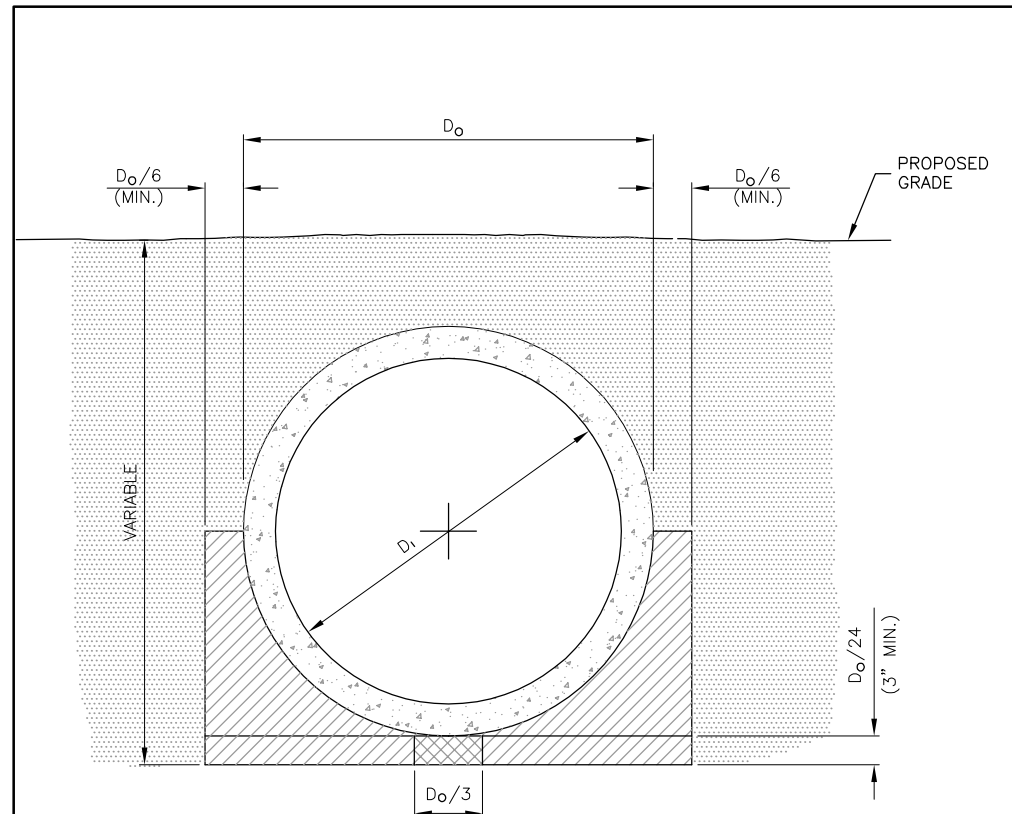
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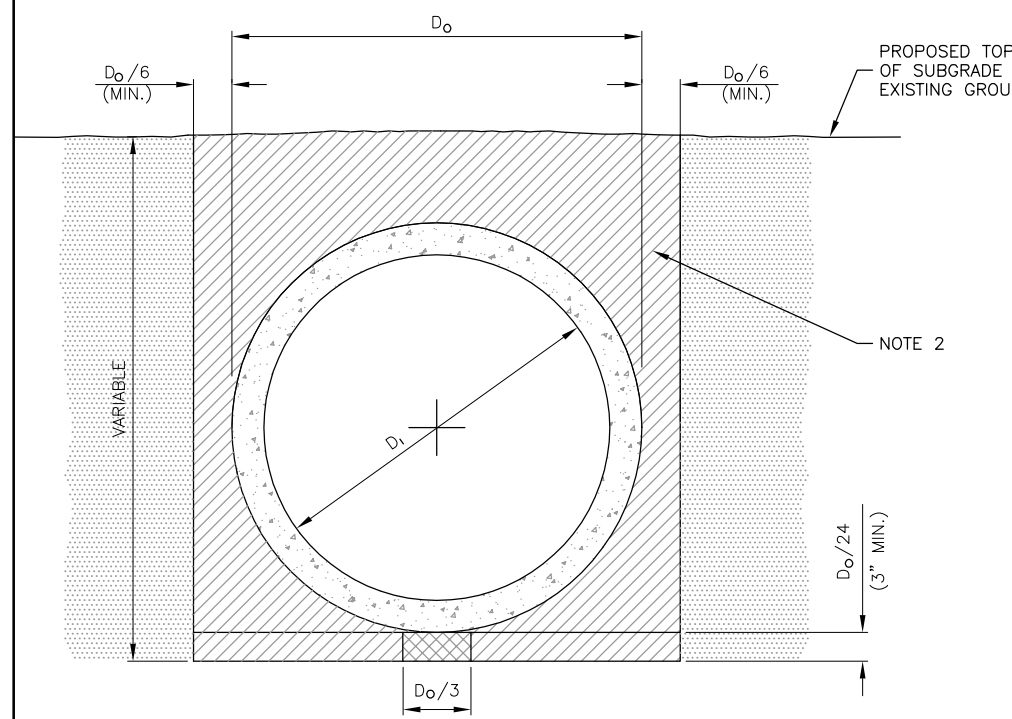


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



**STANDARD TRENCH INSTALLATION
 NON-PAVED AREA**
 N.T.S.



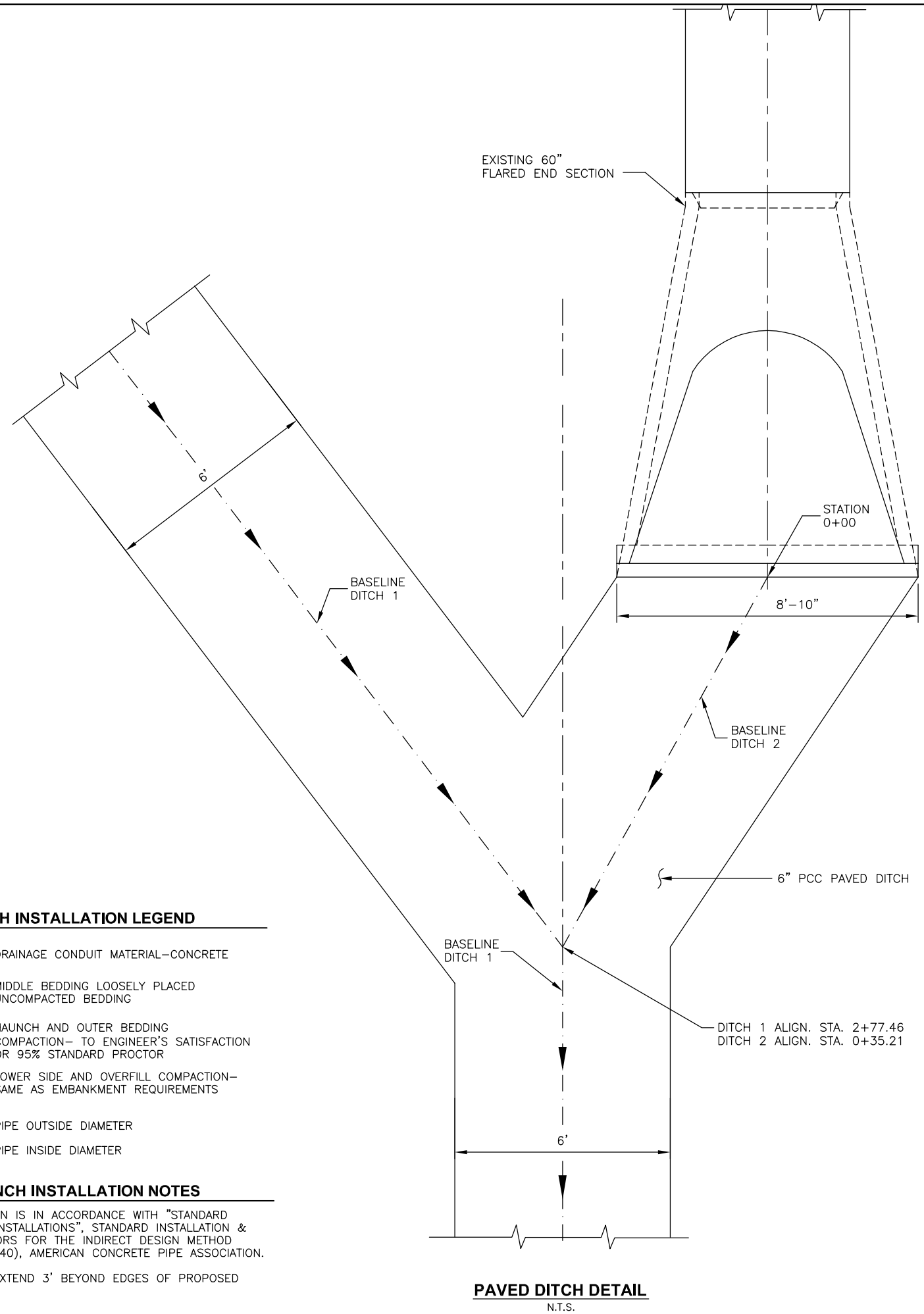
**STANDARD TRENCH INSTALLATION
 PROPOSED PAVED AREA**
 N.T.S.

TRENCH INSTALLATION LEGEND

-  DRAINAGE CONDUIT MATERIAL—CONCRETE
-  MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
-  HAUNCH AND OUTER BEDDING COMPACTION— TO ENGINEER'S SATISFACTION OR 95% STANDARD PROCTOR
-  LOWER SIDE AND OVERFILL COMPACTION— SAME AS EMBANKMENT REQUIREMENTS
- D_o PIPE OUTSIDE DIAMETER
- D_i PIPE INSIDE DIAMETER

TRENCH INSTALLATION NOTES

- BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS", STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
- BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.

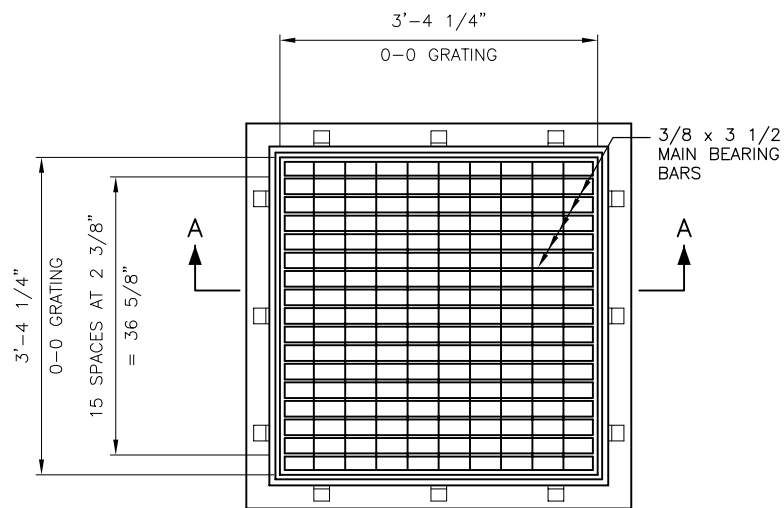


PAVED DITCH DETAIL
 N.T.S.

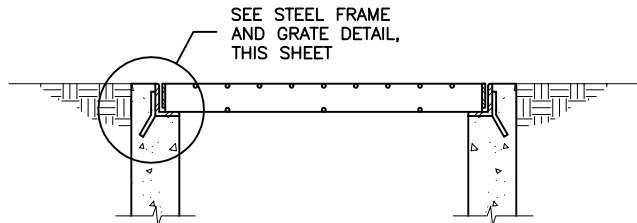
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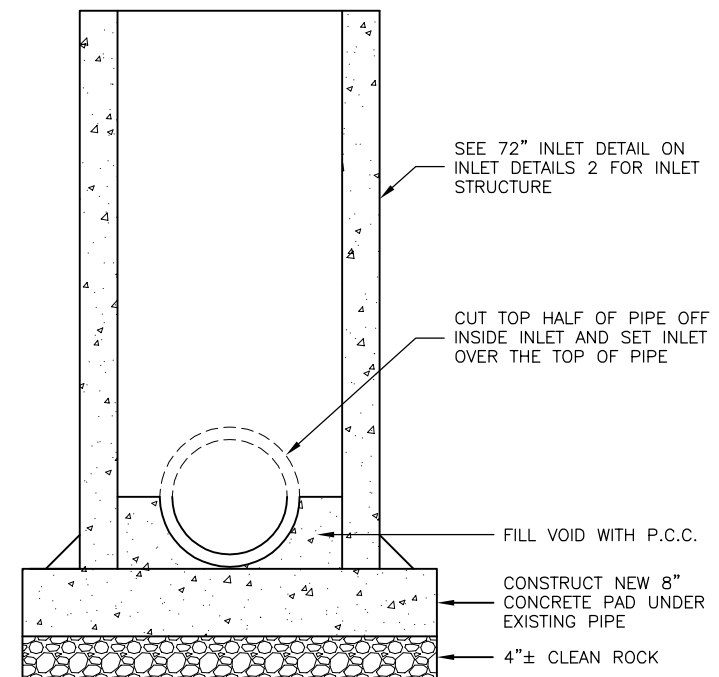
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 AT FULL SCALE (34X22).



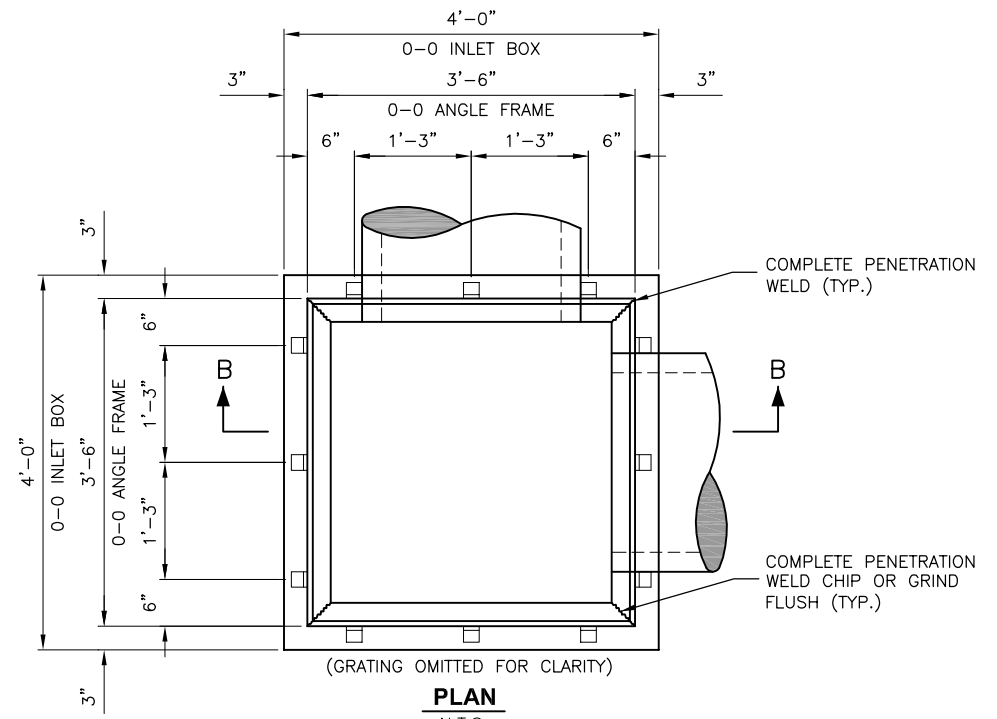
PLAN
N.T.S.



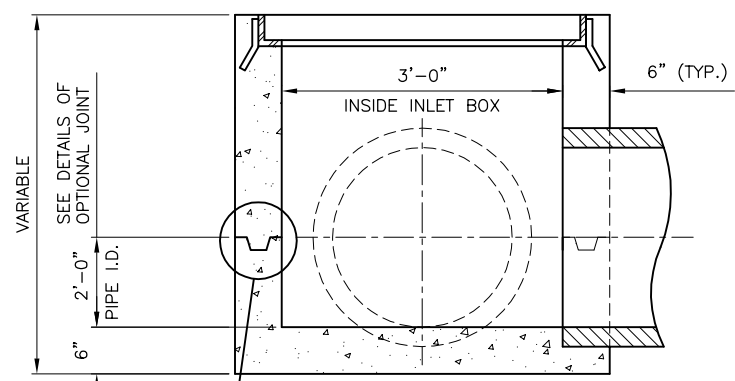
SECTION A-A
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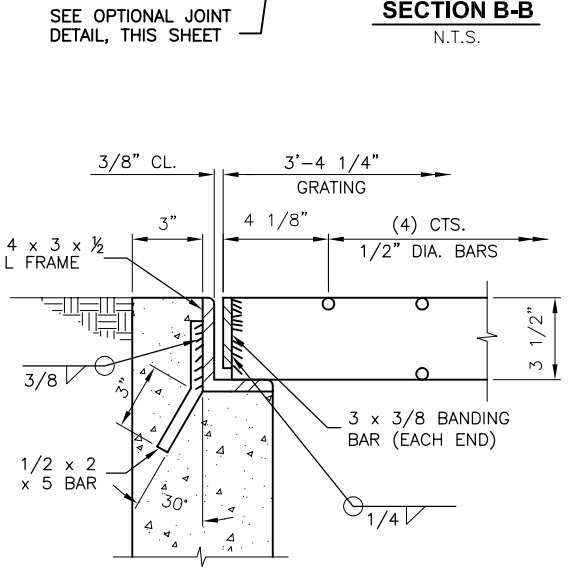
INLET OVER EXISTING PIPE DETAIL
N.T.S.



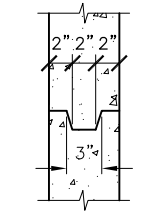
PLAN
N.T.S.



SECTION B-B
N.T.S.

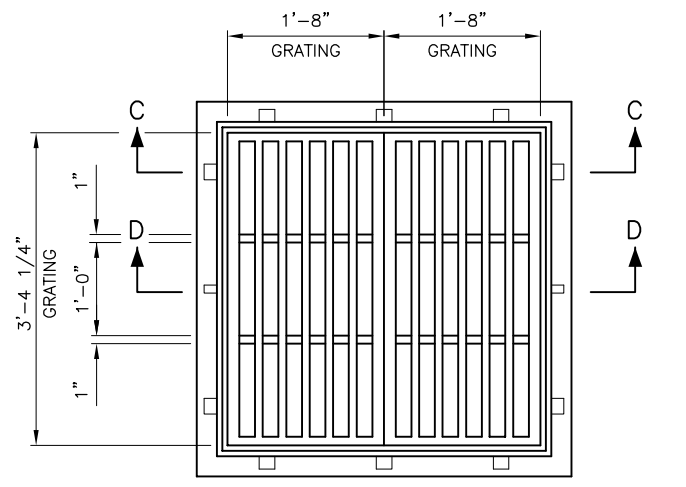


STEEL FRAME & GRATE DETAIL
N.T.S.

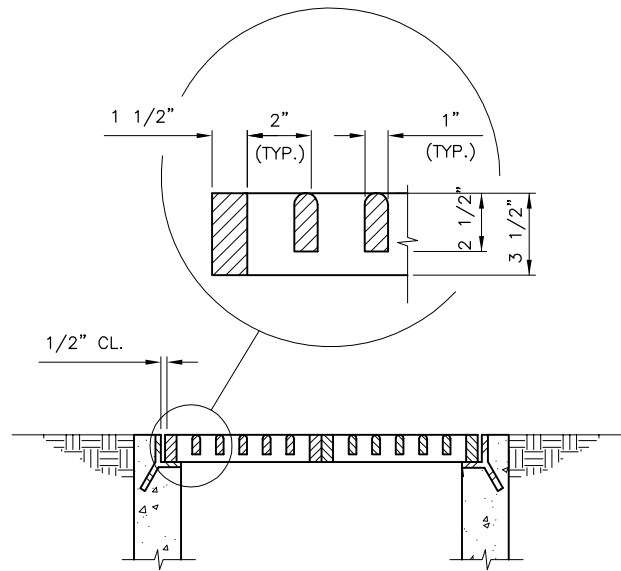


OPTIONAL JOINT DETAIL
N.T.S.

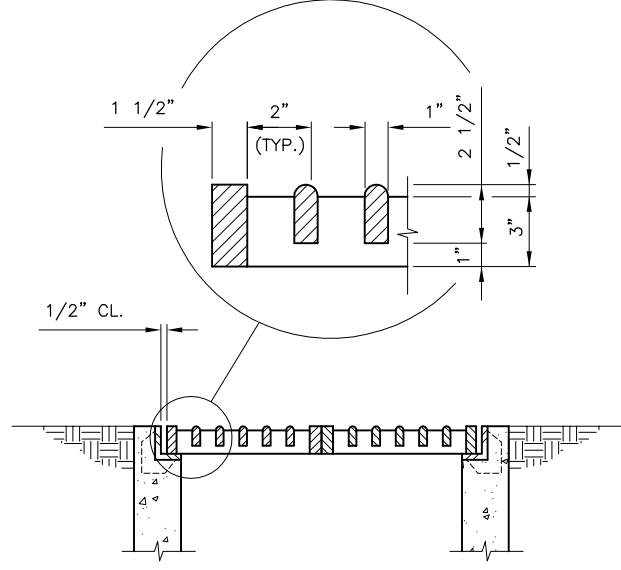
INLET NOTE
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PLAN
N.T.S.

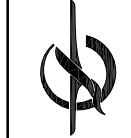


SECTION C-C
N.T.S.



SECTION D-D

CAST FRAME & GRATE DETAILS
N.T.S.



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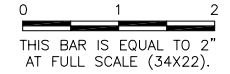
EXTEND TAXIWAY Y
 INLET DETAILS 1



DESIGN BY: RLV/JMW
 DRAWN BY: CMT
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: APRIL 27, 2012
 JOB No: 110350400

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SPRINGFIELD AIRPORT AUTHORITY
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 SPRINGFIELD, ILLINOIS

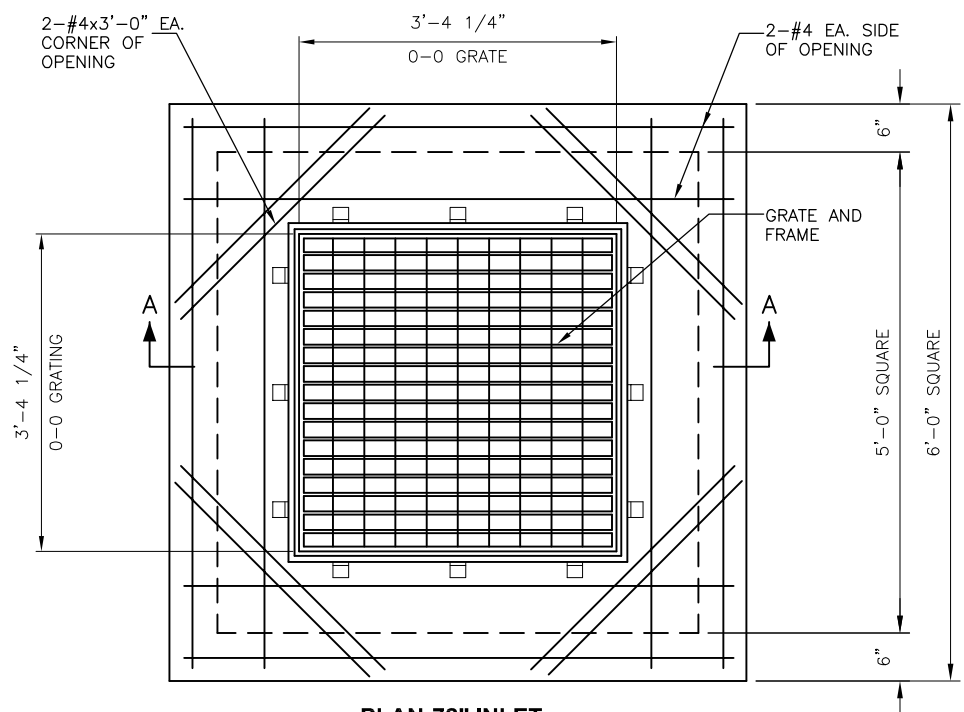
EXTEND TAXIWAY Y
 INLET DETAILS 2

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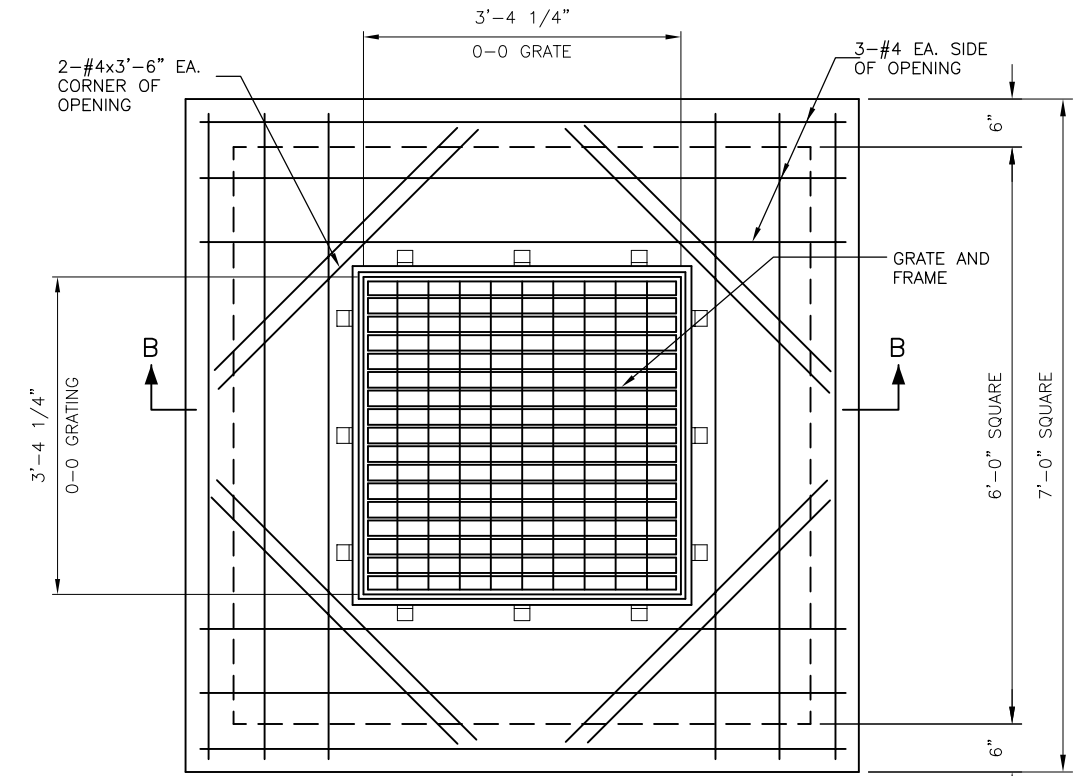


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| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |

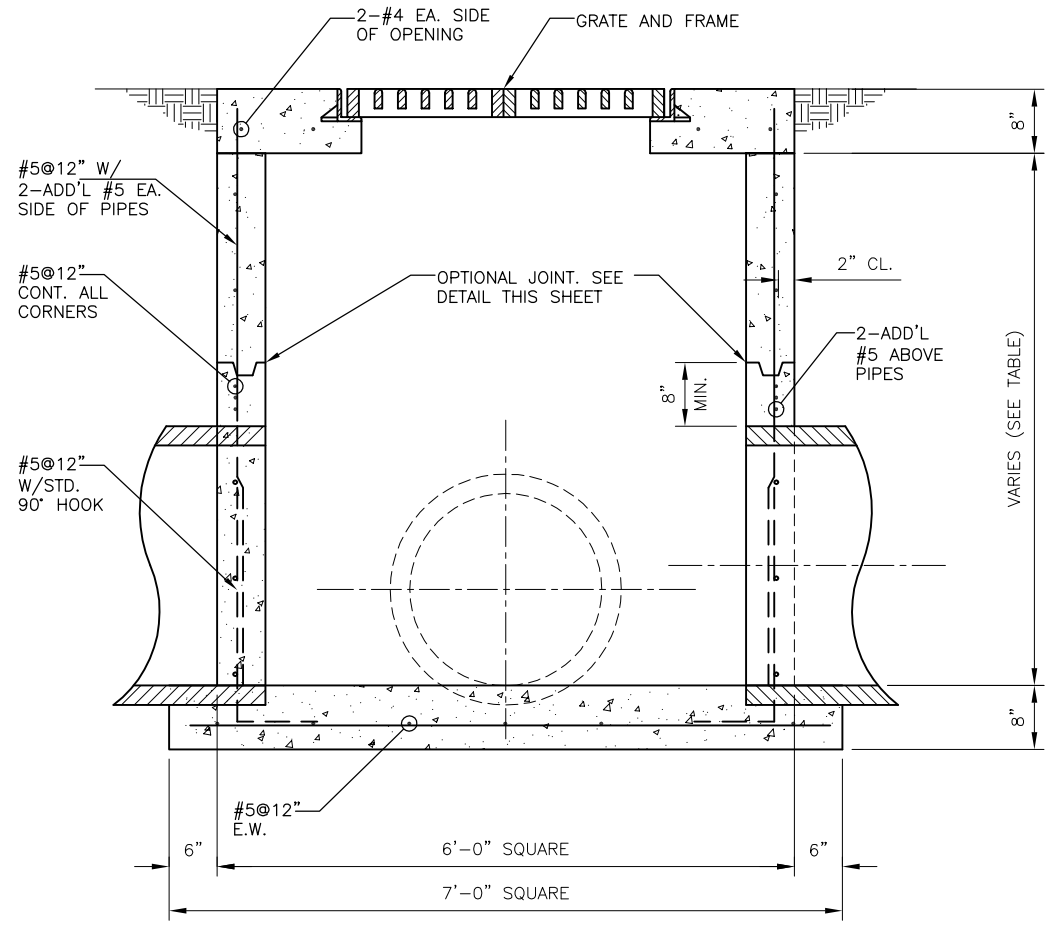
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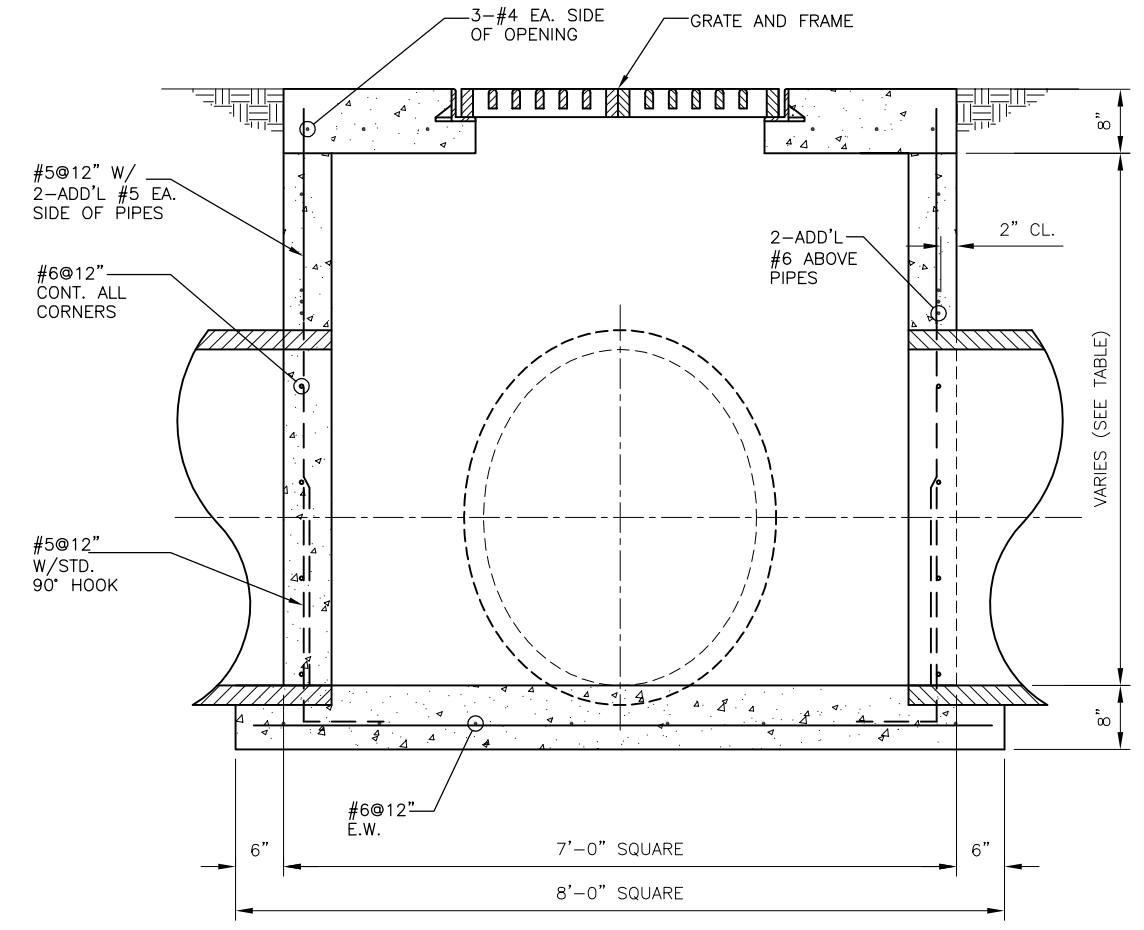
PLAN-72 INLET
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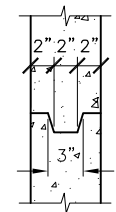
PLAN-84 INLET
 N.T.S.



SECTION A-A
 N.T.S.



SECTION B-B
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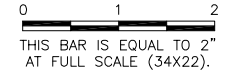


OPTIONAL JOINT DETAIL
 N.T.S.

INLET NOTE
 FRAME & GRATES ARE NEENAH R-3807
 OR APPROVED EQUAL.

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EXTEND TAXIWAY Y

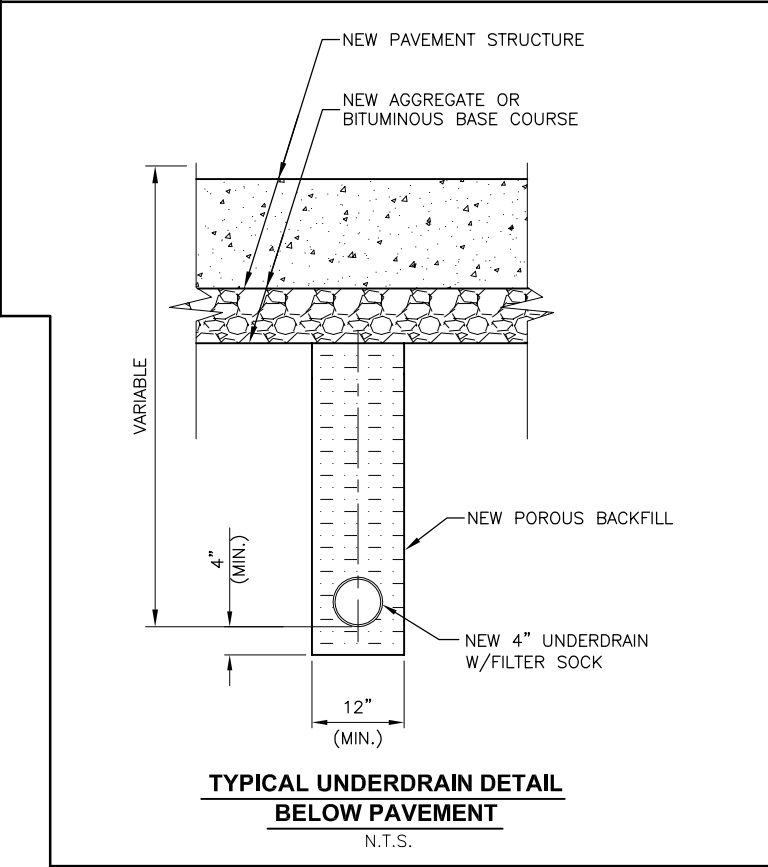
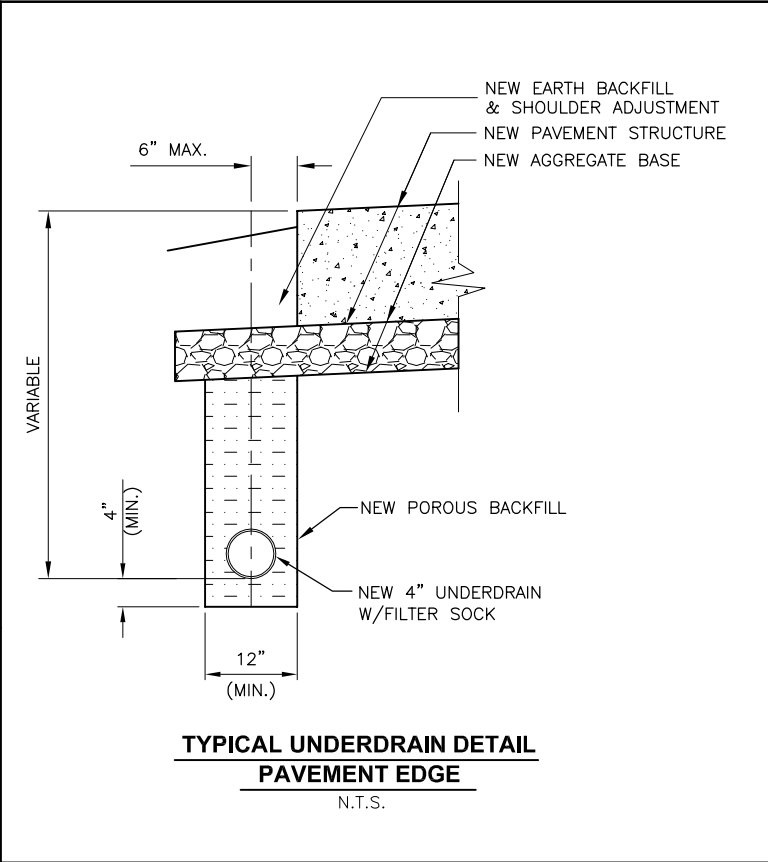
UNDERDRAIN DETAILS

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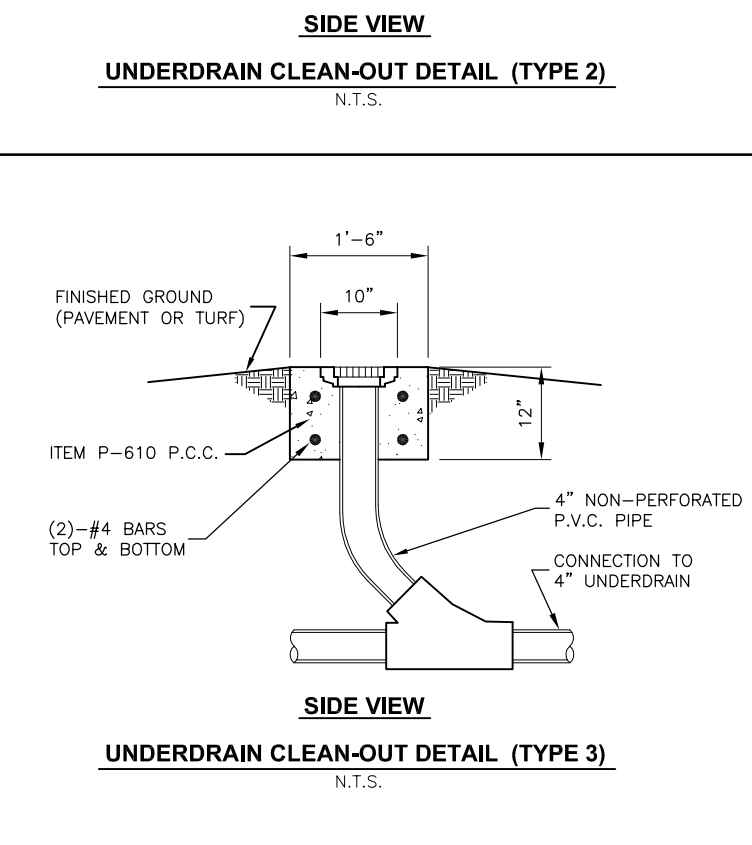
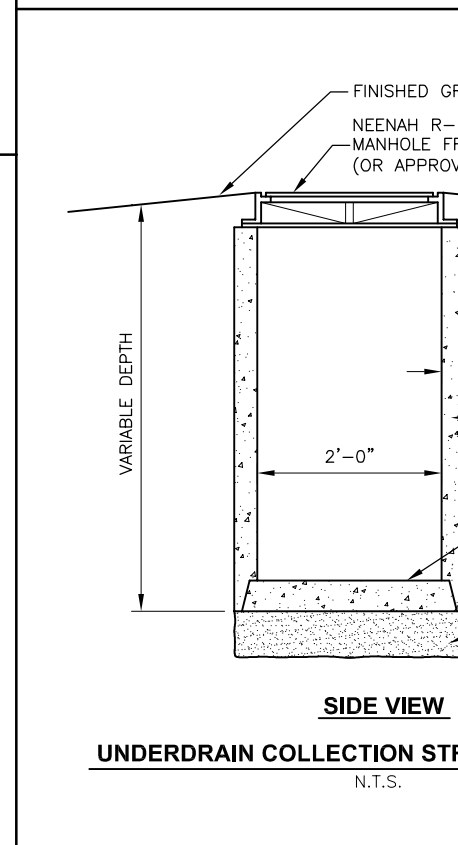
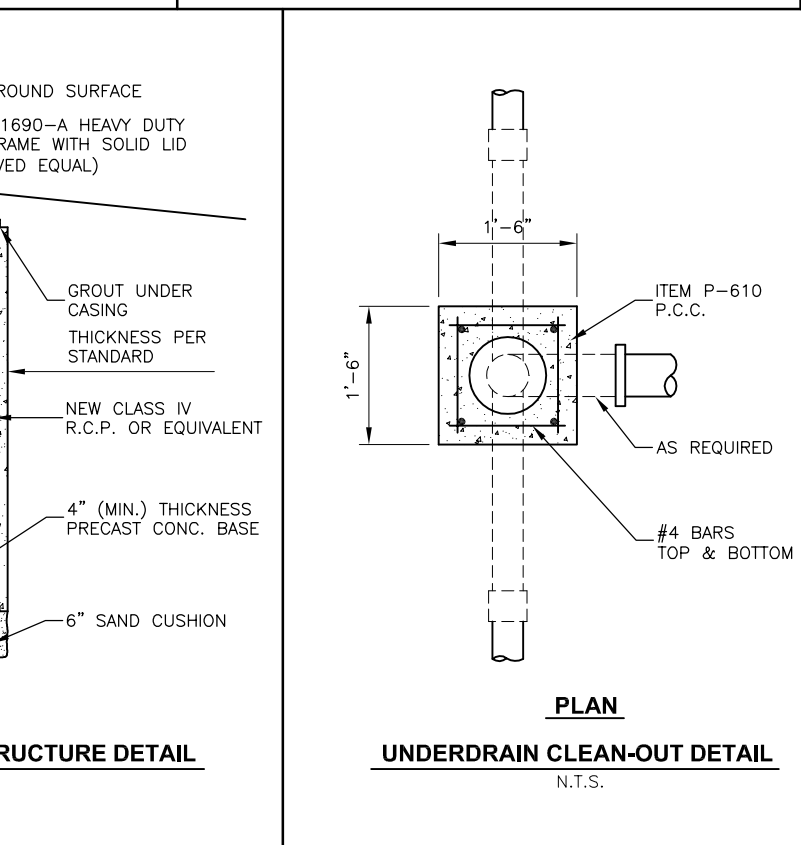
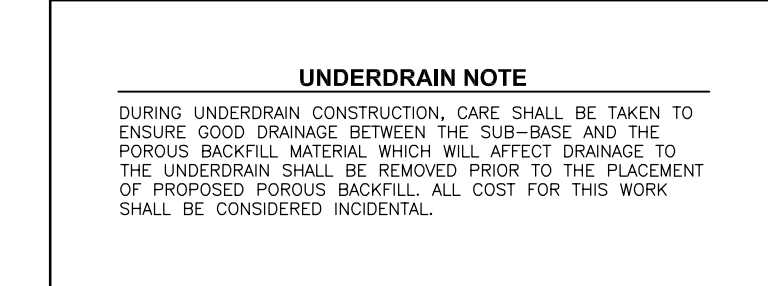
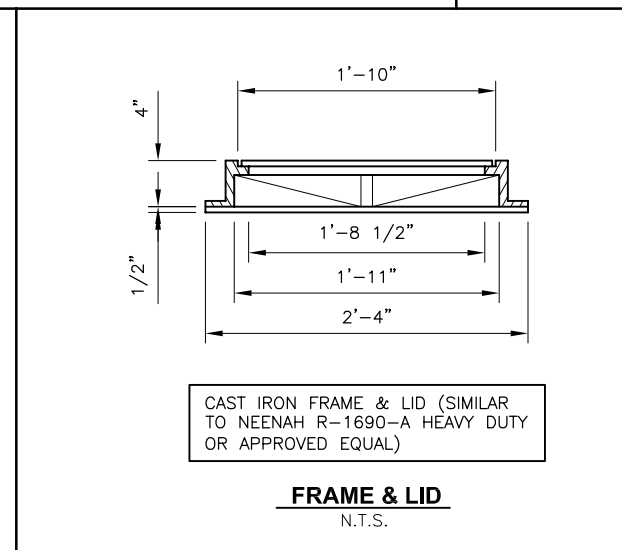
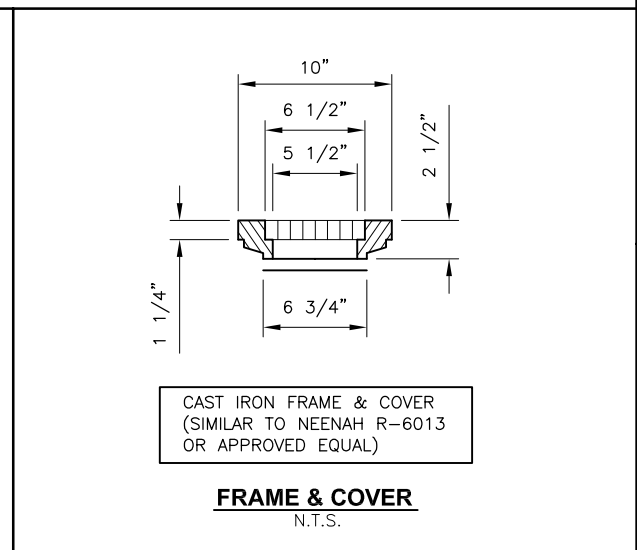
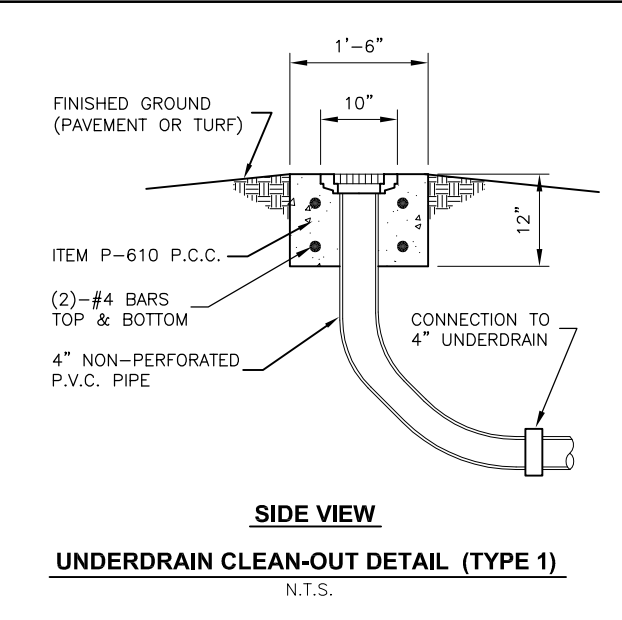
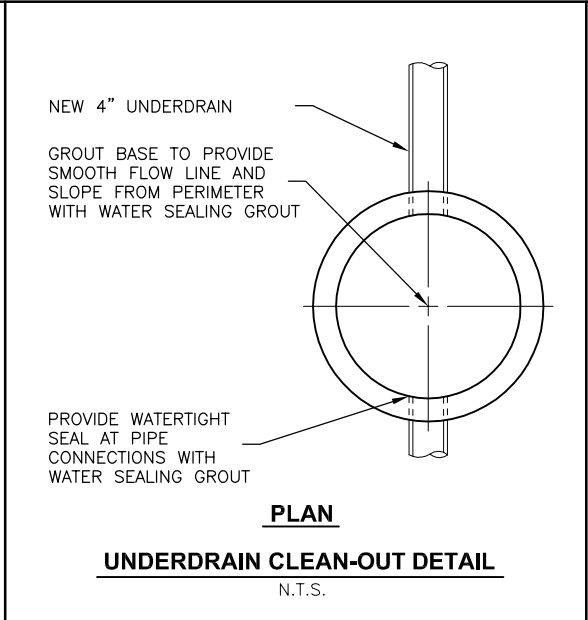
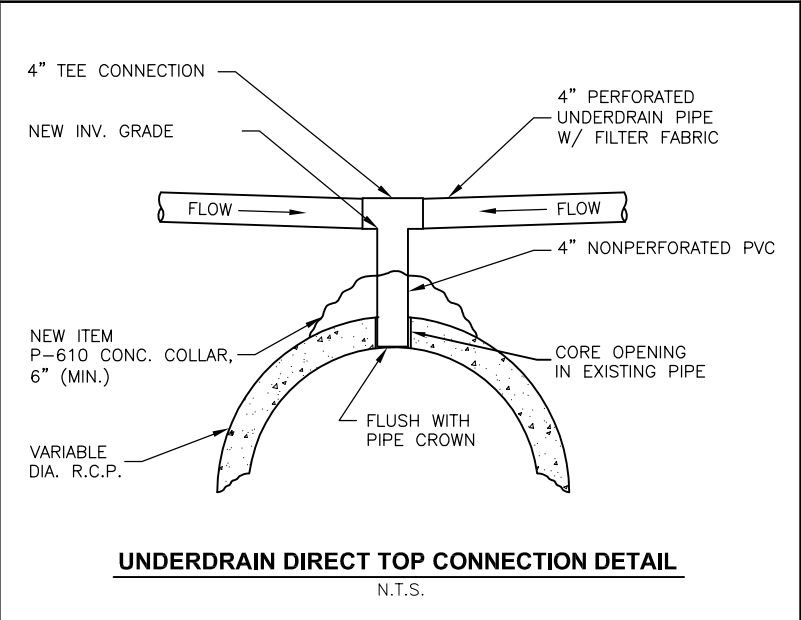
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| SHEET 42 OF 106 SHEETS | |

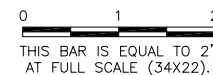


UNDERDRAIN NOTE
 DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE TO THE UNDERDRAIN SHALL BE REMOVED PRIOR TO THE PLACEMENT OF PROPOSED POROUS BACKFILL. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL.



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| UNDERDRAIN STRUCTURE SCHEDULE | | | | | | | |
|-------------------------------|----------------------|------------|----------|----------|-----------------|-----------------|-----------------|
| STRUCTURE | TYPE | STATION | OFFSET | RIM ELEV | NW INVERT ELEV. | SE INVERT ELEV. | NE INVERT ELEV. |
| CO-1 | CLEANOUT TYPE 1 | 154+84.97' | 57' RT | 589.24' | --- | --- | 586.24' |
| CS-1 | COLLECTION STRUCTURE | 159+75' | 19.5' RT | 588.25' | 584.38' | 584.38' | 584.38' |
| CO-2 | CLEANOUT TYPE 1 | 162+59.75' | 65' RT | 588.02' | 585.02' | --- | --- |
| CO-3 | CLEANOUT TYPE 1 | 163+16.72' | 86' RT | 588.11' | --- | --- | 585.11' |
| CO-4 | CLEANOUT TYPE 3 | 166+85.63' | 18.5' RT | 586.35' | 584.08' | 584.08' | --- |
| CO-5 | CLEAN OUT TYPE 3 | 170+85.63' | 18.5' RT | 585.30' | 583.05' | 583.05' | --- |
| DC-1 | DIRECT CONNECTION** | 172+08.25' | 168' RT | --- | --- | --- | 582.39' |

** UNDERDRAIN DIRECT CONNECTION DETAIL DOES NOT APPLY AT THIS DIRECT CONNECTION; CONNECT UNDERDRAIN PIPE INTO SIDEWALL OF EXISTING 18" R.C.P.

| UNDERDRAIN PIPE SCHEDULE | | | | | | | |
|--------------------------|--------------------|----------------------|-----------------|-------------------|--------------|-------|-------------------|
| LINE | UPSTREAM STRUCTURE | DOWNSTREAM STRUCTURE | UPSTREAM INVERT | DOWNSTREAM INVERT | LENGTH (FT.) | SLOPE | PIPE |
| P-1 | CO-1 | CS-1 | 586.24' | 584.38' | 500.2' | 0.37% | 4" PERFORATED |
| P-2 | CO-2 | CS-1 | 585.02' | 584.38' | 306.0' | 0.21% | 4" PERFORATED |
| P-3 | CS-1 | IN-1 | 584.38' | 583.63' | 75.0' | 1.00% | 4" NON-PERFORATED |
| P-4 | CO-3 | CO-4 | 585.11' | 584.08' | 400.0' | 0.26% | 4" PERFORATED |
| P-5 | CO-4 | CO-5 | 584.08' | 583.05' | 400.0' | 0.26% | 4" PERFORATED |
| P-6 | CO-5 | DC-1 | 583.05' | 582.39' | 255.0' | 0.26% | 4" PERFORATED |

| STORM SEWER PIPE SCHEDULE | | | | | | | | |
|---------------------------|------|------|-------------------|--------------------|-------------|-----------------|-------------------|-------|
| LINE | TYPE | SIZE | UPSTEAM STRUCTURE | DOWNTOWN STRUCTURE | LENGTH (FT) | UPSTREAM INVERT | DOWNSTREAM INVERT | SLOPE |
| SS-1 | RCP | 18" | IN-1 | EIN-1 | 347.5' | 580.50' | 578.76' | 0.50% |
| SS-2 | RCP | 18" | IN-4 | IN-5 | 61.7' | 567.30' | 567.06' | 0.38% |
| SS-3 | RCP | 18" | IN-5 | IN-6 | 169.2' | 567.06' | 566.42' | 0.38% |
| SS-4 | RCP | 30" | IN-7 | IN-8 | 582.0' | 576.61' | 574.99' | 0.28% |
| SS-5 | RCP | 42" | IN-8 | IN-9 | 816.0' | 574.99' | 572.26' | 0.33% |
| SS-6 | RCP | 42" | IN-9 | IN-10 | 634.0' | 572.26' | 567.53' | 0.75% |

| STORM SEWER STRUCTURE SCHEDULE | | | | | | | | |
|--------------------------------|---------------|----------|-----------|-----------|--------------|------------------------|--------------------------------------|---|
| STRUCTURE | BASELINE | STATION | OFFSET | RIM ELEV. | INVERT ELEV. | STRUCTURE TYPE | CASTING | COMMENTS |
| IN-1 | TX Y | 159+75.0 | 55.0' LT | 585.00' | 580.50' | 48" X 48" SQUARE INLET | NEENAH R-3807 OR APPROVED EQUIVALENT | NEW INLET |
| IN-2 | ARFF RD. | 307+88.6 | 34.0' RT | 586.32' | 581.36' | 72" X 72" SQUARE INLET | NEENAH R-3807 OR APPROVED EQUIVALENT | INLET TO BE PLACED OVER EXISTING 36" RCP |
| IN-3 | ARFF RD. | 308+98.9 | 52.4' LT | 584.65' | 580.75' | 72" X 72" SQUARE INLET | NEENAH R-3807 OR APPROVED EQUIVALENT | REMOVE AND REPLACE EXISTING INLET |
| IN-4 | PERIMETER RD. | 6+04.8 | 26.7' LT | 570.30'' | 567.30' | 48" X 48" SQUARE INLET | NEENAH R-3807 OR APPROVED EQUIVALENT | NEW INLET |
| IN-5 | PERIMETER RD. | 6+04.8 | 35.0' RT | 571.34' | 567.06' | 48" X 48" SQUARE INLET | NEENAH R-3807 OR APPROVED EQUIVALENT | NEW INLET |
| IN-6 | PERIMETER RD. | 4+36.2 | 49.5' RT | 571.20' | 566.42' | 48" X 48" SQUARE INLET | NEENAH R-3807 OR APPROVED EQUIVALENT | REMOVE EXISTING 24" CMP FES AND INSTALL NEW INLET |
| IN-7 | TX Y | 165+49.0 | 391.6' LT | 583.65' | 576.61' | 72" X 72" SQUARE INLET | NEENAH R-3807 OR APPROVED EQUIVALENT | REMOVE AND REPLACE EXISTING INLET |
| IN-8 | TX Y | 171+31.0 | 380.7' LT | 582.49' | 574.99' | 72" X 72" SQUARE INLET | NEENAH R-3807 OR APPROVED EQUIVALENT | REMOVE AND REPLACE EXISTING INLET |
| IN-9 | TX Y | 179+47.0 | 365.4' LT | 582.43' | 572.26' | 84" X 84" SQUARE INLET | NEENAH R-3807 OR APPROVED EQUIVALENT | REMOVE AND REPLACE EXISTING INLET |
| IN-10 | TX Y | 185+81.0 | 353.2' LT | 578.49' | 567.53' | 72" X 72" SQUARE INLET | NEENAH R-3807 OR APPROVED EQUIVALENT | REMOVE AND REPLACE EXISTING INLET |



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EXTEND TAXIWAY Y

DRAINAGE SCHEDULE

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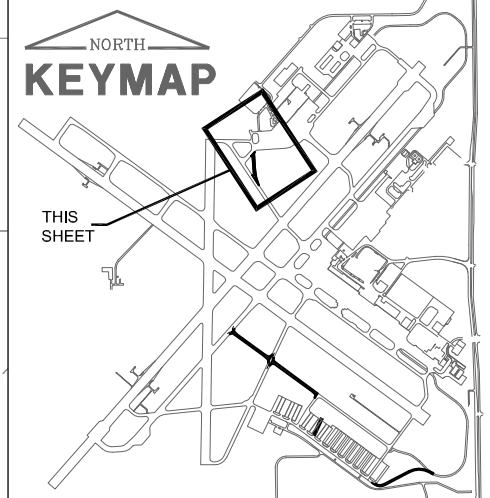
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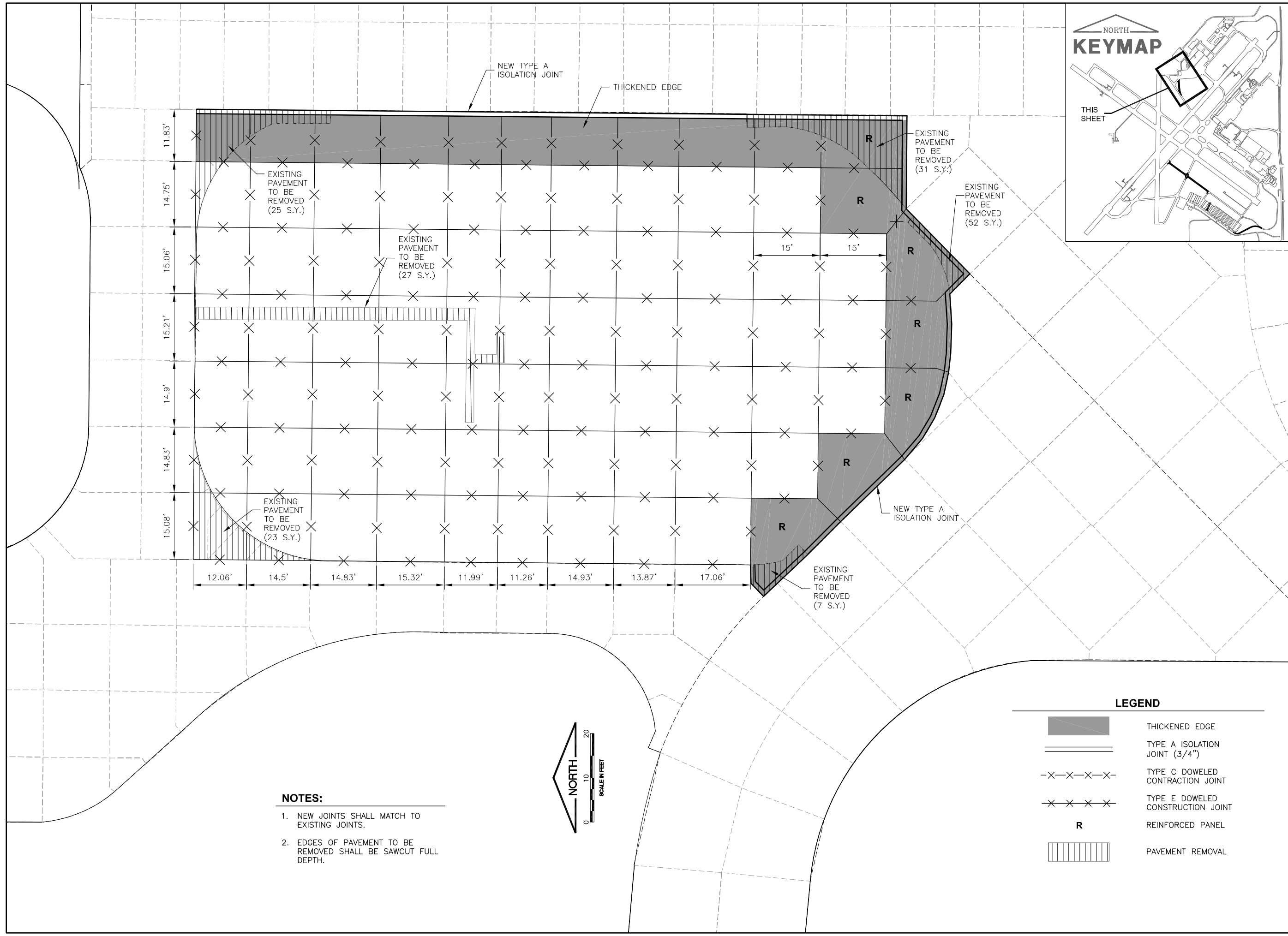


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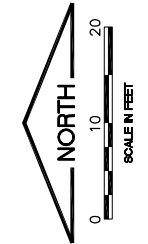
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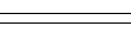
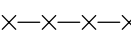
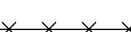

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



- NOTES:**
1. NEW JOINTS SHALL MATCH TO EXISTING JOINTS.
 2. EDGES OF PAVEMENT TO BE REMOVED SHALL BE SAWCUT FULL DEPTH.



LEGEND

-  THICKENED EDGE
-  TYPE A ISOLATION JOINT (3/4")
-  TYPE C DOWELED CONTRACTION JOINT
-  TYPE E DOWELED CONSTRUCTION JOINT
- REINFORCED PANEL
-  PAVEMENT REMOVAL



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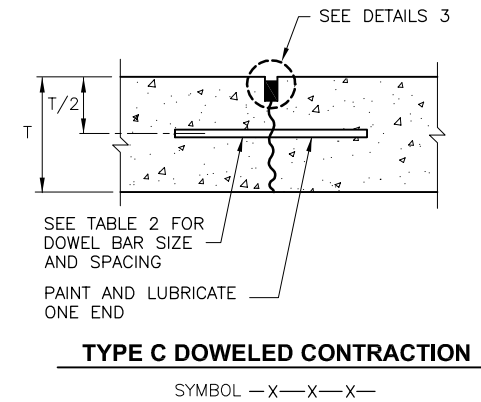
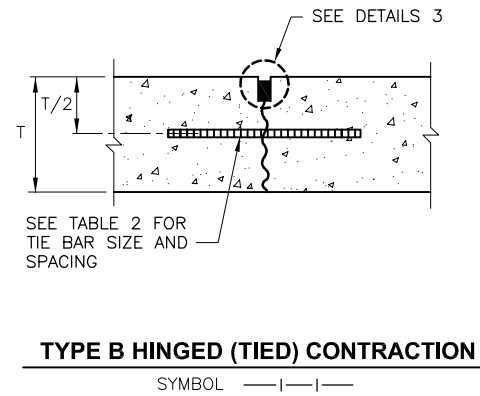
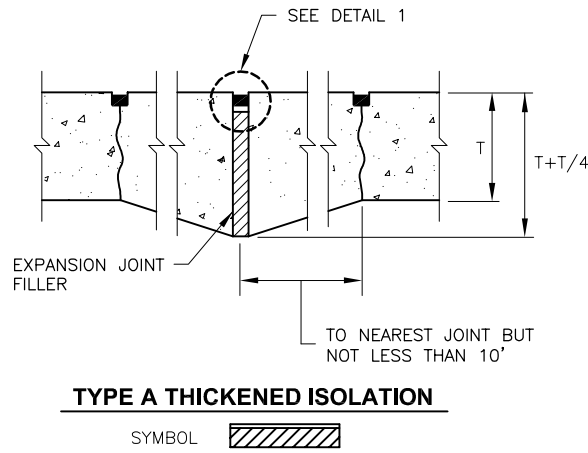
EXTEND TAXIWAY Y
 JOINTING PLAN

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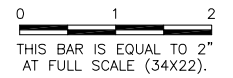
| PAVEMENT THICKNESS T - INCHES | DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I=(T/3) \pm 1/4"$ |
|-------------------------------|--|
| 5 | 1.67" |
| 6 | 2.00" |
| 7 | 2.33" |
| 8 | 2.67" |
| 9 | 3.00" |
| 10 | 3.33" |
| 11 | 3.67" |
| 12 | 4.00" |
| 13 | 4.33" |
| 14 | 4.67" |
| 15 | 5.00" |
| 16 | 5.33" |
| 17 | 5.67" |
| 18 | 6.00" |



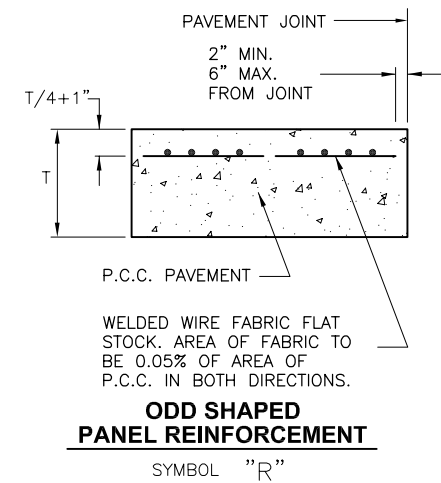
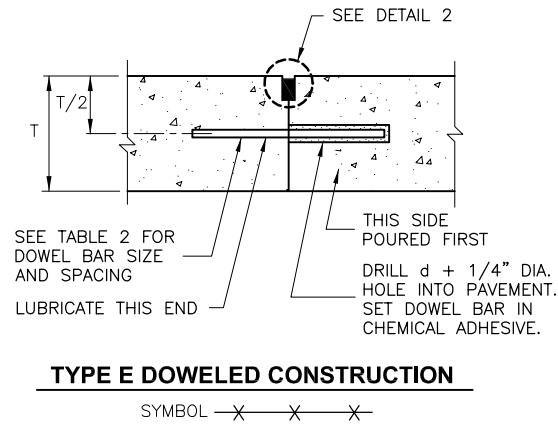
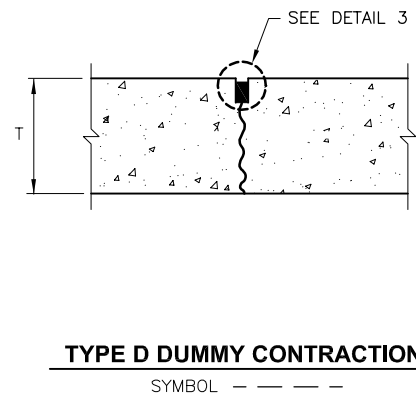
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| PAVEMENT THICKNESS T - INCHES | DOWEL BAR DETAILS | | | TIE BAR DETAILS | | |
|-------------------------------|-------------------|--------|---------|-----------------|--------|---------|
| | DIA. | LENGTH | SPACING | BAR SIZE | LENGTH | SPACING |
| 5 | 5/8" | 12" | 12" | #4 | 24" | 30" |
| 6 | 3/4" | 18" | 12" | #5 | 30" | 30" |
| 7 | 3/4" | 18" | 12" | #5 | 30" | 30" |
| 8 | 1" | 19" | 12" | #5 | 30" | 30" |
| 9 | 1" | 19" | 12" | #5 | 30" | 30" |
| 10 | 1" | 19" | 12" | #5 | 30" | 30" |
| 11 | 1" | 19" | 12" | #5 | 30" | 30" |
| 12 | 1" | 19" | 12" | #5 | 30" | 30" |
| 13 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 14 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 15 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 16 | 1 - 1/4" | 20" | 15" | #5 | 30" | 30" |
| 17 | 1 - 1/2" | 20" | 18" | #5 | 30" | 30" |
| 18 | 1 - 1/2" | 20" | 18" | #5 | 30" | 30" |

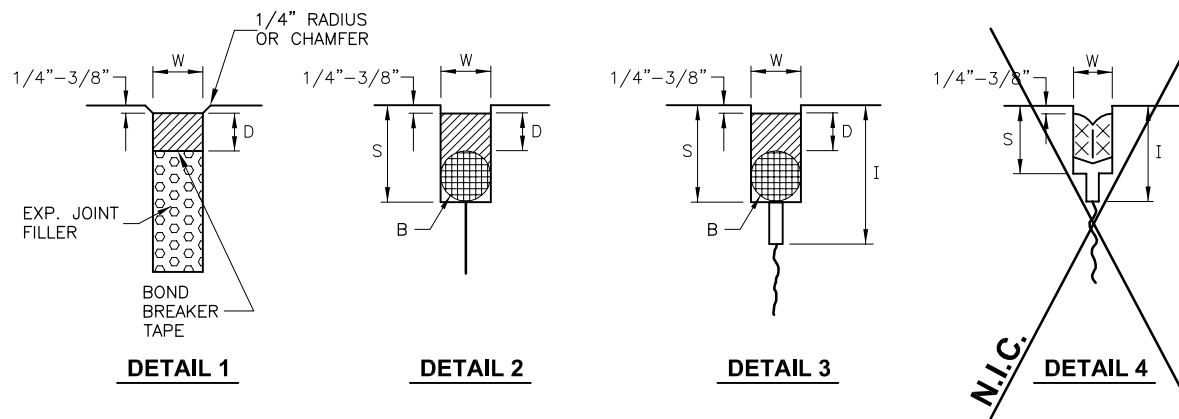


| | DETAIL 1 | DETAIL 2 | DETAIL 3 | DETAIL 4 |
|-------------------------------------|----------|----------|----------|-----------------|
| W=WIDTH OF SEALANT RESERVOIR (IN.) | 3/4 | 1/2 | 1/2 | 3/8" COMPRESSED |
| D=DEPTH OF SEALANT RESERVOIR (IN.) | 3/4 | 1/2 | 1/2 | N/A |
| B=BACKER ROD DIAMETER (IN.) | N/A | 5/8 | 5/8 | N/A |
| S=SECOND SAWCUT DEPTH (IN.) MINIMUM | N/A | 1-3/8 | 1-3/8 | 1-1/2" |

JOINT NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

JOINT SEALING DETAILS



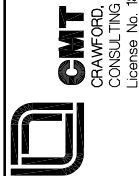
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**EXTEND TAXIWAY Y
JOINTING DETAILS**

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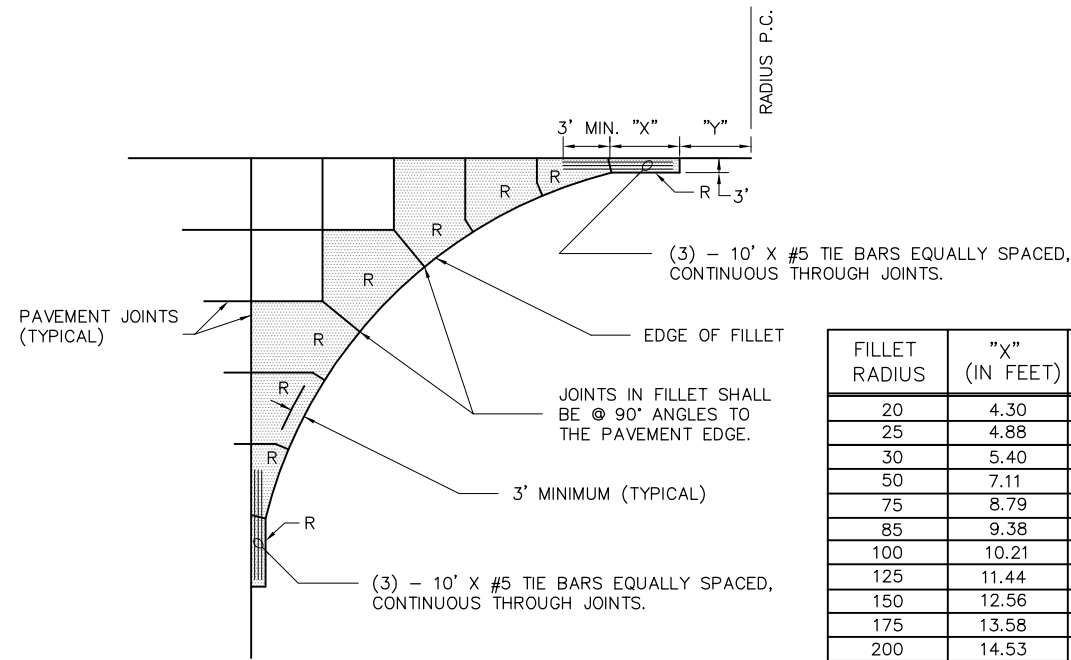
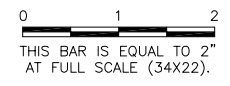


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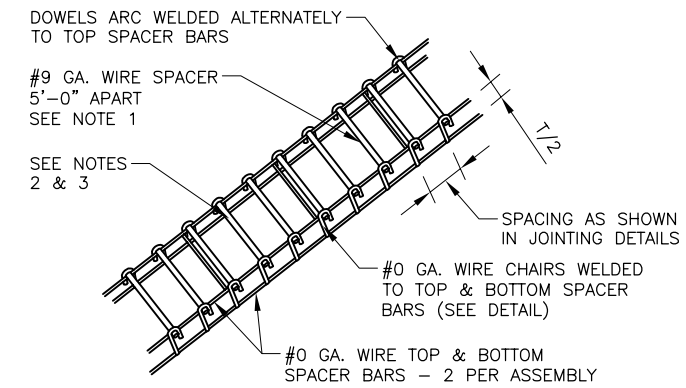
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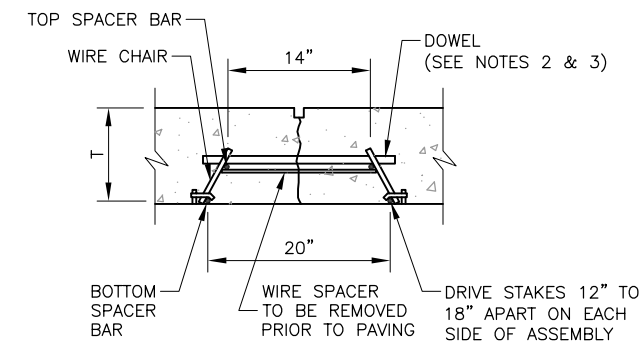
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|---------------|---------------|---------------|
| 20 | 4.30 | 6.24 |
| 25 | 4.88 | 7.00 |
| 30 | 5.40 | 7.68 |
| 50 | 7.11 | 9.95 |
| 75 | 8.79 | 12.21 |
| 85 | 9.38 | 13.00 |
| 100 | 10.21 | 14.11 |
| 125 | 11.44 | 15.78 |
| 150 | 12.56 | 17.29 |
| 175 | 13.58 | 18.68 |
| 200 | 14.53 | 19.98 |

Ⓡ DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WELDED WIRE FABRIC AS SHOWN ON JOINTING DETAILS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)

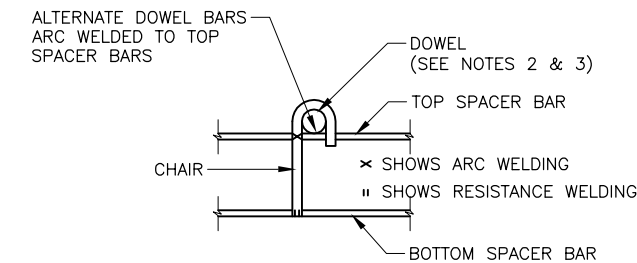
FILLET DETAIL & FILLET REINFORCING LAYOUT
N.T.S.



DOWEL BASKET ASSEMBLY DETAIL
N.T.S.



DOWEL BAR INSTALLATION DETAIL
N.T.S.

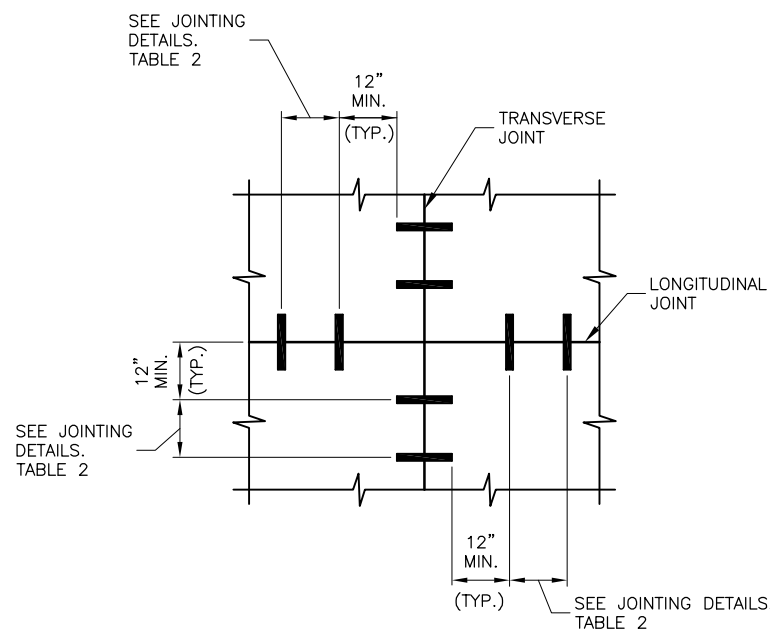


TYPICAL DOWEL BASKET ELEVATION DETAIL SHOWING CHAIR
N.T.S.

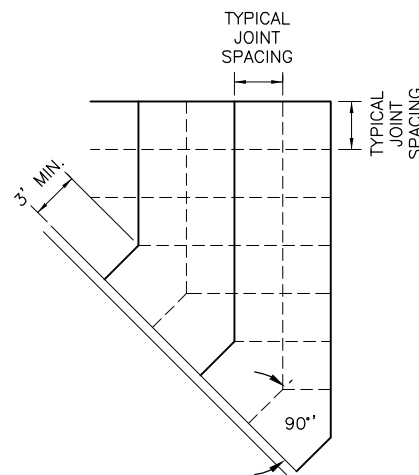
DOWEL BASKET DETAILS
N.T.S.

DOWEL BASKET NOTES

- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.



DOWEL PLACEMENT DETAIL
N.T.S.



JOINTING AT SKEWED EDGE
N.T.S.

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EXTEND TAXIWAY Y
 PAVING AND MISCELLANEOUS DETAILS

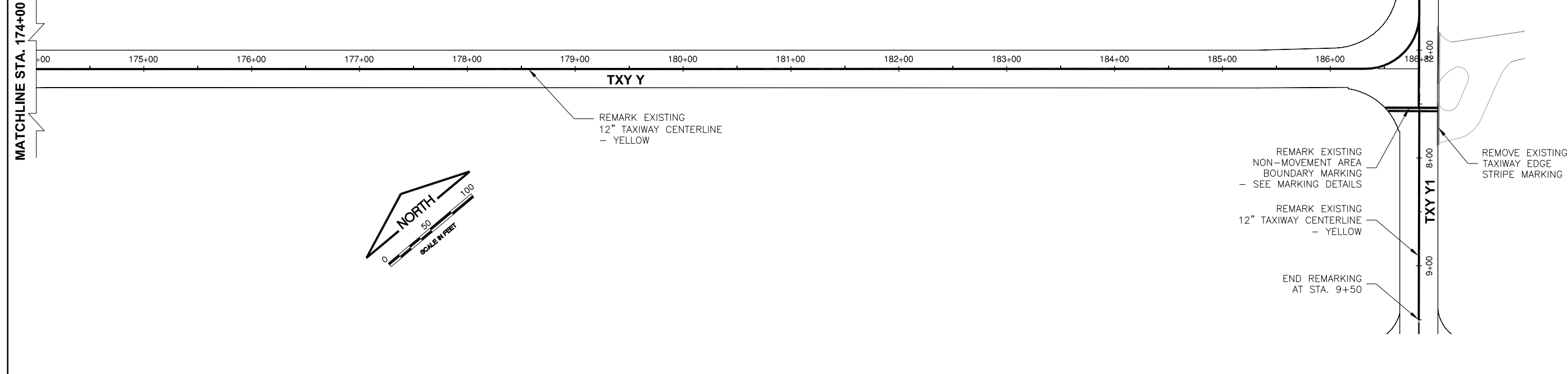
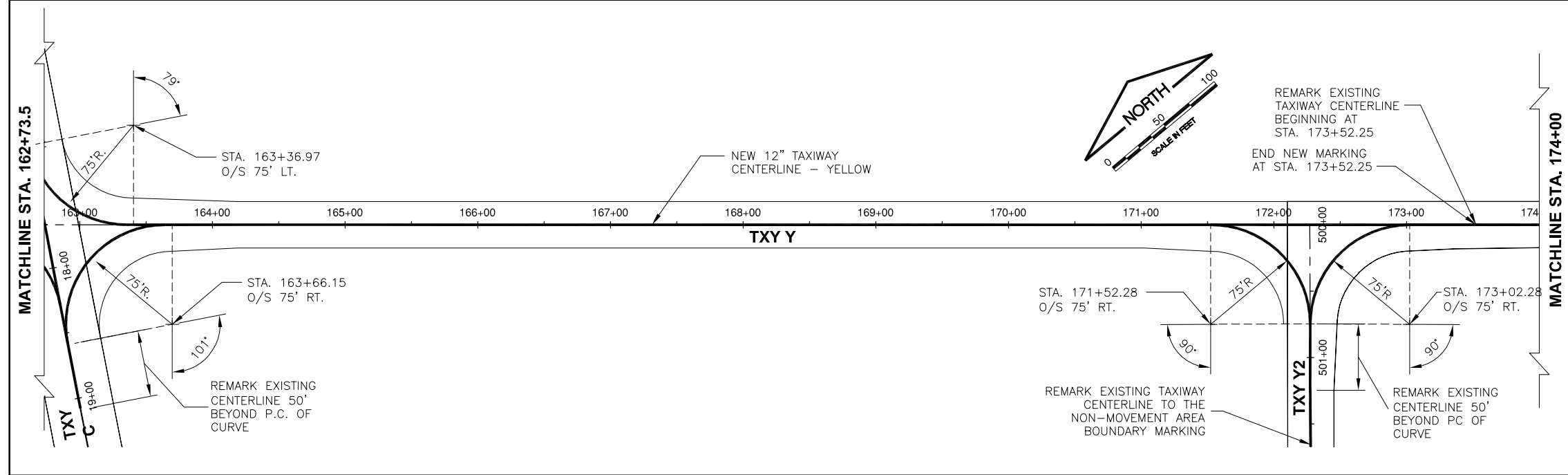
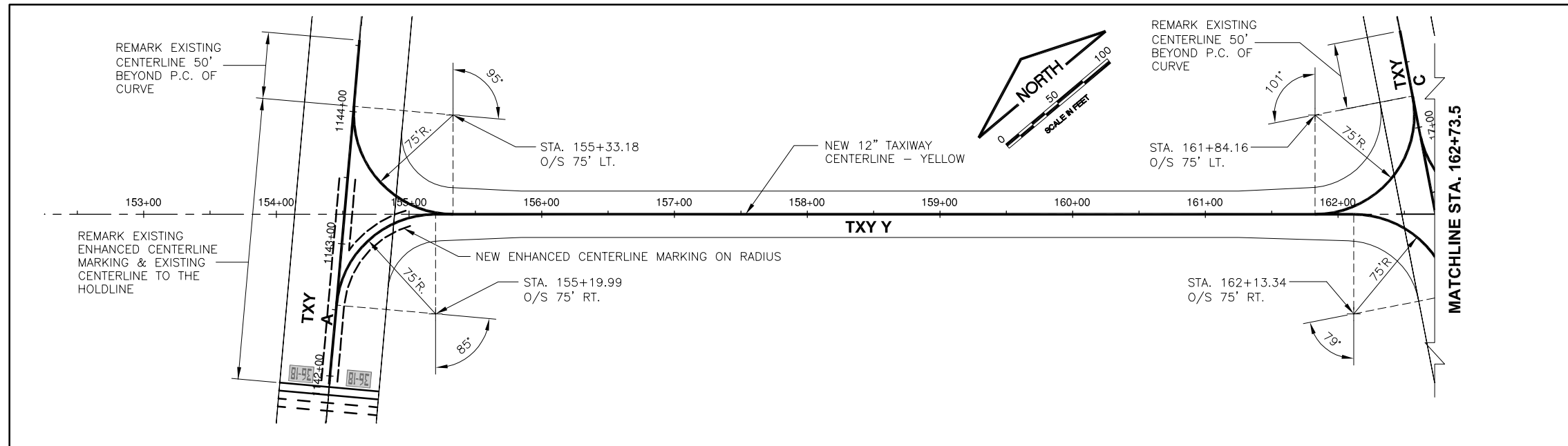
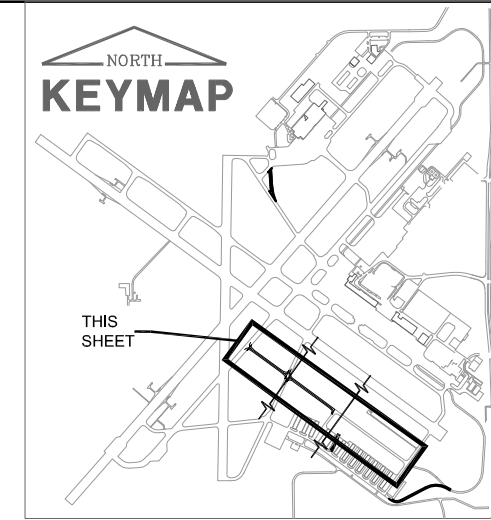
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



LEGEND
 ——— EXISTING MARKING
 ——— NEW MARKING



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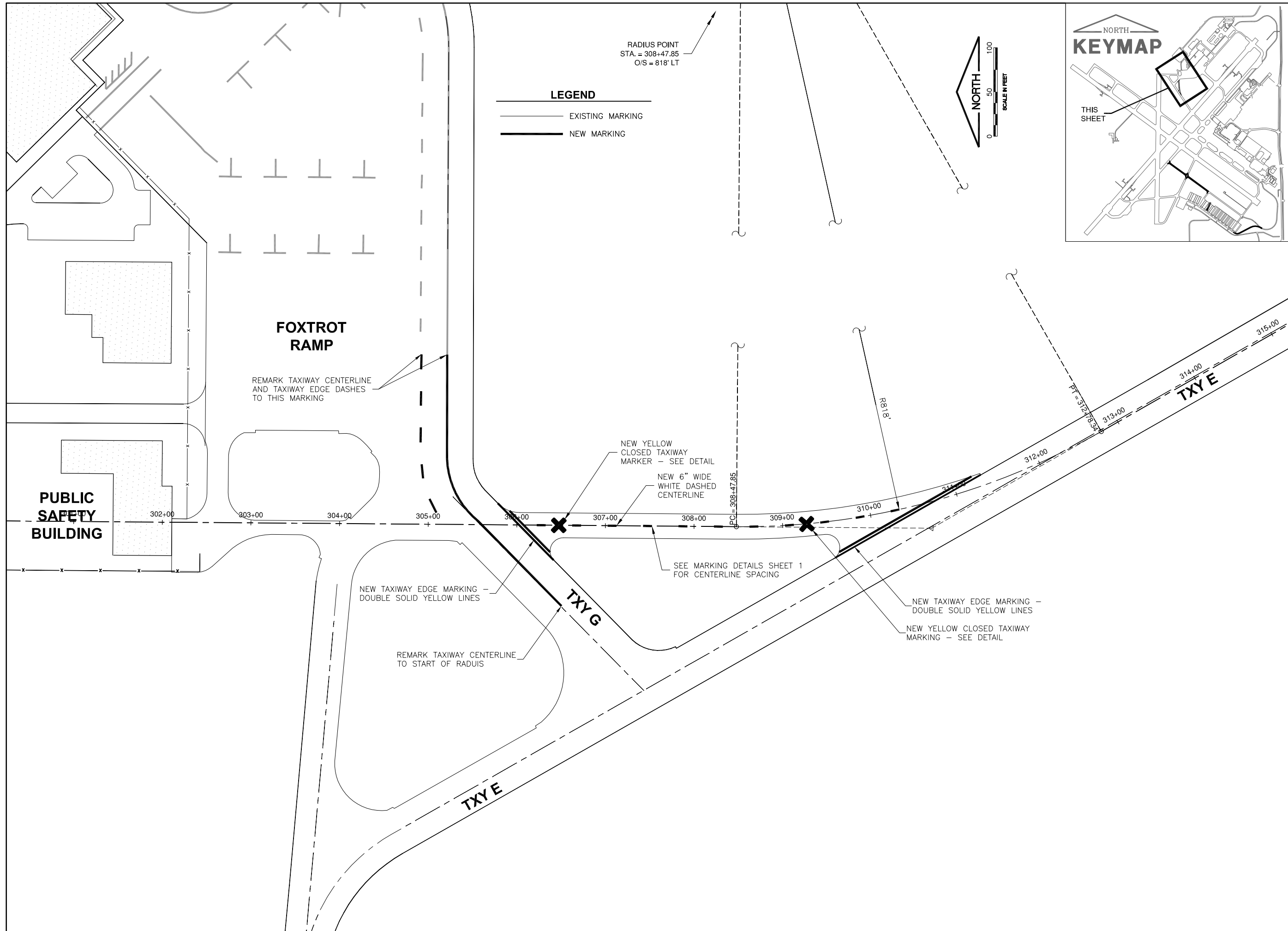
EXTEND TAXIWAY Y
MARKING PLAN 1

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DESIGN BY: RLV/JMW
 DRAWN BY: CMT
 CHECKED BY: **RLV**
 APPROVED BY: **RLV**
 DATE: APRIL 27, 2012
 JOB No: 110350400

SPI-4156
 3-17-0096-XX

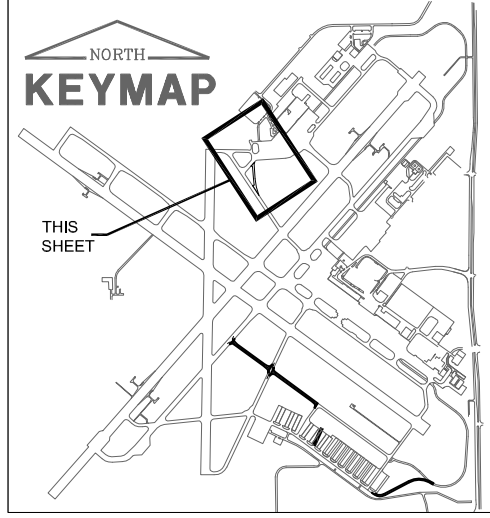
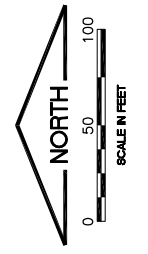


RADIUS POINT
 STA. = 308+47.85
 O/S = 818' LT

LEGEND

— EXISTING MARKING

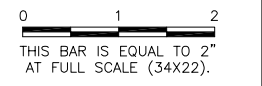

— NEW MARKING



K:\Springfield\1103504\Draw\Sheets
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 UPDATE BY: TJ Heavysides
 PLOT DATE: 5/8/2012 9:11 PM
 KEYMAP
 SPIBASE_EXISTGEO
 1103504-C-SPAL
 BASE_PROP_GEO
 BASE_EXIST_TOPO
 SPI-BASE CURRENT GEOMETRY
 BASE_PROP_MARK

CA014

| REVISIONS | | |
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| NUMBER | BY | DATE |
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SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

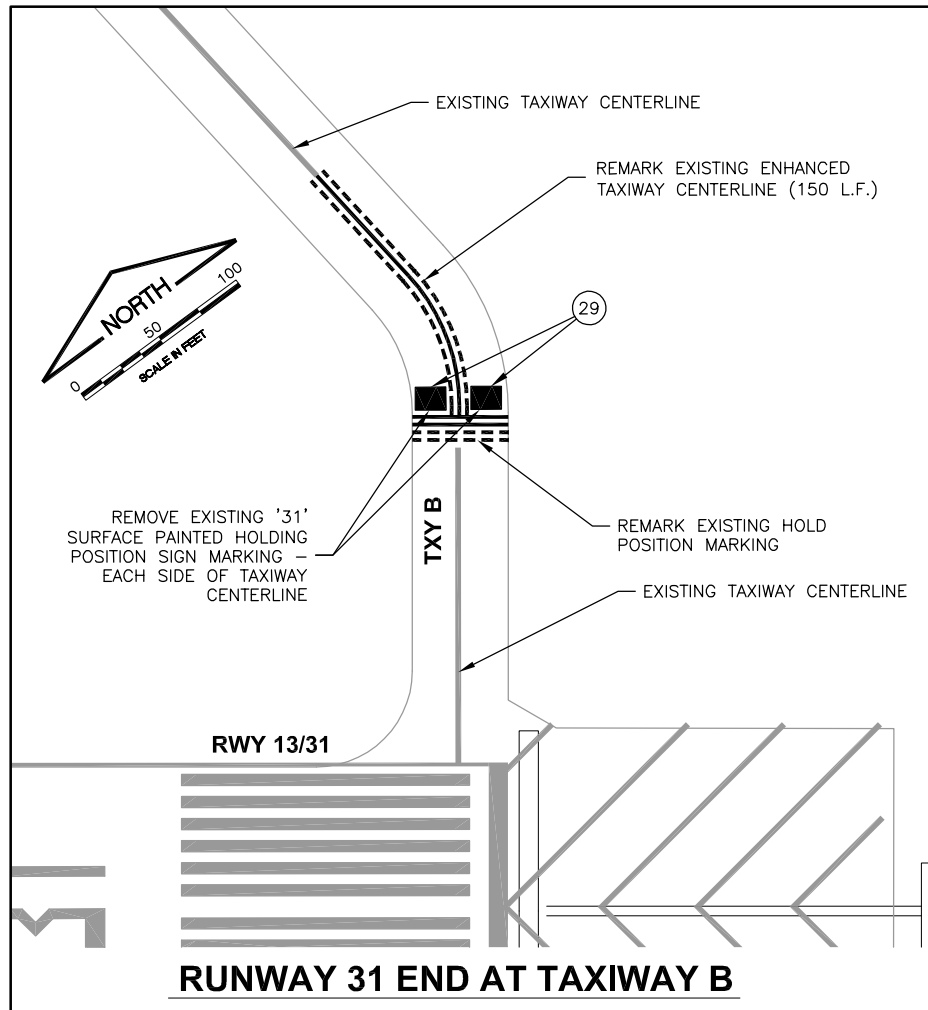
EXTEND TAXIWAY Y
MARKING PLAN 2

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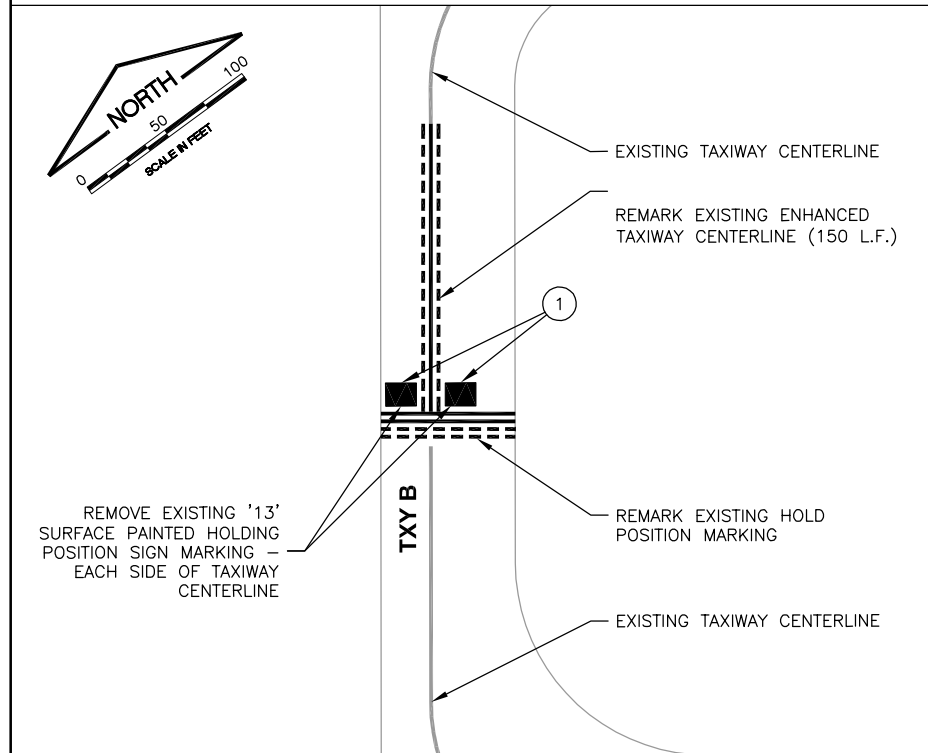


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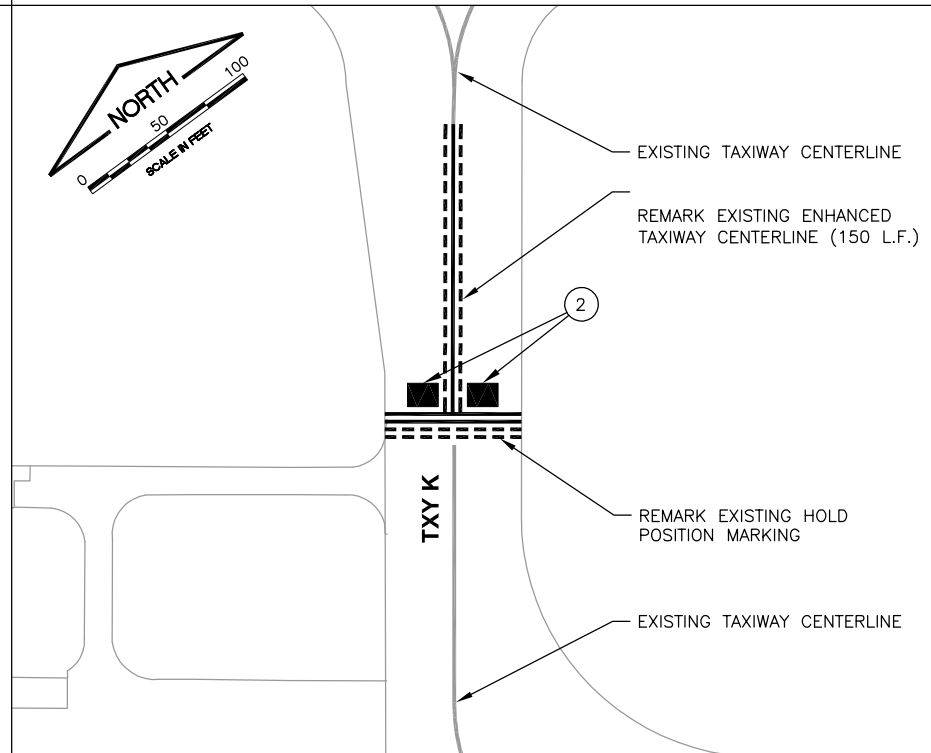
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|--------------------------|----------------|
| DESIGN BY: | RLV/JMW |
| DRAWN BY: | CMT |
| CHECKED BY: | RLV |
| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET 48 OF 106 SHEETS | |



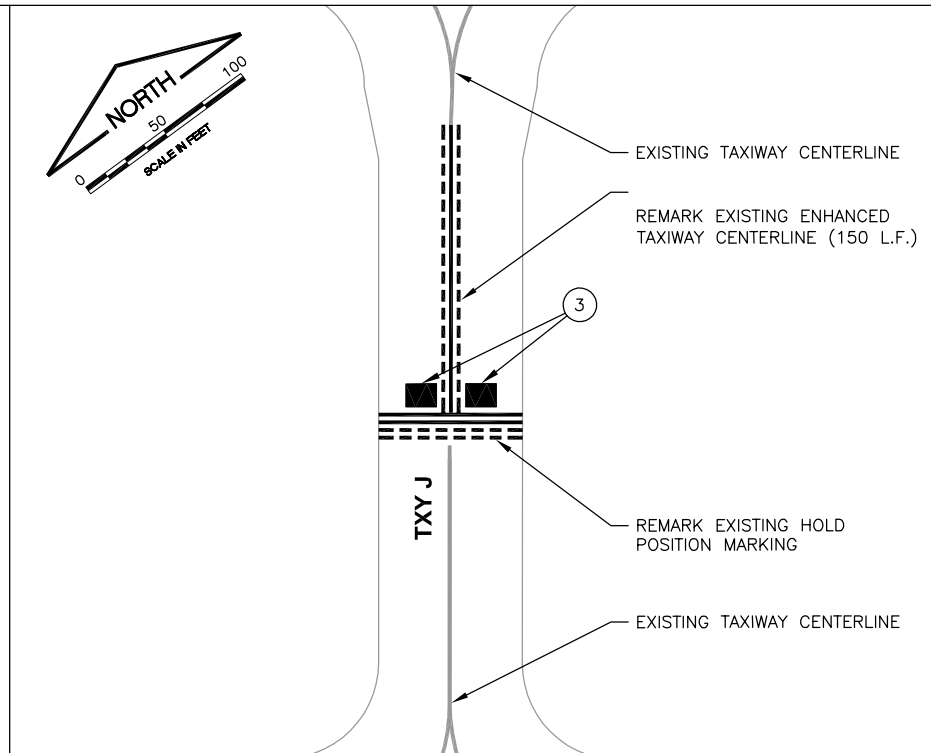
RUNWAY 31 END AT TAXIWAY B



RUNWAY 13 END AT TAXIWAY B



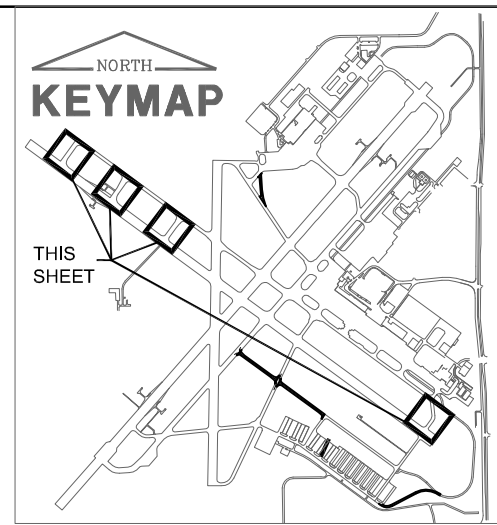
RUNWAY 13/31 AT TAXIWAY K



RUNWAY 13/31 AT TAXIWAY J

NOTES:

1. THE SURFACE PAINTED HOLDING POSITION MARKINGS SHOWN ARE USED TO EXPRESS THE APPROXIMATE LOCATIONS. SEE MARKING DETAILS AND MARKING SCHEDULE FOR DIMENSIONS AND FURTHER INFORMATION.
2. ALL SURFACE PAINTED HOLDING POSITION MARKINGS SHALL CONFORM TO THE REGULATIONS SET FORTH IN THE FAA ADVISORY CIRCULAR AC 150/5340-1, LATEST EDITION.



LEGEND

- EXISTING MARKING
- NEW MARKING
- Ⓝ MARKING NUMBER - SEE DETAILS AND SCHEDULE FOR FURTHER INFORMATION

K:\Springfield\1103504\Draw\Sheets
 FILE: MARKING PLAN 3.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/8/2012 9:12 PM
 SPIBASE_EXISTGEO
 KEYMAP
 BASE_EXISTMARK
 BASE_PROP_GEO

CA014

| REVISIONS | | |
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| NUMBER | BY | DATE |
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**SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

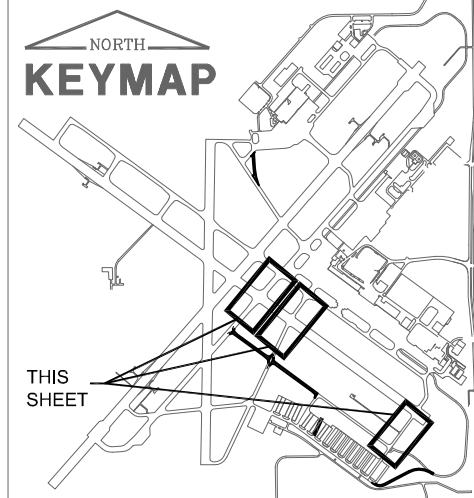
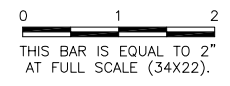
**EXTEND TAXIWAY Y
 MARKING PLAN 3**

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| CHECKED BY: | RLV |
| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |

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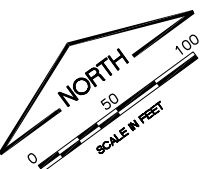
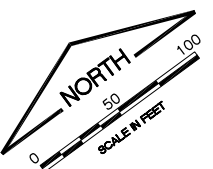
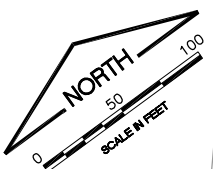
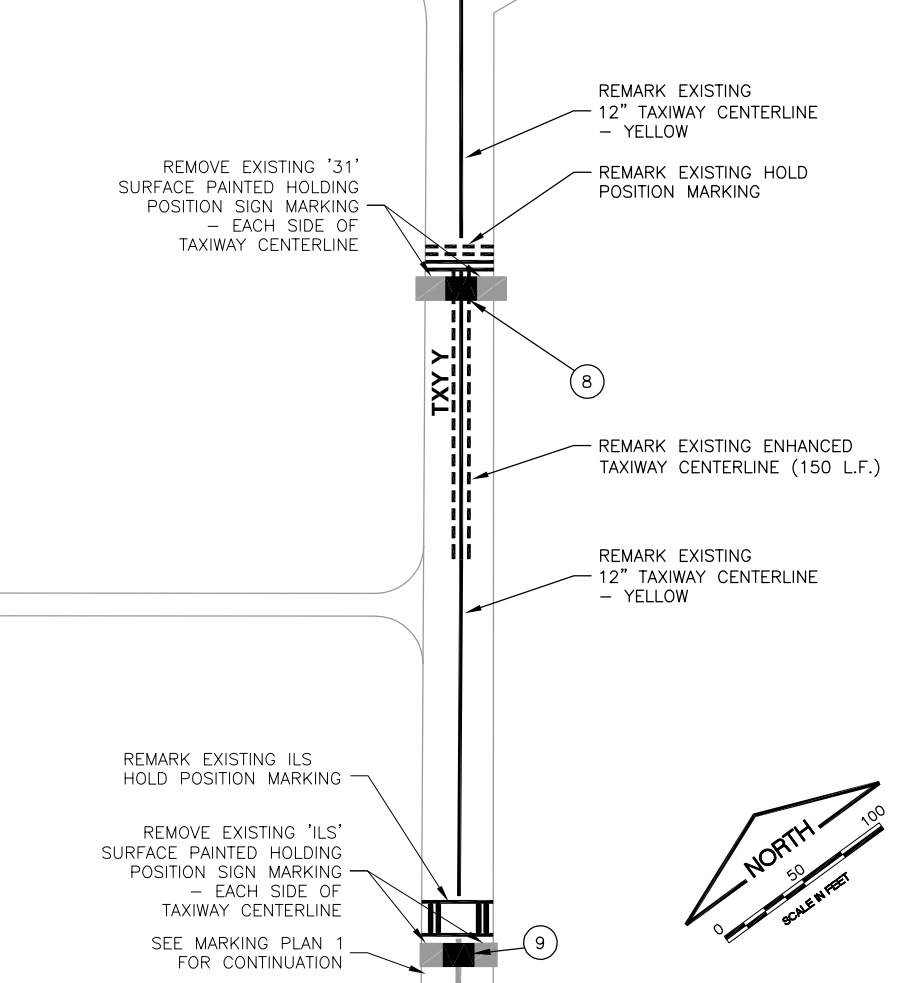
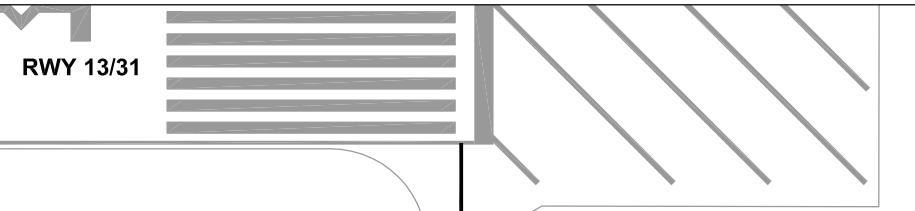
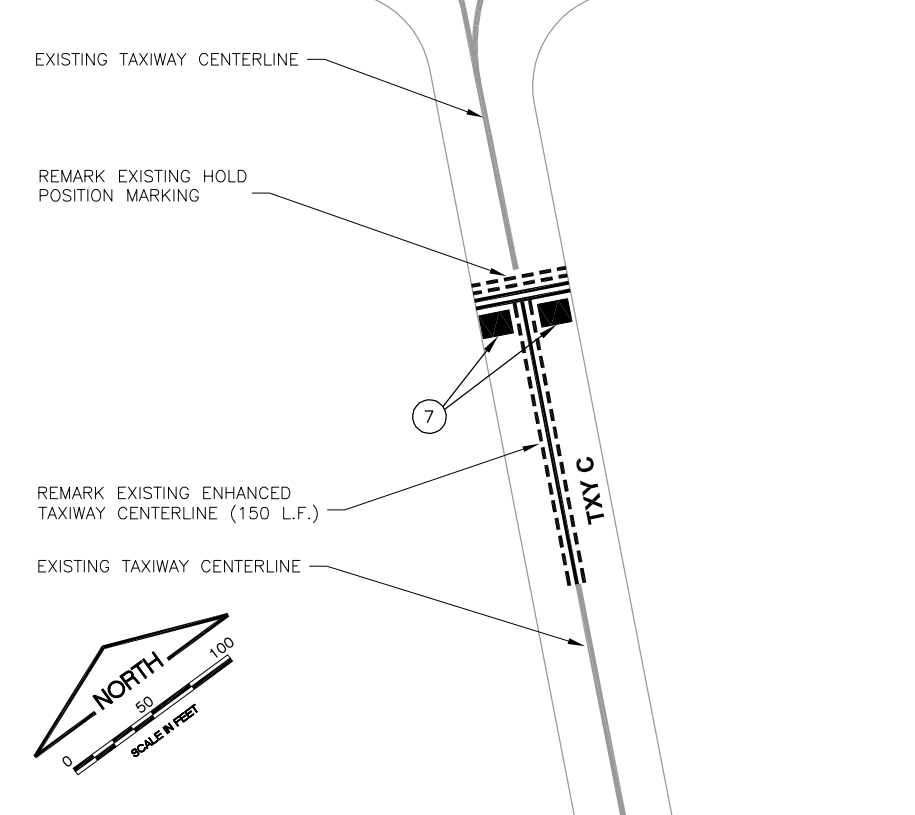
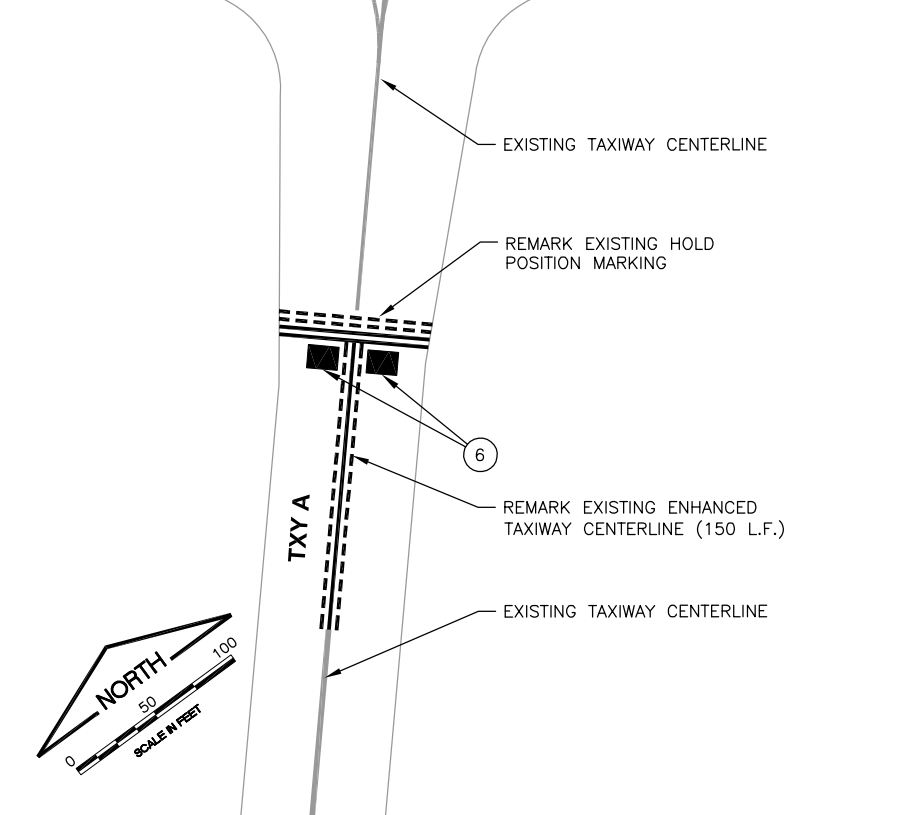
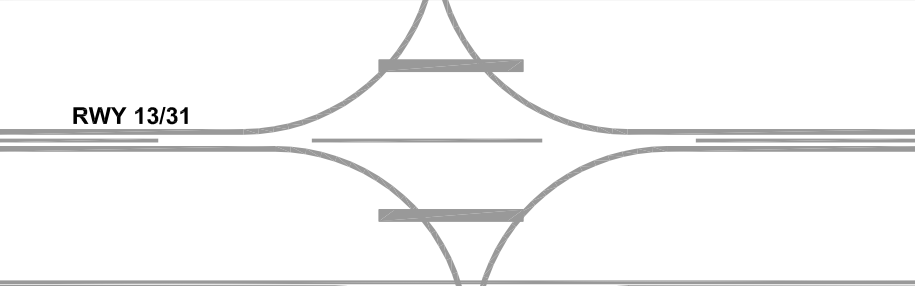
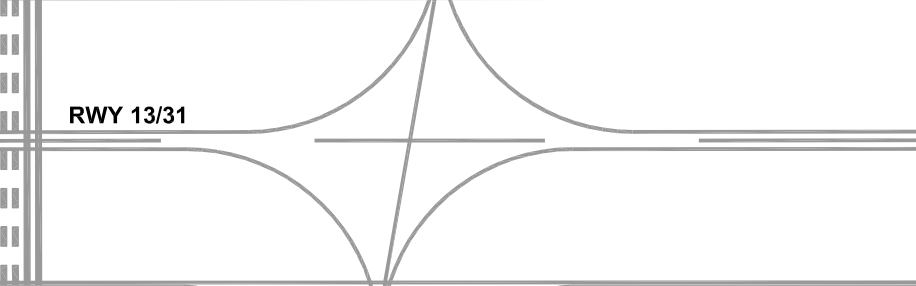
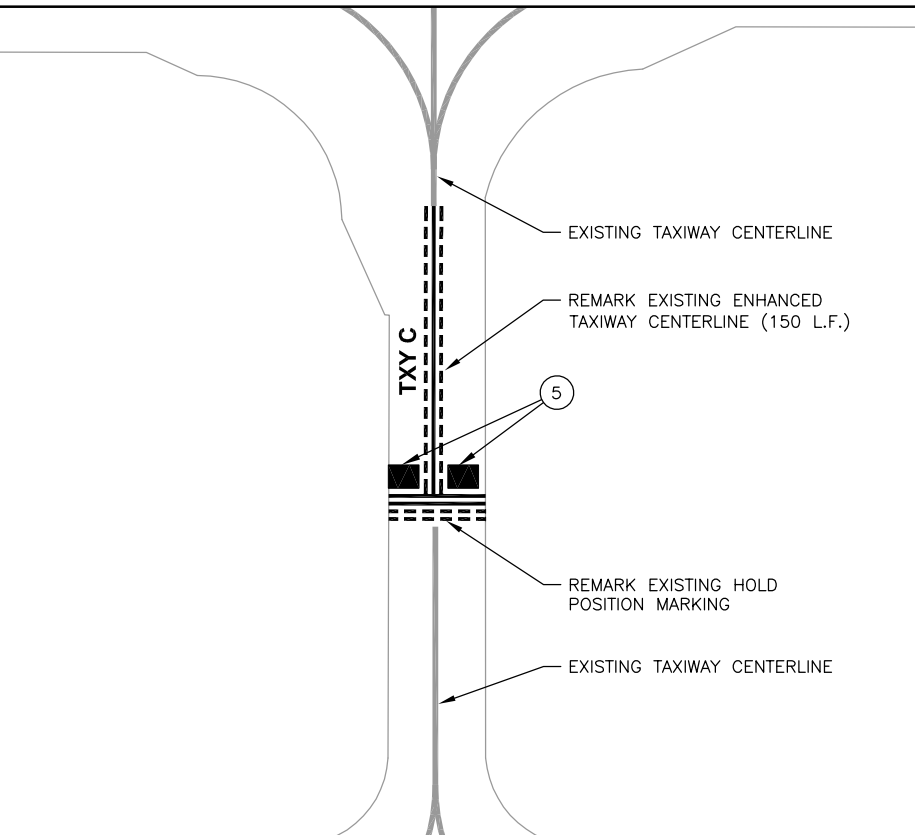
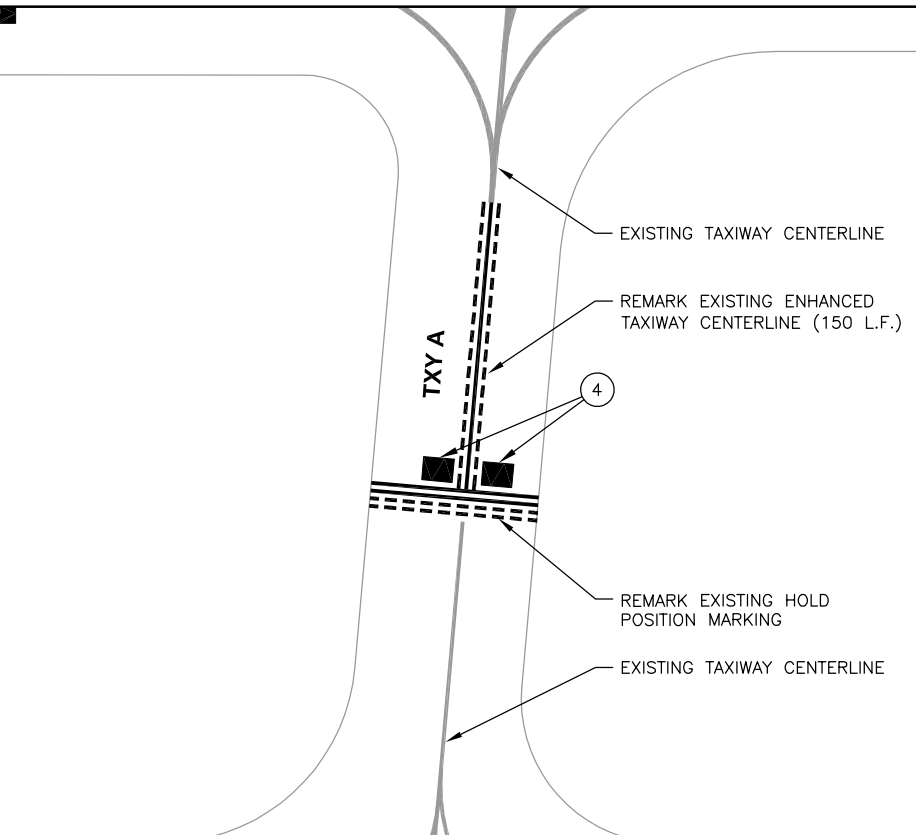


NOTES:

1. THE SURFACE PAINTED HOLDING POSITION MARKINGS SHOWN ARE USED TO EXPRESS THE APPROXIMATE LOCATIONS. SEE MARKING DETAILS AND MARKING SCHEDULE FOR DIMENSIONS AND FURTHER INFORMATION.
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LEGEND

- EXISTING MARKING
- NEW MARKING
- MARKING NUMBER - SEE DETAILS AND SCHEDULE FOR FURTHER INFORMATION



RUNWAY 13/31 AT TAXIWAY A

RUNWAY 13/31 AT TAXIWAY C

RUNWAY 13/31 AT TAXIWAY Y



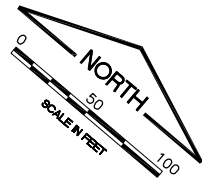
**SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

**EXTEND TAXIWAY Y
 MARKING PLAN 4**



| | |
|--------------|----------------|
| DESIGN BY: | RLV/JMW |
| DRAWN BY: | CMT |
| CHECKED BY: | RLV |
| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |

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 3-17-0096-XX



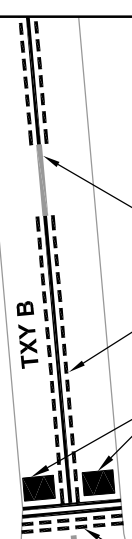
EXISTING TAXIWAY CENTERLINE

REMARK EXISTING ENHANCED TAXIWAY CENTERLINE (150 L.F.)

10

REMARK EXISTING HOLD POSITION MARKING

EXISTING TAXIWAY CENTERLINE



RWY 4/22

EXISTING TAXIWAY CENTERLINE

REMARK EXISTING HOLD POSITION MARKING

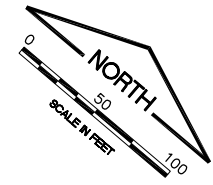
12

TXY B

REMARK EXISTING ENHANCED TAXIWAY CENTERLINE (150 L.F.)

EXISTING TAXIWAY CENTERLINE

RUNWAY 4/22 AT TAXIWAY B



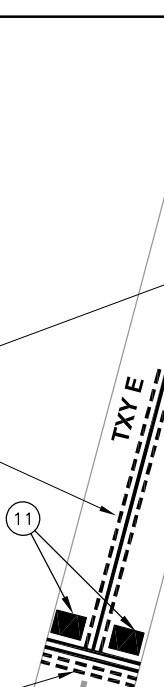
EXISTING TAXIWAY CENTERLINE

REMARK EXISTING ENHANCED TAXIWAY CENTERLINE (150 L.F.)

11

REMARK EXISTING HOLD POSITION MARKING

EXISTING TAXIWAY CENTERLINE



RWY 4/22

EXISTING TAXIWAY CENTERLINE

REMARK EXISTING HOLD POSITION MARKING

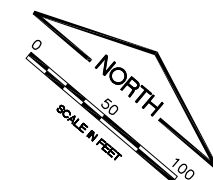
13

TXY E

REMARK EXISTING ENHANCED TAXIWAY CENTERLINE (150 L.F.)

EXISTING TAXIWAY CENTERLINE

RUNWAY 4/22 AT TAXIWAY E



EXISTING TAXIWAY CENTERLINE

REMARK EXISTING HOLD POSITION MARKING

14

TXY A

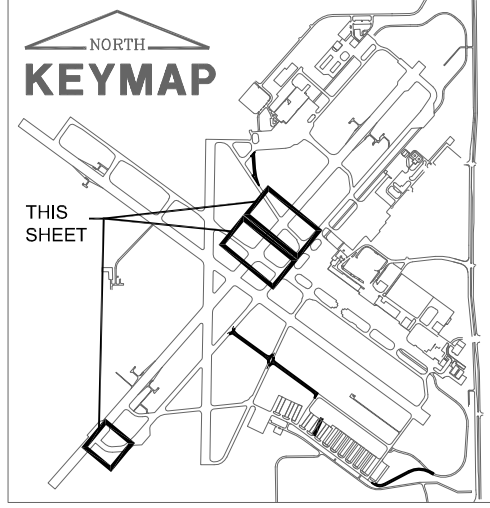
REMARK EXISTING ENHANCED TAXIWAY CENTERLINE (150 L.F.)

EXISTING TAXIWAY CENTERLINE

RUNWAY 4/22 AT TAXIWAY A

NOTES:

1. THE SURFACE PAINTED HOLDING POSITION MARKINGS SHOWN ARE USED TO EXPRESS THE APPROXIMATE LOCATIONS. SEE MARKING DETAILS AND MARKING SCHEDULE FOR DIMENSIONS AND FURTHER INFORMATION.
2. ALL SURFACE PAINTED HOLDING POSITION MARKINGS SHALL CONFORM TO THE REGULATIONS SET FORTH IN THE FAA ADVISORY CIRCULAR AC 150/5340-1, LATEST EDITION.



LEGEND

- EXISTING MARKING
- NEW MARKING
- Ⓝ MARKING NUMBER - SEE DETAILS AND SCHEDULE FOR FURTHER INFORMATION

K:\Springfield\1103504\Draw\Sheets
 FILE: MARKING PLAN 5.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/8/2012 9:12 PM
 SPIBASE_EXISTGEO
 KEYMAP
 BASE_EXISTMARK
 BASE_PROP_GEO

CA014

| REVISIONS | | |
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| NUMBER | BY | DATE |
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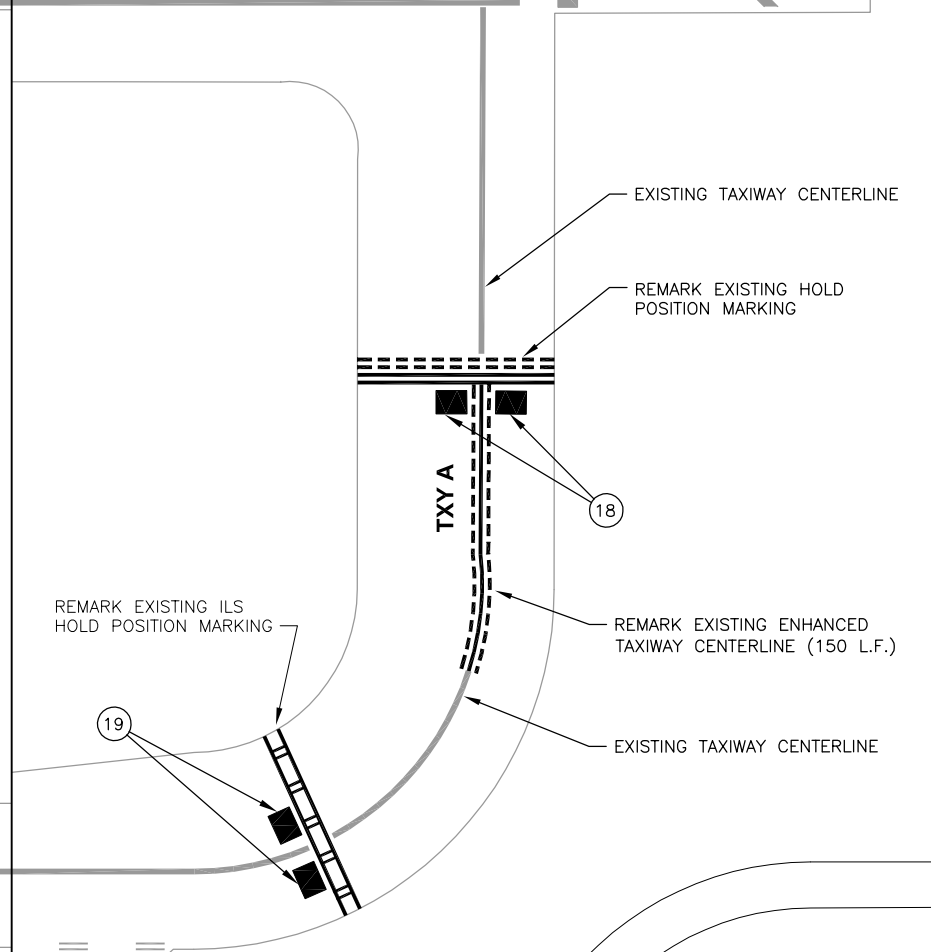
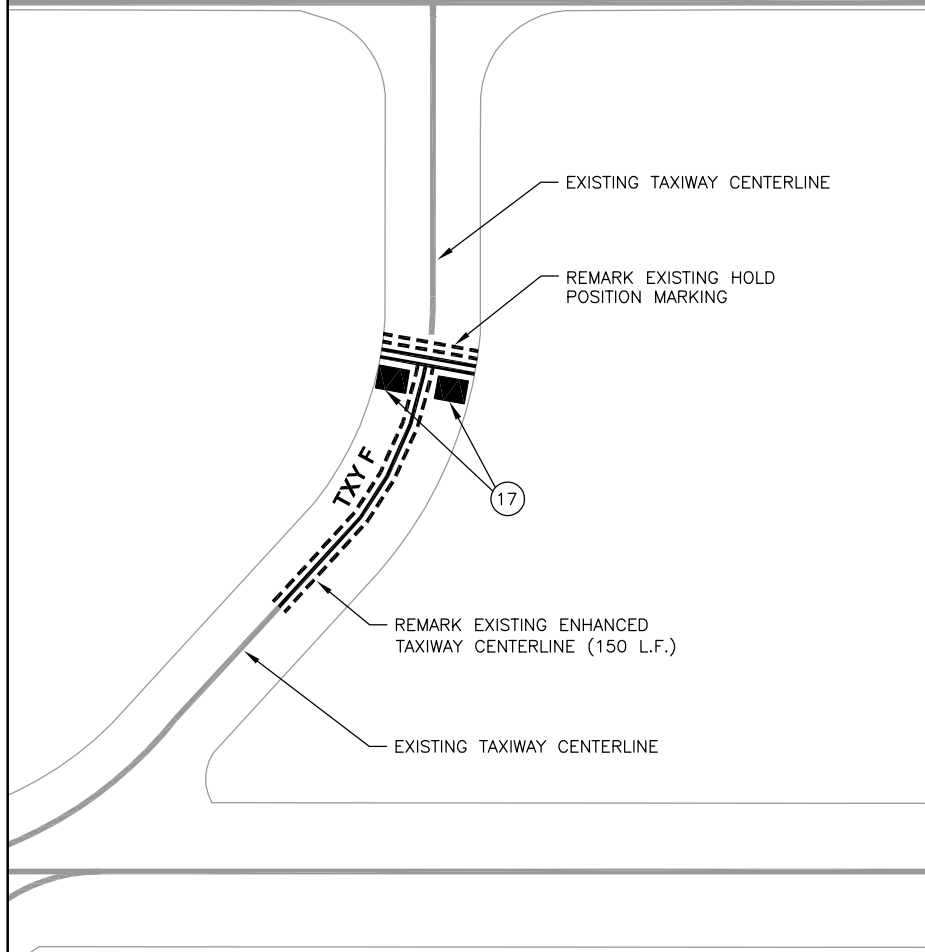
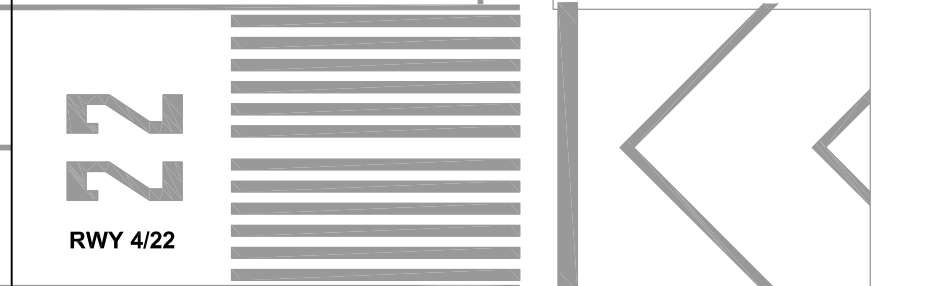
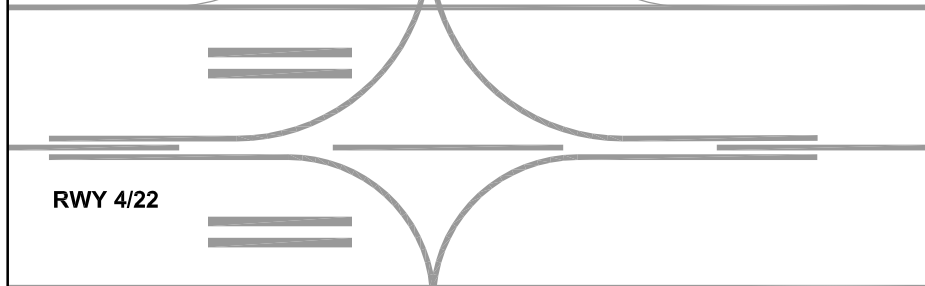
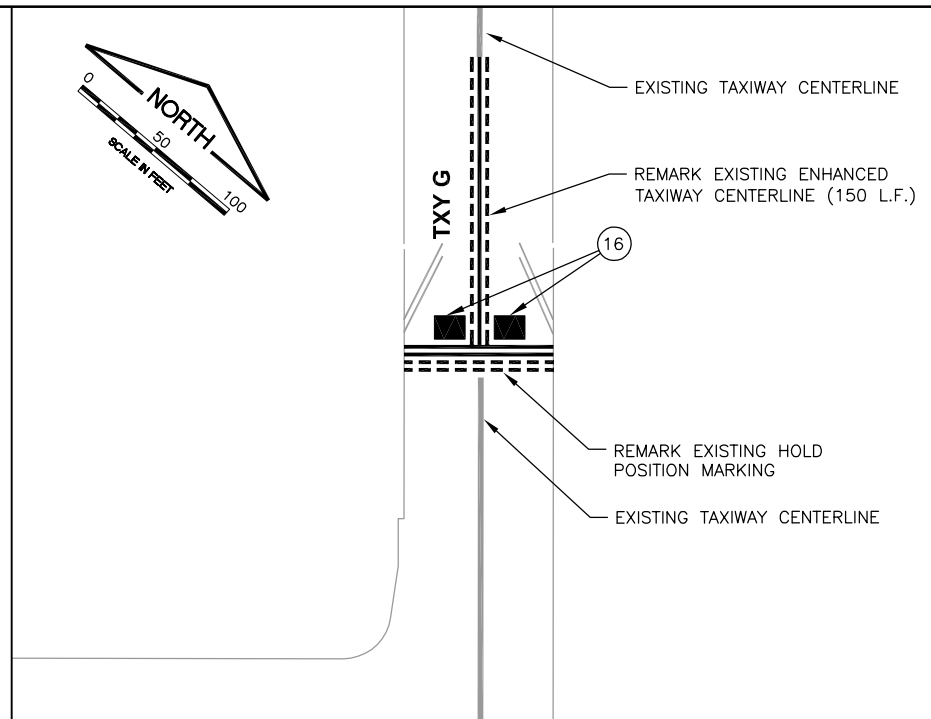
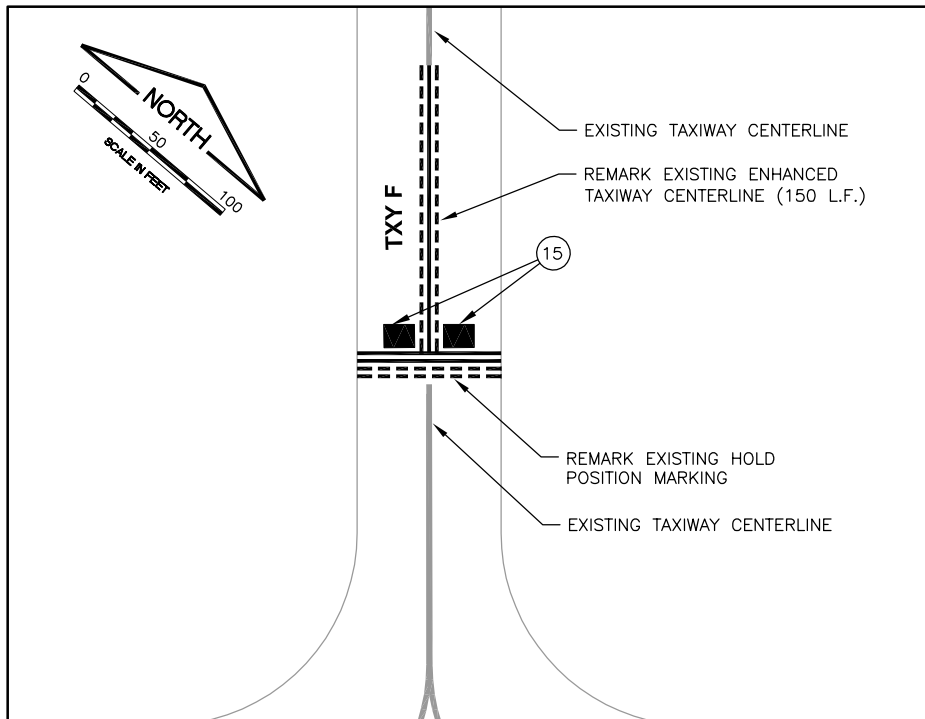
0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).


 SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y
 MARKING PLAN 5

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| CHECKED BY: | RLV |
| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET 51 OF 106 SHEETS | |



RUNWAY 4/22 AT TAXIWAY F

RUNWAY 4/22 AT TAXIWAYS A & G

NOTES:

1. THE SURFACE PAINTED HOLDING POSITION MARKINGS SHOWN ARE USED TO EXPRESS THE APPROXIMATE LOCATIONS. SEE MARKING DETAILS AND MARKING SCHEDULE FOR DIMENSIONS AND FURTHER INFORMATION.
2. ALL SURFACE PAINTED HOLDING POSITION MARKINGS SHALL CONFORM TO THE REGULATIONS SET FORTH IN THE FAA ADVISORY CIRCULAR AC 150/5340-1, LATEST EDITION.

LEGEND

— EXISTING MARKING

— NEW MARKING

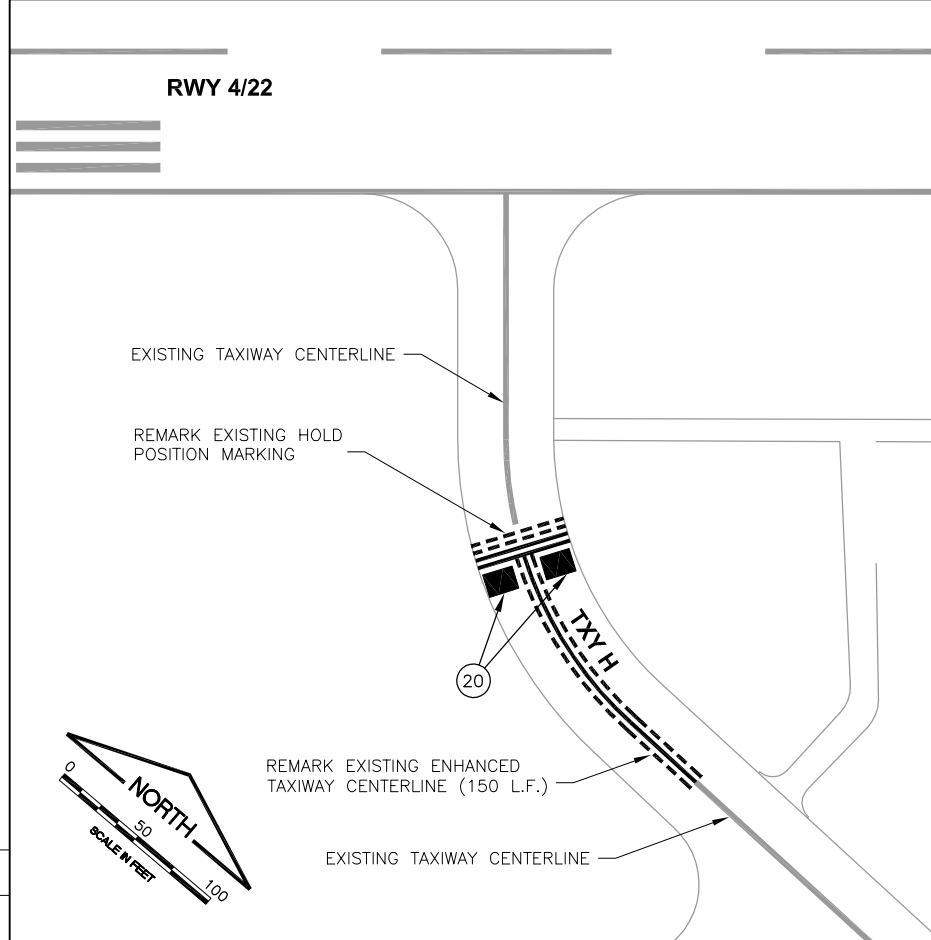
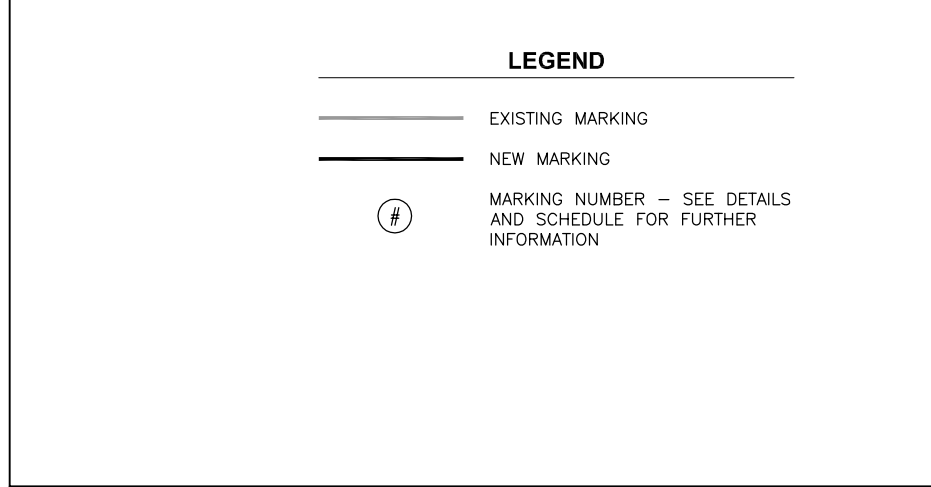
MARKING NUMBER - SEE DETAILS AND SCHEDULE FOR FURTHER INFORMATION

KEYMAP

NORTH

THIS SHEET

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



RUNWAY 4/22 AT TAXIWAY H

K:\Springfield\1103504\Draw\Sheets
FILE: MARKING PLAN 6.dwg
UPDATE BY: TJ Heavisides
PLOT DATE: 5/8/2012 9:12 PM
SPIBASE_EXISTGEO
KEYMAP
BASE_EXISTMARK
BASE_PROP_GEO

CA014

| REVISIONS | | |
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| NUMBER | BY | DATE |
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0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

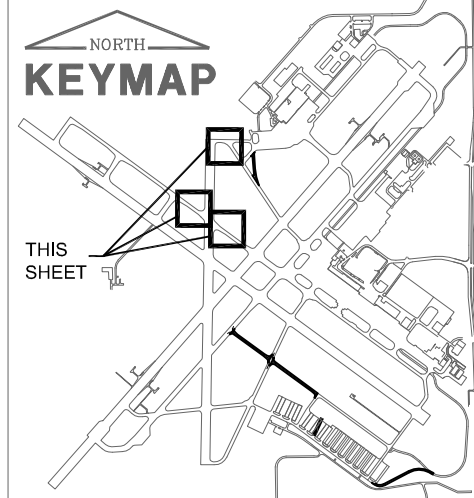
EXTEND TAXIWAY Y
MARKING PLAN 6

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| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |

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SHEET 52 OF 106 SHEETS

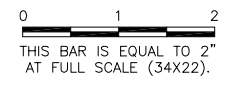


NOTES:

1. THE SURFACE PAINTED HOLDING POSITION MARKINGS SHOWN ARE USED TO EXPRESS THE APPROXIMATE LOCATIONS. SEE MARKING DETAILS AND MARKING SCHEDULE FOR DIMENSIONS AND FURTHER INFORMATION.
2. ALL SURFACE PAINTED HOLDING POSITION MARKINGS SHALL CONFORM TO THE REGULATIONS SET FORTH IN THE FAA ADVISORY CIRCULAR AC 150/5340-1, LATEST EDITION.

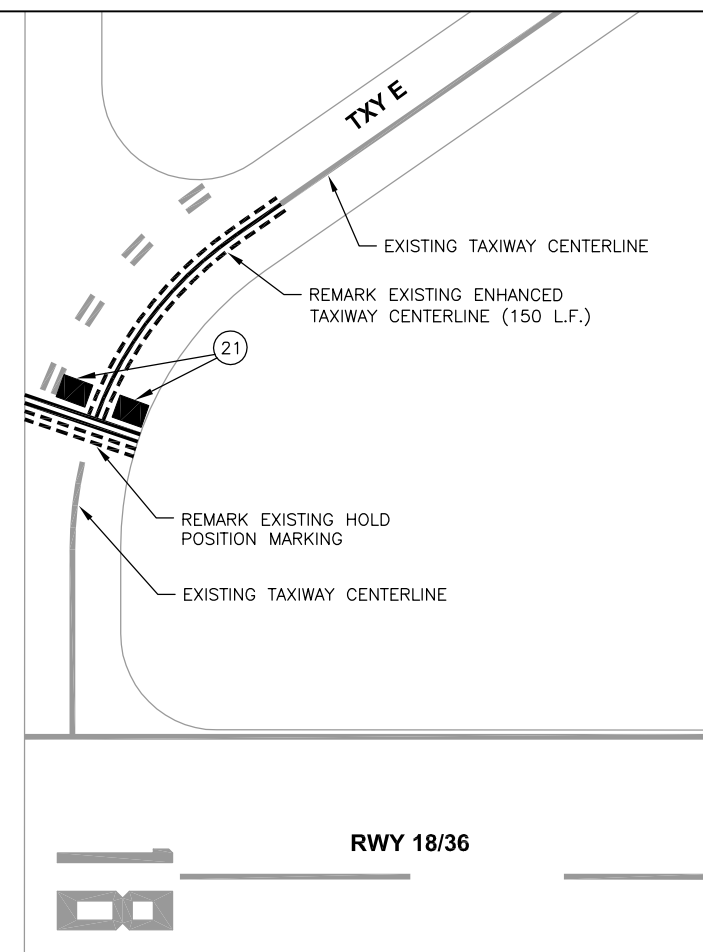
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| REVISIONS | | |
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| NUMBER | BY | DATE |
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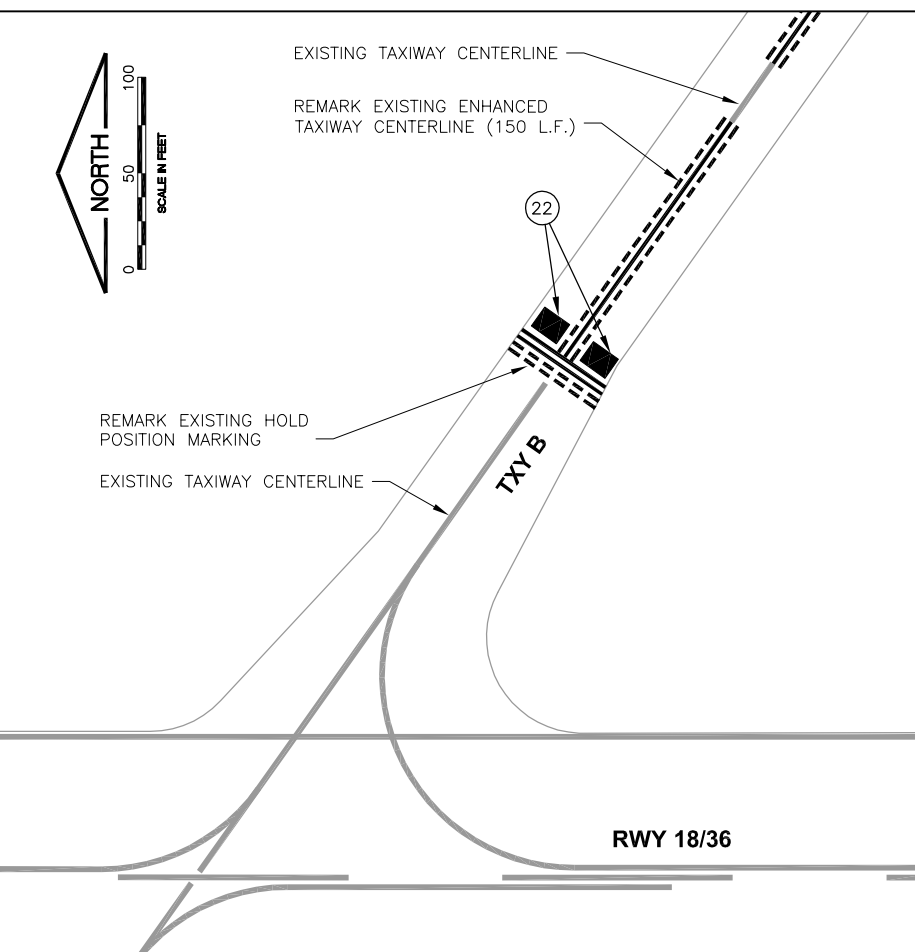


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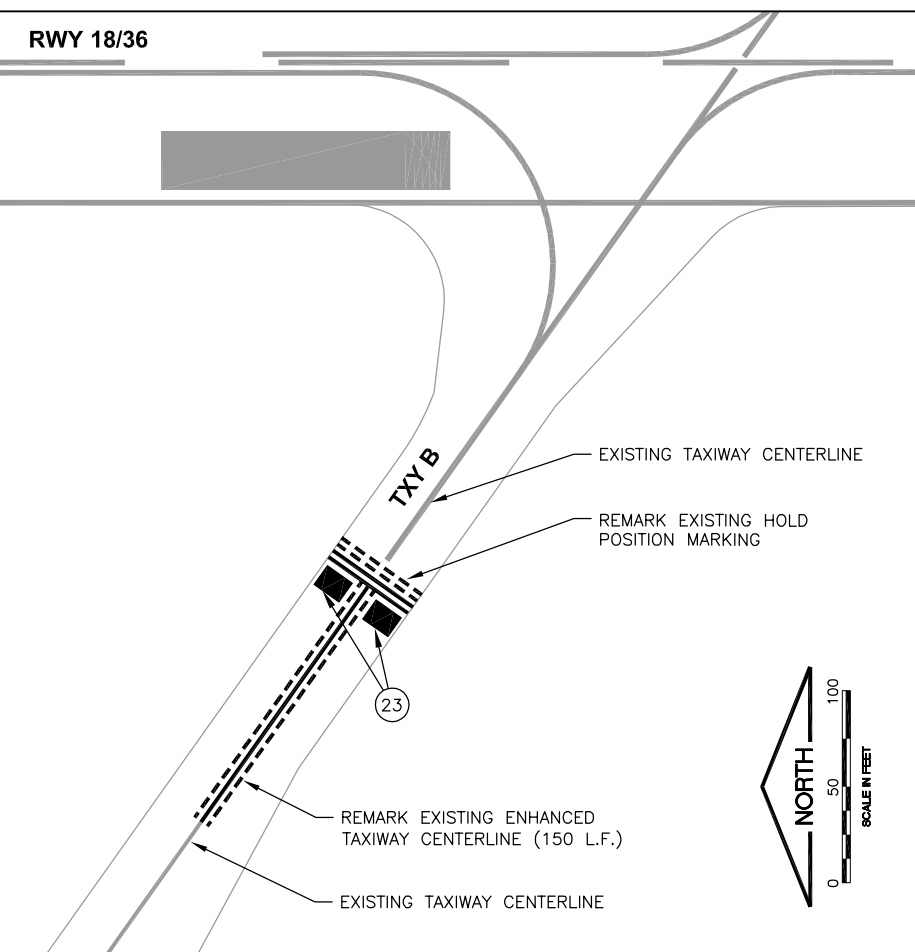
- EXISTING MARKING
- NEW MARKING
- MARKING NUMBER - SEE DETAILS AND SCHEDULE FOR FURTHER INFORMATION



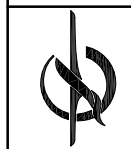
RUNWAY 18/36 AT TAXIWAY E



RUNWAY 18/36 AT TAXIWAY B



RUNWAY 18/36 AT TAXIWAY B



**SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

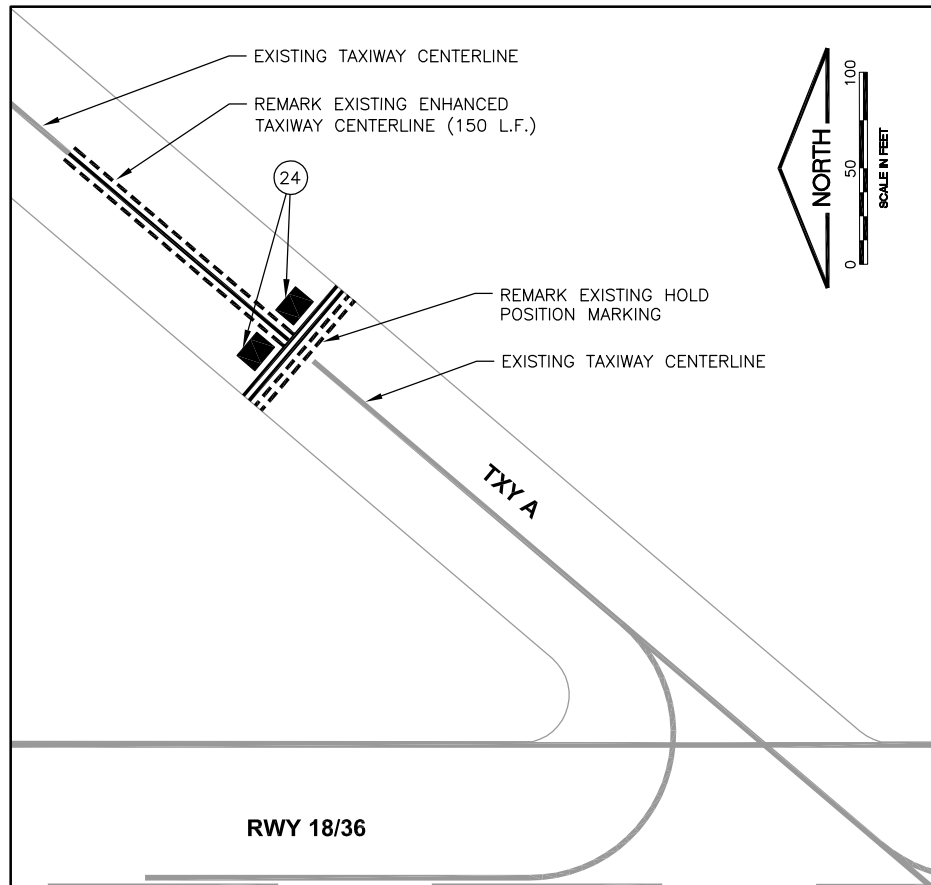
**EXTEND TAXIWAY Y
 MARKING PLAN 7**

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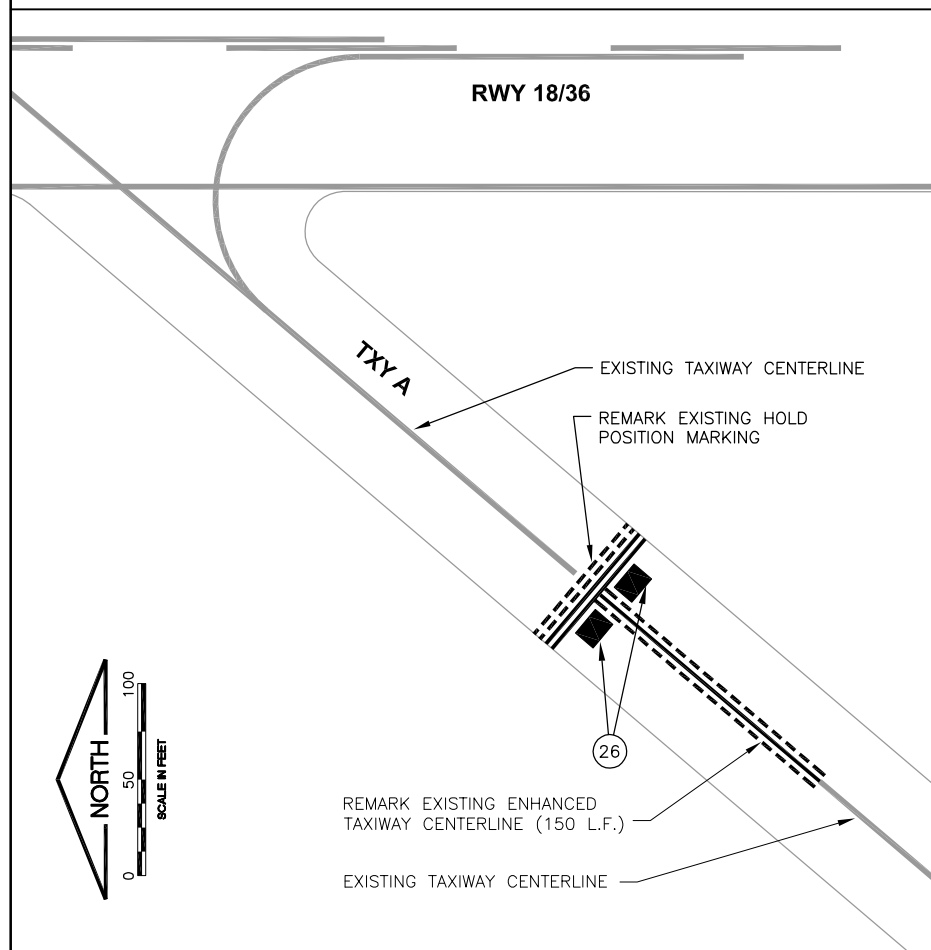
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|--------------|----------------|
| DESIGN BY: | RLV/JMW |
| DRAWN BY: | CMT |
| CHECKED BY: | RLV |
| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |

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 3-17-0096-XX



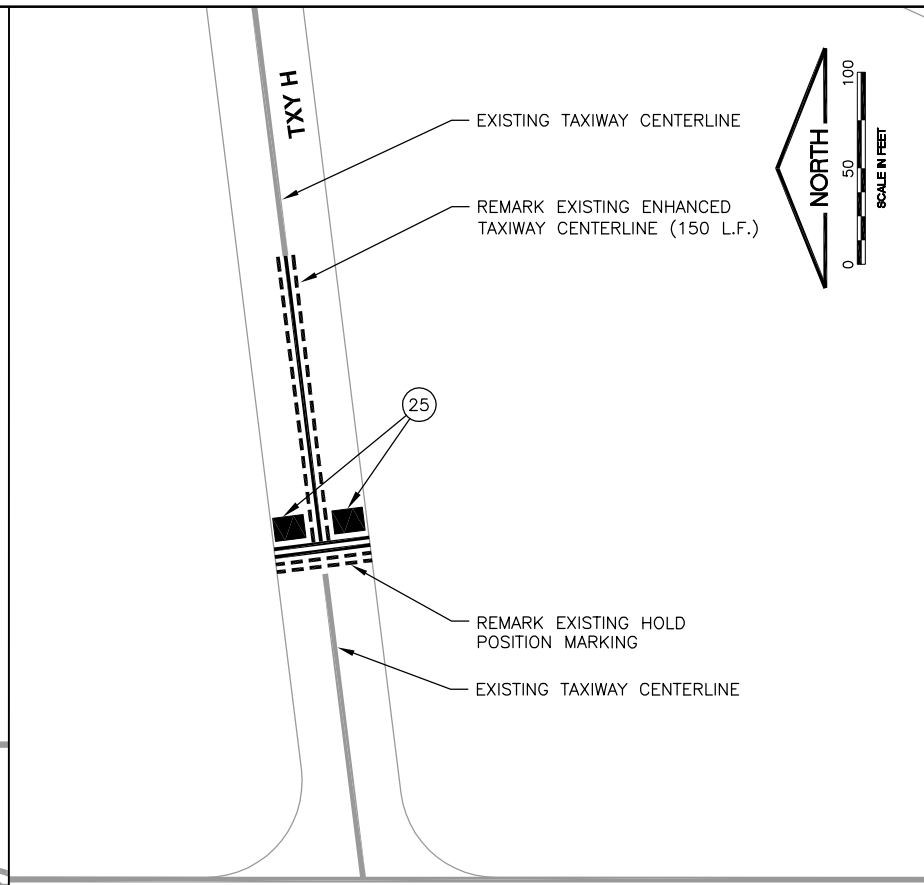
RWY 18/36

RUNWAY 18/36 AT TAXIWAY A



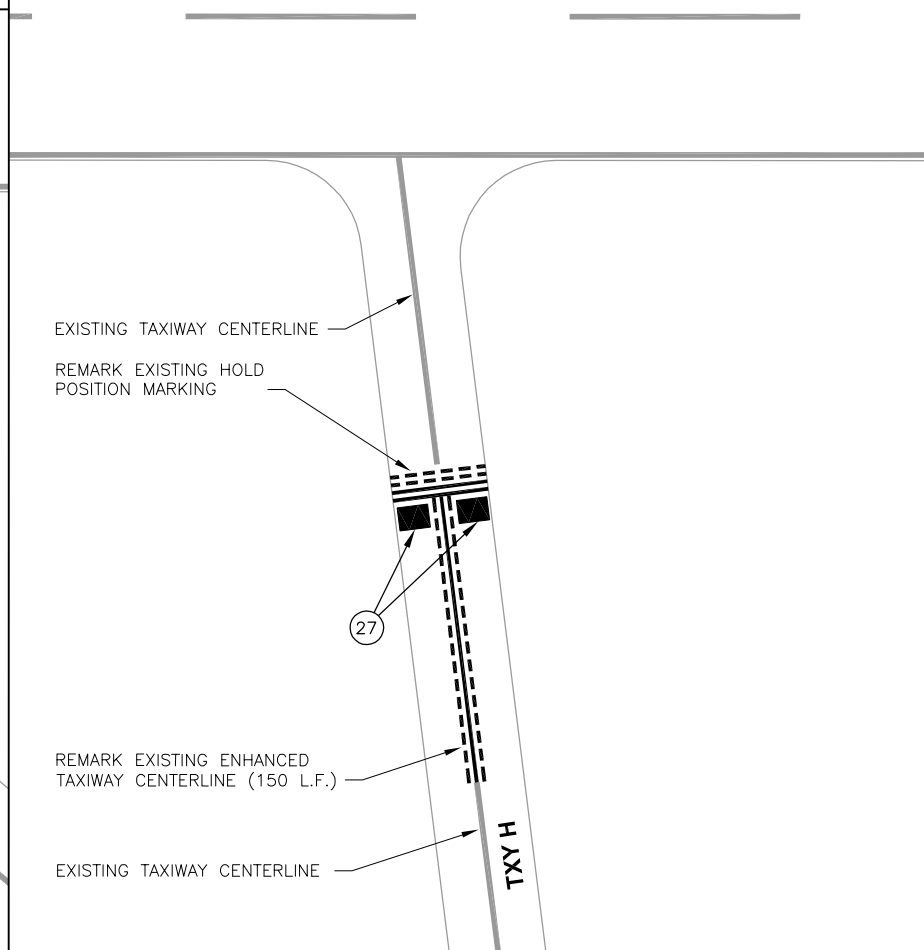
RWY 18/36

RUNWAY 18/36 AT TAXIWAY A



RWY 18/36

RUNWAY 18/36 AT TAXIWAY H



EXISTING TAXIWAY CENTERLINE
REMARK EXISTING HOLD POSITION MARKING

REMARK EXISTING ENHANCED TAXIWAY CENTERLINE (150 L.F.)

EXISTING TAXIWAY CENTERLINE

NOTES:

1. THE SURFACE PAINTED HOLDING POSITION MARKINGS SHOWN ARE USED TO EXPRESS THE APPROXIMATE LOCATIONS. SEE MARKING DETAILS AND MARKING SCHEDULE FOR DIMENSIONS AND FURTHER INFORMATION.
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LEGEND

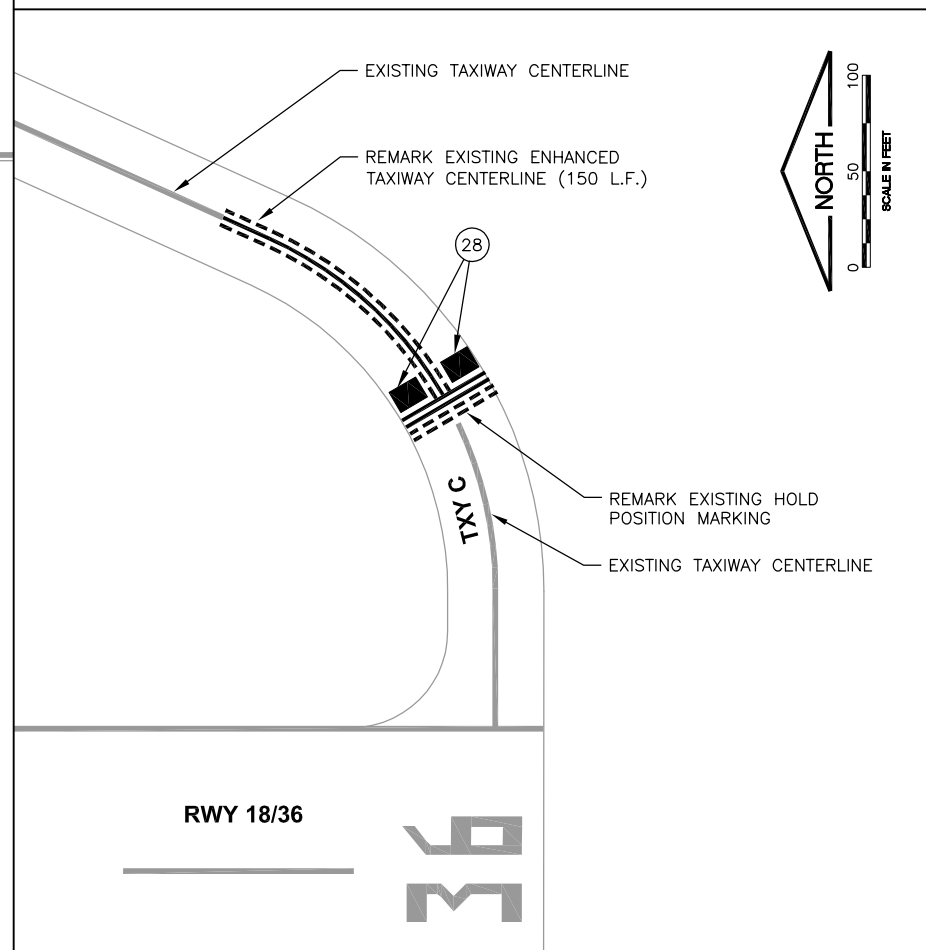
— EXISTING MARKING
 — NEW MARKING
 # MARKING NUMBER — SEE DETAILS AND SCHEDULE FOR FURTHER INFORMATION

KEYMAP

NORTH

THIS SHEET

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



RWY 18/36

RUNWAY 18/36 AT TAXIWAY C

K:\Springfield\1103504\Draw\Sheets
 FILE: MARKING PLAN 8.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/8/2012 9:12 PM
 SPIBASE_EXISTGEO
 KEYMAP
 BASE_EXISTMARK
 BASE_PROP_GEO

CA014

| REVISIONS | | |
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| NUMBER | BY | DATE |
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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y
MARKING PLAN 8

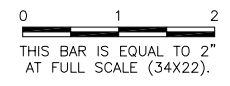
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|--------------------------|----------------|
| DESIGN BY: | RLV/JMW |
| DRAWN BY: | CMT |
| CHECKED BY: | RLV |
| APPROVED BY: | RLV |
| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET 54 OF 106 SHEETS | |

CA014

| REVISIONS | | |
|-----------|----|------|
| NUMBER | BY | DATE |
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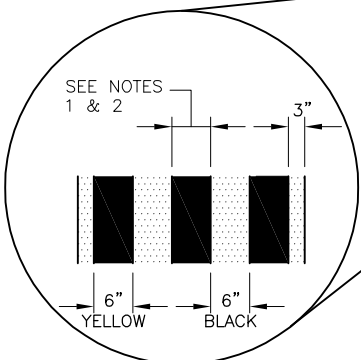
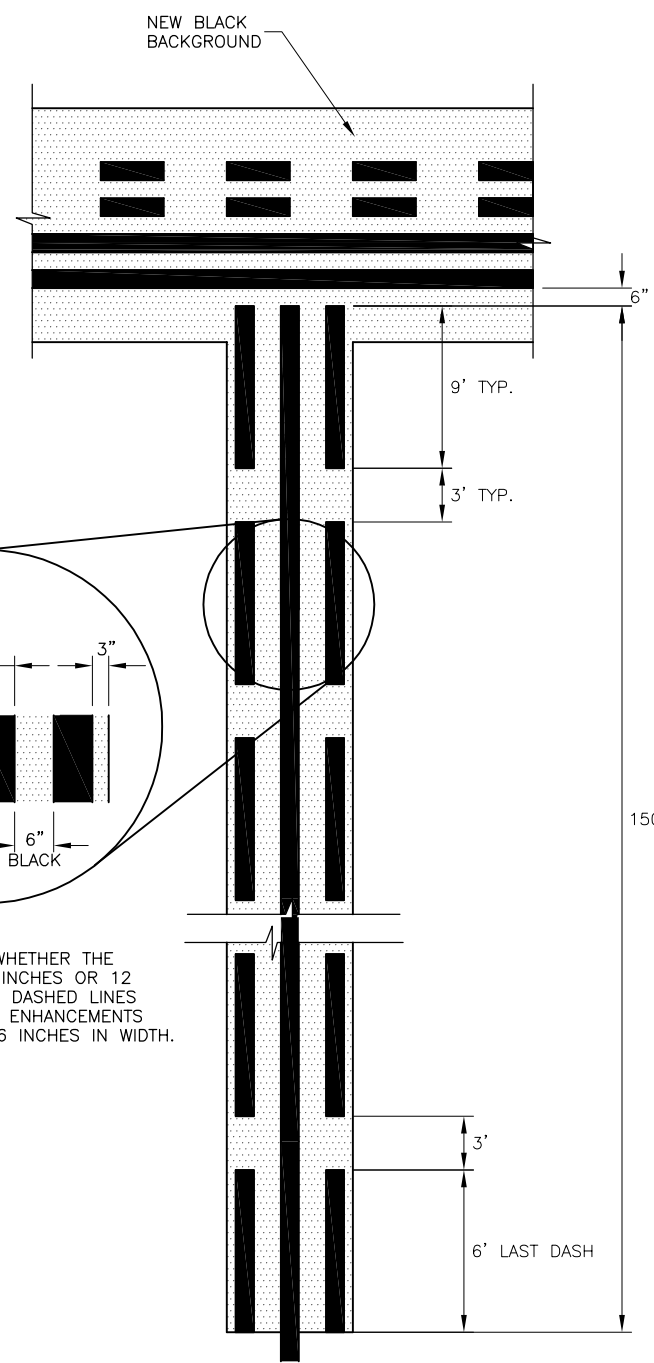



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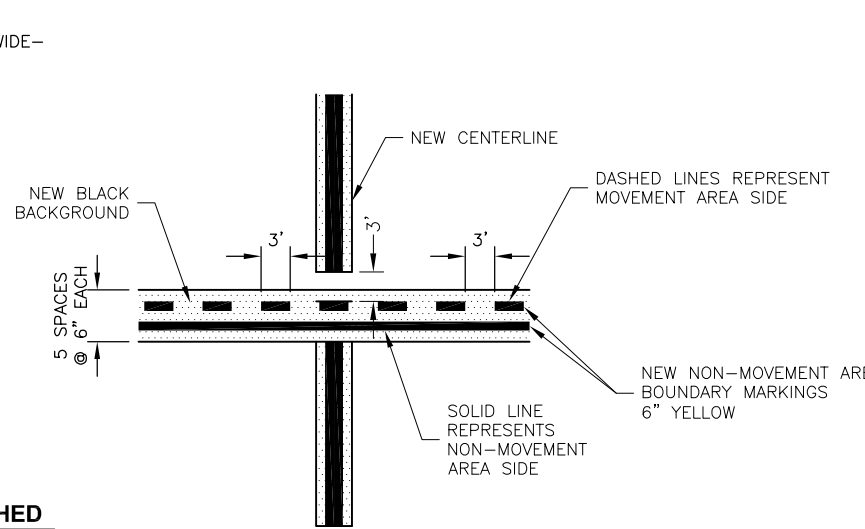
EXTEND TAXIWAY Y
MARKING DETAILS 1


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 License No. 184-000613

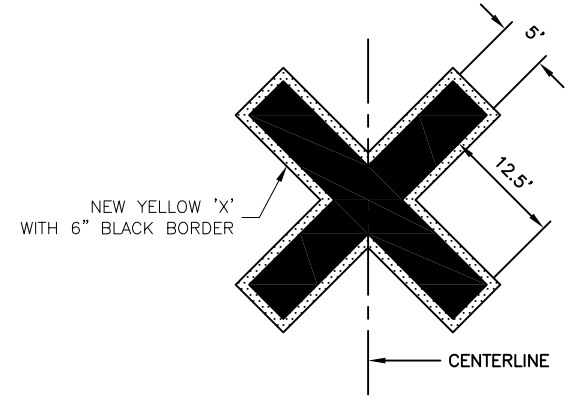
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| DATE: | APRIL 27, 2012 |
| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET 55 OF 106 SHEETS | |



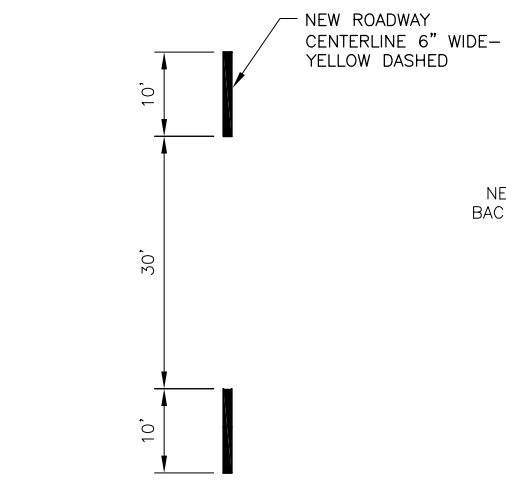
NOTE 1:
 REGARDLESS OF WHETHER THE CENTERLINE IS 6 INCHES OR 12 INCHES WIDE, THE DASHED LINES PROVIDED BY THE ENHANCEMENTS WILL ALWAYS BE 6 INCHES IN WIDTH.



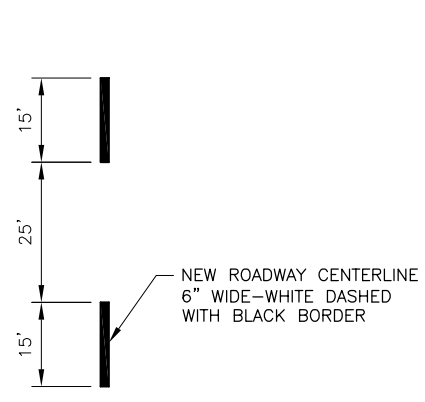
NON-MOVEMENT AREA BOUNDARY MARKING
 N.T.S.



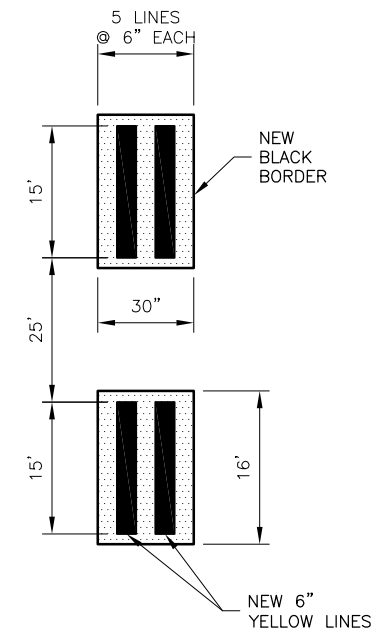
CLOSED TAXIWAY MARKING
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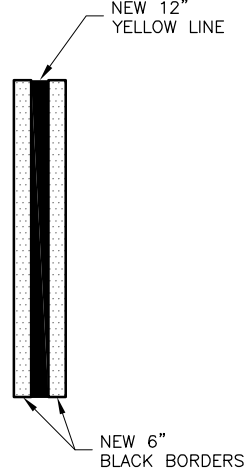
PERIMETER ROAD CENTERLINE - DASHED
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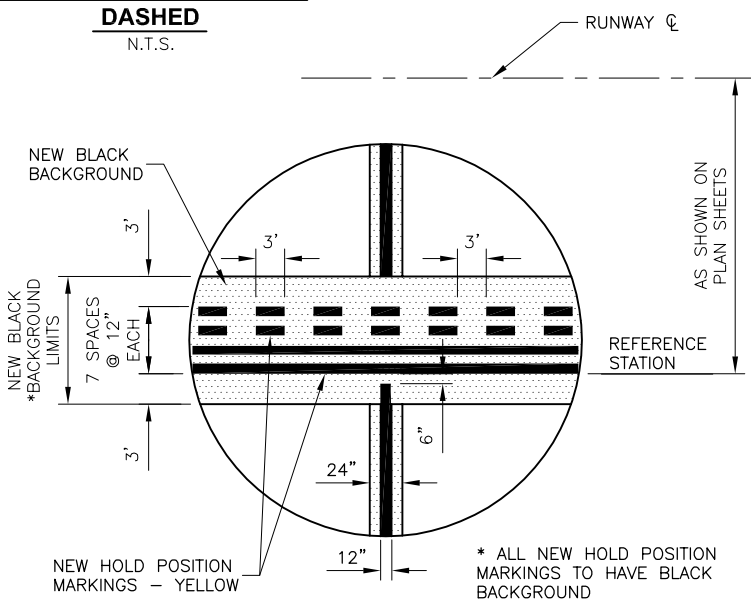
ARFF ROADWAY CENTERLINE - DASHED
 N.T.S.



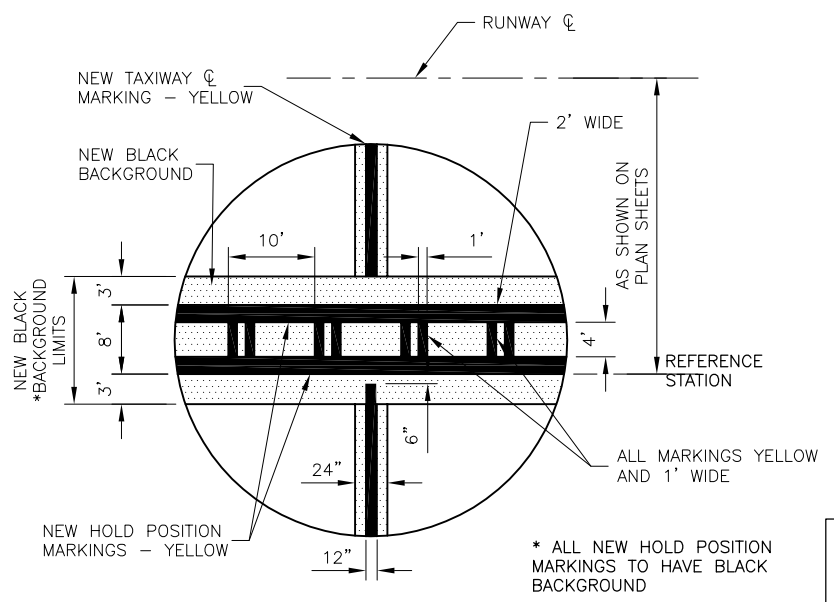
TAXIWAY EDGE MARKING DASHED
 N.T.S.



TAXIWAY CENTERLINE CONTINUOUS
 N.T.S.



HOLD POSITION MARKING DETAIL
 N.T.S.



ILS HOLD POSITION MARKING DETAIL
 N.T.S.

MARKING NOTE
 ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.

TAXIWAY EDGE MARKING CONTINUOUS
 N.T.S.

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0 1 2
 THIS BAR IS EQUAL TO 2"
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EXTEND TAXIWAY Y

MARKING DETAILS 2

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CHECKED BY: RLV

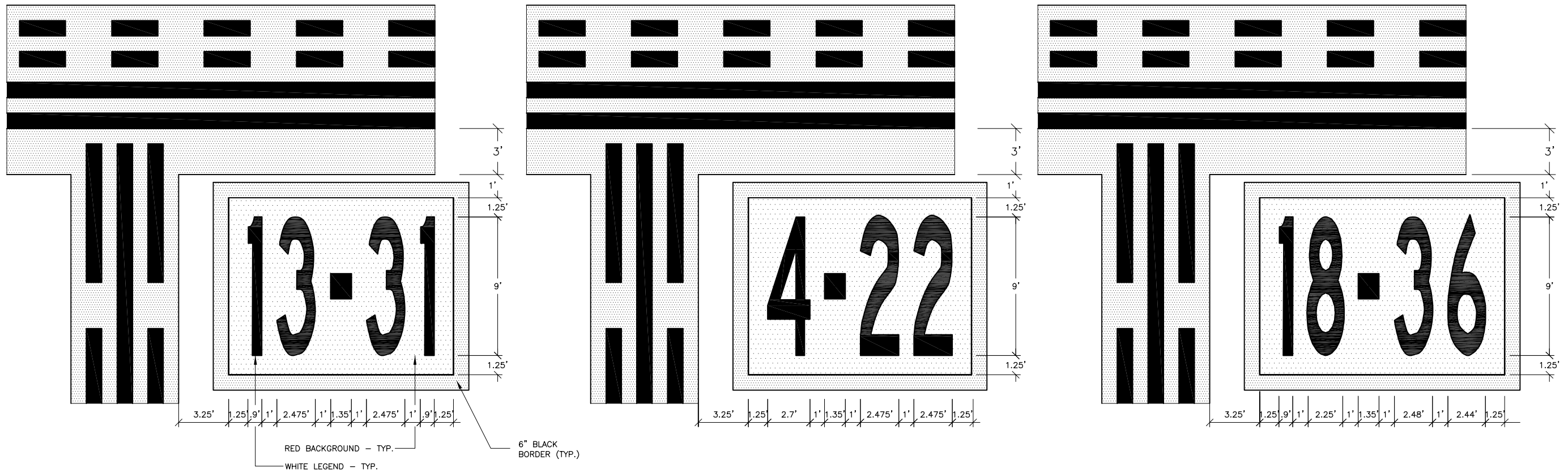
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DATE: 2/17/10 2

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SHEET 56 OF 106 SHEETS

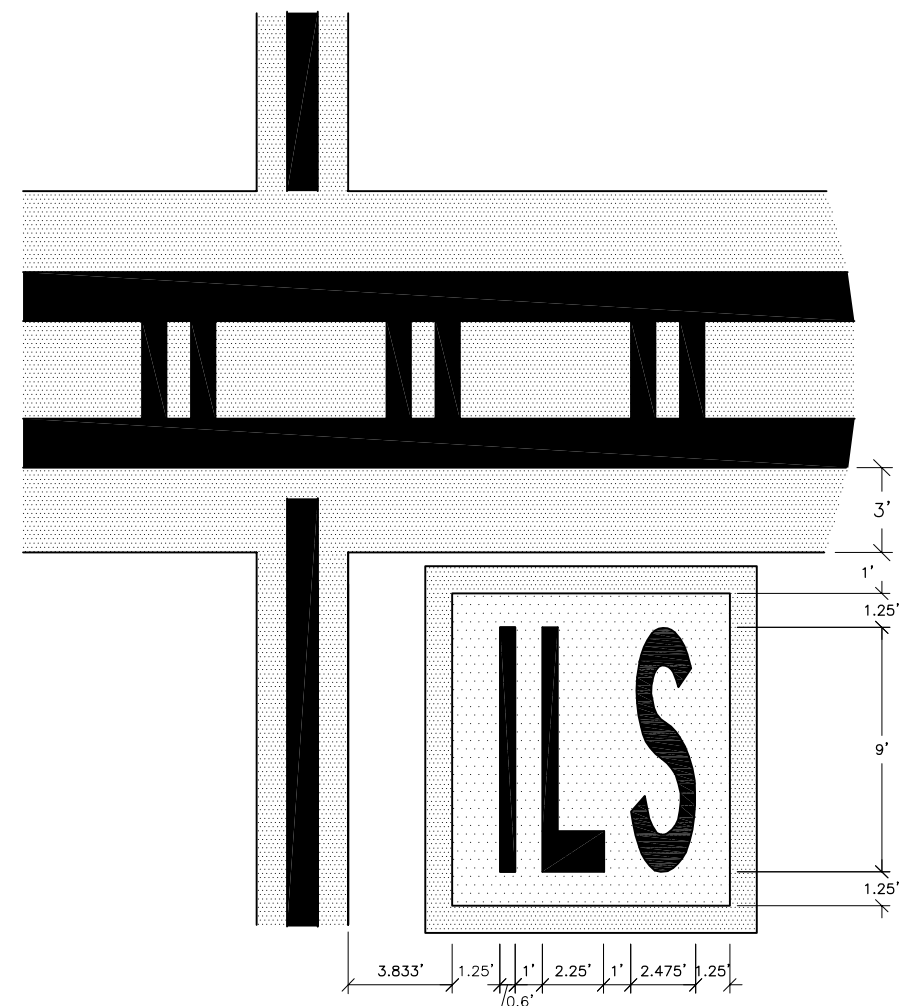


MARKING NOTES

- ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.
- MARKING DIMENSION IN MARKING SCHEDULE IS THE DIMENSIONS OF THE RED BACKGROUND.
- THE AIRPORT WILL PROVIDE THE STENCILS NEEDED TO PAINT THE NINE (9) FOOT WHITE LEGEND INSCRIPTIONS ON THE SURFACE PAINTED HOLDING POSITION SIGNS. THE USE OF THESE STENCILS SHALL BE COORDINATED WITH THE CONTRACTOR AND THE AIRPORT THROUGH THE ENGINEER.

MARKING SCHEDULE

| MARK # | MARKING LEGEND | TAXIWAY HOLDING SHORT | LOCATION ON TAXIWAY | WHITE LEGEND ON RED BACKGROUND WITH BLACK BORDER | NUMBER OF CHARACTERS | MARKING DIMENSION | RUNWAY HOLDING | NOTES |
|--------|----------------|-----------------------|---------------------|--|----------------------|-------------------|-----------------|---------------------------------------|
| 1 | 31-13 | TAXIWAY B | BOTH SIDES OF TXY ☐ | 31-13 | 5 | 11.5'h x 14.60'w | RWY 13/31 | NEW HOLD POSITION MARKING |
| 2 | 31-13 | TAXIWAY K | BOTH SIDES OF TXY ☐ | 31-13 | 5 | 11.5'h x 14.60'w | RWY 13/31 | REMARK EXISTING HOLD POSITION MARKING |
| 3 | 31-13 | TAXIWAY J | BOTH SIDES OF TXY ☐ | 31-13 | 5 | 11.5'h x 14.60'w | RWY 13/31 | REMARK EXISTING HOLD POSITION MARKING |
| 4 | 31-13 | TAXIWAY A | BOTH SIDES OF TXY ☐ | 31-13 | 5 | 11.5'h x 14.60'w | RWY 13/31 | REMARK EXISTING HOLD POSITION MARKING |
| 5 | 31-13 | TAXIWAY C | BOTH SIDES OF TXY ☐ | 31-13 | 5 | 11.5'h x 14.60'w | RWY 13/31 | REMARK EXISTING HOLD POSITION MARKING |
| 6 | 13-31 | TAXIWAY A | BOTH SIDES OF TXY ☐ | 13-31 | 5 | 11.5'h x 14.60'w | RWY 13/31 | REMARK EXISTING HOLD POSITION MARKING |
| 7 | 13-31 | TAXIWAY C | BOTH SIDES OF TXY ☐ | 13-31 | 5 | 11.5'h x 14.60'w | RWY 13/31 | REMARK EXISTING HOLD POSITION MARKING |
| 8 | 13-31 | TAXIWAY Y | ACROSS TXY ☐ | 13-31 | 5 | 8.3'h x 9.45'w | RWY 13/31 | NEW HOLD POSITION MARKING |
| 9 | ILS | TAXIWAY Y | ACROSS TXY ☐ | ILS | 3 | 8.3'h x 6.20'w | ILS - RWY 13/31 | NEW HOLD POSITION MARKING |
| 10 | 22-4 | TAXIWAY B | BOTH SIDES OF TXY ☐ | 22-4 | 4 | 11.5'h x 14.50'w | RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 11 | 22-4 | TAXIWAY E | BOTH SIDES OF TXY ☐ | 22-4 | 4 | 11.5'h x 14.50'w | RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 12 | 4-22 | TAXIWAY B | BOTH SIDES OF TXY ☐ | 4-22 | 4 | 11.5'h x 14.50'w | RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 13 | 4-22 | TAXIWAY E | BOTH SIDES OF TXY ☐ | 4-22 | 4 | 11.5'h x 14.50'w | RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 14 | 4 | TAXIWAY A | BOTH SIDES OF TXY ☐ | 4 | 1 | 11.5'h x 5.20'w | RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 15 | 22-4 | TAXIWAY F | BOTH SIDES OF TXY ☐ | 22-4 | 4 | 11.5'h x 14.50'w | RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 16 | 22 | TAXIWAY G | BOTH SIDES OF TXY ☐ | 22 | 2 | 11.5'h x 14.50'w | RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 17 | 4-22 | TAXIWAY F | BOTH SIDES OF TXY ☐ | 4-22 | 4 | 11.5'h x 14.50'w | RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 18 | 22 | TAXIWAY A | BOTH SIDES OF TXY ☐ | 22 | 2 | 11.5'h x 14.50'w | RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 19 | ILS | TAXIWAY A | BOTH SIDES OF TXY ☐ | ILS | 3 | 11.5'h x 9.71'w | ILS - RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 20 | 4-22 | TAXIWAY H | BOTH SIDES OF TXY ☐ | 4-22 | 4 | 11.5'h x 14.50'w | RWY 4/22 | REMARK EXISTING HOLD POSITION MARKING |
| 21 | 18 | TAXIWAY E | BOTH SIDES OF TXY ☐ | 18 | 2 | 11.5'h x 6.65'w | RWY 18/36 | REMARK EXISTING HOLD POSITION MARKING |
| 22 | 36-18 | TAXIWAY B | BOTH SIDES OF TXY ☐ | 36-18 | 5 | 11.5'h x 15.67'w | RWY 18/36 | REMARK EXISTING HOLD POSITION MARKING |
| 23 | 18-36 | TAXIWAY B | BOTH SIDES OF TXY ☐ | 18-36 | 5 | 11.5'h x 15.67'w | RWY 18/36 | REMARK EXISTING HOLD POSITION MARKING |
| 24 | 36-18 | TAXIWAY A | BOTH SIDES OF TXY ☐ | 36-18 | 5 | 11.5'h x 15.67'w | RWY 18/36 | REMARK EXISTING HOLD POSITION MARKING |
| 25 | 36-18 | TAXIWAY H | BOTH SIDES OF TXY ☐ | 36-18 | 5 | 11.5'h x 15.67'w | RWY 18/36 | REMARK EXISTING HOLD POSITION MARKING |
| 26 | 18-36 | TAXIWAY A | BOTH SIDES OF TXY ☐ | 18-36 | 5 | 11.5'h x 15.67'w | RWY 18/36 | REMARK EXISTING HOLD POSITION MARKING |
| 27 | 18-36 | TAXIWAY H | BOTH SIDES OF TXY ☐ | 18-36 | 5 | 11.5'h x 15.67'w | RWY 18/36 | REMARK EXISTING HOLD POSITION MARKING |
| 28 | 36 | TAXIWAY C | BOTH SIDES OF TXY ☐ | 36 | 2 | 11.5'h x 8.42'w | RWY 18/36 | REMARK EXISTING HOLD POSITION MARKING |
| 29 | 31-13 | TAXIWAY B | BOTH SIDES OF TXY ☐ | 31-13 | 5 | 11.5'h x 14.60'w | RWY 13/31 | NEW HOLD POSITION MARKING |

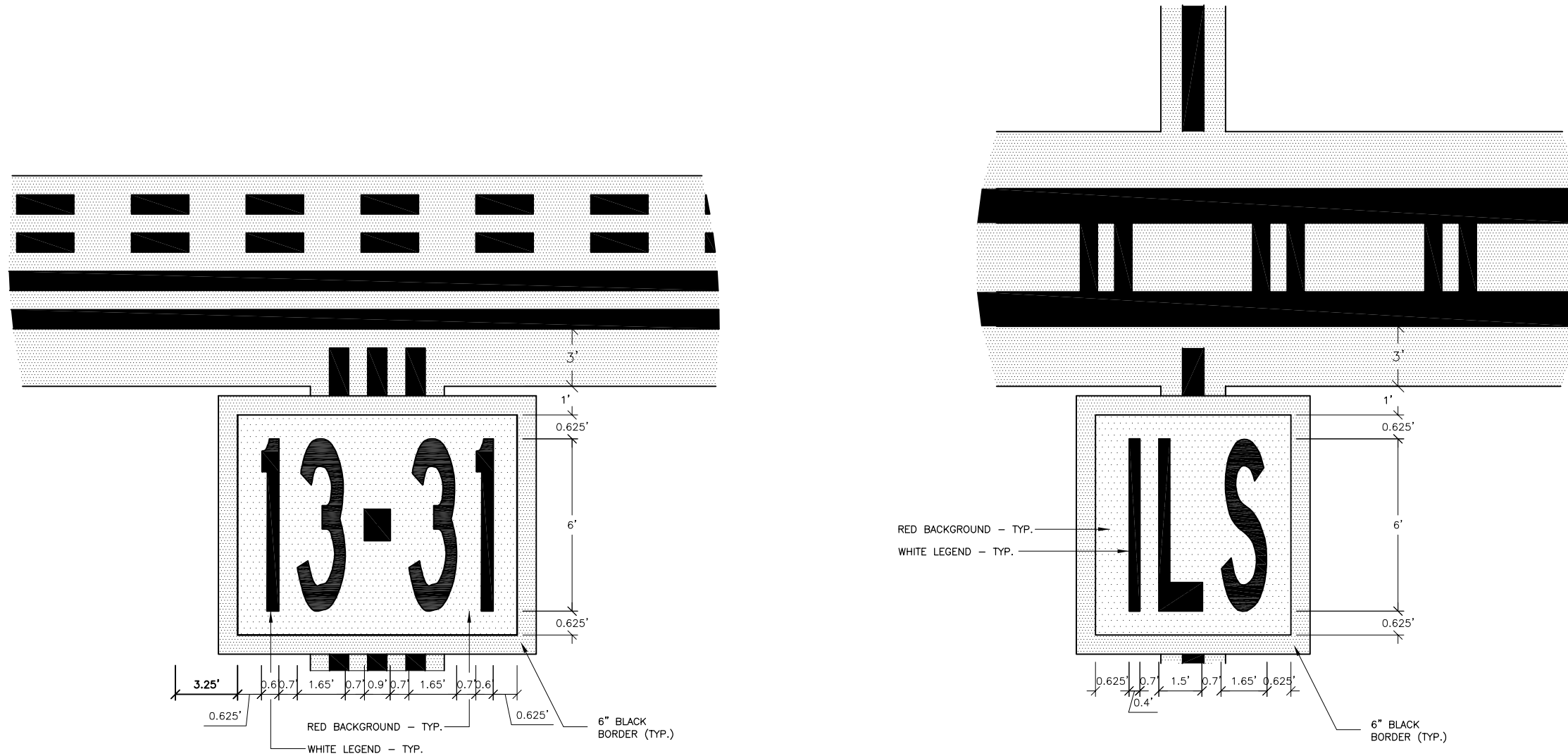


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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



MARKING NOTES

1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.
2. SEE MARKING DETAILS 2 FOR MARKING SCHEDULE.
3. SURFACE PAINTED HOLDING POSITION SIGNS TO BE LOCATED ACROSS THE TAXIWAY CENTERLINE, AS DETAILED ON THIS SHEET, SHALL BE CENTERED ON THE TAXIWAY CENTERLINE AND THE INSCRIPTION HEIGHTS WILL BE REDUCED TO SIX (6') FEET. THE WIDTH AND ALL OTHER DIMENSIONS OF THE SIGNS SHALL BE REDUCED AS SHOWN ON THIS SHEET.
4. THE CONTRACTOR SHALL ACQUIRE THE STENCILS TO MARK THE SIX (6') FOOT WHITE LEGEND INSCRIPTIONS AS DETAILED ON THIS SHEET. THE STENCILS WILL BE PURCHASED NEW BY THE CONTRACTOR AND OWNERSHIP OF THE STENCILS WILL BE HANDED OVER TO THE AIRPORT UPON COMPLETION OF THE PROJECT FOR THE AIRPORT'S FUTURE MAINTENANCE USE. THE CONTRACTOR SHALL, BY A METHOD APPROVED BY AND TO THE SATISFACTION OF THE AIRPORT, MARK THESE STENCILS WITH A NOTE STATING: "FOR USE ON TAXIWAY Y ONLY".



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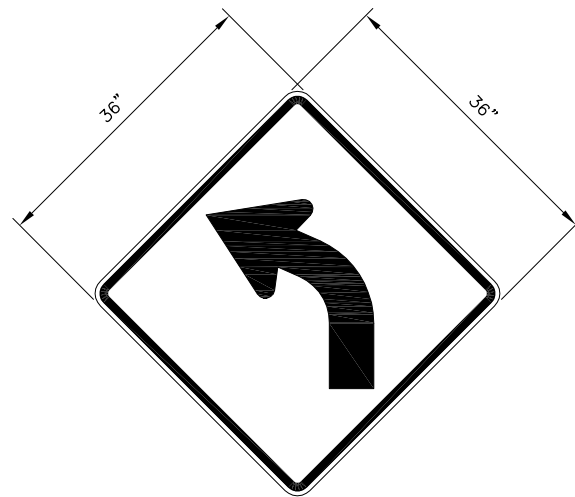
EXTEND TAXIWAY Y
 MARKING DETAILS 3

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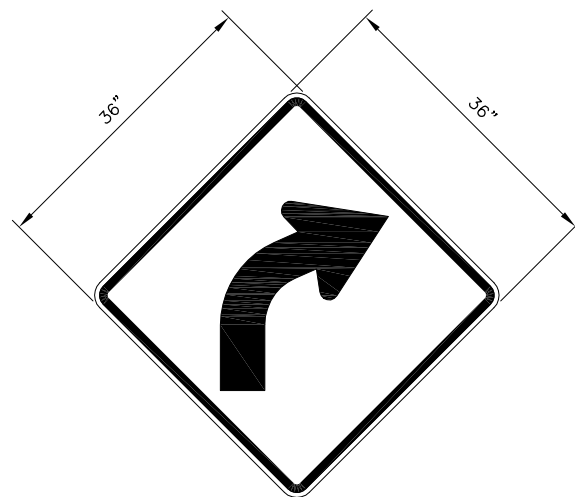


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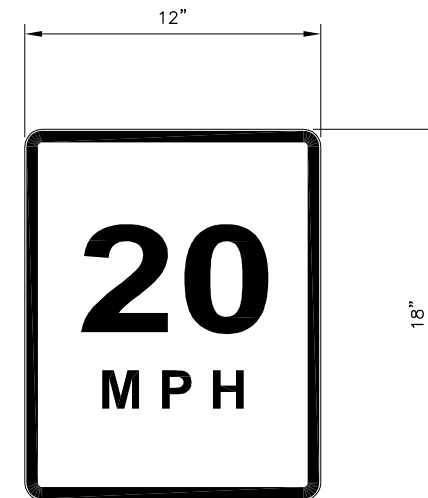
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WI-2L
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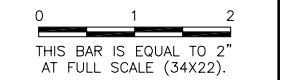
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**ROADWAY EDGELINES ADJACENT
TO APRON - CONTINUOUS**
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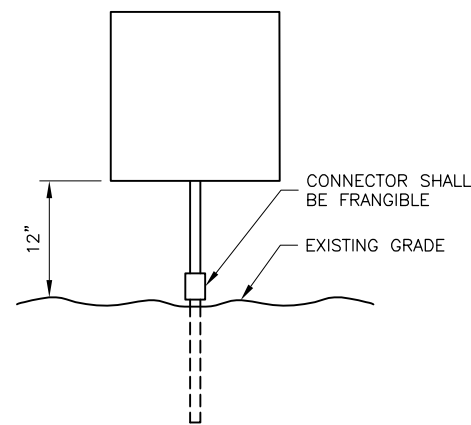
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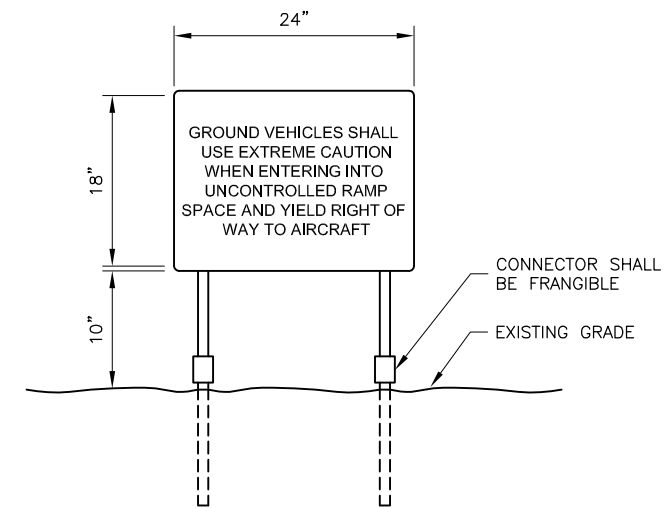


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EXTEND TAXIWAY Y
 ROADWAY SIGNAGE DETAILS



SIGN POST DETAIL
N.T.S.



INFORMATION SIGN LEGEND
N.T.S.

INFORMATION SIGN NOTE

1. FINAL SIGN FACE LEGEND SHALL BE DETERMINED BY THE AIRPORT.
2. SIGN LEGEND SHALL BE BLACK LETTERING ON WHITE BACKGROUND.

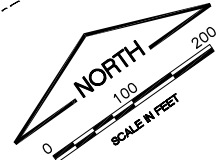
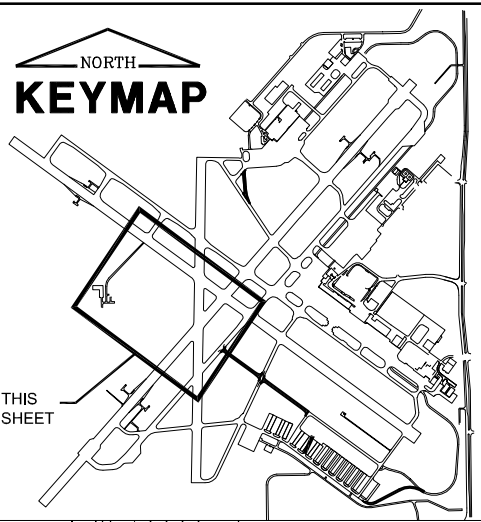
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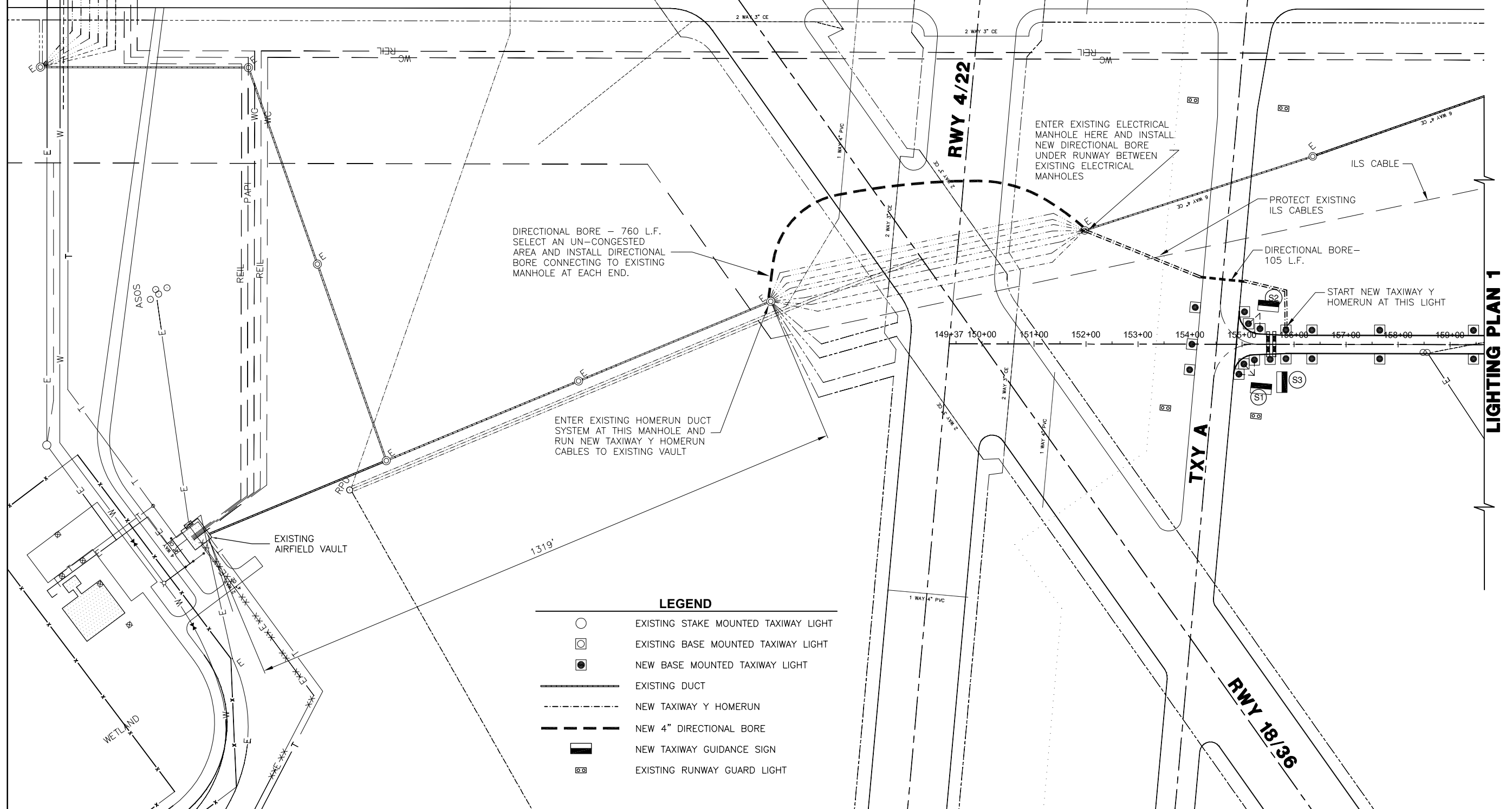
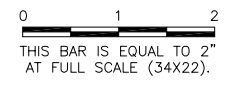
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 BASE_EXIST_UTL
 BASE_PROP_ELEC_2
 SPI-BASE CURRENT GEOMETRY

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EXTEND TAXIWAY Y
HOMERUN PLAN

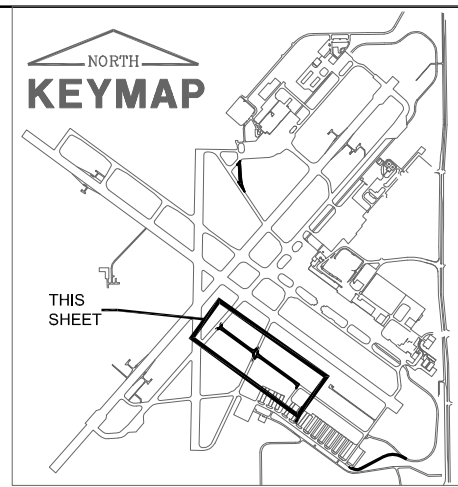
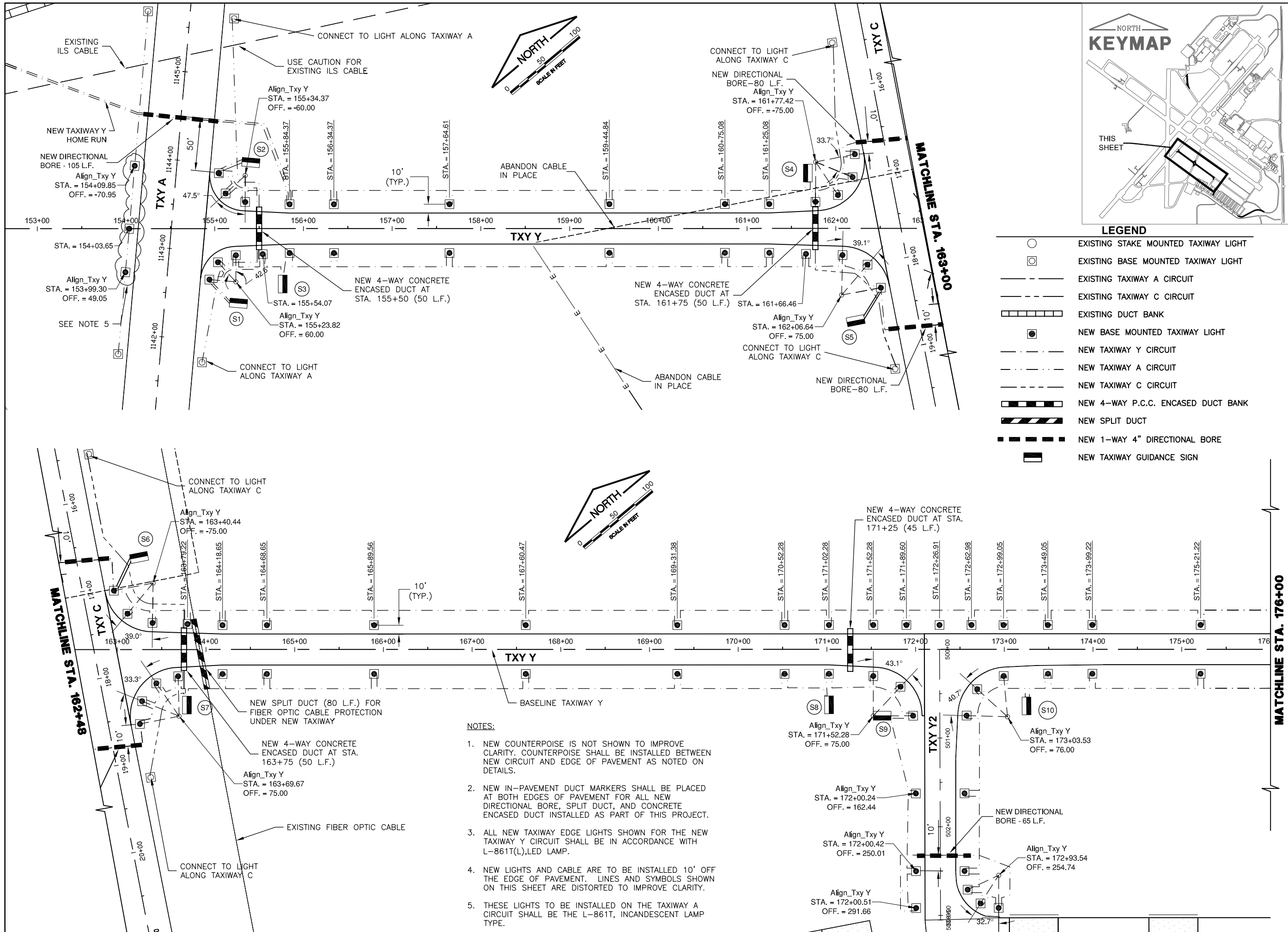
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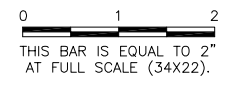
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 ACAD-1103504-V-VF2D_2
 AS-ELEC
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LEGEND

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| | EXISTING STAKE MOUNTED TAXIWAY LIGHT |
| | EXISTING BASE MOUNTED TAXIWAY LIGHT |
| | EXISTING TAXIWAY A CIRCUIT |
| | EXISTING TAXIWAY C CIRCUIT |
| | EXISTING DUCT BANK |
| | NEW BASE MOUNTED TAXIWAY LIGHT |
| | NEW TAXIWAY Y CIRCUIT |
| | NEW TAXIWAY A CIRCUIT |
| | NEW TAXIWAY C CIRCUIT |
| | NEW 4-WAY P.C.C. ENCASED DUCT BANK |
| | NEW SPLIT DUCT |
| | NEW 1-WAY 4" DIRECTIONAL BORE |
| | NEW TAXIWAY GUIDANCE SIGN |

- NOTES:**
1. NEW COUNTERPOISE IS NOT SHOWN TO IMPROVE CLARITY. COUNTERPOISE SHALL BE INSTALLED BETWEEN NEW CIRCUIT AND EDGE OF PAVEMENT AS NOTED ON DETAILS.
 2. NEW IN-PAVEMENT DUCT MARKERS SHALL BE PLACED AT BOTH EDGES OF PAVEMENT FOR ALL NEW DIRECTIONAL BORE, SPLIT DUCT, AND CONCRETE ENCASED DUCT INSTALLED AS PART OF THIS PROJECT.
 3. ALL NEW TAXIWAY EDGE LIGHTS SHOWN FOR THE NEW TAXIWAY Y CIRCUIT SHALL BE IN ACCORDANCE WITH L-861T(L),LED LAMP.
 4. NEW LIGHTS AND CABLE ARE TO BE INSTALLED 10' OFF THE EDGE OF PAVEMENT. LINES AND SYMBOLS SHOWN ON THIS SHEET ARE DISTORTED TO IMPROVE CLARITY.
 5. THESE LIGHTS TO BE INSTALLED ON THE TAXIWAY A CIRCUIT SHALL BE THE L-861T, INCANDESCENT LAMP TYPE.

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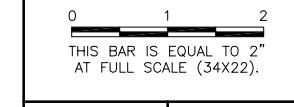
EXTEND TAXIWAY Y
LIGHTING PLAN 1

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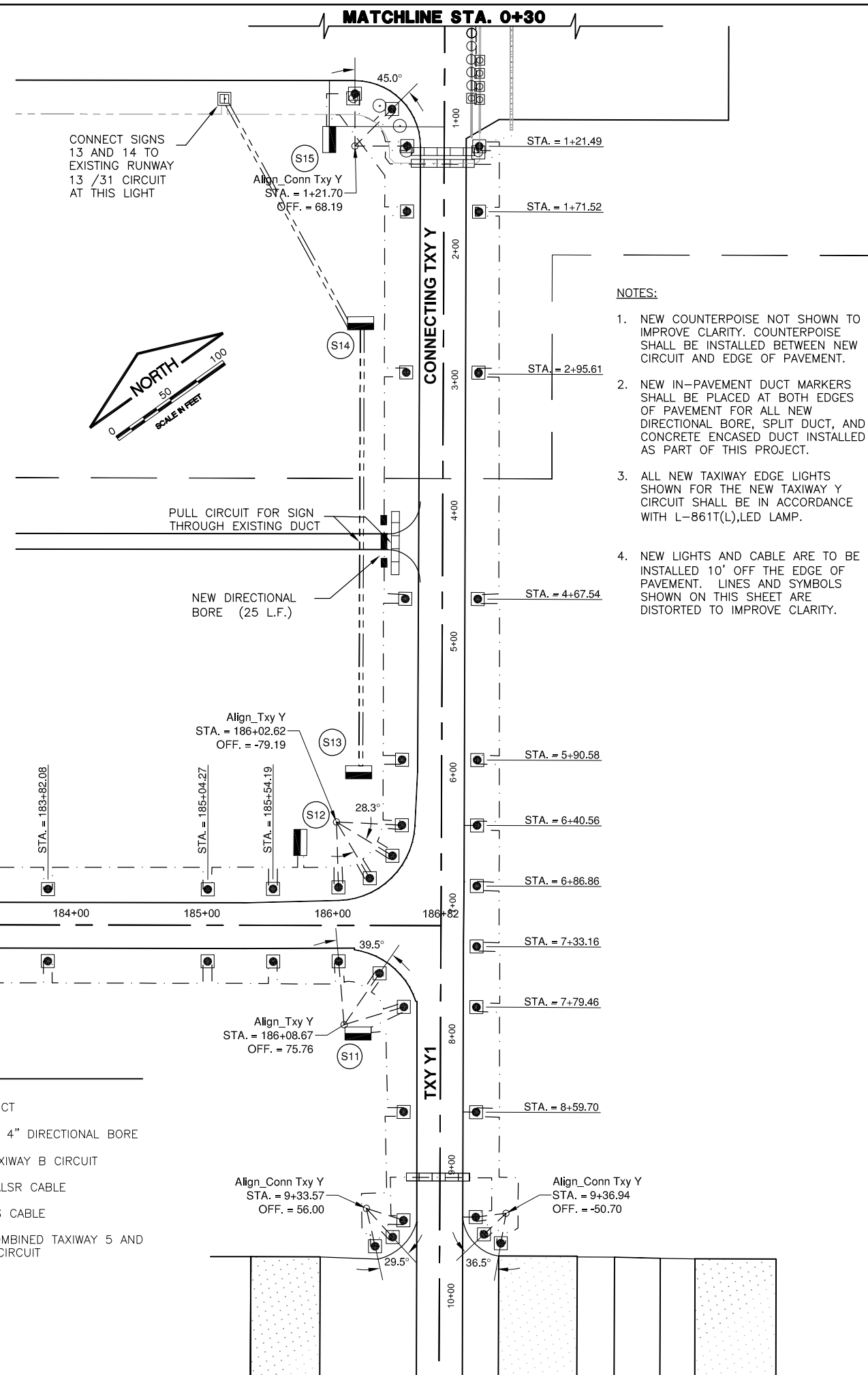


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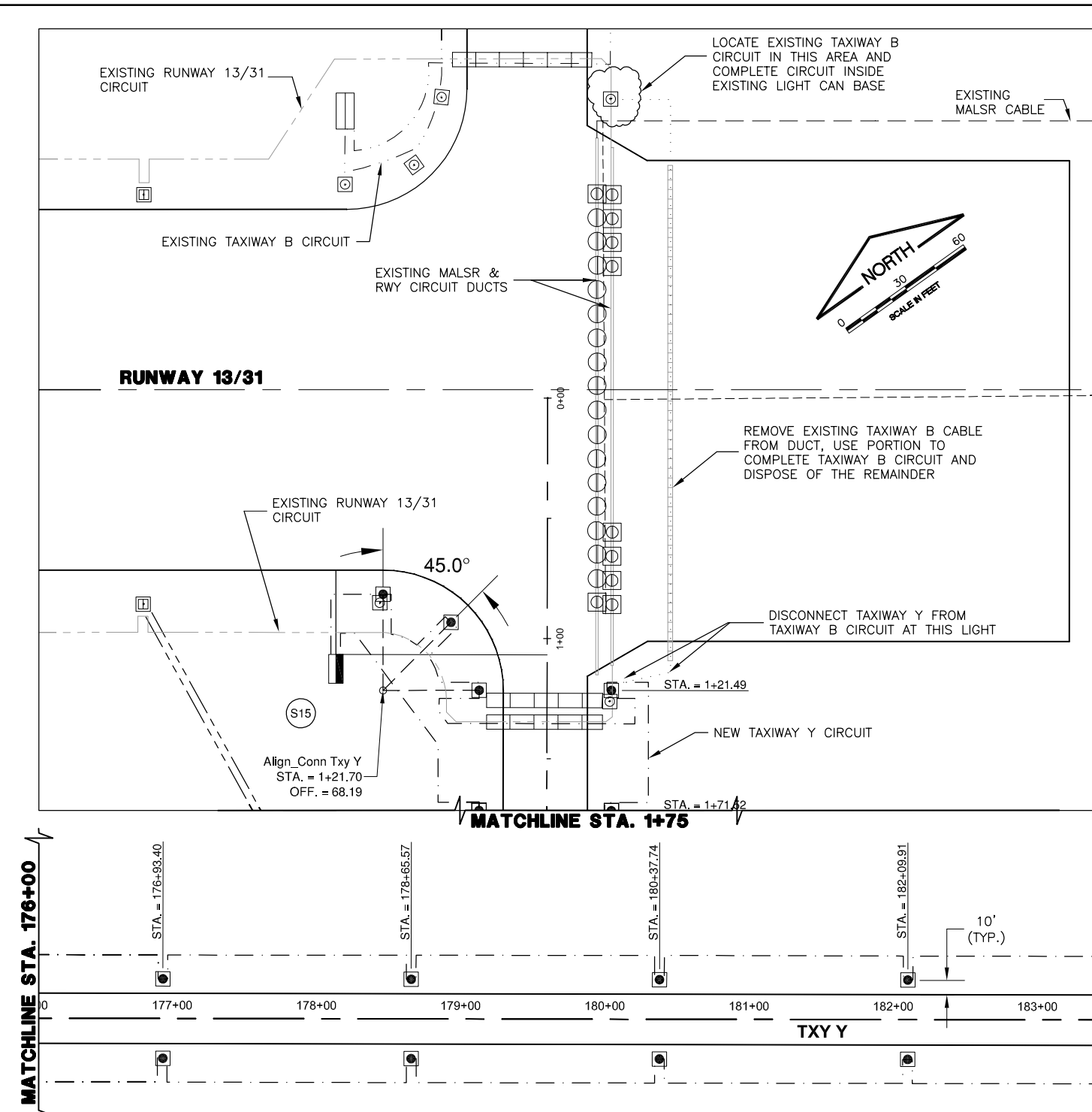
**EXTEND TAXIWAY Y
 LIGHTING PLAN 2**

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| SHEET 61 OF 106 SHEETS | |

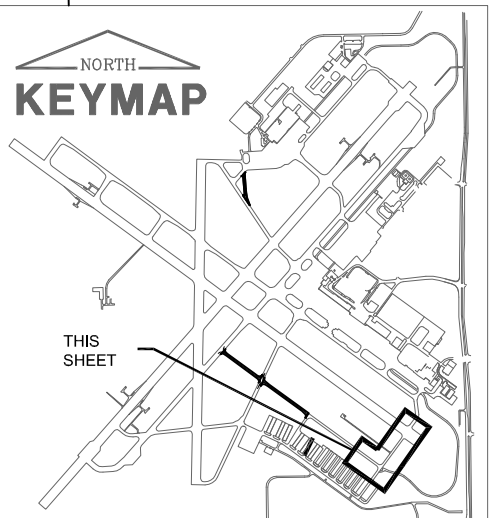


- NOTES:**
1. NEW COUNTERPOISE NOT SHOWN TO IMPROVE CLARITY. COUNTERPOISE SHALL BE INSTALLED BETWEEN NEW CIRCUIT AND EDGE OF PAVEMENT.
 2. NEW IN-PAVEMENT DUCT MARKERS SHALL BE PLACED AT BOTH EDGES OF PAVEMENT FOR ALL NEW DIRECTIONAL BORE, SPLIT DUCT, AND CONCRETE ENCASED DUCT INSTALLED AS PART OF THIS PROJECT.
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LEGEND

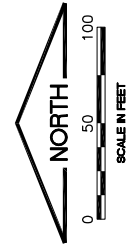
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|------|--------------------------------------|------|---|
| ○ | EXISTING STAKE MOUNTED TAXIWAY LIGHT | ▬▬▬▬ | EXISTING DUCT |
| ◉ | EXISTING BASE MOUNTED TAXIWAY LIGHT | ▬▬▬▬ | NEW 1-WAY 4" DIRECTIONAL BORE |
| □ | EXISTING STAKE MOUNTED RUNWAY LIGHT | ▬▬▬▬ | EXISTING TAXIWAY B CIRCUIT |
| ◻ | EXISTING BASE MOUNTED RUNWAY LIGHT | ▬▬▬▬ | EXISTING MALSR CABLE |
| ▬▬▬▬ | EXISTING RUNWAY 13/31 CIRCUIT | ▬▬▬▬ | EXISTING ILS CABLE |
| ● | NEW BASE MOUNTED TAXIWAY LIGHT | ▬▬▬▬ | EXISTING COMBINED TAXIWAY 5 AND TAXIWAY 4 CIRCUIT |
| ▬▬▬▬ | NEW TAXIWAY Y CIRCUIT | | |
| ▬▬▬▬ | NEW RUNWAY 13/31 RUNWAY CIRCUIT | | |
| ▬▬▬▬ | NEW 4-WAY P.C.C. ENCASED DUCT BANK | | |



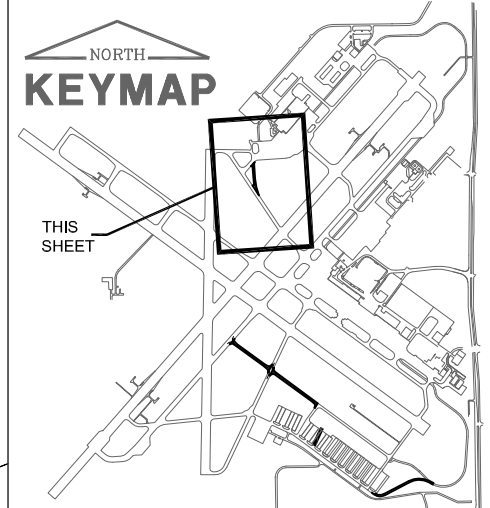
MATCHLINE STA. 176+00

MATCHLINE STA. 1+75

MATCHLINE STA. 0+30

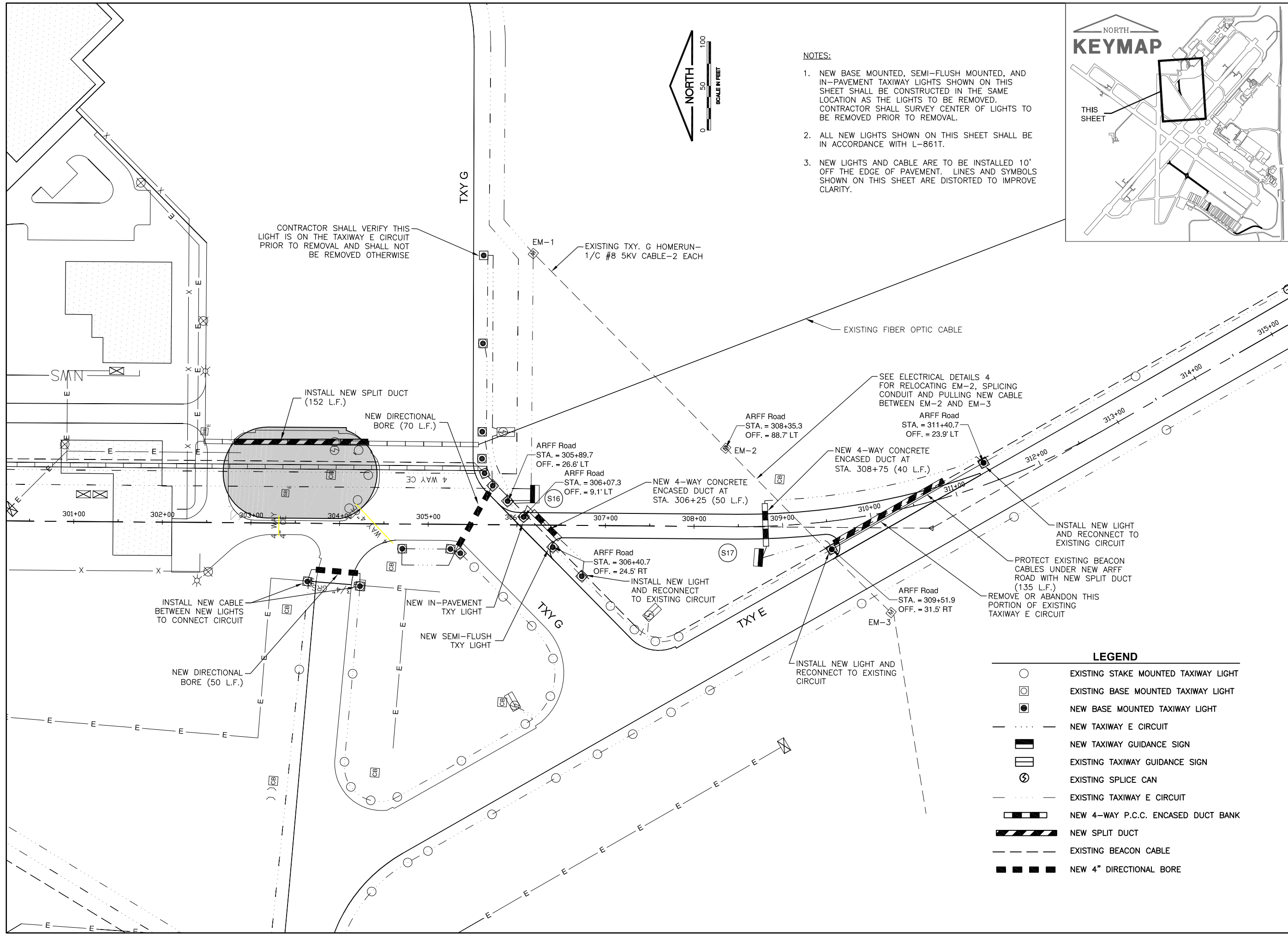
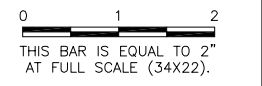


- NOTES:**
1. NEW BASE MOUNTED, SEMI-FLUSH MOUNTED, AND IN-PAVEMENT TAXIWAY LIGHTS SHOWN ON THIS SHEET SHALL BE CONSTRUCTED IN THE SAME LOCATION AS THE LIGHTS TO BE REMOVED. CONTRACTOR SHALL SURVEY CENTER OF LIGHTS TO BE REMOVED PRIOR TO REMOVAL.
 2. ALL NEW LIGHTS SHOWN ON THIS SHEET SHALL BE IN ACCORDANCE WITH L-861T.
 3. NEW LIGHTS AND CABLE ARE TO BE INSTALLED 10' OFF THE EDGE OF PAVEMENT. LINES AND SYMBOLS SHOWN ON THIS SHEET ARE DISTORTED TO IMPROVE CLARITY.



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 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/8/2012 9:13 PM
 KEYMAP
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 1103504-V-VF2D
 BASE_EXIST_ELEC
 BASE_PROP_ELEC_2
 BASE_EXIST_GEO
 SPL_UH
 SPI-BASE CURRENT GEOMETRY
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LEGEND

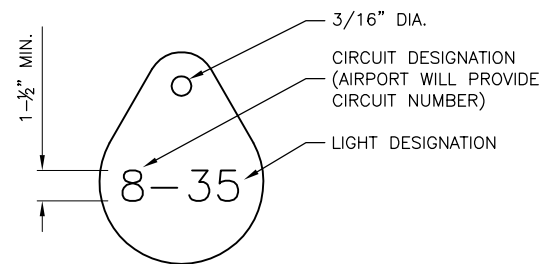
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| | EXISTING STAKE MOUNTED TAXIWAY LIGHT |
| | EXISTING BASE MOUNTED TAXIWAY LIGHT |
| | NEW BASE MOUNTED TAXIWAY LIGHT |
| | NEW TAXIWAY E CIRCUIT |
| | EXISTING TAXIWAY E CIRCUIT |
| | NEW TAXIWAY GUIDANCE SIGN |
| | EXISTING TAXIWAY GUIDANCE SIGN |
| | EXISTING SPLICE CAN |
| | EXISTING TAXIWAY E CIRCUIT |
| | NEW 4-WAY P.C.C. ENCASED DUCT BANK |
| | NEW SPLIT DUCT |
| | EXISTING BEACON CABLE |
| | NEW 4" DIRECTIONAL BORE |

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EXTEND TAXIWAY Y
LIGHTING PLAN 3

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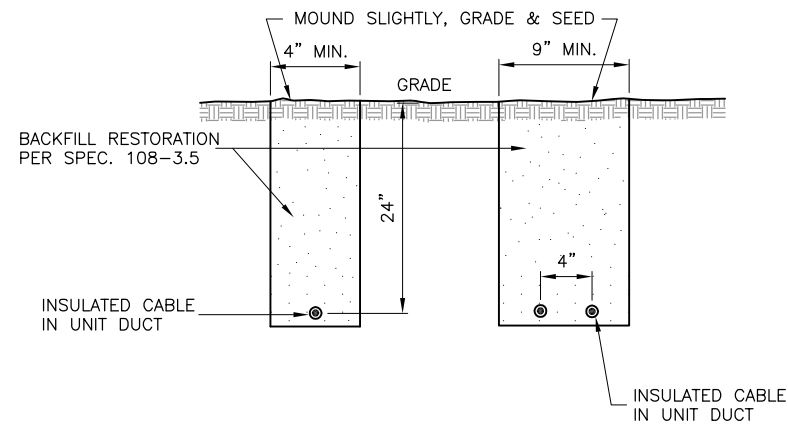
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| APPROVED BY: | RLV |
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| JOB No: | 110350400 |
| SPI-4156 3-17-0096-XX | |
| SHEET 62 OF 106 SHEETS | |



LIGHT IDENTIFICATION DETAIL
N.T.S.

LIGHT IDENTIFICATION NOTES

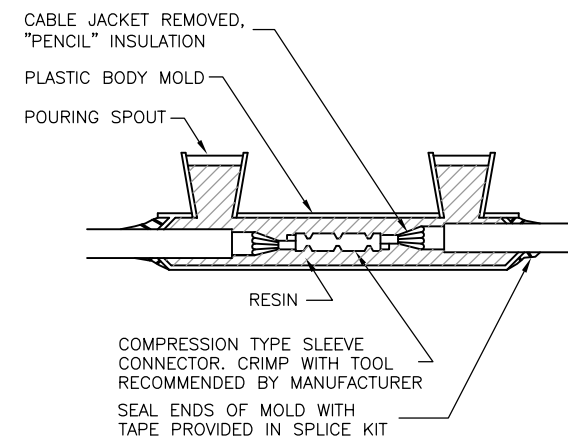
1. INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH SET SCREW.
2. LEGENDS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. CONTRACTOR TO COORDINATE LEGEND WITH AIRPORT.
3. THE CONTRACTOR SHALL NUMBER THE EXISTING/PROPOSED LIGHTS AND SIGNS IN EACH CIRCUIT STARTING AT THE HOMERUN CONTINUING AROUND THE ENTIRE CIRCUIT BACK TO THE HOMERUN.
4. AIRFIELD SIGNS SHALL BE TAGGED & NUMBERED.



TRENCH DETAIL
N.T.S.

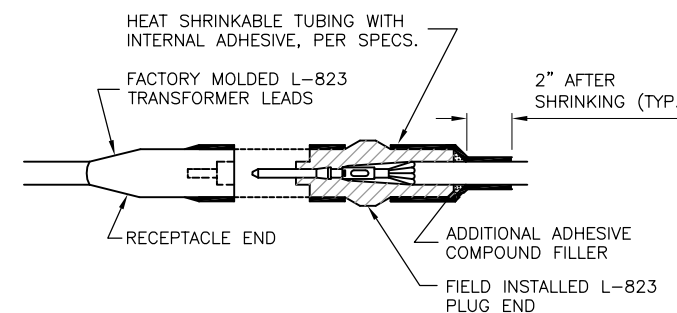
TRENCH NOTES

1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
2. DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.



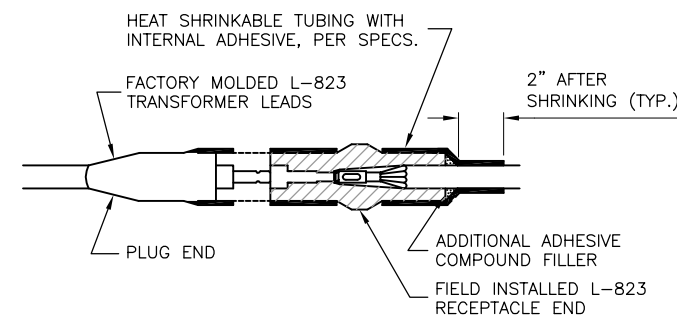
FOR IN-LINE CONNECTIONS OF EXISTING CABLES CUT DURING CONSTRUCTION.

TYPE A



FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

TYPE C



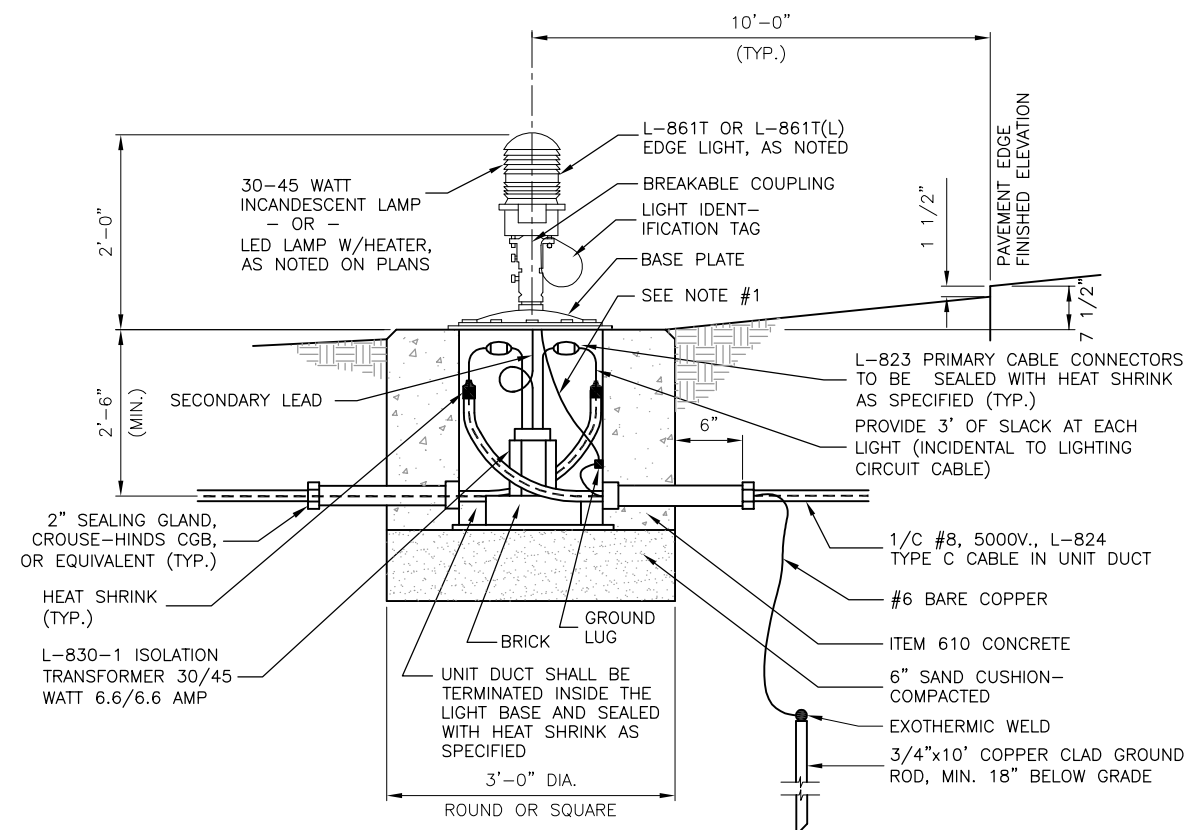
FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

TYPE D

CABLE SPLICES
N.T.S.

CABLE SPLICE NOTES

1. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
2. THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
3. THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.



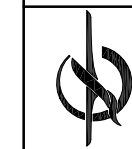
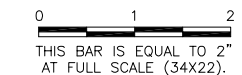
BASE MOUNTED EDGE LIGHTS
N.T.S.

EDGE LIGHT NOTES

1. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.
2. L-861T EDGE LIGHTS SHALL BE THE 30-45 WATT INCANDESCENT LAMP. THE L-861(L) EDGE LIGHTS SHALL BE THE LED LAMPS WITH HEATERS.
3. SEE LIGHTING PLAN SHEETS FOR NOTES REGARDING WHERE EACH TYPE IS REQUIRED.

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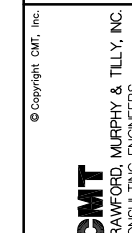
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EXTEND TAXIWAY Y

ELECTRICAL DETAILS 1

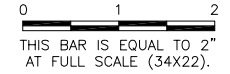


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EXTEND TAXIWAY Y

ELECTRICAL DETAILS 2

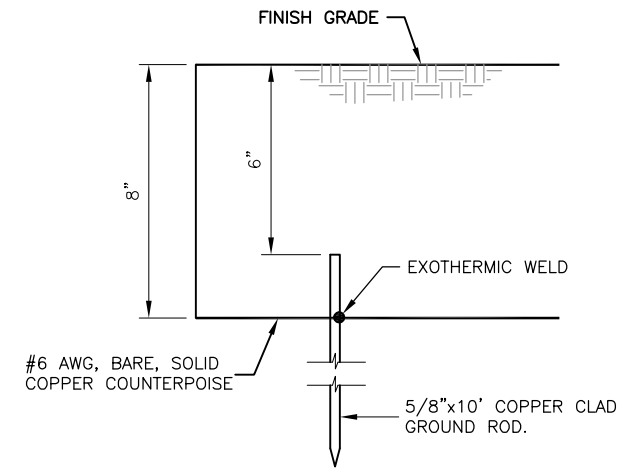
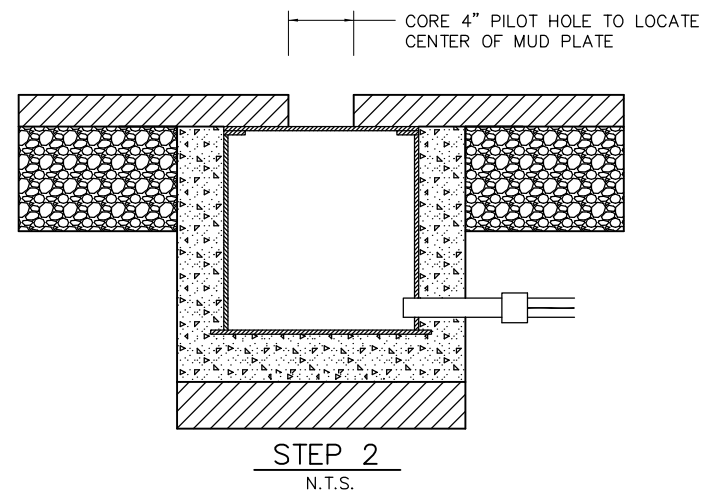
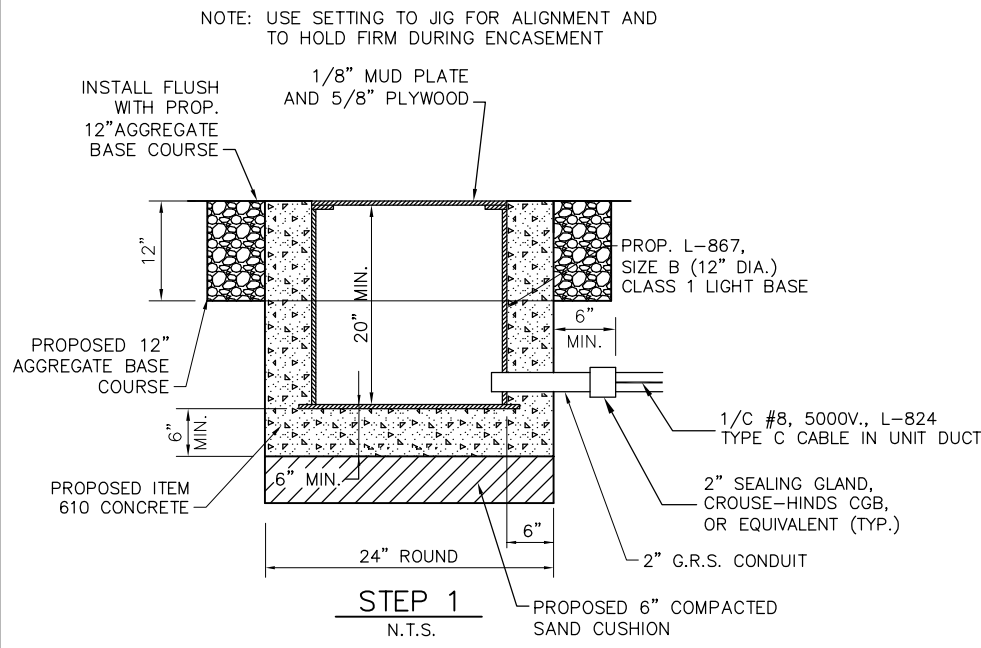
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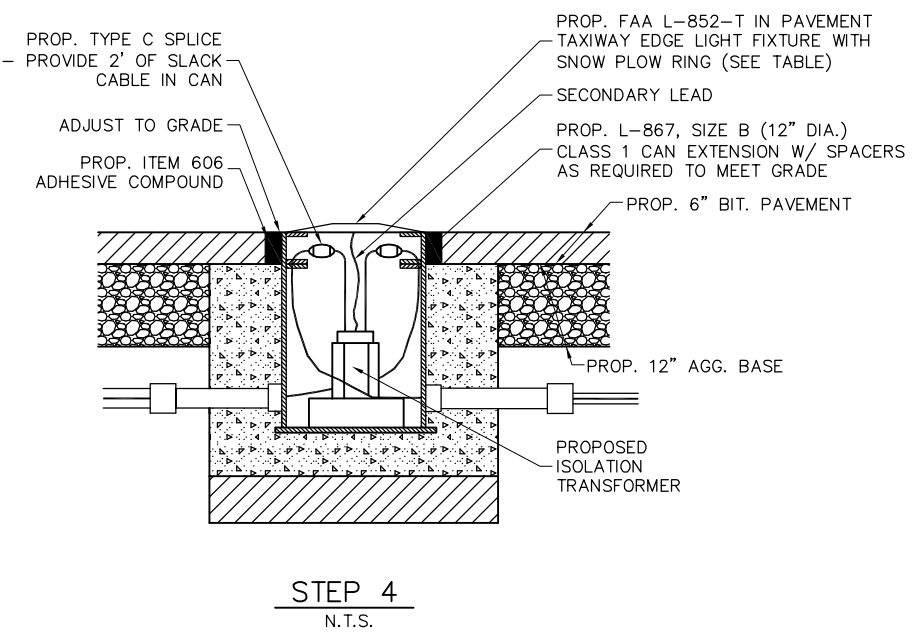
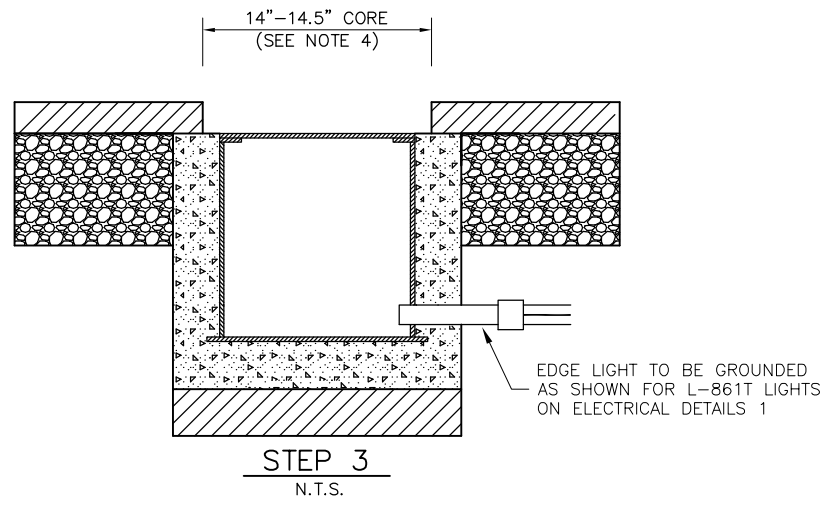
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- NOTES:
- GROUND RODS SHALL BE INSTALLED AT 500' INTERVALS.
 - COUNTERPOISE SHALL BE INSTALLED 5' OFF THE EDGE OF PAVEMENT BETWEEN THE NEW EDGE CIRCUIT CABLE AND THE EDGE OF PAVEMENT.

COUNTERPOISE INSTALLATION
N.T.S.

- NOTES:
- INSTALLATION SEQUENCE SHALL BE AS DETAILED IN STEP 1 THROUGH 4, AND AS DESCRIBED AS BELOW.
 - USE MANUFACTURER'S SETTING JIG (OR OTHER DEVICE APPROVED BY THE ENGINEER) FOR PROPERLY ALIGNING THE BASE AND FOR PREVENTING MOVEMENT DURING CONCRETE ENCASEMENT.
 - SURVEY LOCATION OF COVER PLATE BULLSEYE.
 - AFTER FINAL PAVING AT THE FIXTURE LOCATION, CORE A 4" DIAMETER PILOT HOLE TO LOCATE THE CENTER OF THE BULLSEYE PLATE. CORE 14" TO 14.5" DIAMETER HOLE (LEAVING 1/2" + ANNULAR SPACE BETWEEN BASE CAN AND SURROUNDING PAVEMENT), CENTERED ON THE LIGHT BASE. REMOVE THE COVER PLATE, ADJUST CAN TO REQUIRED ELEVATION, AND INSTALL LIGHT ASSEMBLY. SHIMS MAY BE REQUIRED.
 - AFTER FIXTURE INSTALLATION, FILL THE ANNULAR SPACE BETWEEN THE FIXTURE BASE AND SURROUNDING PAVEMENT WITH P-606 ADHESIVE COMPOUND. ENSURE P-606 IS INSTALLED CLEANLY AND DOES NOT BOND TO LIGHT FIXTURE SO AS TO PREVENT FIXTURE REMOVAL.
 - THE L-867, SIZE B TYPE 1 BASES SHALL BE ADJUSTED SO THAT THE TOP FLANGE EDGE OF THE BASE EXTENSION IS AT THE SAME ELEVATION AS THE FINISHED PAVEMENT SURFACE TO (+) 0" (-) 1/16" TOLERANCE.
 - REUSE SHIPPING BOLTS INCLUDED WITH THE BASE ONLY IF BOLTS ARE STAINLESS STEEL AND IF INTENDED TO BE REUSED FOR LIGHT FIXTURE INSTALLATION. USE INDUSTRY STANDARD ANTI-SEIZE COMPOUND (GREASE) AND TORQUE PER MANUFACTURER'S INSTRUCTIONS.
 - PROVIDE (3) THREE COPIES OF AN INSTRUCTION AND MAINTENANCE MANUAL TO THE ENGINEER 14 DAYS PRIOR TO START OF INSTALLATION.

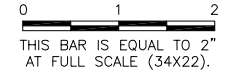


| FAA FIXTURE TYPE | LAMP WATTAGE | LAMP TYPE | TRANSFORMER TYPE | TRANSFORMER PRI/SEC. |
|------------------|--------------|-----------|------------------|----------------------|
| L-852-T | 45-65 | QUARTZ | L-830 | 6.6/6.6 |

ARRF ROAD IN-PAVEMENT LIGHT
N.T.S.

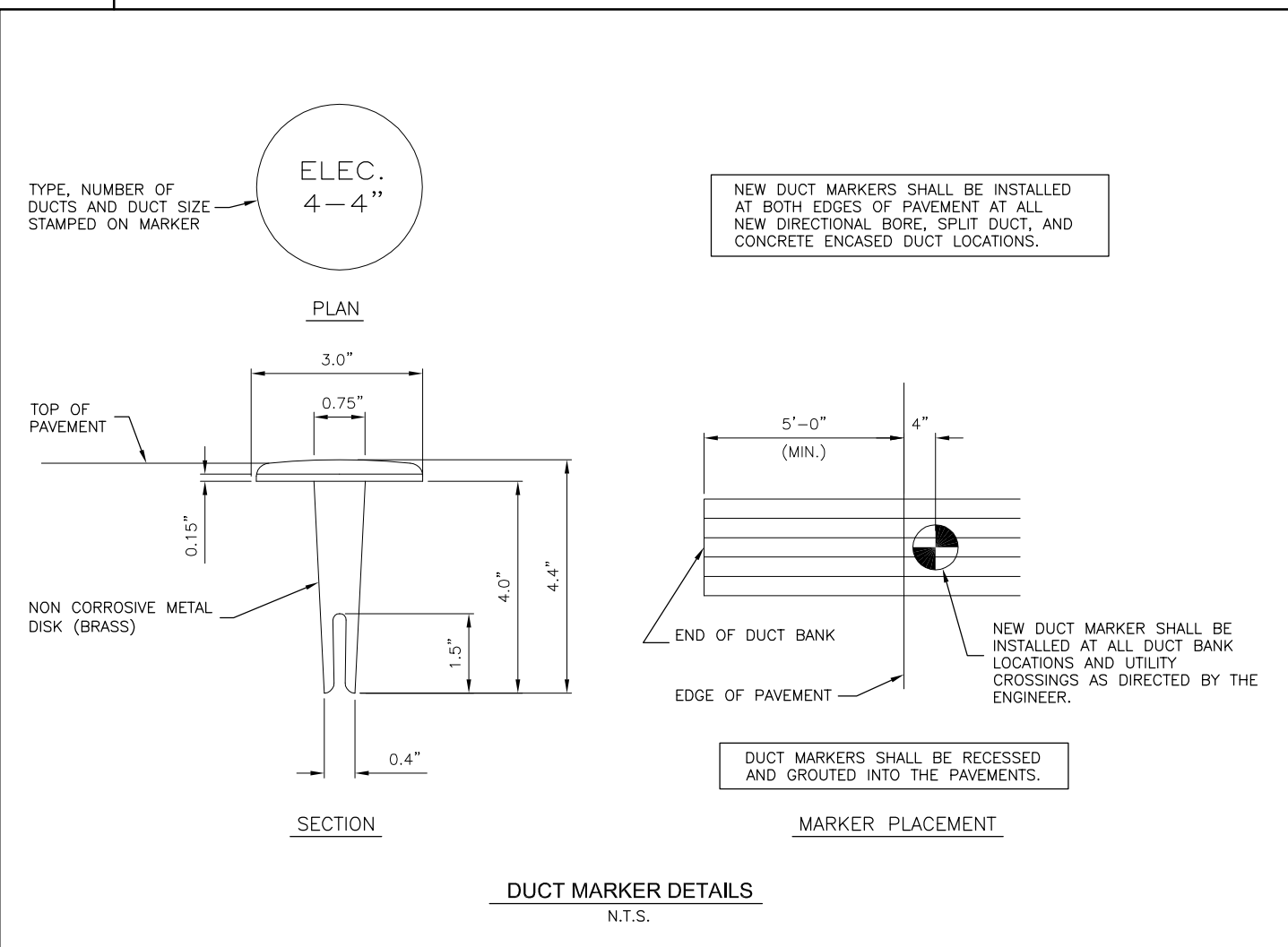
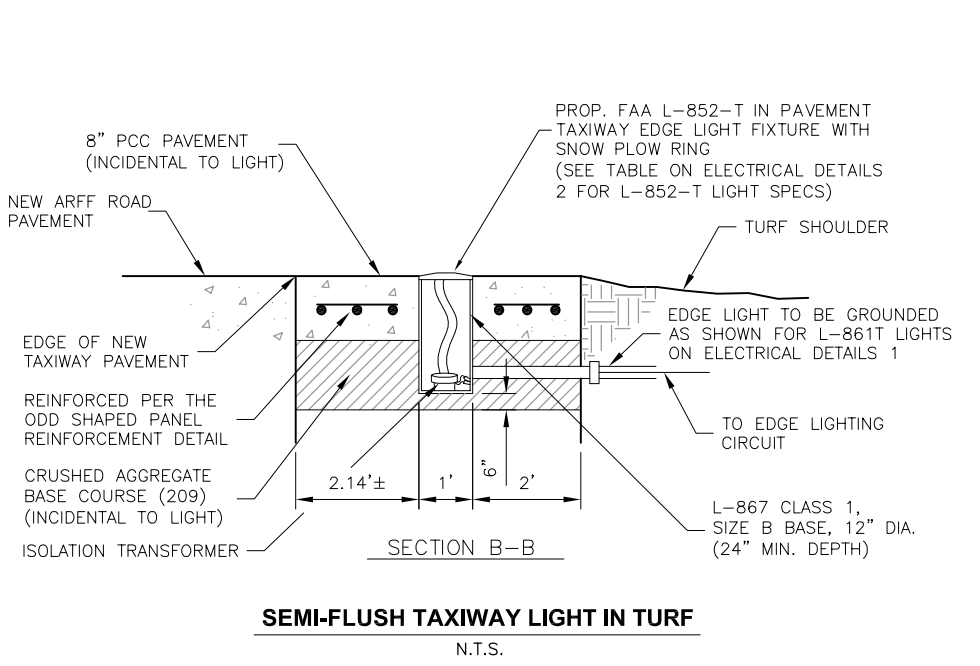
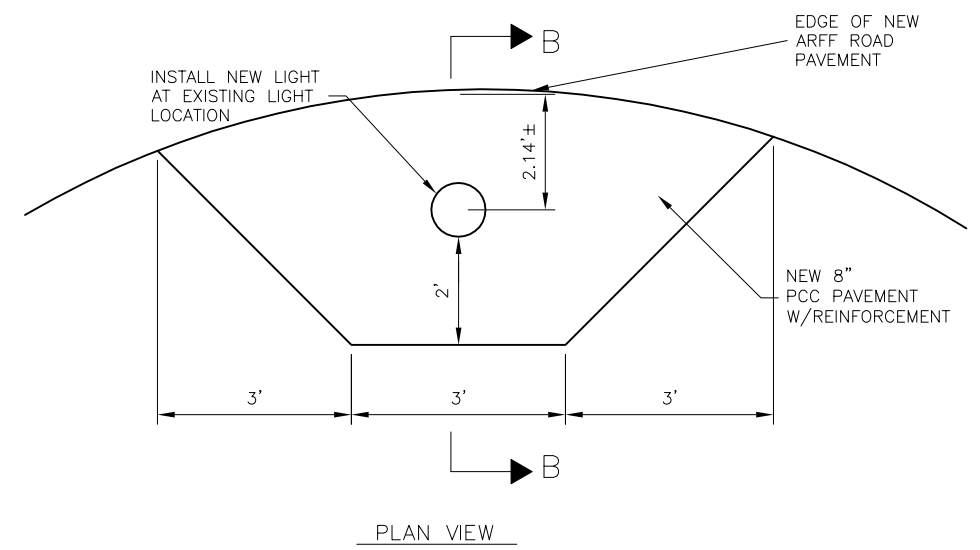
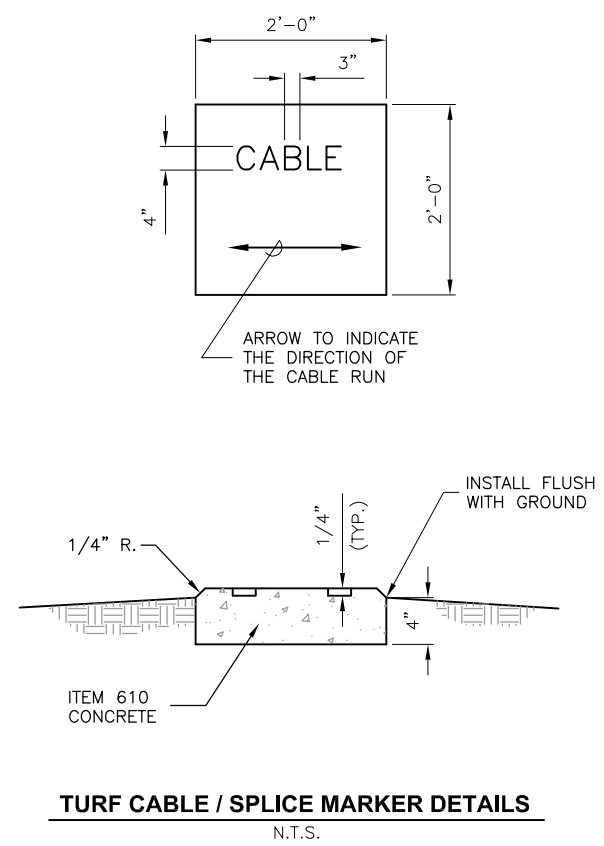
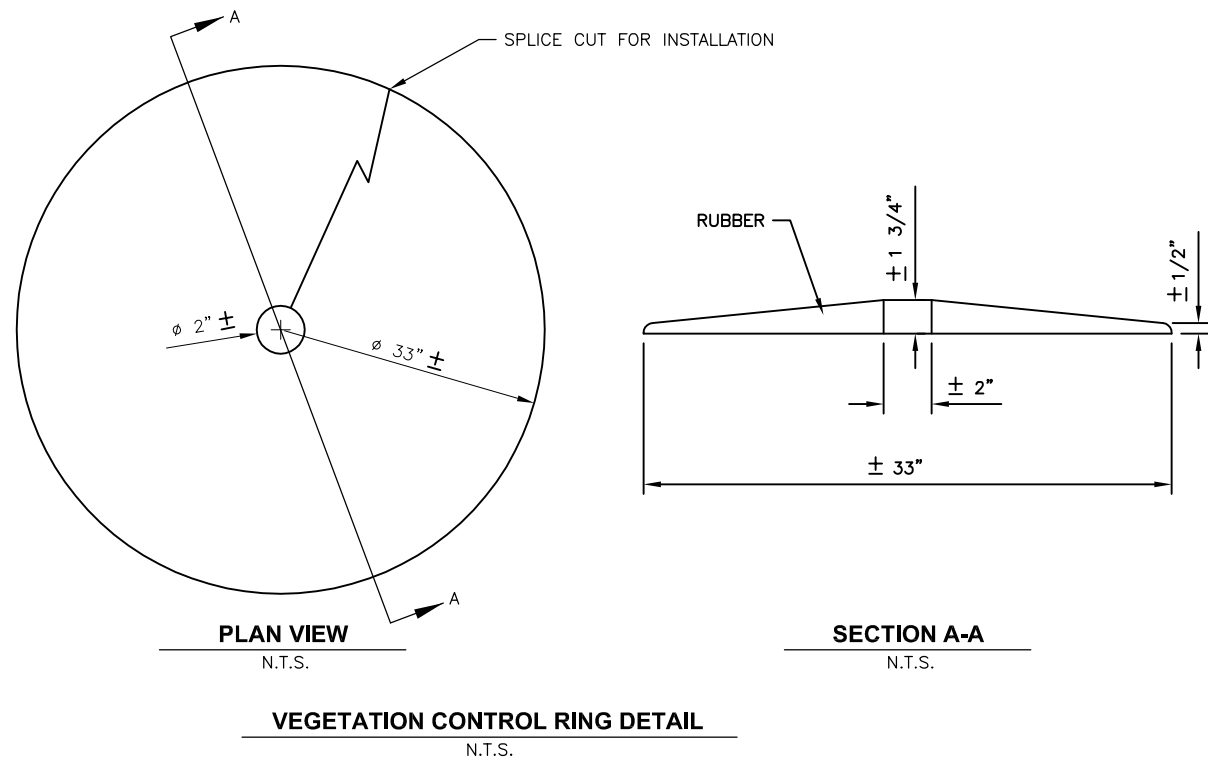
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**TURF CABLE /
 SPLICE MARKER NOTES**

1. CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
2. ITEM 610 CONCRETE SHALL BE USED.
3. ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
4. THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
5. 0.049 CU. YD. CONCRETE PER MARKER.
6. A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.



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EXTEND TAXIWAY Y
 ELECTRICAL DETAILS 3



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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



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EXTEND TAXIWAY Y

ELECTRICAL DETAILS 4

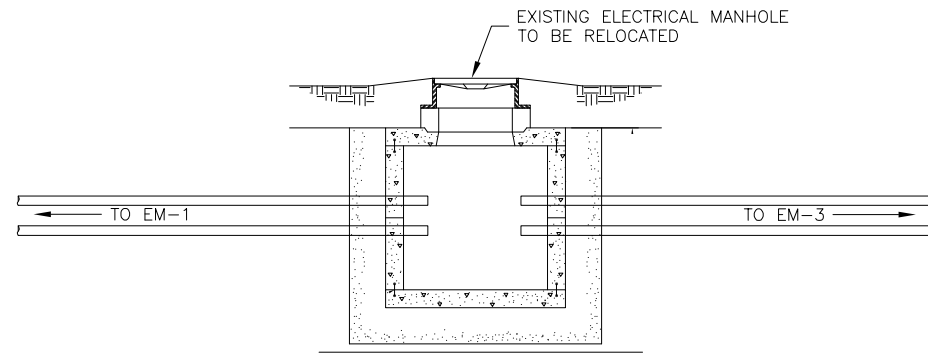
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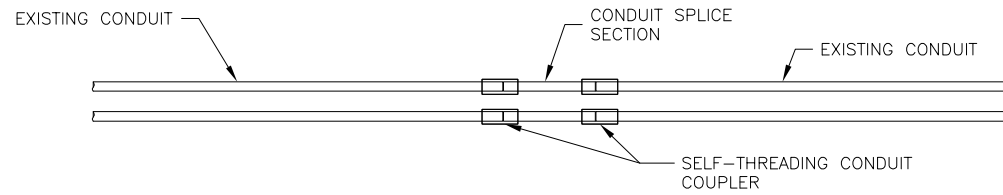


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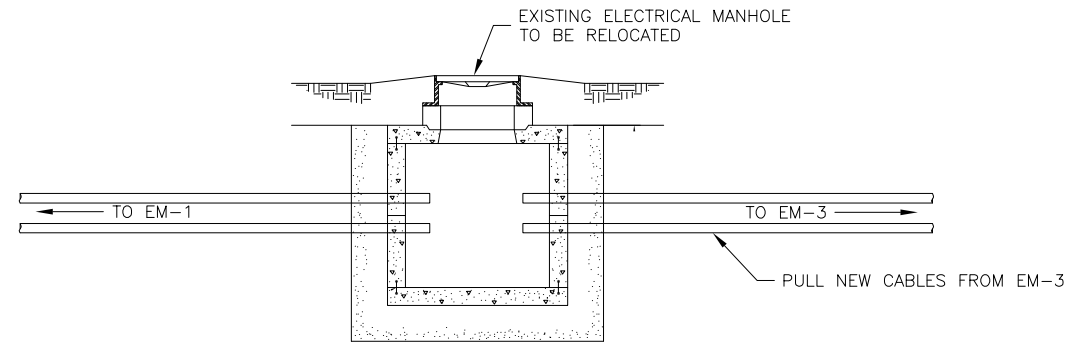
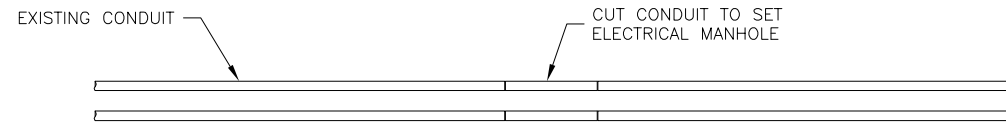


ELEVATION-EXISTING
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ELEVATION-AFTER REMOVAL AT EM-2
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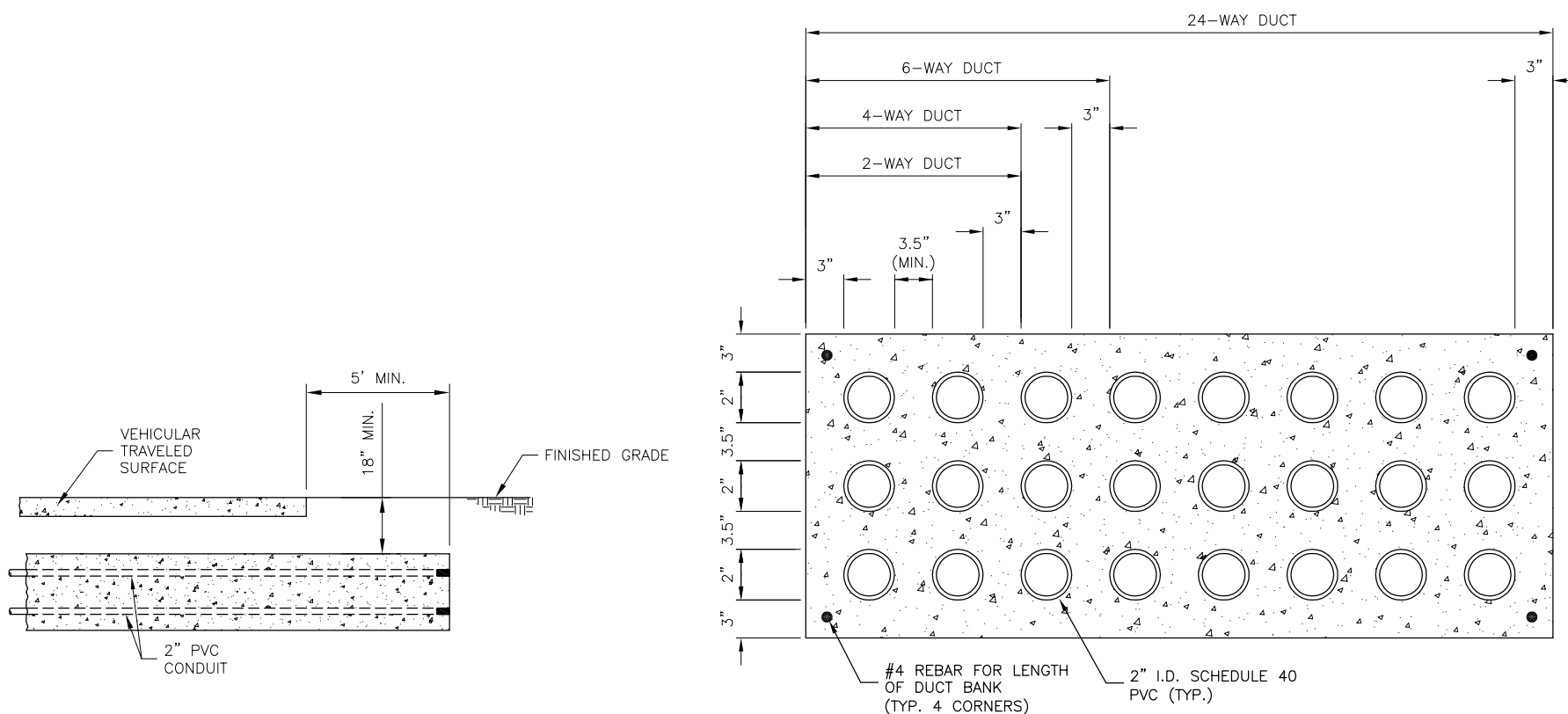
NOTE: EM-2 TO BE RELOCATED TO LOCATION SHOWN ON LIGHTING PLAN 3. A NEW SECTION OF CONDUIT SHALL BE PLACED TO CONNECT THE GAP CREATED IN THE EXISTING CONDUIT. SELF-THREADING COUPLER SHALL BE USED TO SECURELY CONNECT THE ENDS OF THE EXISTING CONDUIT TO THE ENDS OF THE NEW CONDUIT SECTION.



ELEVATION-PROPOSED
 N.T.S.

NOTE: THE EXISTING CONDUIT SHALL BE CUT IN ORDER FOR THE PLACEMENT OF THE RELOCATED EM-2. ONCE EM-2 IS SET IN PLACE NEW CABLE SHALL BE PULLED FROM EM-3 AND A CONNECTION SHALL BE MADE IN EM-2 TO CONNECT TO THE EXISTING CABLE.

ELECTRICAL MANHOLE DETAILS
 N.T.S.



DUCT BANK DETAILS
 N.T.S.

DUCT BANK NOTES

- DIMENSIONS SHOWN ARE MINIMUM.
- TOP OF CONCRETE ENCASUREMENT TO BE NOT LESS THAN 24" BELOW FINISHED SUBGRADE.
- DUCT CONCRETE TO BE ITEM 610 STRUCTURAL P.C.C. CONCRETE.
- ALL 24-WAY DUCT WILL BE 2" INSIDE DIAMETER. ALL 2-WAY DUCT WILL BE 6" INSIDE DIAMETER.
- LOCATIONS SHOWN ARE APPROXIMATE. DUCT BANKS TO BE INSTALLED AT LOCATIONS DESIGNATED BY THE ENGINEER.
- CONTRACTOR WILL INSTALL DUCT BANKS AT A DEPTH WHICH WILL NOT CONFLICT WITH ELEVATION SENSITIVE UTILITIES.
- A PULL WIRE MUST BE PROVIDED IN EACH UNUSED CONDUIT. UNUSED DUCT MUST BE SEALED AT CAN PLAZA TO THE SATISFACTION OF THE ENGINEER.

SIGNAGE SCHEDULE

| SIGN # | SIDE | NEW SIGN LEGEND | WHITE WITH BLACK OUTLINE ON RED BACKGROUND | BLACK LEGEND ON YELLOW BACKGROUND | YELLOW LEGEND ON BLACK BACKGROUND | BASELINE | STATION | OFFSET | NUMBER OF CHARACTERS | NEW POWER CIRCUIT | NOTES |
|--------|----------|------------------|--|-----------------------------------|-----------------------------------|-----------------|----------|-----------|----------------------|-------------------|----------|
| S1 | NE SW | A A Y→ | | Y→ | A A | TX Y | 153+72.3 | 70.5' RT | 3 | TX Y | NEW SIGN |
| S2 | NE SW | ←Y A A | | ←Y | A A | TX Y | 155+30.7 | 75.5' LT | 3 | TX Y | NEW SIGN |
| S3 | NW SE | Y Y←A→ | | ←A→ | Y Y | TX Y | 155+76.6 | 52.5' RT | 4 | TX Y | NEW SIGN |
| S4 | NW SE | Y←C→ Y | | ←C→ | Y Y | TX Y | 161+68.6 | 52.5' LT | 4 | TX Y | NEW SIGN |
| S5 | N S | C C←Y→ | | ←Y→ | C C | TX Y | 162+32.3 | 102.0' RT | 4 | TX Y | NEW SIGN |
| S6 | N S | C←Y→ C | | ←Y→ | C C | TX Y | 163+14.8 | 102.0' LT | 4 | TX Y | NEW SIGN |
| S7 | NW SE | Y Y←C→ | | ←C→ | Y Y | TX Y | 163+78.5 | 52.5' RT | 4 | TX Y | NEW SIGN |
| S8 | NW SE | Y Y2→ Y | | Y2→ | Y Y | TX Y | 171+25.0 | 52.5' LT | 4 | TX Y | NEW SIGN |
| S9 | NE SW | Y2 RMP↑ Y2←Y→ | | RMP↑ ←Y→ | Y2 Y2 | TX Y | 171+72.3 | 74.8' RT | 6 | TX Y | NEW SIGN |
| S10 | NW SE | Y ←Y2 Y | | ←Y2 | Y Y | TX Y | 173+25.0 | 53.8' RT | 4 | TX Y | NEW SIGN |
| S11 | NE SW | Y1 ←Y Y1 Y↑ | | ←Y Y↑ | Y1 Y1 | CONNECTING TX Y | 8+00.0 | 53.0' RT | 6 | TX Y | NEW SIGN |
| S12 | NW SE | ←Y Y Y1→ Y | | ←Y Y1→ | Y Y | TX Y | 185+75.0 | 53.4' LT | 6 | TX Y | NEW SIGN |
| S13 | SW | ILS | ILS | | | CONNECTING TX Y | 6+00.3 | 53.3' RT | 3 | RWY 13/31 | NEW SIGN |
| S14 | NE SW | Y Y 13-31 | 13-31 | | Y Y | CONNECTING TX Y | 2+57.0 | 53.2' RT | 6 | RWY 13/31 | NEW SIGN |
| S15 | NW | Y→ | | Y→ | | CONNECTING TX Y | 1+06.8 | 87.8' RT | 2 | TX Y | NEW SIGN |
| S16 | N | ⊖ | ⊖ | | | ARFF ROAD | 306+20 | 25' LT | 1 | TX Y | NEW SIGN |
| S17 | S | ⊖ | ⊖ | | | ARFF ROAD | 308+73 | 25' RT | 1 | TX Y | NEW SIGN |

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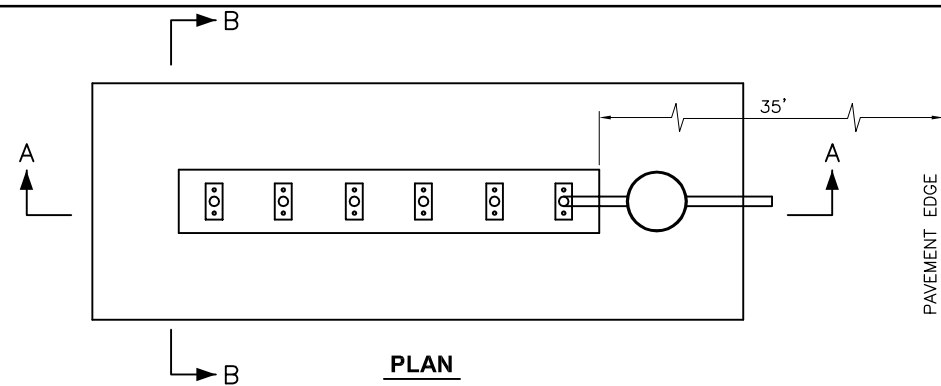
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THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).



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EXTEND TAXIWAY Y

AIRFIELD SIGNAGE DETAIL

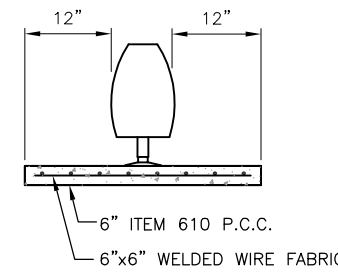
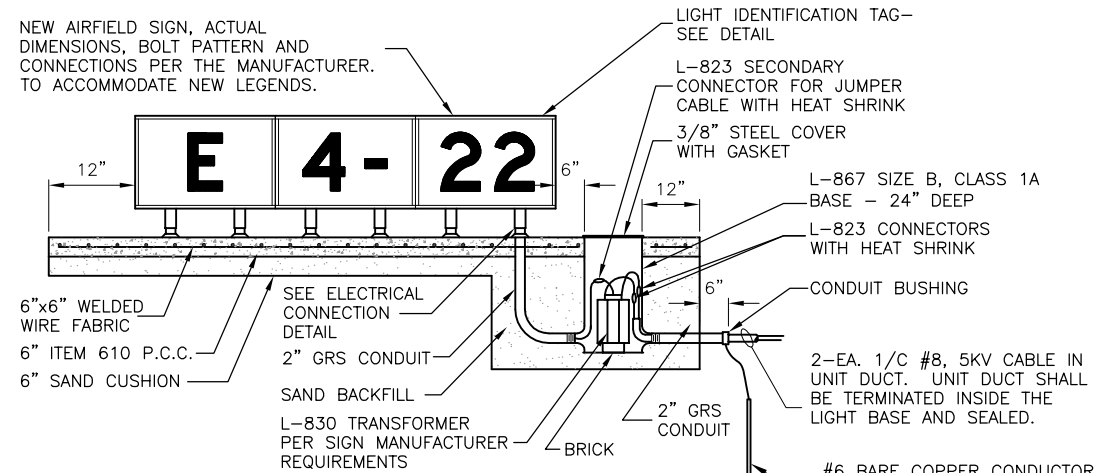


AIRFIELD SIGN NOTES

1. TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY SIGN MANUFACTURER.
2. SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS.
3. SIGNS SHALL BE SIZE 2, STYLE 2 OR 3, CLASS 2.
4. SEE "NEW BASE MOUNTED EDGE LIGHT" DETAIL FOR GROUNDING DETAILS.

NOTES

1. SIGN SCHEDULE IS SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN.
 2. CHANGES TO NEW LEGENDS MAY OCCUR SUBJECT TO 1 ABOVE.
 3. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS WITH NEW LEGENDS FOR APPROVAL PRIOR TO STARTING MANUFACTURE.
 4. EXISTING SIGNS ARE LUMACURVE.
- RUNWAY 13/31 CIRCUIT IS STYLE 3 CIRCUIT.

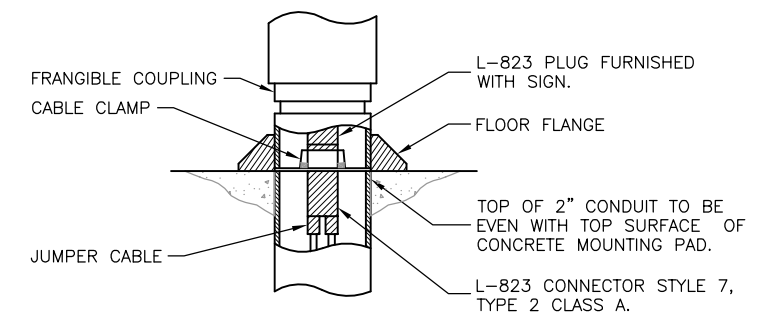


SECTION A-A

SECTION B-B

L-858 AIRFIELD SIGN DETAILS

N.T.S.



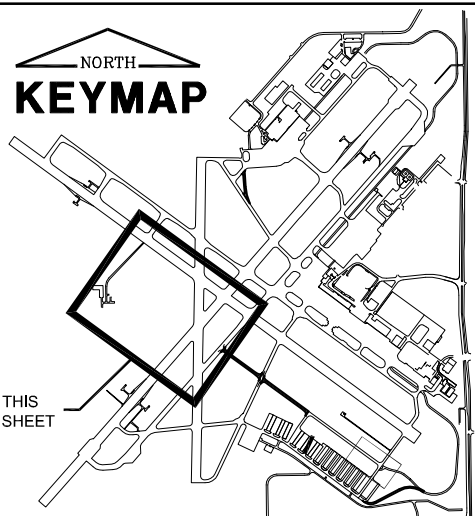
ELECTRICAL CONNECTION DETAIL

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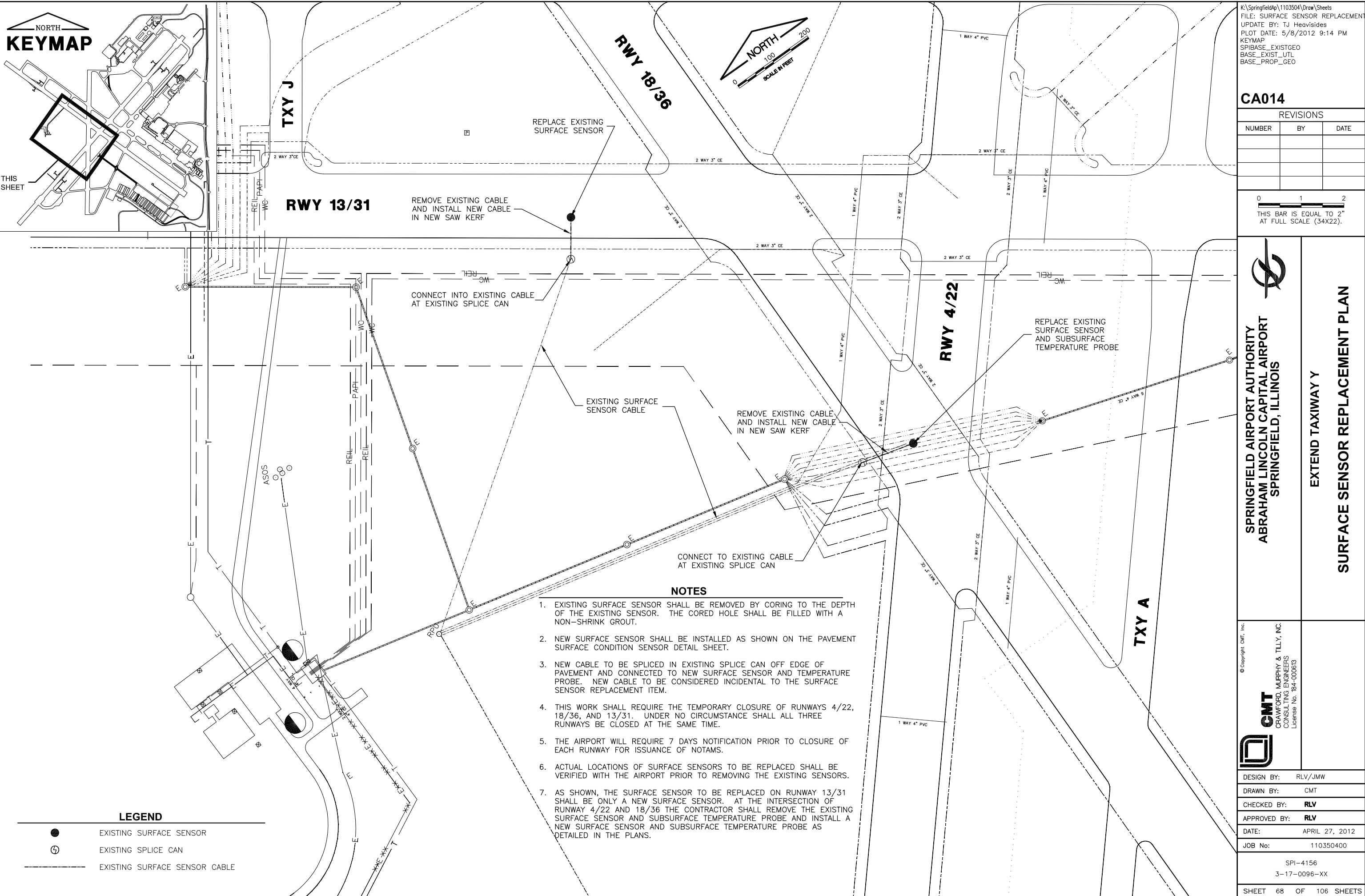
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 APPROVED BY: RLV
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THIS SHEET



LEGEND

| | |
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| ● | EXISTING SURFACE SENSOR |
| ⊕ | EXISTING SPLICE CAN |
| --- | EXISTING SURFACE SENSOR CABLE |

REPLACE EXISTING SURFACE SENSOR

REMOVE EXISTING CABLE AND INSTALL NEW CABLE IN NEW SAW KERF

CONNECT INTO EXISTING CABLE AT EXISTING SPLICE CAN

EXISTING SURFACE SENSOR CABLE

REMOVE EXISTING CABLE AND INSTALL NEW CABLE IN NEW SAW KERF

CONNECT TO EXISTING CABLE AT EXISTING SPLICE CAN

REPLACE EXISTING SURFACE SENSOR AND SUBSURFACE TEMPERATURE PROBE

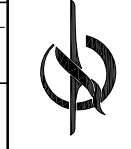
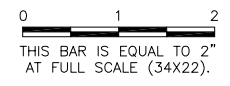
NOTES

1. EXISTING SURFACE SENSOR SHALL BE REMOVED BY CORING TO THE DEPTH OF THE EXISTING SENSOR. THE CORED HOLE SHALL BE FILLED WITH A NON-SHRINK GROUT.
2. NEW SURFACE SENSOR SHALL BE INSTALLED AS SHOWN ON THE PAVEMENT SURFACE CONDITION SENSOR DETAIL SHEET.
3. NEW CABLE TO BE SPLICED IN EXISTING SPLICE CAN OFF EDGE OF PAVEMENT AND CONNECTED TO NEW SURFACE SENSOR AND TEMPERATURE PROBE. NEW CABLE TO BE CONSIDERED INCIDENTAL TO THE SURFACE SENSOR REPLACEMENT ITEM.
4. THIS WORK SHALL REQUIRE THE TEMPORARY CLOSURE OF RUNWAYS 4/22, 18/36, AND 13/31. UNDER NO CIRCUMSTANCE SHALL ALL THREE RUNWAYS BE CLOSED AT THE SAME TIME.
5. THE AIRPORT WILL REQUIRE 7 DAYS NOTIFICATION PRIOR TO CLOSURE OF EACH RUNWAY FOR ISSUANCE OF NOTAMS.
6. ACTUAL LOCATIONS OF SURFACE SENSORS TO BE REPLACED SHALL BE VERIFIED WITH THE AIRPORT PRIOR TO REMOVING THE EXISTING SENSORS.
7. AS SHOWN, THE SURFACE SENSOR TO BE REPLACED ON RUNWAY 13/31 SHALL BE ONLY A NEW SURFACE SENSOR. AT THE INTERSECTION OF RUNWAY 4/22 AND 18/36 THE CONTRACTOR SHALL REMOVE THE EXISTING SURFACE SENSOR AND SUBSURFACE TEMPERATURE PROBE AND INSTALL A NEW SURFACE SENSOR AND SUBSURFACE TEMPERATURE PROBE AS DETAILED IN THE PLANS.

K:\Springfield\1103504\Draw\Sheets
 FILE: SURFACE SENSOR REPLACEMENT
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/8/2012 9:14 PM
 KEYMAP
 SPIBASE_EXISTGEO
 BASE_EXIST_UTL
 BASE_PROP_GEO

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EXTEND TAXIWAY Y

SURFACE SENSOR REPLACEMENT PLAN

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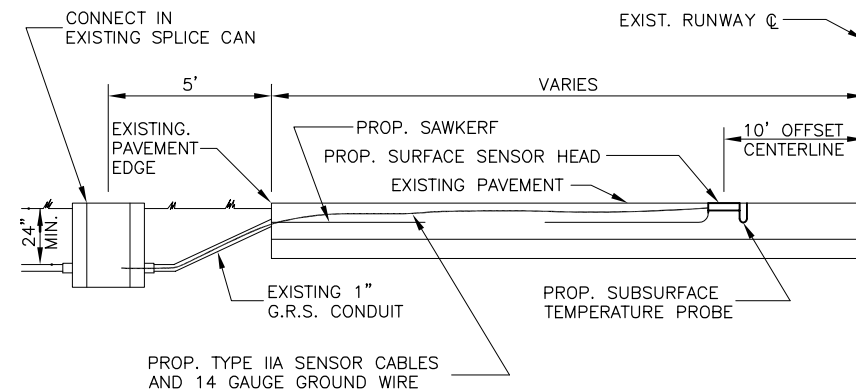
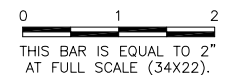


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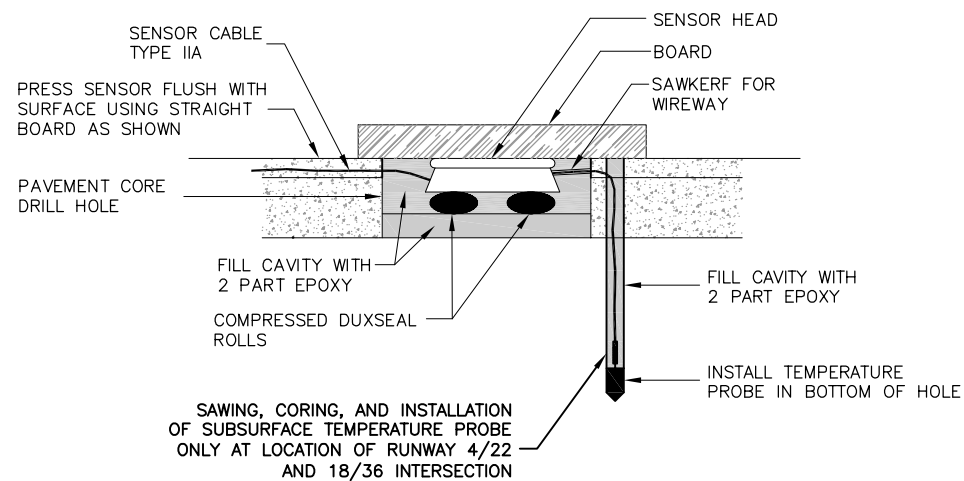
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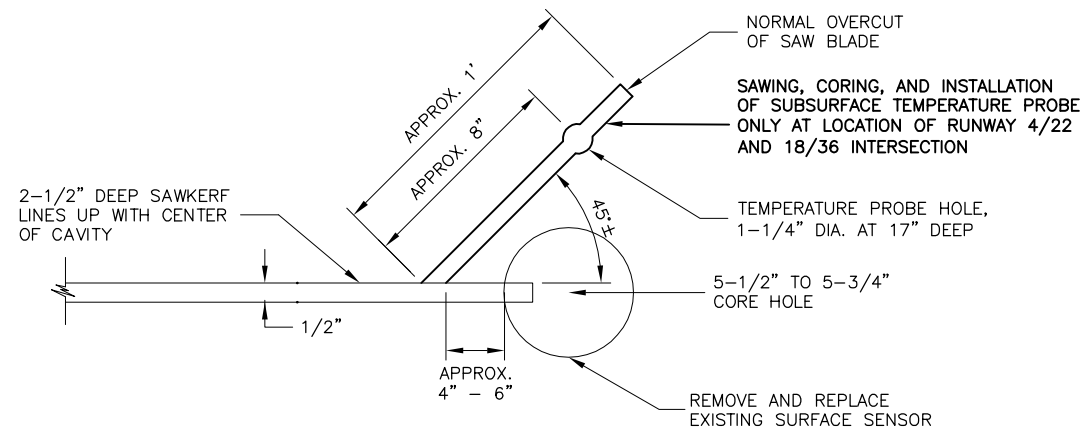
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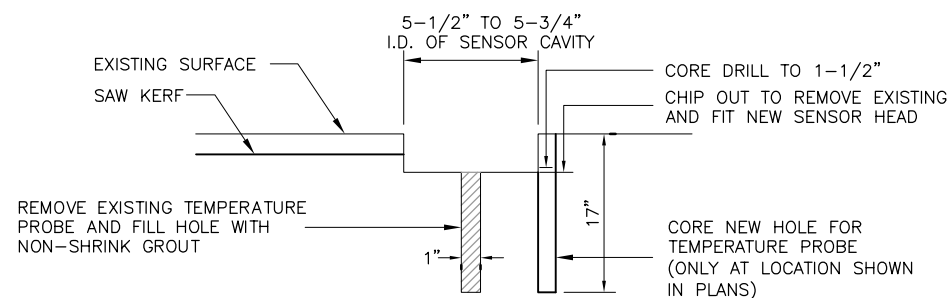
SURFACE SENSOR INSTALLATION OVERVIEW
 N.T.S.



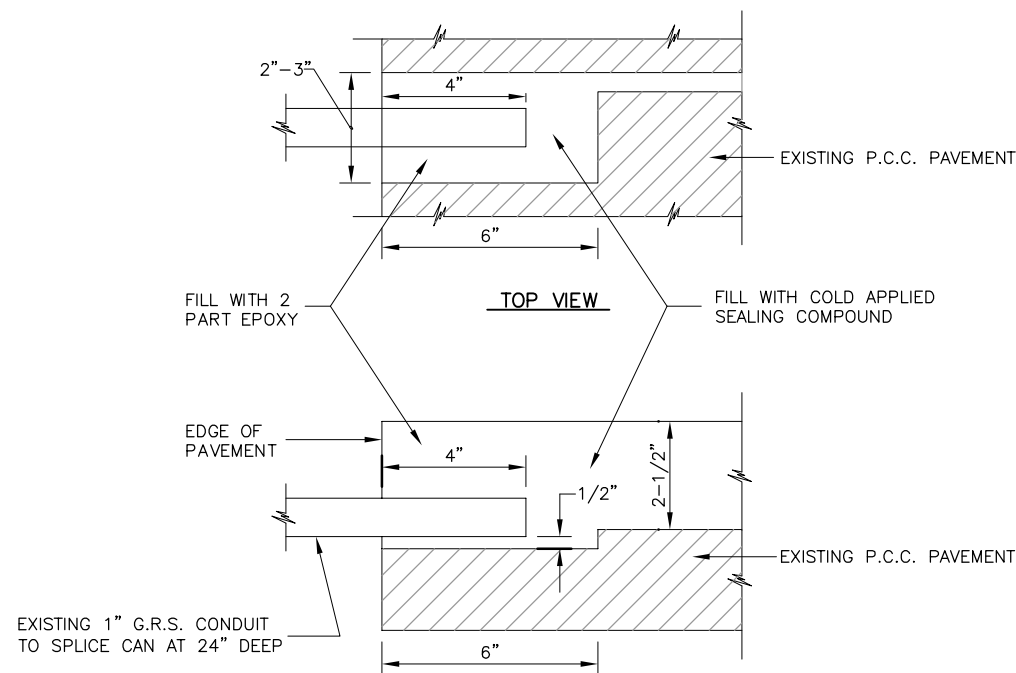
SENSOR INSTALLATION DETAIL - SIDE VIEW
 N.T.S.



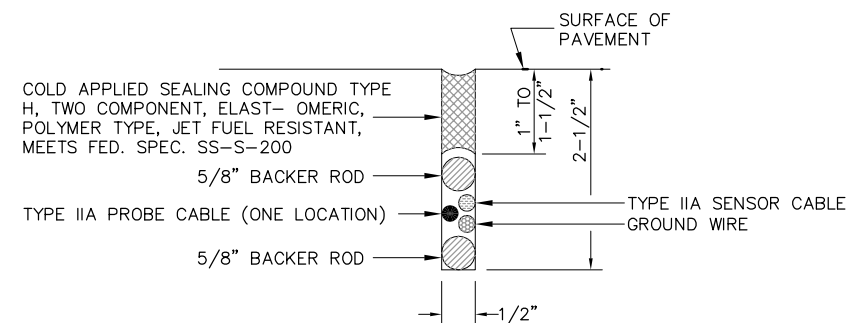
SAWING AND CORING DETAIL - TOP VIEW
 N.T.S.



SAWING AND CORING DETAIL - SIDE VIEW
 N.T.S.



SAWKERF DETAIL
 N.T.S.

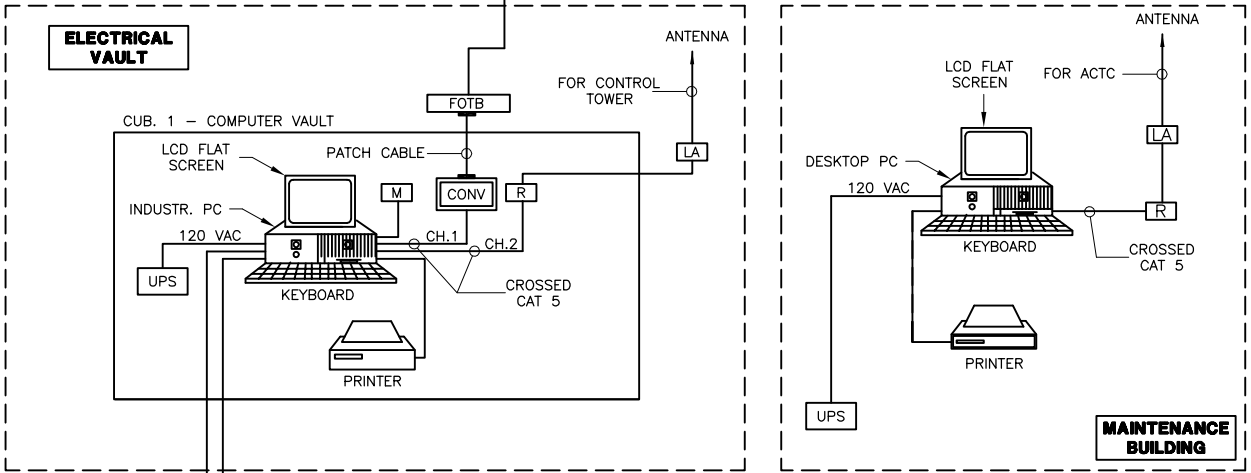
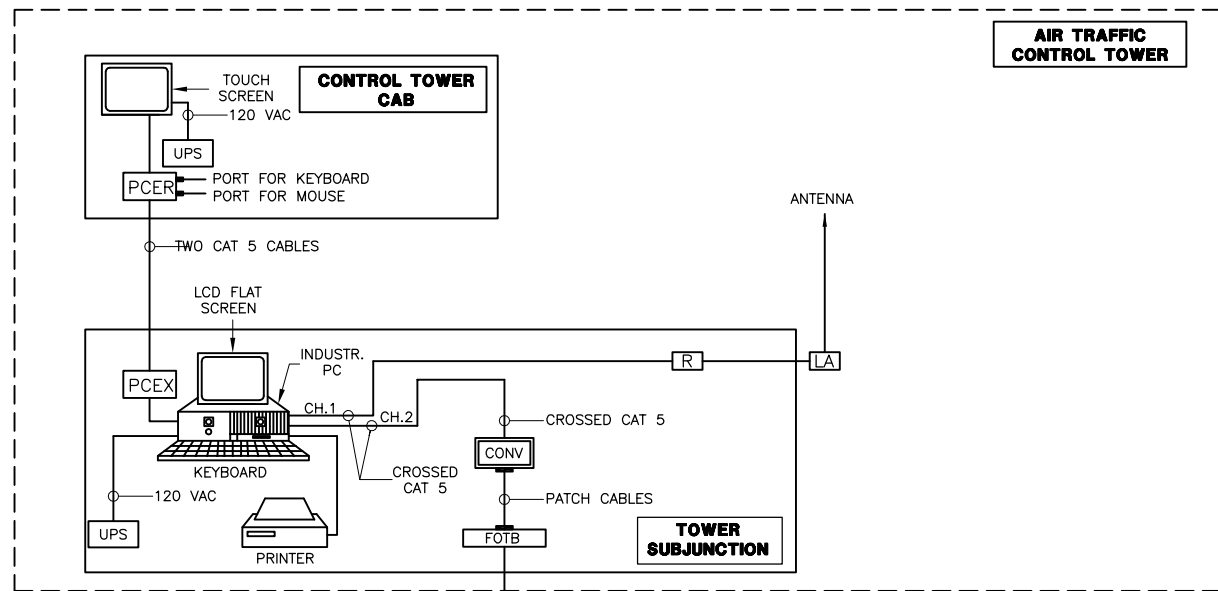


PROPOSED CABLE CROSS SECTION
 N.T.S.

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 EXTEND TAXIWAY Y
 PAVEMENT SURFACE CONDITION
 SENSOR DETAILS



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KEYED NOTES

- EXISTING RUNWAY OR TAXIWAY REGULATOR.
- EXISTING ACE® UNIT.
- EXISTING RUNWAY 13/31 & RUNWAY 04/22 REGULATOR SELECTOR CABINET.
- EXISTING L-847 SELECTOR SWITCH.
- EXISTING CONTACTOR CABINET FOR WIND CONE 31 (WC 31), WIND CONE 13 (WC 13), PAPI 13 AND REIL 13.
- EXISTING L-854 RADIO CONTROLLER.
- EXISTING 900 MHZ SPREAD SPECTRUM RADIO TRANSMITTER. TRANSMITS ON/OFF CONTROL TO ROTATING BEACON.
- EXISTING 600A AUTOMATIC TRANSFER SWITCH, 3-PHASE, 4-WIRE.
- EXISTING 200 KW STANDBY GENERATOR, 277Y/480V, 3-PHASE, 4-WIRE.
- NEW TAXIWAY Y, Y1, Y2 REGULATOR (CKT. T-9), L-828, 10 KW, 480V INPUT, 3-STEP 6.6A OUTPUT.
- NEW ACE® UNIT.
- NEW REGULATOR CONTROL AND MONITOR WIRING AS REQUIRED TO ACCOMPLISH THE FOLLOWING:
 - 3-STEP BRIGHTNESS CONTROL
 - LOSS OF INPUT POWER TO THE CCR.
 - CCR SHUTDOWN BY OPEN-CIRCUIT / OVER-CURRENT PROTECTIVE DEVICES.
 - DROP OF MORE THAN 10% IN THE CCR VA LOAD.
 - FAILURE OF THE CCR TO DELIVER THE SELECTED OUTPUT CURRENT.
 - THE NUMBER OF BURNT-OUT LAMPS IN EACH SERIES CIRCUIT.
 - REMOTE / LOCAL STATUS OF THE CCR.
 - ACTUAL CCR OUTPUT CURRENT.
 - ACTUAL CCR OUTPUT VOLTAGE.
 - ACTUAL CCR OUTPUT LOAD (WATTAGE)
 - SERIES CIRCUIT CABLE INSULATION RESISTANCE MONITOR (IRMS)

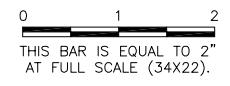
L-890 BLOCK DIAGRAM LEGEND

- ACE-1 - DISTRIBUTED CONTROL AND MONITORING INTERFACE
- # = INTERFACE #
- T - TERMINATOR
- UPS - UNINTERRUPTIBLE POWER SUPPLY
- CONV - COPPER TO FIBER CONVERSION MODULE
- FOTB - FIBER OPTIC TERMINATOR BOX
- LA - LIGHTNING ARRESTOR
- M - MODEM
- PCEX - VIDEO EXTENDER (TRANSMITTER)
- PCER - VIDEO EXTENDER (RECEIVER)
- R - RADIO

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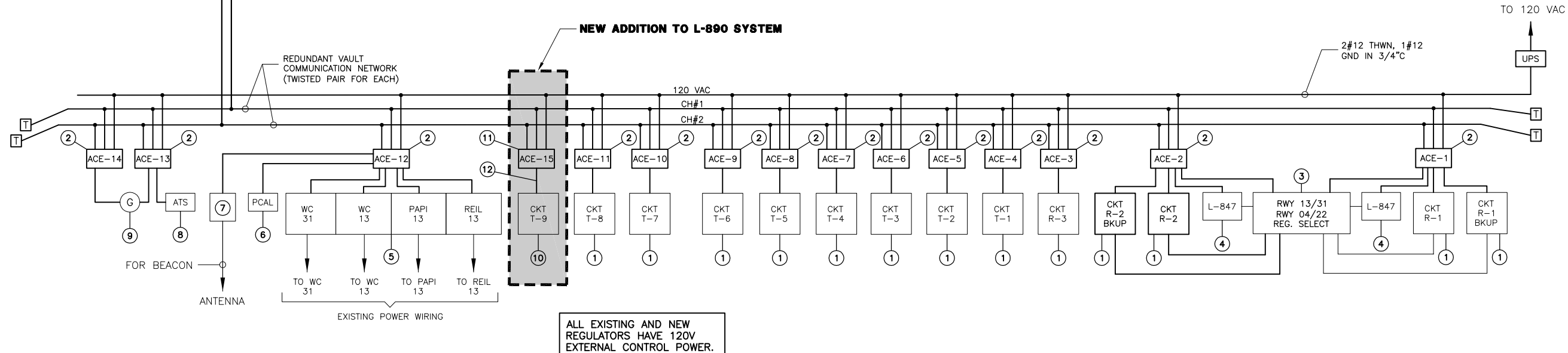
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WIRELESS MAINTENANCE COMPUTER

A WIRELESS ROAMING MAINTENANCE COMPUTER SHALL BE PROVIDED TO ALLOW FOR REMOTE CONNECTION TO THE ALCMS SYSTEM FROM A MAINTENANCE VEHICLE OR OTHER REMOTE LOCATION. THE ROAMING MAINTENANCE COMPUTER SHALL BE A NOTEBOOK COMPUTER WITH DOCKING STATION TO ALLOW THE UNIT TO BE DOCKED AND RE-CHARGED. A 2.4 GHZ RADIO AND REMOTE ANTENNA WITH MAGNET FOR TEMPORARY VEHICLE ROOF-TOP MOUNTING SHALL BE PROVIDED. THE COMPUTER SHALL PROVIDE REAL-TIME CONTROL AND MONITORING OF THE AIRFIELD LIGHTING SYSTEM WHEN IN RANGE OF THE WIRELESS COVERAGE. THE WIRELESS COMPUTER SHALL USE WIRELESS ETHERNET TO PROVIDE WIRELESS ROAMING ACCESS TO THE ALCMS SYSTEM.



EXISTING L-890 SYSTEM BLOCK DIAGRAM

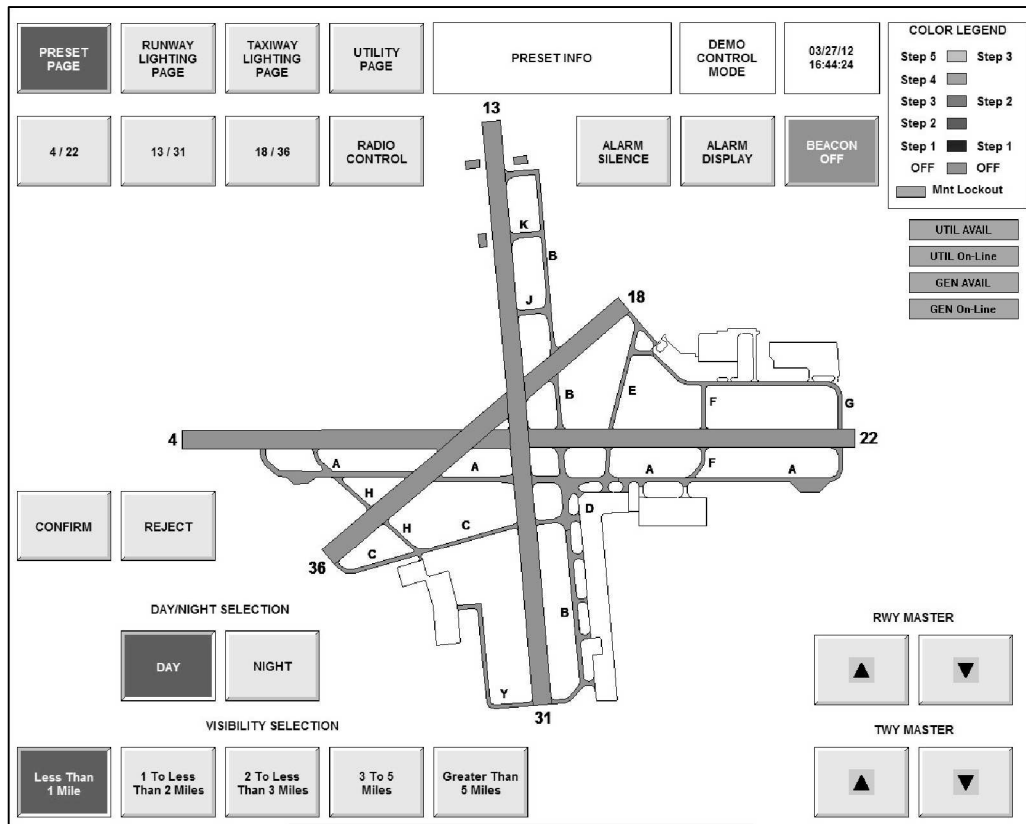
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 L-890 AIRFIELD LIGHTING CONTROL
 AND MONITORING SYSTEM (ALCMS)

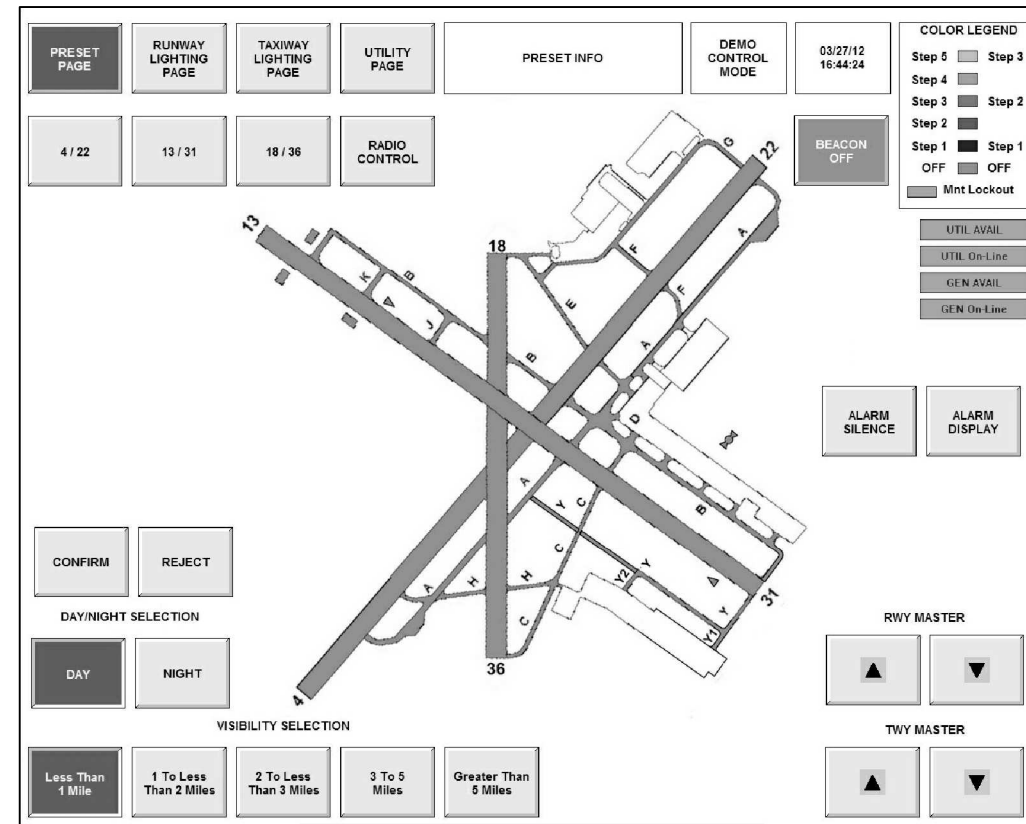
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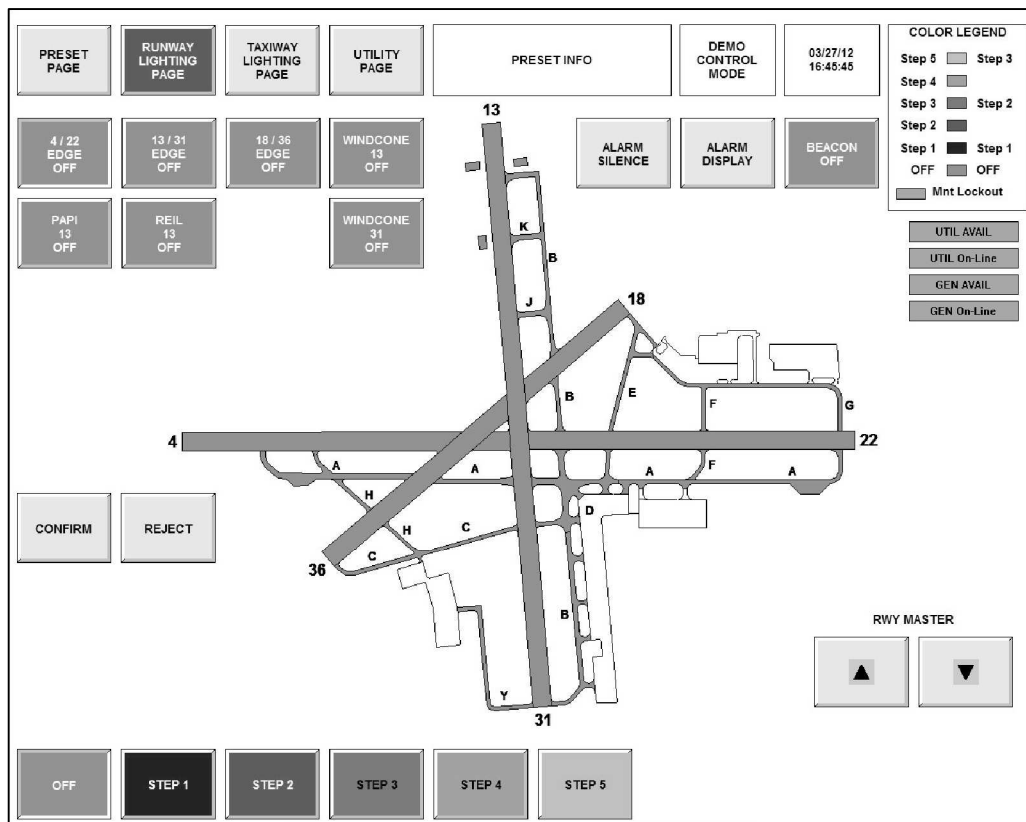
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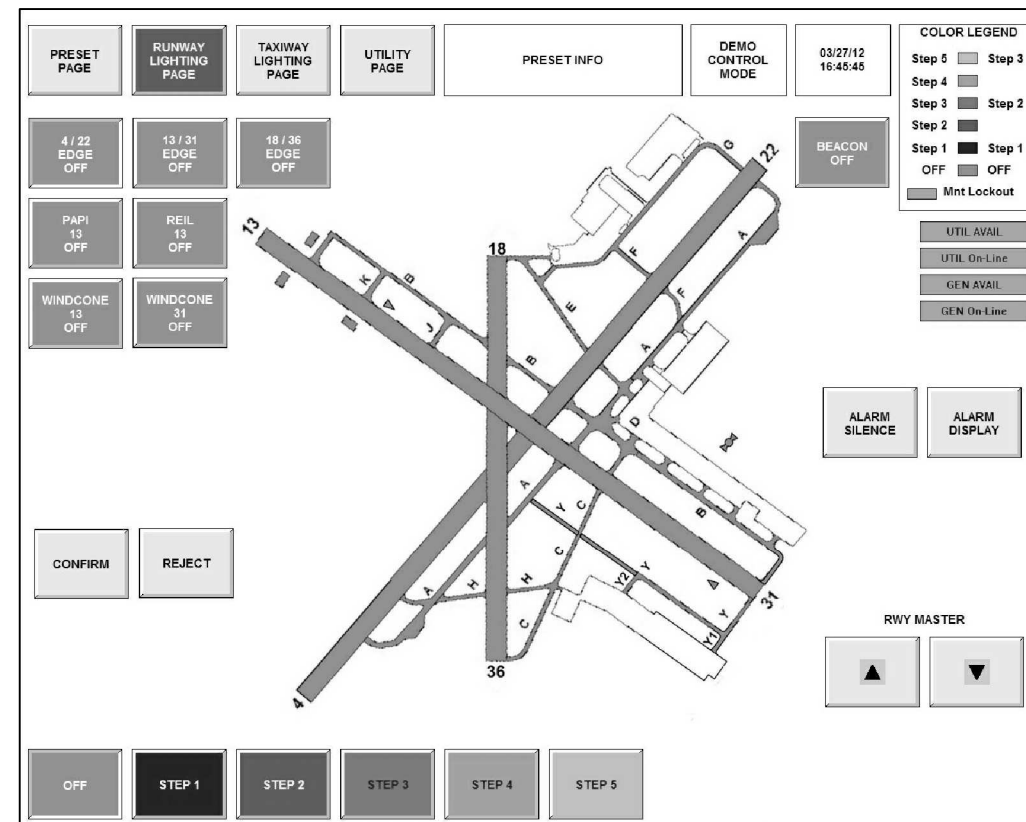
EXISTING PRESET PAGE GRAPHIC DISPLAY



REVISED PRESET PAGE GRAPHIC DISPLAY



EXISTING RUNWAY PAGE GRAPHIC DISPLAY



REVISED RUNWAY PAGE GRAPHIC DISPLAY

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



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EXTEND TAXIWAY Y

L-890 ALCMS GRAPHIC DISPLAY
 MODIFICATIONS - 1

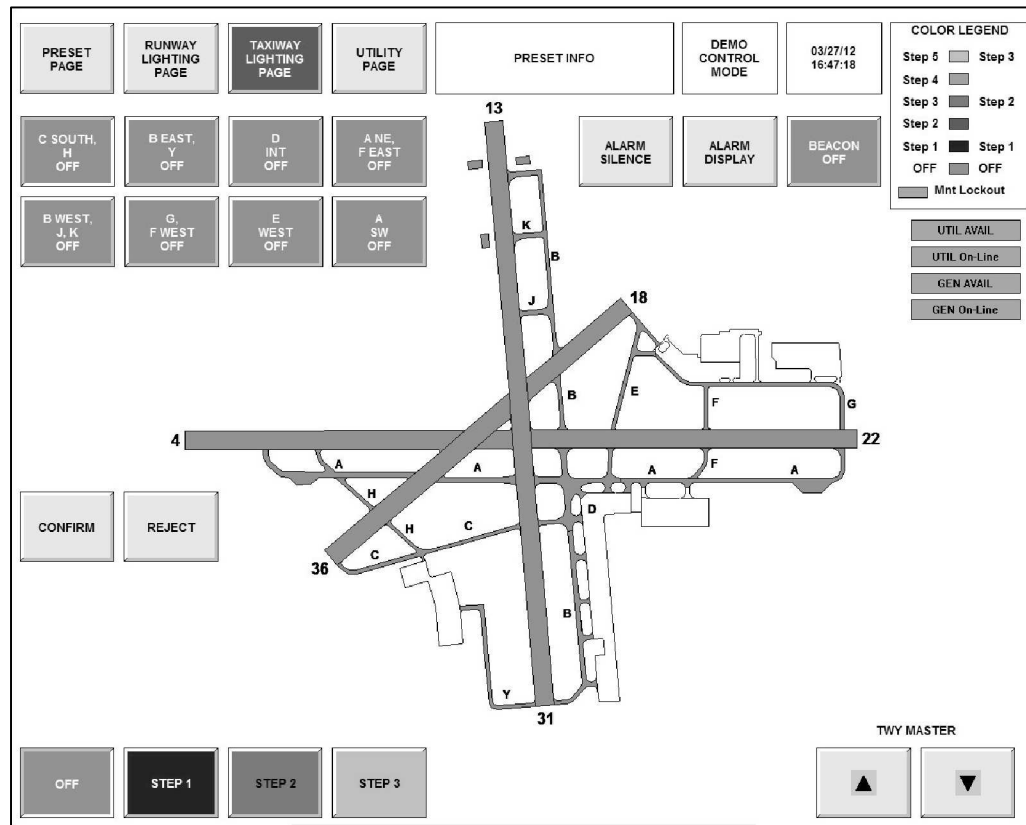
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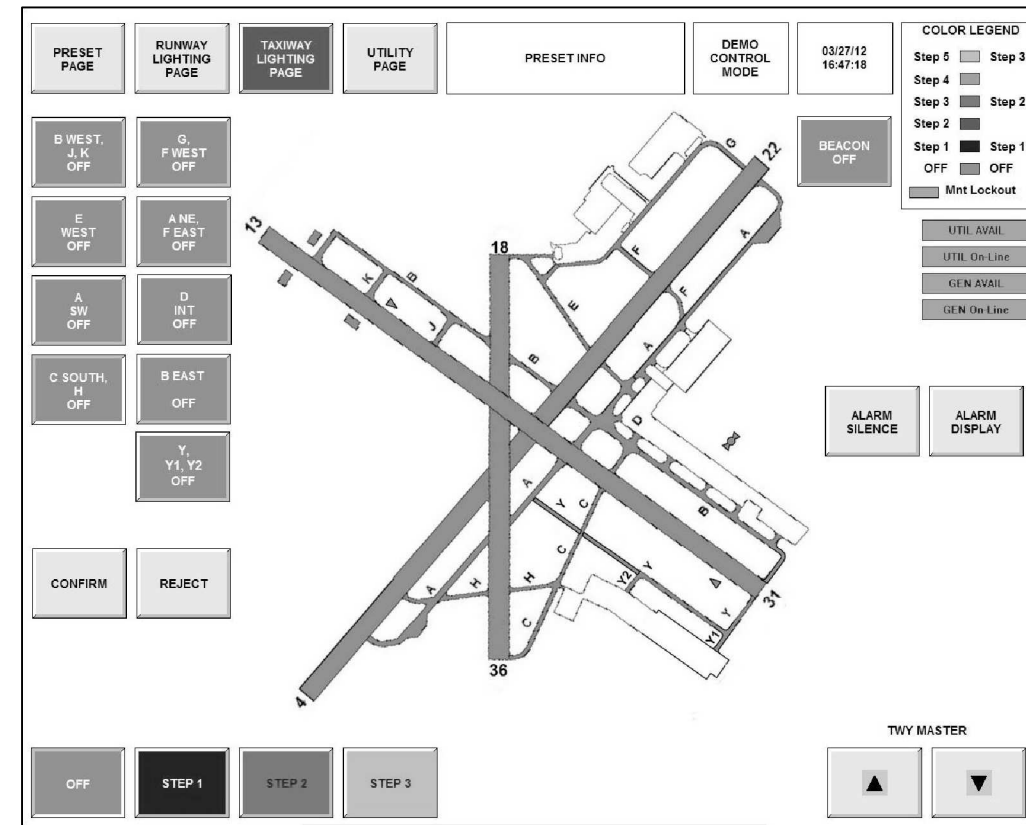
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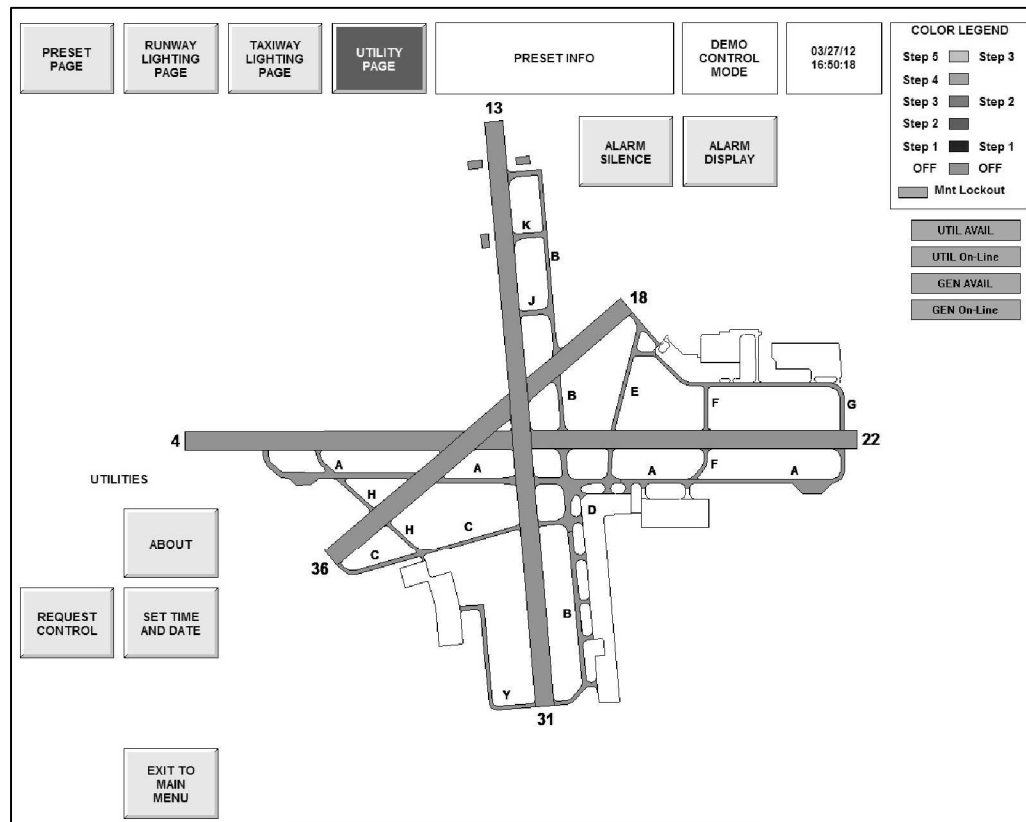
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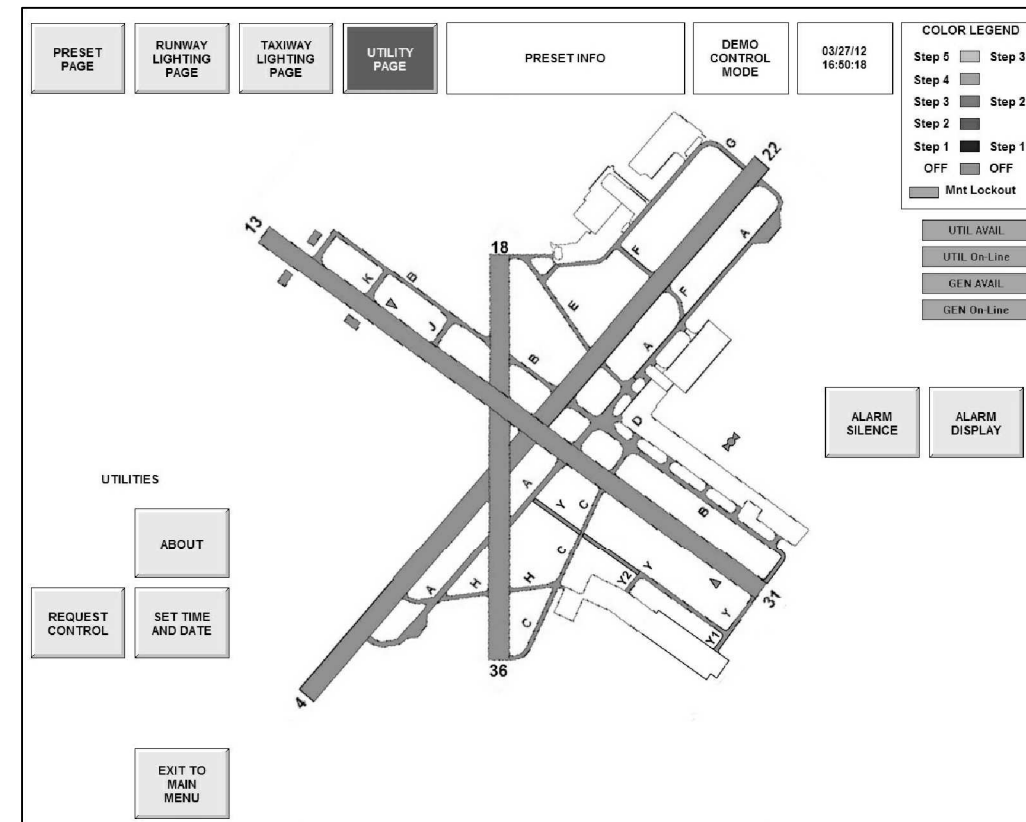
EXISTING TAXIWAY PAGE GRAPHIC DISPLAY



REVISED TAXIWAY PAGE GRAPHIC DISPLAY



EXISTING UTILITY PAGE GRAPHIC DISPLAY



REVISED UTILITY PAGE GRAPHIC DISPLAY

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0 1 2
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EXTEND TAXIWAY Y

L-890 ALCMS GRAPHIC DISPLAY
 MODIFICATIONS - 2

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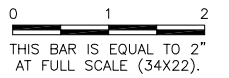
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L-890 ALCMS GRAPHIC DISPLAY
 MODIFICATIONS - 3

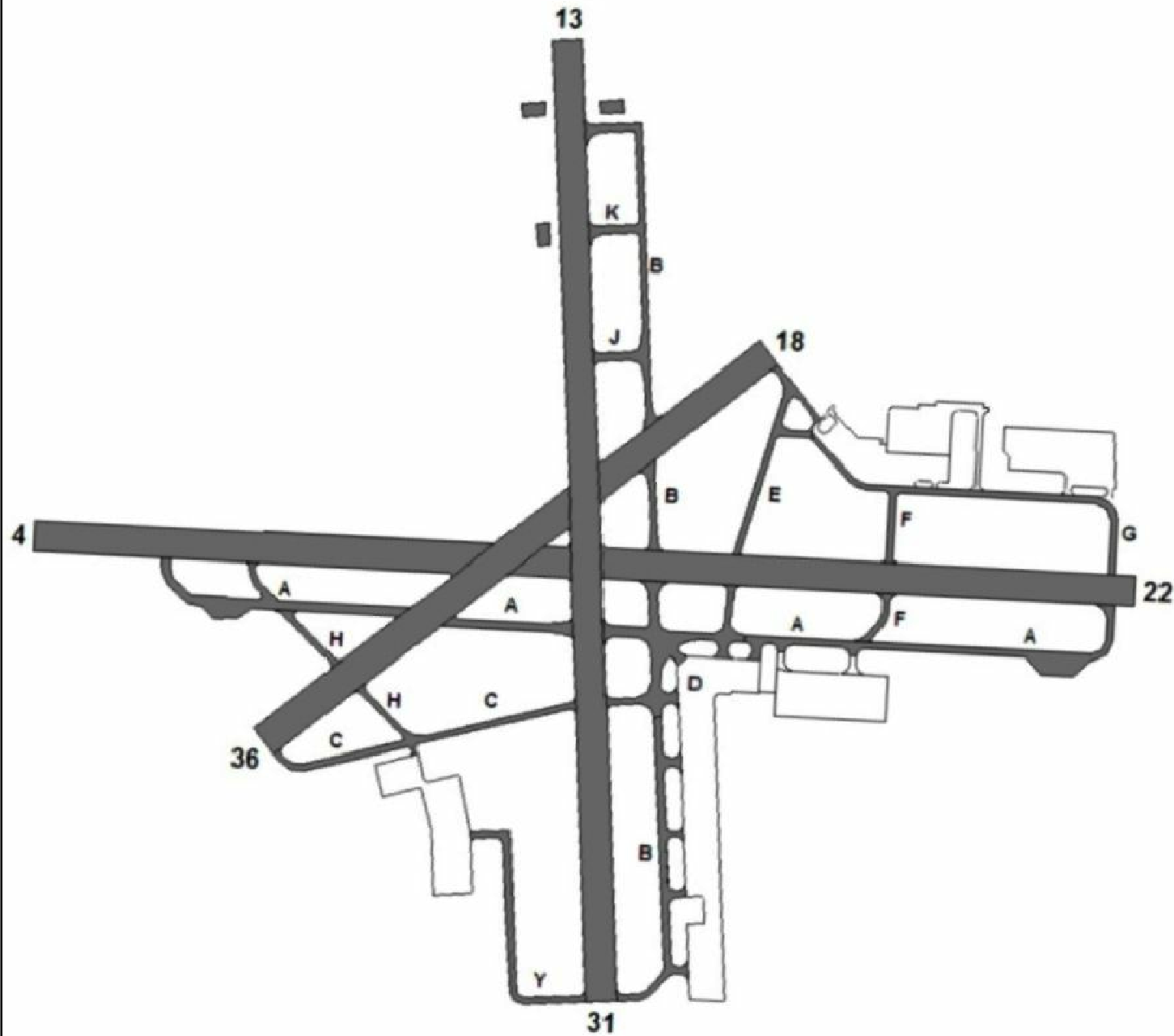
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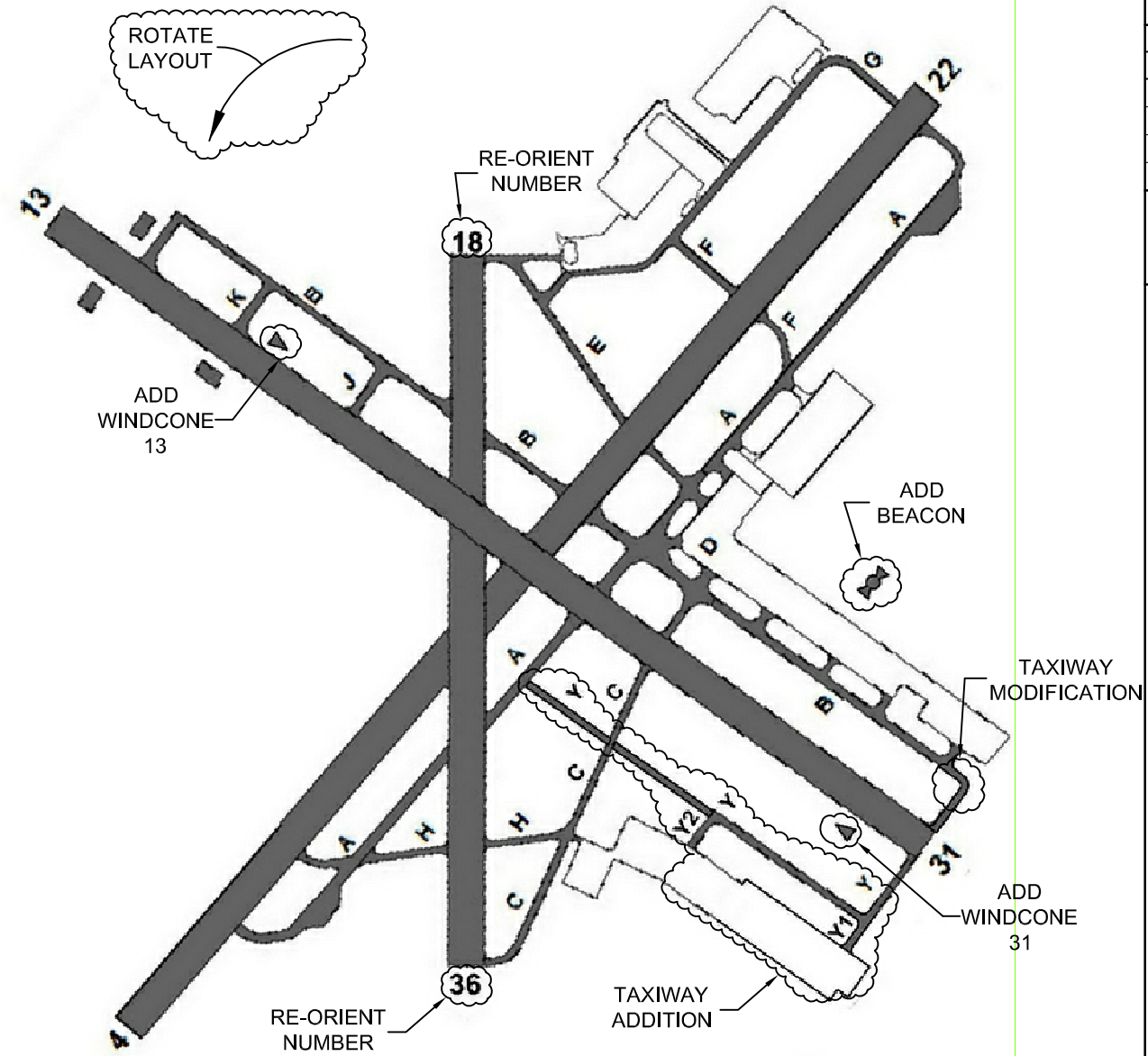
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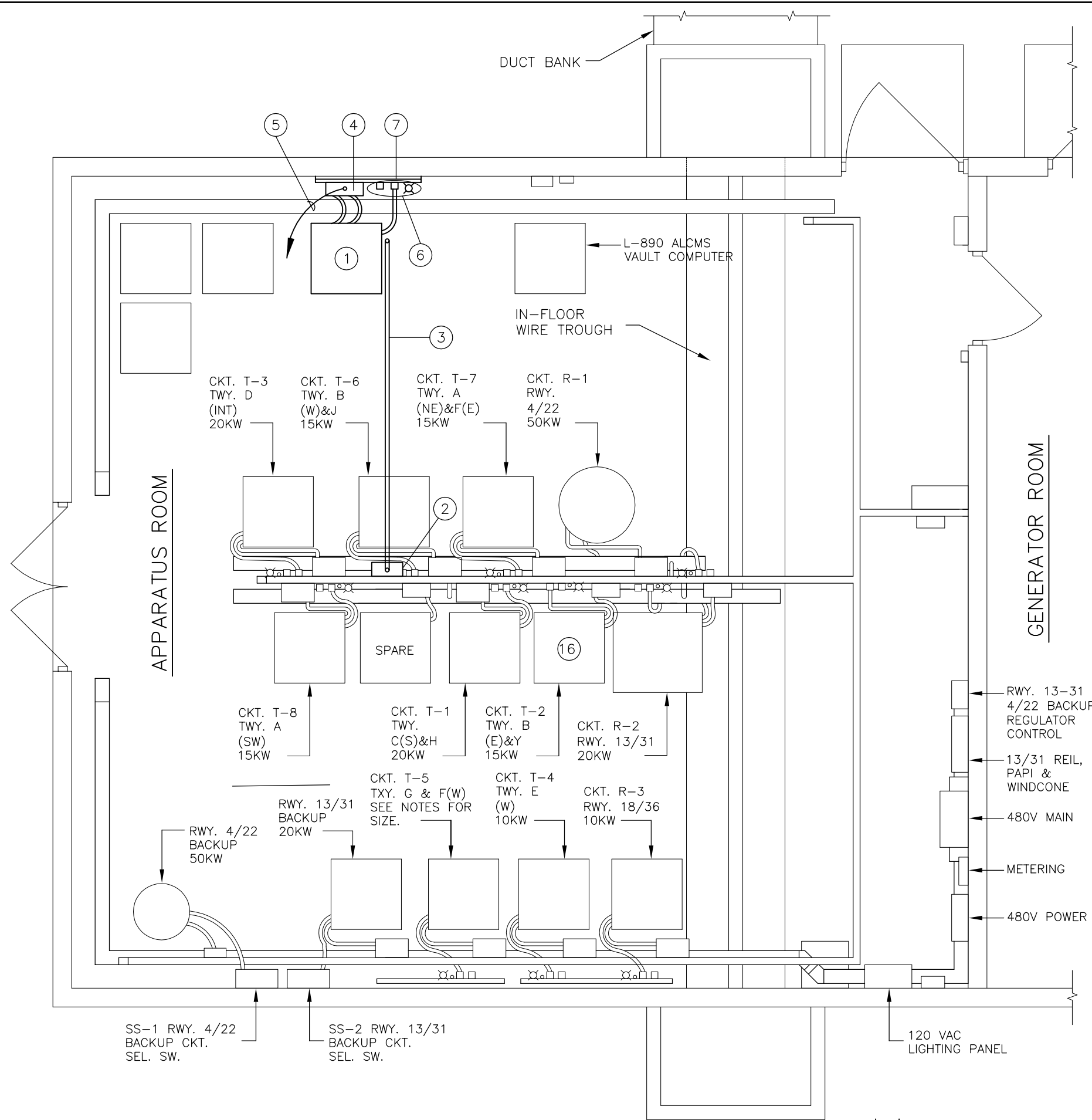
EXISTING AIRFIELD LAYOUT



REVISED AIRFIELD LAYOUT

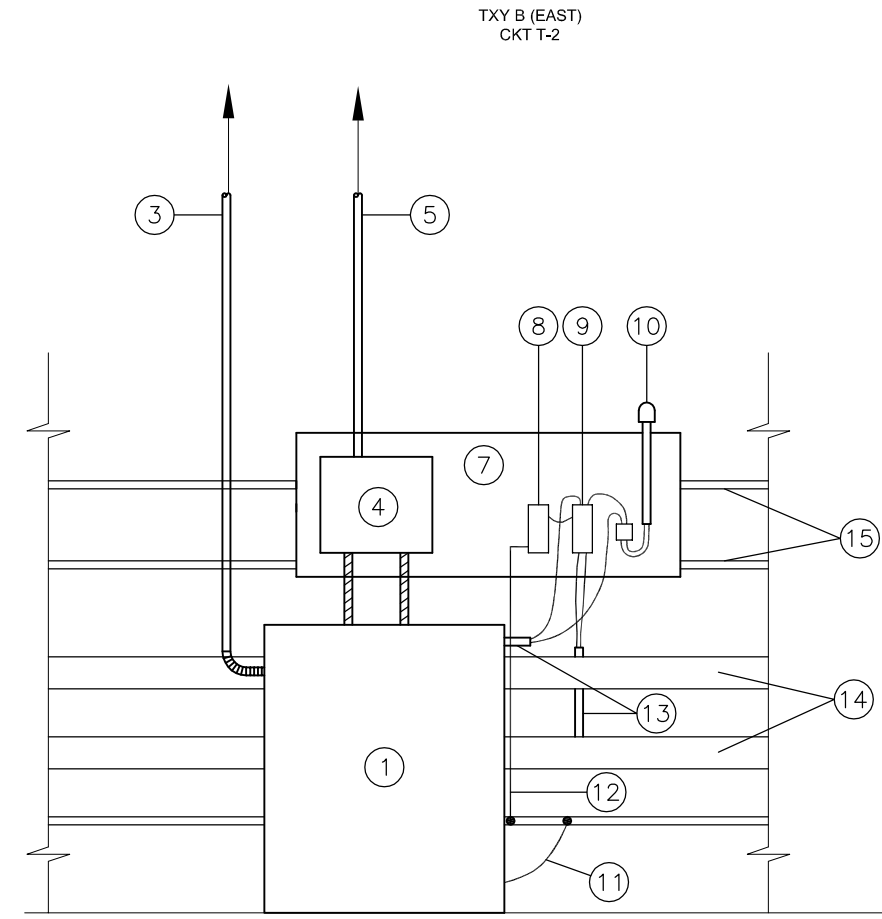
NOTES

- EXISTING GRAPHIC DISPLAYS SHOWN, AND RESULTANT MODIFIED GRAPHIC DISPLAYS ARE BASED ON BEST AVAILABLE INFORMATION.
- THE BEACON GRAPHIC, WIND CONE 13 GRAPHIC AND WIND CONE 31 GRAPHIC SHALL CHANGE STATE (COLOR) WHEN THE ASSOCIATED DEVICE IS TURNED ON.



VAULT PLAN
 1/4" = 1'-0"

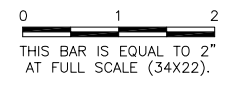
- KEYED NOTES**
- NEW TAXIWAY Y, Y1, Y2 REGULATOR (CKT. T-9), L-828, 10 KW, 480V INPUT, 3-STEP 6.6A OUTPUT. PROVIDE ENGRAVED NAMEPLATE READING:
 TXY Y
 CKT T-9
 - NEW SQUARE D 480V, 30A, 3P PLUG-IN BUSWAY FEEDER CIRCUIT BREAKER. INSTALL IN EXISTING OVERHEAD SQUARE D BUSWAY. (NOTE: ONLY TWO POLES SHALL BE USED.) PROVIDE ENGRAVED NAMEPLATE READING "TXY Y".
 - NEW TWO #10 THWN, ONE #10 GROUND IN 3/4" GRS CONDUIT TO NEW REGULATOR. CONNECTION TO REGULATOR SHALL BE VIA LIQUIDTIGHT FLEXIBLE METAL CONDUIT.
 - NEW ACE® UNIT. INSTALL AND WIRE AS DIRECTED BY ALCMS SUPPLIER.
 - NEW REDUNDANT VAULT COMMUNICATION NETWORK (TWISTED PAIR FOR EACH) AND TWO #12 THWN, ONE #12 GROUND (POWER) TO EXISTING COMMUNICATION NETWORK AND POWER AS DIRECTED BY ALCMS SUPPLIER.
 - NEW TAXIWAY EDGE LIGHT, PLUG CUTOUT AND GROUNDING DISCONNECT.
 - NEW MOUNTING PANEL, SIZED AS REQUIRED, PAINTED WHITE. ATTACH TO EXISTING STRUT-TYPE FRAMING.
 - NEW GROUNDING DISCONNECT.
 - NEW PLUG CUTOUT.
 - NEW TAXIWAY EDGE LIGHT AND L-830 ISOLATION TRANSFORMER.
 - NEW #6 GROUND WIRE CLAMPED TO EXISTING GROUND BUS.
 - NEW #8, L-824, 5 KV, TYPE C CABLE, CLAMPED TO EXISTING GROUND BUS.
 - NEW #8, L-824, 5 KV, TYPE C CABLES IN LIQUIDTIGHT FLEXIBLE METAL CONDUIT.
 - EXISTING HIGH VOLTAGE (SERIES CIRCUIT) AND LOW VOLTAGE WIREWAYS.
 - EXISTING STRUT-TYPE FRAMING.
 - REMOVE EXISTING NAME PLATE AND REPLACE WITH NEW ENGRAVED NAMEPLATE READING:
 TXY B (EAST)
 CKT T-2



TAXIWAY Y REGULATOR ELEVATION
 1/2" = 1'-0"

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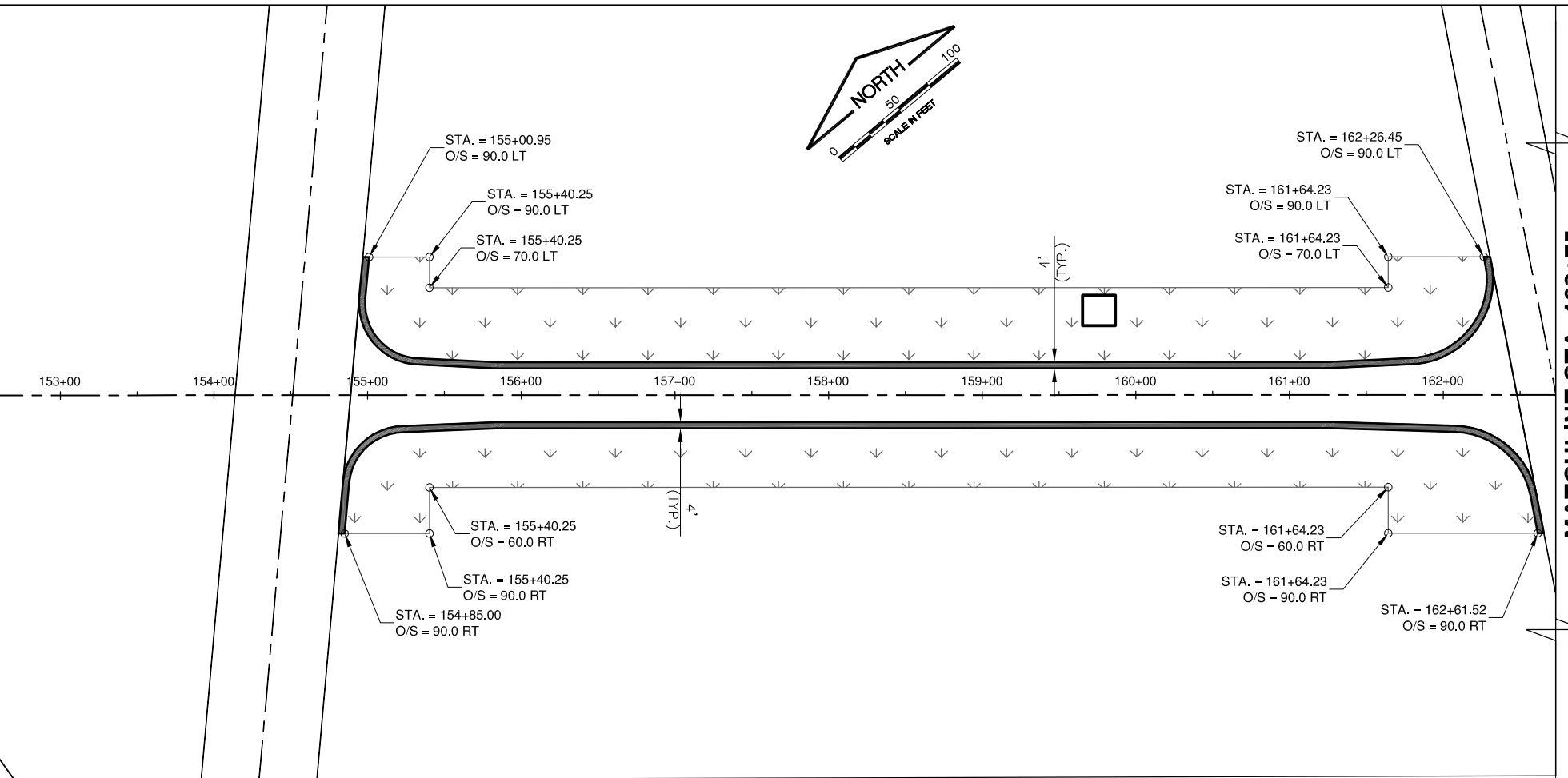
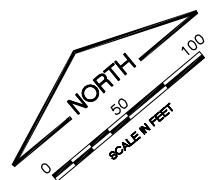
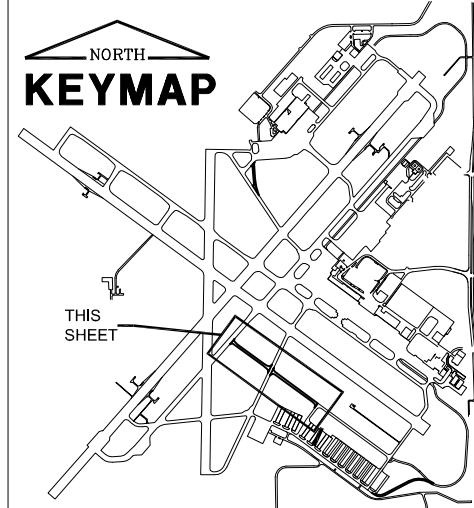
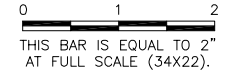
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**EXTEND TAXIWAY Y
 VAULT IMPROVEMENTS**

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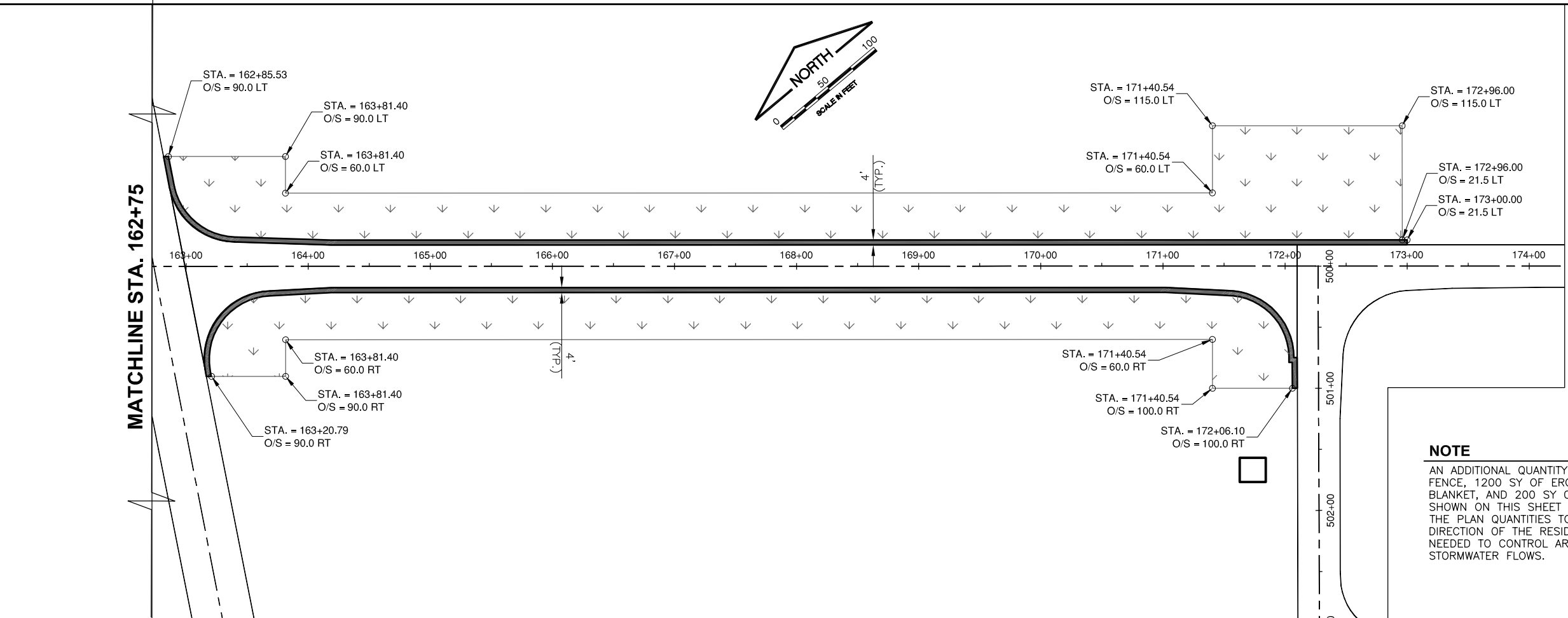
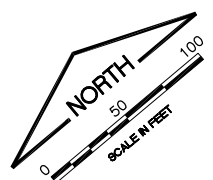
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LEGEND

- NEW SOD AND SHOULDER ADJUSTMENT LIMITS
- NEW SEEDING AND MULCHING LIMITS
- INLET PROTECTION



NOTE
 AN ADDITIONAL QUANTITY OF 1000 LF OF SILT FENCE, 1200 SY OF EROSION CONTROL BLANKET, AND 200 SY OF SOD WHICH ARE NOT SHOWN ON THIS SHEET HAVE BEEN ADDED TO THE PLAN QUANTITIES TO BE PLACED AT THE DIRECTION OF THE RESIDENT ENGINEER AS NEEDED TO CONTROL AREAS OF CONCENTRATED STORMWATER FLOWS.



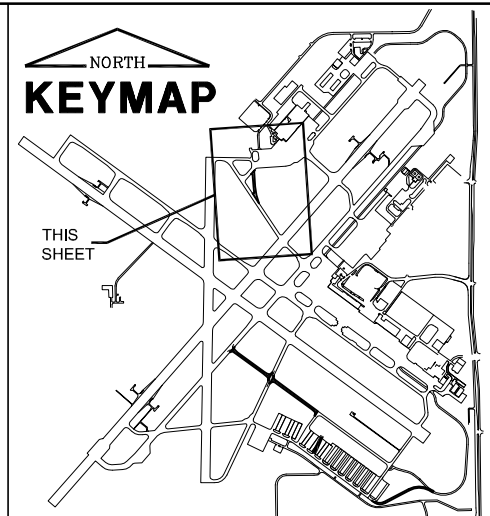
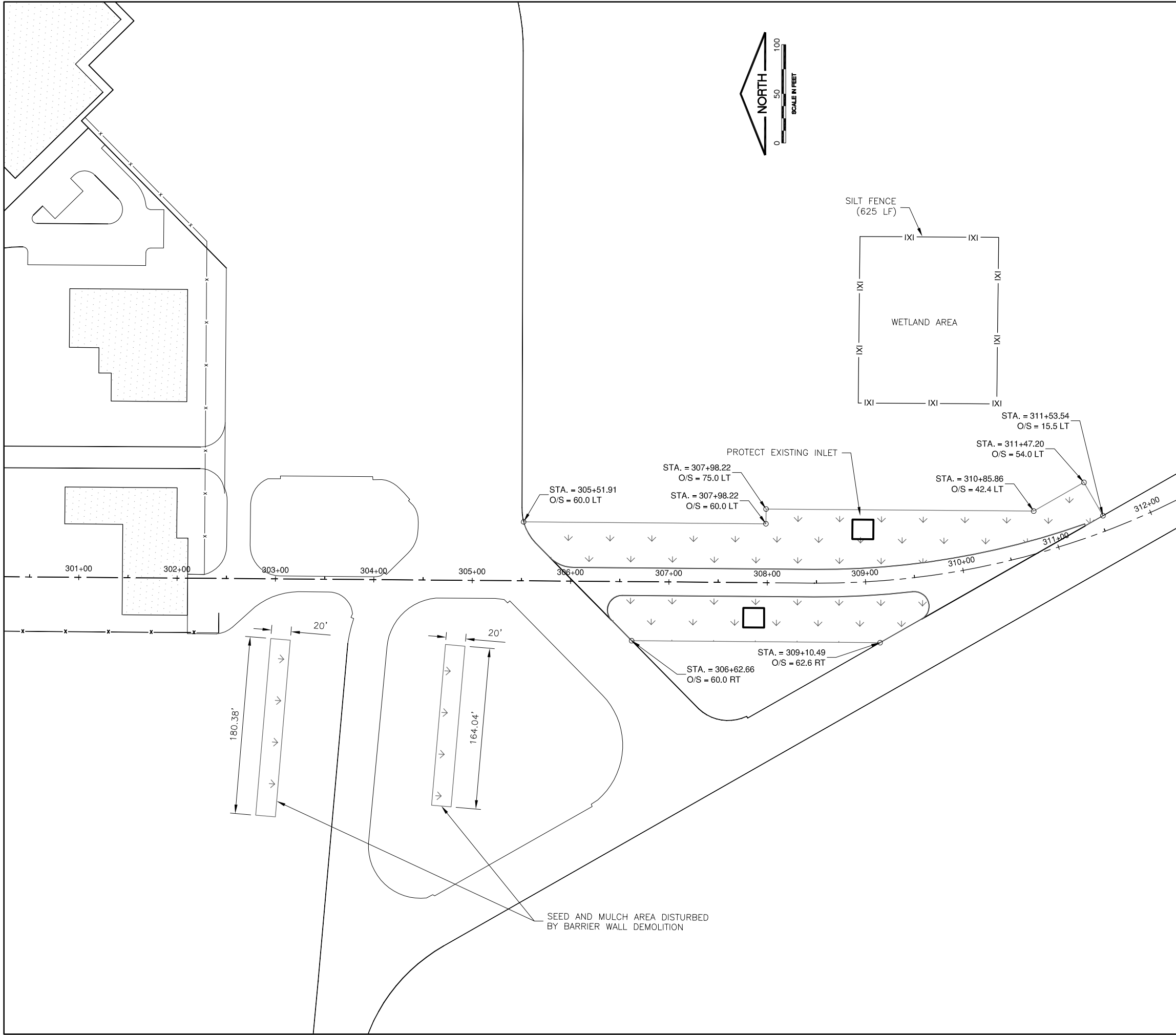
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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

**EXTEND TAXIWAY Y
 TURFING & EROSION CONTROL PLAN 1**

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| SPI-4156 3-17-0096-XX | |
| SHEET 75 OF 106 SHEETS | |

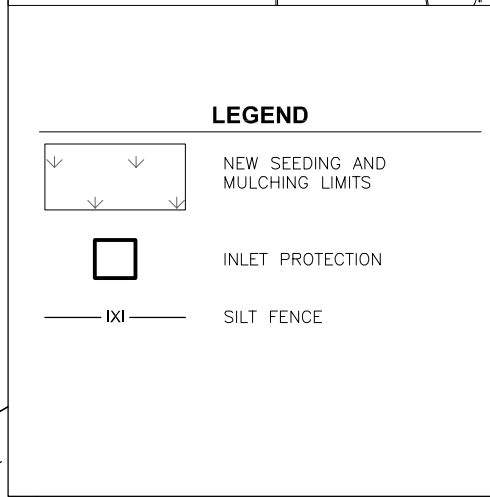


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KEYMAP
BASE_PROP_GEO
SPI-BASE CURRENT GEOMETRY
BASE_PROP_TURF

CA014

| REVISIONS | | |
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0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



NOTE

- SILT FENCE SHALL PROTECT THE WETLAND AREA. THIS AREA SHALL NOT BE DISTURBED AT ANY POINT BEFORE, DURING, AND AFTER CONSTRUCTION; OR DURING THE INSTALLATION OF SILT FENCE.

SPRINGFIELD AIRPORT AUTHORITY
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SPRINGFIELD, ILLINOIS

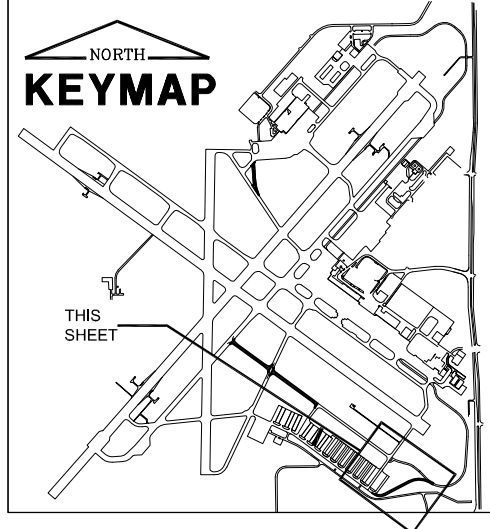
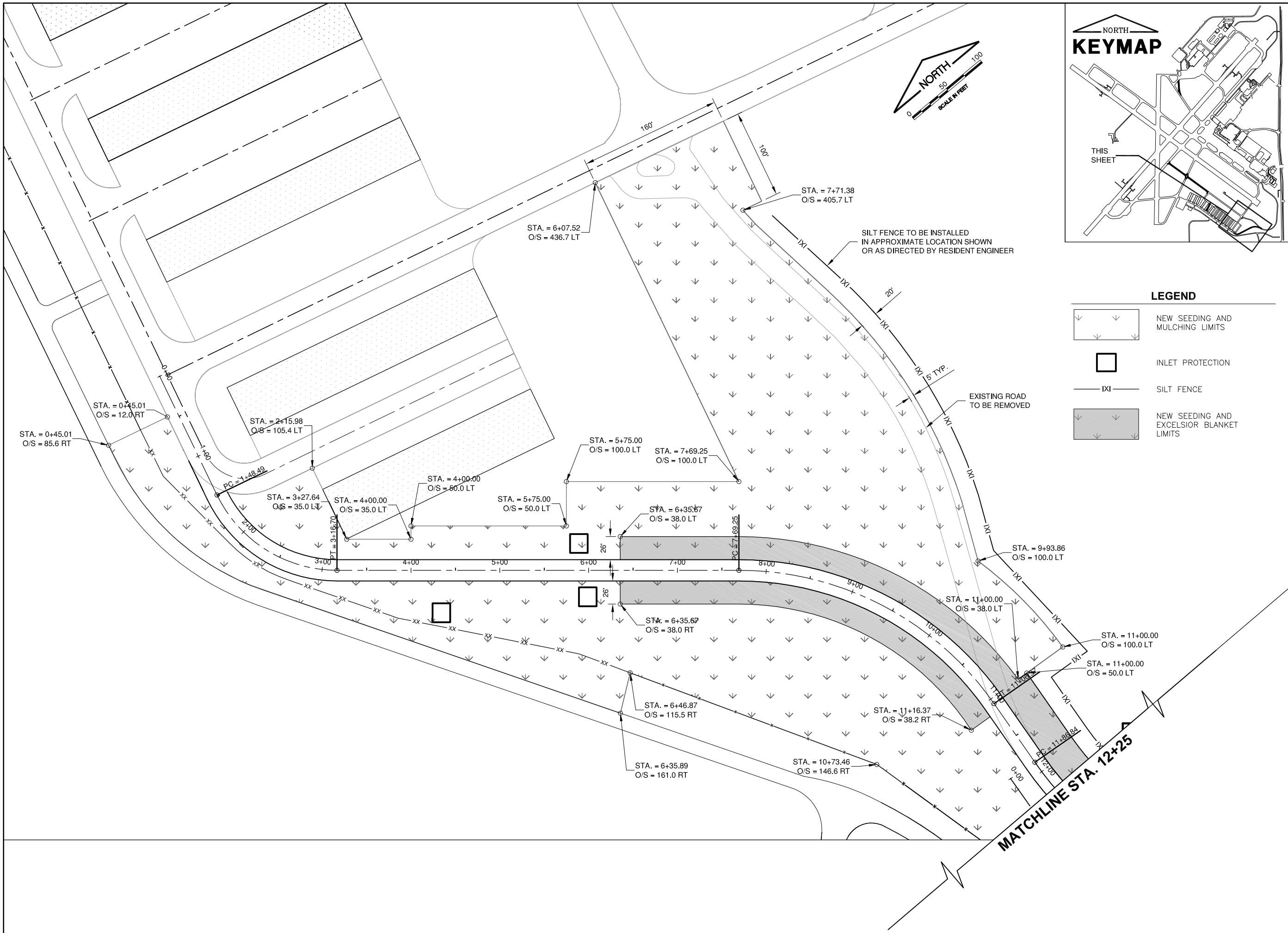
EXTEND TAXIWAY Y

TURFING & EROSION CONTROL PLAN 2

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| SHEET 76 OF 106 SHEETS | |



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 SPI-BASE CURRENT GEOMETRY
 SPL_UH
 KEYMAP
 BASE_PROP_GEO
 BASE_PROP_TURF

CA014

| REVISIONS | | |
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| NUMBER | BY | DATE |
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| | | |

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

LEGEND

| | |
|--|--|
| | NEW SEEDING AND MULCHING LIMITS |
| | INLET PROTECTION |
| | SILT FENCE |
| | NEW SEEDING AND EXCELSIOR BLANKET LIMITS |

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y

TURFING & EROSION CONTROL PLAN 3



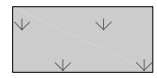
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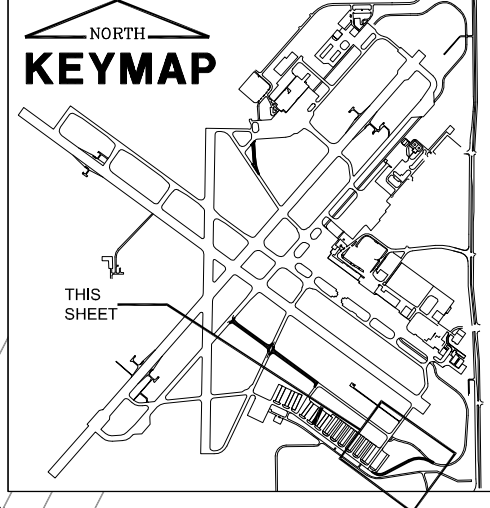
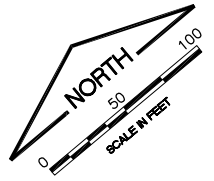
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| JOB No: | 110350400 |
| SHEET 77 OF 106 SHEETS | |

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LEGEND

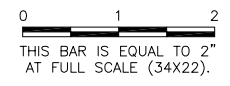
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-  INLET PROTECTION
-  NEW SEEDING AND EXCELSIOR BLANKET LIMITS



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 SPL_UH
 KEYMAP
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 BASE_PROP_TURF

CA014

| REVISIONS | | |
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| NUMBER | BY | DATE |
| | | |
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MATCHLINE STA. 12+25

- STA. = 12+78.47
O/S = 50.3 LT
- STA. = 12+75.36
O/S = 12.4 LT
- STA. = 1+44.00
O/S = 35.0 LT
- STA. = 2+10.97
O/S = 35.0 LT
- STA. = 2+50.00
O/S = 35.0 LT
- STA. = 2+50.00
O/S = 12.5 LT
- STA. = 1+24.52
O/S = 38.2 RT
- STA. = 1+43.02
O/S = 35.5 RT
- STA. = 2+13.93
O/S = 35.1 RT
- STA. = 2+50.00
O/S = 35.0 RT
- STA. = 2+50.00
O/S = 12.5 RT

NEW SEEDING AND MULCHING LIMITS
 NEW SEEDING AND EXCELSIOR BLANKET LIMITS

NEW PAVED DITCH

END SEEDING AT EXISTING FENCE

PLACE NEW SILT FENCE ALONG LIMITS OF DISTURBANCE AT EXISTING FENCE

Tower
31.8

Tower
40.9



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
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EXTEND TAXIWAY Y
TURFING & EROSION CONTROL PLAN 4

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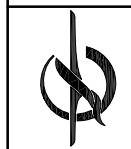
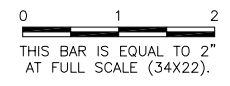


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| NUMBER | BY | DATE |
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SPRINGFIELD AIRPORT AUTHORITY
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 SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y

EROSION CONTROL DETAILS

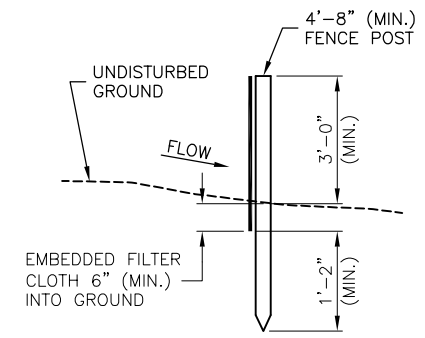
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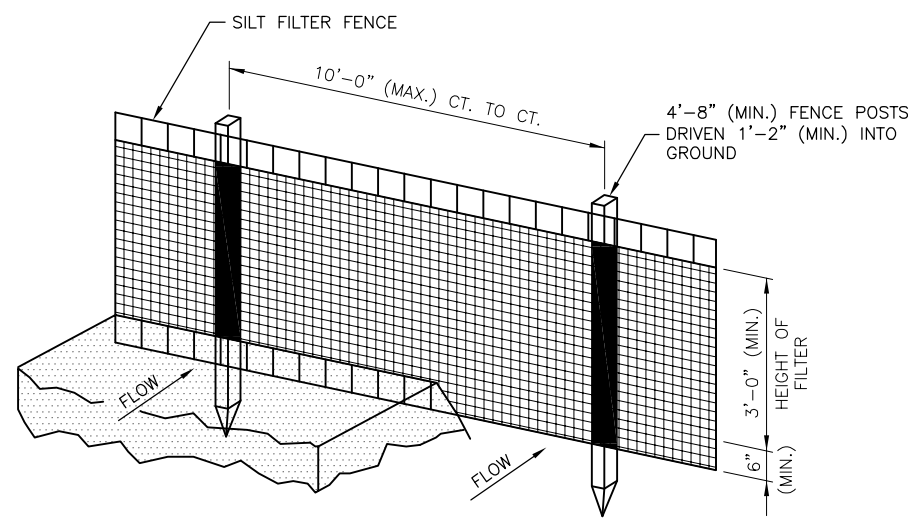


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SECTION



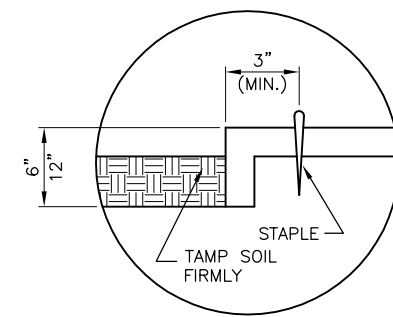
PERSPECTIVE VIEW

EROSION CONTROL FABRIC FENCE DETAILS

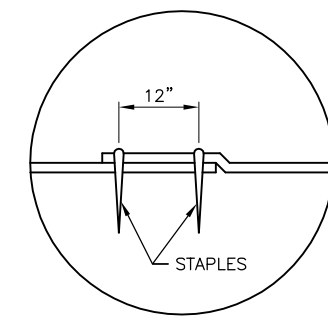
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EROSION CONTROL FABRIC FENCE NOTES

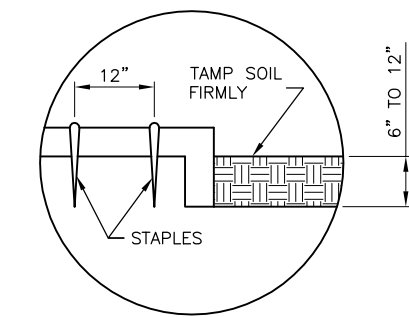
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



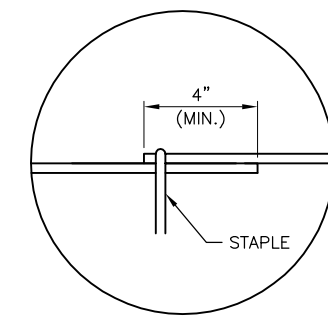
DETAIL 1 - TERMINAL FOLD



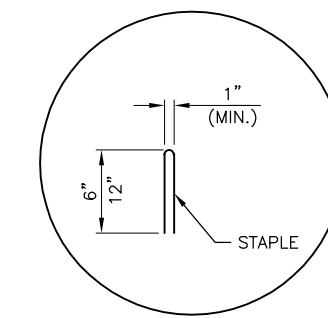
DETAIL 2 - JUNCTION SLOT



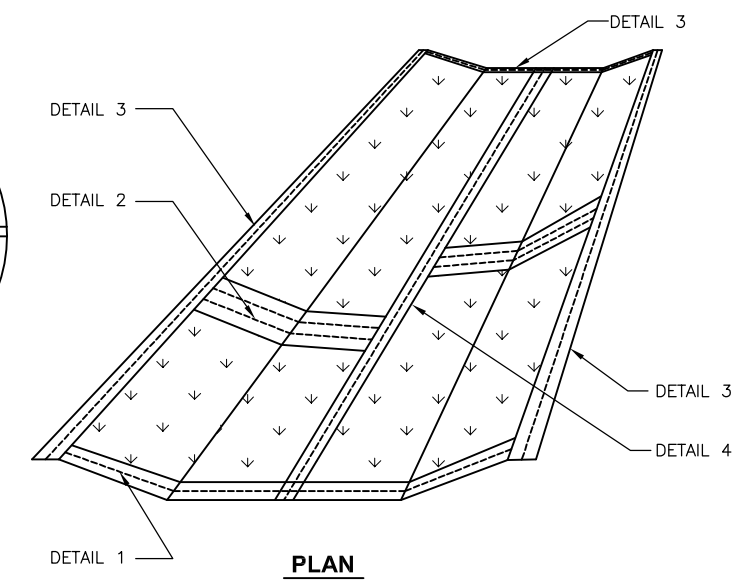
DETAIL 3 - ANCHOR SLOT



DETAIL 4 - LAP JOINT



DETAIL 5 - STAPLE DETAIL



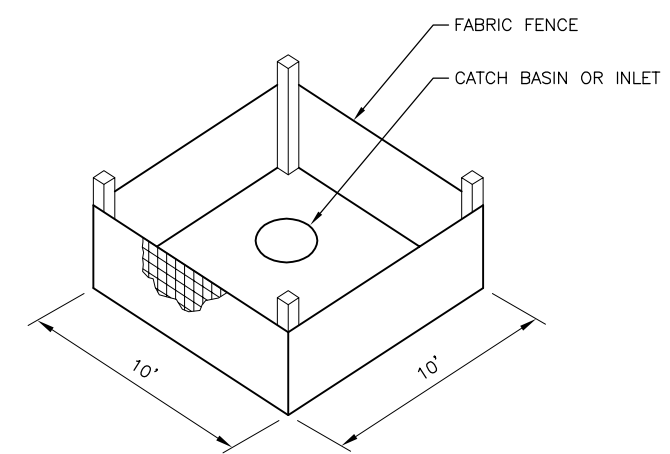
PLAN

EXCELSIOR BLANKET DETAILS

N.T.S.

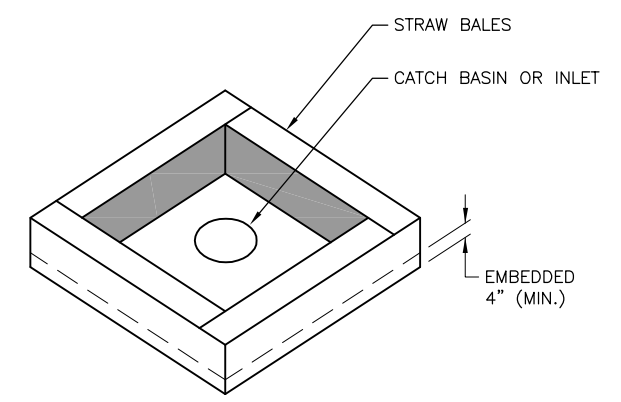
EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINALS ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



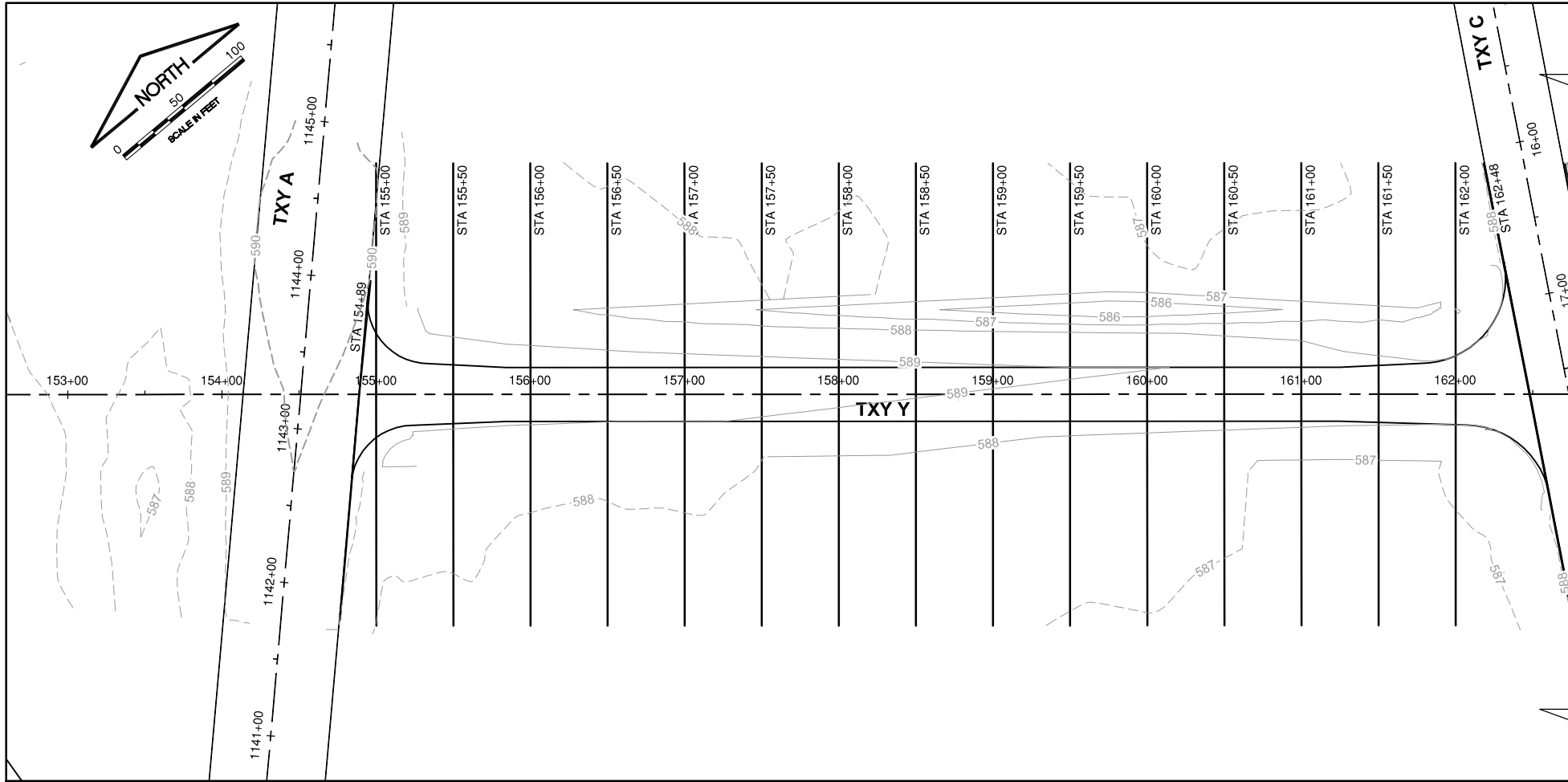
INLET PROTECTION WITH FABRIC

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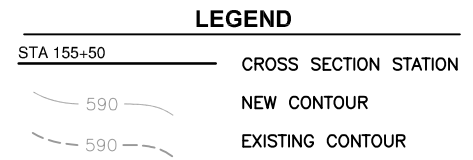
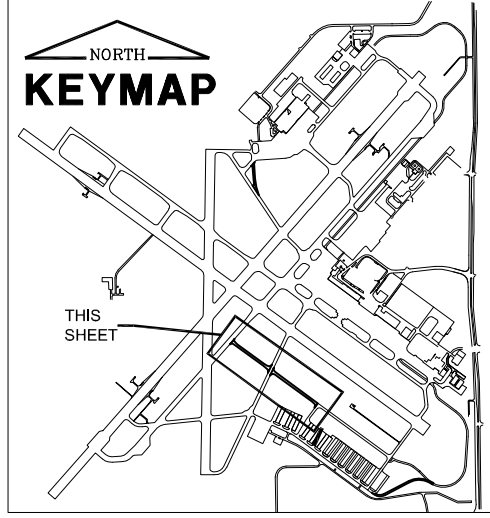


INLET PROTECTION WITH STRAW BALES

N.T.S.



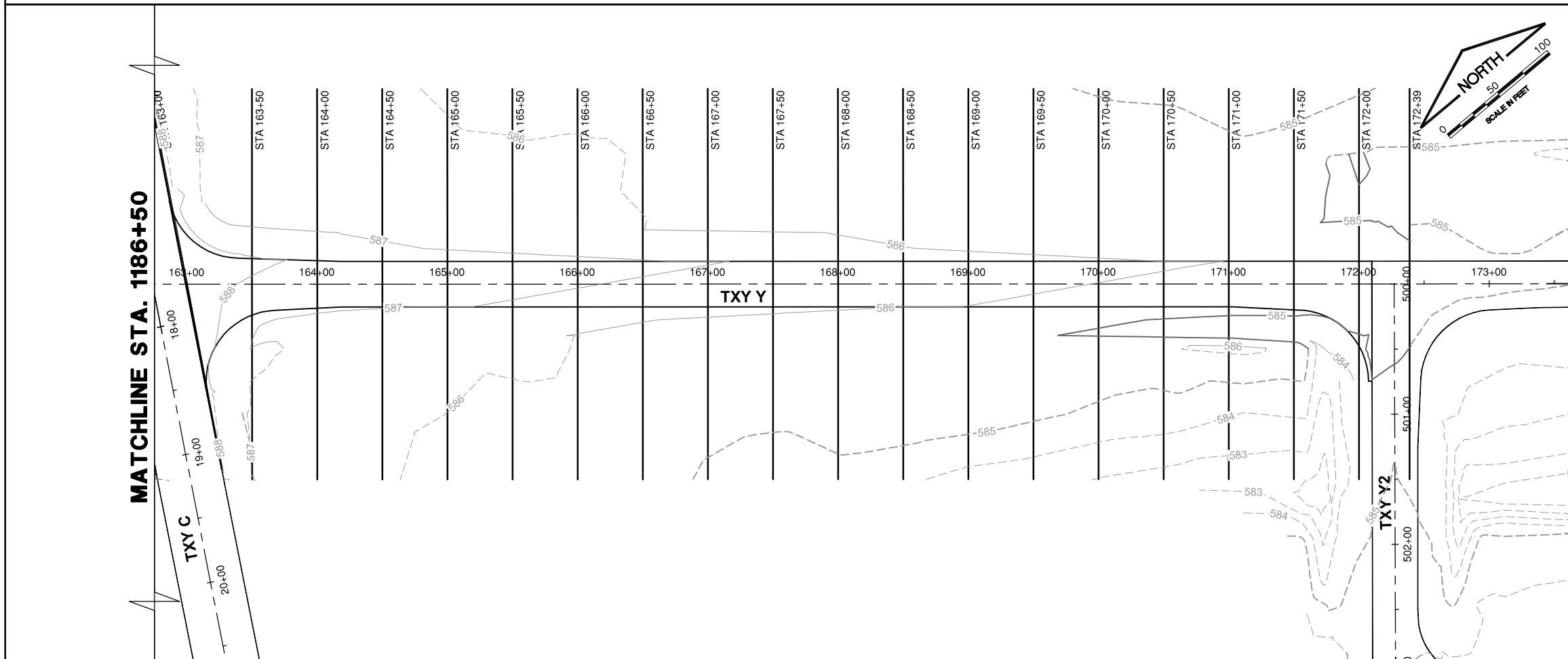
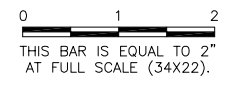
MATCHLINE STA. 162+75



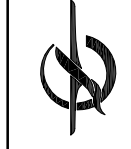
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MATCHLINE STA. 1186+50

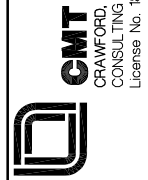


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 SPRINGFIELD, ILLINOIS**

EXTEND TAXIWAY Y

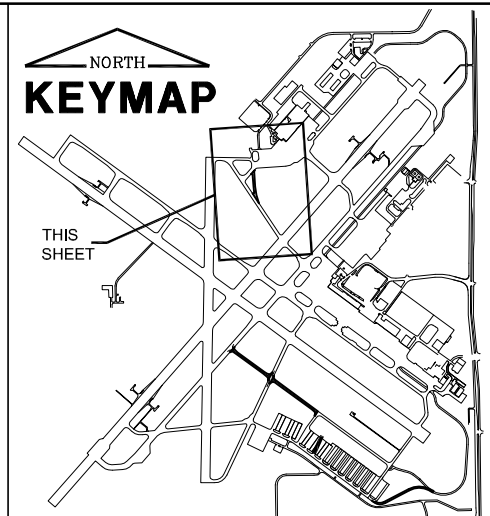
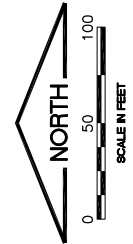
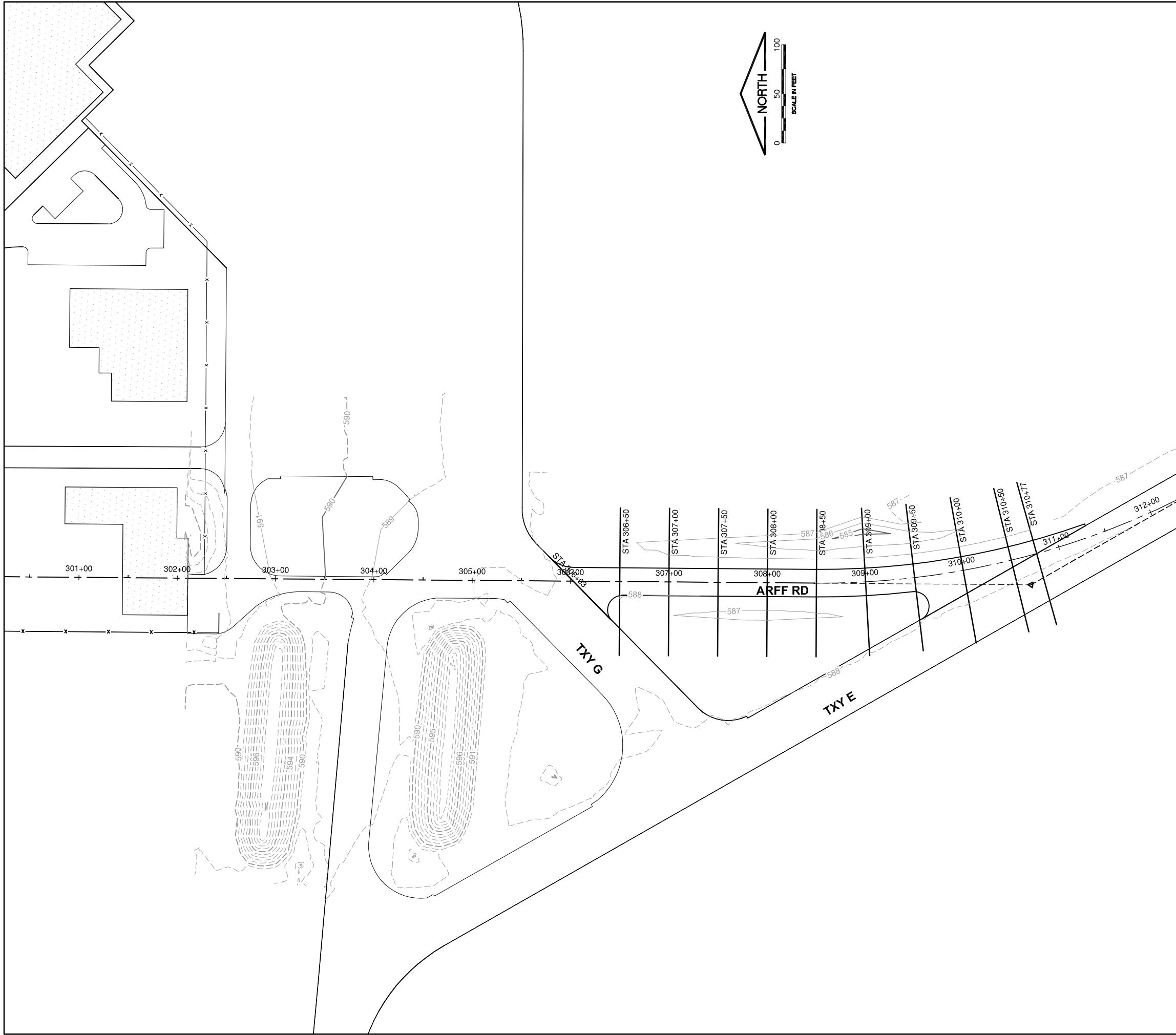
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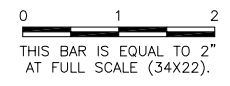
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LEGEND

- STA 155+50 — CROSS SECTION STATION
- 590 — NEW CONTOUR
- - - 590 — EXISTING CONTOUR



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y
INDEX TO CROSS SECTIONS 2

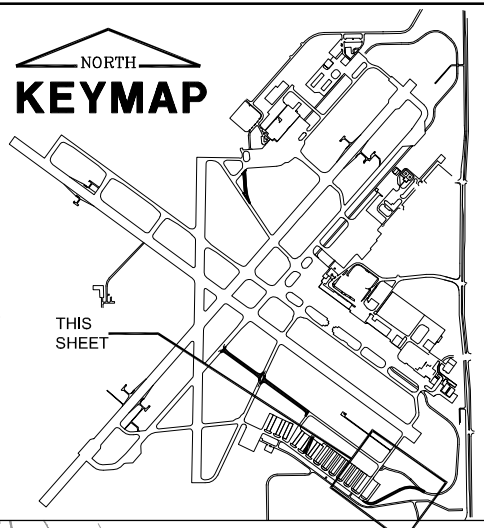
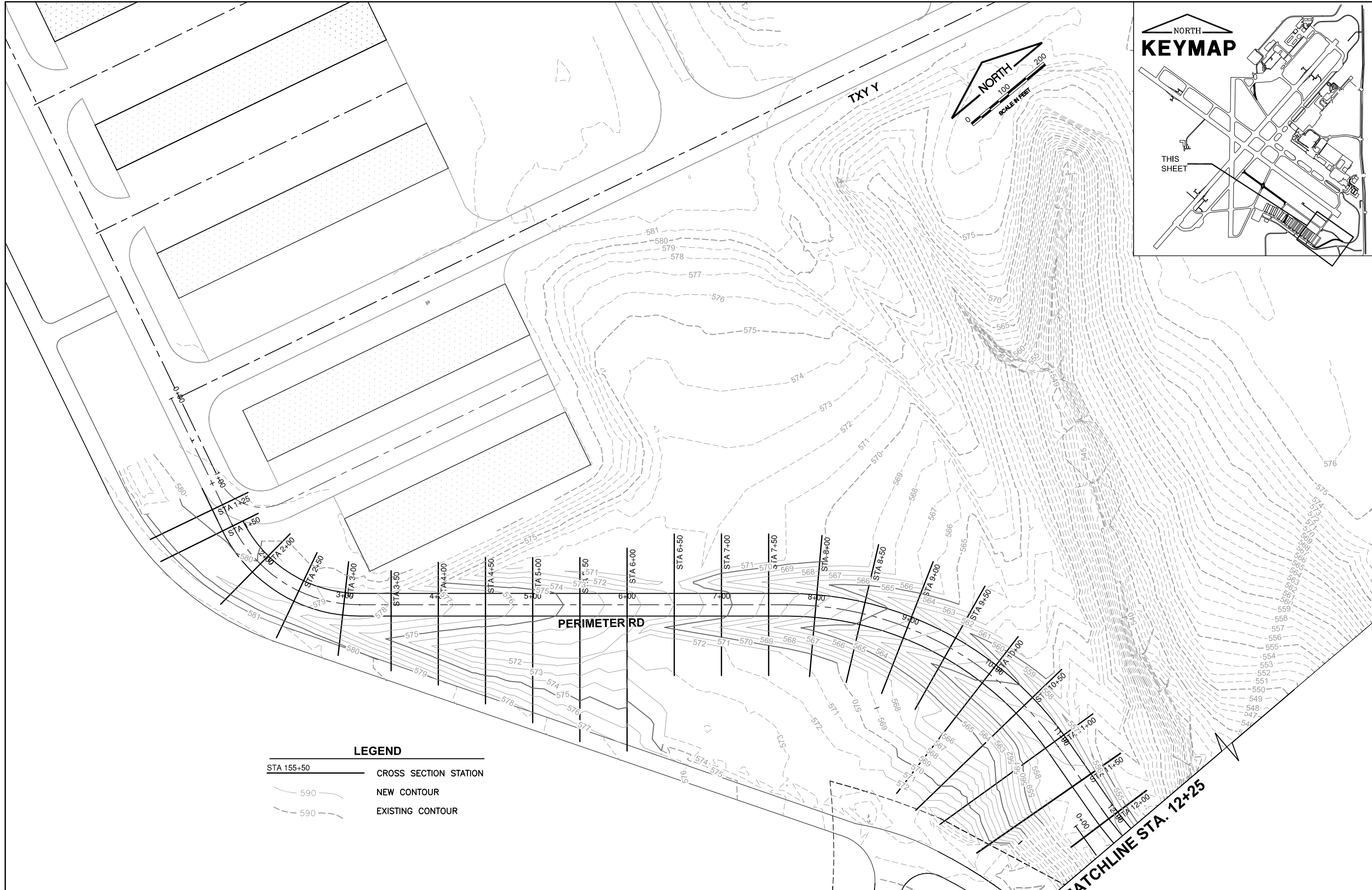
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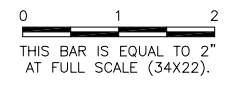
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LEGEND

| | |
|-----------------|-----------------------|
| — STA 155+50 | CROSS SECTION STATION |
| — 590 — | NEW CONTOUR |
| - - - 590 - - - | EXISTING CONTOUR |


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EXTEND TAXIWAY Y
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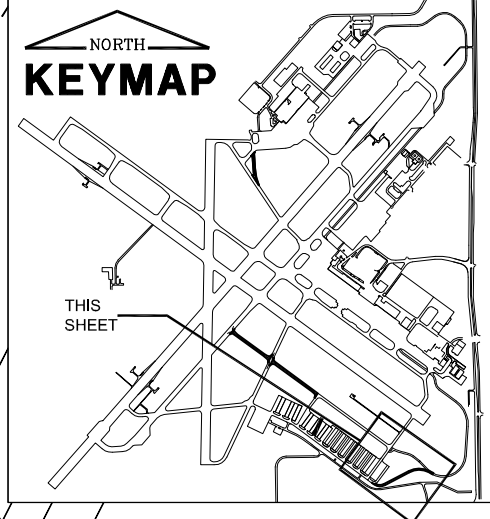
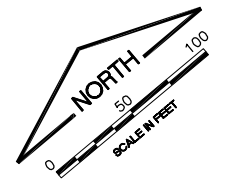
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 SHEET 82 OF 106 SHEETS

LEGEND

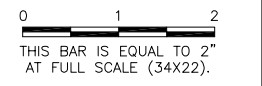
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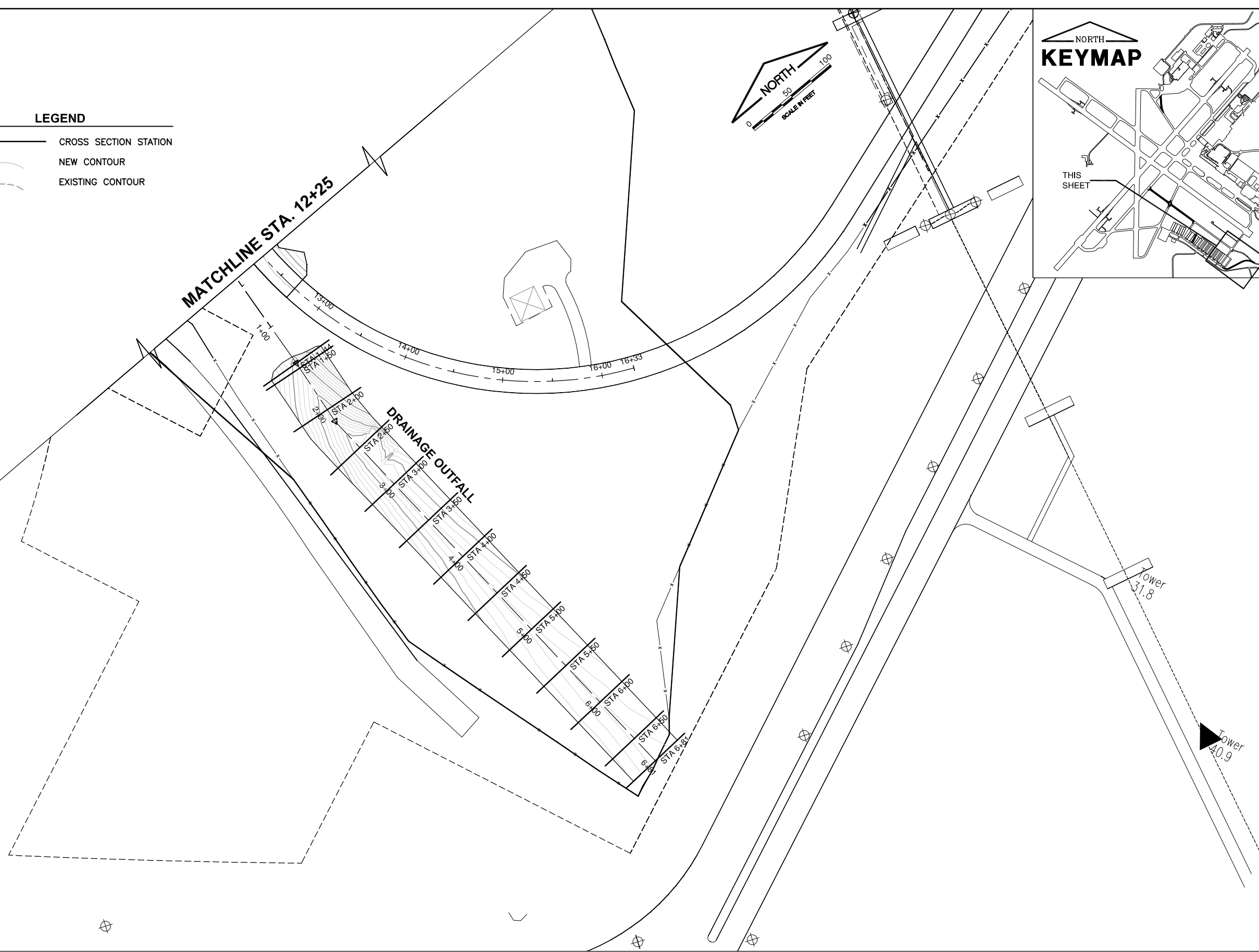
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MATCHLINE STA. 12+25

DRAINAGE OUTFALL



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**EXTEND TAXIWAY Y
 INDEX TO CROSS SECTIONS 4**

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 3-17-0096-XX

Earthwork Calcs : Taxiway Y

| | STATION | DISTANCE (Ft) | Earth Exc (Removal) | Embankment |
|---------------------|---------|------------------|---------------------|------------|
| | | | A (SF) | A (SF) |
| BEGIN LIMITS | 154+88 | | 104.8 | 0.0 |
| | 155+00 | 12 | 26.2 | 7.6 |
| | 155+50 | 50 | 2.4 | 22.6 |
| | 156+00 | 50 | 1.7 | 30.5 |
| | 156+50 | 50 | 3.6 | 28.9 |
| | 157+00 | 50 | 8.5 | 26.3 |
| | 157+50 | 50 | 10.3 | 28.9 |
| | 158+00 | 50 | 16.7 | 28.0 |
| | 158+50 | 50 | 22.3 | 31.2 |
| | 159+00 | 50 | 32.2 | 20.2 |
| | 159+50 | 50 | 30.2 | 32.9 |
| | 160+00 | 50 | 25.8 | 36.8 |
| | 160+50 | 50 | 14.6 | 44.2 |
| | 161+00 | 50 | 6.1 | 69.2 |
| | 161+50 | 50 | 2.1 | 61.1 |
| | 162+00 | 50 | 7.3 | 19.9 |
| | 162+48 | 48 | 141.2 | 0.0 |
| | 163+00 | 52 | 117.9 | 0.0 |
| | 163+50 | 50 | 5.1 | 25.5 |
| | 164+00 | 50 | 7.9 | 19.0 |
| | 164+50 | 50 | 6.4 | 18.1 |
| | 165+00 | 50 | 7.4 | 14.8 |
| | 165+50 | 50 | 8.5 | 13.9 |
| | 166+00 | 50 | 3.5 | 20.0 |
| | 166+50 | 50 | 1.3 | 23.8 |
| | 167+00 | 50 | 0.9 | 26.6 |
| | 167+50 | 50 | 1.0 | 29.3 |
| | 168+00 | 50 | 1.5 | 26.2 |
| | 168+50 | 50 | 3.9 | 22.2 |
| | 169+00 | 50 | 12.7 | 13.3 |
| | 169+50 | 50 | 29.5 | 6.5 |
| | 170+00 | 50 | 39.0 | 5.0 |
| | 170+50 | 50 | 48.1 | 2.9 |
| | 171+00 | 50 | 67.2 | 0.7 |
| | 171+50 | 50 | 94.7 | 0.0 |
| | 172+00 | 50 | 45.3 | 15.1 |
| END LIMITS | 172+09 | 9 | 33.9 | 0.0 |

Earthwork Calcs : AARF Road

| | STATION | DISTANCE (Ft) | Earth Exc (Removal) | Embankment |
|---------------------|---------|------------------|---------------------|------------|
| | | | A (SF) | A (SF) |
| BEGIN LIMITS | 306+02 | | 132.3 | 0.0 |
| | 306+50 | 48 | 42.0 | 4.1 |
| | 307+00 | 50 | 58.0 | 4.4 |
| | 307+50 | 50 | 60.5 | 5.1 |
| | 308+00 | 50 | 55.3 | 7.2 |
| | 308+50 | 50 | 56.5 | 9.5 |
| | 309+00 | 50 | 80.7 | 5.3 |
| | 309+50 | 50 | 45.1 | 6.2 |
| | 310+00 | 50 | 55.8 | 2.5 |
| | 310+50 | 50 | 33.0 | 1.7 |
| | | 26 | | |
| END LIMITS | 310+76 | | 14.0 | 7.7 |

Earthwork Calcs : Drainage Outfall

| | STATION | DISTANCE (Ft) | Earth Exc (Removal) | Embankment |
|---------------------|---------|------------------|---------------------|------------|
| | | | A (SF) | A (SF) |
| BEGIN LIMITS | 01+44 | | 8.9 | 1.5 |
| | 01+50 | 6 | 19.6 | 10.7 |
| | 02+00 | 50 | 29.4 | 13.9 |
| | 02+50 | 100 | 130.9 | 0.0 |
| | 03+00 | 50 | 92.1 | 4.1 |
| | 03+50 | 50 | 71.4 | 0.0 |
| | 04+00 | 50 | 81.4 | 0.0 |
| | 04+50 | 50 | 99.3 | 0.0 |
| | 05+00 | 50 | 70.1 | 0.0 |
| | 05+50 | 50 | 43.0 | 0.0 |
| | 06+00 | 50 | 30.9 | 9.2 |
| | 06+50 | 50 | 26.3 | 5.1 |
| | | 30 | | |
| END LIMITS | 06+80 | | 29.8 | 0.1 |

Earthwork Calcs : Perimeter Road

| | STATION | DISTANCE (Ft) | Earth Exc (Removal) | Embankment |
|---------------------|---------|------------------|---------------------|------------|
| | | | A (SF) | A (SF) |
| BEGIN LIMITS | 01+25 | | 8.4 | 39.1 |
| | 01+50 | 25 | 34.0 | 23.5 |
| | 02+00 | 50 | 13.5 | 26.8 |
| | 02+50 | 50 | 4.7 | 73.7 |
| | 03+00 | 50 | 5.8 | 109.8 |
| | 03+50 | 50 | 2.3 | 195.3 |
| | 04+00 | 50 | 0.3 | 370.3 |
| | 04+50 | 50 | 0.2 | 482.8 |
| | 05+00 | 50 | 0.6 | 360.0 |
| | 05+50 | 50 | 6.2 | 235.1 |
| | 06+00 | 50 | 27.7 | 117.6 |
| | 06+50 | 50 | 61.3 | 0.2 |
| | 07+00 | 50 | 208.5 | 0.0 |
| | 07+50 | 50 | 290.9 | 0.0 |
| | 08+00 | 50 | 366.7 | 0.0 |
| | 08+50 | 50 | 403.4 | 0.0 |
| | 09+00 | 50 | 417.9 | 0.0 |
| | 09+50 | 50 | 369.8 | 0.0 |
| | 10+00 | 50 | 298.4 | 0.0 |
| | 10+50 | 50 | 212.5 | 0.0 |
| | 11+00 | 150 | 265.7 | 12.0 |
| | 11+50 | 50 | 25.7 | 7.6 |
| | 12+00 | 50 | 28.9 | 8.0 |
| | 12+50 | 50 | 34.4 | 3.7 |
| | | 18 | | |
| END LIMITS | 12+68 | | 36.3 | 0.6 |

Earthwork Calcs : Perimeter Road

| | Depth | AREA |
|------|--------|---------|
| | L (IN) | A (SF) |
| AREA | 21" | 23275.0 |

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 THIS BAR IS EQUAL TO 2"
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EXTEND TAXIWAY Y
 EARTHWORK SUMMARY

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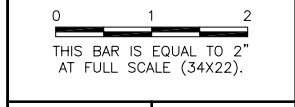
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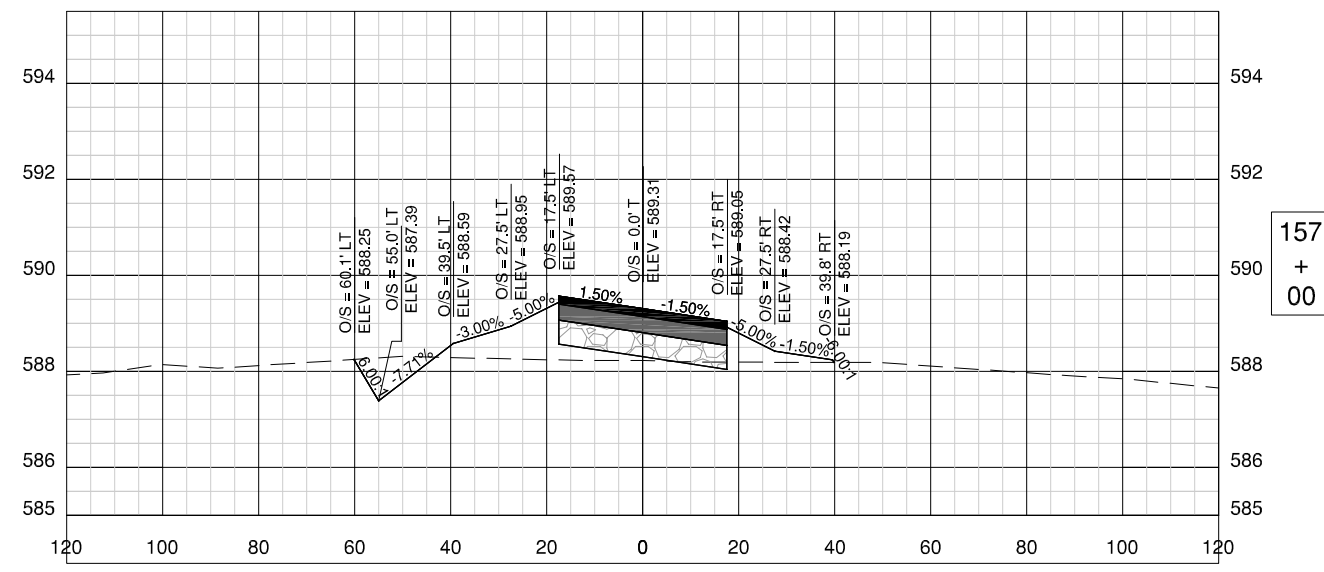
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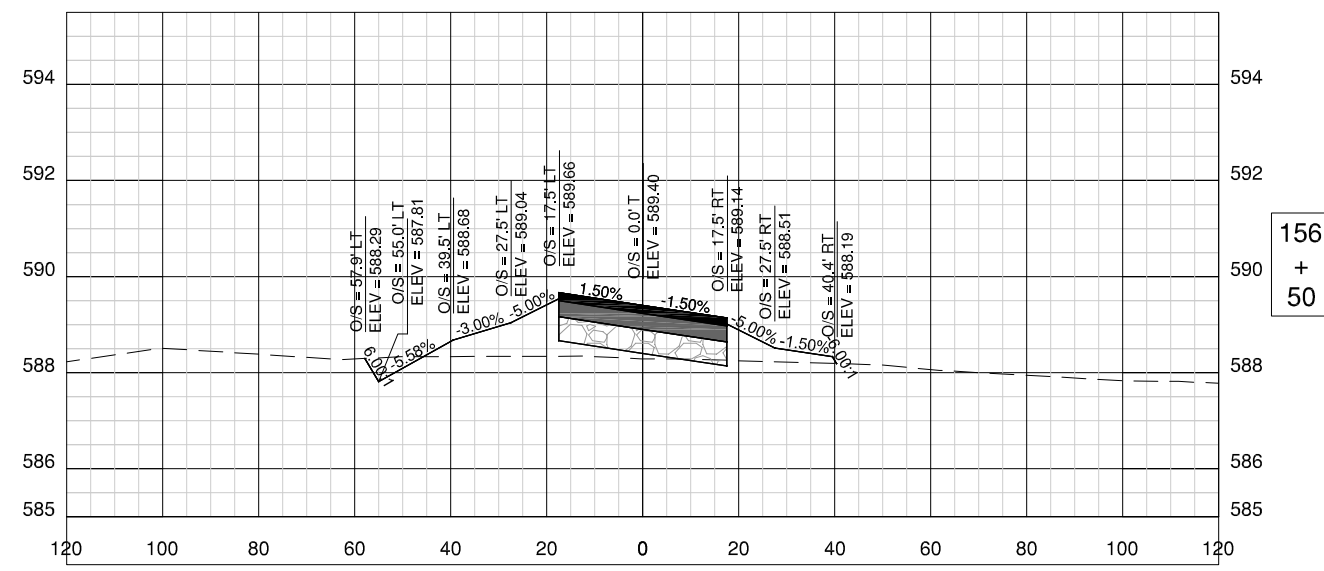


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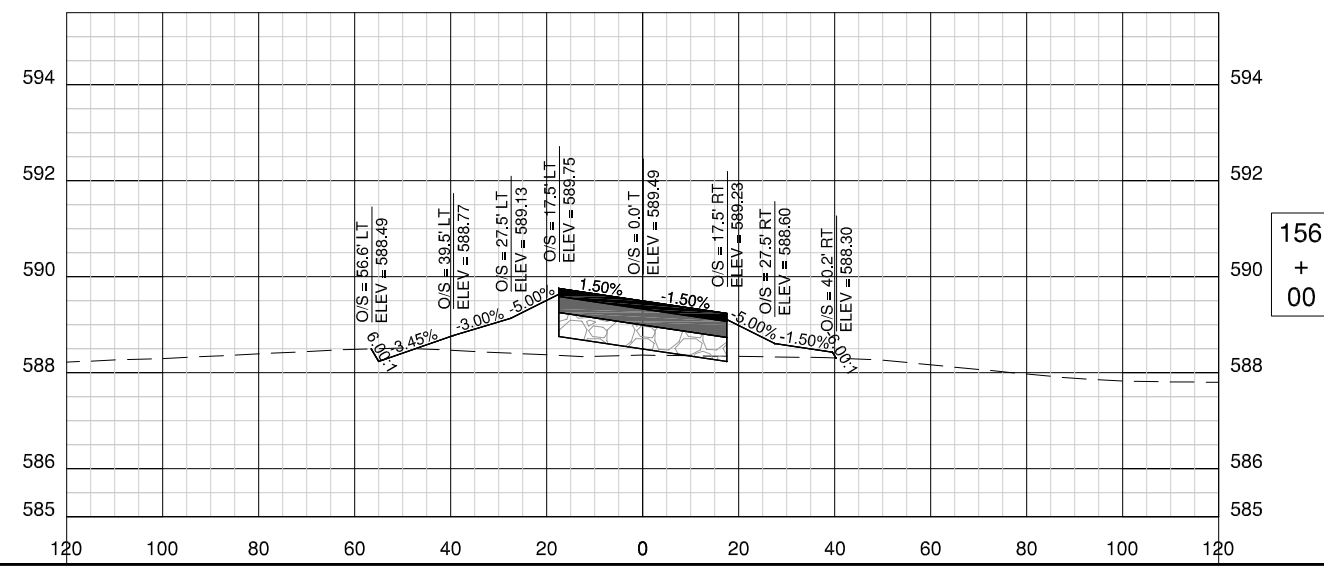
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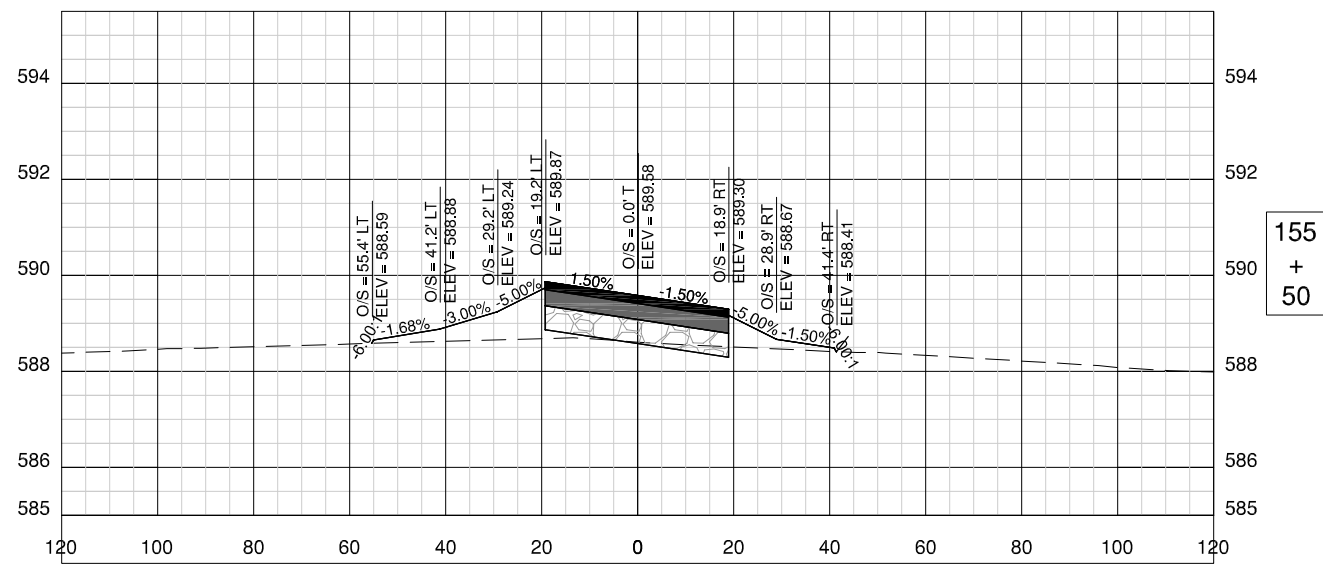
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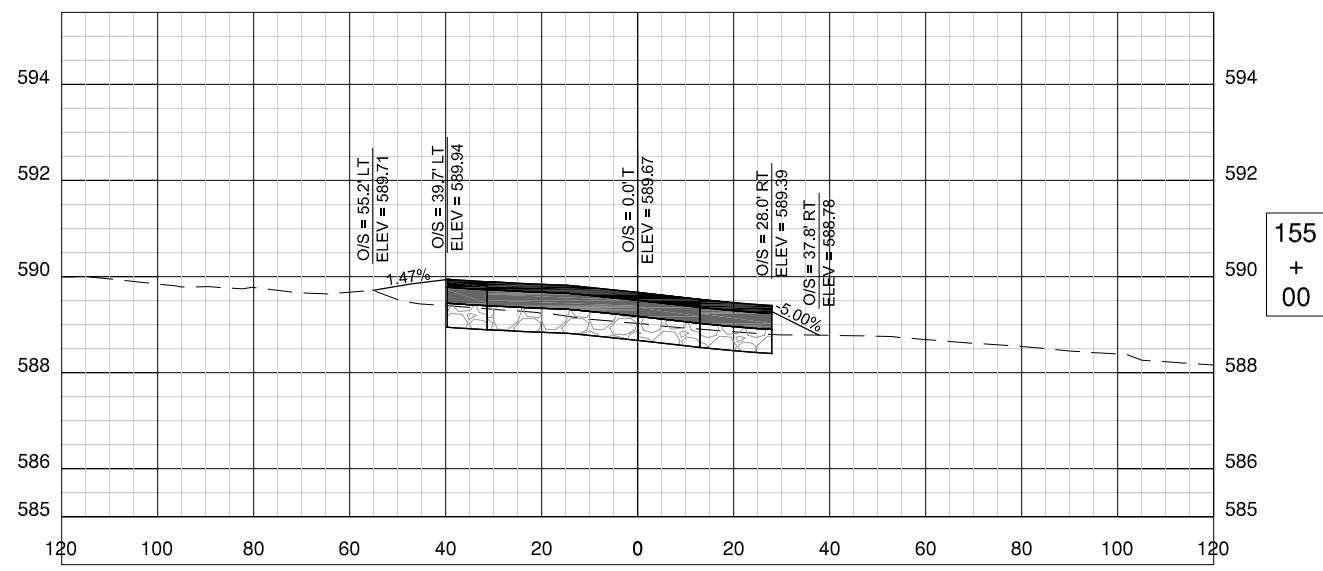
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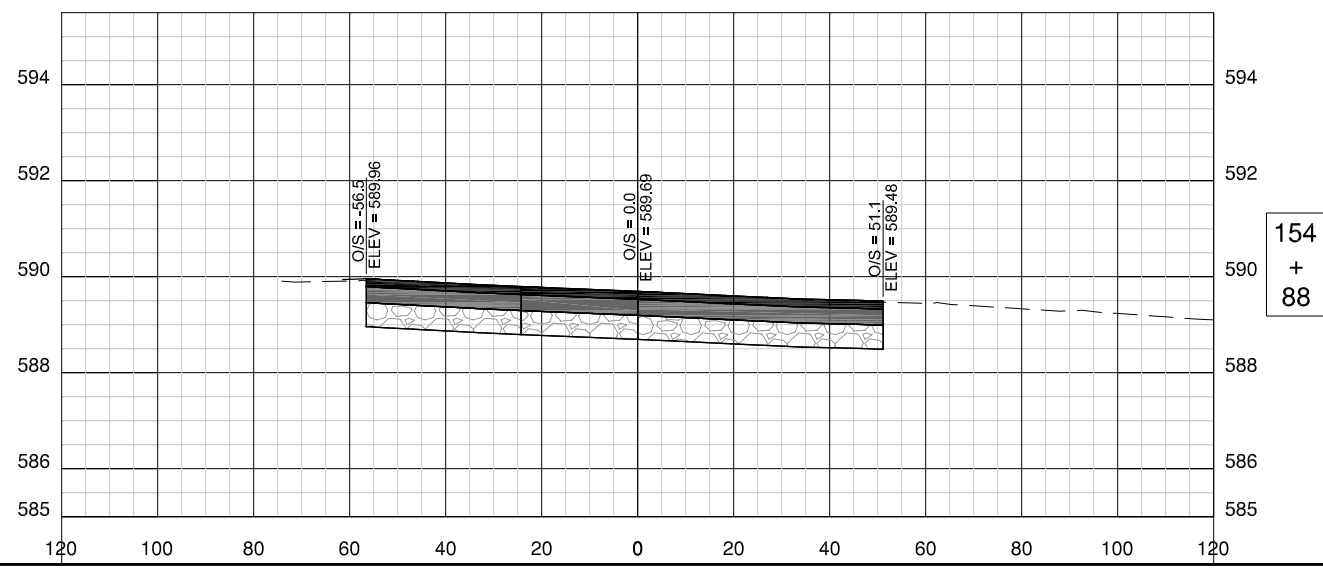
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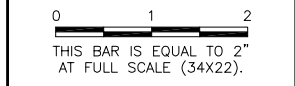
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**EXTEND TAXIWAY Y
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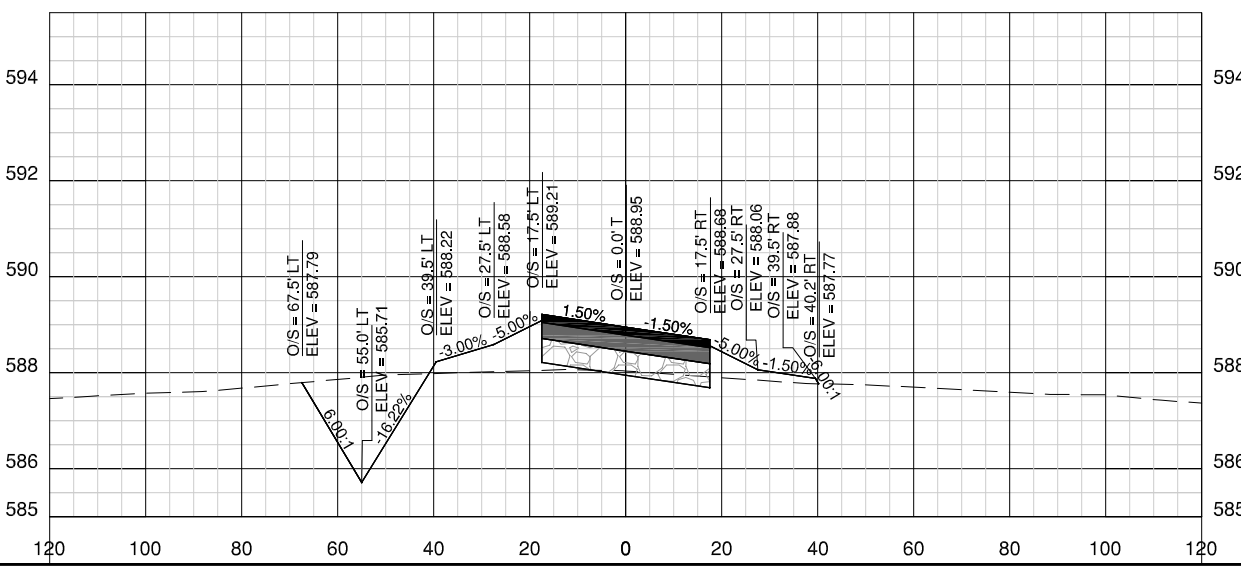
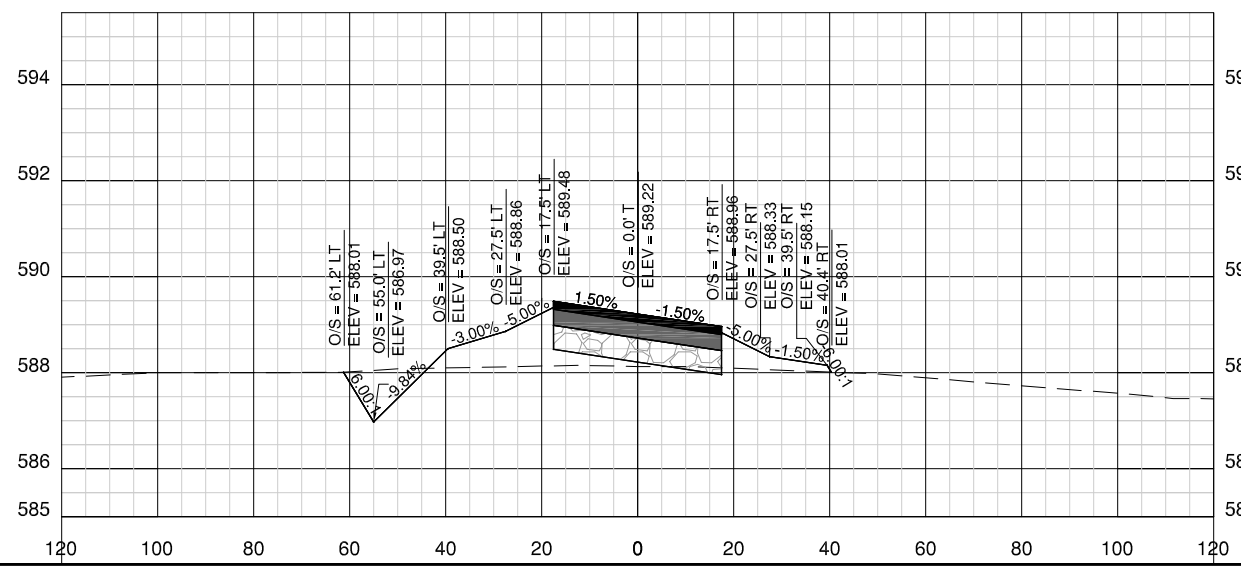
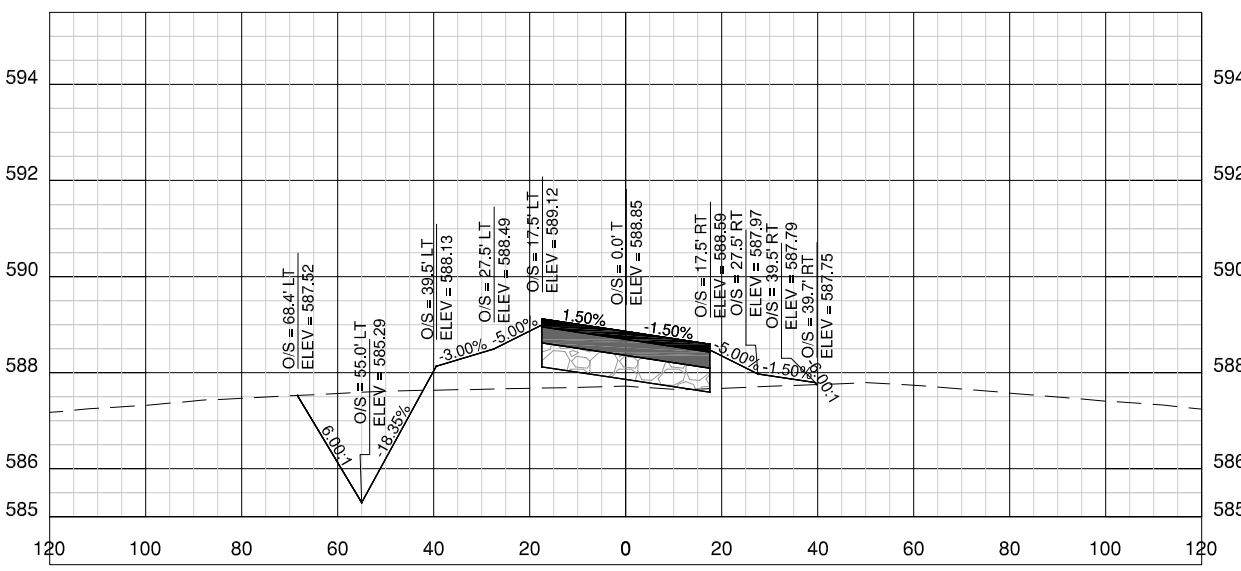
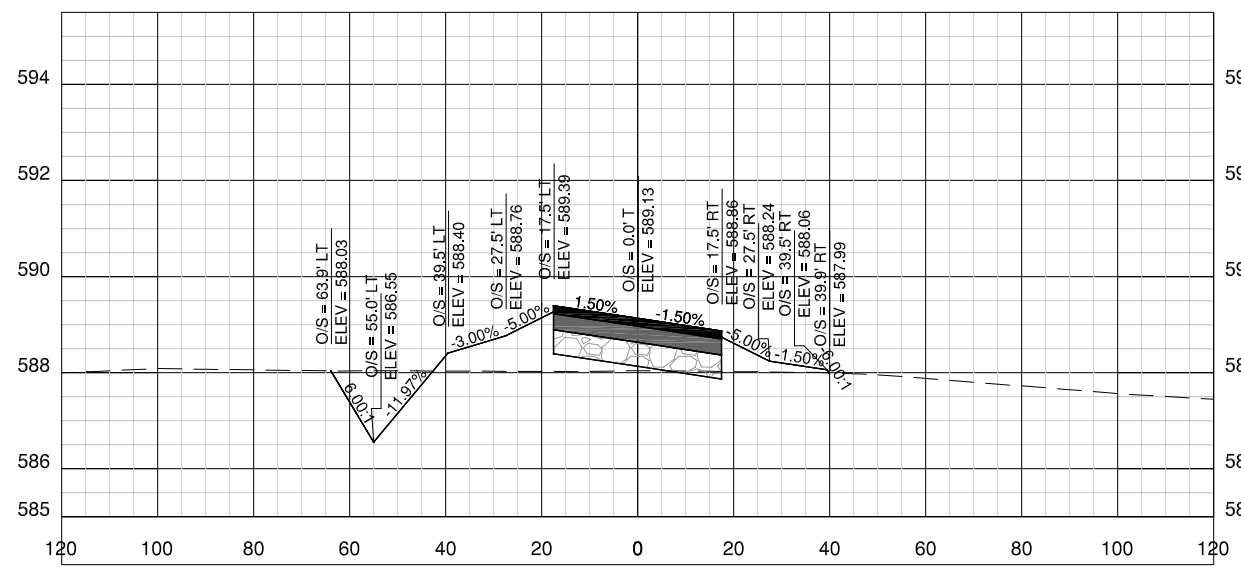
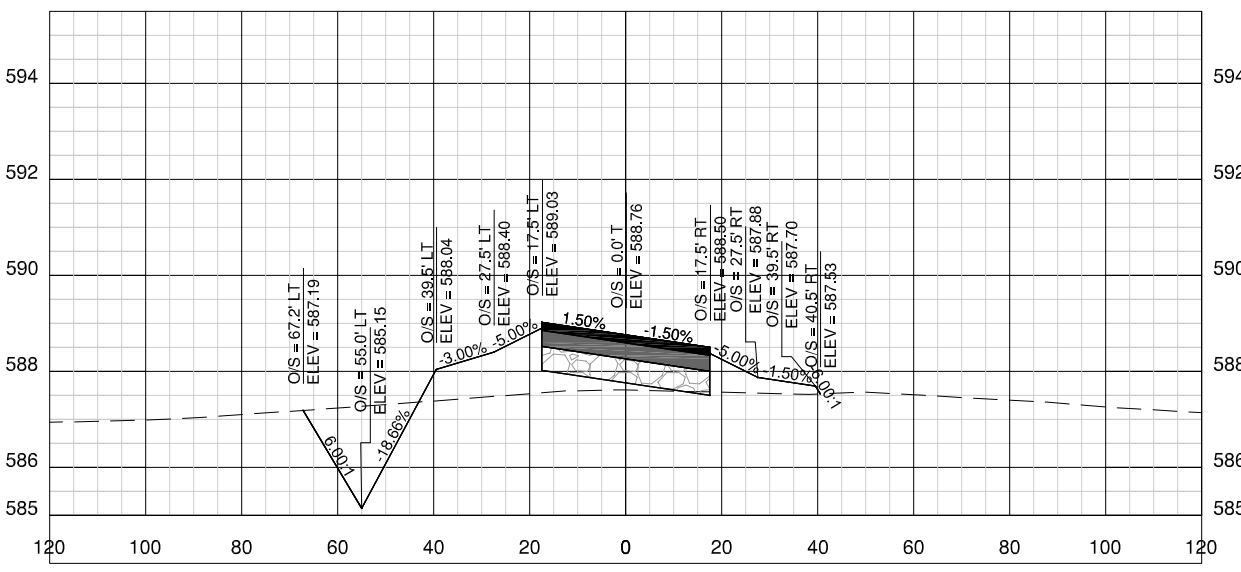
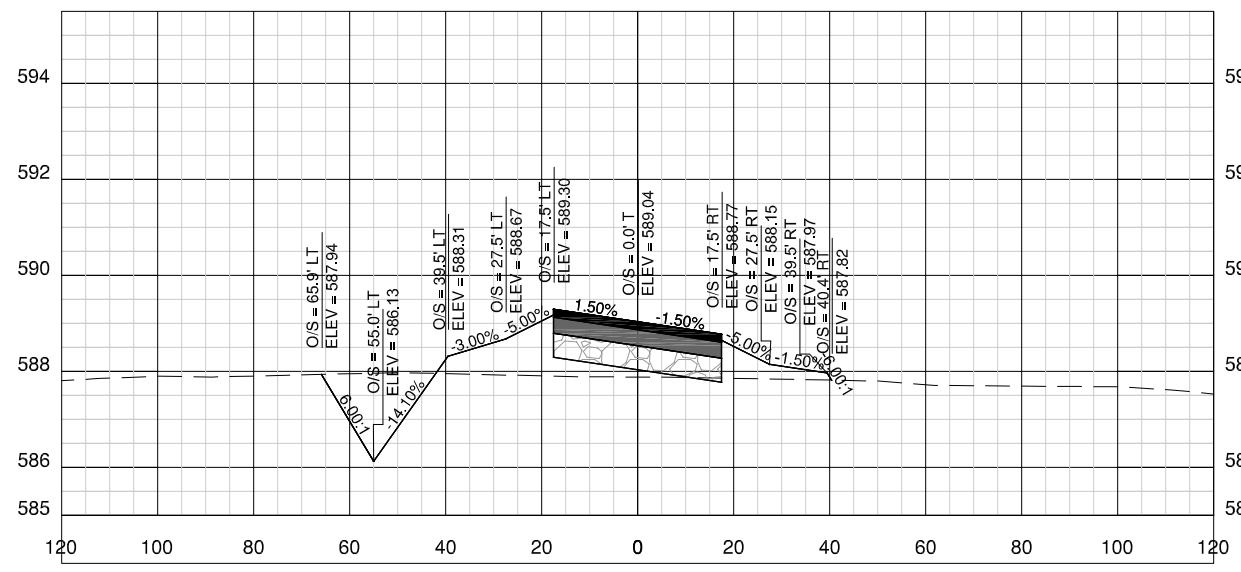
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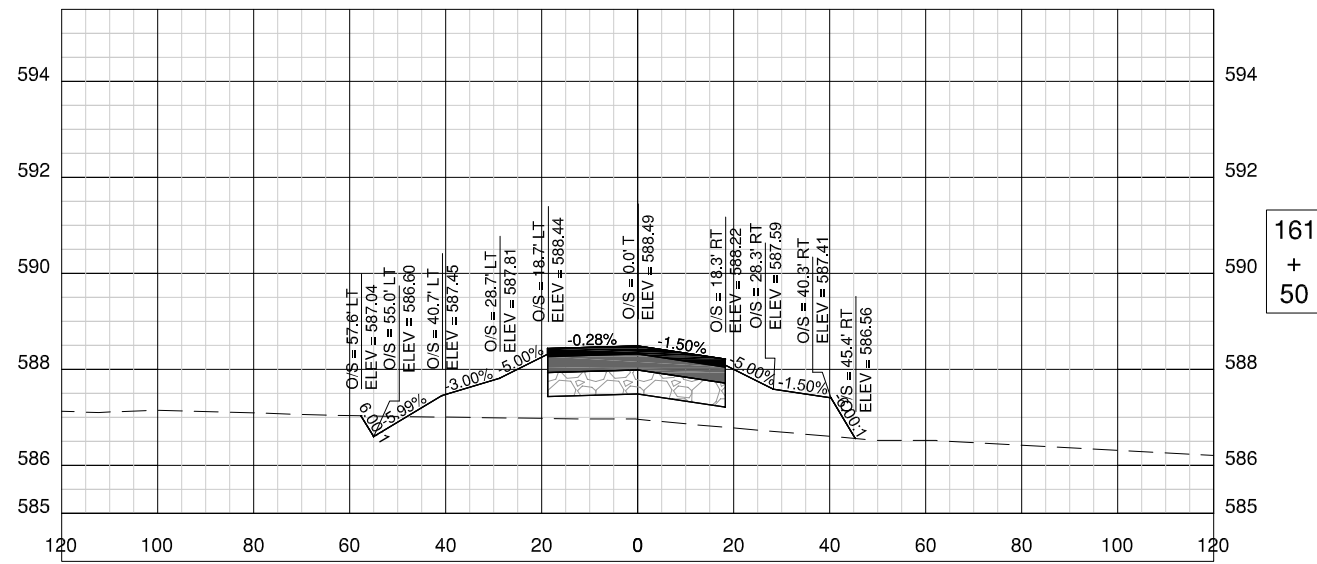
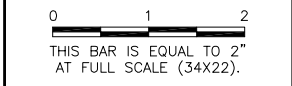
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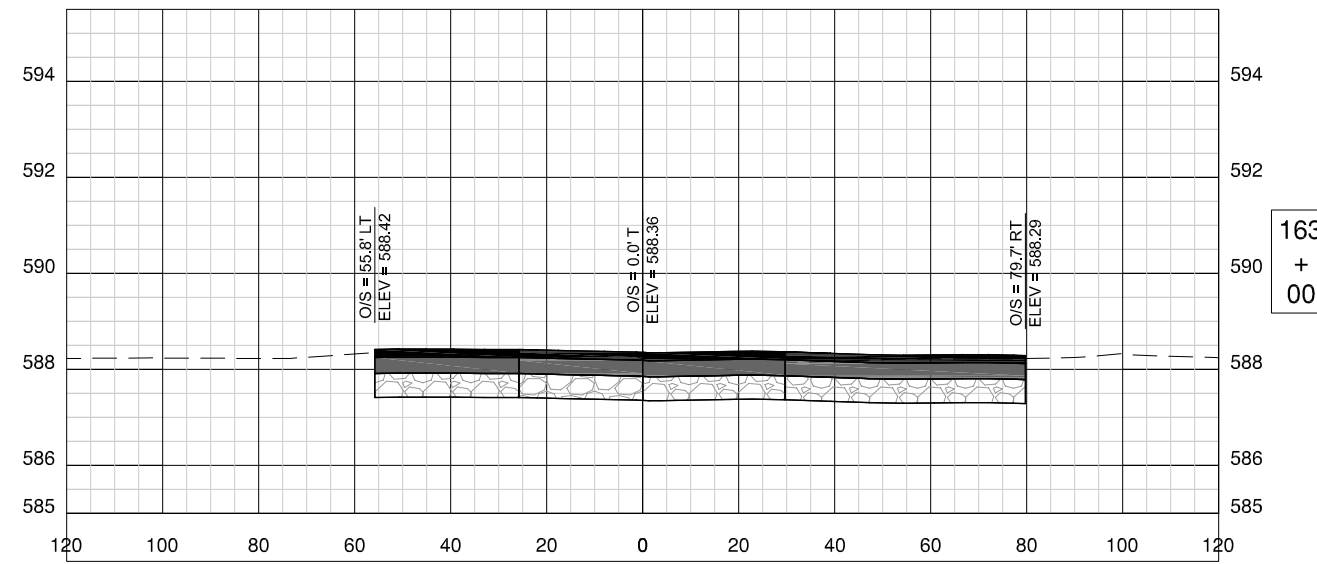


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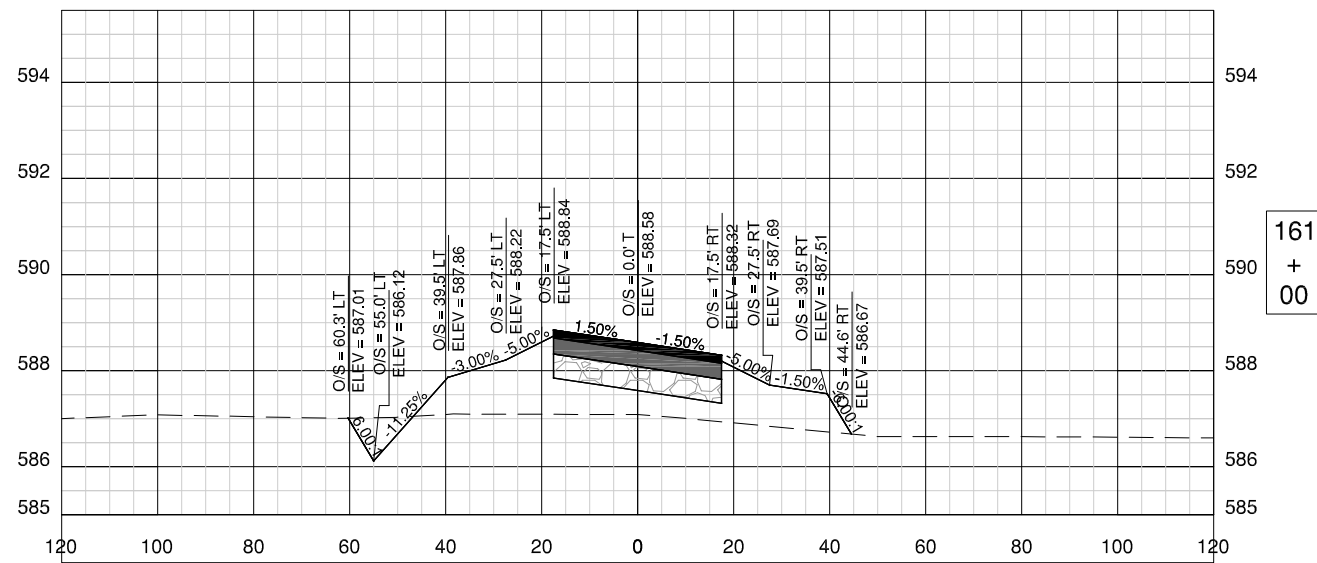
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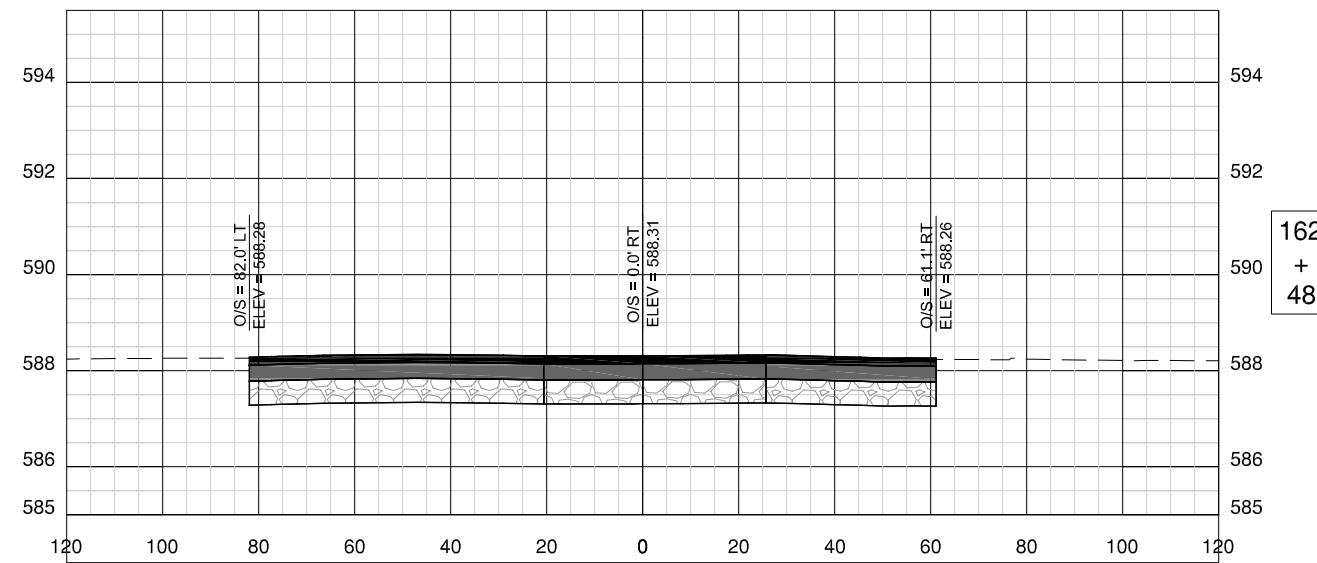
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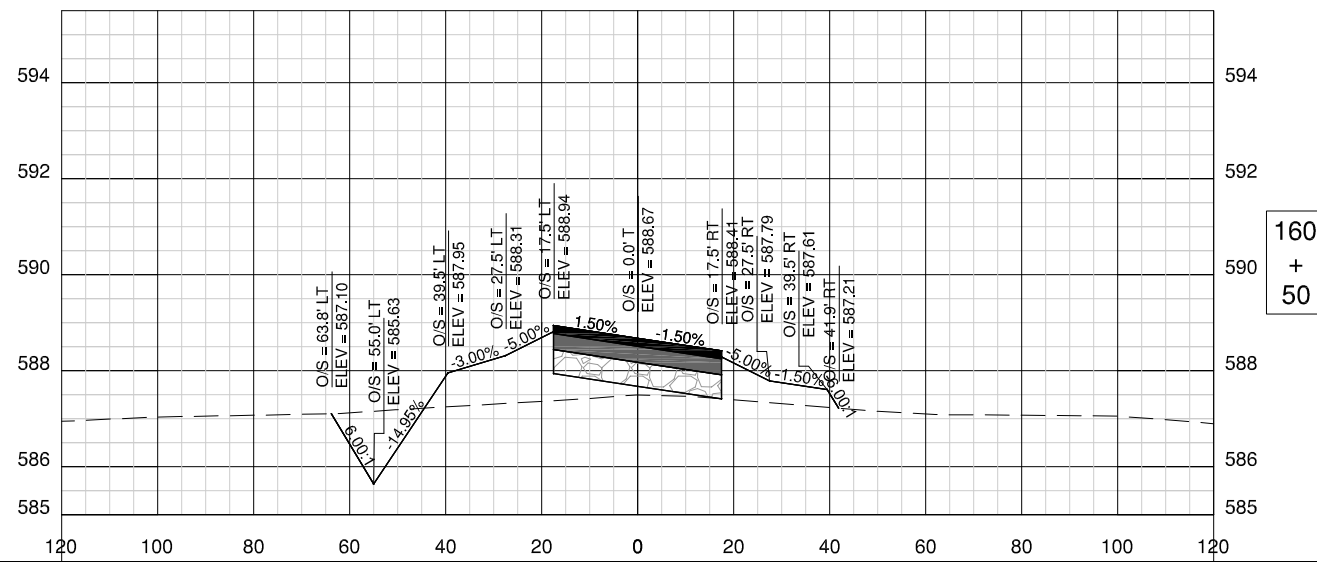
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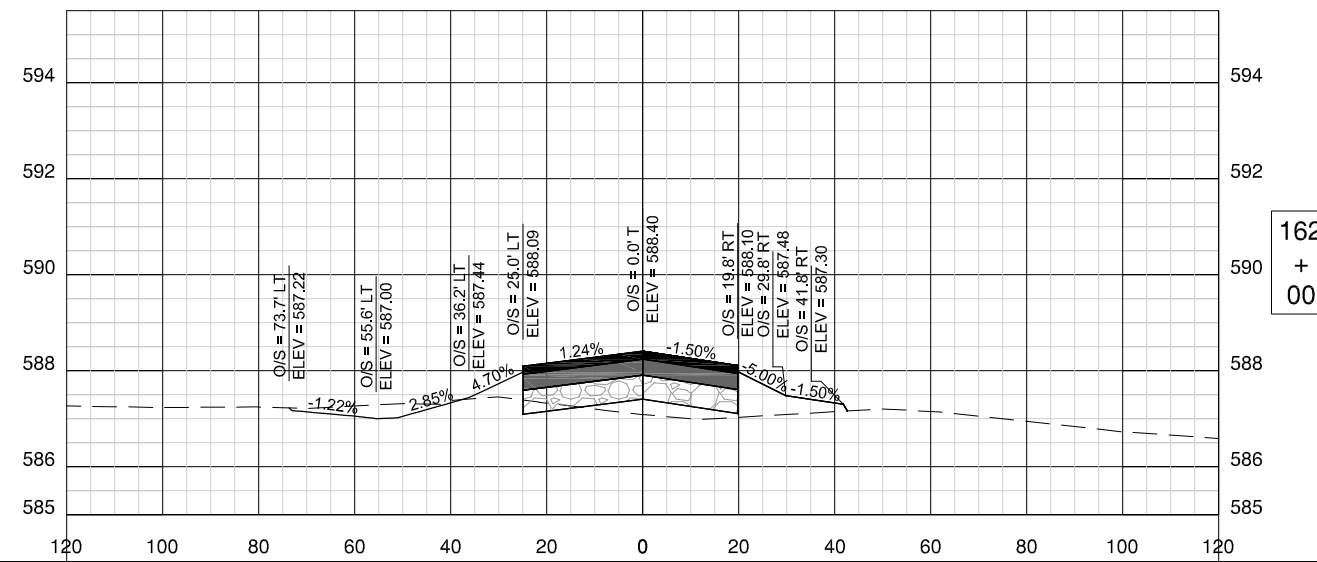
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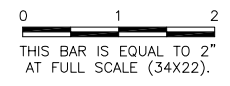
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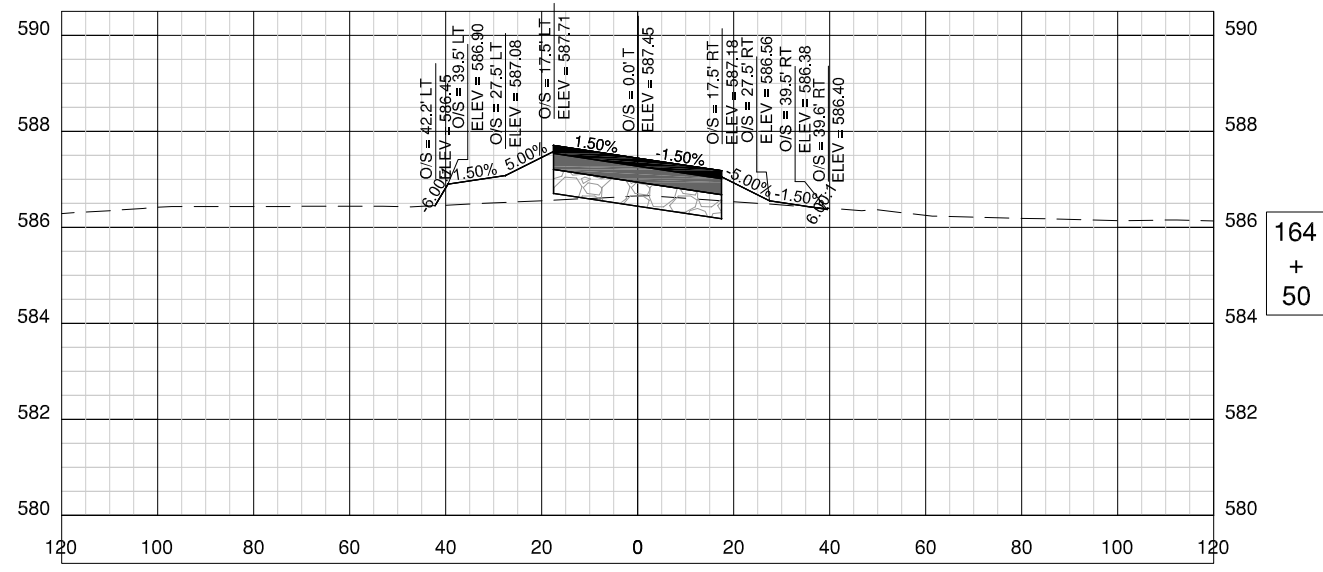


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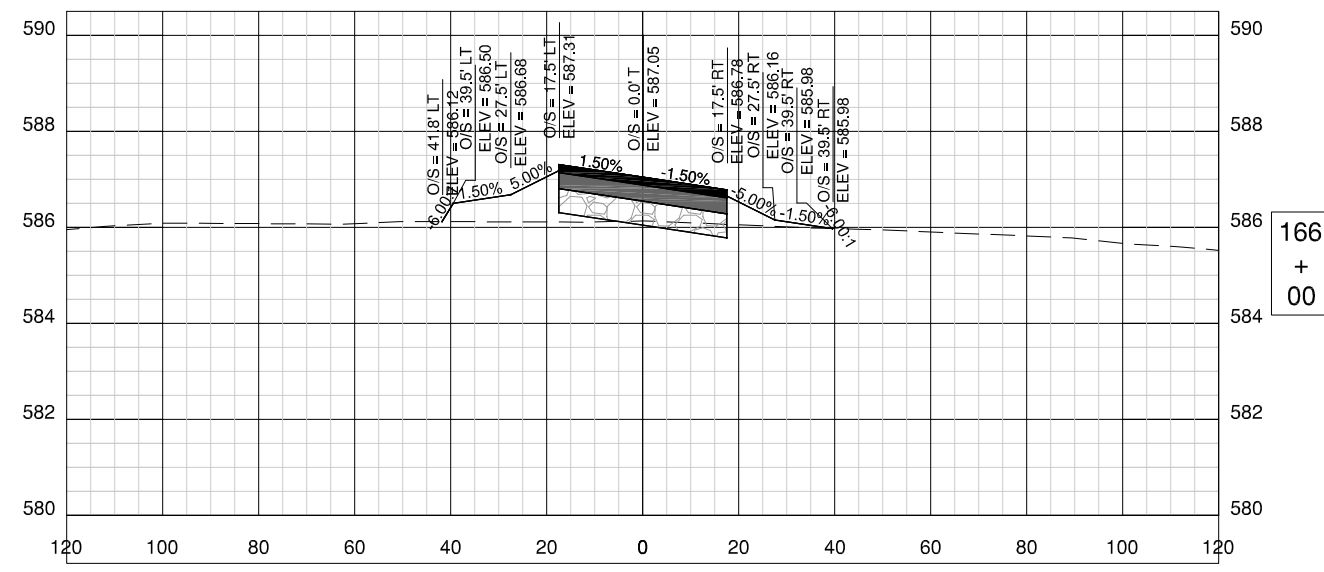
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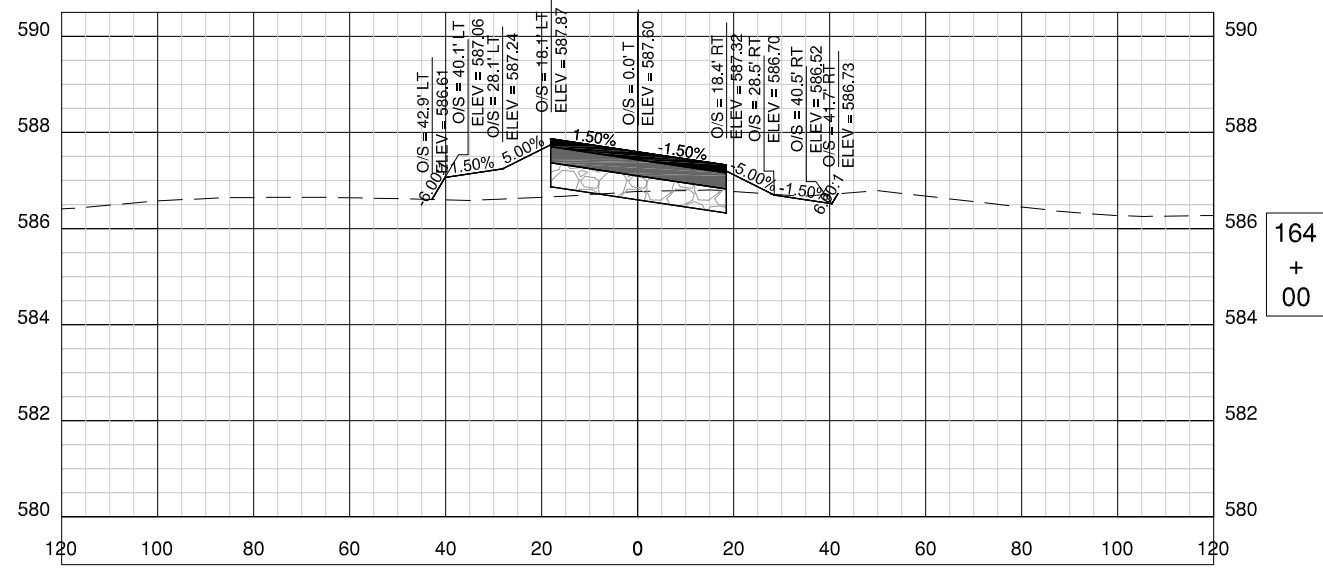
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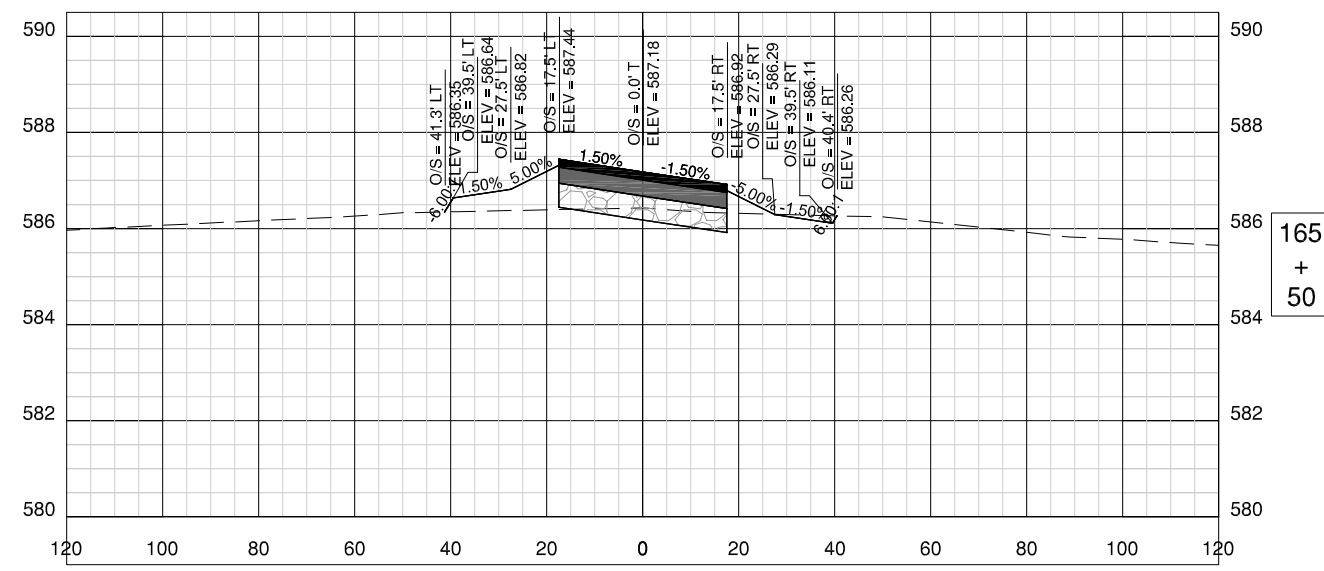
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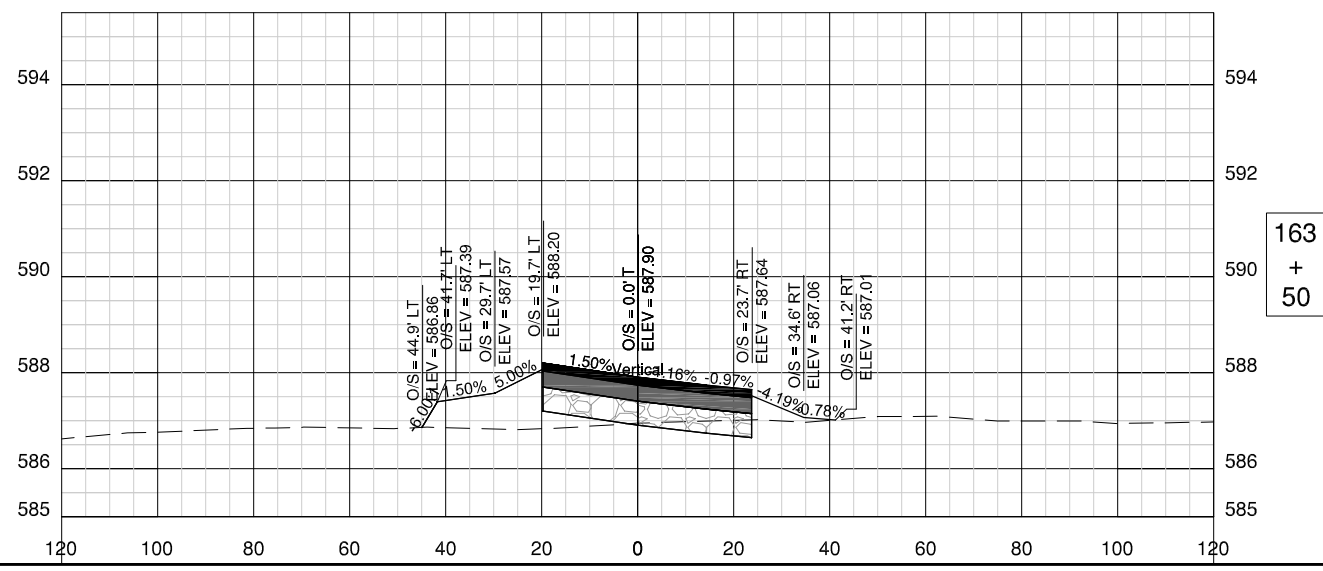
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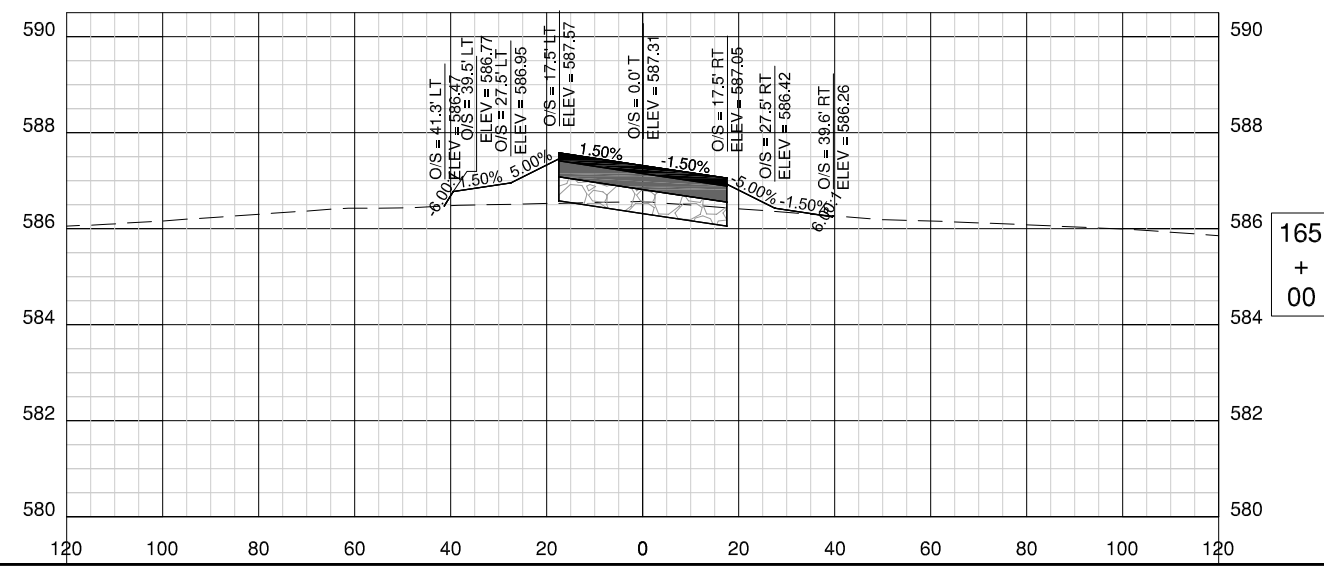
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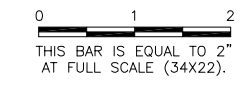
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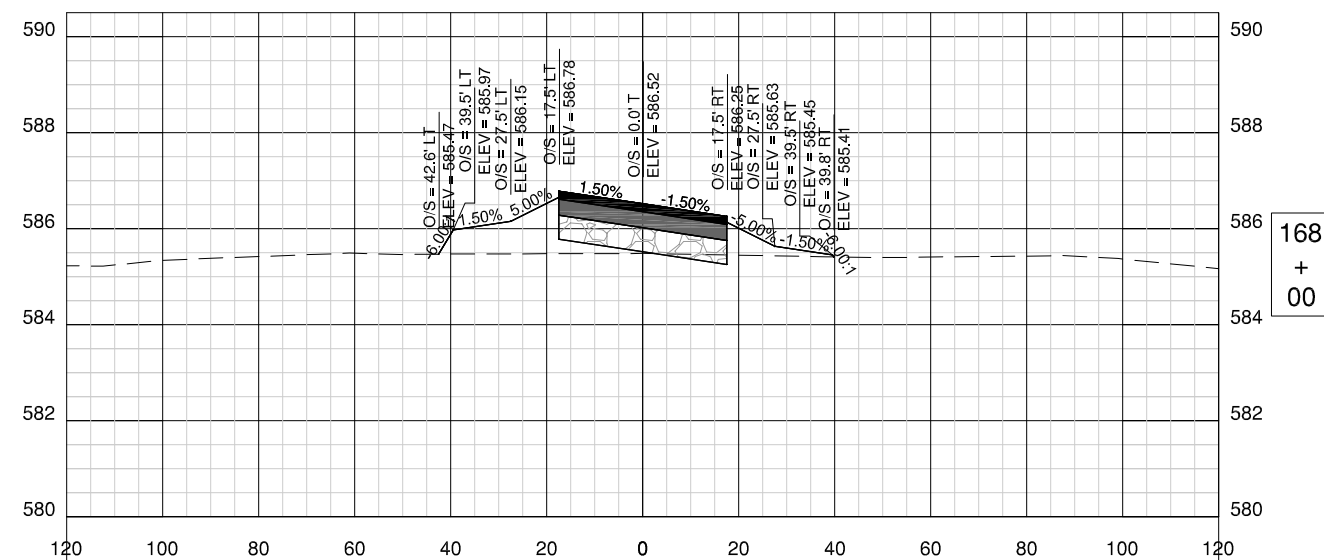
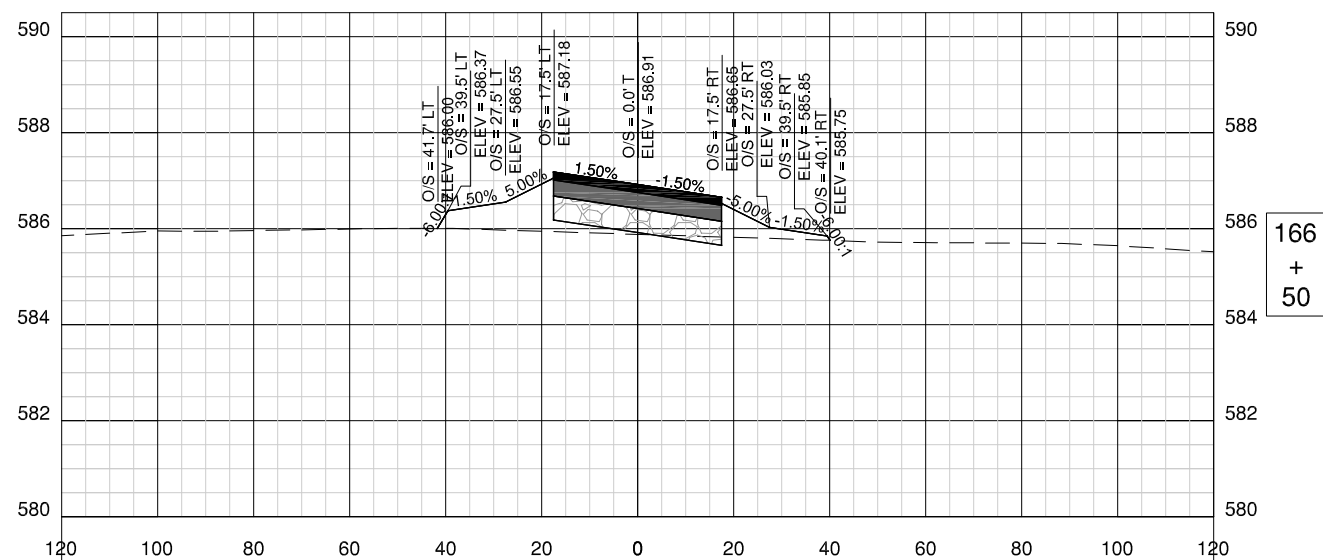
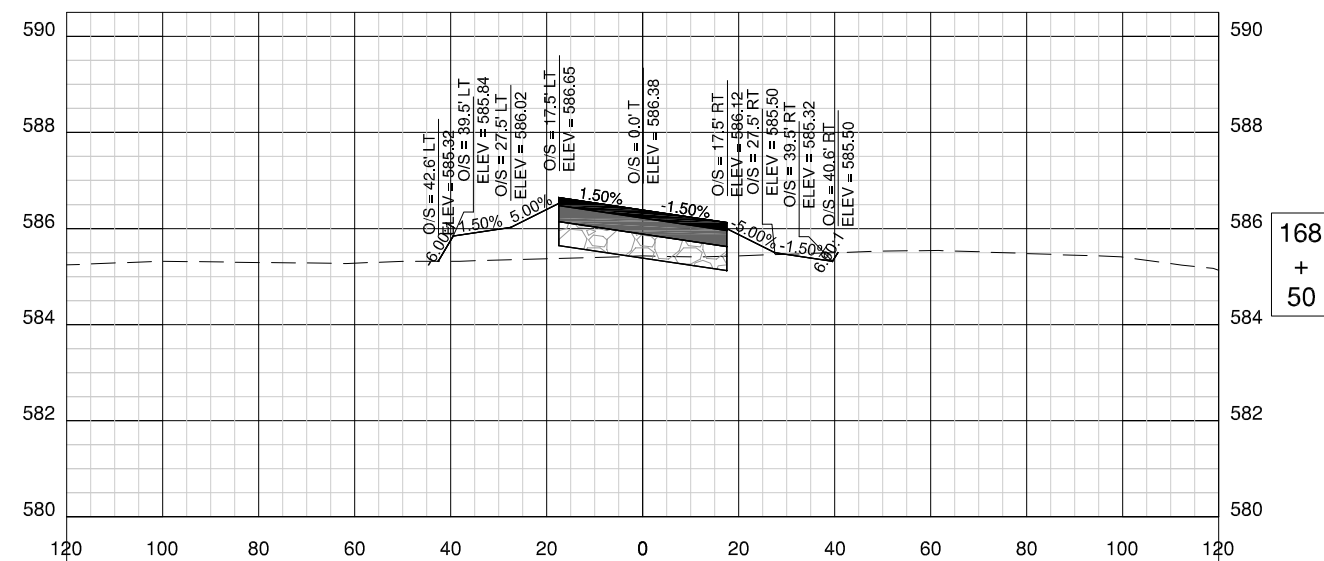
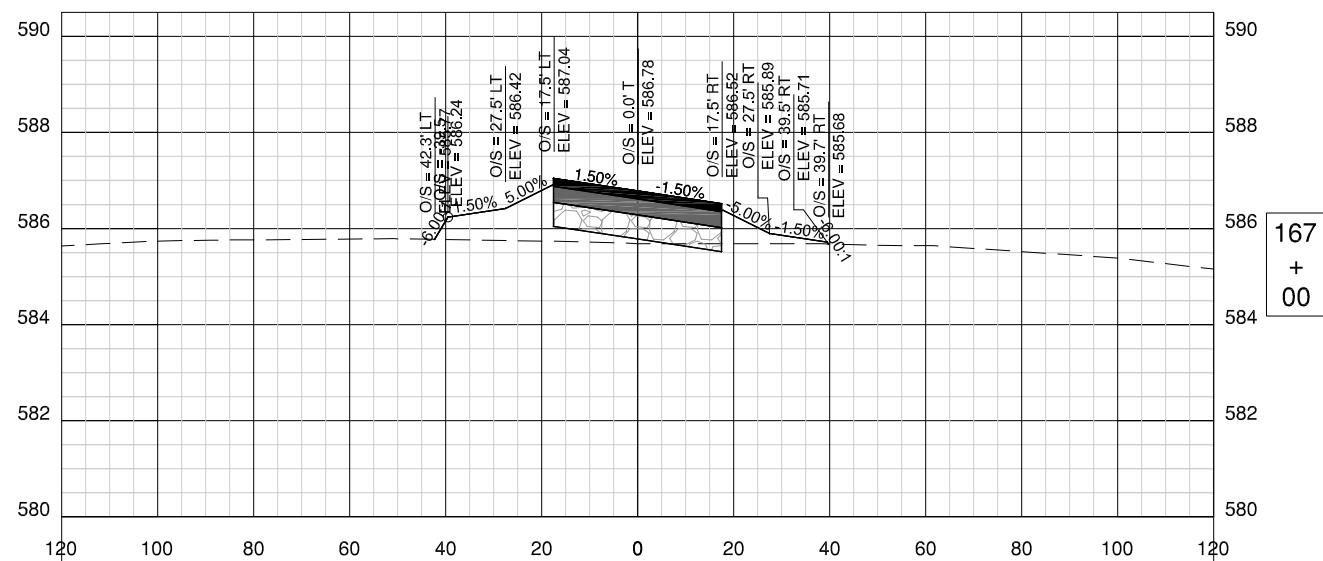
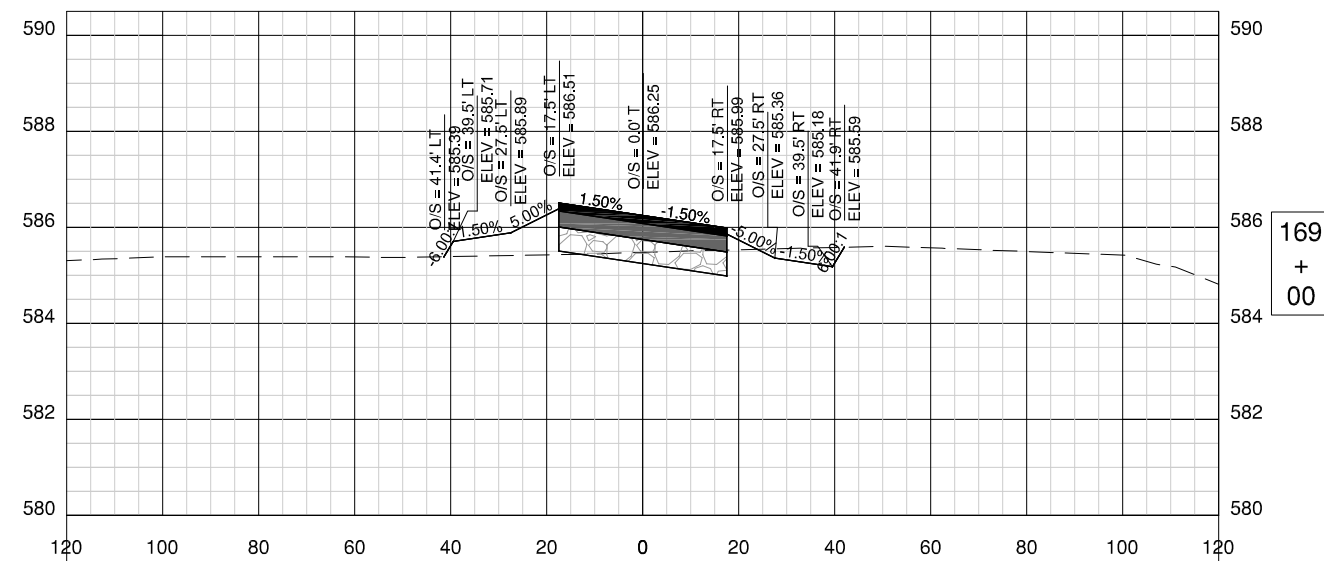
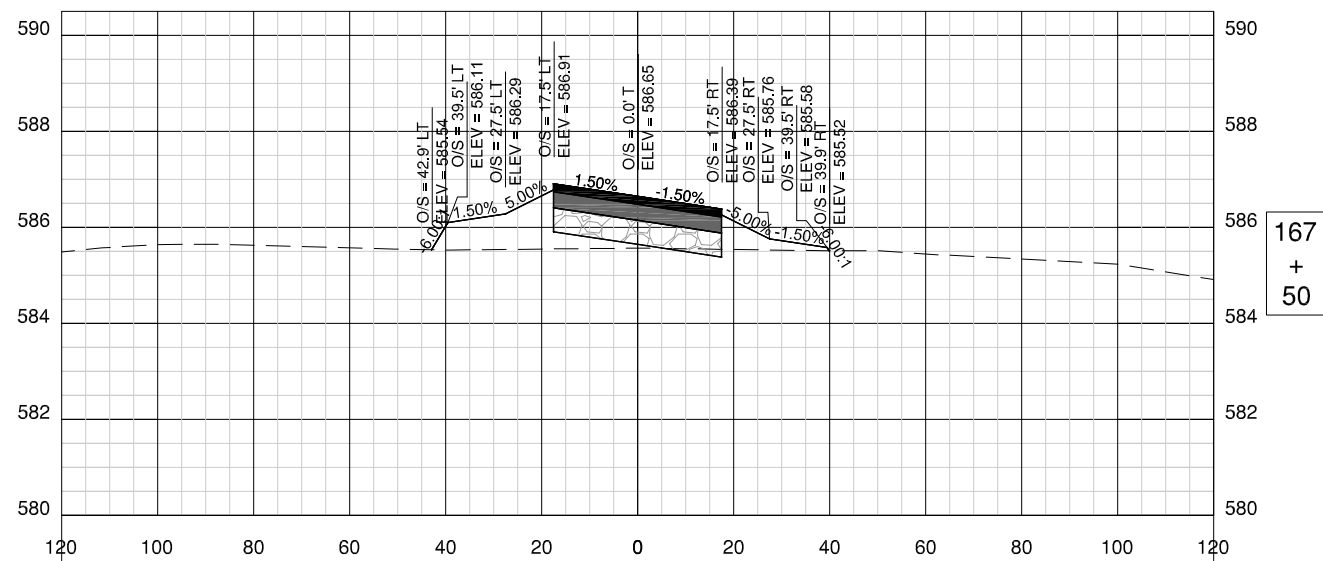
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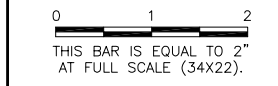
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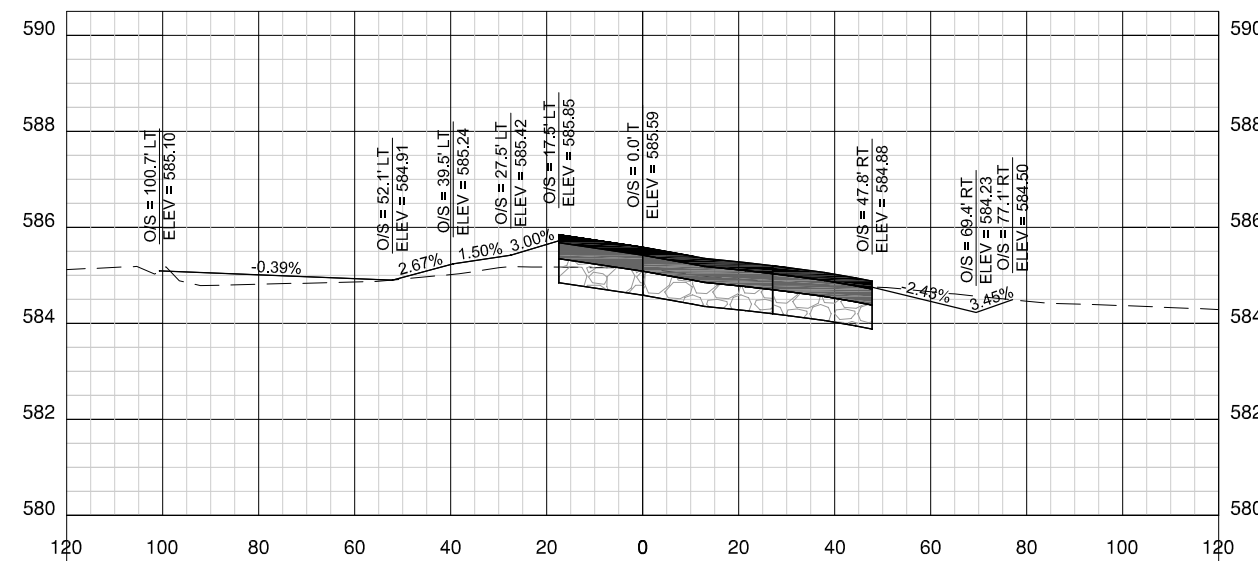
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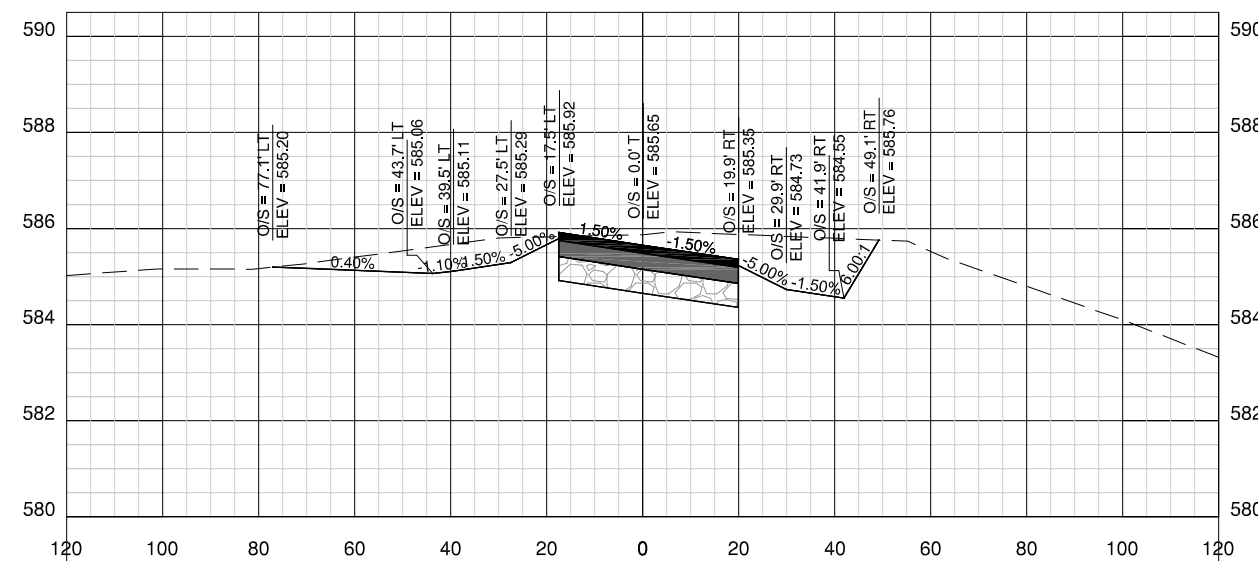
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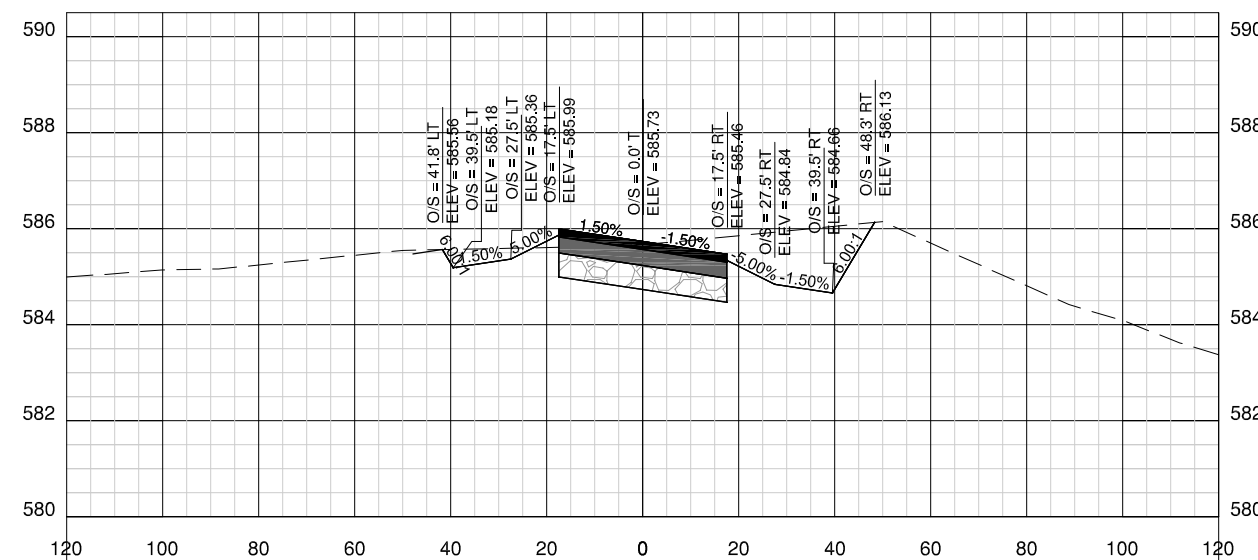
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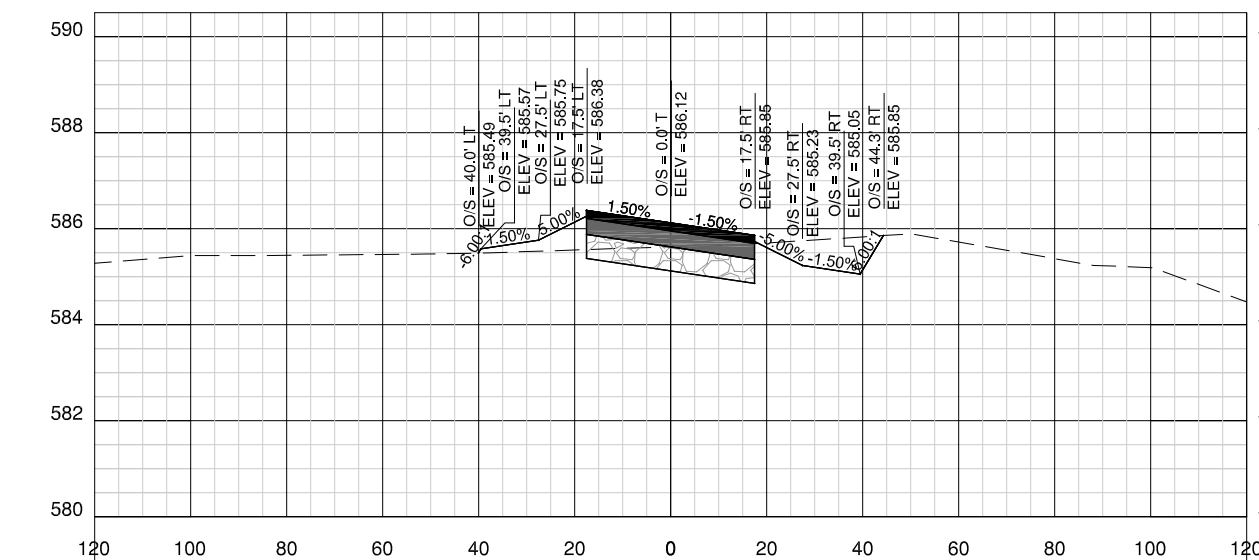
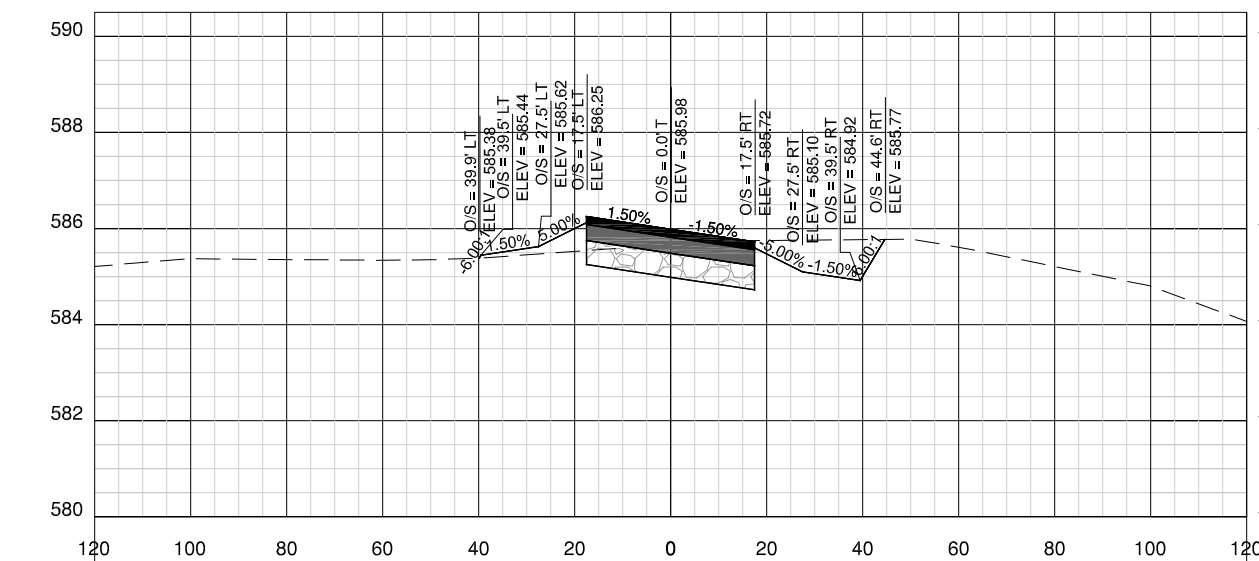
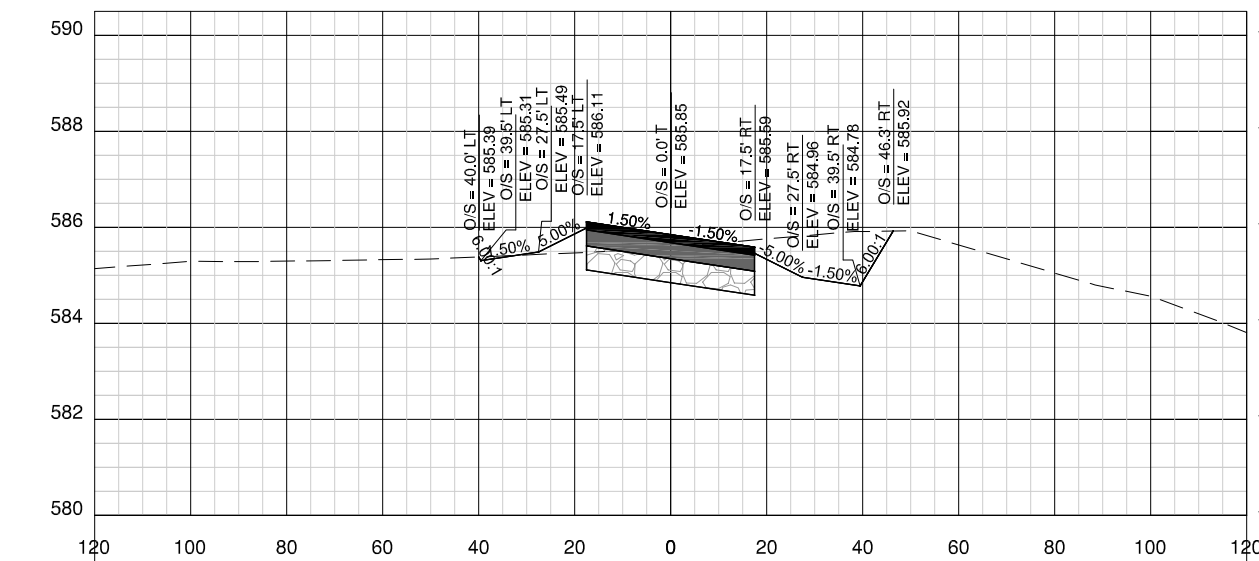


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EXTEND TAXIWAY Y
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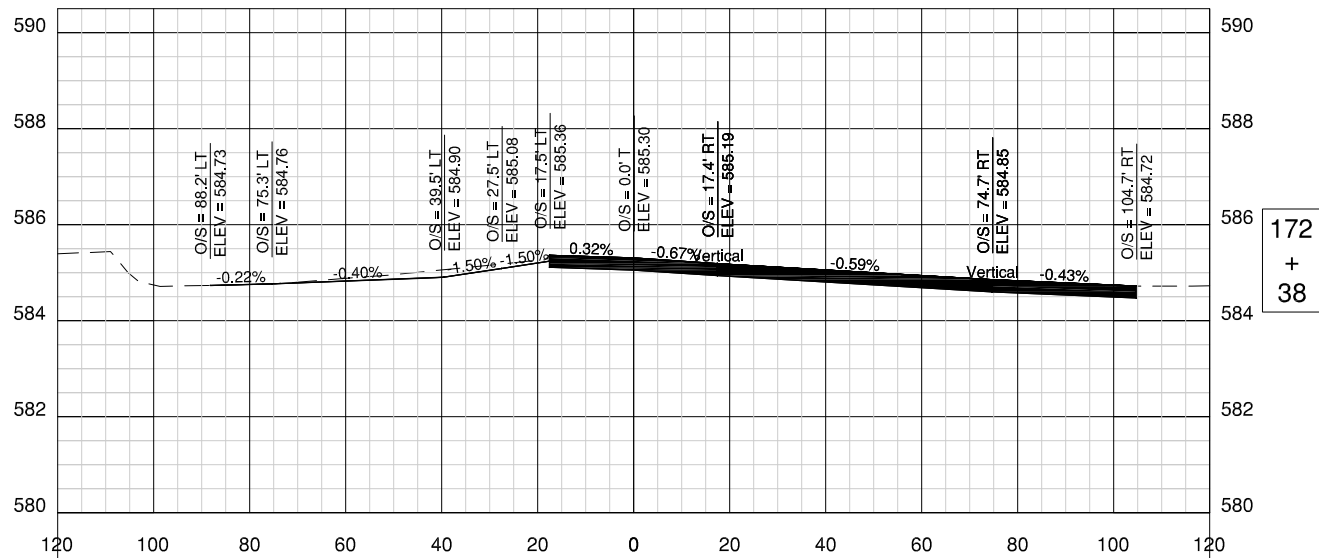
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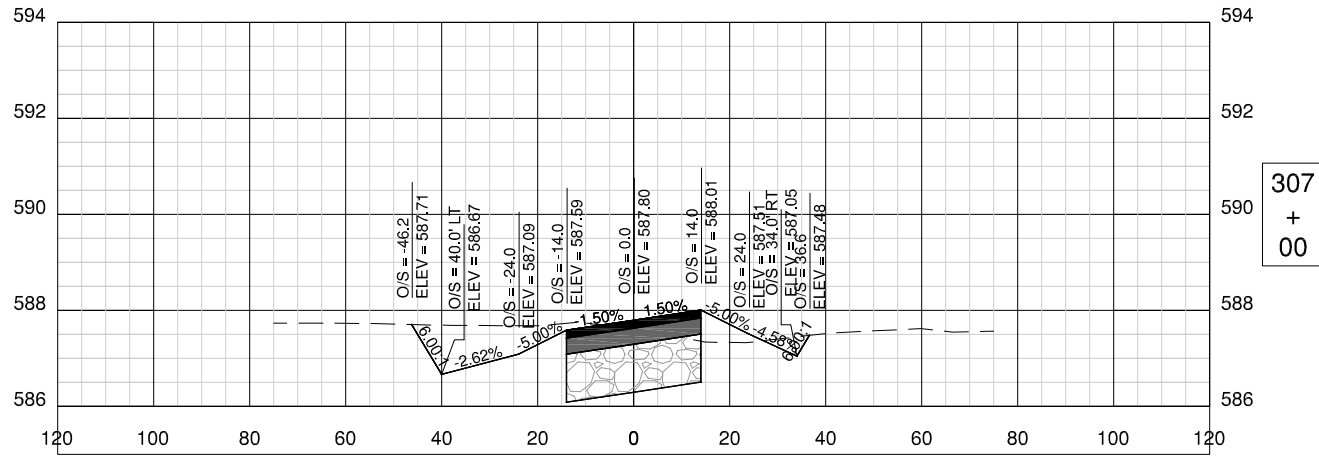
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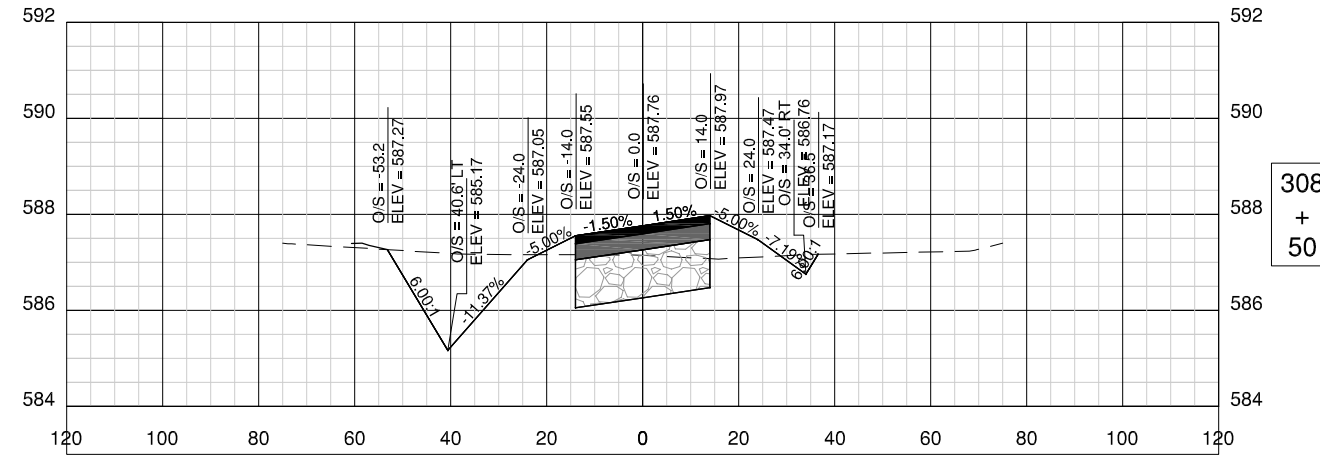
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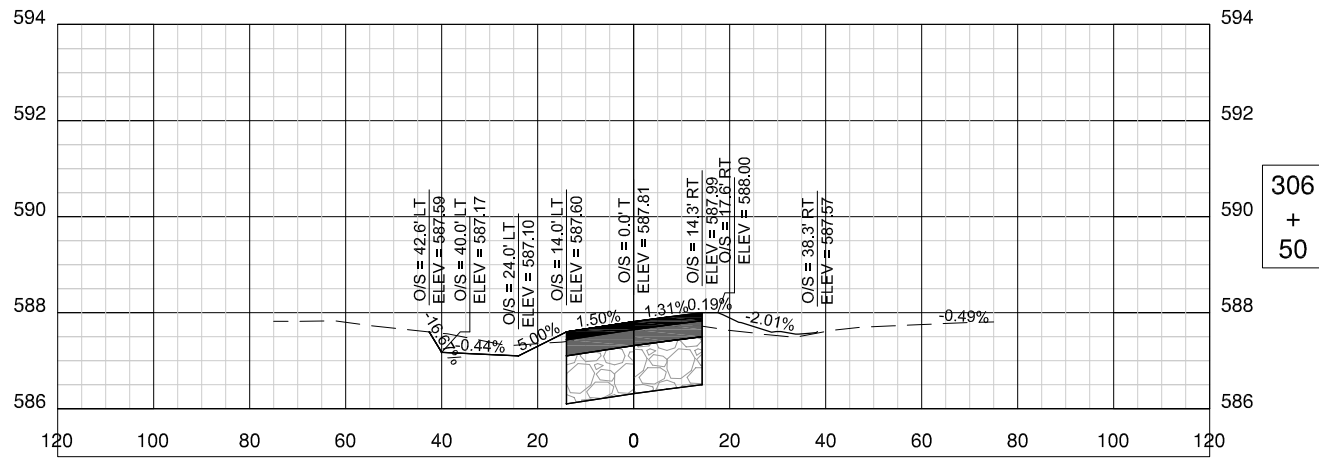
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| SHEET | 92 OF 106 SHEETS |



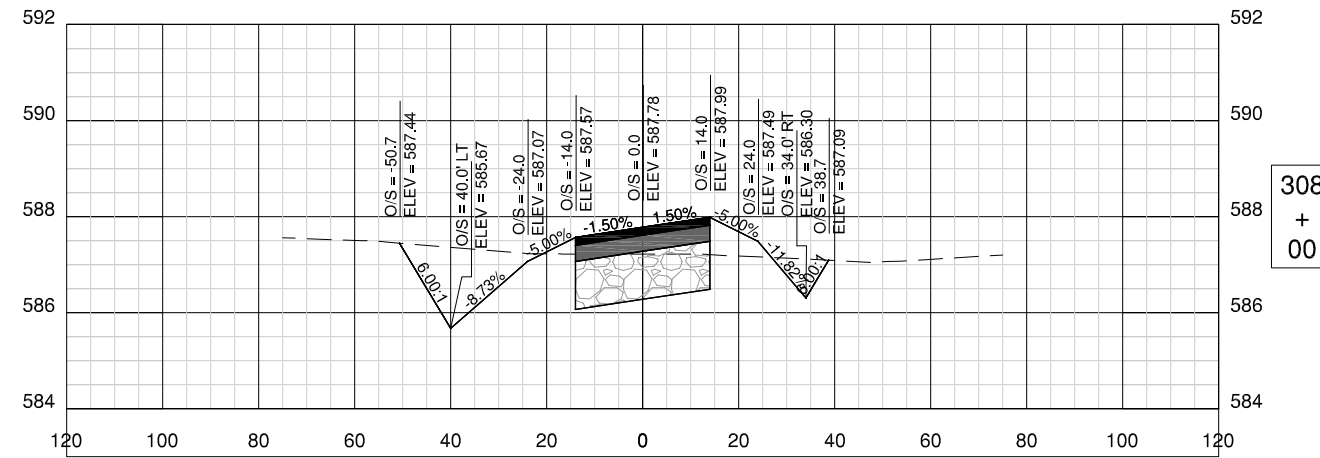
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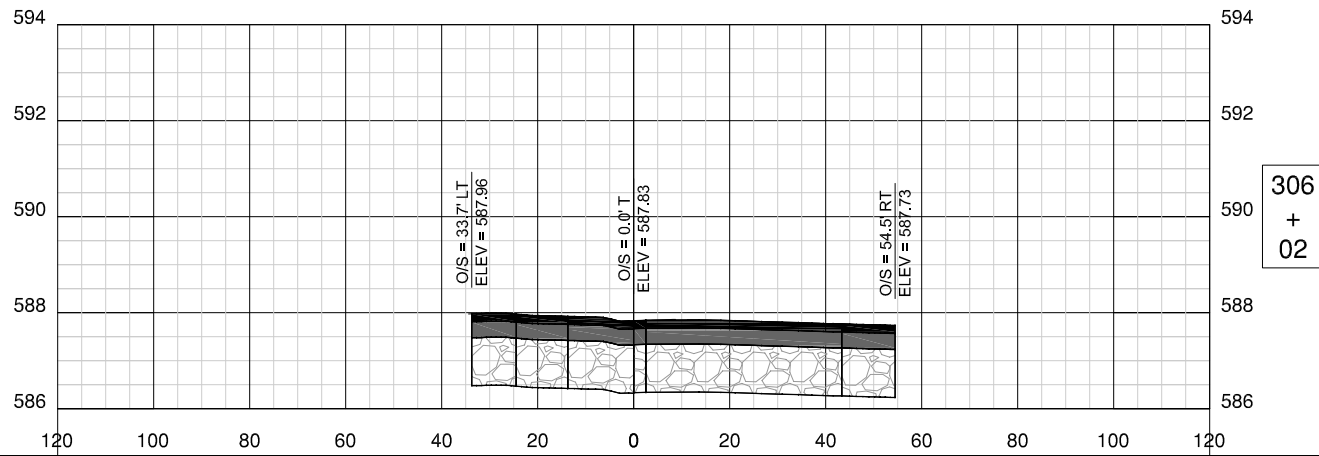
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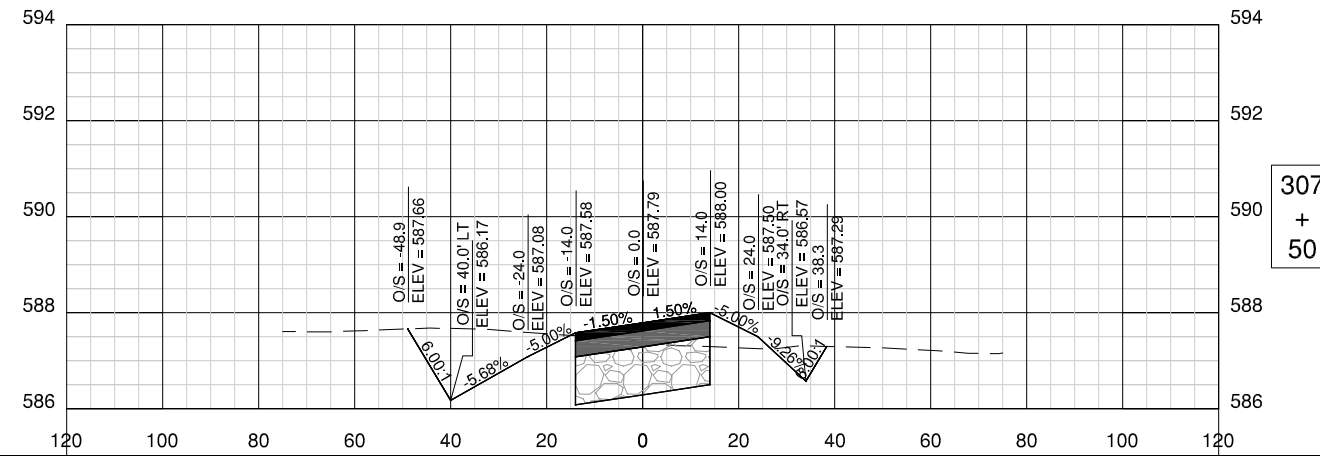
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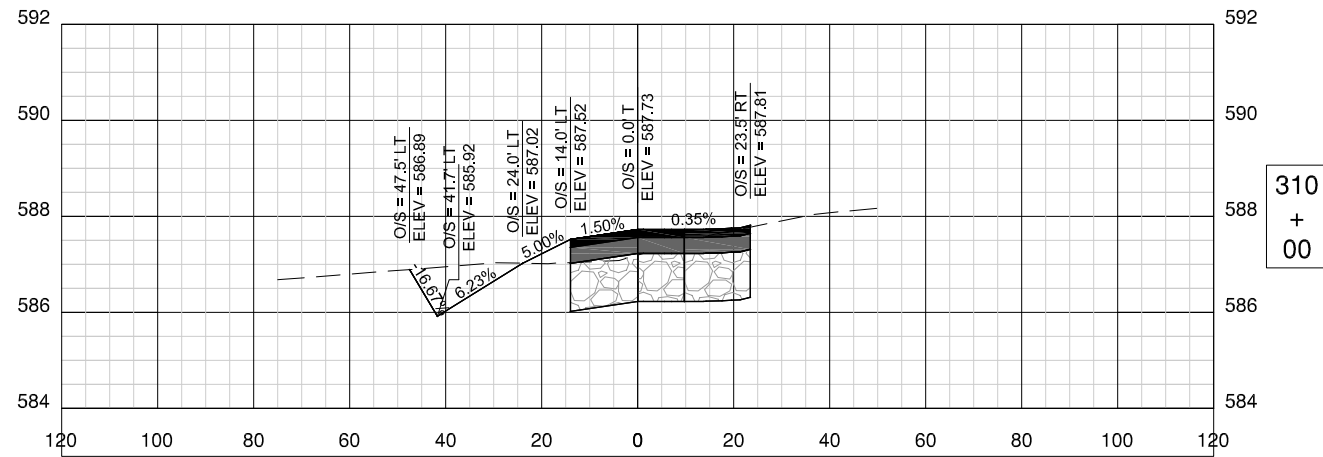
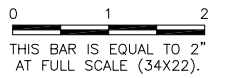


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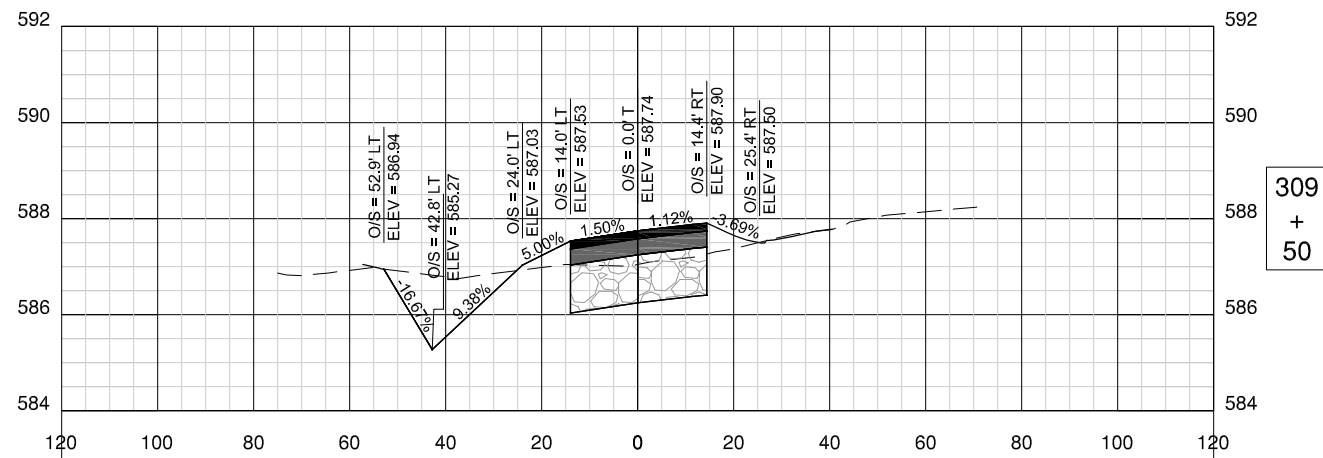
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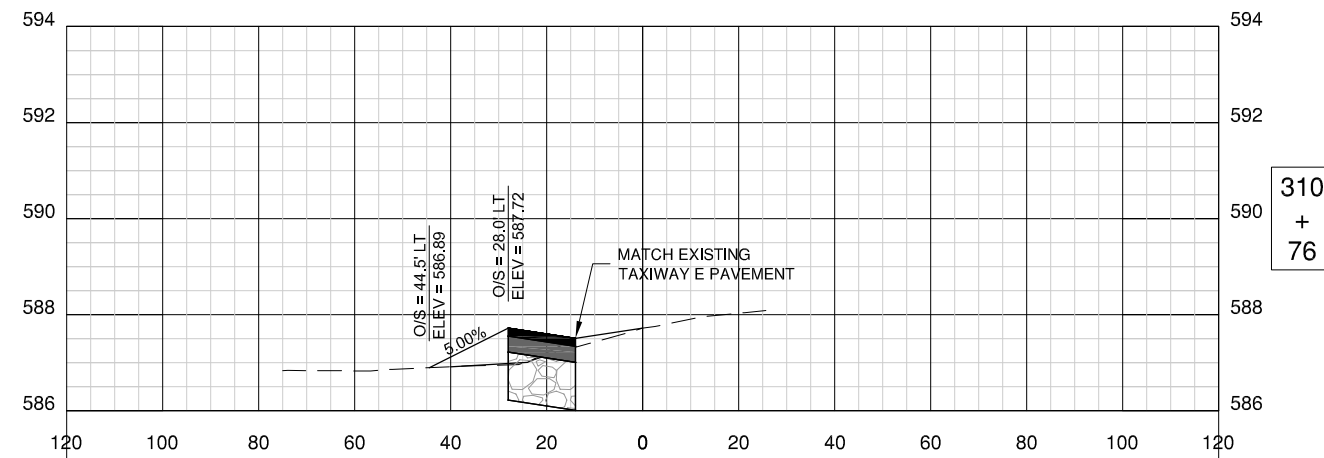
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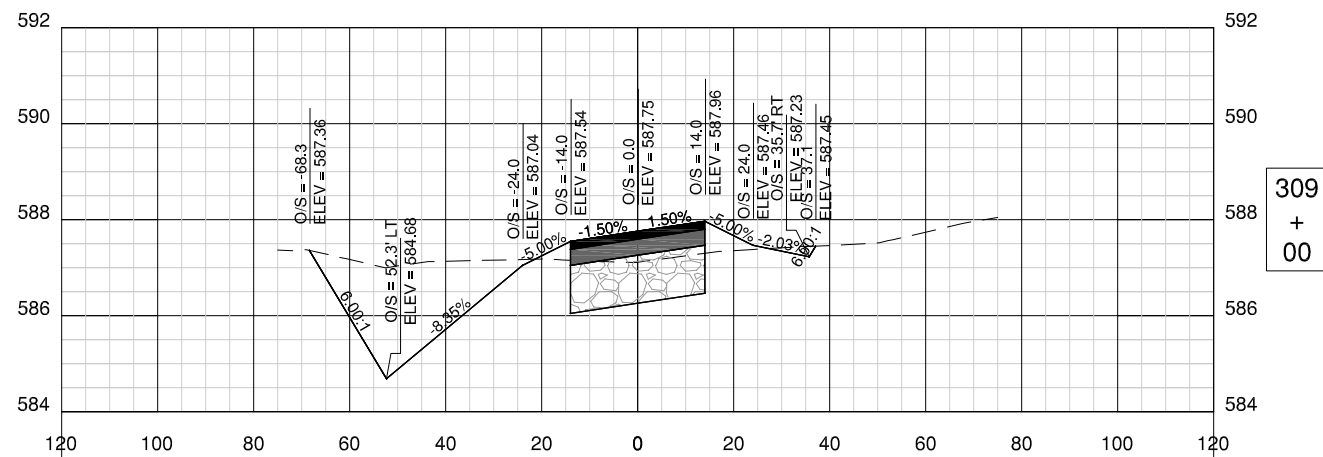
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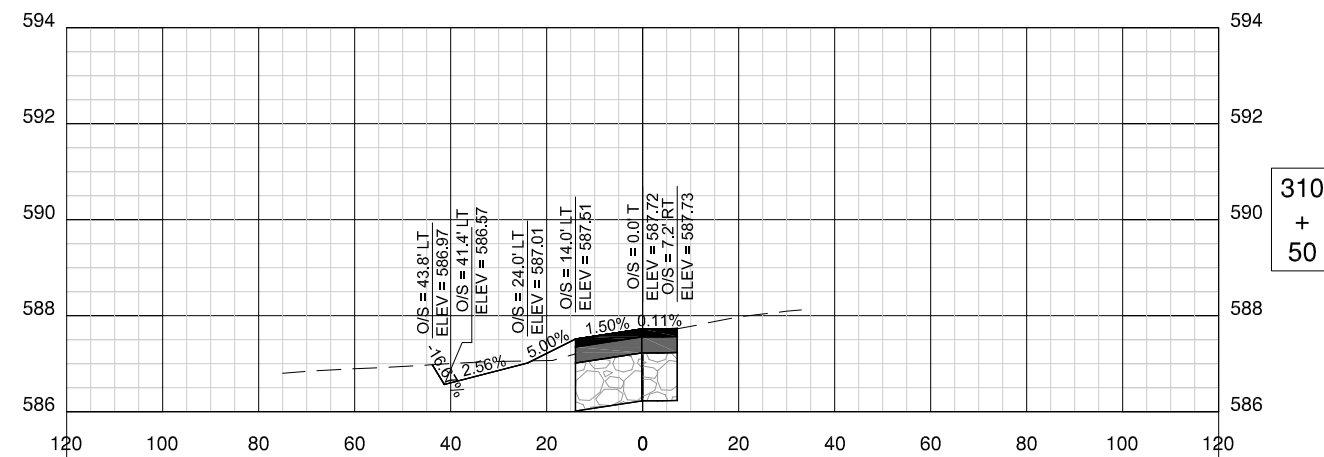
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EXTEND TAXIWAY Y
 ARFF ROAD CROSS SECTIONS 2

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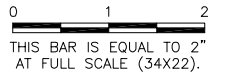


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 SPRINGFIELD, ILLINOIS**

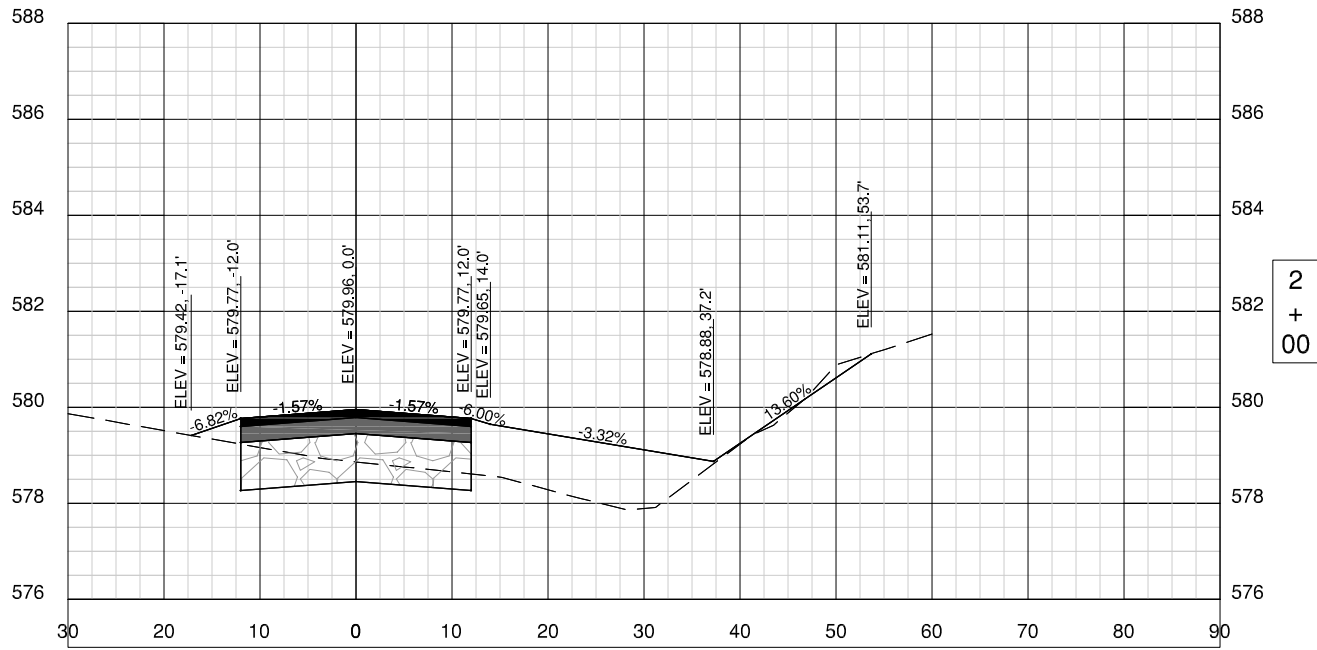
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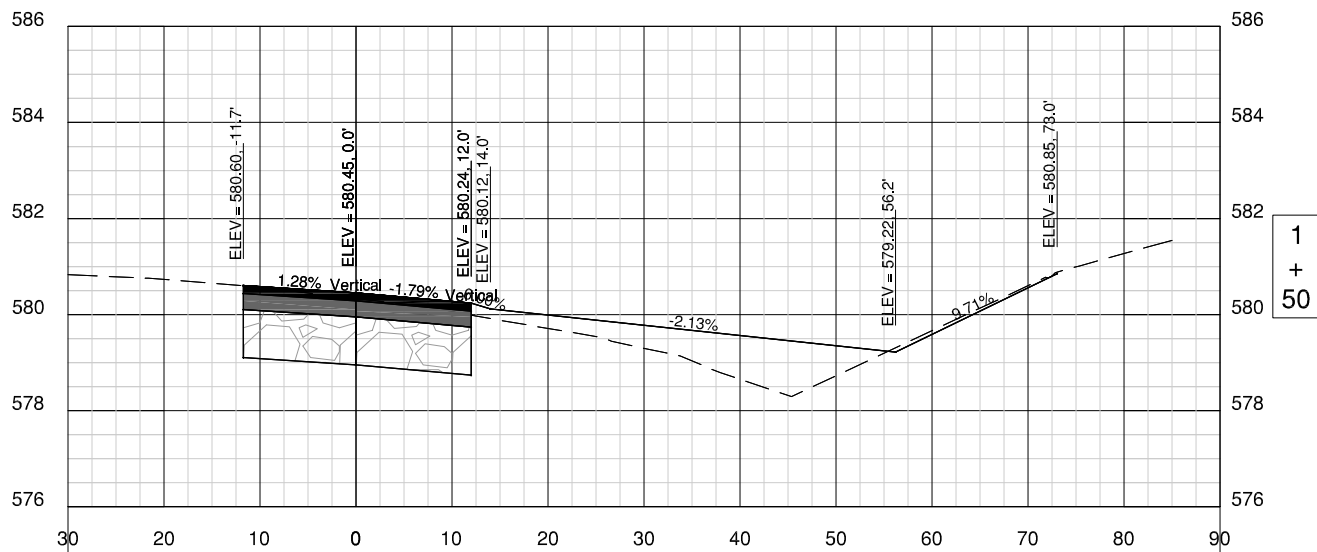


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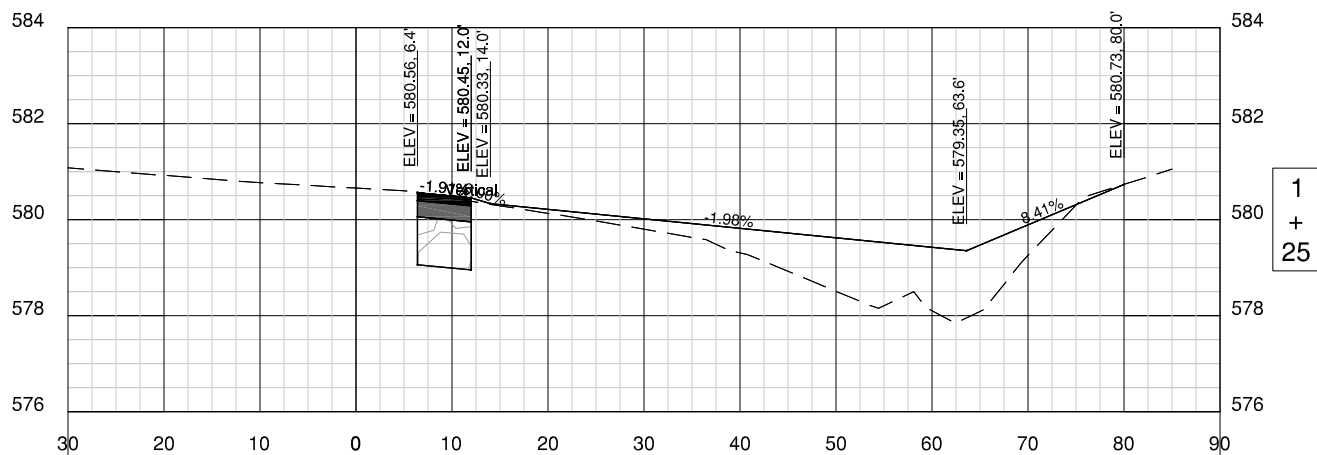
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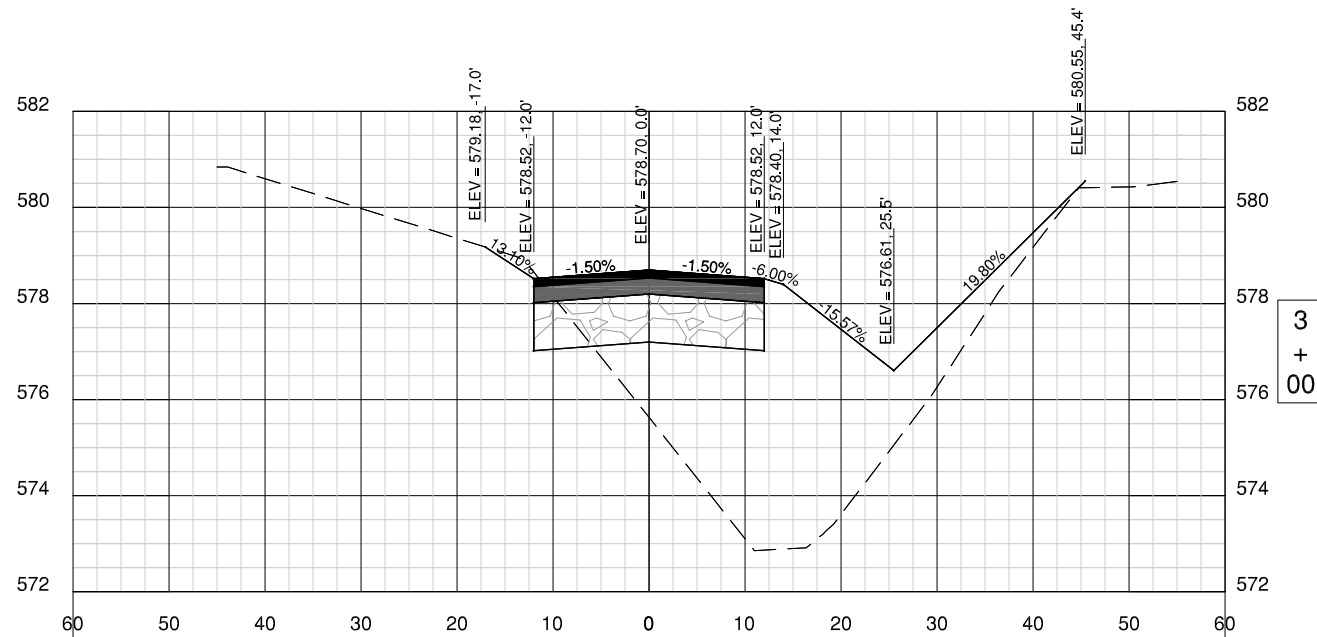
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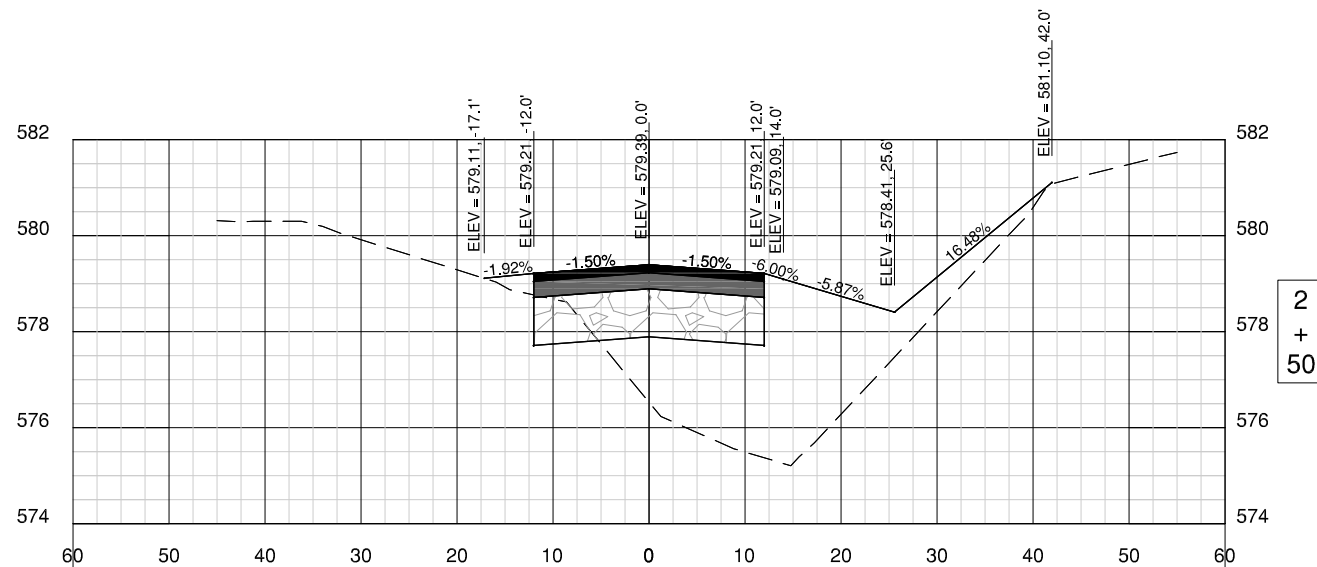
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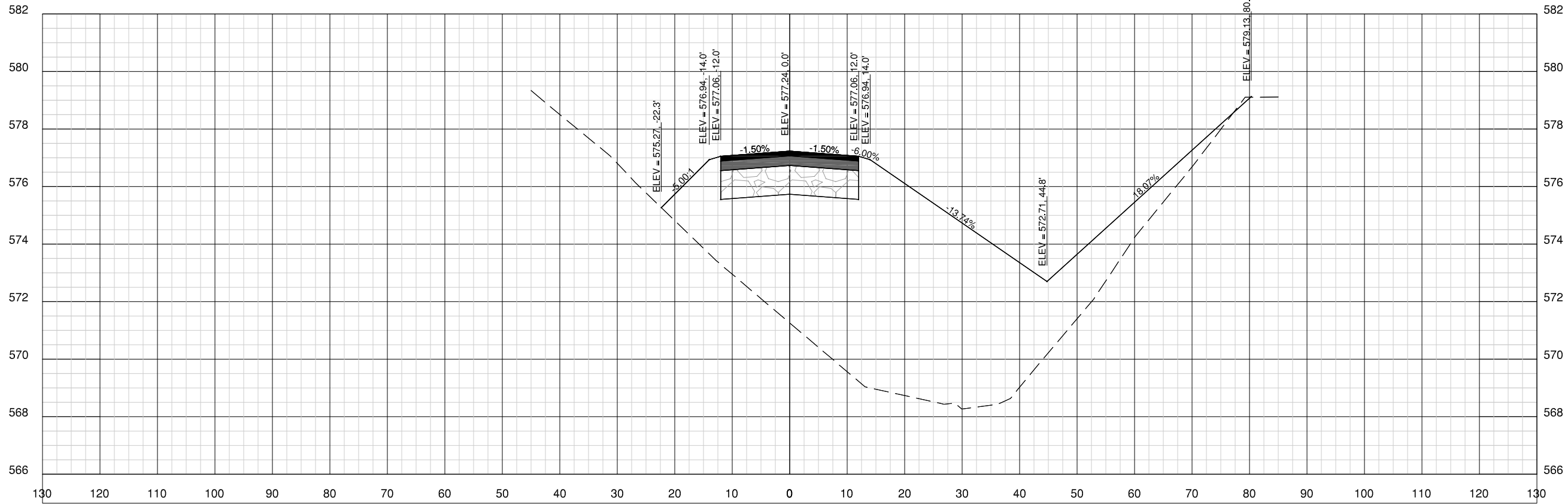
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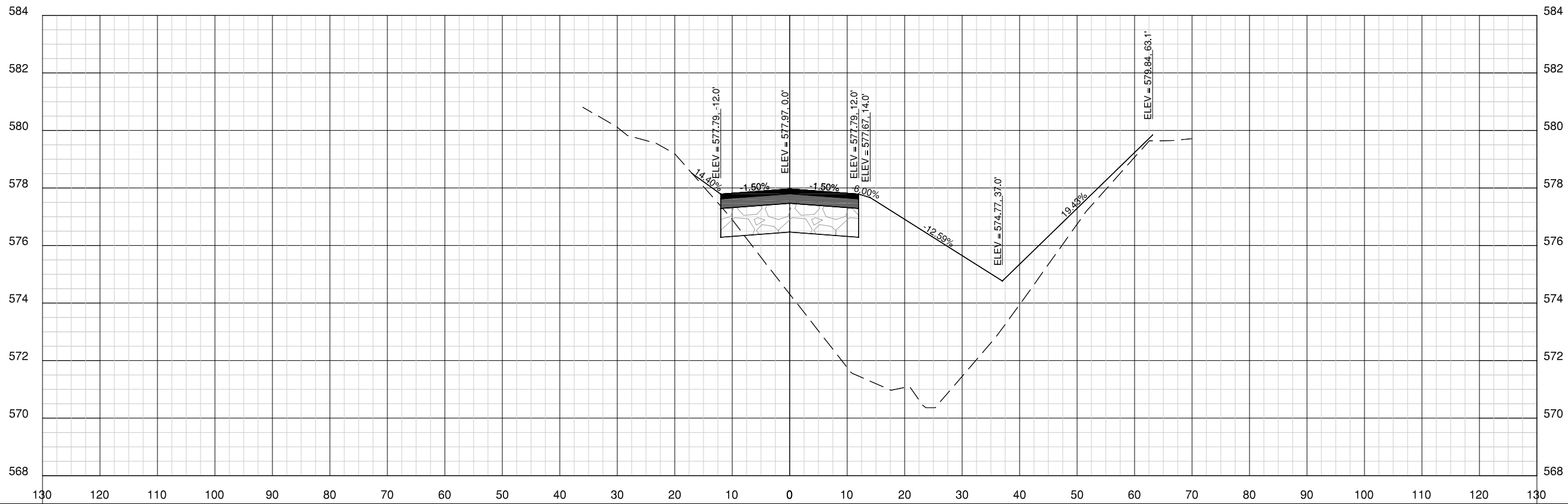
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EXTEND TAXIWAY Y
 SE PERIMETER ROAD CROSS SECTION - 2



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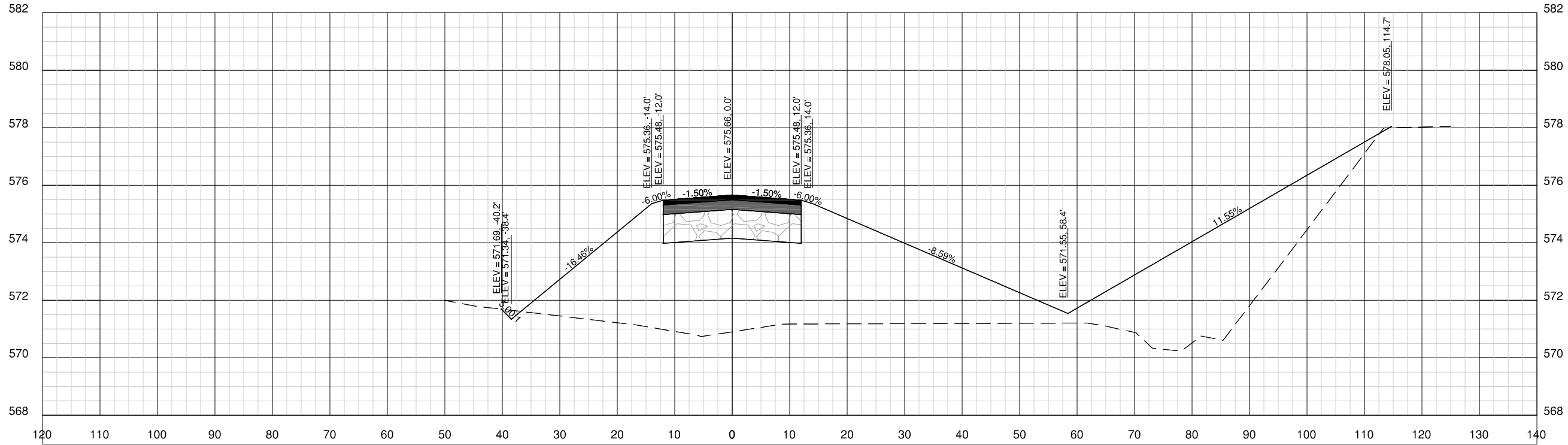
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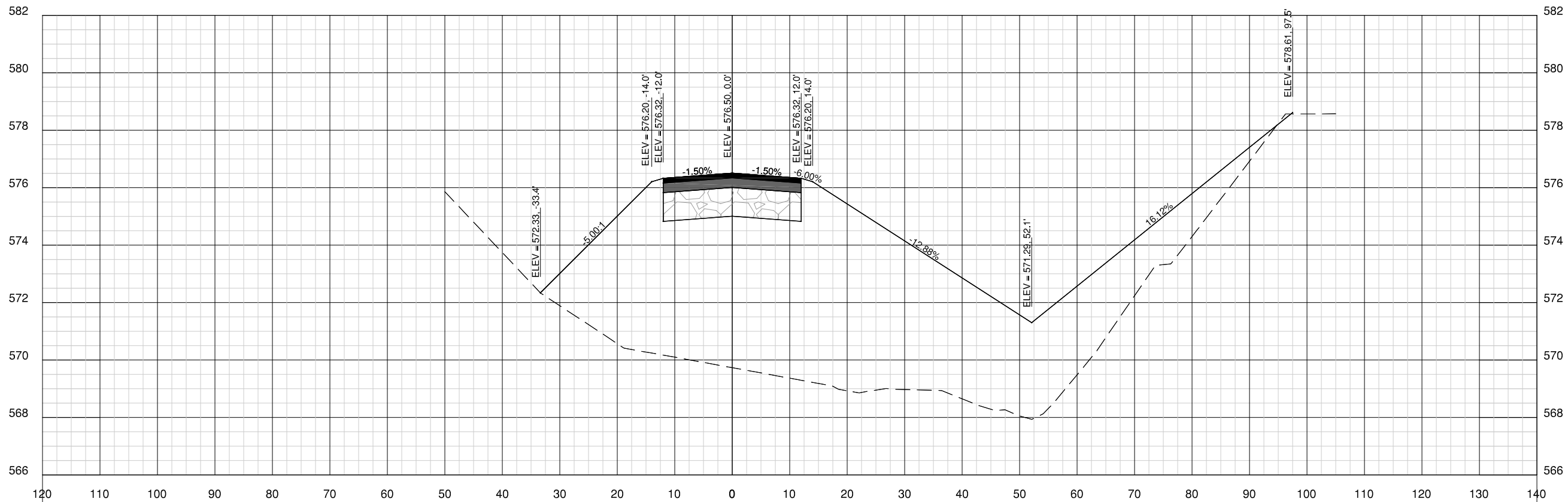
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EXTEND TAXIWAY Y
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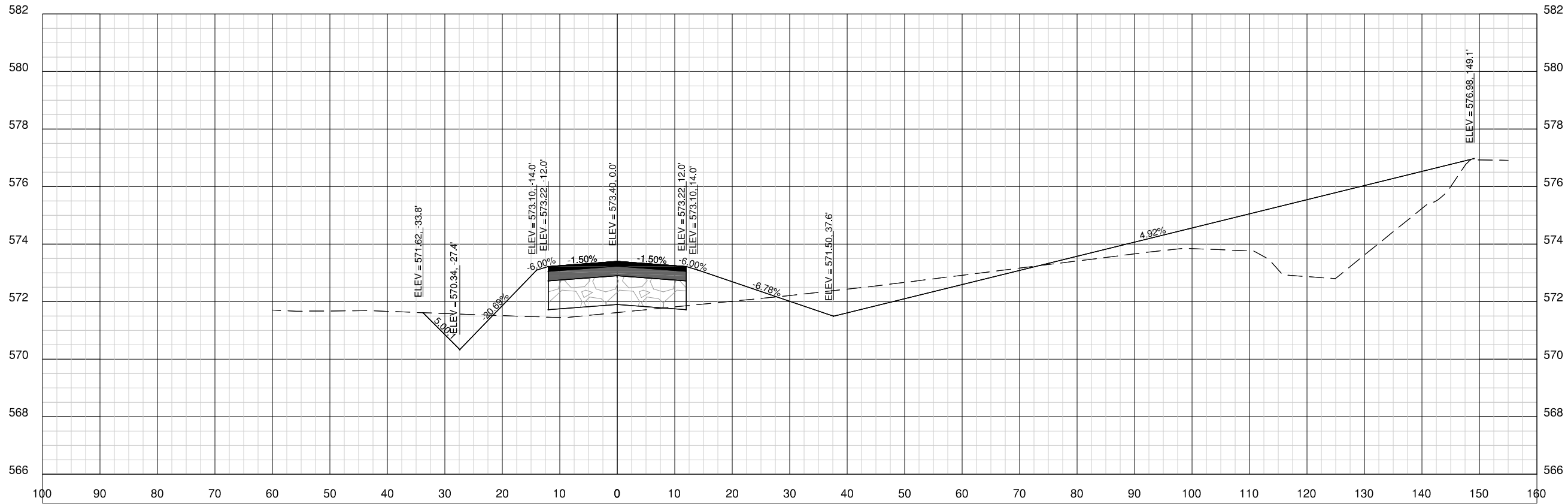
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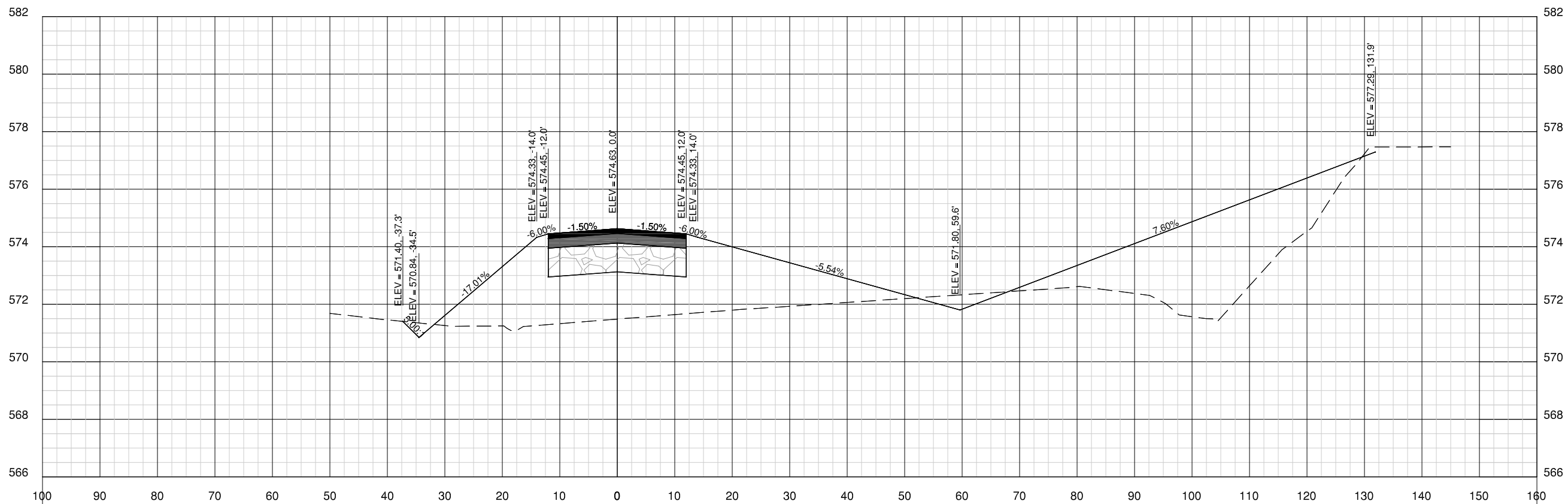
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 SPRINGFIELD, ILLINOIS

EXTEND TAXIWAY Y
 SE PERIMETER ROAD CROSS SECTION - 4

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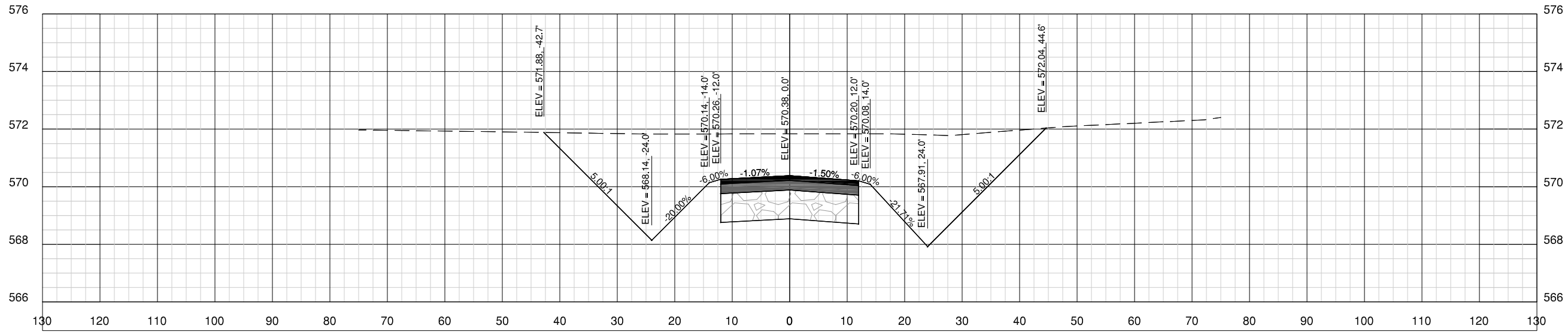
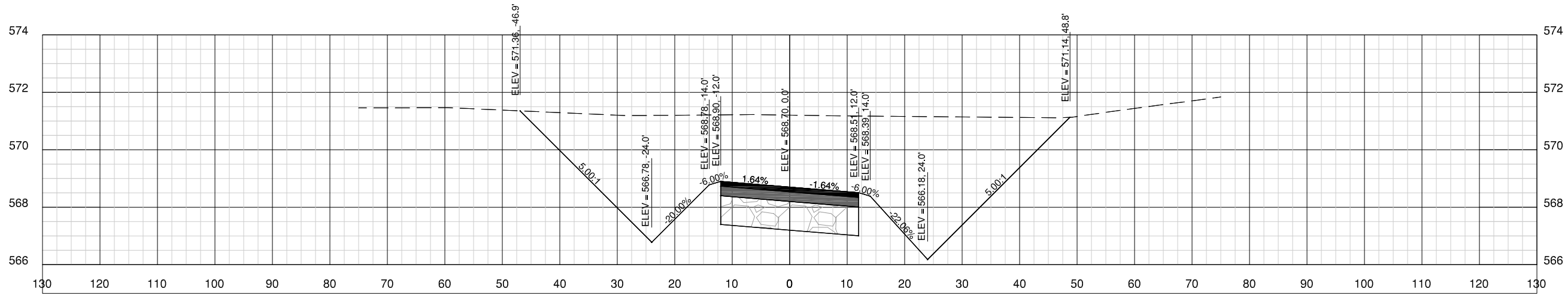
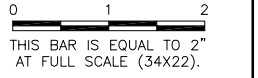
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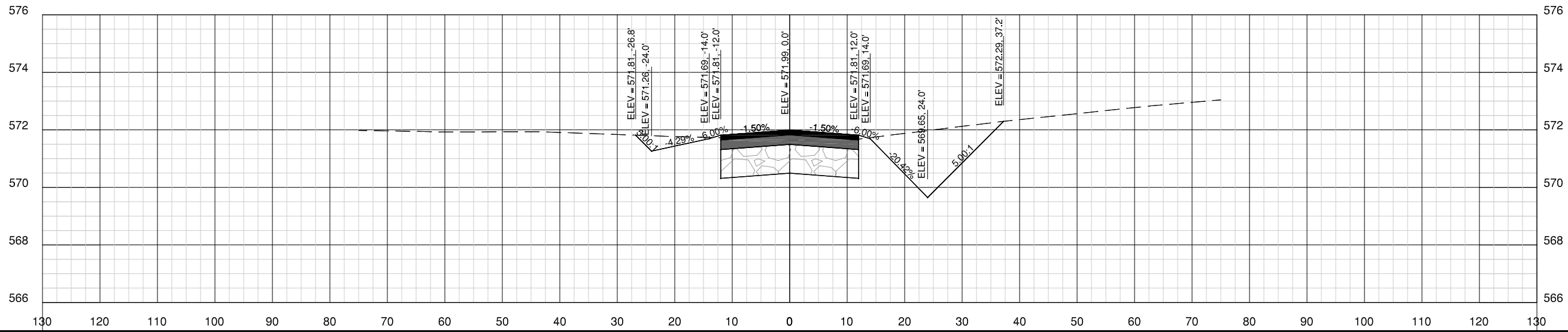
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 SPRINGFIELD, ILLINOIS

**EXTEND TAXIWAY Y
 SE PERIMETER ROAD CROSS SECTION - 5**



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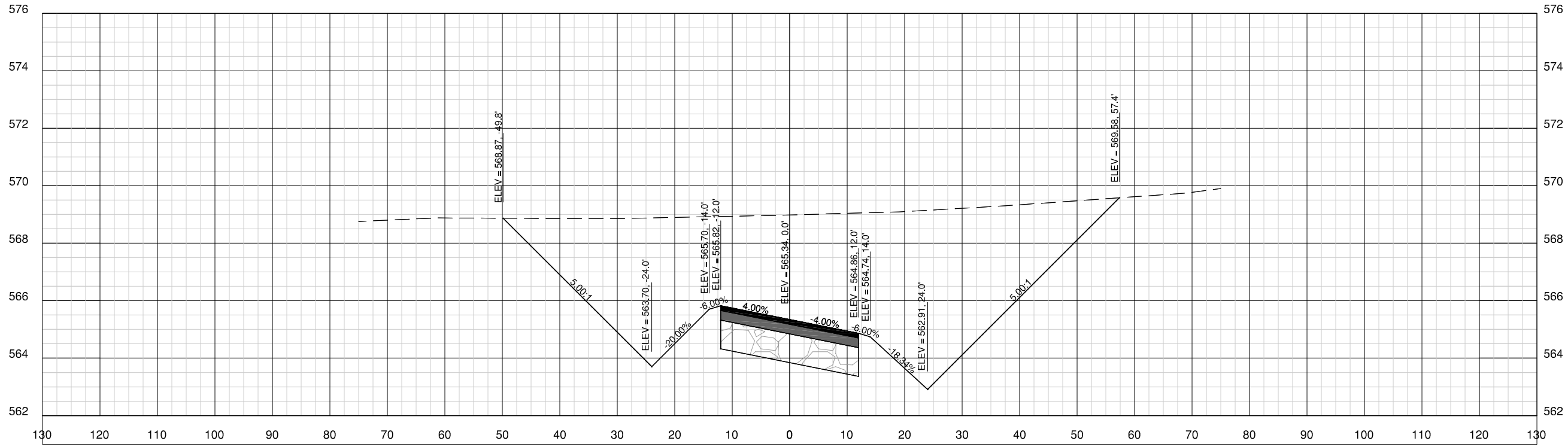
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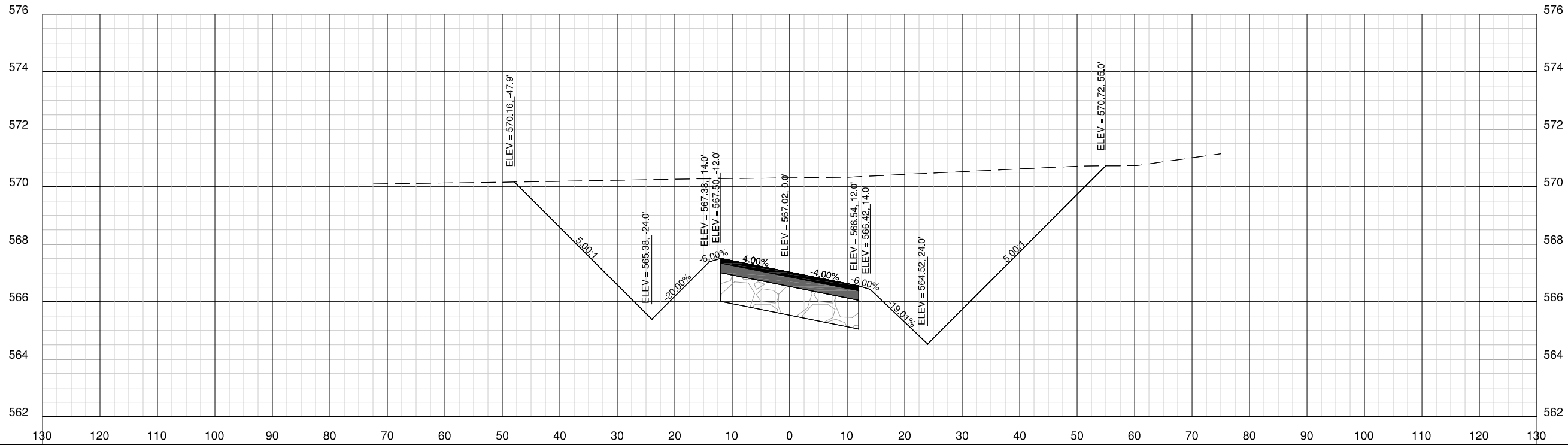
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EXTEND TAXIWAY Y
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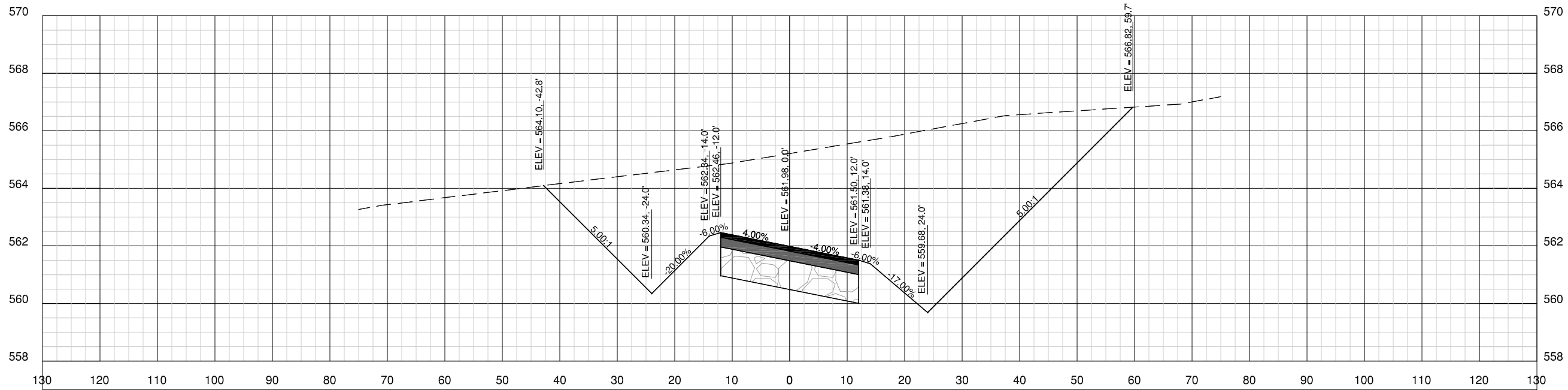
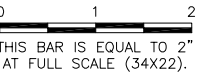
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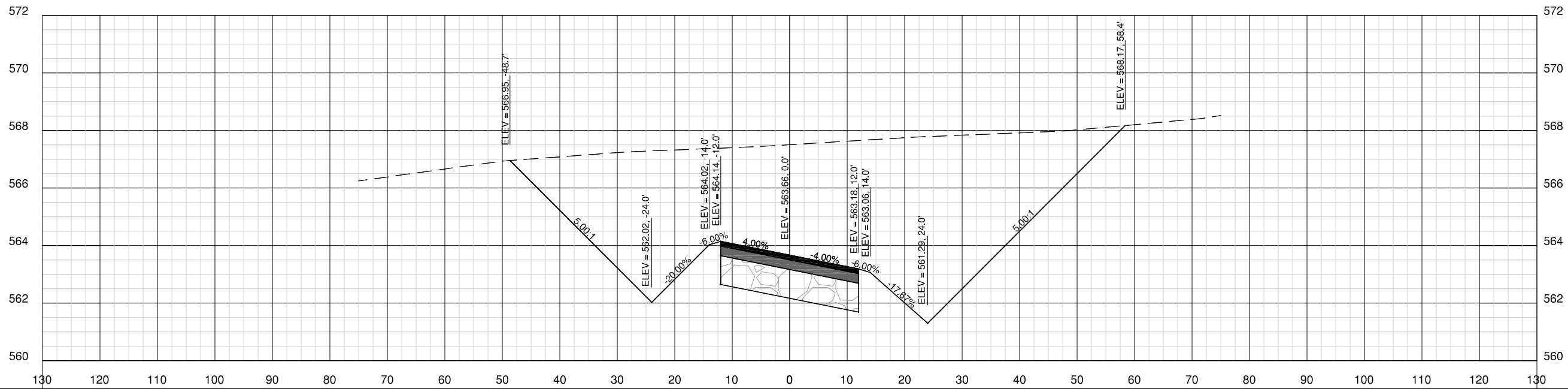
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SE PERIMETER ROAD CROSS SECTION - 7

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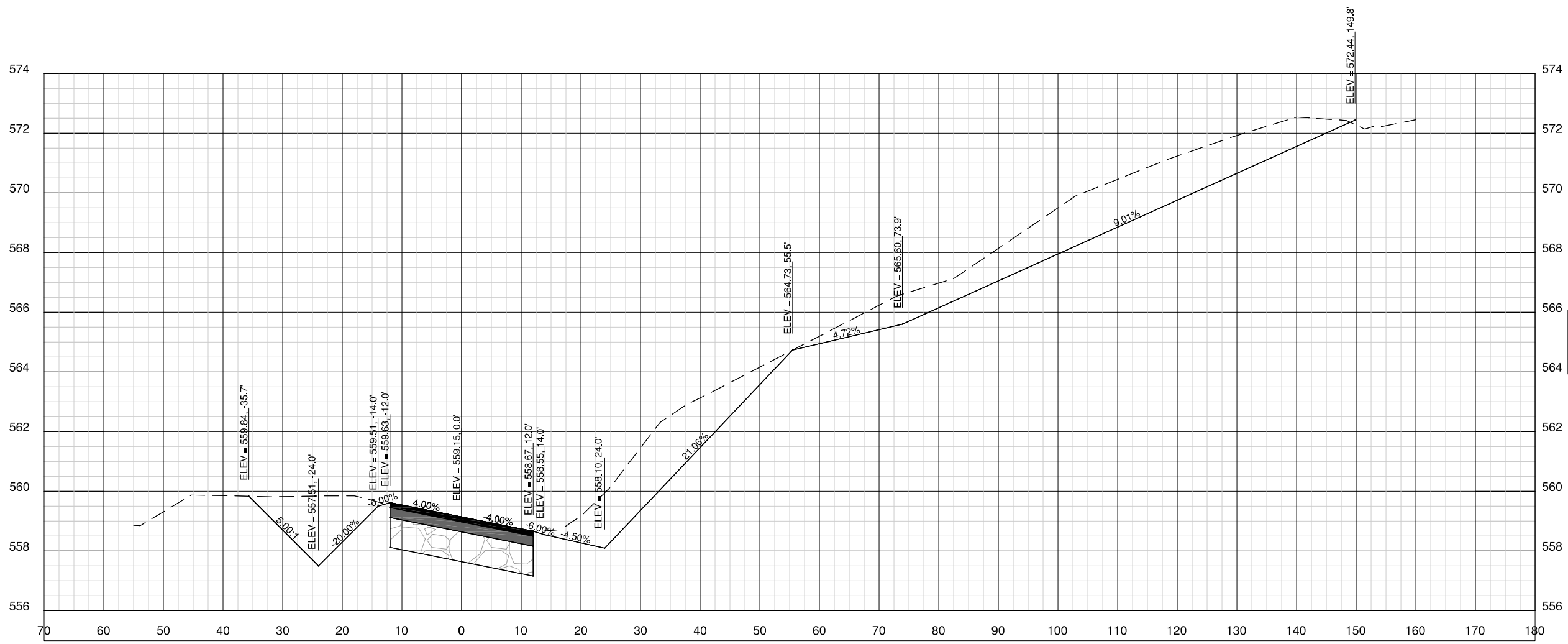
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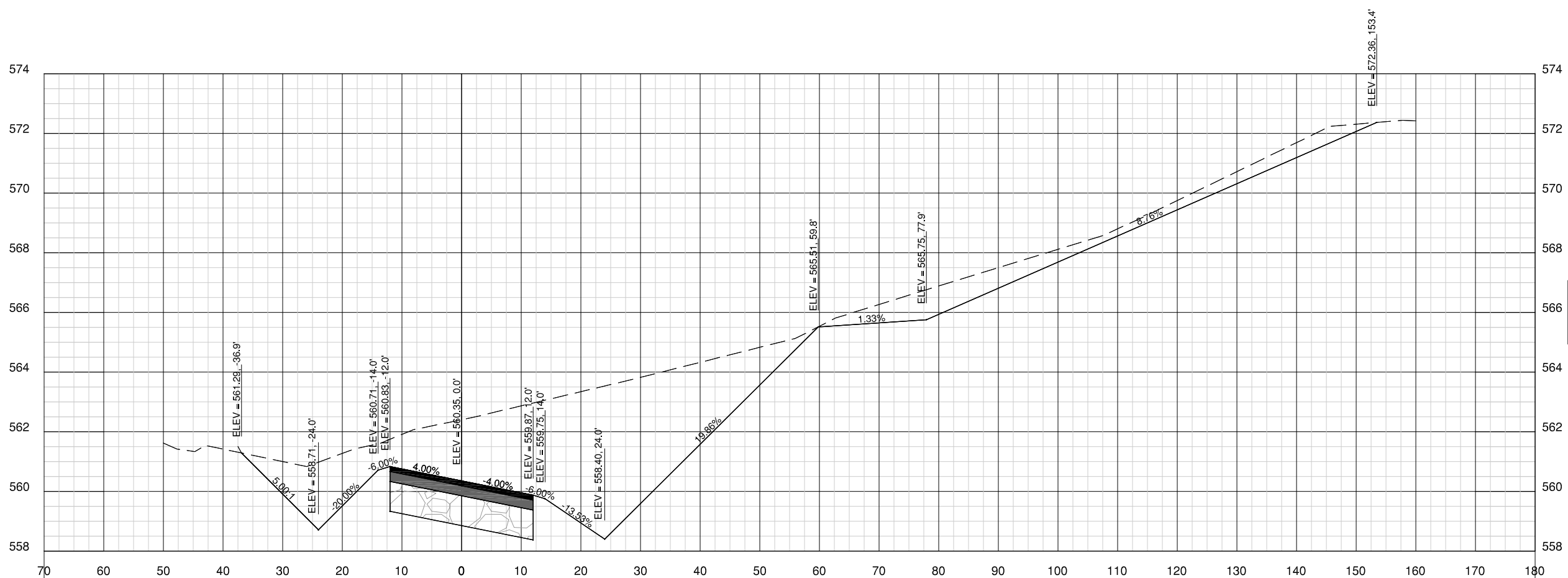
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**EXTEND TAXIWAY
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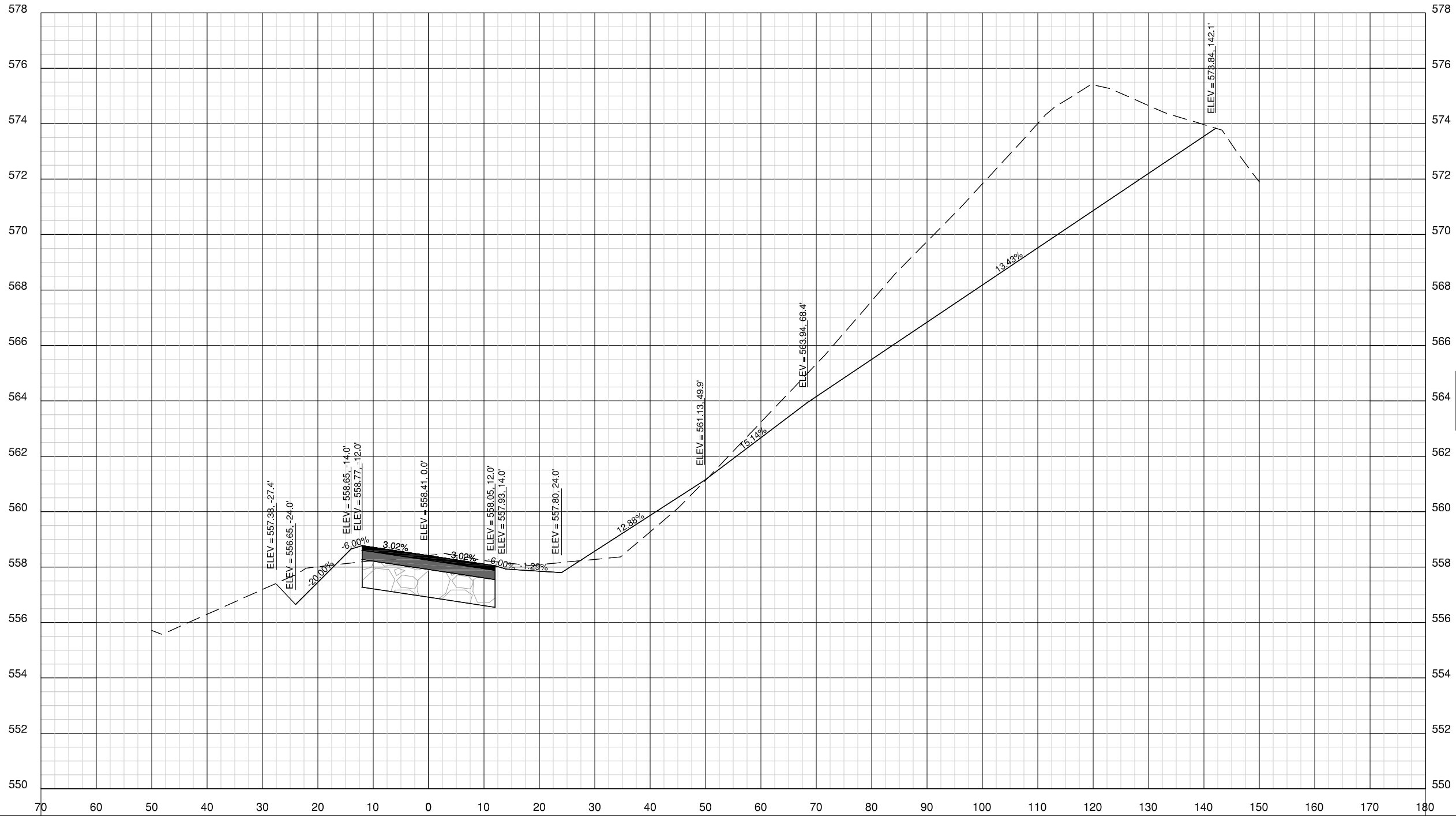
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EXTEND TAXIWAY Y
 SE PERIMETER ROAD CROSS SECTION - 9

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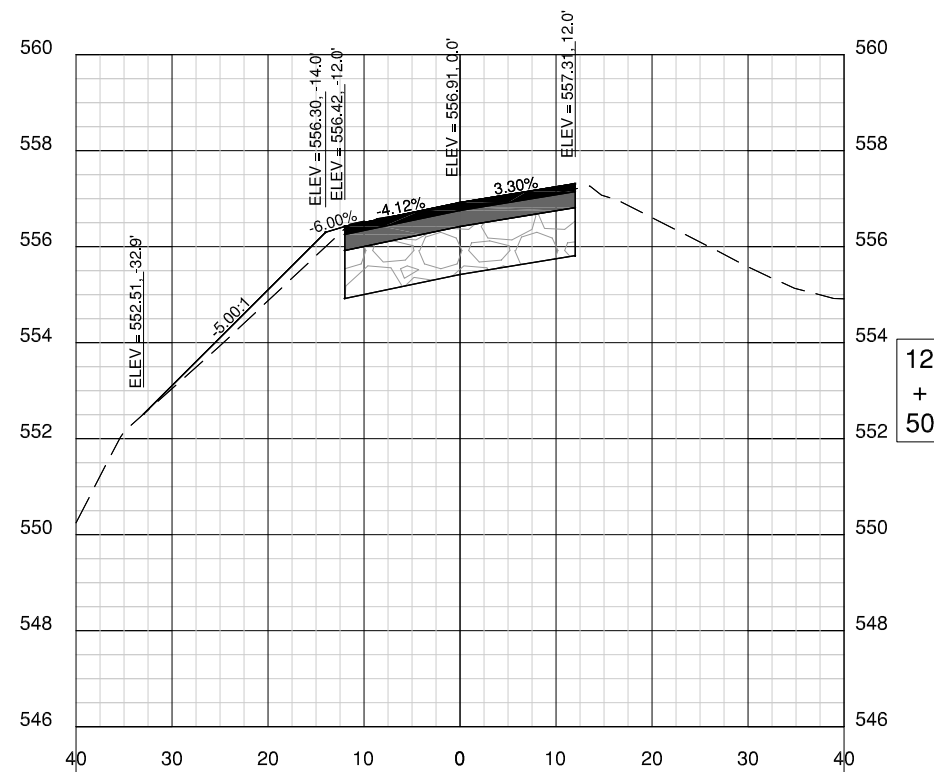
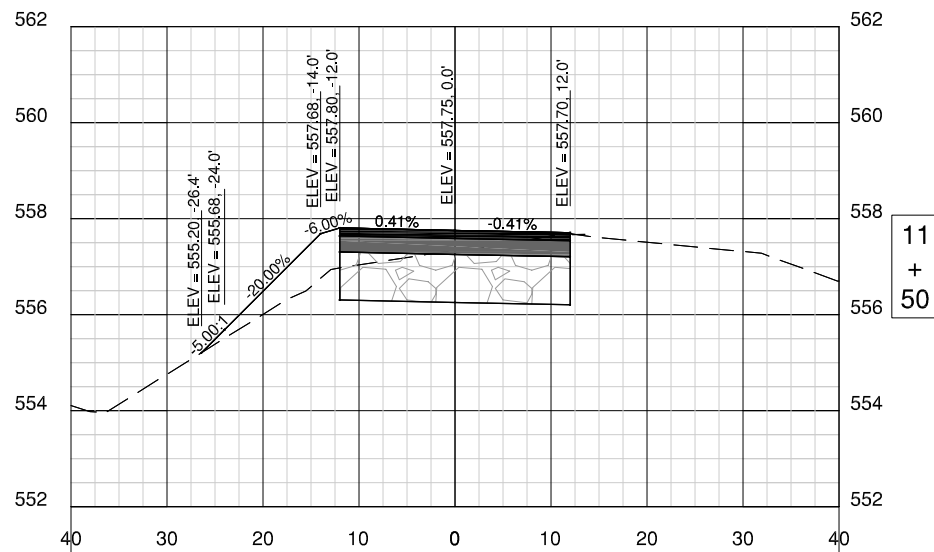
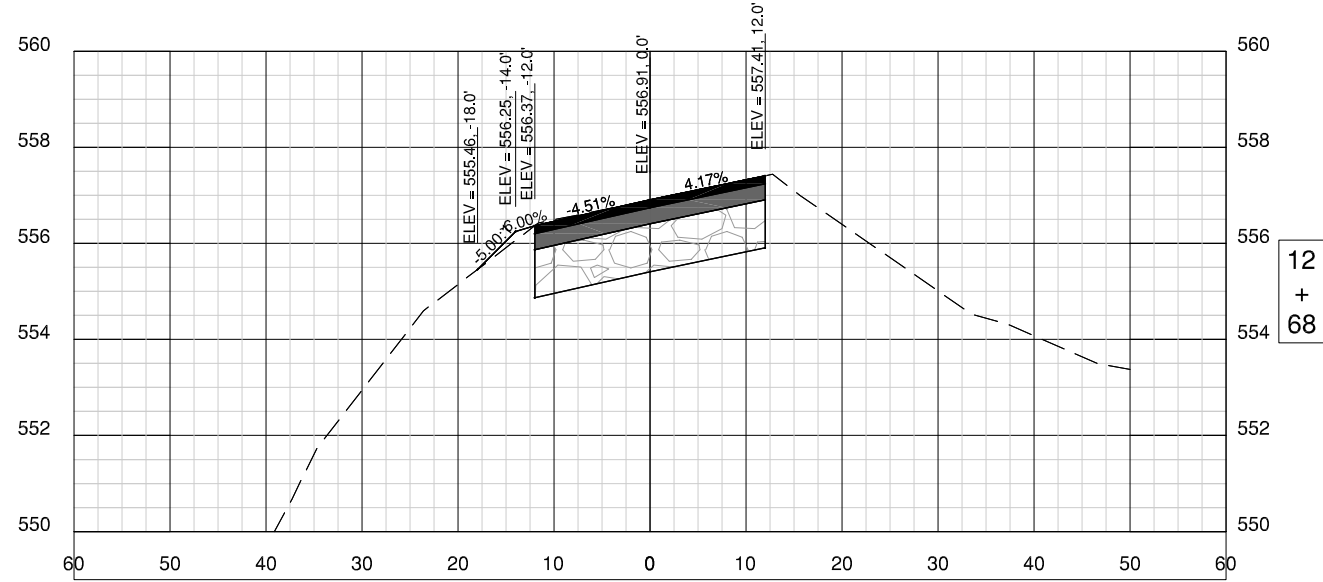
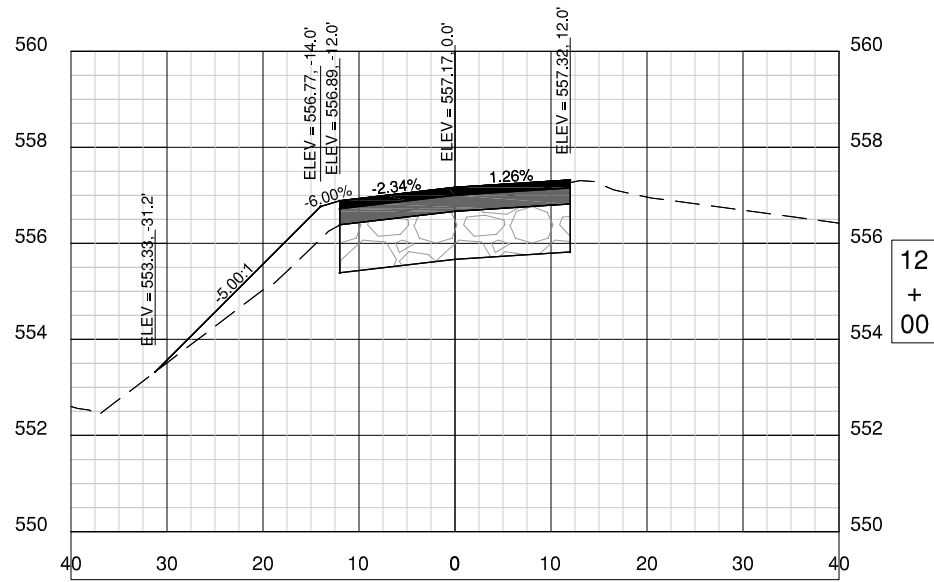
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 EXTEND TAXIWAY Y
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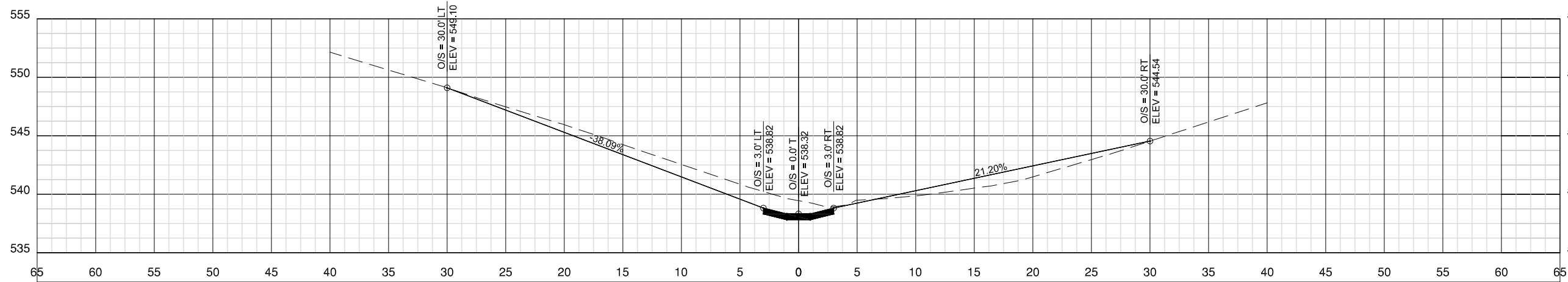
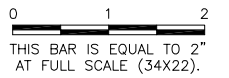
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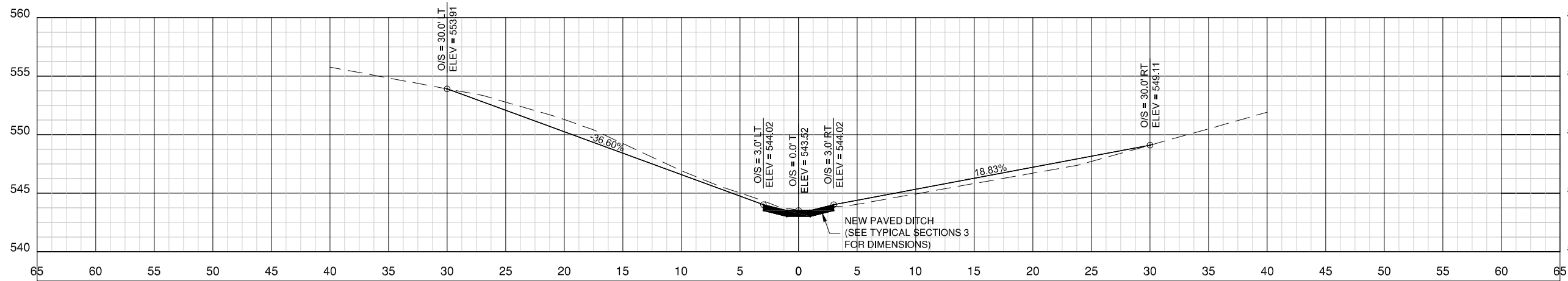
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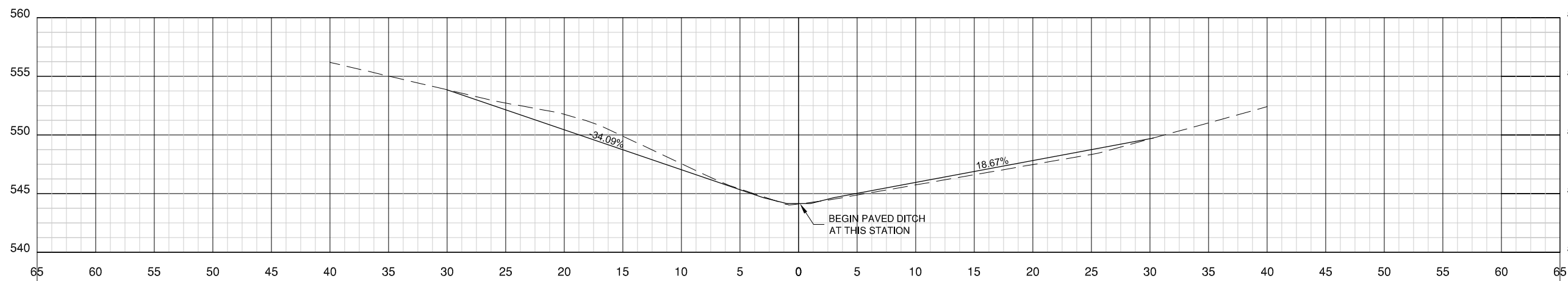
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EXTEND TAXIWAY Y
 DRAINAGE OUTFALL CROSS SECTIONS 1

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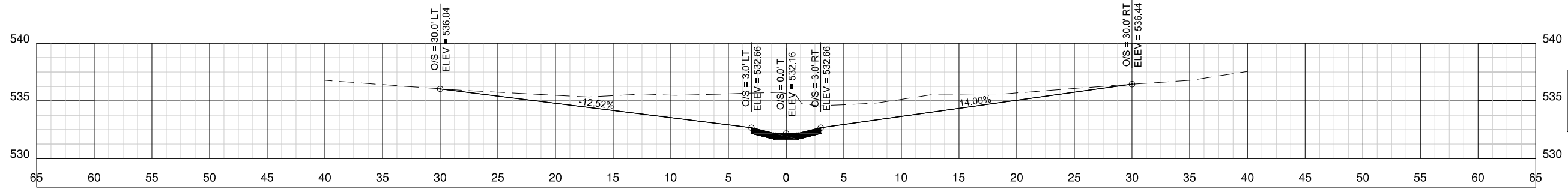
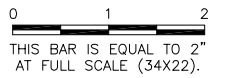
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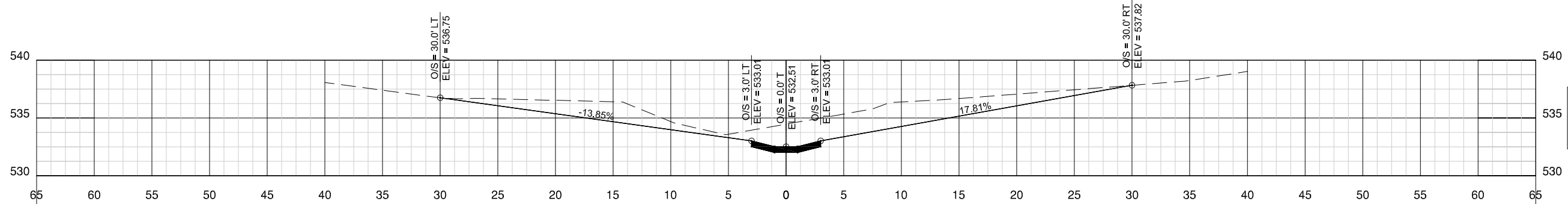
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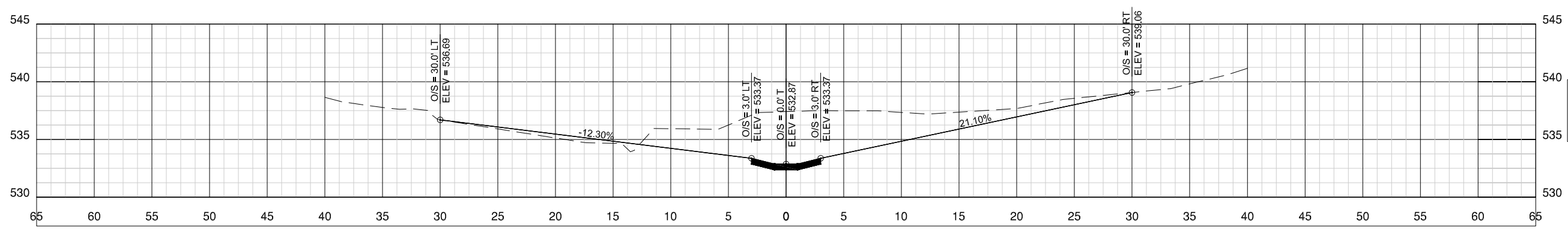
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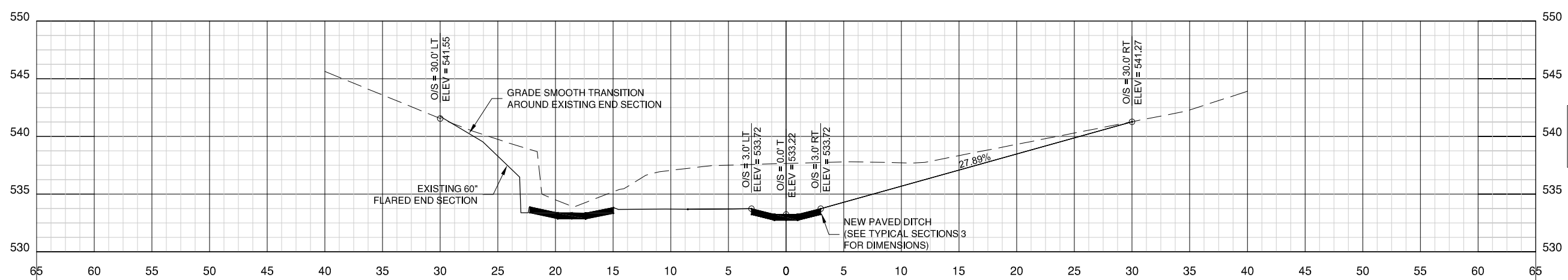
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EXTEND TAXIWAY Y

DRAINAGE OUTFALL CROSS SECTIONS 2

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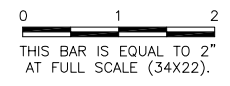


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