

**INDEX TO SHEETS**

**GENERAL**

1. COVER SHEET / INDEX TO SHEETS (CVR1)
2. SUMMARY OF QUANTITIES (QTY1)
3. SITE PLAN AND CONTROL (SPL1)
4. PHASING PLAN SUMMARY (PPM1)
5. PHASING PLAN NOTES (PPN1)
6. PHASING PLAN DETAILS (PPD1)
7. ONSITE BORROW / MILLING STOCKPILE (OBM1)
8. STORMWATER POLLUTION PREVENTION PLAN (SWP1)
9. STORMWATER POLLUTION PREVENTION NOTES (SWN1)
10. STORMWATER POLLUTION PREVENTION DETAILS (SWD1)

**REHABILITATE RWY 1/19**

11. TYPICAL SECTION 1/19 (TYP1)
12. TYPICAL SECTION TXY D - 1 (TYP2)
13. TYPICAL SECTION TXY D - 2 (TYP3)
- 14-15. EXISTING CONDITIONS AND REMOVALS (EXC1-2)
- 16-17. RUNWAY 1/19 PLAN / PROFILE (RPP1-2)
18. TAXIWAY D PLAN/PROFILE (DPP1)
19. RUNWAY 1/19 GRADING PLAN (RGP1)
20. TAXIWAY D GRADING PLAN (DGP1)
- 21-22. JOINTING PLAN (JTP1-2)
23. JOINTING DETAILS (JTD1)
- 24-25. ELECTRICAL AND MARKING PLAN (EMP1-2)
26. HOMERUN PLAN (HRP1)
27. GROOVING PLAN AND DETAILS (GPD1)
28. ENGINEERING INFORMATION (ENG1)

**CONSTRUCT TXY F SHOULDERS AND WIDEN TXY M AND K**

29. GENERAL NOTES AND LEGENDS TXY'S F, K & M (GNL1)
30. TYPICAL SECTIONS TXY'S F, K & M (TYP4)
- 31-33. EXISTING CONDITIONS AND REMOVALS TXY'S F, K & M - 1 (EXC3-5)
- 34-36. PAVING AND DRAINAGE PLAN TXY'S F, K & M - 1 (PDP1-3)
- 37-39. GRADING PLAN TXY'S F, K & M - 1 (GDP1-3)
- 40-41. LIGHTING AND MARKING PLAN TXY'S F, K & M - 1 (LMP1-2)
- 42-43. ELECTRICAL PLAN TXY'S F, K & M - 1 (ELP1-2)
44. INDEX TO CROSS SECTIONS TXY'S F, K, M (INX1)
- 45-48. CROSS SECTIONS TXY'S F, K, & M (XSC1-4)

**DETAILS**

49. DRAINAGE AND UNDERDRAIN DETAILS (SSD1)
50. STORM SEWER INLET ADJUSTMENT DETAIL (SSD2)
51. STORM SEWER MANHOLE ADJUSTMENT DETAIL (SSD3)
52. SANITARY SEWER MANHOLE ADJUSTMENT DETAIL (SND1)
- 53-58. ELECTRICAL DETAILS (ELD1-6)
- 59-61. MARKING DETAILS (MRD1-3)

**ADD ALT # 1 AND #2**

62. SITE PLAN AND CONTROL (ASP1)
63. TYPICAL SECTION (ATY1)
64. EXISTING CONDITIONS AND REMOVALS (AEX1)
65. RUNWAY 1/19 PLAN AND PROFILE ADD ALT #1 (APP1)
66. RUNWAY 1/19 PLAN AND PROFILE ADD ALT #2 (APP2)
67. RUNWAY 1/19 GRADING PLAN (AGP1)
68. JOINTING PLAN (AJT1)
69. ELECTRICAL / MARKING AND LANDSCAPING (AEM1)
70. GROOVING PLAN (AGP1)

# GREATER ROCKFORD AIRPORT AUTHORITY ROCKFORD, ILLINOIS

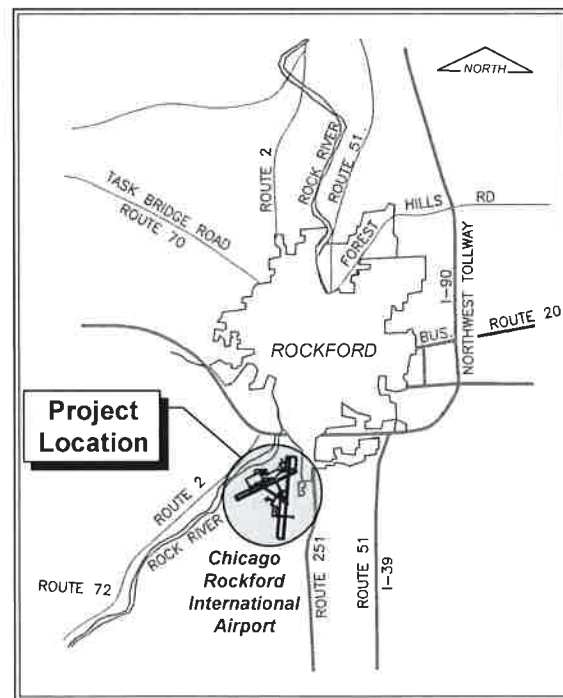
## CONSTRUCTION PLANS FOR CHICAGO ROCKFORD INTERNATIONAL AIRPORT

### REHABILITATE RUNWAY 1/19 PHASE 2

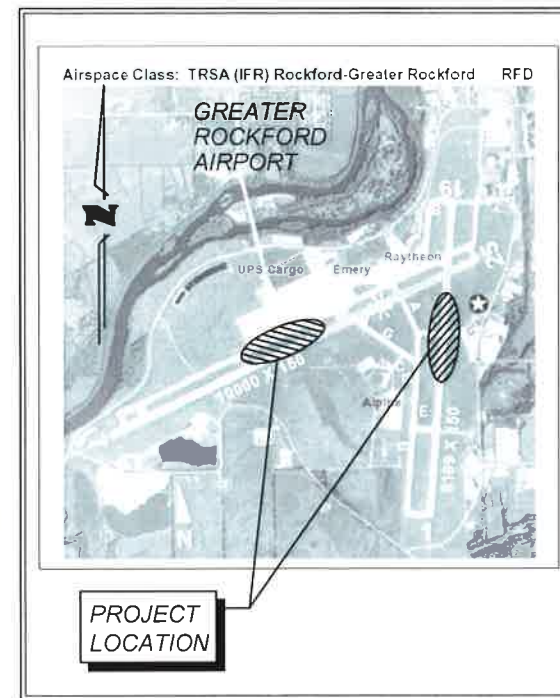
**FINAL SUBMITTAL**

**ILLINOIS PROJECT: RFD-4183  
A.I.P. PROJECT: 3-17-0088-XX**

**APRIL 20, 2012  
(JUNE 15, 2012 LETTING)**




**LOCATION MAP**



**SITE PLAN**



Greater Rockford Airport Authority

APPROVED BY   
**MIKE DUNN**  
DIRECTOR

DATE APRIL 20, 2012

**DESIGN INFORMATION**

TOWNSHIP: 43 NORTH COUNTY: WINNEBAGO  
RANGE 1 EAST ROCKFORD TOWNSHIP  
ALP DATA: APPROACH CAT. / DESIGN GROUP: D5  
DESIGN AIRCRAFT: 747-400  
(MAXIMUM EQUIPMENT HEIGHT = 25')



**CALL J.U.L.I.E BEFORE  
EXCAVATING  
1-800-892-0123**

Illinois Professional Engineering Practice Act of 1989



SUBMITTED BY   
**Travis A. Strait, P.E.**

DATE 4/20/12  
license expires 11-30-2013

# SUMMARY OF QUANTITIES

RO020



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION  
PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
**SUMMARY OF  
QUANTITIES**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **QTY1** REVISION

SHEET 2 OF 70 SHEETS

SEQ NO	ITEM NO	DESCRIPTION	UNIT	BASE BID - RUNWAY 1/19 REHABILITATION (PHASE 2)		BASE BID - CONSTRUCT TAXIWAY F SHOULDERS		ADDITIVE ALTERNATE #1 - ADDITIONAL RUNWAY 1/19 REHABILITATION		ADDITIVE ALTERNATE #2 - ADDITIONAL RUNWAY 1/19 REHABILITATION	
				QUANTITY (F/S/L)	RECORD QUANTITY (AR)	QUANTITY (F/S/L)	TOTAL COST (AR)	QUANTITY (F/S/L)	RECORD QUANTITY (AS)	QUANTITY (F/S/L)	RECORD QUANTITY (AT)
1	AR108108	1/C #8 5 KV UG CABLE	LF	10,260.00		4,155.00		1,610.00		1,620.00	
2	AR108158	1/C #8 5 KV UG CABLE IN UD	LF	650.00		6,140.00					
3	AR110501	1-WAY CONC. ENCASED DUCT	LF	400.00		2,900.00					
4	AR110550	SPLIT DUCT	LF			120.00					
5	AR110554	EXTEND 4-WAY DUCT	LF			160.00					
6	AR110610	ELECTRICAL HANDHOLE	EACH	2.00		4.00					
7	AR110906	REMOVE ELECTRICAL HANDHOLE	EACH	1.00							
8	AR125415	MITL-BASE MOUNTED	EACH			6.00					
9	AR125525	HIRL, INPAVEMENT	EACH			2.00					
10	AR125565	SPLICE CAN	EACH			2.00					
11	AR125902	REMOVE BASE MOUNTED LIGHT	EACH			2.00					
12	AR125916	RELOCATE RGL	EACH			3.00					
13	AR125942	ADJUST BASE MOUNTED LIGHT	EACH	28.00				2.00			
14	AR125943	ADJUST INPAVEMENT LIGHT	EACH	40.00				5.00		8.00	
15	AR125946	ADJUST SPLICE CAN	EACH	2.00						1.00	
16	AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	2.00		35.00				5.00	
17	AR125963	RELOCATE INPAVEMENT LIGHT	EACH	1.00							
18	AR125964	RELOCATE TAXI GUIDANCE SIGN	EACH	2.00		11.00					
19	AR150510	ENGINEER'S FIELD OFFICE	LS	1.00							
20	AR150515	FIELD LABORATORY	LS	1.00							
21	AR150520	MOBILIZATION	LS	1.00							
22	AR152410	UNCLASSIFIED EXCAVATION	CY			5,468.00				1,000.00	
23	AR152441	ON-SITE BORROW	CY			4,500.00					
24	AR152540	SOIL STABILIZATION FABRIC	SY			13,180.00					
25	AR156510	SILT FENCE	LF	3,650.00		1,000.00					
26	AR156512	BALES	EACH	56.00		72.00					
27	AR208515	POROUS GRANULAR EMBANKMENT	CY			100.00					
28	AR209608	CRUSHED AGG. BASE COURSE - 8"	SY			9,475.00				1,200.00	
29	AR209612	CRUSHED AGG. BASE COURSE - 12"	SY			3,705.00					
30	AR401610	BITUMINOUS SURFACE COURSE	TON	5,170.00		2,445.00		455.00		200.00	
31	AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1.00							
32	AR401640	BITUMINOUS PAVEMENT GROOVING	SY	5,500.00							
33	AR401900	REMOVE BITUMINOUS PAVEMENT	SY	1,425.00		770.00		270.00		4,300.00	
34	AR401915	REM & REPLACE BIT PAVEMENT - TYPE A	SY	115.00		40.00					
35	AR401916	REM & REPLACE BIT PAVEMENT - TYPE B	SY	115.00							
36	AR403610	BITUMINOUS BASE COURSE	TON	350.00		2,750.00		65.00		473.00	
37	AR403630	BITUMINOUS BASE TEST SECTION	EACH			1.00					
38	AR501520	20" PCC PAVEMENT	SY	28,675.00				5,000.00		6,000.00	
39	AR501530	PCC TEST BATCH	EACH	1.00							
40	AR501540	PCC PAVEMENT GROOVING	SY	23,200.00				4,350.00		5,200.00	
41	AR602510	BITUMINOUS PRIME COAT	GAL			6,590.00				825.00	
42	AR603510	BITUMINOUS TACK COAT	GAL	4,650.00		3,850.00		350.00		420.00	
43	AR620520	PAVEMENT MARKING - WATERBORNE	SF	24,050.00		11,500.00		2,400.00		1,980.00	
44	AR620525	PAVEMENT MARKING - BLACK BORDER	SF	17,360.00		12,500.00		2,040.00		2,000.00	
45	AR620900	PAVEMENT MARKING REMOVAL	SF			13,945.00					
46	AR701518	18" RCP, CLASS IV	LF			70.00					
47	AR705506	6" PERFORATED UNDERDRAIN	LF	3,200.00		2,318.00		600.00		720.00	
48	AR705635	UNDERDRAIN COLLECTION STRUCTURE	EACH			1.00					
49	AR705640	UNDERDRAIN CLEANOUT	EACH			11.00					
50	AR705900	REMOVE UNDERDRAIN	LF	3,200.00		2,389.00		600.00		720.00	
51	AR705904	REMOVE UNDERDRAIN CLEANOUT	EACH			5.00					
52	AR705905	REMOVE COLLECTION STRUCTURE	EACH			1.00					
53	AR751411	INLET - TYPE A	EACH			3.00					
54	AR751550	MANHOLE 5'	EACH			2.00					
55	AR751900	REMOVE INLET	EACH			3.00					
56	AR751940	ADJUST INLET	EACH			2.00					
57	AR751943	ADJUST MANHOLE	EACH			1.00					
58	AR770905	REMOVE SANITARY MANHOLE	EACH	1.00							
59	AR770945	ADJUST SANITARY MANHOLE	EACH	1.00							
60	AR800850	2 - 1/C #2 XLP-USE, 1/C #8 GND IN UD	LF			1,700.00					
61	AR800856	17" BITUMINOUS PAVEMENT MILLING	SY	28,700.00				5,000.00		6,000.00	
62	AR800857	2" BITUMINOUS PAVEMENT MILLING	SY	4,200.00							
63	AR800858	0-2" BITUMINOUS PAVEMENT MILLING	SY	6,150.00							
64	AR800860	ADJUST PAVEMENT SENSOR	EACH							1.00	
65	AR901510	SEEDING	ACRE	3.00		3.30		0.49		1.27	
66	AR905530	TOPSOILING	SY	14,000.00							
67	AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	3.00		3.30		0.49		1.27	

**SUMMARY OF QUANTITIES  
(QTY1)**





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

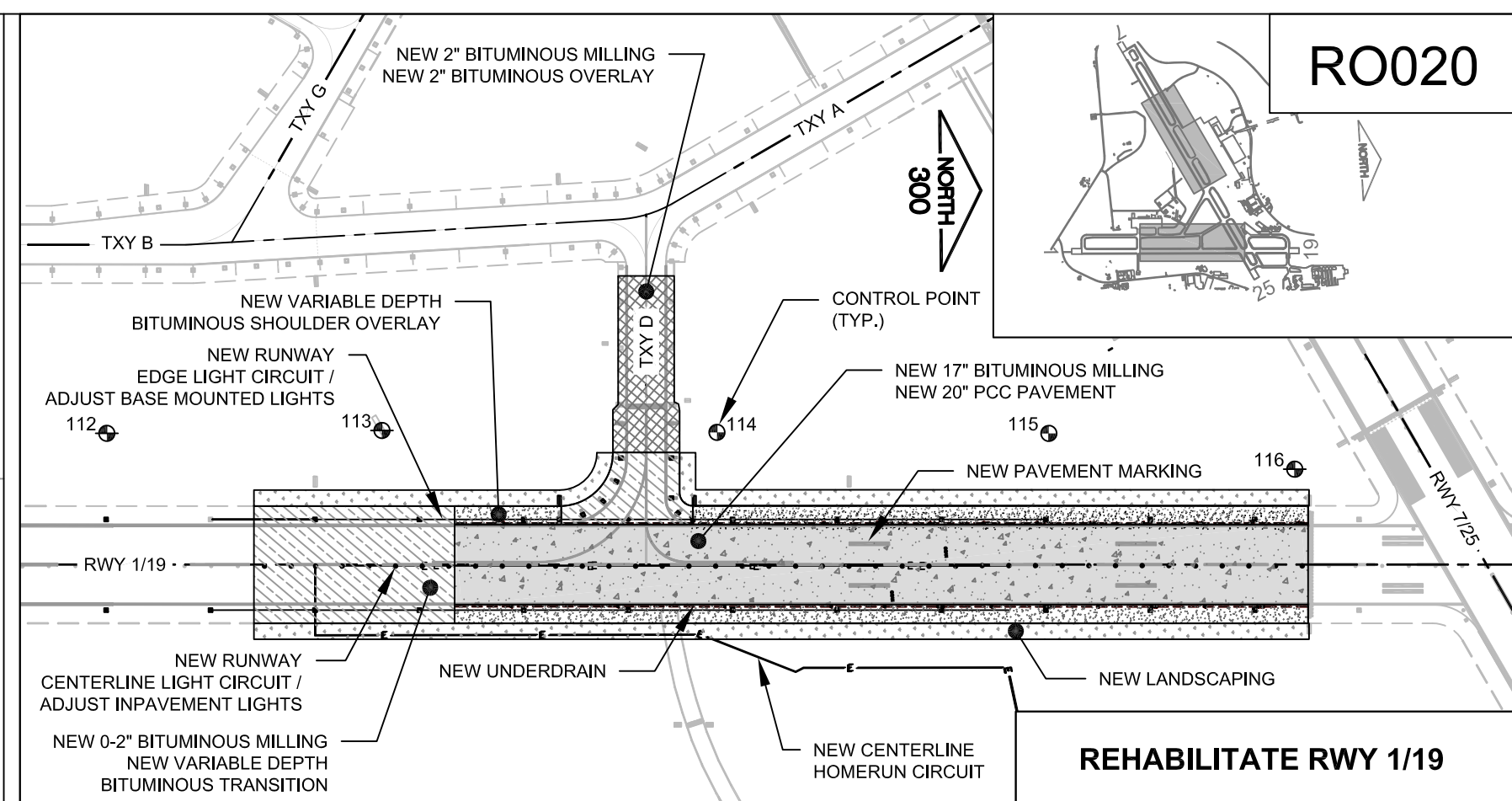
SITE PLAN  
AND CONTROL

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **SPL1**    REVISION

SHEET 3 OF 70 SHEETS

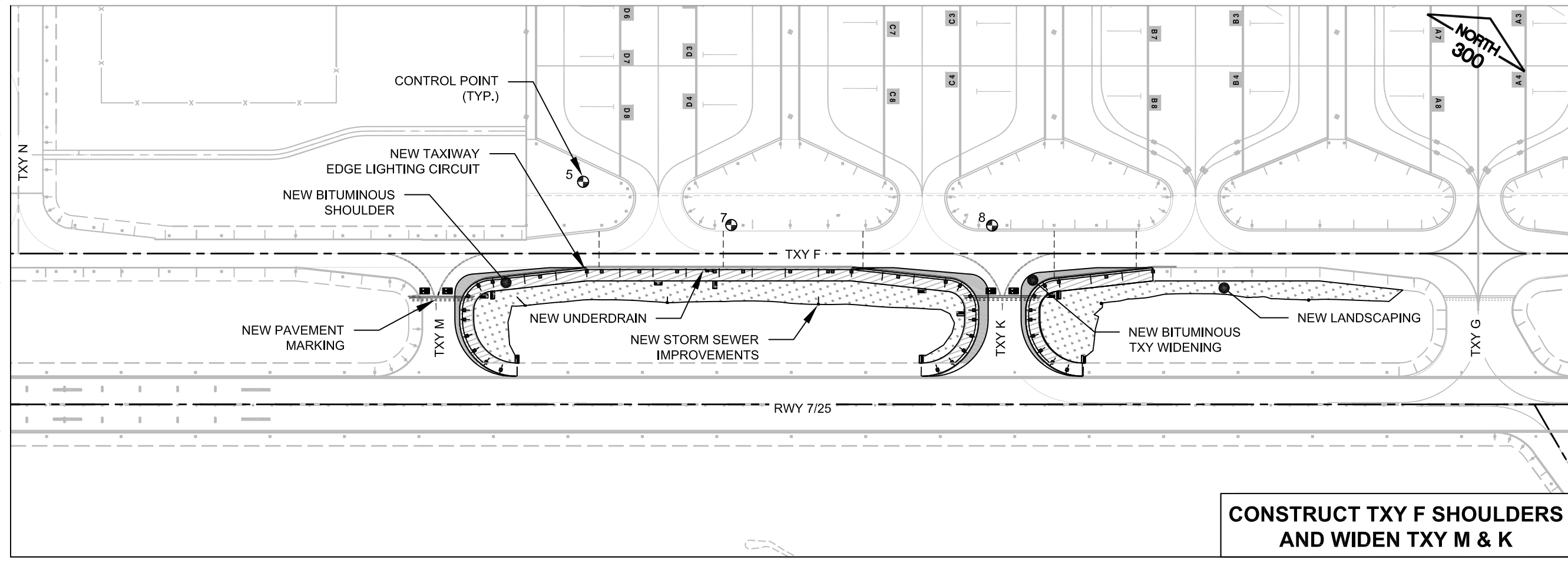


**LEGEND**

- NEW 20" PCC PAVEMENT INLAY AND 17" BITUMINOUS PAVEMENT MILLING REMOVAL
- NEW 2" BITUMINOUS OVERLAY AND 2" BITUMINOUS PAVEMENT MILLING
- NEW VARIABLE DEPTH BITUMINOUS SHOULDER OVERLAY
- NEW VARIABLE DEPTH BITUMINOUS TRANSITION AND 0-2" BITUMINOUS PAVEMENT MILLING
- NEW LANDSCAPING (TOPSOILING / SEEDING / MULCHING)
- NEW BITUMINOUS PAVEMENT WIDENING (14" BIT / 12" AGG. BASE)
- NEW BITUMINOUS SHOULDER PAVEMENT (4" BIT / 8" AGG. BASE)

HORIZONTAL AND VERTICAL CONTROL

POINT NO.	STATION / OFFSET	NORTHING	EASTING	ELEVATION	DESCRIPTION
112	49+09.12 / 245.95' LT	2014936.28	2588059.78	730.45	IRON PIN
113	54+24.89 / 251.27' LT	2015450.75	2588096.82	729.09	IRON PIPE
114	60+52.91 / 247.75' LT	2016076.36	2588151.89	729.26	IRON PIN
115	66+75.05 / 245.78' LT	2016696.24	2588204.92	729.63	IRON PIN
116	71+36.03 / 178.73' LT	2017150.16	2588309.59	730.96	IRON PIN
5	145+25.94 / 190.919' LT	2015796.48	2583543.93	720.63	PK NAIL
7	149+18.54 / 75.816' LT	2015860.19	2583948.06	723.99	CHISELED "X"
8	156+10.26 / 74.472' LT	2016154.58	2584574.02	725.82	CHISELED "X"





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

REV	DATE	DESCRIPTION

APPROVED AS WORKING PLAN  
BY: \_\_\_\_\_

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

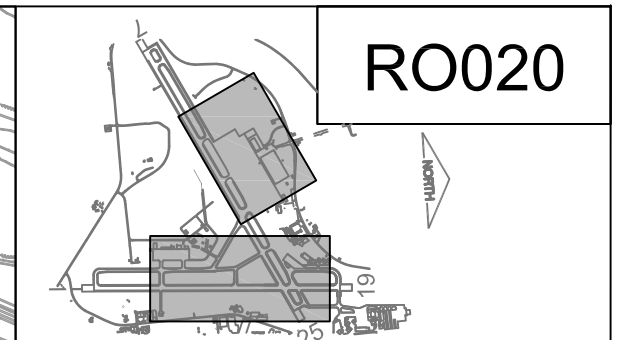
SHEET TITLE:  
**PHASING PLAN  
SUMMARY**

DESIGNED:	DRAWN:	CHECKED:
CMT-RFD	CMT-RFD	CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.	REVISION
<b>PPM1</b>	

SHEET 4 OF 70 SHEETS

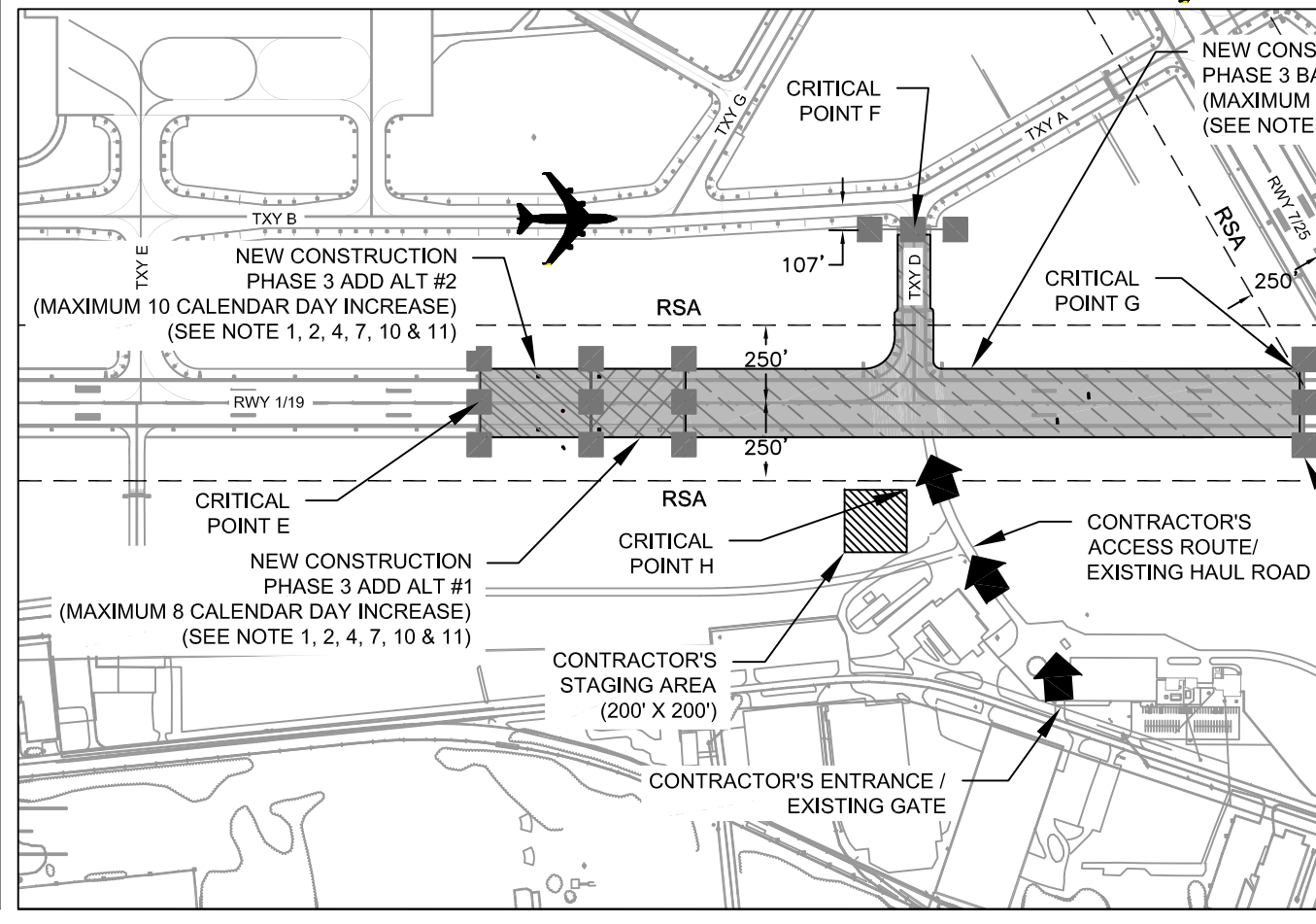
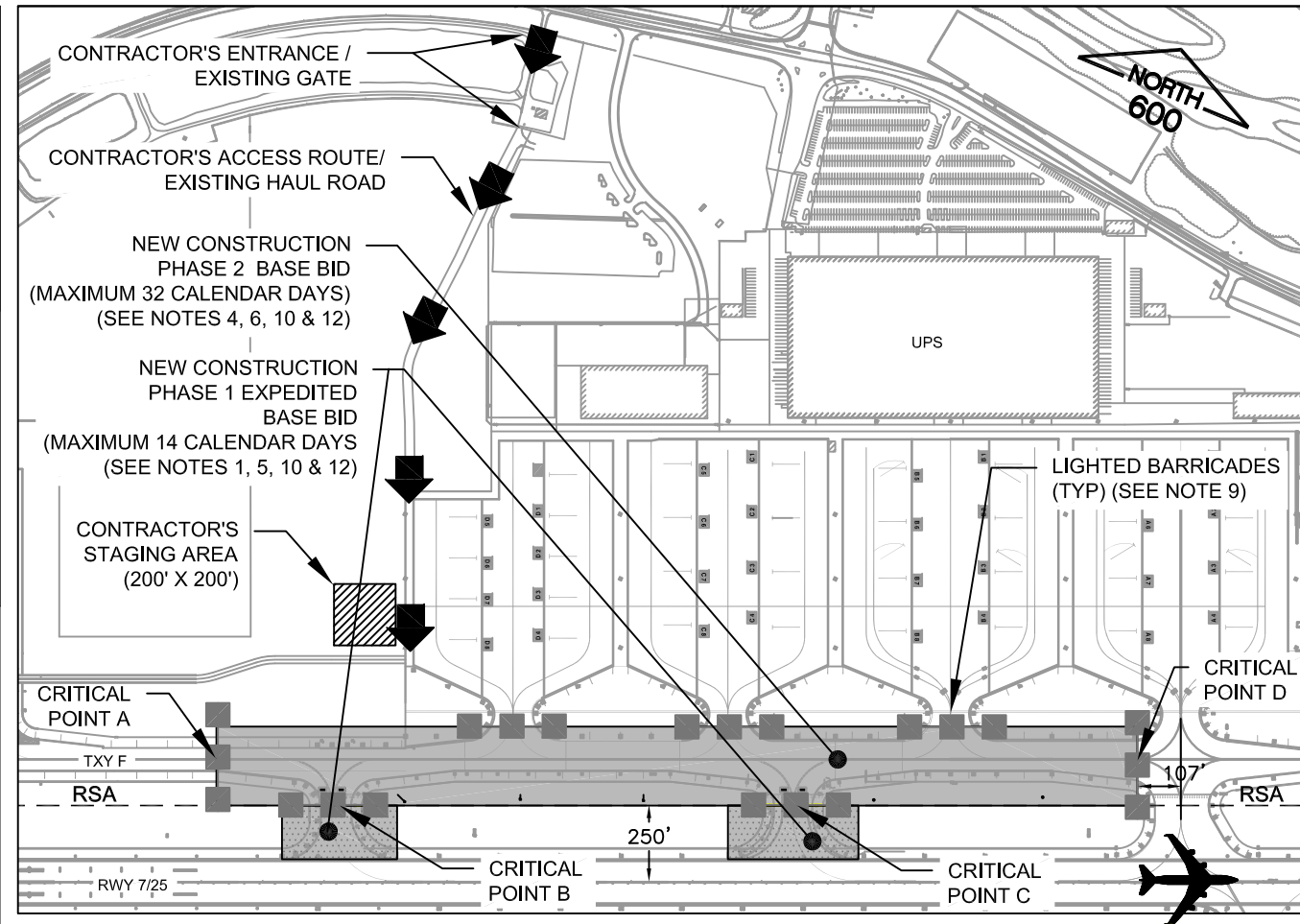


**CRITICAL POINTS**

POINT NO.	LATITUDE	LONGITUDE	ELEV.*
A	42° 11' 41.41"	89° 06' 35.74"	751.56
B	42° 11' 41.62"	89° 06' 30.36"	745.40
C	42° 11' 47.79"	89° 06' 12.22"	750.65
D	42° 11' 53.88"	89° 05' 59.10"	752.21
E	42° 11' 33.21"	89° 05' 24.89"	756.00
F	42° 11' 47.44"	89° 05' 30.32"	755.31
G	42° 11' 59.21"	89° 05' 22.59"	758.99
H	42° 11' 46.45"	89° 05' 19.50"	753.50

\* ELEVATION INCLUDES 25' ANTICIPATED  
MAXIMUM EQUIPMENT HEIGHT

**CONSTRUCT TXY F SHOULDERS  
AND WIDEN TXY M & K**



**REHABILITATE RWY 1/19**

**LEGEND**

	PHASE 1 CONSTRUCTION EXPEDITED
	PHASE 2 CONSTRUCTION
	PHASE 3 CONSTRUCTION
	ADD ALT #1
	ADD ALT #2
	LIGHTED BARRICADES
	CONTRACTOR'S CONSTRUCTION ACCESS/HAUL ROAD
	RUNWAY SAFETY AREA (R.S.A.)
	AIRCRAFT MOVEMENT AREAS

- NOTES**
- SIMULTANEOUS RUNWAY CLOSURES WILL NOT BE PERMITTED.
  - SHOULD ONE OR BOTH OF THE ADDITIVE ALTERNATES BE AWARDED, CONSTRUCTION ACTIVITIES SHALL TAKE PLACE CONCURRENTLY WITH PHASE 3 WORK.
  - ANY TIME THE HAUL ROUTE USES OR CROSSES EXISTING AIRPORT PAVEMENT, THE PAVEMENT SHALL BE SWEEPED AND CLEANED AS REQUIRED TO THE SATISFACTION OF THE AIRPORT AND/OR RESIDENT ENGINEER.
  - ALL PHASE 1 EXPEDITED WORK SHALL BE COMPLETED PRIOR TO THE START OF PHASE 2 AND PHASE 3 WORK. CONCURRENT CONSTRUCTION OF PHASE 2 AND PHASE 3 WILL BE ALLOWED.
  - ALL WORK DURING PHASE 1 EXPEDITED WILL REQUIRE THE TEMPORARY CLOSURE OF RUNWAY 7/25. ONLY DAILY CLOSURES OF RUNWAY WILL BE PERMITTED, NIGHTTIME CLOSURES WILL NOT BE ALLOWED. ALL WORK SHALL BE EXPEDITED TO MINIMIZE IMPACT ON AIRPORT OPERATIONS.
  - ALL PHASE 2 WORK WILL REQUIRE THE TEMPORARY CLOSURE OF TAXIWAY F AS SHOWN. DAILY CLOSURES OF TAXIWAY F WILL BE PERMITTED FROM 7:00 AM - 7:00 PM MONDAY THROUGH SATURDAY. IN ADDITION, TAXIWAY F CLOSURE WILL BE PERMITTED FROM 7:00 AM SUNDAY TO 7:00 PM MONDAY (36 HOURS). ALL WORK SHALL BE EXPEDITED TO MINIMIZE IMPACT ON AIRPORT OPERATIONS.
  - ALL PHASE 3 WORK WILL REQUIRE THE CLOSURE OF RUNWAY 1/19 AS SHOWN FOR THE DURATION OF CONSTRUCTION ACTIVITIES.
  - PROPOSED HOMERUN CABLING IS NOT SHOWN FOR CLARITY. THIS WORK SHALL BE INCLUDED IN THE BASE BID.
  - THE LOCATION OF THE BARRICADES SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER PER THE INDIVIDUAL PHASES. CONSTRUCTION PHASING SHALL REQUIRE THE BARRICADES TO BE REPOSITIONED. ANY REPOSITIONING OF THE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
  - IF CONSTRUCTION IS NOT COMPLETED WITHIN THE 2012 CONSTRUCTION SEASON, CONSTRUCTION SHALL BE PHASED AS NOT TO IMPACT THE ROCKFORD AIRFEST SCHEDULED FOR JUNE 1-2, 2013.
  - RUNWAY 1/19 CLOSURE SHALL NOT EXCEED 60 CALENDAR DAYS FOR BASE BID, 68 CALENDAR DAYS FOR BASE BID AND ADD. ALT. #1 IF AWARDED, 78 CALENDAR DAYS FOR BASE BID AND ADD. ALT. #1 AND #2 IF AWARDED. ANY ADDITIONAL CLOSURE OF RUNWAY 1/19 WILL RESULT IN LIQUIDATED DAMAGES PER SECTION 60-09 OF THE DIVISION'S STANDARD SPECIFICATION FOR CONSTRUCTION OF AIRPORTS.
  - PRIOR TO OPENING RUNWAY 7/25 AND TAXIWAY F AT THE END OF EACH DAYS CONSTRUCTION ACTIVITY, NO DROP OFFS GREATER THAN 3 INCHES AND/OR OPEN EXCAVATION WILL BE ALLOWED WITHIN THE RSA (250' CENTERED ON RUNWAY) OR TSA (214' CENTERED ON TAXIWAY). IN ADDITION, ALL SURFACES WITHIN THE RSA AND TSA SHALL BE SMOOTH GRADED.



RO020

**PHASING PLAN PROJECT NOTES:**

**PHASING PLAN GENERAL NOTES:**

**1. APPROVED PROGRESS SCHEDULE:**

PRIOR TO THE START OF CONSTRUCTION, AN APPROVED PROGRESS SCHEDULE SHALL BE EXECUTED BY THE CONTRACTOR AND MONITORED BY THE RESIDENT ENGINEER. THIS SCHEDULE SHALL SHOW START/ STOP DATES OF ALL PHASES, INCLUDING ALL AOA WORK REQUIRING DAYTIME ONLY CLOSURES OF TAXIWAYS AND RUNWAYS. THE APPROVED PROGRESS SCHEDULE SHALL BE DISTRIBUTED TO ALL PARTIES AT LEAST 3 WORKING DAYS PRIOR TO START OF CONSTRUCTION.

**2. NOTAM (NOTICE TO AIRMEN) COORDINATION:**

THE RESIDENT ENGINEER SHALL COORDINATE NOTAM AND FAA FACILITY COORDINATION WITH AIRPORT / FAA PERSONNEL.

**3. CONSTRUCTION SITE ACCESS AND STAGING AREA:**

THE CONTRACTOR ACCESS ROAD AND STAGING AREAS SHALL BE AS SHOWN ON THE REFERENCED PLAN. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE CONSTRUCTION ACCESS ROAD AND STAGING AREA TO ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ALTERNATE STAGING AREAS AND ACCESS FOR THIS AREA WILL NOT BE ALLOWED. THIS WORK AREA SHALL BE RESTRICTED FROM ACTIVE AOA AREAS WITH THE BARRICADE/FENCING PERIMETERS SHOWN.

THE ENTRANCES SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO THE CITY OF ROCKFORD AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.

**4. CONSTRUCTION EQUIPMENT FLAGGING / BEACON REQUIREMENTS:**

THE CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.

**5. GATE SECURITY:**

THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE DEPUTY DIRECTOR OF OPERATIONS. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD EMPLOYED BY THE CONTRACTOR SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURE GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY. AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

**6. CONSTRUCTION OUTSIDE OF BARRICADED AREAS REQUIRING TAXIWAY / RUNWAY CLOSURES:**

WORK OUTSIDE THE BARRICADED LINES WITHIN THE AOA AREAS SHOWN SHALL REQUIRE TEMPORARY DAYTIME ONLY CLOSURES OF THE RUNWAY 7/25 AND TAXIWAY F. THIS WORK SHALL BE EXPEDITED AND PRIORITIZED TO MINIMIZE CLOSURE TIME OF THE ACTIVE PAVEMENTS. CONSTRUCTION ACTIVITIES WILL NOT BE PERMITTED WITHIN 250' OF CENTERLINE ON ALL ACTIVE RUNWAYS AND WITHIN 160' OF CENTERLINE ON ALL ACTIVE TAXIWAYS. IN ADDITION, THIS WORK WILL REQUIRE ALL CREWS TO SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO TO COMMUNICATE DIRECTLY WITH THE ATCT (AIR TRAFFIC CONTROL TOWER). THE OPERATOR OF THE AIRPORT RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND BE TUNED INTO THE GROUND CONTROL FREQUENCY AT ALL TIMES.

**7. UNAUTHORIZED ACCESS TO AIRFIELD:**

THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AN AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS, MAY BE FINED \$10,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND DEDUCTED FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

**1. SUGGESTED SEQUENCE OF CONSTRUCTION:**

THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS.

**2. STAGING AREA RESTORATION:**

ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

**3. HAUL ROUTE / HAUL ROUTE RESTORATION:**

THE CONTRACTOR SHALL USE THE HAUL ROUTE(S) AS SHOWN IN THESE PLANS. THE COST OF MAINTAINING THE HAUL ROUTE(S) SHALL BE INCIDENTAL TO CONTRACT. ALL HAUL ROUTE(S) INCLUDING EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE HAUL ROUTE(S) SHALL BE INCIDENTAL TO CONTRACT.

**4. AIRPORT APPROVAL OF PHASING:**

THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

**5. AIRFIELD PAVEMENT / SITE DEBRIS REMOVAL:**

THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

(NOTES CONTINUED ON PHASING PLAN DETAILS - PPD1)

PHASE	WORK AREA	ALLOWABLE WORK PERIODS	AIRPORT OPERATIONAL RESTRICTIONS	ANTICIPATED SCHEDULE
1 (EXPEDITED)	TAXIWAY M AND K WITHIN RUNWAY 7/25 SAFETY AREA	DAILY CLOSURES OF RUNWAY 7/25 WILL BE PERMITTED 7:00 A.M. - 7:00 P.M.	WORK WITHIN 250' OF RUNWAY 7/25 CENTERLINE WILL REQUIRE THE TEMPORARY CLOSURE OF RUNWAY 7/25. WORK REQUIRING ANY CLOSURE SHALL BE EXPEDITED AND PRIORITIZED TO MINIMIZE CLOSURE TIME. AT THE END OF EACH DAYS CONSTRUCTION ACTIVITIES THE RUNWAY SHALL BE REOPENED.	(14 CALENDAR DAYS)
2	TAXIWAY F BETWEEN STATION 137+50 TO 167+50 AND TAXIWAY M AND K OUTSIDE RUNWAY 7/25 SAFETY AREA	DAILY CLOSURES OF TAXIWAY F WILL BE PERMITTED 7:00 A.M. - 7:00 P.M.	WORK ON TAXIWAY F WILL REQUIRE THE TEMPORARY CLOSURE OF TAXIWAY F BETWEEN STATION 137+50 TO 167+50. WORK REQUIRING ANY CLOSURE SHALL BE EXPEDITED AND PRIORITIZED TO MINIMIZE CLOSURE TIME. AT THE END OF EACH DAYS CONSTRUCTION ACTIVITIES THE TAXIWAY SHALL BE REOPENED.	(32 CALENDAR DAYS)
3	RUNWAY 1/19 STATION 45+50 TO 71+50. IF AWARDED ADDITIVE ALTERNATE 1 AND 2 APPLICABLE AREAS	NO RESTRICTIONS	WORK ON RUNWAY 1/19 WILL REQUIRE THE CLOSURE OF RUNWAY 1/19 FOR THE DURATION OF THE CONSTRUCTION ACTIVITIES.	(60 CALENDAR DAYS)
				106 CALENDAR DAYS

IF ADD ALT #1 IS AWARDED, 8 ADDITIONAL CALENDAR DAYS WILL BE AWARDED TO PHASE 3 (TOTAL PROJECT CALENDAR DAYS = 114 DAYS).  
IF ADD ALT #1 AND #2 ARE AWARDED, 18 ADDITIONAL CALENDAR DAYS WILL BE AWARDED TO PHASE 3 (TOTAL PROJECT CALENDAR DAYS = 124 DAYS)

**PHASING PLAN NOTES  
(PPN1)**



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**PHASING PLAN  
NOTES**

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **PPN1**    REVISION

SHEET 5 OF 70 SHEETS



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

PHASING PLAN  
DETAILS

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
----------------------	-------------------	---------------------

PROJECT NO.:	11258-03-00/06
DATE:	APRIL 20, 2012

SHEET NO.	PPD1	REVISION
-----------	------	----------

SHEET 6 OF 70 SHEETS

RO020

**PHASING PLAN GENERAL NOTES (CONTD):**

(NOTES CONTINUED ON PHASING PLAN NOTES - PPN1)

**6. PROJECT LIGHTING OUTSIDE OF DAYLIGHT HOURS:**  
WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

**7. EXISTING UTILITY COORDINATION:**  
COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATIONS.

**8. TRAFFIC CONTROL PAYMENT:**  
PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. LOW PROFILE BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE PLACED ON 15' CENTERS. LOW PROFILE BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE RESIDENT ENGINEER. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL NON AIRFIELD LOCATIONS REQUIRING BARRICADES SHALL BE TYPE II BARRICADES AND SHALL CONFORM TO IDOT STANDARD 702001. FOR AIRSIDE BARRICADE PLACEMENT, SEE PHASING PLAN SUMMARY SHEETS. FOR ROADWAY PROTECTION, SEE TEMPORARY TRAFFIC CONTROL PLAN AND GENERAL NOTES AND DETAILS SHEETS.

**9. DRIVERS TRAINING AND BADGING:**  
DRIVER'S TRAINING AND BADGING SHALL BE REQUIRED FOR THE CONTRACTOR'S SUPERVISORY PERSONNEL, OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. THE DRIVER'S TRAINING AND BADGING OF THE INITIAL SUPERVISORY PERSONNEL MUST BE COMPLETED PRIOR TO THE START OF CONSTRUCTION.

**10. DUST CONTROL REQUIREMENTS:**  
THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DEPUTY DIRECTOR OF OPERATIONS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

**11. OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION (AC 150/5370-2F):**  
ALL WORK SHALL CONFORM TO AC 150/5370-2F OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION. THIS AC IS AVAILABLE AT [www.faa.gov/arp/pdf/5370-2f.pdf](http://www.faa.gov/arp/pdf/5370-2f.pdf).

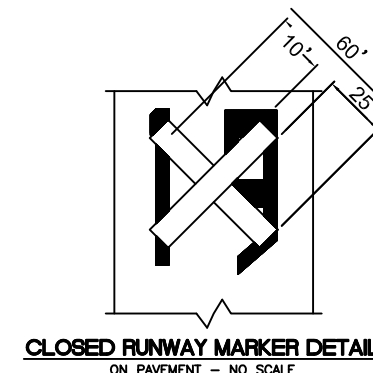
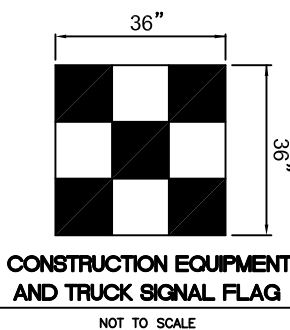
**12. STAGING AREA:**  
THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

**13. AIRFIELD LIGHTING COORDINATION:**  
THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE DONE SO AT NO EXTRA COST TO THE CONTRACT.

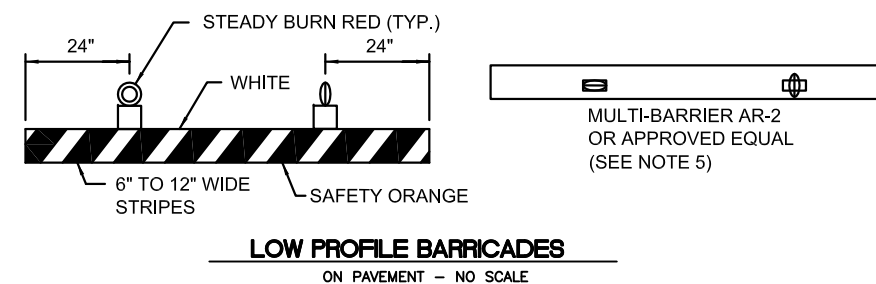
**14. WEEKLY COORDINATION MEETINGS:**  
WEEKLY COORDINATION MEETINGS SHALL BE REQUIRED TO DISCUSS PROJECT PROGRESS. AS A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY.

**15. TEMPORARY CABLES:**  
ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT: • REHABILITATE TAXIWAY J AND L



1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER AND THE AIRPORT.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
4. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.



1. LOW PROFILE BARRICADES SHALL BE PLACED 15' ON CENTER AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. BARRICADES SHALL BE WEIGHTED WITH A MINIMUM OF 6 SAND BAGS TO PREVENT THEM FROM BEING BLOWN OVER.
2. LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
3. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
4. ALTERNATE LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
6. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

**PHASING PLAN DETAILS  
(PPD1)**

8

7

6

5

4

3

2

1

8

7

6

5

4

3

2

1





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

ONSITE BORROW /  
MILLING STOCKPILE

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.    REVISION

**OBM1**

SHEET 7 OF 70 SHEETS

RO020



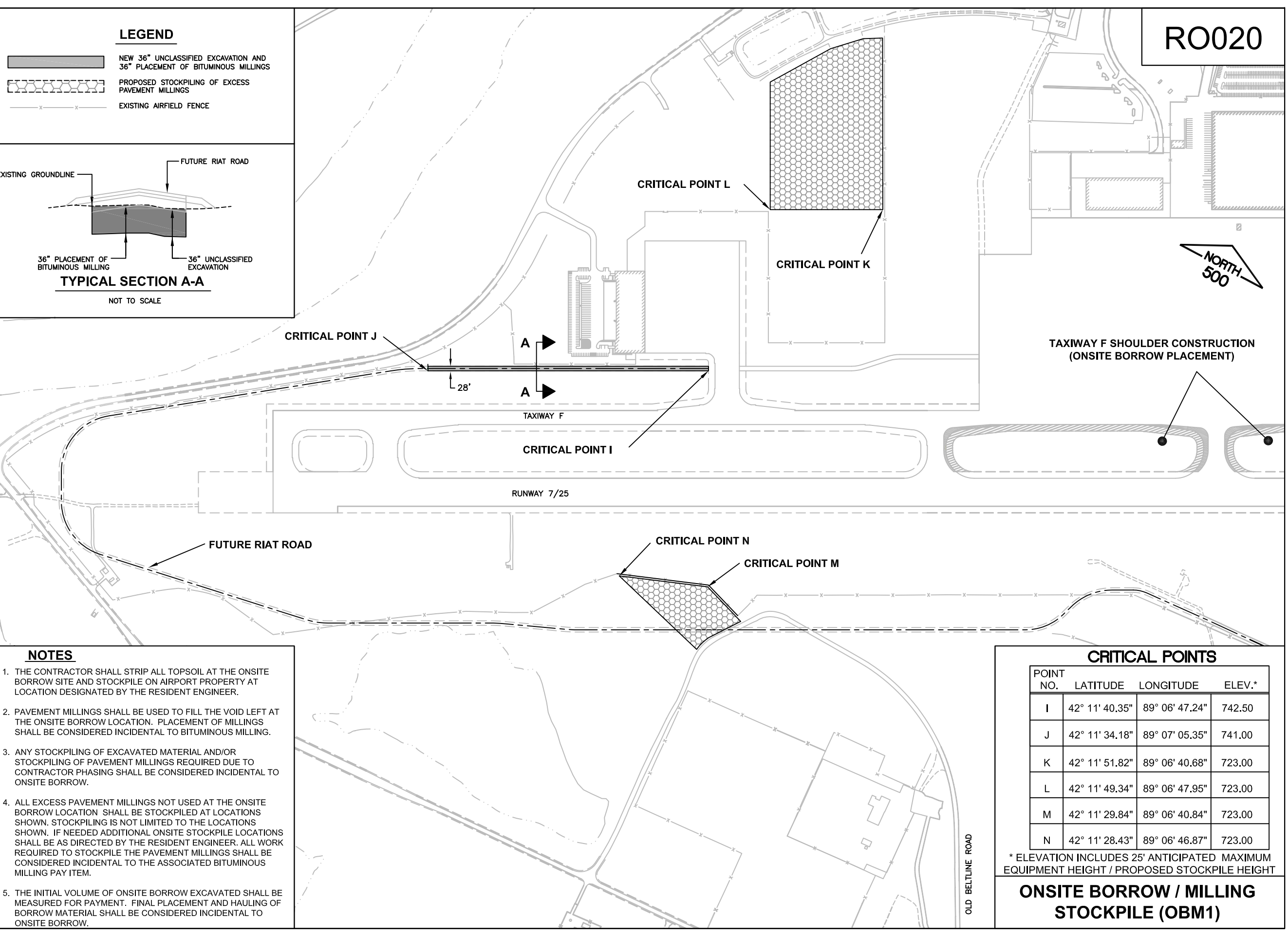
TAXIWAY F SHOULDER CONSTRUCTION  
(ONSITE BORROW PLACEMENT)

OLD BELTLINE ROAD

CRITICAL POINTS			
POINT NO.	LATITUDE	LONGITUDE	ELEV.*
I	42° 11' 40.35"	89° 06' 47.24"	742.50
J	42° 11' 34.18"	89° 07' 05.35"	741.00
K	42° 11' 51.82"	89° 06' 40.68"	723.00
L	42° 11' 49.34"	89° 06' 47.95"	723.00
M	42° 11' 29.84"	89° 06' 40.84"	723.00
N	42° 11' 28.43"	89° 06' 46.87"	723.00

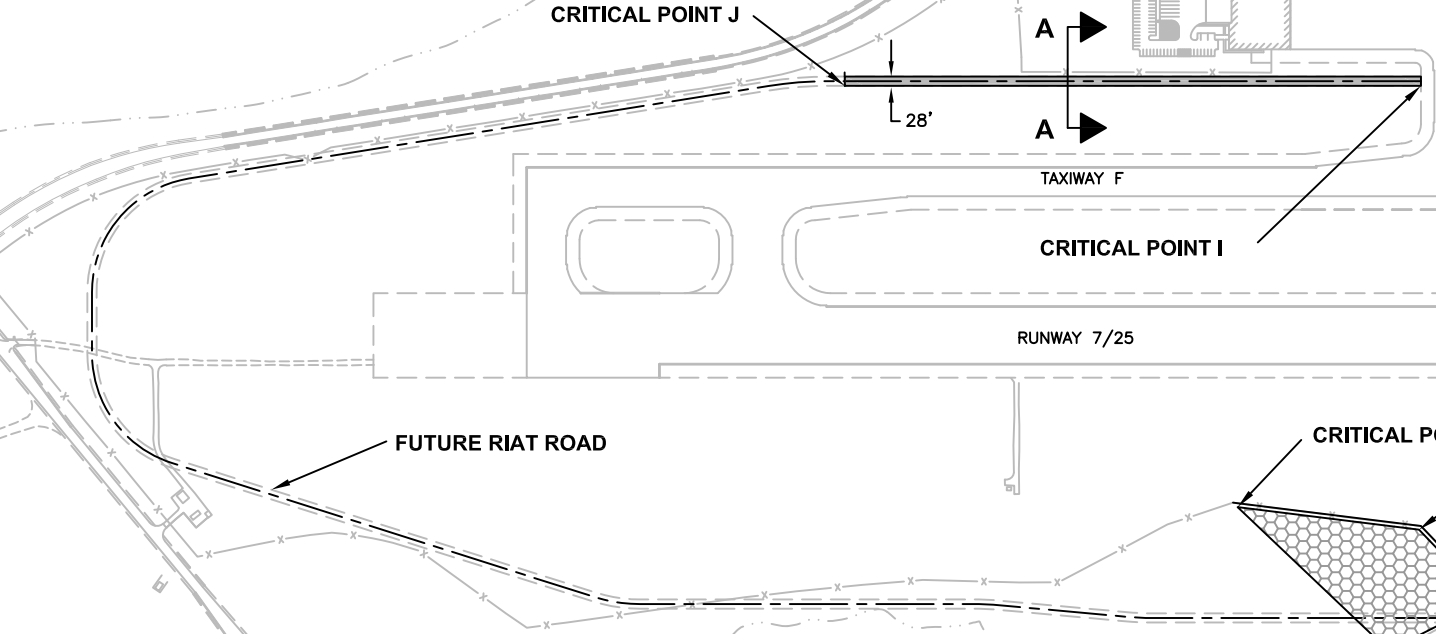
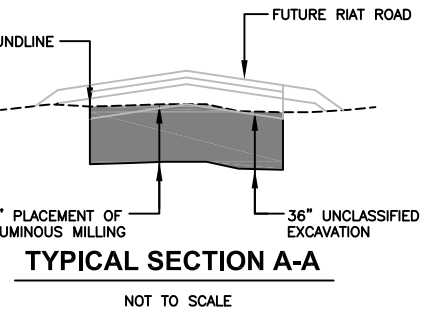
\* ELEVATION INCLUDES 25' ANTICIPATED MAXIMUM EQUIPMENT HEIGHT / PROPOSED STOCKPILE HEIGHT

**ONSITE BORROW / MILLING STOCKPILE (OBM1)**



**LEGEND**

- NEW 36" UNCLASSIFIED EXCAVATION AND 36" PLACEMENT OF BITUMINOUS MILLINGS
- PROPOSED STOCKPIILING OF EXCESS PAVEMENT MILLINGS
- EXISTING AIRFIELD FENCE



- NOTES**
- THE CONTRACTOR SHALL STRIP ALL TOPSOIL AT THE ONSITE BORROW SITE AND STOCKPILE ON AIRPORT PROPERTY AT LOCATION DESIGNATED BY THE RESIDENT ENGINEER.
  - PAVEMENT MILLINGS SHALL BE USED TO FILL THE VOID LEFT AT THE ONSITE BORROW LOCATION. PLACEMENT OF MILLINGS SHALL BE CONSIDERED INCIDENTAL TO BITUMINOUS MILLING.
  - ANY STOCKPIILING OF EXCAVATED MATERIAL AND/OR STOCKPIILING OF PAVEMENT MILLINGS REQUIRED DUE TO CONTRACTOR PHASING SHALL BE CONSIDERED INCIDENTAL TO ONSITE BORROW.
  - ALL EXCESS PAVEMENT MILLINGS NOT USED AT THE ONSITE BORROW LOCATION SHALL BE STOCKPIILED AT LOCATIONS SHOWN. STOCKPIILING IS NOT LIMITED TO THE LOCATIONS SHOWN. IF NEEDED ADDITIONAL ONSITE STOCKPILE LOCATIONS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER. ALL WORK REQUIRED TO STOCKPILE THE PAVEMENT MILLINGS SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED BITUMINOUS MILLING PAY ITEM.
  - THE INITIAL VOLUME OF ONSITE BORROW EXCAVATED SHALL BE MEASURED FOR PAYMENT. FINAL PLACEMENT AND HAULING OF BORROW MATERIAL SHALL BE CONSIDERED INCIDENTAL TO ONSITE BORROW.

8 7 6 5 4 3 2 1

H G F E D C B A



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

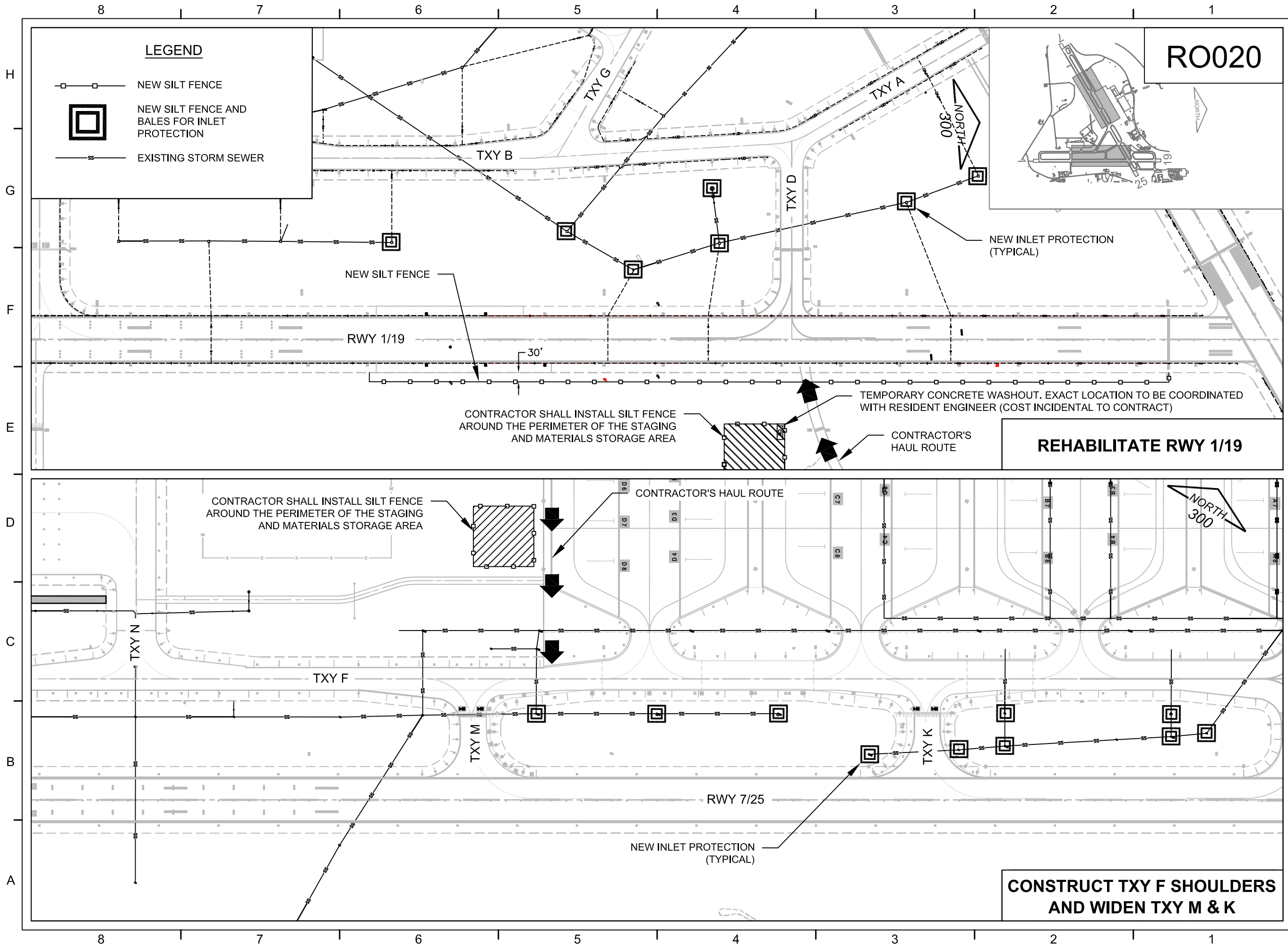
SHEET TITLE:  
**STORMWATER  
POLLUTION PREVENTION  
PLAN**

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **SWP1** REVISION

SHEET 8 OF 70 SHEETS



**REHABILITATE RWY 1/19**

**CONSTRUCT TXY F SHOULDERS  
AND WIDEN TXY M & K**

**RO020**

NORTH  
300

NORTH  
300

**LEGEND**

- NEW SILT FENCE
- NEW SILT FENCE AND BALES FOR INLET PROTECTION
- EXISTING STORM SEWER

NEW SILT FENCE

RWY 1/19

CONTRACTOR SHALL INSTALL SILT FENCE AROUND THE PERIMETER OF THE STAGING AND MATERIALS STORAGE AREA

TEMPORARY CONCRETE WASHOUT. EXACT LOCATION TO BE COORDINATED WITH RESIDENT ENGINEER (COST INCIDENTAL TO CONTRACT)

CONTRACTOR'S HAUL ROUTE

CONTRACTOR SHALL INSTALL SILT FENCE AROUND THE PERIMETER OF THE STAGING AND MATERIALS STORAGE AREA

CONTRACTOR'S HAUL ROUTE

RWY 7/25

NEW INLET PROTECTION (TYPICAL)

TXY F

TXY M

TXY K

TXY B

TXY G

TXY A

TXY D

TXY N

8

7

6

5

4

3

2

1

H

G

F

E

D

C

B

A

H

G

F

E

D

C

B

A

8

7

6

5

4

3

2

1



**STORM WATER POLLUTION PREVENTION PLAN**

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATING RUNWAYS, TAXIWAYS, AND CONSTRUCTING A RIAT ROAD AT THE CHICAGO ROCKFORD INTERNATIONAL AIRPORT. THE PROJECT INCLUDES GRADING, EXCAVATION, FILL, TOPSOIL PLACEMENT, PAVEMENT CONSTRUCTION, ELECTRICAL, LANDSCAPING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER SILT FENCE AND INLET PROTECTION.
2. EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS.
3. PAVEMENT CONSTRUCTION.
4. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
5. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.
6. REMOVAL OF TEMPORARY EROSION CONTROL / PROTECTION FACILITIES.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 40.0 ACRES OF WHICH 6.0 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE KISHWAUKEE RIVER VIA OVERLAND FLOW AND THROUGH A STORM SEWER SYSTEM.

CONTROLS - EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
- B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
- C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
- D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.

4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.

6. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

RO020



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
**STORMWATER  
POLLUTION PREVENTION  
PLAN NOTES**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **SWN1** REVISION

**STORMWATER POLLUTION  
PREVENTION PLAN NOTES (SWN1)**

MIKE DUNN  
DIRECTOR

CMT  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS

Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
**STORMWATER POLLUTION  
PREVENTION PLAN  
DETAILS**

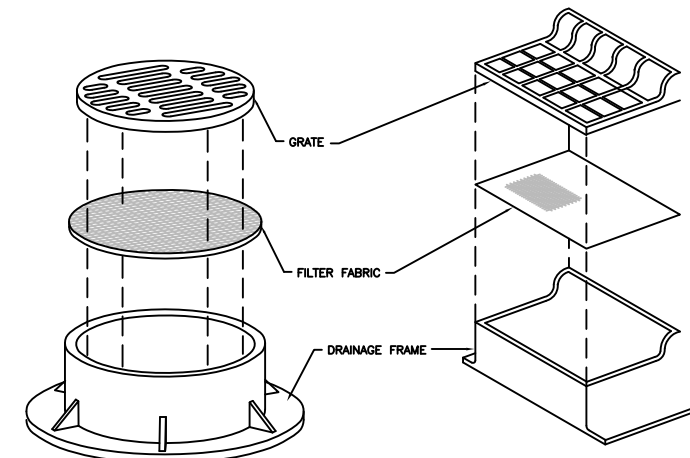
DESIGNED:	DRAWN:	CHECKED:
CMT-RFD	CMT-RFD	CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.	REVISION
SWD1	

SHEET 10 OF 70 SHEETS

# RO020



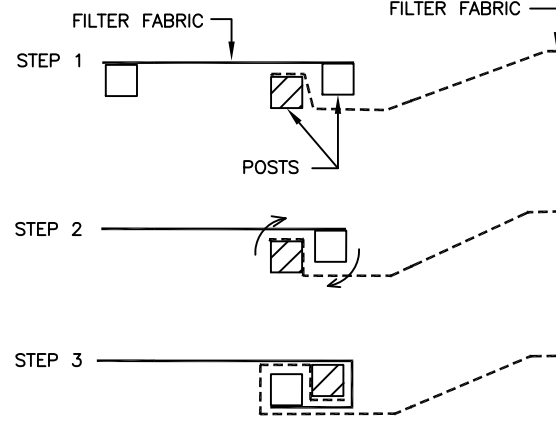
**DRAINAGE STRUCTURE FILTER WRAP**  
N.T.S.

### NOTES

1. FILTER WRAP TO BE PLACED IN ALL INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
2. FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR SILT FENCE.
3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MIN. OF 80% OF COVERAGE.
6. COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO BALES.

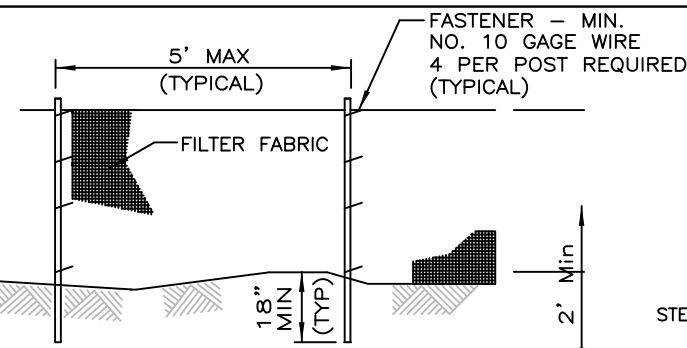
### NOTES:

1. TEMPORARY SEDIMENT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. THEY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND REMOVED IN CONJUNCTION WITH THE FINAL GRADING AND SITE STABILIZATION.
2. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR ITEM AR156000 EROSION CONTROL IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.
3. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.
4. SILT FENCE SHALL BE INSTALLED PER STORM WATER POLLUTION PREVENTION PLAN OR AS DIRECTED BY THE ENGINEER.
5. FENCE POSTS SHALL BE EITHER STANDARD STEEL POSTS OR WOOD POSTS WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SQ. IN..
6. PLACE THE END POST OF THE SECOND FENCE INSIDE THE END POST OF THE FIRST FENCE.
7. ROTATE BOTH POSTS AT LEAST 180 DEGREES IN A CLOCKWISE DIRECTION TO CREATE A TIGHT SEAL WITH THE FABRIC MATERIAL.
8. DRIVE BOTH POSTS A MINIMUM OF 18 INCHES INTO THE GROUND AND BURY THE FLAP.

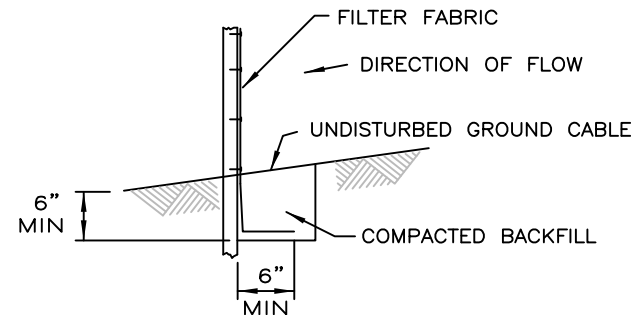


### ATTACHING TWO SILT FENCES

NOT TO SCALE  
FROM NRCS STANDARD DRAWING NO. IL-620

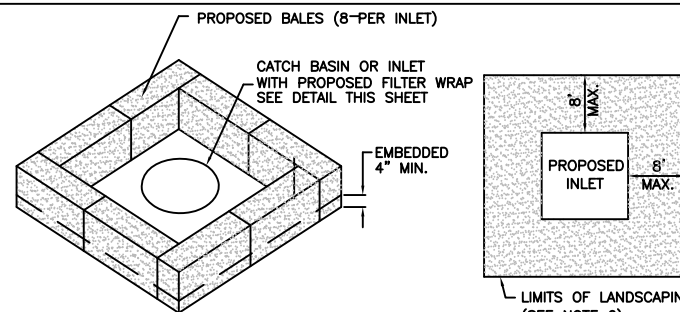


**ELEVATION**



### FABRIC ANCHOR DETAIL

NOT TO SCALE  
FROM NRCS STANDARD DRAWING NO. IL-620



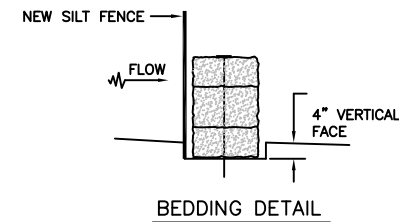
**INLET PLACEMENT**

**INLET PROTECTION - TURF AREAS**

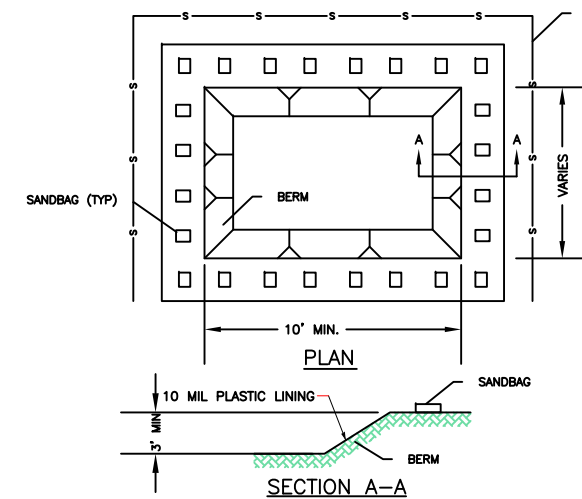
N.T.S.

### INLET PROTECTION NOTES:

1. BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
2. EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
3. BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
4. INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR BALES.
6. AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SEED AND MULCH OVER THE DISTURBED AREAS, COST INCIDENTAL TO BALES.
7. SILT FENCE SHALL BE MEASURED AND PAID FOR SEPARATELY.



**BEDDING DETAIL**

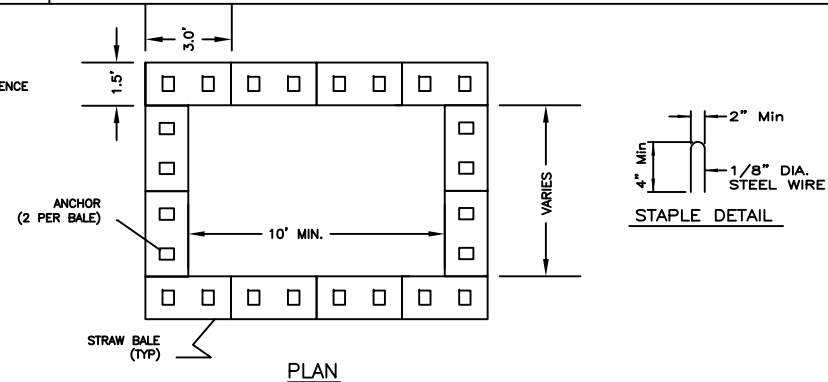


**BELOW GROUND TEMPORARY WASHOUT**

### NOTES:

1. CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
2. WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
3. SANDBAGS SHALL BE INSTALLED TO ANCHOR THE LINING. THE NUMBER OF SANDBAGS SHALL BE DETERMINED BY THE CONTRACTOR. THE CONTRACTOR SHALL ADD SANDBAGS SO AS TO MAINTAIN ANCHORING OF THE LINING.
4. A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT" IN 6" TALL LETTERS.
5. THE TEMPORARY WASHOUT FACILITY SHALL BE SURROUNDED BY SILT FENCE ON ALL SIDES.
6. INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
7. MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
8. UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.
9. COST INCIDENTAL TO ITEM AR501515.

**CONCRETE WASHOUT**  
NOT TO SCALE



**PLAN**

**SECTION**

**ABOVE GROUND TEMPORARY WASHOUT**

### NOTES:

1. CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
2. WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
3. A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT" IN 6" TALL LETTERS.
4. INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
5. MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
6. UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.
7. COST INCIDENTAL TO ITEM AR501515.

### STORMWATER POLLUTION PREVENTION PLAN DETAILS (SWD1)





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**TYPICAL SECTION  
RWY 1/19**

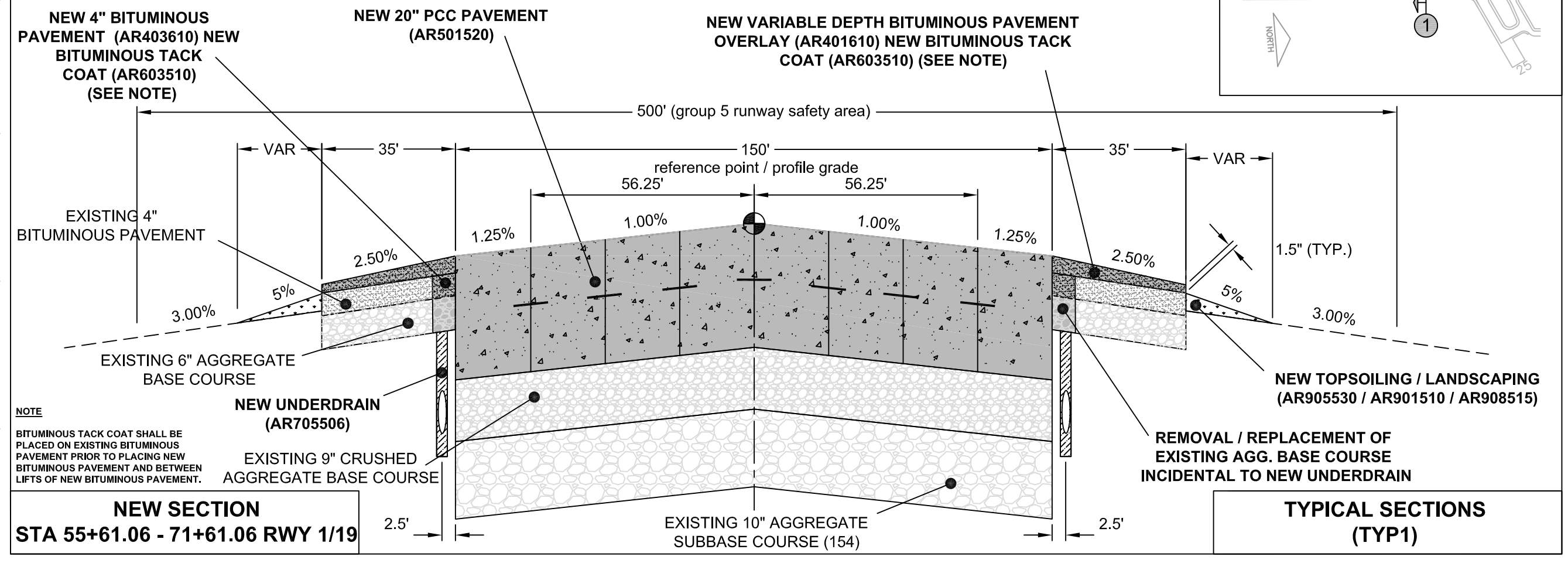
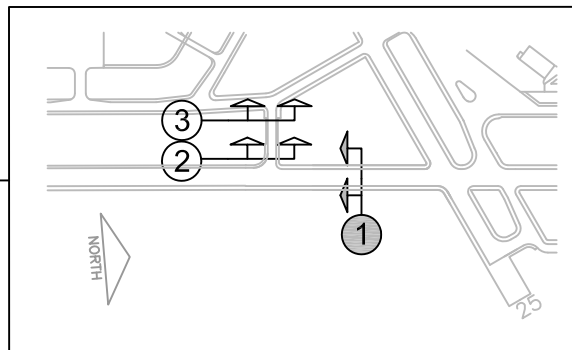
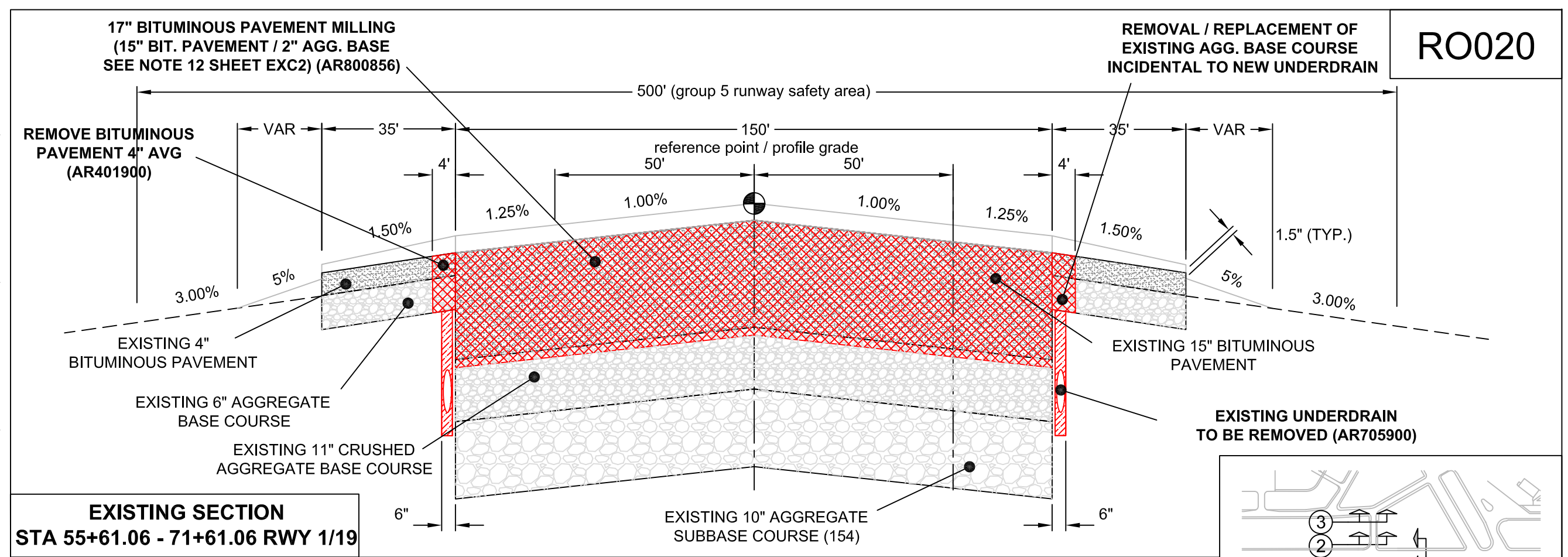
DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **TYP1** REVISION

SHEET 11 OF 70 SHEETS

**RO020**



**NOTE**  
BITUMINOUS TACK COAT SHALL BE PLACED ON EXISTING BITUMINOUS PAVEMENT PRIOR TO PLACING NEW BITUMINOUS PAVEMENT AND BETWEEN LIFTS OF NEW BITUMINOUS PAVEMENT.

**NEW SECTION  
STA 55+61.06 - 71+61.06 RWY 1/19**

**TYPICAL SECTIONS  
(TYP1)**







MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

REV	DATE	DESCRIPTION

REHABILITATE  
RUNWAY 1/19  
PHASE 2

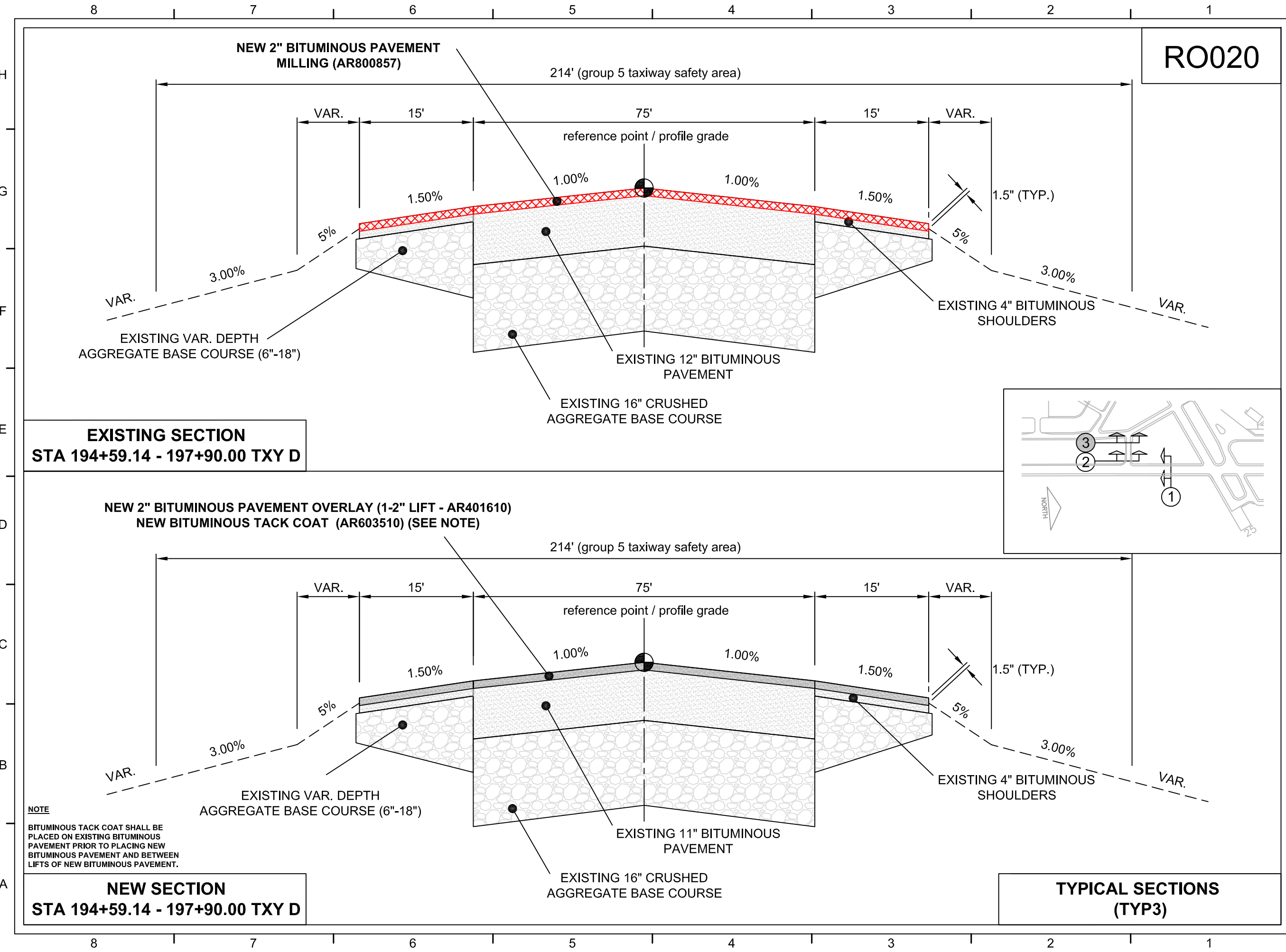
SHEET TITLE:  
TYPICAL SECTION  
TXY D - 2

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **TYP3**    REVISION

SHEET 13 OF 70 SHEETS



**NOTE**  
BITUMINOUS TACK COAT SHALL BE PLACED ON EXISTING BITUMINOUS PAVEMENT PRIOR TO PLACING NEW BITUMINOUS PAVEMENT AND BETWEEN LIFTS OF NEW BITUMINOUS PAVEMENT.



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

EXISTING CONDITIONS  
AND REMOVALS - 1

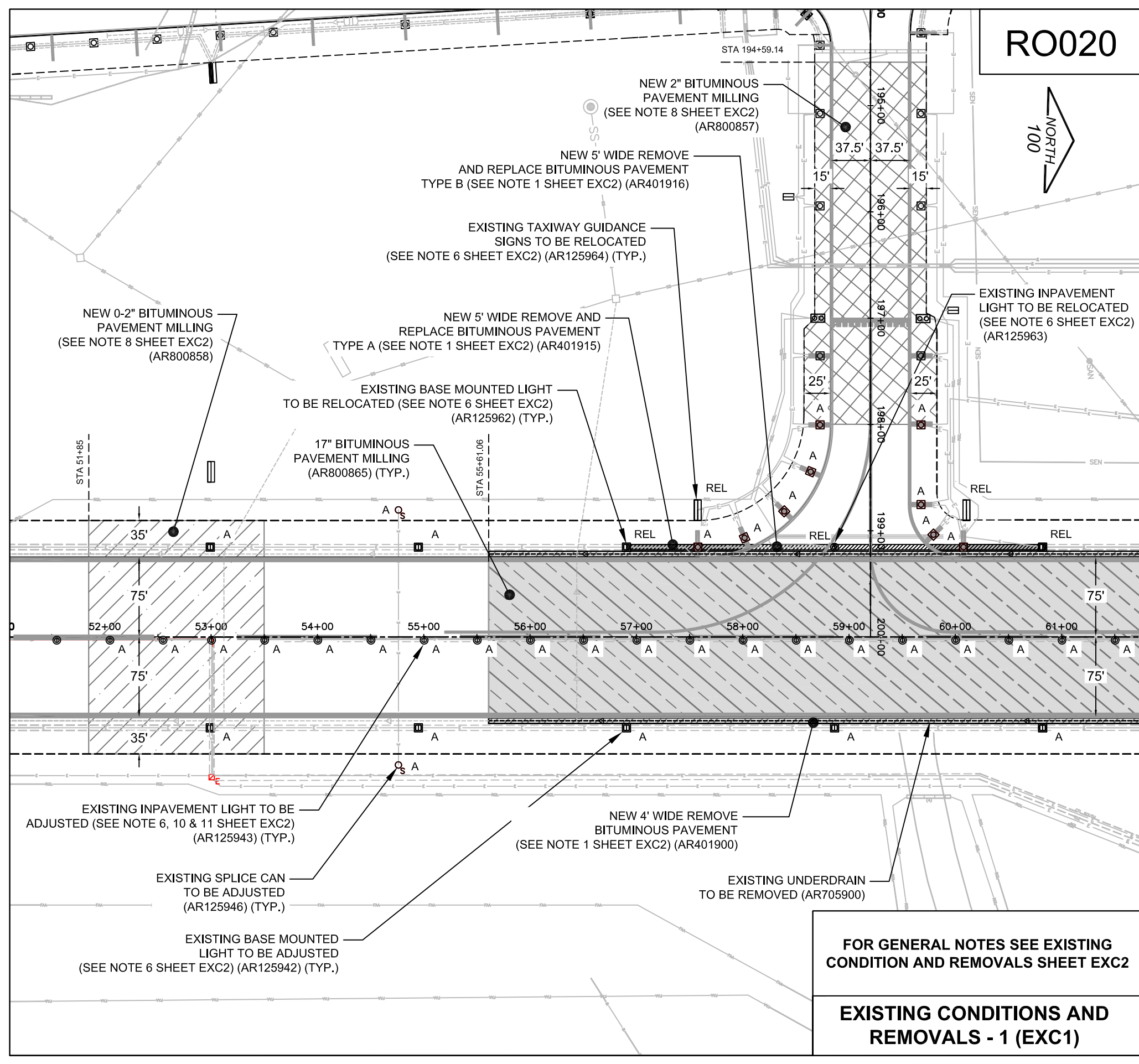
DESIGNED:	DRAWN:	CHECKED:
CMT-RFD	CMT-RFD	CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012







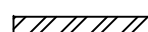
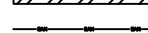



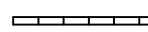
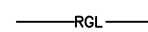
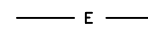














SHEET NO.	REVISION
EXC1	

SHEET 14 OF 70 SHEETS

RO020



**LEGEND**

-  NEW 17" BIT. PAVEMENT MILLING (AVG.)
-  NEW 0-2" BITUMINOUS PAVEMENT MILLING (AVG)
-  NEW 2" BIT. PAVEMENT MILLING (AVG.)
-  REMOVE AND REPLACE BIT. PAVEMENT TYPE A (4" AVG)
-  REMOVE AND REPLACE BIT. PAVEMENT TYPE B (10-12" AVG)
-  REMOVE BITUMINOUS PAVEMENT
-  EXISTING SANITARY LINE
-  EXISTING STORM SEWER
-  EXISTING UNDERDRAIN
-  EXISTING WATERMAIN
-  EXISTING DUCT
-  EXISTING RWY GUIDANCE LIGHT CIRCUIT
-  EXISTING AIRFIELD CIRCUIT
-  EXISTING SURFACE SENSOR CIRCUIT
-  EXISTING MITL, BASE MOUNTED
-  EXISTING HIRL, BASE MOUNTED
-  EXISTING ELECTRICAL HANDHOLE
-  EXISTING RGL
-  EXISTING TAXIWAY GUIDANCE SIGN
-  EXISTING DRAINAGE INLET
-  EXISTING MANHOLE
-  EXISTING ITEM TO BE ADJUSTED
-  EXISTING ITEM TO BE RELOCATED
-  EXISTING ITEM TO BE REMOVED
-  EXISTING PAVEMENT SENSOR
-  EXISTING IN-PAVEMENT LIGHT
-  EXISTING VASI
-  EXISTING SPLICE CAN

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

**FOR GENERAL NOTES SEE EXISTING  
CONDITION AND REMOVALS SHEET EXC2**

**EXISTING CONDITIONS AND  
REMOVALS - 1 (EXC1)**

H  
G  
F  
E  
D  
C  
B  
A

H  
G  
F  
E  
D  
C  
B  
A

8 7 6 5 4 3 2 1

8 7 6 5 4 3 2 1





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**EXISTING CONDITIONS  
AND REMOVALS - 2**

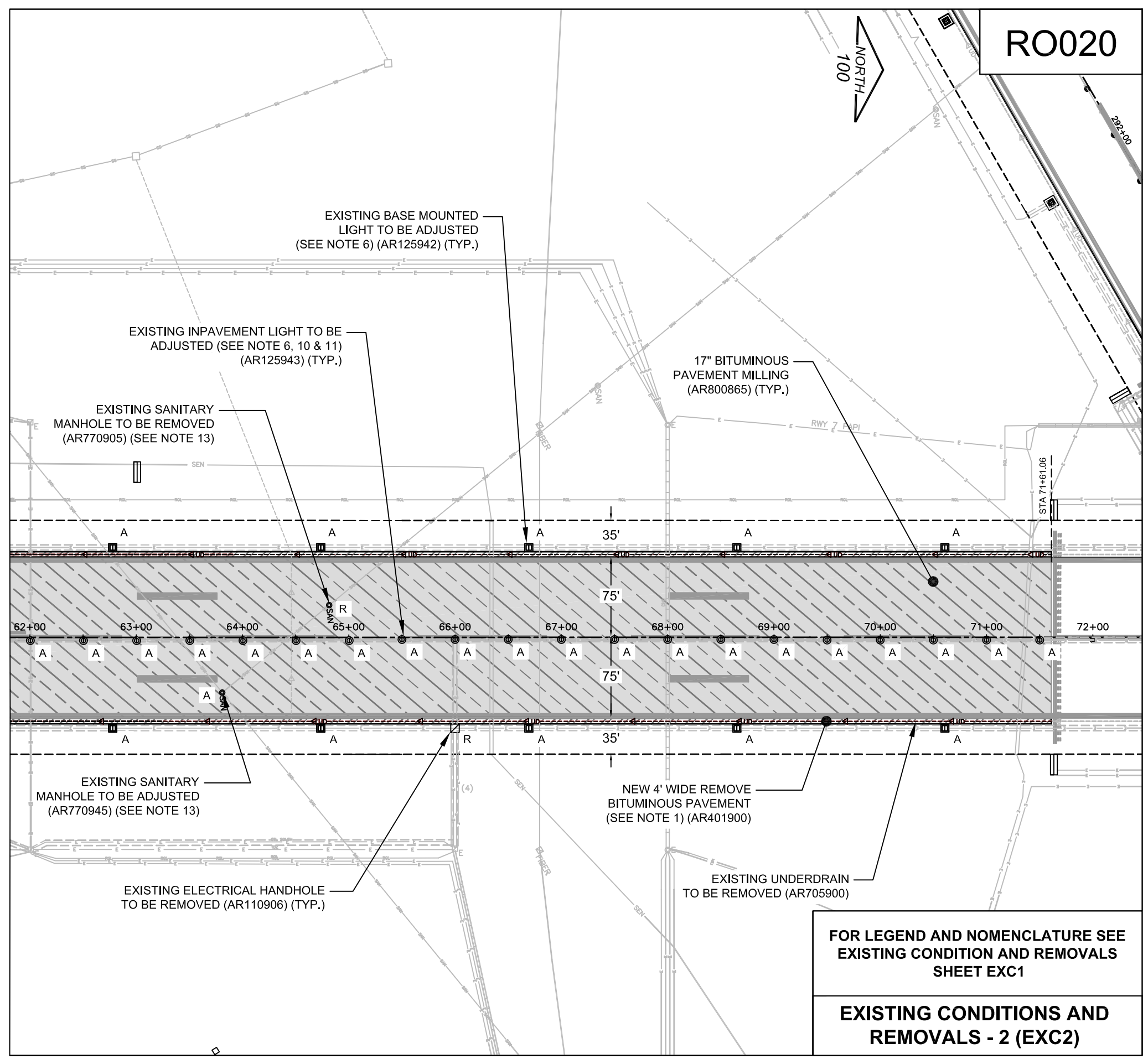
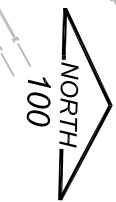
DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

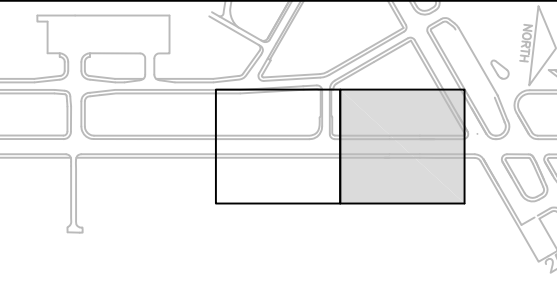
SHEET NO. **EXC2**    REVISION

SHEET 15 OF 70 SHEETS

**RO020**



- NOTES:**
- EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
  - TRANSFORMER TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT. CONCRETE LIGHT BASES SHALL BE DISPOSED OF OFF SITE.
  - ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
  - PAVEMENT MILLINGS SHALL BE USED AS FILL MATERIAL AT THE ONSITE BORROW LOCATION. ANY ADDITIONAL PAVEMENT MILLINGS SHALL BE STOCKPILED ON AIRPORT PROPERTY AT LOCATION DESIGNATED BY THE RESIDENT ENGINEER. ALL ASSOCIATED COST SHALL BE CONSIDERED INCIDENTAL TO THE CORRESPONDING BITUMINOUS PAVEMENT MILLING PAY ITEM.
  - ALL LIGHT BASES REMOVED SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 208. COST OF BACKFILL SHALL BE INCLUDED IN THE REMOVAL PAY ITEM.
  - ALL EXISTING LIGHT FIXTURES AND SIGNS WILL BE REUSED FOR THE FINAL ASSEMBLY OF THE ASSOCIATED PAY ITEM.
  - ANY PAVEMENT REMOVED BY THE CONTRACTOR BEYOND THE LIMITS SHOWN IN THE PLANS TO FACILITATE PCC PAVEMENT CONSTRUCTION AND SUBSEQUENT PAVEMENT REPLACEMENT SHALL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE PAVEMENT REMOVAL ITEM.
  - THE EXACT PAVEMENT MILLING LIMIT AND DEPTH REQUIRED SHALL BE DETERMINED FROM THE PLAN AND PROFILE AND THE GRADING PLAN.
  - ALL REGRADING AND RESHAPING OF EXISTING AGGREGATE BASE COURSE SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAVING PAY ITEM.
  - IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN EXISTING INPAVEMENT BASE CAN AND CONDUIT. ANY BASE CAN OR CONDUIT DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
  - ALL WORK REQUIRED TO PREPARE EXISTING INPAVEMENT BASE CAN AND CONDUIT FOR PAVING OPERATIONS SHALL BE INCIDENTAL TO THE ASSOCIATED PAVING PAY ITEM.
  - IF THE HMA PAVEMENT SECTION IS LESS THAN THE 17" PAVEMENT MILLING SHOWN, THE REMOVAL OF THE EXISTING AGGREGATE SUBBASE TO THE CORRECT PROFILE AND DIMENSIONS SHALL BE REMOVED BY PAVEMENT MILLING. THIS COST SHALL BE INCIDENTAL TO ASSOCIATED BITUMINOUS MILLING PAY ITEM.
  - THE EXISTING SANITARY MANHOLE TO BE ADJUSTED AND REMOVED ARE BURIED BELOW THE PAVEMENT SURFACE. THE LOCATION ON THE PLANS ARE APPROXIMATE LOCATIONS ONLY. ALL WORK TO FIELD VERIFY THE MANHOLE LOCATION AND TO EXPOSE THE MANHOLE SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM.



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

**FOR LEGEND AND NOMENCLATURE SEE  
EXISTING CONDITION AND REMOVALS  
SHEET EXC1**

**EXISTING CONDITIONS AND  
REMOVALS - 2 (EXC2)**



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

RUNWAY 1/19  
PLAN/PROFILE -1

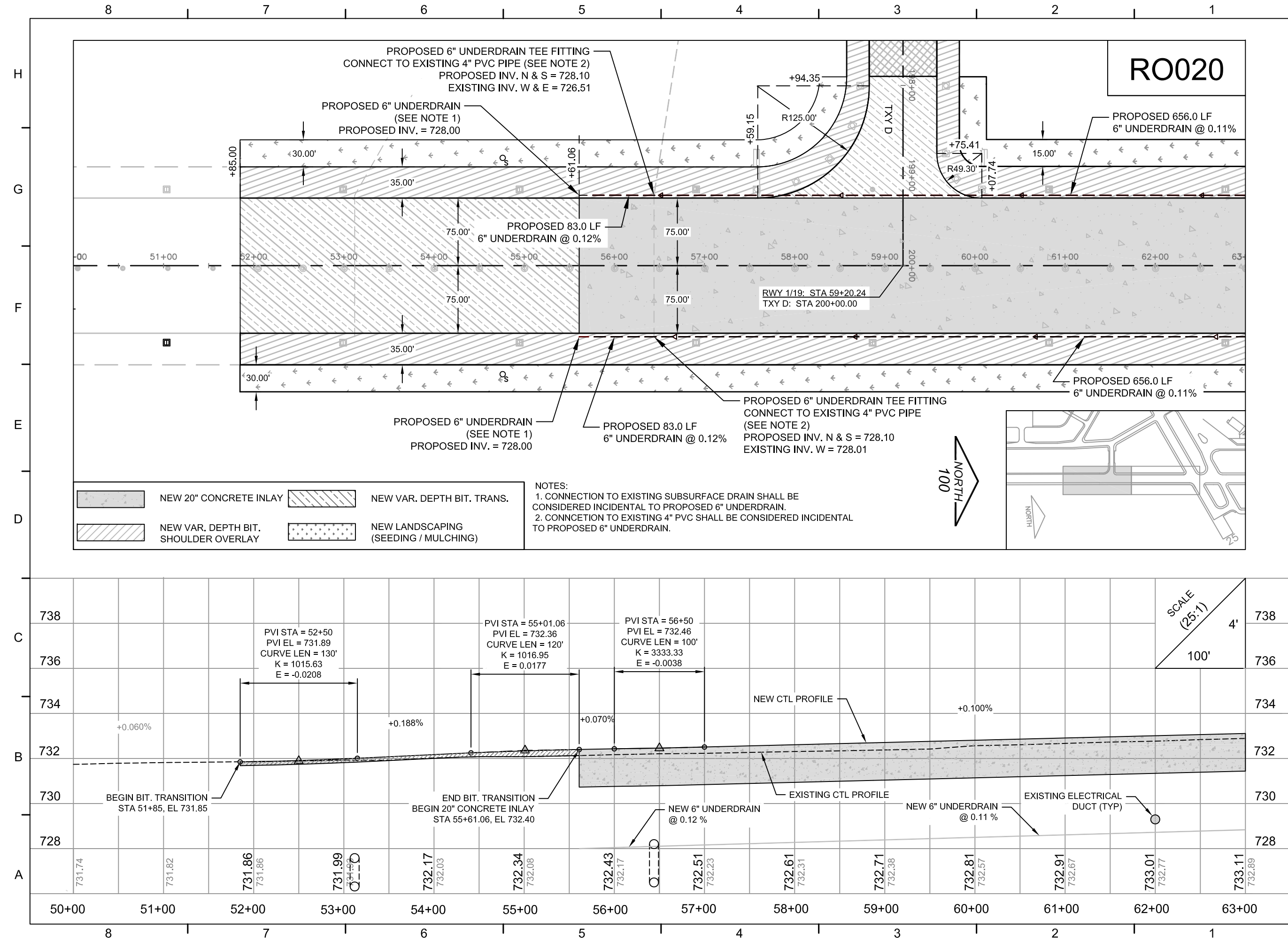
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. REVISION

RPP1

SHEET 16 OF 70 SHEETS







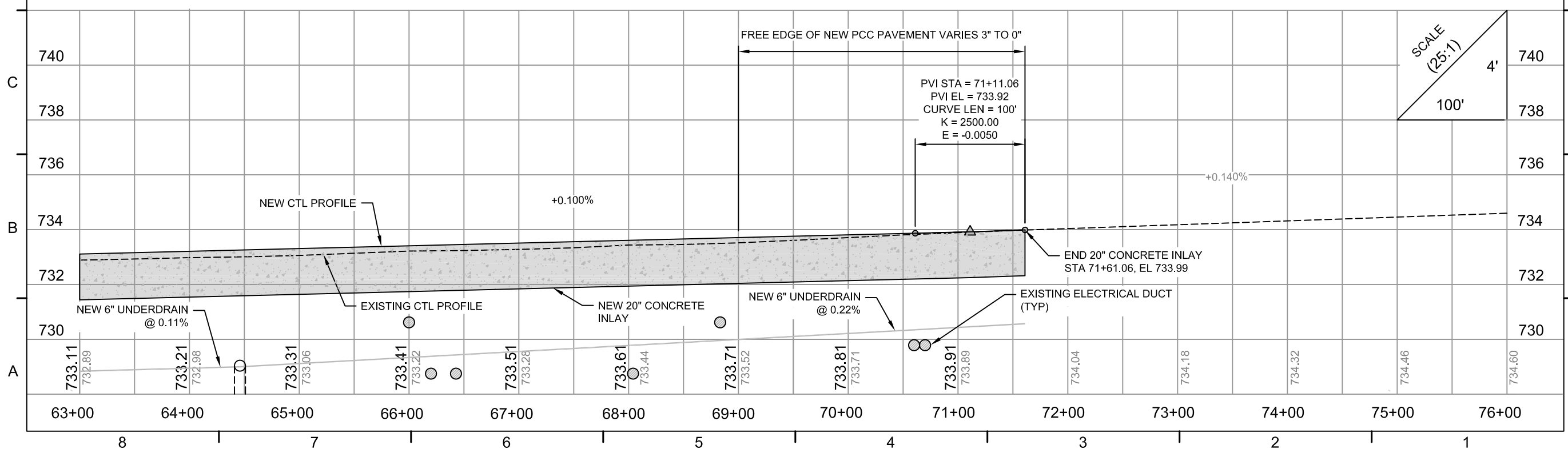
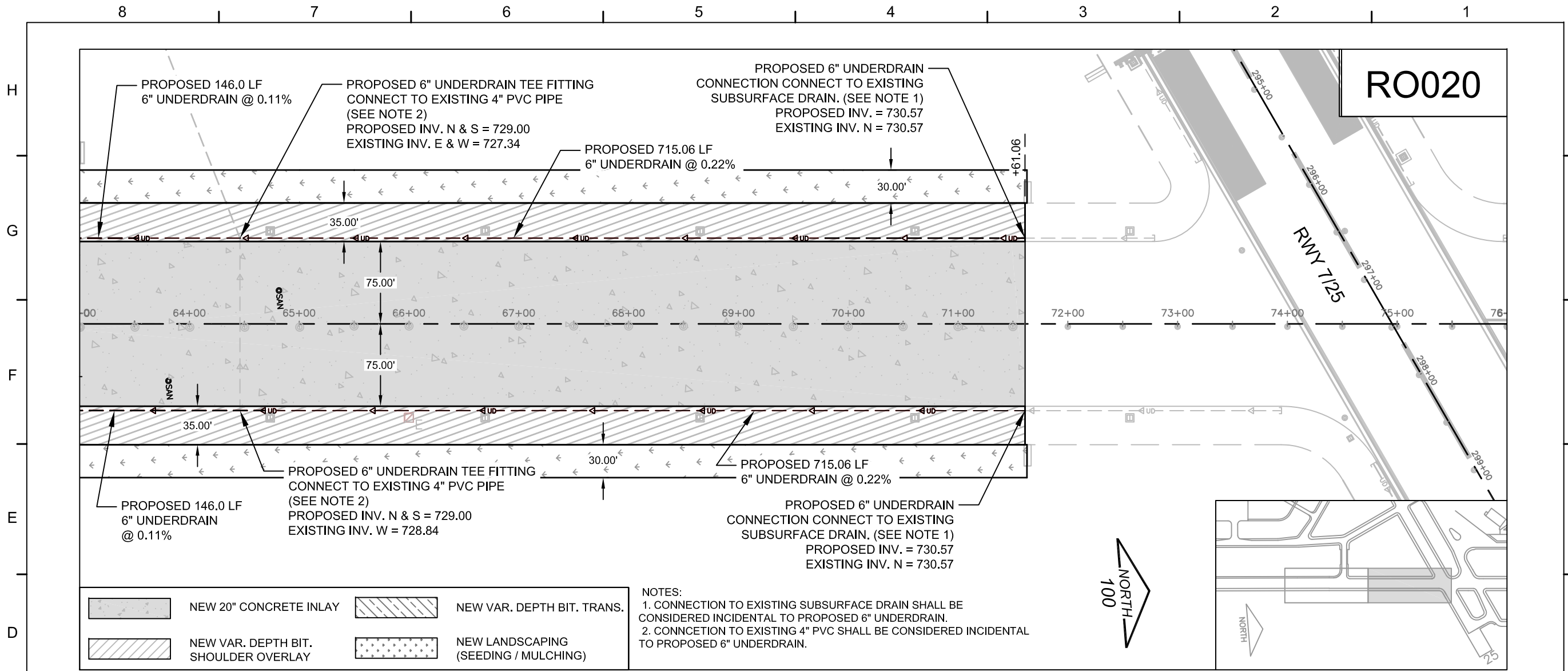
MIKE DUNN  
DIRECTOR



Chicago Rockford International Airport  
Rockford, Illinois

Greater Rockford Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183



APPROVED AS WORKING PLAN BY:

--	--

REV	DATE	DESCRIPTION

PROJECT NAME:

**REHABILITATE RUNWAY 1/19 PHASE 2**

SHEET TITLE:  
**RUNWAY 1/19 PLAN/PROFILE -2**

DESIGNED:	DRAWN:	CHECKED:
CMT-RFD	CMT-RFD	CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.	REVISION
<b>RPP2</b>	

SHEET 17 OF 70 SHEETS



MIKE DUNN  
DIRECTOR

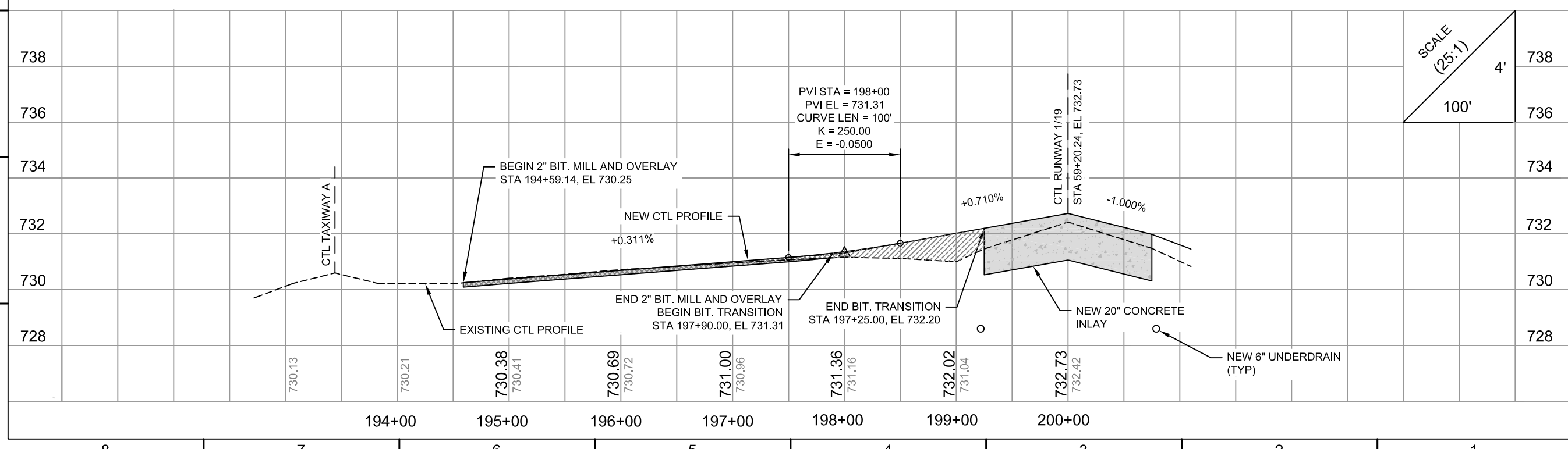
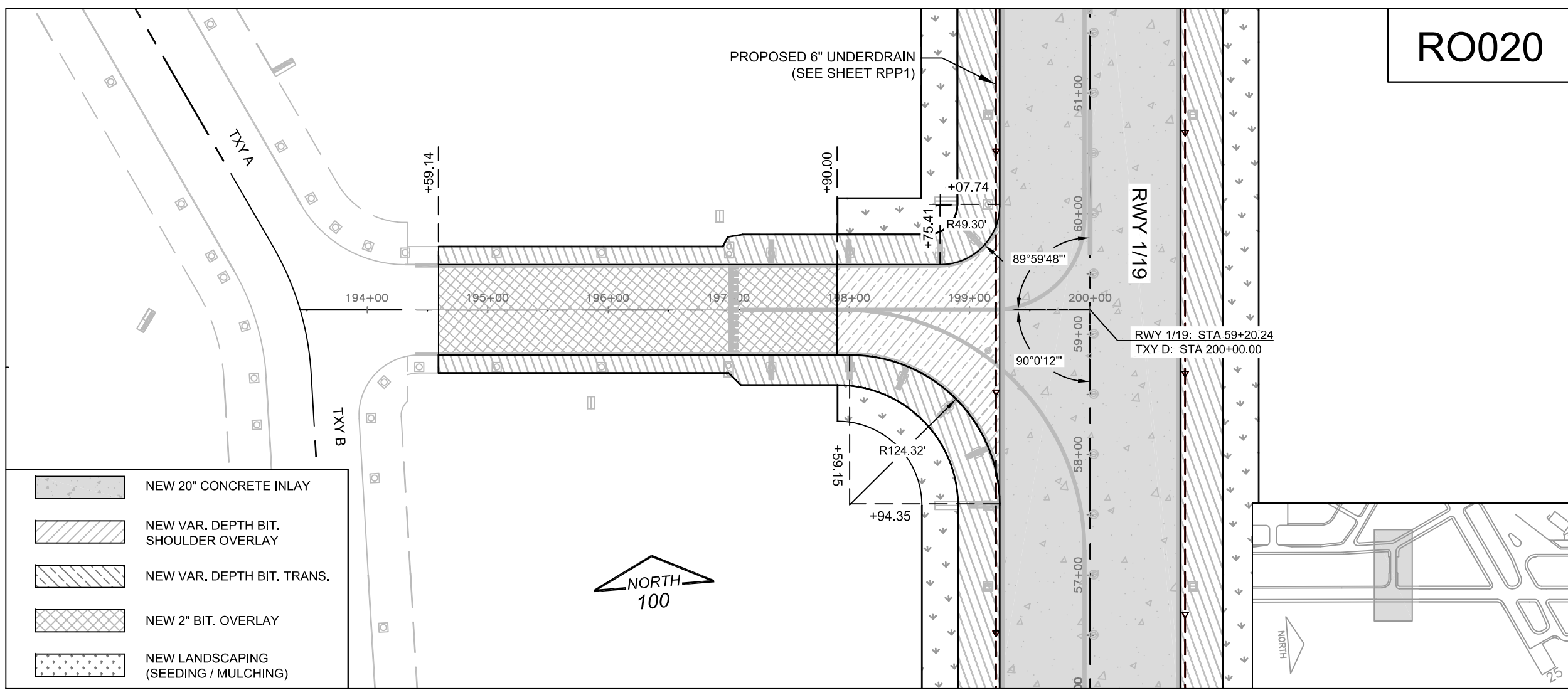


Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

RO020



APPROVED AS WORKING PLAN  
BY:


REV    DATE    DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
TAXIWAY D  
PLAN/PROFILE -1

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
PROJECT NO.: 11258-03-00/06	DATE: APRIL 20, 2012	
SHEET NO. <b>DPP1</b>	REVISION	

SHEET 18 OF 70 SHEETS





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN	
BY:	
REV	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

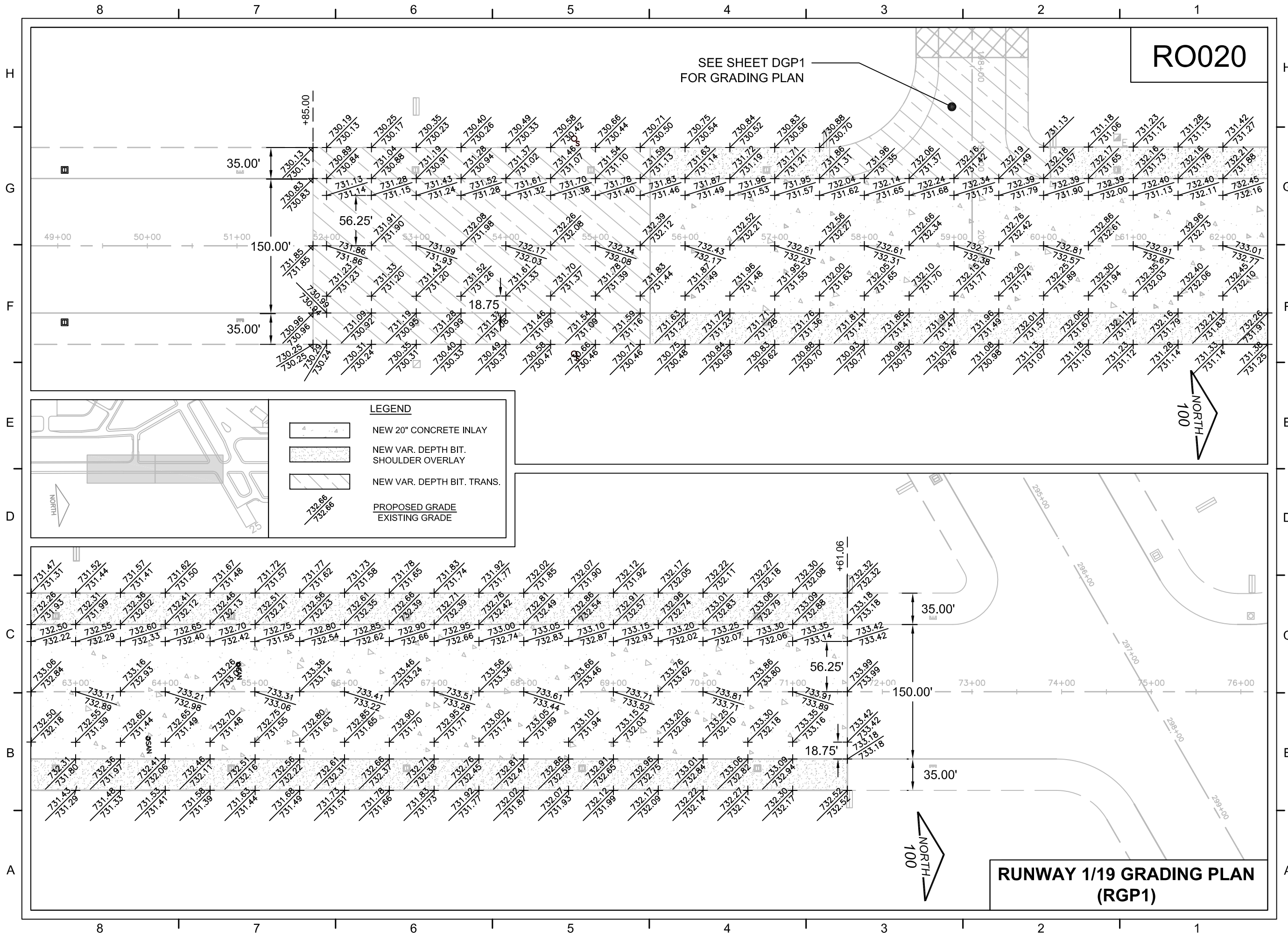
SHEET TITLE:  
**RUNWAY 1/19  
GRADING PLAN**

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **RGP1**    REVISION

SHEET 19 OF 70 SHEETS





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

TAXIWAY D  
GRADING PLAN

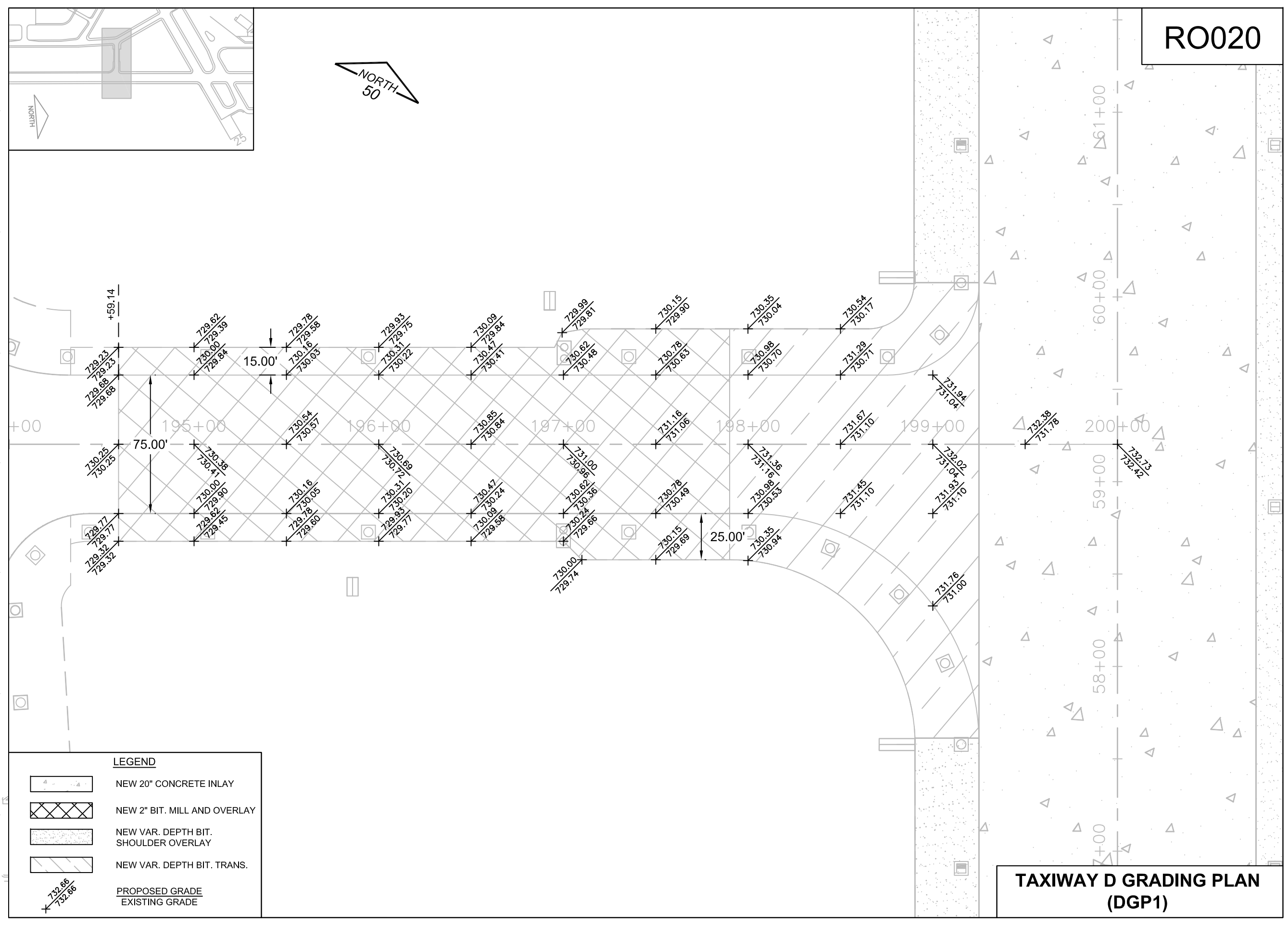
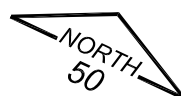
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **DGP1** REVISION

SHEET 20 OF 70 SHEETS

RO020



**TAXIWAY D GRADING PLAN  
(DGP1)**

**LEGEND**

- NEW 20" CONCRETE INLAY
- NEW 2" BIT. MILL AND OVERLAY
- NEW VAR. DEPTH BIT. SHOULDER OVERLAY
- NEW VAR. DEPTH BIT. TRANS.
- PROPOSED GRADE
- EXISTING GRADE

8 7 6 5 4 3 2 1

H

G

F

E

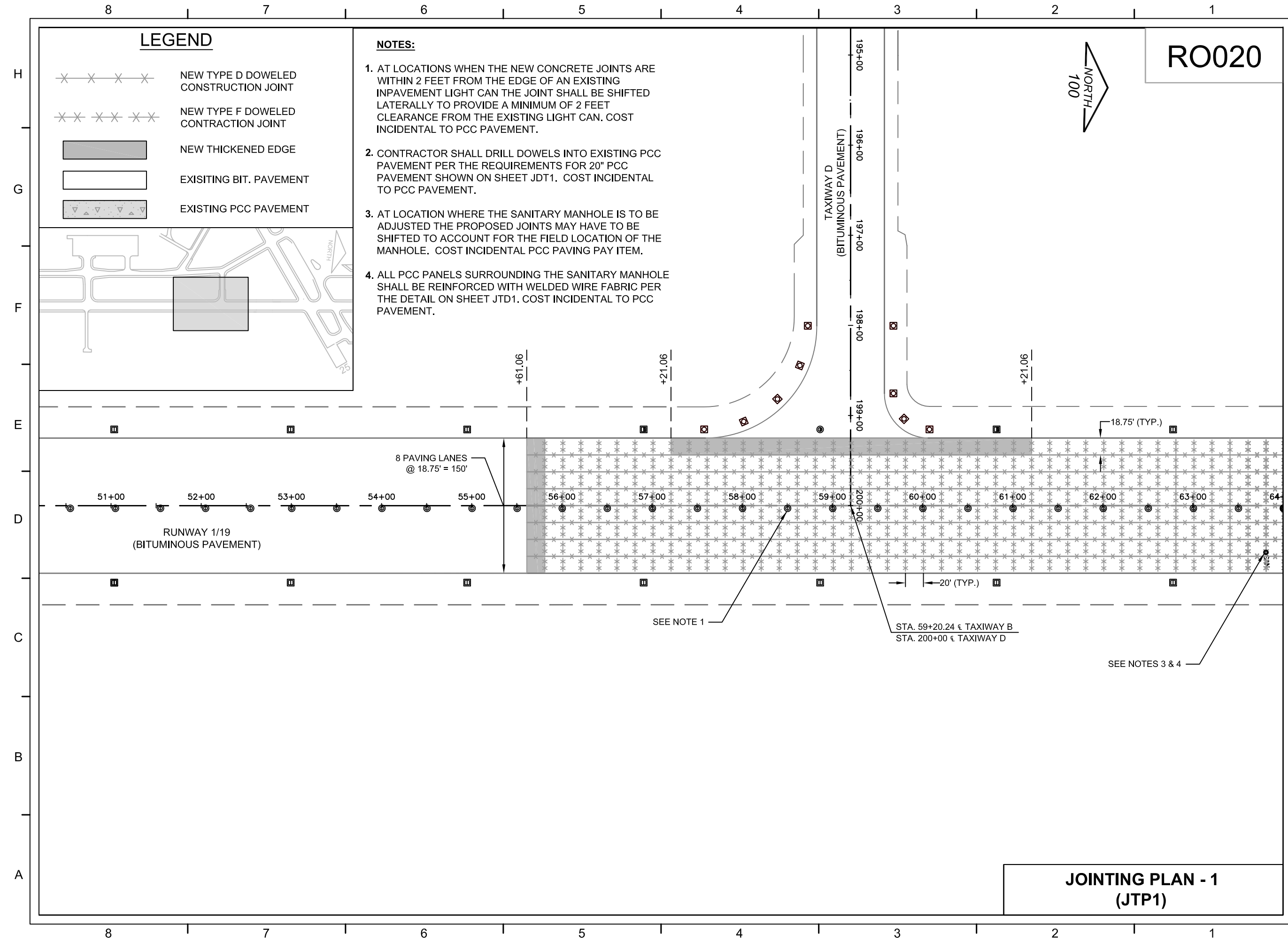
D

C

B

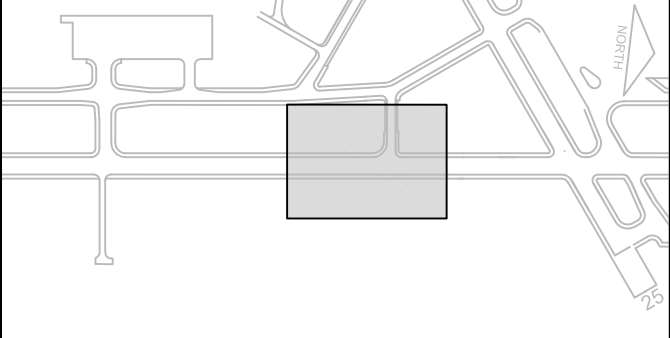
A





**LEGEND**

- NEW TYPE D DOWELED CONSTRUCTION JOINT
- NEW TYPE F DOWELED CONSTRUCTION JOINT
- NEW THICKENED EDGE
- EXISTING BIT. PAVEMENT
- EXISTING PCC PAVEMENT



**NOTES:**

1. AT LOCATIONS WHEN THE NEW CONCRETE JOINTS ARE WITHIN 2 FEET FROM THE EDGE OF AN EXISTING INPAVEMENT LIGHT CAN THE JOINT SHALL BE SHIFTED LATERALLY TO PROVIDE A MINIMUM OF 2 FEET CLEARANCE FROM THE EXISTING LIGHT CAN. COST INCIDENTAL TO PCC PAVEMENT.
2. CONTRACTOR SHALL DRILL DOWELS INTO EXISTING PCC PAVEMENT PER THE REQUIREMENTS FOR 20" PCC PAVEMENT SHOWN ON SHEET JDT1. COST INCIDENTAL TO PCC PAVEMENT.
3. AT LOCATION WHERE THE SANITARY MANHOLE IS TO BE ADJUSTED THE PROPOSED JOINTS MAY HAVE TO BE SHIFTED TO ACCOUNT FOR THE FIELD LOCATION OF THE MANHOLE. COST INCIDENTAL PCC PAVING PAY ITEM.
4. ALL PCC PANELS SURROUNDING THE SANITARY MANHOLE SHALL BE REINFORCED WITH WELDED WIRE FABRIC PER THE DETAIL ON SHEET JTD1. COST INCIDENTAL TO PCC PAVEMENT.

RO020



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

JOINTING PLAN - 1

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. JTP1    REVISION

SHEET 21 OF 70 SHEETS

**JOINTING PLAN - 1  
(JTP1)**



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

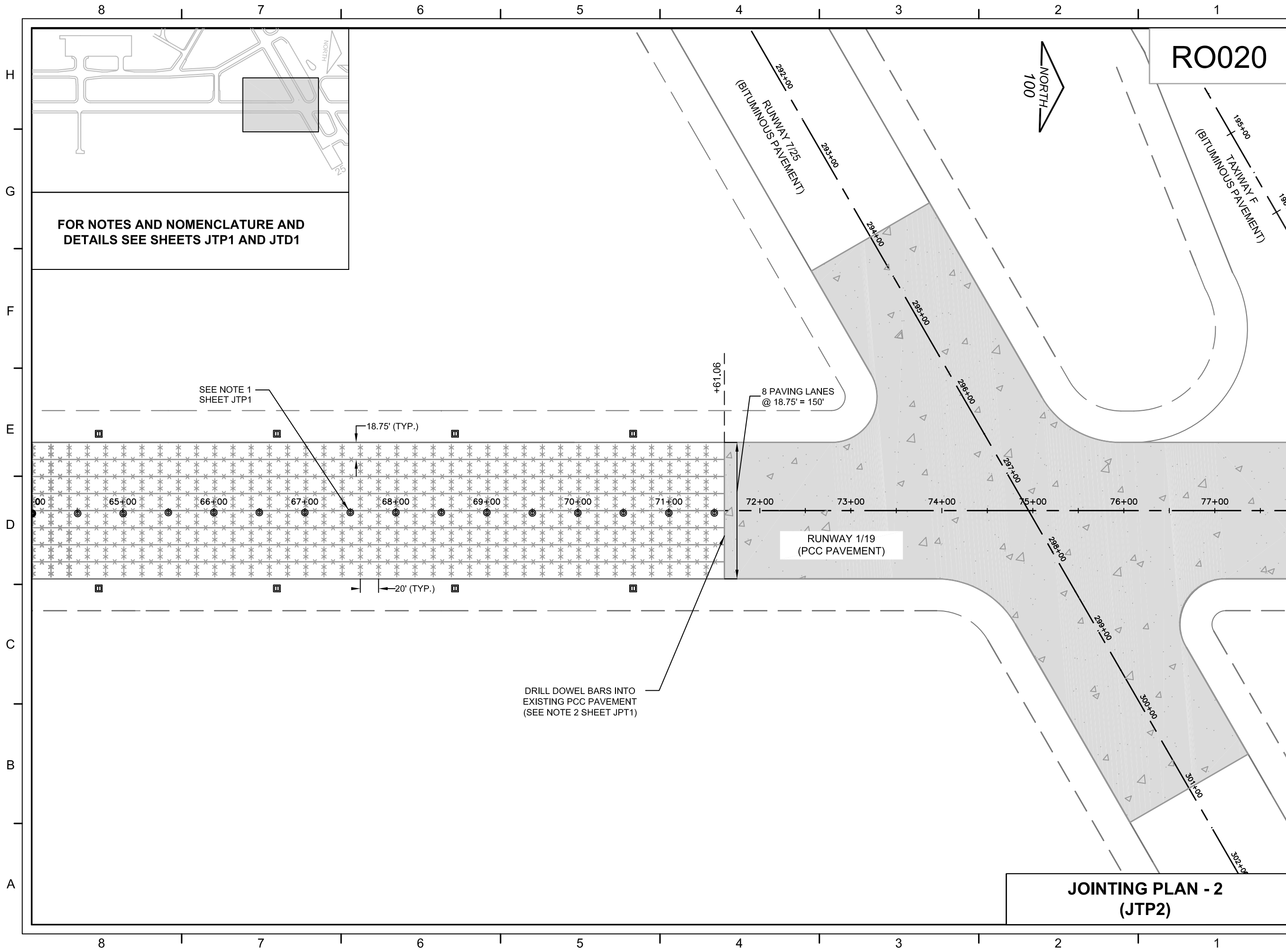
JOINTING PLAN - 2

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **JTP2**    REVISION

SHEET 22 OF 70 SHEETS



FOR NOTES AND NOMENCLATURE AND  
DETAILS SEE SHEETS JTP1 AND JTD1

SEE NOTE 1  
SHEET JTP1

8 PAVING LANES  
@ 18.75' = 150'

RUNWAY 1/19  
(PCC PAVEMENT)

DRILL DOWEL BARS INTO  
EXISTING PCC PAVEMENT  
(SEE NOTE 2 SHEET JPT1)

**JOINTING PLAN - 2  
(JTP2)**

**RO020**



+61.06

18.75' (TYP.)

20' (TYP.)

H  
G  
F  
E  
D  
C  
B  
A

H  
G  
F  
E  
D  
C  
B  
A

8 7 6 5 4 3 2 1

8 7 6 5 4 3 2 1

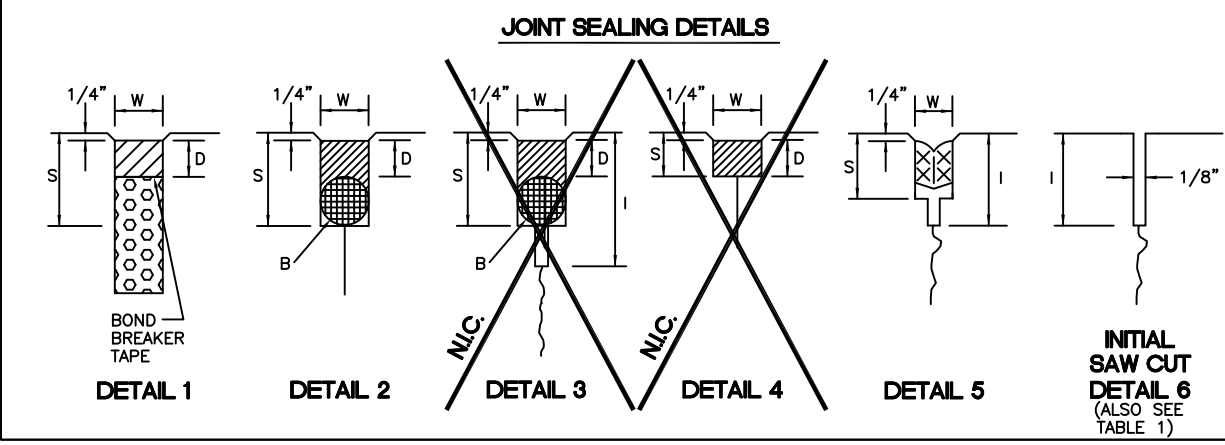
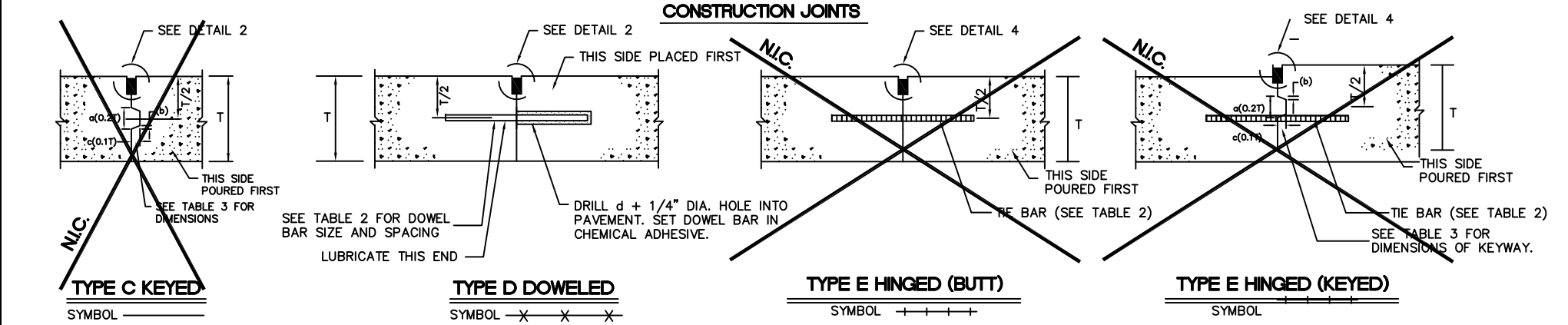
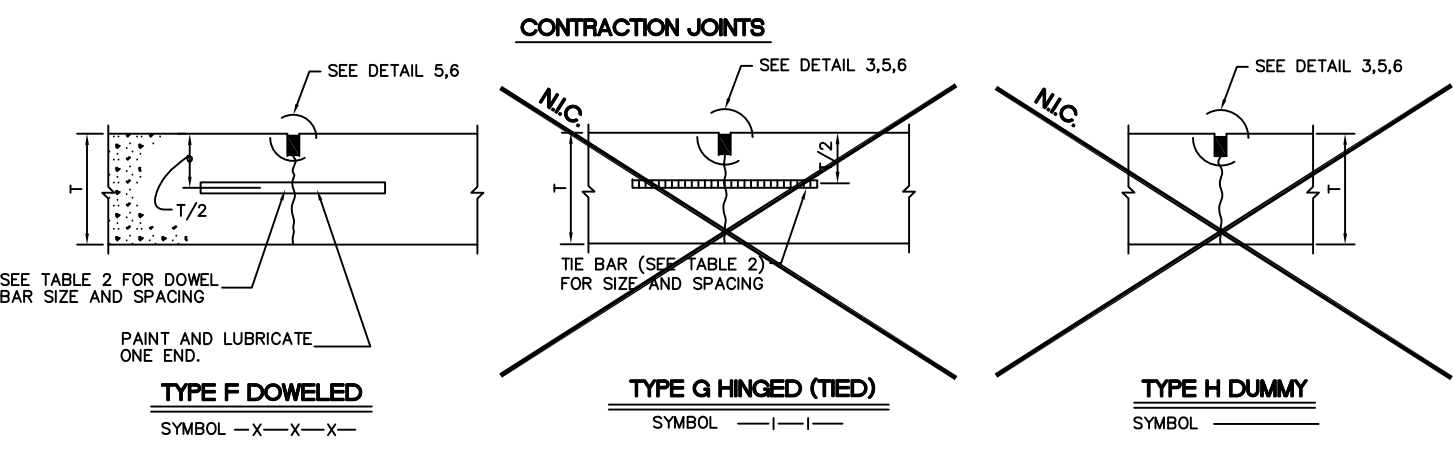
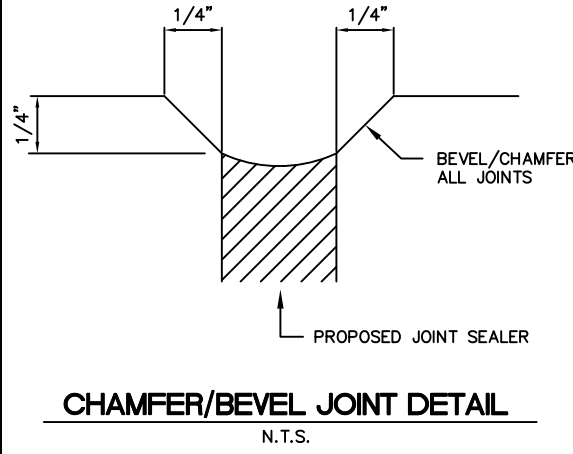
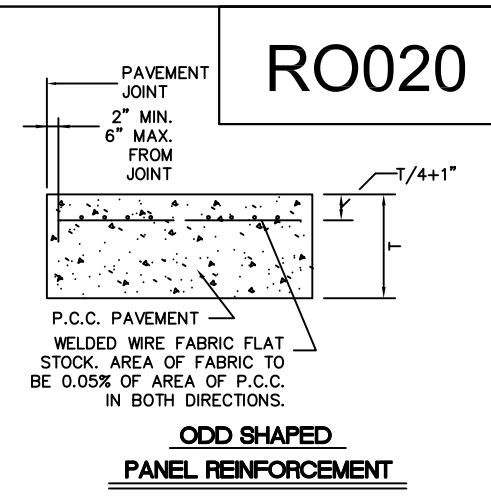
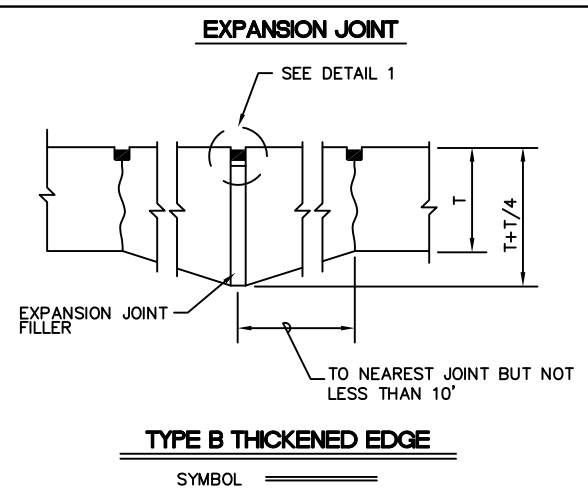


**TABLE 1**

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I=(T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"
19	6.33"
20	6.67"

**TABLE 2**

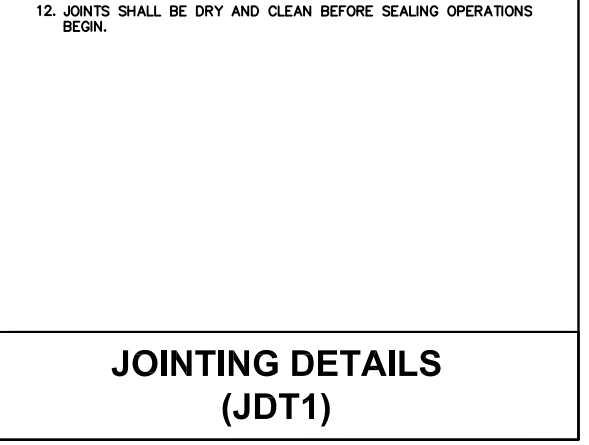
PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"
19	1 - 1/2"	20"	18"	#5	30"	30"
20	1 - 1/2"	20"	18"	#5	30"	30"



NOTE: JOINT SEALING SHALL BE PER SECTION 605 OF SPECIAL PROVISIONS.

	DETAIL 1	DETAIL 2	DETAIL 3	DETAIL 4	DETAIL 5
	HOT POUR	HOT POUR	HOT POUR	HOT/COLD POUR	SILICONE PREFORMED
W=WIDTH OF SEALANT RESERVOIR (IN.)	1-1/2	1/2	1/2	1/2	3/8 (COM-PRESSED)
D=DEPTH OF SEALANT RESERVOIR (IN.)	1-1/2	1/2	1/2	1/2	N/A
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1-3/8	3/4	1-1/2

- JOINTING NOTES**
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
  - THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
  - ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLY SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO INSTALLATION. ALTERNATE METHODS OF PLACEMENT OF DOWEL BARS MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. TRANSVERSE DOWEL BAR IMPLANTING WILL NOT BE ALLOWED.
  - ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
  - THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
  - COST OF ALL JOINT SAWING, CLEANING AND SEALING SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
  - SHOULD THE POURING OPERATION REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
  - EPOXY-COATED DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE RESIDENT ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
  - CONCRETE / BITUMINOUS INTERFACE SHALL BE SEALED PER TYPE E HINGED JOINT DETAIL ABOVE.
  - TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
  - CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS AT NO ADDITIONAL COST.
  - JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
**JOINTING DETAILS**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **JDT1** REVISION



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

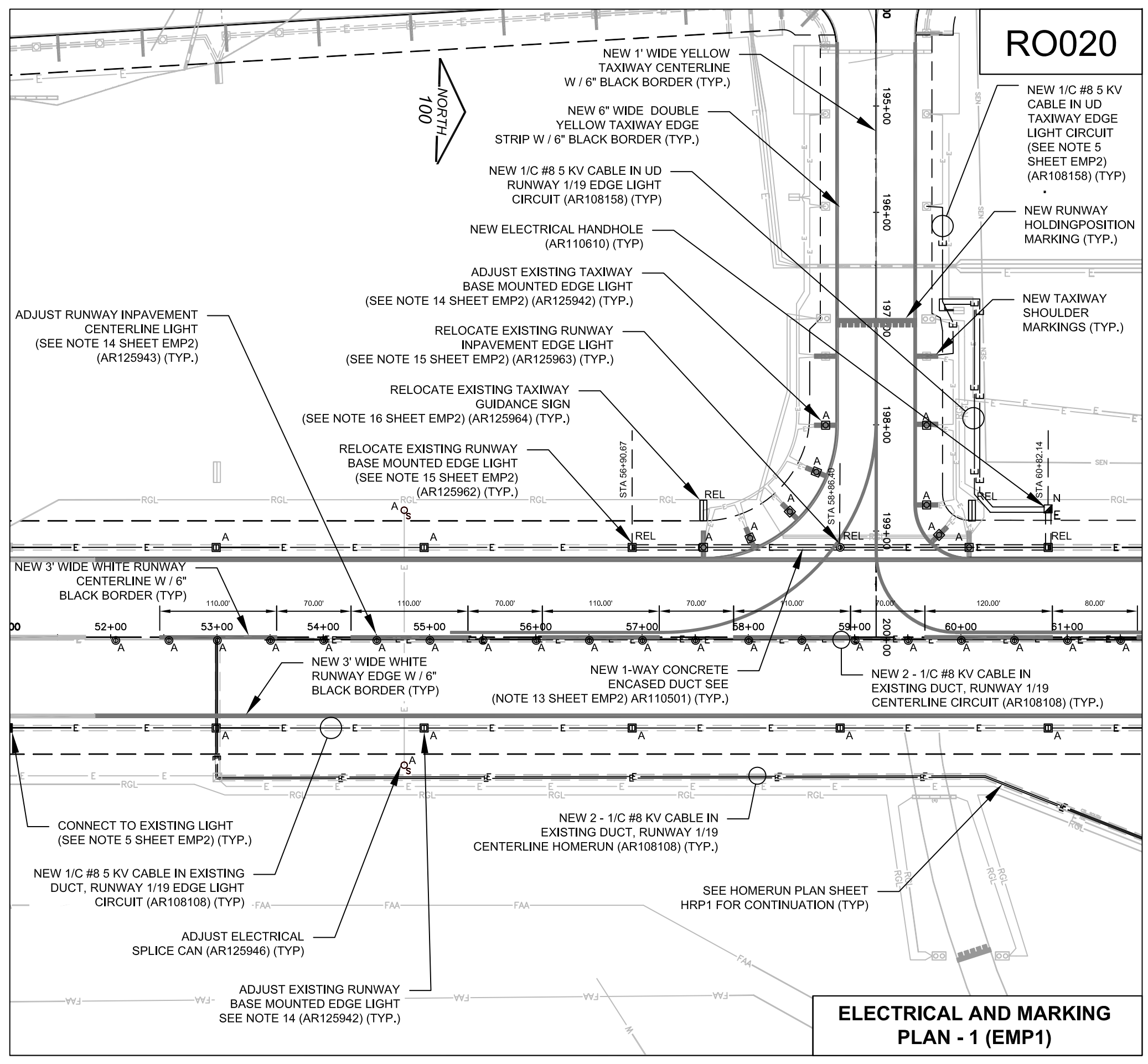
SHEET TITLE:  
**ELECTRICAL AND MARKING  
PLAN - 1**

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **EMP1**    REVISION

SHEET 24 OF 70 SHEETS



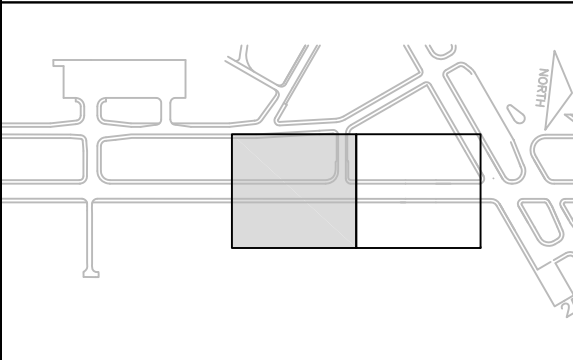
**RO020**



**LEGEND**

- NEW LIGHTING CIRCUIT - 1/C#8, 5KV L-824 TYPE C IN CONCRETE ENCASED 2" PVC DUCT
- NEW LIGHTING CIRCUIT - 1/C#8, 5KV L-824 TYPE C IN EXISTING DUCT
- NEW RUNWAY CIRCUIT - 1/C#8, 5KV L-824 TYPE C IN 3/4" UNIT DUCT
- EXISTING CIRCUITS
- NEW PAVEMENT MARKING
- EXISTING PAVEMENT MARKING
- EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- ADJUST EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING ELEVATED RUNWAY GUARD LIGHT
- NEW ELECTRICAL HANDHOLE
- EXISTING ELECTRICAL HANDHOLE
- EXISTING CONDUIT/DUCT
- RELOCATE EXISTING AIRFIELD GUIDANCE SIGN
- EXISTING AIRFIELD GUIDANCE SIGN
- RELOCATE BASE MOUNTED RUNWAY EDGE LIGHT
- ADJUST BASE MOUNTED RUNWAY EDGE LIGHT
- RELOCATE INPAVEMENT LIGHT
- ADJUST INPAVEMENT LIGHT
- EXISTING SPLICE CAN
- ADJUST SPLICE CAN
- EXISTING SURFACE SENSOR
- ADJUST SURFACE SENSOR

**FOR GENRAL NOTES SEE ELECTRICAL  
AND MARKING PLAN SHEET EMP2**



**ELECTRICAL AND MARKING  
PLAN - 1 (EMP1)**



**ELECTRICAL NOTES:**

1. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
2. AT ANY LOCATION WHERE THE PROPOSED DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
3. THE LOCATION OF EXISTING UTILITIES ARE APPROXIMATE. THE CONTRACTOR SHALL OPEN THE ENTIRE TRENCH BETWEEN MANHOLES BEFORE ANY CONDUIT IS LAID TO ASCERTAIN THE EXISTENCE AND POSITION OF ANY OBSTRUCTIONS.
4. CONTRACTOR SHALL COORDINATE THE LOCATION OF THE EXISTING AND PROPOSED UTILITIES PRIOR TO INSTALLATION OF THE PROPOSED UNIT DUCTS, CONDUITS AND DUCT BANKS. ANY DAMAGES TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
5. CONTRACTOR SHALL CONNECT EXISTING CABLE TO PROPOSED CABLE IN SIGN/LIGHT/MANHOLE/HANDHOLE. COST OF CONNECTION INCIDENTAL TO CABLE.
6. ALL NEW CABLE UNDER EXISTING PAVEMENT TO REMAIN SHALL BE INSTALLED WITHIN EXISTING CONDUITS. EXISTING CABLES SHALL BE REMOVED. COST OF REMOVAL SHALL BE INCIDENTAL TO NEW CABLE.
7. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.
8. THE ROUTING OF THE PROPOSED DUCTS AND CONDUITS ARE SHOWN FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE COORDINATED WITH THE RESIDENT ENGINEER.
9. SEE DETAILS ON SHEETS ELD1-6.
10. ALL EXISTING UNUSED AIRFIELD LIGHTING CABLE SHALL BE REMOVED FROM THE EXISTING DUCT. COST SHALL BE INCIDENTAL TO INSTALLATION OF NEW CABLE.
11. ALL WORK REQUIRED TO CONNNECT NEW LIGHT / SIGN BASE CAN AND NEW ELECTRICAL HANDHOLE TO EXISTING CONDUIT / CABLE SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED ELECTRICAL PAY ITEM.

12. CONNECT IN-PAVEMENT 1/C #6 INSULATED GROUNDING CONDUCTOR TO GROUND ROD AT EDGE LIGHT BASE. TAXIWAY CENTERLINE LIGHTS SHALL BE INSTALLED SO THAT THE NEAREST EDGE IS MINIMUM OF 2 FEET FROM ANY CONCRETE PAVEMENT JOINT.
13. EXISTING PAVEMENT SHALL BE SAWED AND REMOVED IN ACCORDANCE WITH THE ELECTRICAL DETAILS AND NEW PVC CONDUIT INSTALLED. THE TRENCH SHALL BE BACKFILLED WITH 610 CONCRETE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND IN ACCORDANCE WITH THE ELECTRICAL DETAILS. PAVEMENT REMOVAL AND CONCRETE BACKFILL SHALL BE INCIDENTAL TO THE CONCRETE ENCASED DUCT PAY ITEM.
14. ADJUST BASE-MOUNTED AND IN-PAVEMENT LIGHT SHALL INCLUDE INSTALLATION OF REQUIRED GASKETS,EXTENSIONS, SPACER RINGS, Y-FLANGES AND MUD PLATES AS NEEDED, SEALANT, NEW L-830 ISOLATION TRANSFORMERS, L-823 CONNECTORS AND GROUNDING. EXISTING LIGHT FIXTURES AND CANS SHALL BE RE-USED.

15. RELOCATE BASE MOUNTED AND IN-PAVEMENT LIGHT SHALL INCLUDE INSTALLATION OF NEW BASE CANS, REQUIRED GASKETS, EXTENSIONS, SPACER RINGS, Y-FLANGES AND MUD PLATES AS NEEDED, SEALANT, NEW L-830 ISOLATION TRANSFORMERS, L-823 CONNECTORS AND GROUNDING. EXISTING LIGHT FIXTURES WILL BE RE-USED.
16. RELOCATE EXISTING SIGN SHALL INCLUDE NEW CONCRETE FOUNDATION, NEW L-867 CAN, NEW L-830 TRANSFORMER AND GROUNDING. EXISTING SIGN SHALL BE REMOVED AND RE-INSTALLED. EXISTING FOUNDATION SHALL BE COMPLETELY REMOVED.

**PAVEMENT MARKING NOTES**

1. SEE DETAILS ON SHEETS MRD 1-3.
2. ALL RUNWAY MARKINGS ARE WHITE WITH A 6" BLACK BORDER.
3. ALL TAXIWAY MARKINGS ARE YELLOW WITH A 6" BLACK BORDER.
4. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
5. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE GAPPED 6" EACH SIDE OF RUNWAY STRIPE. EXCEPT RUNWAY/RUNWAY HOLDING.
6. CURING COMPOUND ON CONCRETE PAVEMENTS SHALL BE REMOVED BY WATER BLASTING OR OTHER METHODS APPROVED BY THE ENGINEER PRIOR TO MARKING.
7. ALL PROPOSED MARKING MAY NOT BE CALLED OUT FOR CLARITY. ALL MARKING WITHIN THE PROPOSED IMPROVEMENT LIMITS TO INCLUDE THE ADDITIVE ALTERNATES, IF AWARDED, WILL BE RE-MARKED. THE RESIDENT ENGINEER WILL PROVIDE MARKING DIMENSIONS AS NEEDED.

RO020



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

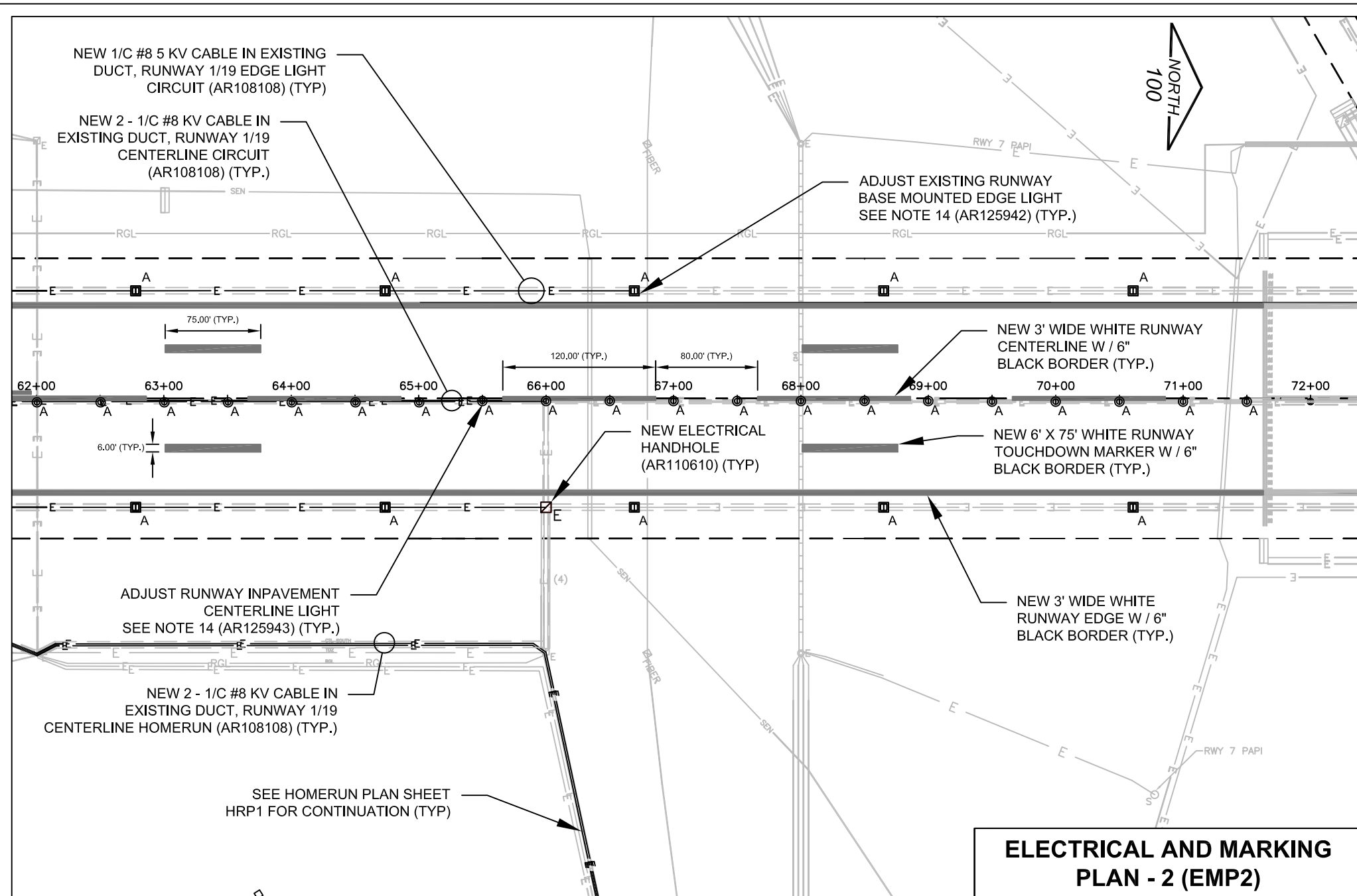
SHEET TITLE:  
**ELECTRICAL AND MARKING  
PLAN - 2**

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

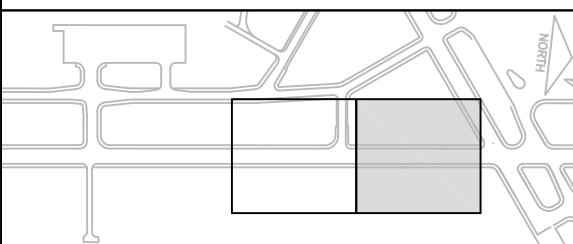
PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **EMP2**    REVISION

SHEET 25 OF 70 SHEETS



**FOR LEGEND AND NOMENCLATURE SEE  
ELECTRICAL AND MARKING PLAN  
SHEET EMP1**



**ELECTRICAL AND MARKING  
PLAN - 2 (EMP2)**







MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION  
PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

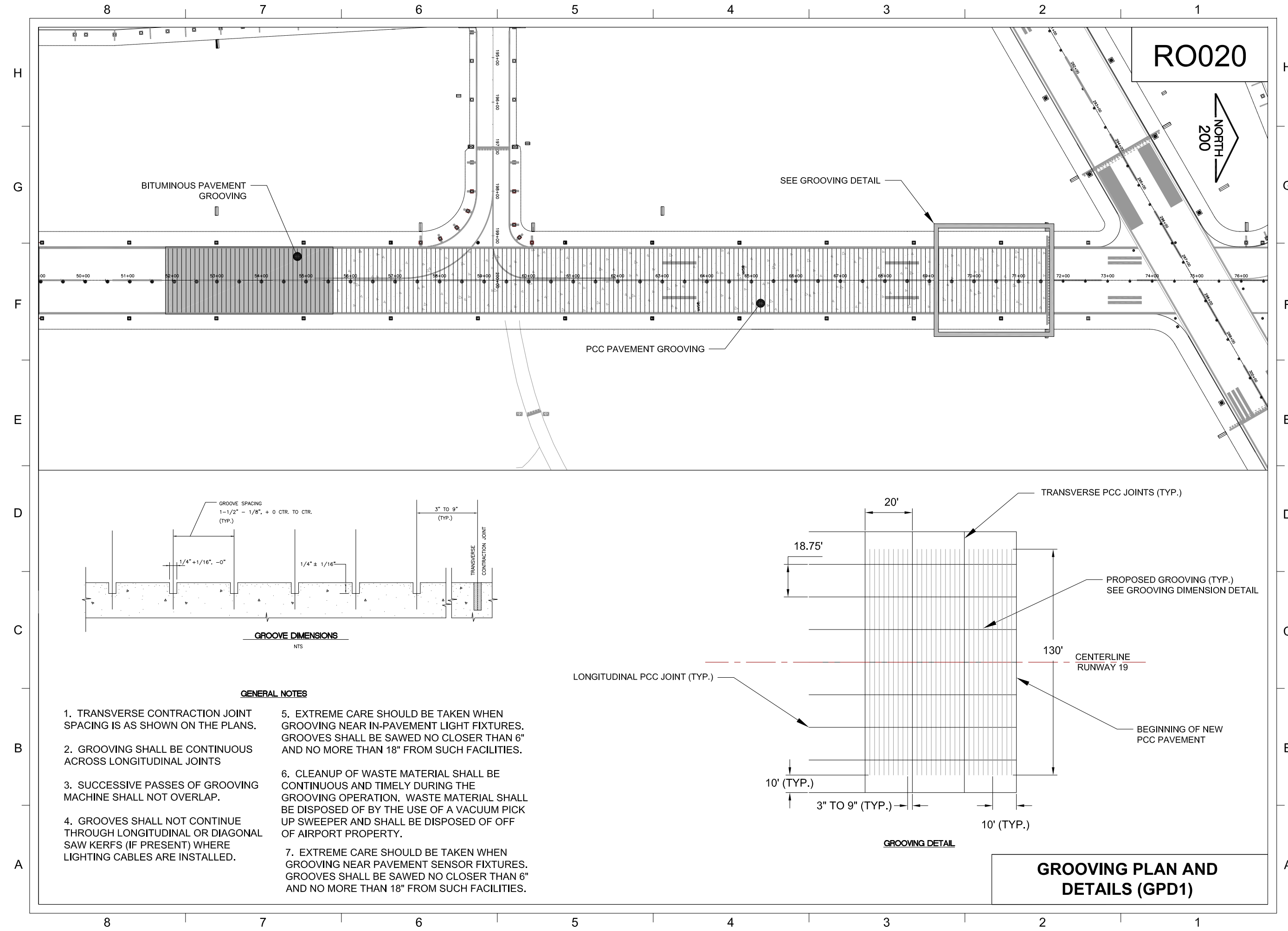
GROOVING PLAN AND  
DETAILS

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. GPD1 REVISION

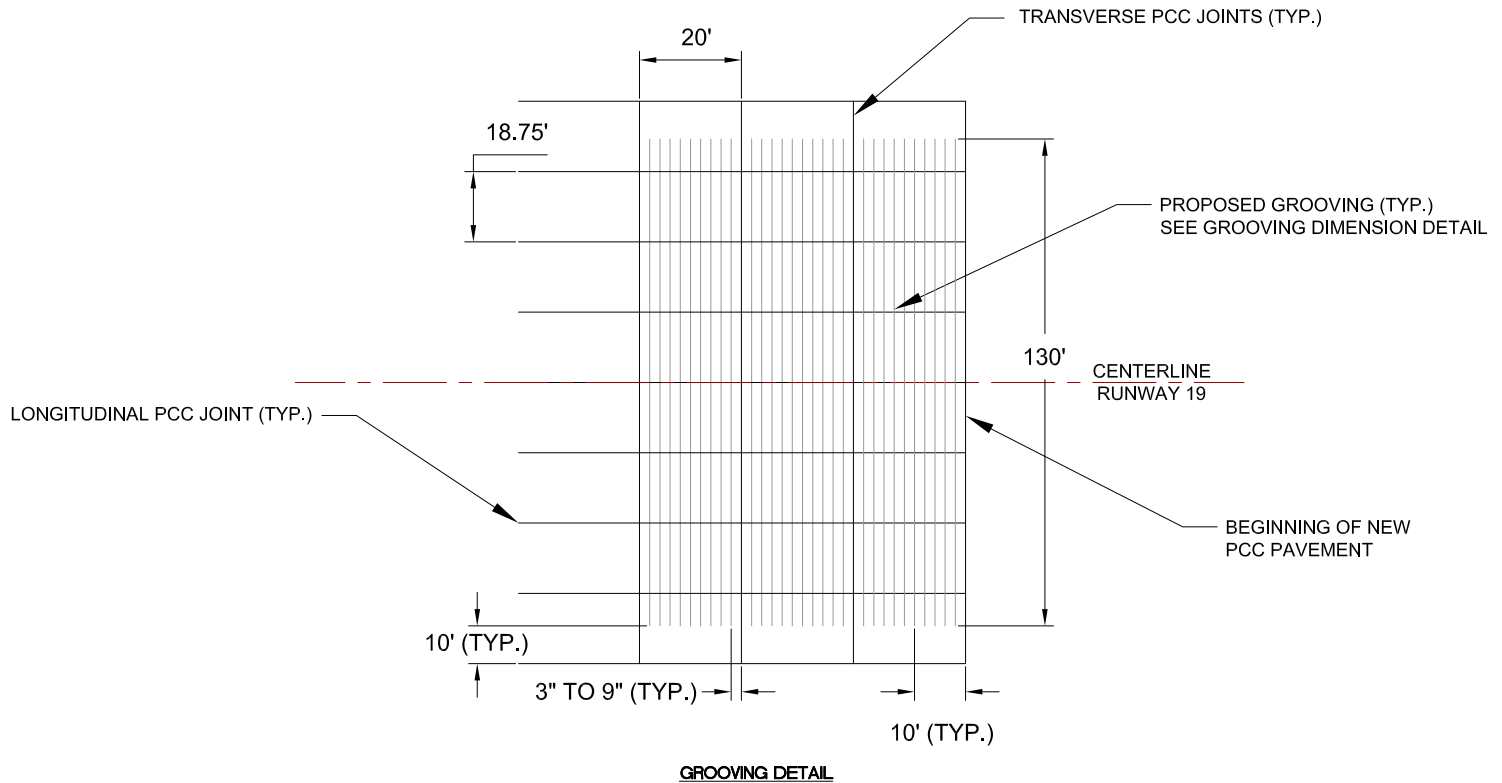
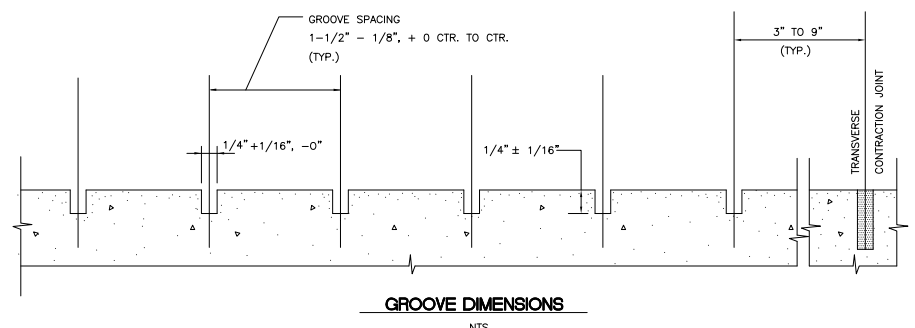
SHEET 27 OF 70 SHEETS



BITUMINOUS PAVEMENT  
GROOVING

SEE GROOVING DETAIL

PCC PAVEMENT GROOVING



**GENERAL NOTES**

1. TRANSVERSE CONTRACTION JOINT SPACING IS AS SHOWN ON THE PLANS.
2. GROOVING SHALL BE CONTINUOUS ACROSS LONGITUDINAL JOINTS
3. SUCCESSIVE PASSES OF GROOVING MACHINE SHALL NOT OVERLAP.
4. GROOVES SHALL NOT CONTINUE THROUGH LONGITUDINAL OR DIAGONAL SAW KERFS (IF PRESENT) WHERE LIGHTING CABLES ARE INSTALLED.
5. EXTREME CARE SHOULD BE TAKEN WHEN GROOVING NEAR IN-PAVEMENT LIGHT FIXTURES. GROOVES SHALL BE SAWED NO CLOSER THAN 6" AND NO MORE THAN 18" FROM SUCH FACILITIES.
6. CLEANUP OF WASTE MATERIAL SHALL BE CONTINUOUS AND TIMELY DURING THE GROOVING OPERATION. WASTE MATERIAL SHALL BE DISPOSED OF BY THE USE OF A VACUUM PICK UP SWEEPER AND SHALL BE DISPOSED OF OFF OF AIRPORT PROPERTY.
7. EXTREME CARE SHOULD BE TAKEN WHEN GROOVING NEAR PAVEMENT SENSOR FIXTURES. GROOVES SHALL BE SAWED NO CLOSER THAN 6" AND NO MORE THAN 18" FROM SUCH FACILITIES.

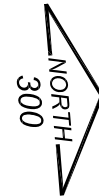
**GROOVING PLAN AND  
DETAILS (GPD1)**

### BITUMINOUS PAVEMENT CORES

BORING / CORE NUMBER	HMA THICKNESS SURFACE & BINDER (INCHES)	AGG. BASE THICKNESS (INCHES)
B-5	14.5	11
B-6	17.8	13
B-7	18.2	17
B-8	15.0	11
B-9	15.0	13

**NOTES:**  
ADDITIONAL SOIL BORING INFORMATION IS AVAILABLE UPON REQUEST.

RO020



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

### ENGINEERING INFORMATION

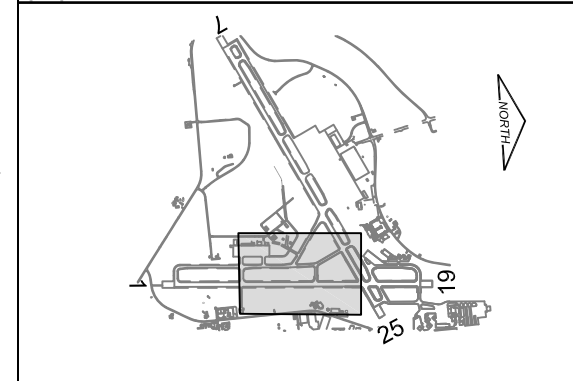
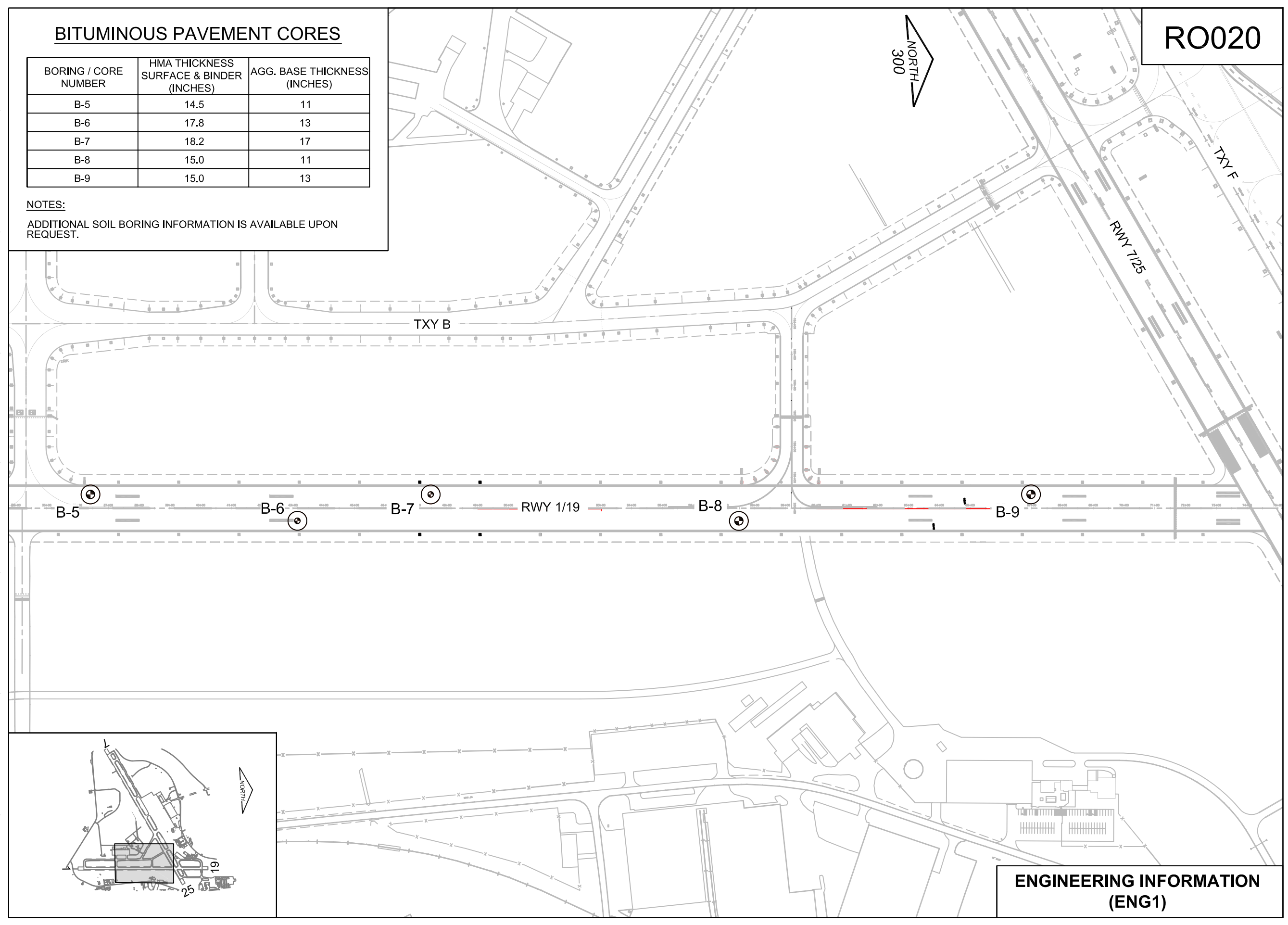
DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **ENG1**    REVISION

SHEET 28 OF 70 SHEETS

**ENGINEERING INFORMATION  
(ENG1)**







MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**GENERAL NOTES & LEGEND  
TXYS F, K & M**

DESIGNED: SMS      DRAWN: CMT      CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **GNL1** REVISION

SHEET 29 OF 70 SHEETS

**RO020**

**EXISTING AND PROPOSED LEGEND**

	<b>NEW FULL-STRENGTH PAVEMENT WIDENING</b> 2" HMA SURFACE COURSE (401) 14" HMA BASE COURSE (403) 12" CRUSHED AGGREGATE BASE COURSE (209)
	<b>NEW PAVED SHOULDER</b> 4" HMA SURFACE COURSE 8" CRUSHED AGGREGATE BASE COURSE (209)
	<b>FULL DEPTH PAVEMENT REMOVAL (12" AVG.)</b>
	<b>EXISTING / NEW STORM SEWER</b>
	<b>EXISTING / NEW DUCT</b>
	<b>EXISTING / NEW RUNWAY 7/25 LIGHT CIRCUIT</b> (NUMBER INDICATES NUMBER OF CABLES)
	<b>EXISTING / NEW TAXIWAY F LIGHT CIRCUIT</b> (NUMBER OF CABLES, D = IN 2" PVC CONCRETE ENCASED DUCT)
	<b>EXISTING / NEW PAPI POWER CABLE</b>
	<b>EXISTING / NEW RDR CIRCUIT (NUMBER OF CABLES, IN UNIT DUCT)</b>
	<b>EXISTING / NEW RGL CIRCUIT</b> (NUMBER INDICATES NUMBER OF CABLES, IN UNIT DUCT)
	<b>EXISTING / NEW UNDERDRAIN</b>
	<b>EXISTING / NEW OR RELOCATED MITL, BASE MOUNTED</b>
	<b>EXISTING / NEW OR RELOCATED HIRL, BASE MOUNTED</b>
	<b>EXISTING / NEW OR RELOCATED HIRL, FLUSH MOUNTED</b>
	<b>EXISTING / NEW UNDERDRAIN CLEANOUT, DIRECT CONNECTION OR COLLECTION STRUCTURE</b>
	<b>EXISTING / RELOCATED RGL</b>
	<b>EXISTING / RELOCATED TAXIWAY GUIDANCE SIGN</b>
	<b>EXISTING / NEW DRAINAGE INLET</b>
	<b>EXISTING / NEW SPLICE CAN</b>
	<b>EXISTING / NEW ELECTRICAL HANDHOLE</b>
	<b>EXISTING / NEW MANHOLE</b>
	<b>EXISTING ITEM TO BE REMOVED</b>
	<b>EXISTING ITEM TO BE ADJUSTED</b>
	<b>EXISTING ITEM TO BE RELOCATED</b>

**EXISTING CONDITIONS AND REMOVALS NOTES, TAXIWAYS F, K AND M:**

- EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND THE PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO PAVEMENT REMOVAL.
- LIGHTS AND SIGNS TO BE RELOCATED SHALL BE STORED AND PROTECTED UNTIL READY FOR RE-INSTALLATION. THE EXISTING SIGN FOUNDATIONS AND LIGHT BASES SHALL BE FULLY REMOVED AND DISPOSED OF OFF-SITE UNLESS OTHERWISE NOTED.
- ANY TEMPORARY CABLING REQUIRED TO KEEP EXISTING CIRCUITS IN OPERATION DURING CONSTRUCTION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MISCELLANEOUS 2" GRS CONDUIT AND EXISTING SIGN BASES WITHOUT SIGNS SHALL BE REMOVED AS NOTED IN THE PLANS. REMOVAL OF THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO UNCLASSIFIED EXCAVATION.
- THE REPLACEMENT OF INLETS WITH REINFORCED CONCRETE PIPE (CLASS IV) SHALL INCLUDE THE COSTS TO REMOVE THE INLET, RC PIPE OF THE TYPE AND SIZE SHOWN, PIPE BEDDING, PCC COLLARS, EXCAVATION, BACKFILL AND COMPACTION OF THE FILL MATERIALS.

**ELECTRICAL NOTES, TAXIWAYS F, K AND M:**

- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND THE AIRPORT FOR ALL PHASES. TEMPORARY CABLING AND SPLICING NECESSARY KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- AT ANY LOCATION WHERE THE PROPOSED DUCT OR CABLE ROUTING SHALL CROSS AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE LOCATIONS OF EXISTING UTILITIES ARE APPROXIMATE. THE CONTRACTOR SHALL OPEN THE ENTIRE TRENCH BETWEEN END POINTS BEFORE ANY CONDUIT IS LAID TO ASCERTAIN THE EXISTENCE AND POSITION OF ANY OBSTRUCTIONS.
- THE CONTRACTOR SHALL COORDINATE THE LOCATION OF THE EXISTING AND PROPOSED UTILITIES PRIOR TO INSTALLATION OF THE PROPOSED UNIT DUCTS, CONDUITS AND DUCT BANKS. ANY DAMAGES TO EXISTING UTILITIES SHALL BE IMMEDIATELY REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- CABLES BEING REPLACED IN EXISTING DUCTS SHALL BE REMOVED AND DISPOSED OF OFF SITE. COSTS TO REMOVE THE EXISTING CABLE SHALL BE CONSIDERED INCIDENTAL TO THE NEW CABLE.
- THE ROUTING OF EXISTING AND PROPOSED CABLES AND DUCTS IS FOR INFORMATION ONLY. THE EXACT ROUTING AND LAYOUT SHALL BE COORDINATED IN THE FIELD AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO INSTALLATION.
- RELOCATED EDGE LIGHTING FIXTURES AND GUIDANCE SIGNS SHALL BE INSTALLED ON NEW FOUNDATIONS WITH A NEW ISOLATION TRANSFORMER AND NEW GROUNDING. SIGNS SHALL BE MOUNTED WITH NEW MOUNTING HARDWARE.
- HIGH INTENSITY FLUSH MOUNT RUNWAY LIGHTS SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
- NEW TAXIWAY EDGE LIGHTS TO BE INSTALLED ALONG TAXIWAYS F, K OR M SHALL BE INCANDESCENT FIXTURES.
- AT LOCATIONS WHERE NEW AIRFIELD LIGHTING CIRCUITS ARE CONNECTED TO EXISTING CIRCUITS, NEW TRANSFORMERS SHALL BE INSTALLED. COSTS FOR THE NEW TRANSFORMERS SHALL BE CONSIDERED INCIDENTAL TO THE NEW CABLE.
- SEE SHEETS ELD1 THROUGH ELD6 FOR ELECTRICAL DETAILS.

**RELOCATED SIGN TYPE LEGEND, TAXIWAYS F, K AND M:**

- 0 - BLANK PANEL, BLACK
- 1 - RUNWAY/TAXIWAY HOLDLINE, WHITE LEGEND ON RED BACKGROUND
- 2 - LOCATION SIGN, YELLOW LEGEND ON BLACK BACKGROUND
- 3 - DIRECTION SIGN, BLACK LEGEND ON BLACK BACKGROUND
- 4 - RUNWAY APPROACH AREA BOUNDARY SIGN, BLACK LEGEND ON YELLOW BACKGROUND

**UNDERDRAIN STRUCTURE SCHEDULE**

STRUCTURE	STATION	OFFSET	RIM ELEV.	INVERT ELEV.	NOTES
CO-1	TXY M STA. 11+12	124' RT.	721.32	718.32	TYPE 1 CLEANOUT
CO-2	TXY F STA. 145+36	42.5' RT.	722.86	718.56	TYPE 2 CLEANOUT
CO-3	TXY F STA. 148+50	42.5' RT.	724.13	719.60	TYPE 2 CLEANOUT
CO-4	TXY F STA. 152+37	42.5' RT.	725.76	720.88	TYPE 2 CLEANOUT (3 - WAY)
CO-5	TXY K STA. 12+56	70' LT.	725.13	722.13	TYPE 1 CLEANOUT
CO-6	TXY K STA. 12+46	70' LT.	725.08	722.08	TYPE 1 CLEANOUT
CO-7	TXY K STA. 11+12	124' LT.	726.37	722.23	TYPE 1 CLEANOUT
CO-8	TXY K STA. 11+12	124' RT.	726.85	721.93	TYPE 1 CLEANOUT
CO-9	TXY K STA. 12+46	70' RT.	725.15	722.15	TYPE 1 CLEANOUT
CO-10	TXY K STA. 12+56	70' RT.	725.17	722.17	TYPE 1 CLEANOUT
CO-11	TXY F STA. 160+38	42.5' RT.	726.49	723.49	TYPE 1 CLEANOUT
CS-1	TXY F STA. 153+00	39.2 RT.	726.03	723.03	COLLECTION STRUCTURE. CONNECT TO CO-4.
DC-1	TXY M STA. 12+83	77' RT.	720.00	717.00	CONNECT TO 48" RCP. TOP OF PIPE = 710.00±.
DC-2	TXY K STA. 11+56	88' LT.	725.53	721.75	CONNECT TO 18" RCP. TOP OF PIPE = 720.80±.
DC-3	TXY K STA. 11+64	84' RT.	725.71	721.19	CONNECT TO 18" RCP. TOP OF PIPE = 720.80±.
DC-4	TXY F STA. 159+00	58' RT.	726.38	721.54	CONNECT TO 21" RCP. TOP OF PIPE = 721.30±.

**UNDERDRAIN SCHEDULE**

LINE	LENGTH (FEET)	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	UPSTREAM INVERT	DOWNSTREAM INVERT	SLOPE (%)
1	187	CO-1	DC-1	718.32	717.00	0.71%
2	344	CO-2	DC-1	718.56	717.42	0.33%
3	314	CO-3	CO-2	719.60	718.56	0.33%
4	387	CO-4	CO-3	720.88	719.60	0.33%
5	372	CO-5	CO-4	722.13	720.88	0.34%
6	94	CO-6	DC-2	722.08	721.75	0.35%
7	57	CO-7	DC-2	722.23	721.75	0.84%
8	67	CO-8	DC-3	721.93	721.19	1.10%
9	63	CO-9	DC-3	722.15	721.19	1.52%
10	233	CO-10	DC-4	722.17	721.54	0.27%
11	139	CO-11	DC-4	723.49	721.54	1.40%
12	61	CS-1	CO-4	723.03	722.00	1.69%

**UNDERDRAIN INSTALLATION NOTES, TAXIWAYS F, K AND M:**

- AT LOCATIONS WHERE EXISTING UNDERDRAIN CONNECTS TO EXISTING STORM DRAINS AND IS BEING REMOVED, THE RISER PIPE SHALL BE CUT BELOW THE TOP OF THE PROPOSED SUBGRADE AND CAPPED.
- WHERE NEW UNDERDRAIN CROSSES EXISTING UTILITIES, THE EXISTING UTILITY SHALL BE EXPOSED BY HAND DIGGING TO ENSURE NO CONFLICT EXISTS BETWEEN THE NEW UNDERDRAIN AND THE EXISTING FACILITY. THE RESIDENT ENGINEER SHALL BE NOTIFIED OF ANY CONFLICT PRIOR TO UNDERDRAIN INSTALLATION. THE COSTS TO LOCATE THE EXISTING UTILITIES SHALL BE INCIDENTAL TO THE UNDERDRAIN.
- UNDERDRAIN PIPE SHALL BE PERFORATED RIGID PVC PIPE, 6" DIAMETER. WHERE THE PIPE FOLLOWS A CURVE, BENDS OR TEES MAY BE REQUIRED. THE COSTS FOR ALL FITTINGS SHALL BE CONSIDERED INCIDENTAL TO THE UNDERDRAIN.
- CARE SHALL BE TAKEN WHEN REPLACING THE EXISTING COLLECTION STRUCTURE WITH COLLECTION STRUCTURE CS-1. DAMAGE TO THE ADJACENT FINGER JOINT SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

8

7

6

5

4

3

2

1

8

7

6

5

4

3

2

1

MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

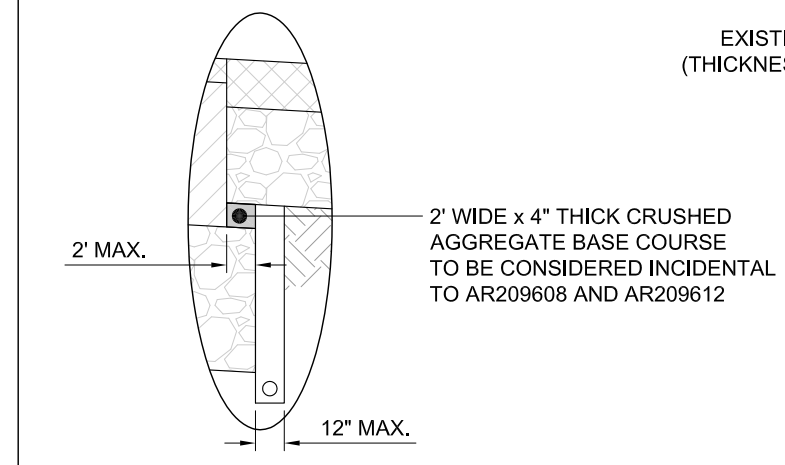
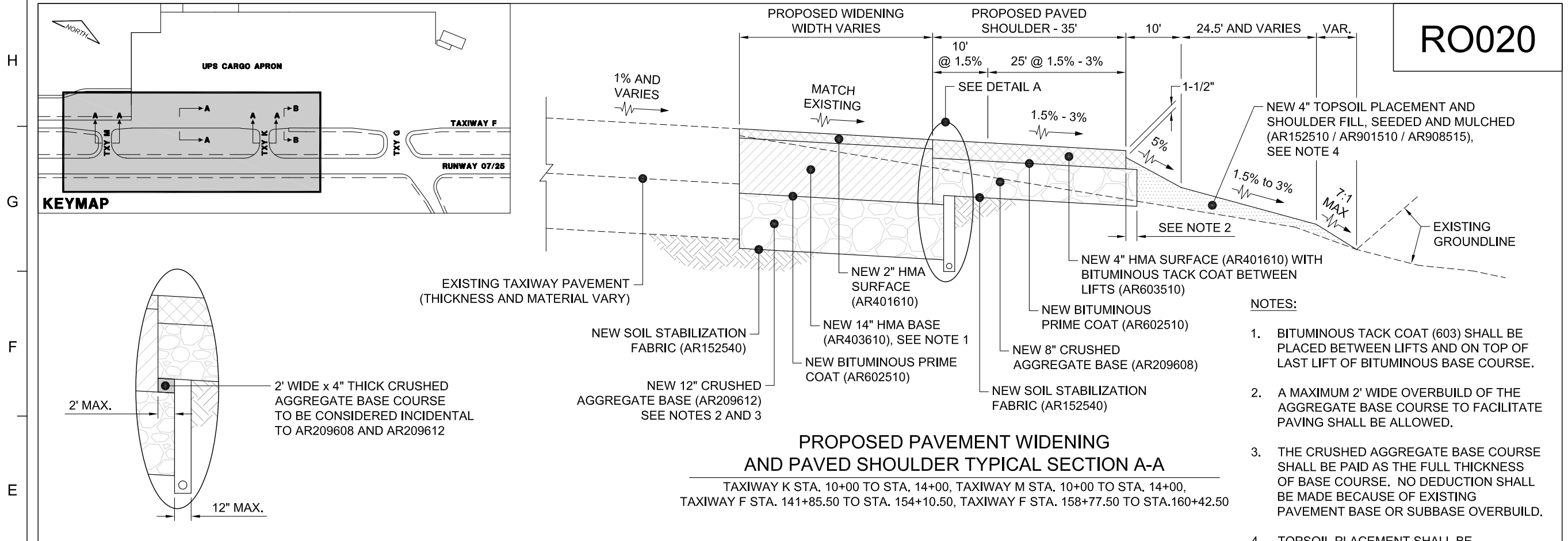
SHEET TITLE:  
**TYPICAL SECTIONS  
TXYS F, K & M**

DESIGNED: SMS    DRAWN: CMT    CHECKED: DLP

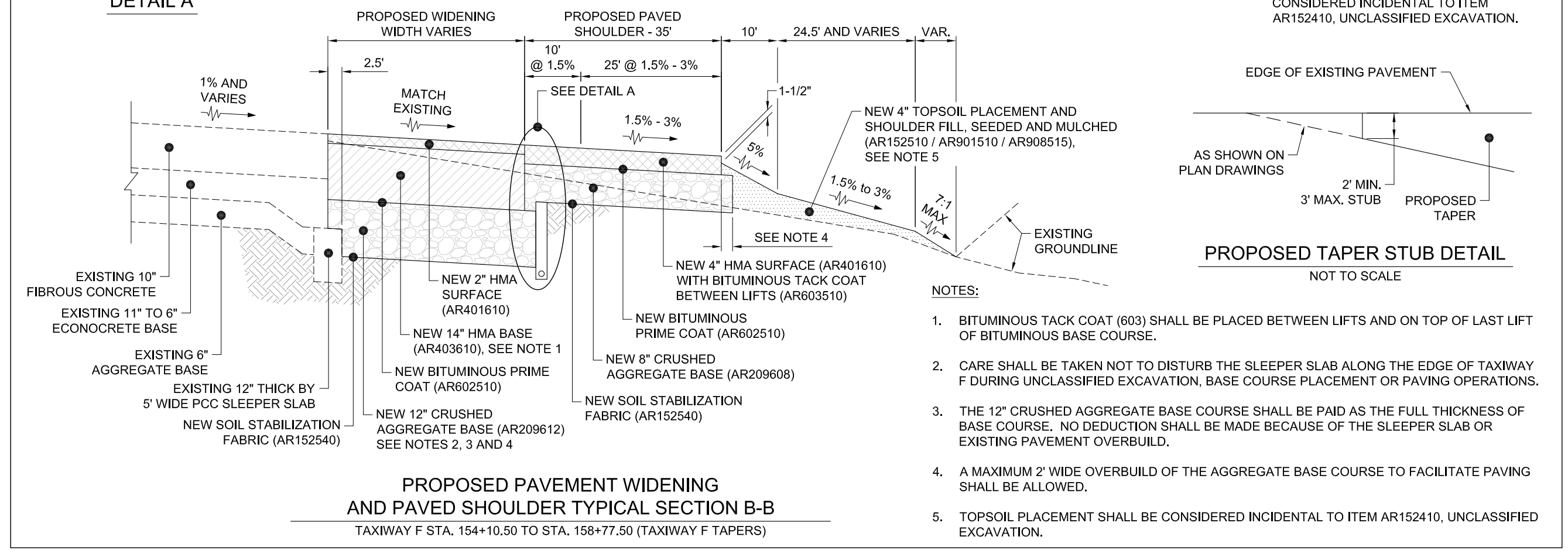
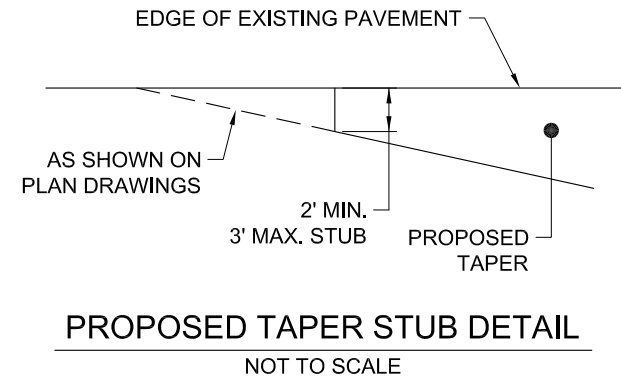
PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **TYP4**    REVISION

# RO020

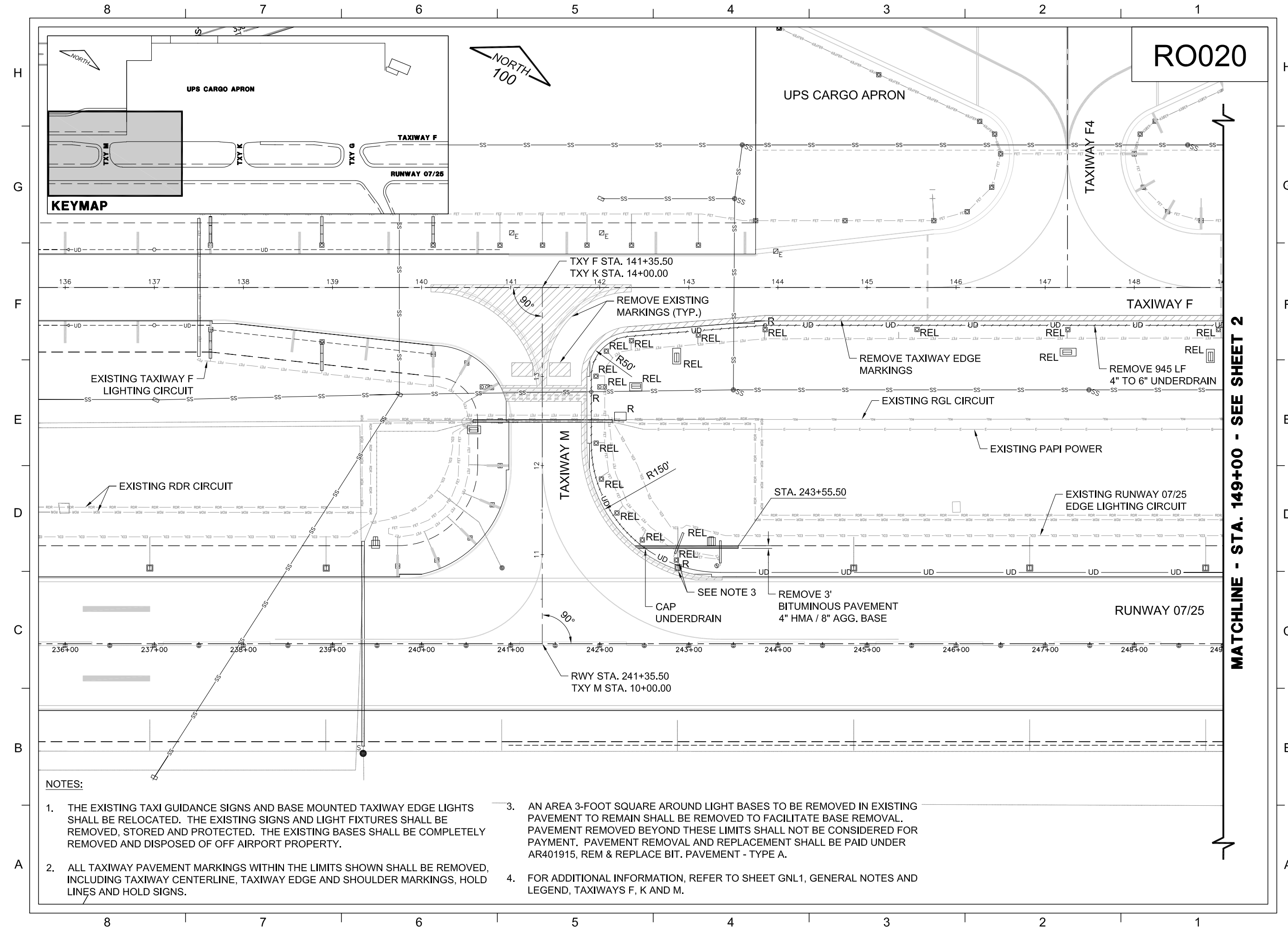


- NOTES:
1. BITUMINOUS TACK COAT (603) SHALL BE PLACED BETWEEN LIFTS AND ON TOP OF LAST LIFT OF BITUMINOUS BASE COURSE.
  2. A MAXIMUM 2' WIDE OVERBUILD OF THE AGGREGATE BASE COURSE TO FACILITATE PAVING SHALL BE ALLOWED.
  3. THE CRUSHED AGGREGATE BASE COURSE SHALL BE PAID AS THE FULL THICKNESS OF BASE COURSE. NO DEDUCTION SHALL BE MADE BECAUSE OF EXISTING PAVEMENT BASE OR SUBBASE OVERBUILD.
  4. TOPSOIL PLACEMENT SHALL BE CONSIDERED INCIDENTAL TO ITEM AR152410, UNCLASSIFIED EXCAVATION.



- NOTES:
1. BITUMINOUS TACK COAT (603) SHALL BE PLACED BETWEEN LIFTS AND ON TOP OF LAST LIFT OF BITUMINOUS BASE COURSE.
  2. CARE SHALL BE TAKEN NOT TO DISTURB THE SLEEPER SLAB ALONG THE EDGE OF TAXIWAY F DURING UNCLASSIFIED EXCAVATION, BASE COURSE PLACEMENT OR PAVING OPERATIONS.
  3. THE 12" CRUSHED AGGREGATE BASE COURSE SHALL BE PAID AS THE FULL THICKNESS OF BASE COURSE. NO DEDUCTION SHALL BE MADE BECAUSE OF THE SLEEPER SLAB OR EXISTING PAVEMENT OVERBUILD.
  4. A MAXIMUM 2' WIDE OVERBUILD OF THE AGGREGATE BASE COURSE TO FACILITATE PAVING SHALL BE ALLOWED.
  5. TOPSOIL PLACEMENT SHALL BE CONSIDERED INCIDENTAL TO ITEM AR152410, UNCLASSIFIED EXCAVATION.





**NOTES:**

1. THE EXISTING TAXI GUIDANCE SIGNS AND BASE MOUNTED TAXIWAY EDGE LIGHTS SHALL BE RELOCATED. THE EXISTING SIGNS AND LIGHT FIXTURES SHALL BE REMOVED, STORED AND PROTECTED. THE EXISTING BASES SHALL BE COMPLETELY REMOVED AND DISPOSED OF OFF AIRPORT PROPERTY.
2. ALL TAXIWAY PAVEMENT MARKINGS WITHIN THE LIMITS SHOWN SHALL BE REMOVED, INCLUDING TAXIWAY CENTERLINE, TAXIWAY EDGE AND SHOULDER MARKINGS, HOLD LINES AND HOLD SIGNS.
3. AN AREA 3-FOOT SQUARE AROUND LIGHT BASES TO BE REMOVED IN EXISTING PAVEMENT TO REMAIN SHALL BE REMOVED TO FACILITATE BASE REMOVAL. PAVEMENT REMOVED BEYOND THESE LIMITS SHALL NOT BE CONSIDERED FOR PAYMENT. PAVEMENT REMOVAL AND REPLACEMENT SHALL BE PAID UNDER AR401915, REM & REPLACE BIT. PAVEMENT - TYPE A.
4. FOR ADDITIONAL INFORMATION, REFER TO SHEET GNL1, GENERAL NOTES AND LEGEND, TAXIWAYS F, K AND M.



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION  
PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

EX. COND. & REMOVALS  
TXYS F, K & M - SHEET 1

DESIGNED: SMS    DRAWN: CMT    CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **EXC3** REVISION

SHEET 31 OF 70 SHEETS



MIKE DUNN  
DIRECTOR



Chicago Rockford International Airport  
Rockford, Illinois

Greater Rockford Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE RUNWAY 1/19 PHASE 2**

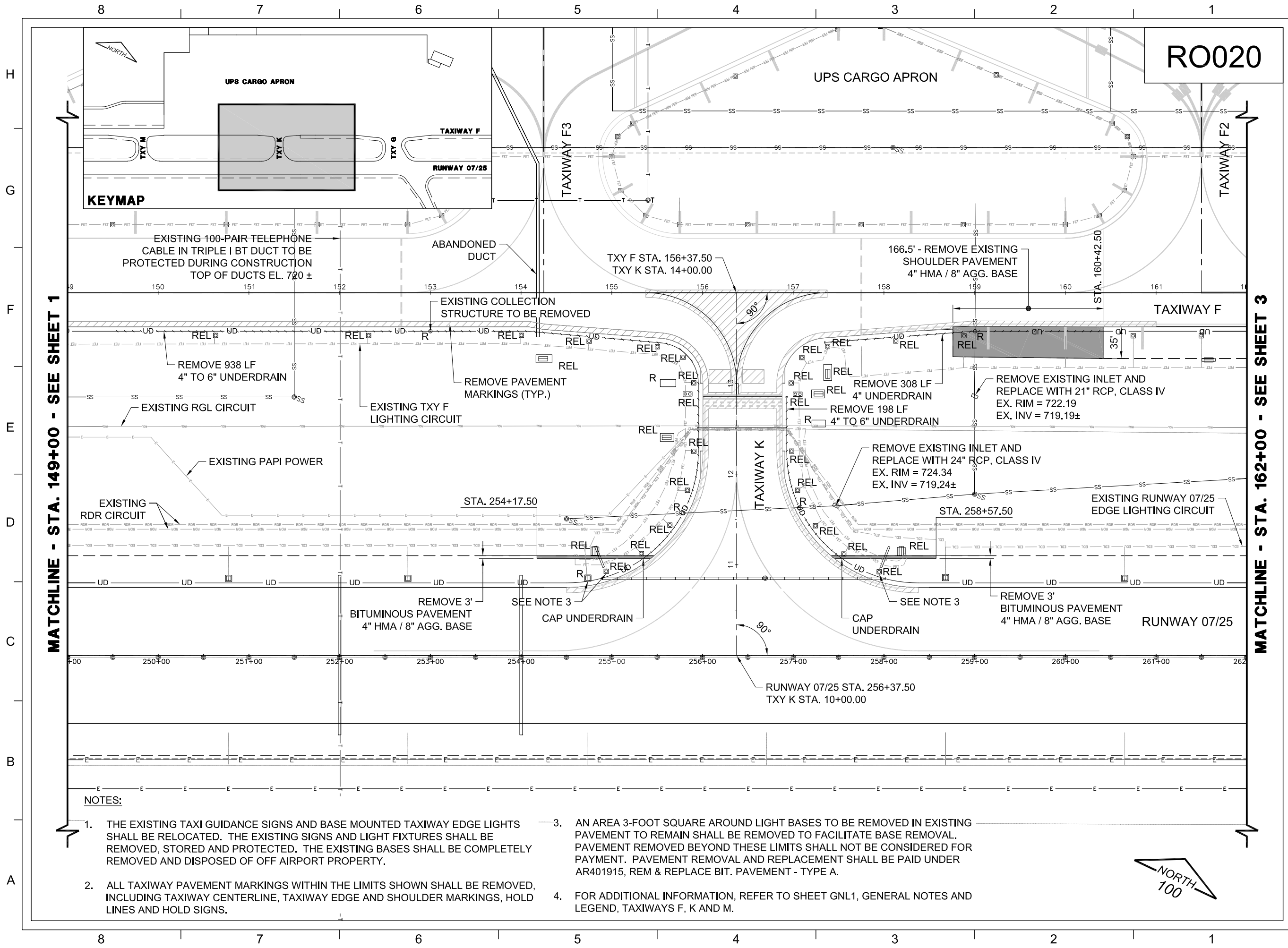
SHEET TITLE:  
**EX. COND. & REMOVALS TXYS F, K & M - SHEET 2**

DESIGNED:	DRAWN:	CHECKED:
SMS	CMT	DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **EXC4** REVISION

SHEET 32 OF 70 SHEETS



**NOTES:**

- THE EXISTING TAXI GUIDANCE SIGNS AND BASE MOUNTED TAXIWAY EDGE LIGHTS SHALL BE RELOCATED. THE EXISTING SIGNS AND LIGHT FIXTURES SHALL BE REMOVED, STORED AND PROTECTED. THE EXISTING BASES SHALL BE COMPLETELY REMOVED AND DISPOSED OF OFF AIRPORT PROPERTY.
- ALL TAXIWAY PAVEMENT MARKINGS WITHIN THE LIMITS SHOWN SHALL BE REMOVED, INCLUDING TAXIWAY CENTERLINE, TAXIWAY EDGE AND SHOULDER MARKINGS, HOLD LINES AND HOLD SIGNS.
- AN AREA 3-FOOT SQUARE AROUND LIGHT BASES TO BE REMOVED IN EXISTING PAVEMENT TO REMAIN SHALL BE REMOVED TO FACILITATE BASE REMOVAL. PAVEMENT REMOVED BEYOND THESE LIMITS SHALL NOT BE CONSIDERED FOR PAYMENT. PAVEMENT REMOVAL AND REPLACEMENT SHALL BE PAID UNDER AR401915, REM & REPLACE BIT. PAVEMENT - TYPE A.
- FOR ADDITIONAL INFORMATION, REFER TO SHEET GNL1, GENERAL NOTES AND LEGEND, TAXIWAYS F, K AND M.



MATCHLINE - STA. 149+00 - SEE SHEET 1

MATCHLINE - STA. 162+00 - SEE SHEET 3

**RO020**





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION  
PROJECT NAME:

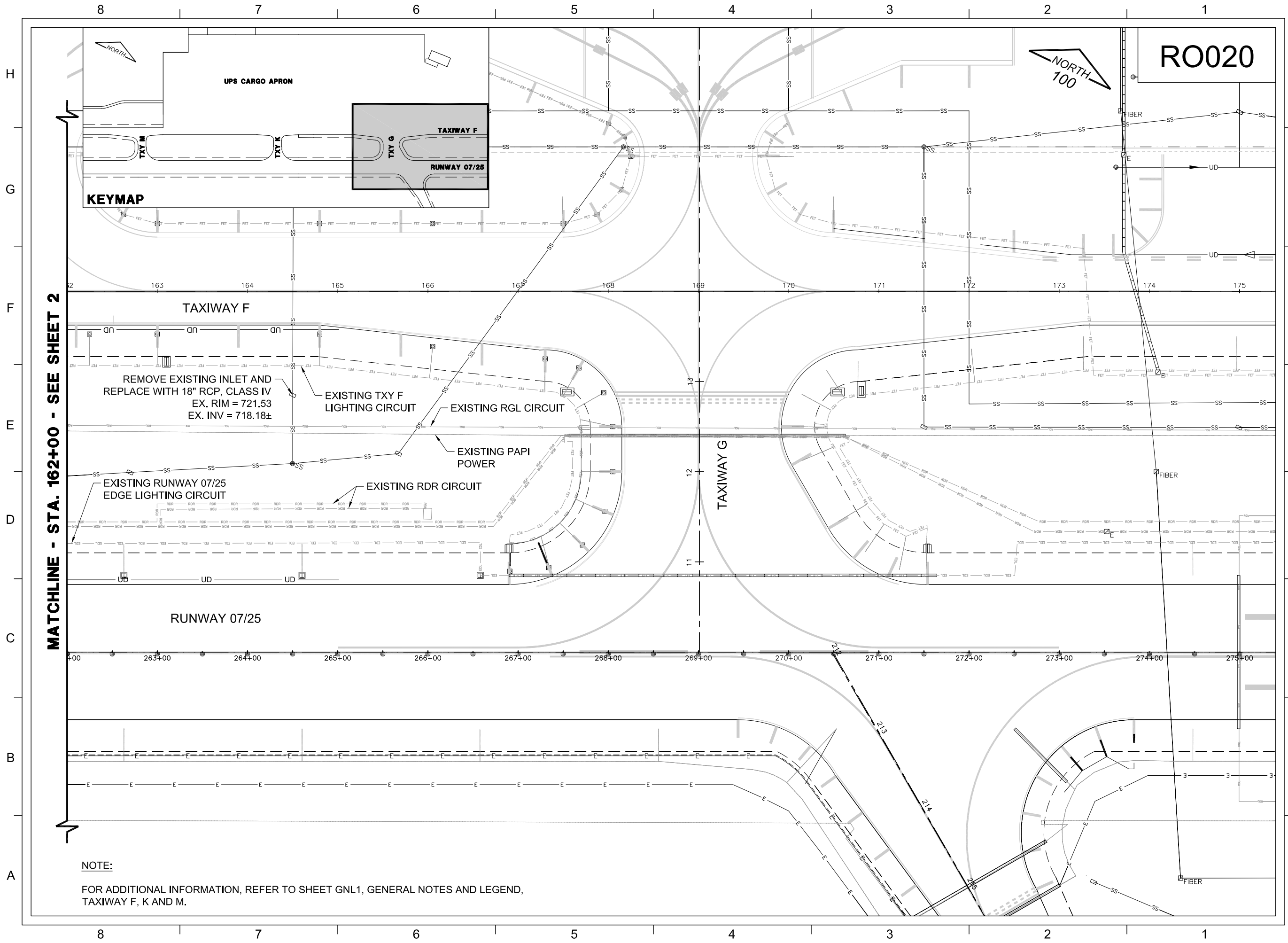
REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
EX. COND. & REMOVALS  
TXYS F, K & M - SHEET 3

DESIGNED: SMS    DRAWN: CMT    CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **EXC5** REVISION  
SHEET 33 OF 70 SHEETS



MATCHLINE - STA. 162+00 - SEE SHEET 2

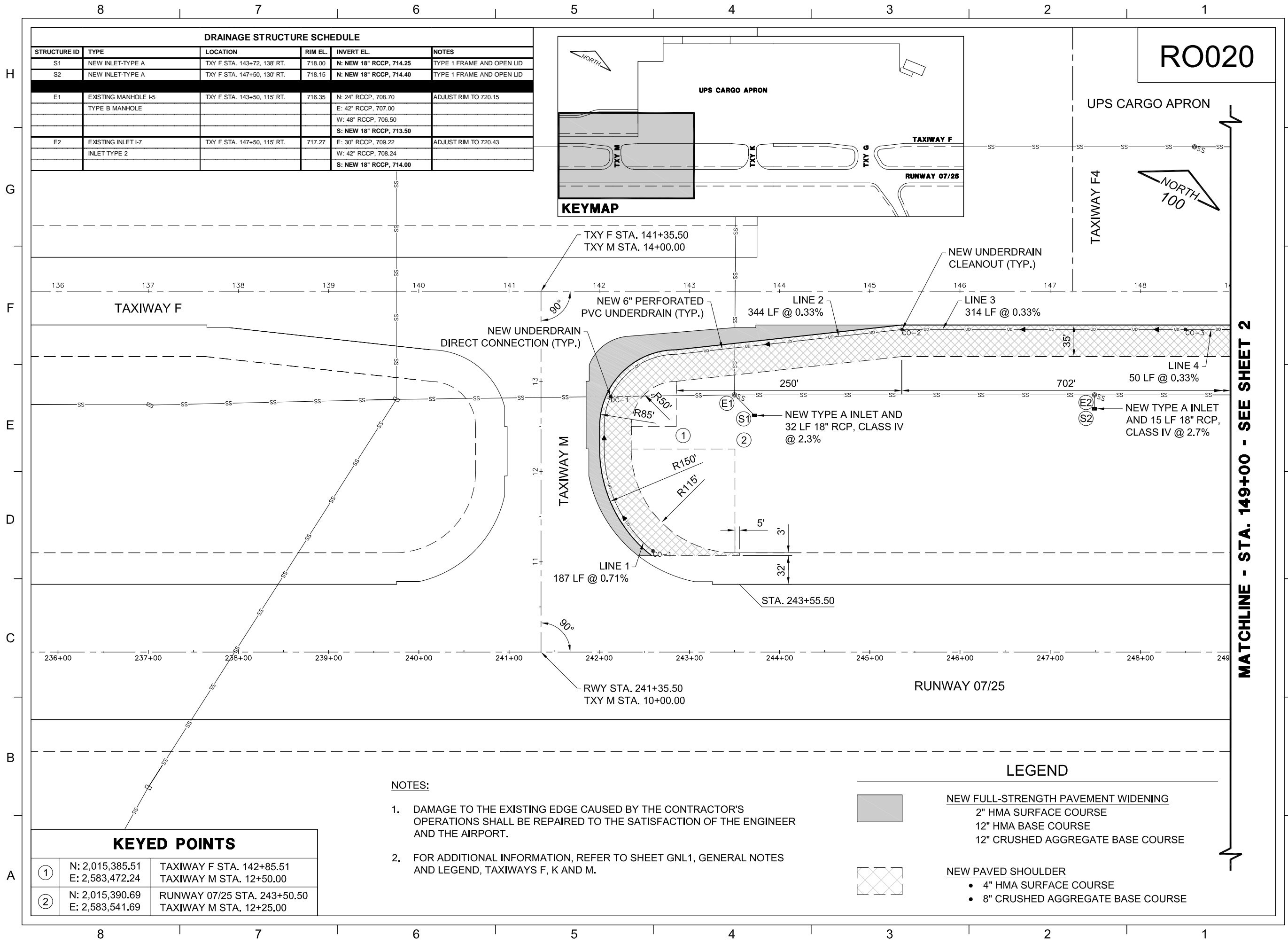
NOTE:

FOR ADDITIONAL INFORMATION, REFER TO SHEET GNL1, GENERAL NOTES AND LEGEND,  
TAXIWAY F, K AND M.

KEYMAP

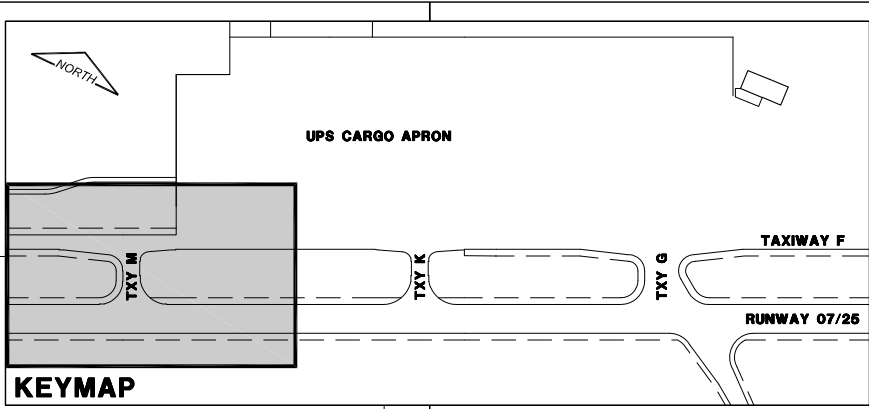
RO020





**DRAINAGE STRUCTURE SCHEDULE**

STRUCTURE ID	TYPE	LOCATION	RIM EL.	INVERT EL.	NOTES
S1	NEW INLET-TYPE A	TXY F STA. 143+72, 138' RT.	718.00	N: NEW 18" RCCP, 714.25	TYPE 1 FRAME AND OPEN LID
S2	NEW INLET-TYPE A	TXY F STA. 147+50, 130' RT.	718.15	N: NEW 18" RCCP, 714.40	TYPE 1 FRAME AND OPEN LID
E1	EXISTING MANHOLE I-5 TYPE B MANHOLE	TXY F STA. 143+50, 115' RT.	716.35	N: 24" RCCP, 708.70 E: 42" RCCP, 707.00 W: 48" RCCP, 706.50 S: NEW 18" RCCP, 713.50	ADJUST RIM TO 720.15
E2	EXISTING INLET I-7 INLET TYPE 2	TXY F STA. 147+50, 115' RT.	717.27	E: 30" RCCP, 709.22 W: 42" RCCP, 708.24 S: NEW 18" RCCP, 714.00	ADJUST RIM TO 720.43



**RO020**



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**PAVING AND DRAINAGE PLAN  
TXYS F, K & M - SHEET 1**

DESIGNED:	DRAWN:	CHECKED:
SMS	CMT	DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.	REVISION
<b>PDP1</b>	

- NOTES:**
- DAMAGE TO THE EXISTING EDGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
  - FOR ADDITIONAL INFORMATION, REFER TO SHEET GNL1, GENERAL NOTES AND LEGEND, TAXIWAYS F, K AND M.

**LEGEND**

	<b>NEW FULL-STRENGTH PAVEMENT WIDENING</b> 2" HMA SURFACE COURSE 12" HMA BASE COURSE 12" CRUSHED AGGREGATE BASE COURSE
	<b>NEW PAVED SHOULDER</b> • 4" HMA SURFACE COURSE • 8" CRUSHED AGGREGATE BASE COURSE

**MATCHLINE - STA. 149+00 - SEE SHEET 2**

**KEYED POINTS**

①	N: 2,015,385.51 E: 2,583,472.24	TAXIWAY F STA. 142+85.51 TAXIWAY M STA. 12+50.00
②	N: 2,015,390.69 E: 2,583,541.69	RUNWAY 07/25 STA. 243+50.50 TAXIWAY M STA. 12+25.00





MIKE DUNN  
DIRECTOR



Chicago Rockford International Airport  
Rockford, Illinois

Greater Rockford Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

REV	DATE	DESCRIPTION

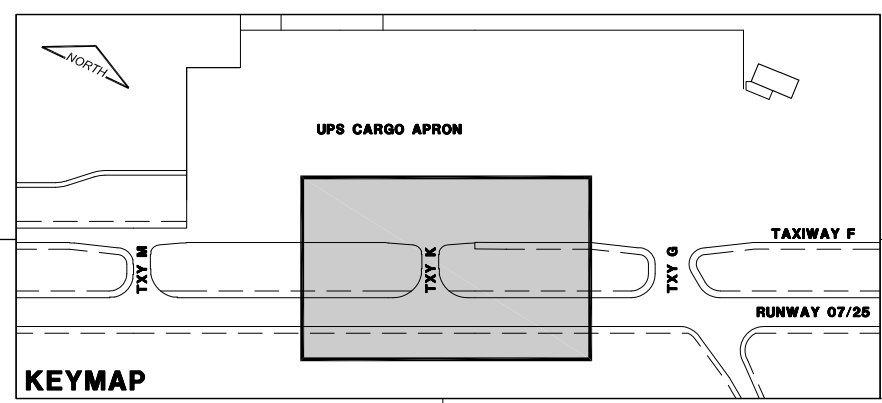
**REHABILITATE RUNWAY 1/19 PHASE 2**

SHEET TITLE:  
**PAVING AND DRAINAGE PLAN TXYS F, K & M - SHEET 2**

DESIGNED: SMS	DRAWN: CMT	CHECKED: DLP
PROJECT NO.: 11258-03-00/06	DATE: APRIL 20, 2012	

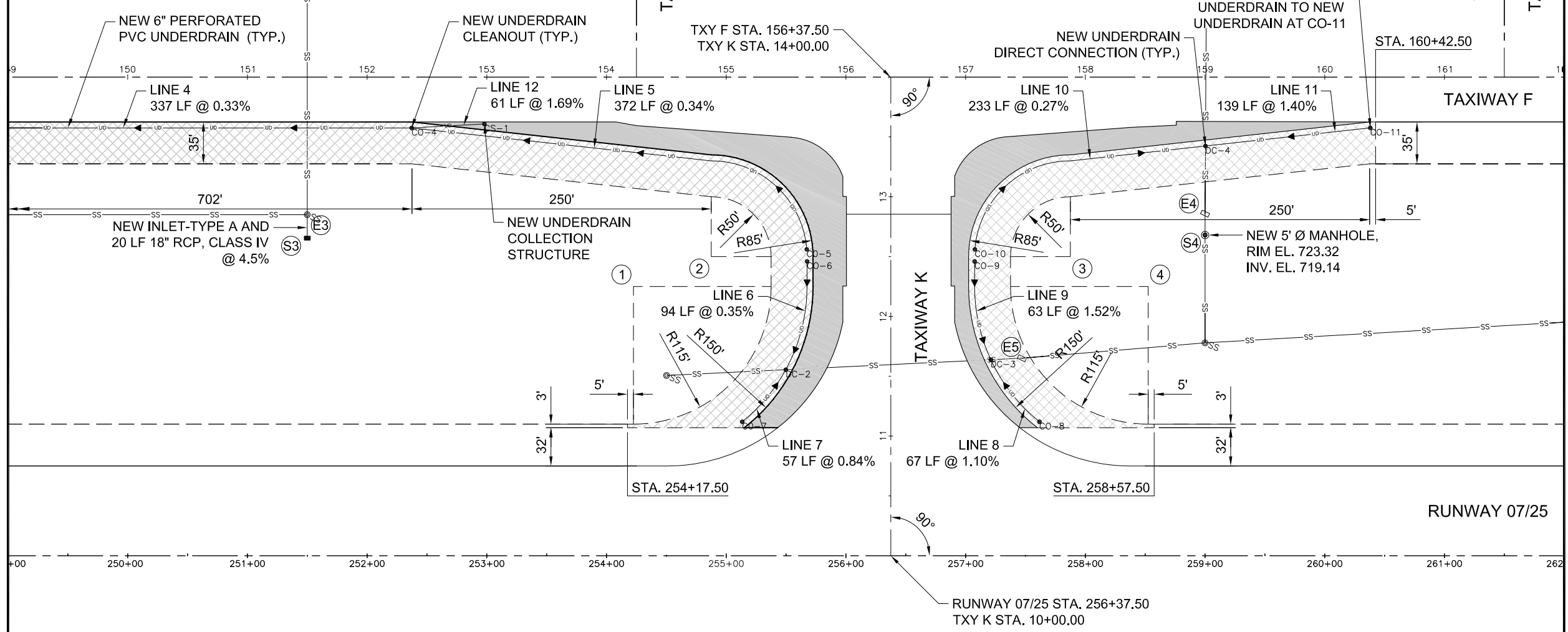
SHEET NO. **PDP2** REVISION  
SHEET 35 OF 70 SHEETS

DRAINAGE STRUCTURE SCHEDULE					
STRUCTURE ID	TYPE	LOCATION	RIM EL.	INVERT EL.	NOTES
S3	NEW INLET-TYPE A	TXY F STA. 151+50, 135' RT.	719.15	N: NEW 18" RCCP, 715.40	TYPE 1 FRAME AND OPEN LID
S4	NEW 5' DIA. MANHOLE	TXY F STA. 159+00, 132' RT.	723.30	N: 21" RCCP, 719.14± S: 21" RCCP, 719.14±	REMOVE 5 LF EXISTING 21" RCP. CONNECT EXISTING PIPE TO NEW MANHOLE.
E3	EXISTING INLET I-8	TXY F STA. 151+50, 115' RT.	718.18	N: 30" RCCP, 714.49 W: 30" RCCP, 714.49	ADJUST RIM TO 722.18
E4	EXISTING INLET I-14	TXY F STA. 159+00, 115' RT.	722.19	N: 21" RCCP, 719.24 S: 21" RCCP, 719.19	REMOVE INLET AND REPLACE WITH 21" RCP, CLASS IV.
E5	EXISTING INLET I-C	TXY F STA. 157+47, 235' RT. RWY 7/25 STA. 257+47, 165' LT.	724.34	E: 24" RCCP, 719.24 W: 18" RCCP, 719.34	REMOVE INLET AND REPLACE WITH 24" RCP, CLASS IV AND 18" RCP REDUCER.



MATCHLINE - STA. 149+00 - SEE SHEET 1

MATCHLINE - STA. 162+00 - SEE SHEET 3



KEYED POINTS		
①	N: 2,015,848.79 E: 2,584,510.88	RUNWAY 07/25 STA. 254+22.50 TAXIWAY K STA. 12+25.00
②	N: 2,015,899.17 E: 2,584,558.96	TAXIWAY F STA. 154+87.50 TAXIWAY K STA. 12+50.00
③	N: 2,016,027.38 E: 2,584,830.18	TAXIWAY F STA. 157+87.50 TAXIWAY K STA. 12+50.00
④	N: 2,016,032.55 E: 2,584,899.63	RUNWAY 07/25 STA. 258+52.50 TAXIWAY K STA. 12+25.00

- NOTES:**
- DAMAGE TO THE EXISTING EDGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
  - CARE SHALL BE TAKEN TO NOT DAMAGE THE SLEEPER SLAB ALONG TAXIWAY F FROM STA. 154+10 TO STA. 158+78.
  - FOR ADDITIONAL INFORMATION, REFER TO SHEET GNL1, GENERAL NOTES AND LEGEND, TAXIWAYS F, K AND M.

LEGEND	
	NEW FULL STRENGTH PAVEMENT WIDENING 2" HMA SURFACE COURSE 12" HMA BASE COURSE 12" CRUSHED AGGREGATE BASE COURSE
	NEW PAVED SHOULDER • 4" HMA SURFACE COURSE • 8" CRUSHED AGGREGATE BASE COURSE



MIKE DUNN  
DIRECTOR



Chicago Rockford International Airport  
Rockford, Illinois

Greater Rockford Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION  
PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
PAVING AND DRAINAGE PLAN  
TXYS F, K & M - SHEET 3

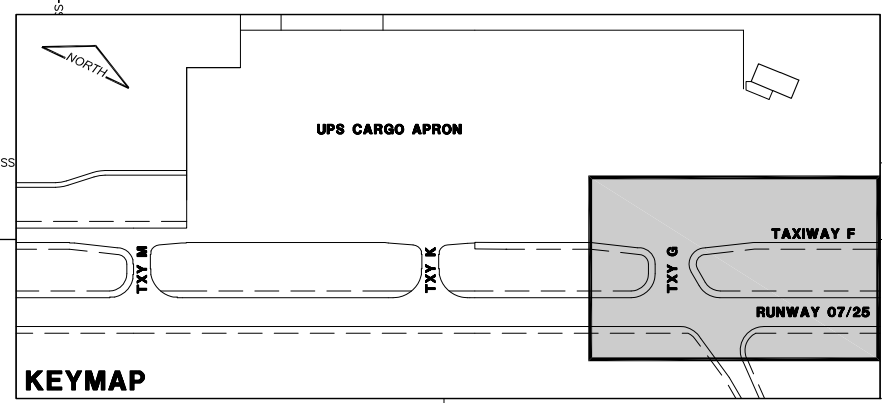
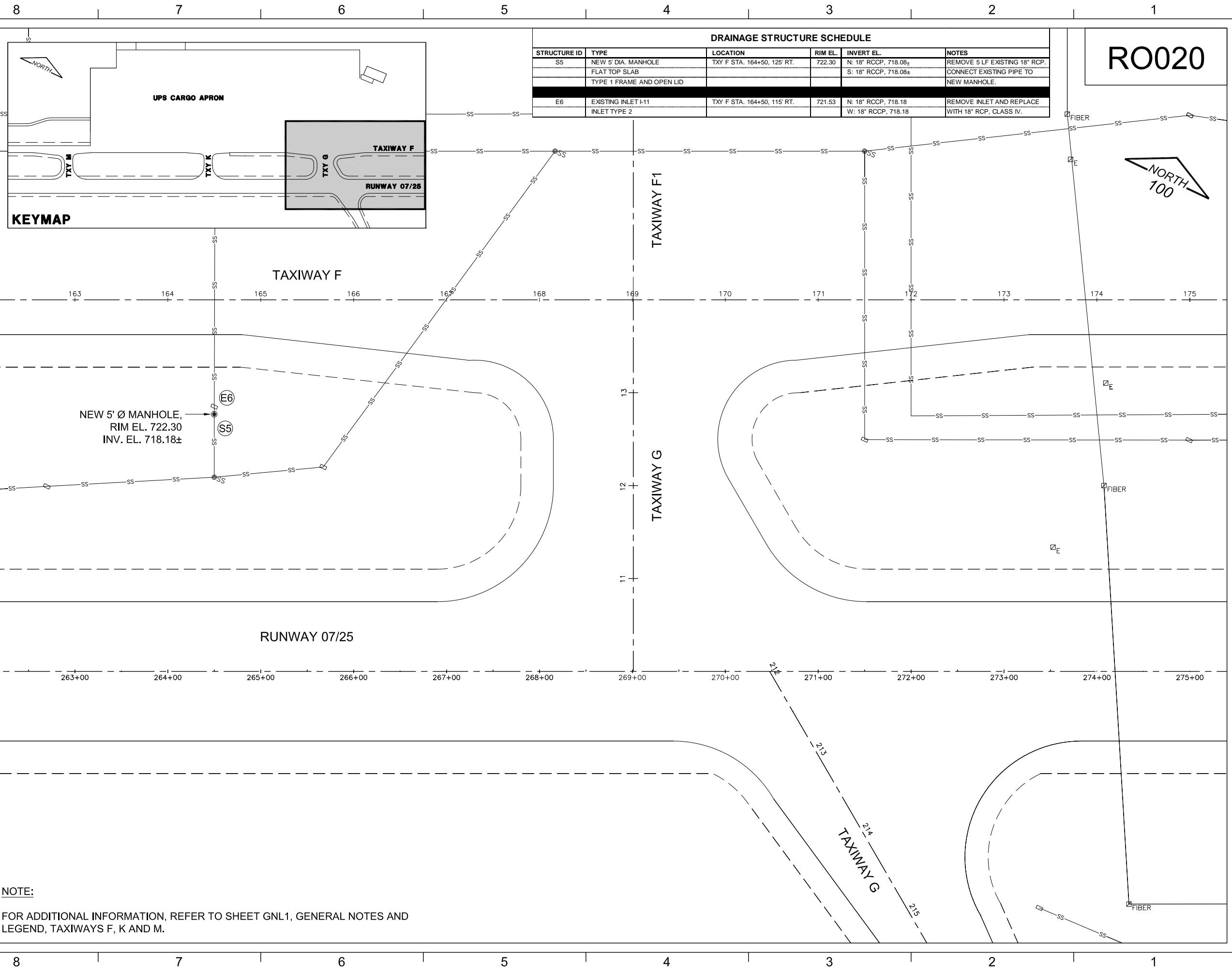
DESIGNED: SMS    DRAWN: CMT    CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **PDP3** REVISION  
SHEET 36 OF 70 SHEETS

DRAINAGE STRUCTURE SCHEDULE					
STRUCTURE ID	TYPE	LOCATION	RIM EL.	INVERT EL.	NOTES
S5	NEW 5' DIA. MANHOLE	TXY F STA. 164+50, 125' RT.	722.30	N: 18" RCCP, 718.08± S: 18" RCCP, 718.08±	REMOVE 5 LF EXISTING 18" RCP.
	FLAT TOP SLAB				CONNECT EXISTING PIPE TO
	TYPE 1 FRAME AND OPEN LID				NEW MANHOLE.
E6	EXISTING INLET I-11	TXY F STA. 164+50, 115' RT.	721.53	N: 18" RCCP, 718.18 W: 18" RCCP, 718.18	REMOVE INLET AND REPLACE
	INLET TYPE 2				WITH 18" RCP, CLASS IV.

RO020

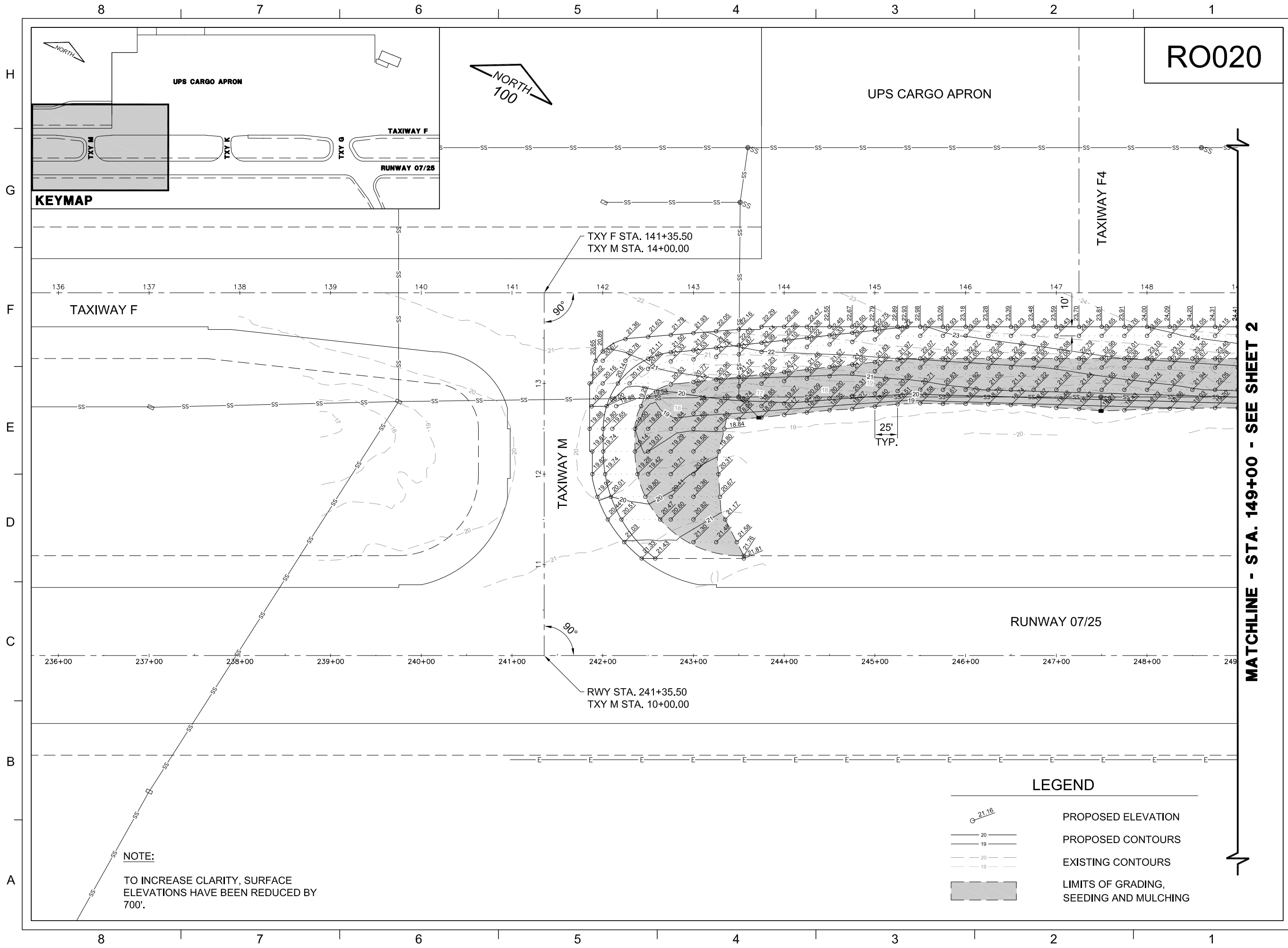


MATCHLINE - STA. 162+00 - SEE SHEET 2

NEW 5' Ø MANHOLE,  
RIM EL. 722.30  
INV. EL. 718.18±

NOTE:  
FOR ADDITIONAL INFORMATION, REFER TO SHEET GNL1, GENERAL NOTES AND LEGEND, TAXIWAYS F, K AND M.





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION  
PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
GRADING PLAN  
TXYS F, K & M - SHEET 1

DESIGNED: SMS	DRAWN: CMT	CHECKED: DLP
------------------	---------------	-----------------

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.	<b>GDP1</b>	REVISION
-----------	-------------	----------

SHEET 37 OF 70 SHEETS



MIKE DUNN  
DIRECTOR



Chicago Rockford International Airport  
Rockford, Illinois

Greater Rockford Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION  
PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

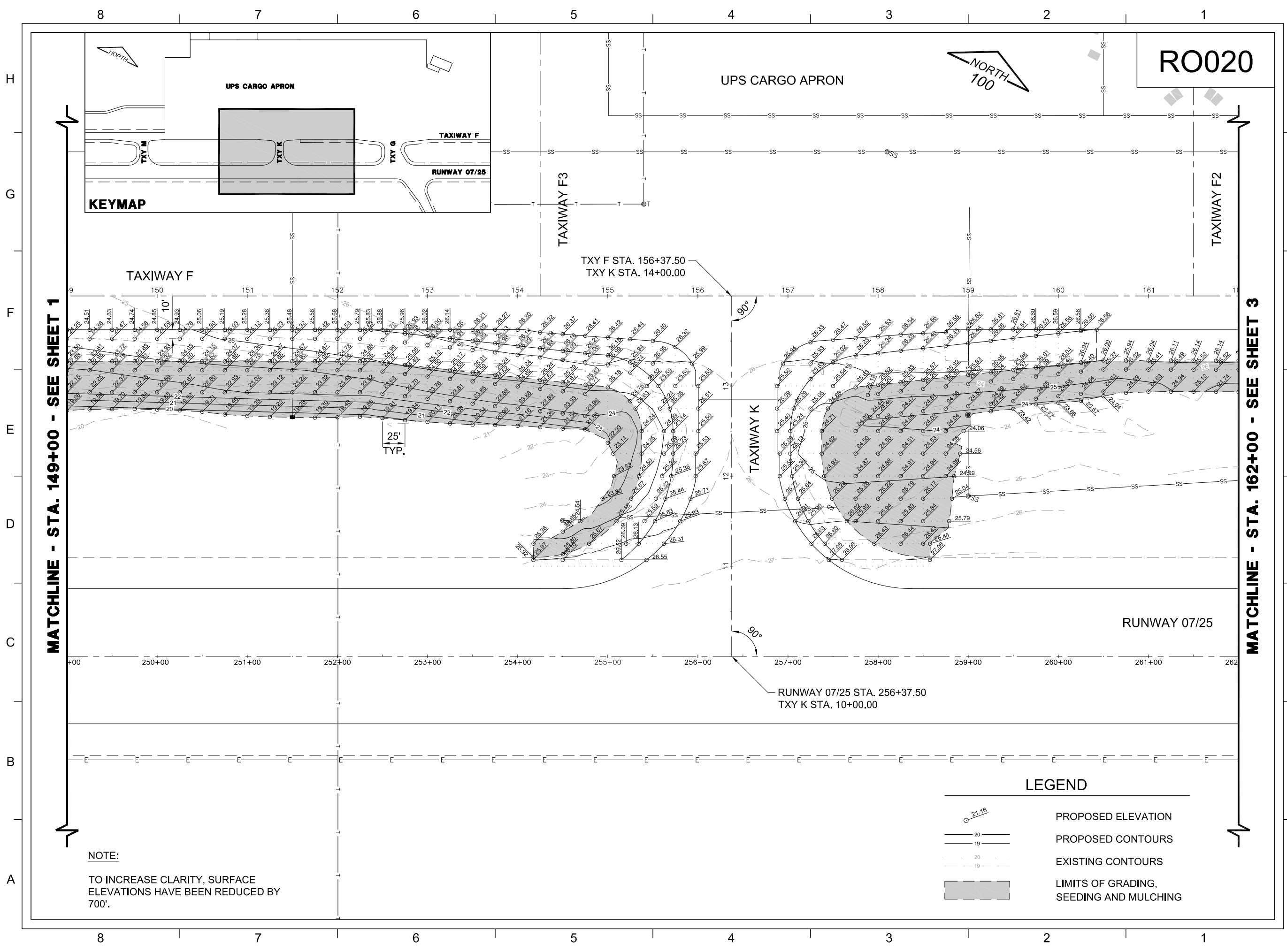
GRADING PLAN  
TXYS F, K & M - SHEET 2

DESIGNED: SMS	DRAWN: CMT	CHECKED: DLP
------------------	---------------	-----------------

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **GDP2** REVISION

SHEET 38 OF 70 SHEETS



KEYMAP

RO020

MATCHLINE - STA. 149+00 - SEE SHEET 1

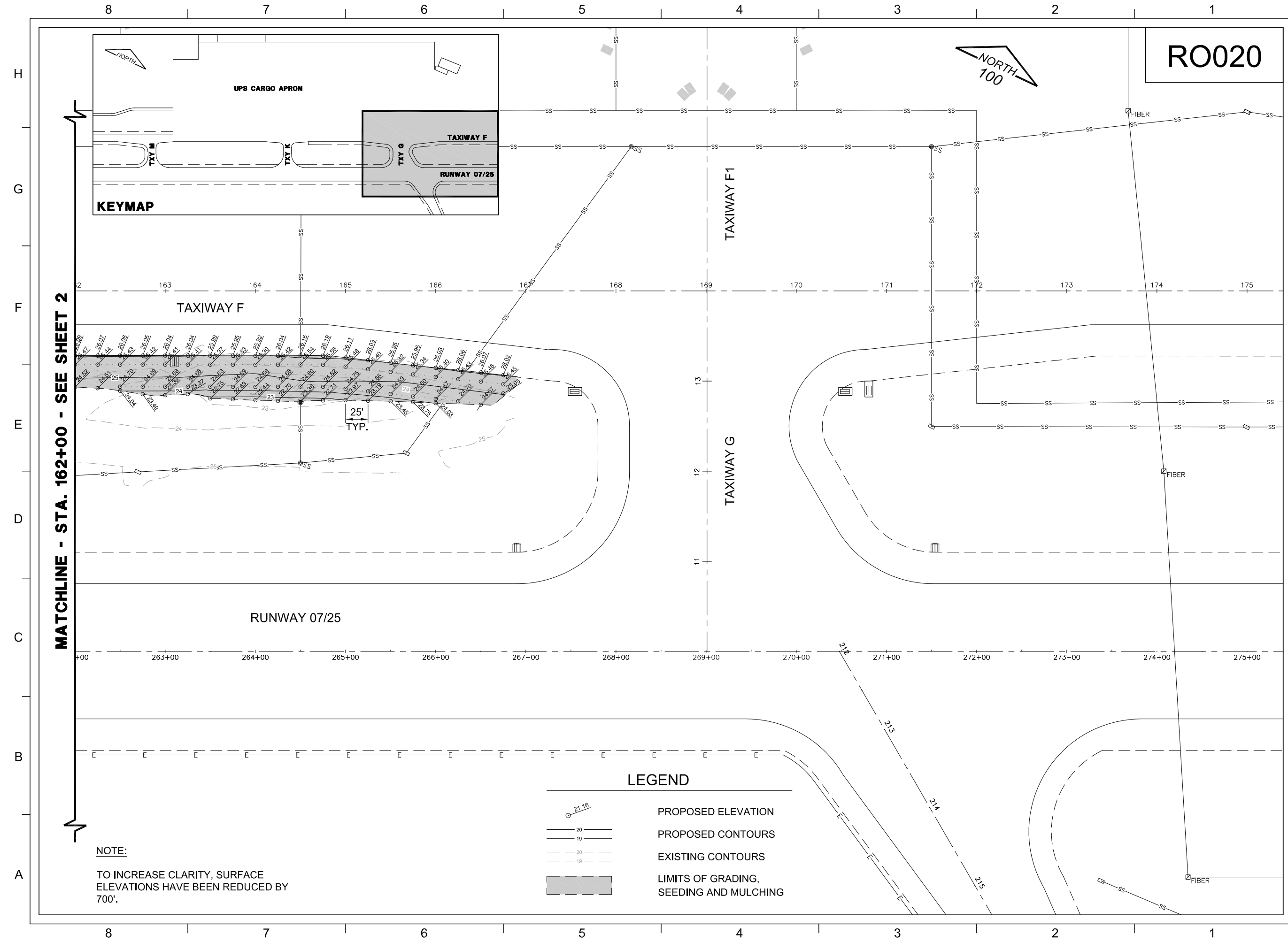
MATCHLINE - STA. 162+00 - SEE SHEET 3

**LEGEND**

- PROPOSED ELEVATION
- PROPOSED CONTOURS
- EXISTING CONTOURS
- LIMITS OF GRADING, SEEDING AND MULCHING

**NOTE:**  
TO INCREASE CLARITY, SURFACE ELEVATIONS HAVE BEEN REDUCED BY 700'.





**MATCHLINE - STA. 162+00 - SEE SHEET 2**

**NOTE:**  
TO INCREASE CLARITY, SURFACE ELEVATIONS HAVE BEEN REDUCED BY 700'.

**LEGEND**

- PROPOSED ELEVATION
- PROPOSED CONTOURS
- EXISTING CONTOURS
- LIMITS OF GRADING, SEEDING AND MULCHING



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**GRADING PLAN  
TXYS F, K & M - SHEET 3**

DESIGNED: SMS	DRAWN: CMT	CHECKED: DLP
------------------	---------------	-----------------

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **GDP3** REVISION

SHEET 39 OF 70 SHEETS















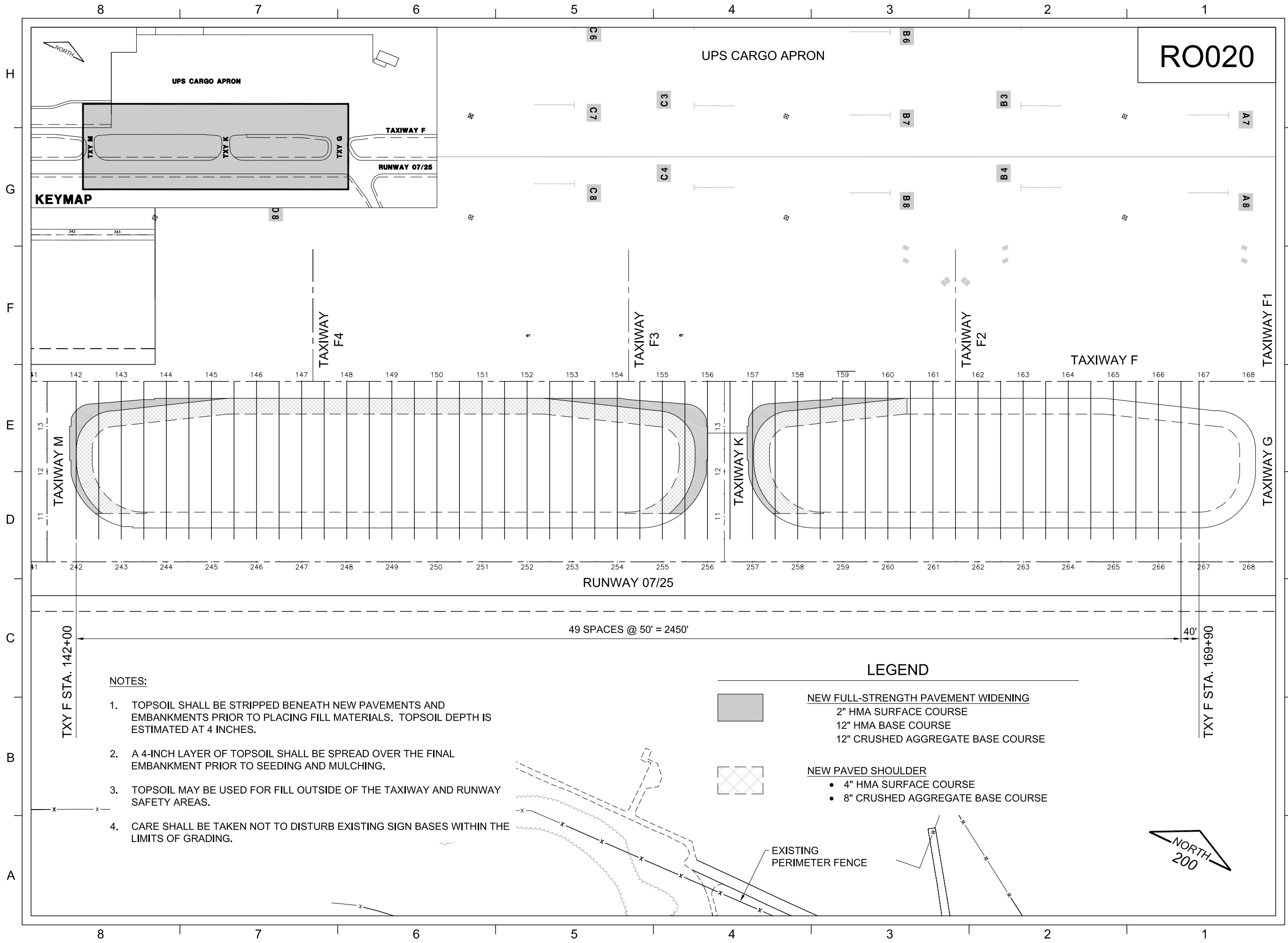
MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183



**KEYMAP**

**RO020**

RUNWAY 07/25

49 SPACES @ 50' = 2450'

TXY F STA. 142+00

TXY F STA. 169+90

**NOTES:**

1. TOPSOIL SHALL BE STRIPPED BENEATH NEW PAVEMENTS AND EMBANKMENTS PRIOR TO PLACING FILL MATERIALS. TOPSOIL DEPTH IS ESTIMATED AT 4 INCHES.
2. A 4-INCH LAYER OF TOPSOIL SHALL BE SPREAD OVER THE FINAL EMBANKMENT PRIOR TO SEEDING AND MULCHING.
3. TOPSOIL MAY BE USED FOR FILL OUTSIDE OF THE TAXIWAY AND RUNWAY SAFETY AREAS.
4. CARE SHALL BE TAKEN NOT TO DISTURB EXISTING SIGN BASES WITHIN THE LIMITS OF GRADING.

**LEGEND**



**NEW FULL-STRENGTH PAVEMENT WIDENING**  
2" HMA SURFACE COURSE  
12" HMA BASE COURSE  
12" CRUSHED AGGREGATE BASE COURSE



**NEW PAVED SHOULDER**  
• 4" HMA SURFACE COURSE  
• 8" CRUSHED AGGREGATE BASE COURSE

EXISTING PERIMETER FENCE



APPROVED AS WORKING PLAN BY:


REV DATE DESCRIPTION


PROJECT NAME:

**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

**SHEET TITLE:  
INDEX TO CROSS SECTIONS  
TXYS F, K & M**

DESIGNED: SMS	DRAWN: CMT	CHECKED: DLP
---------------	------------	--------------

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.	<b>INX1</b>	REVISION
SHEET 44 OF 70 SHEETS		



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

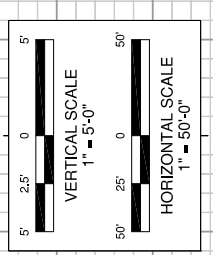
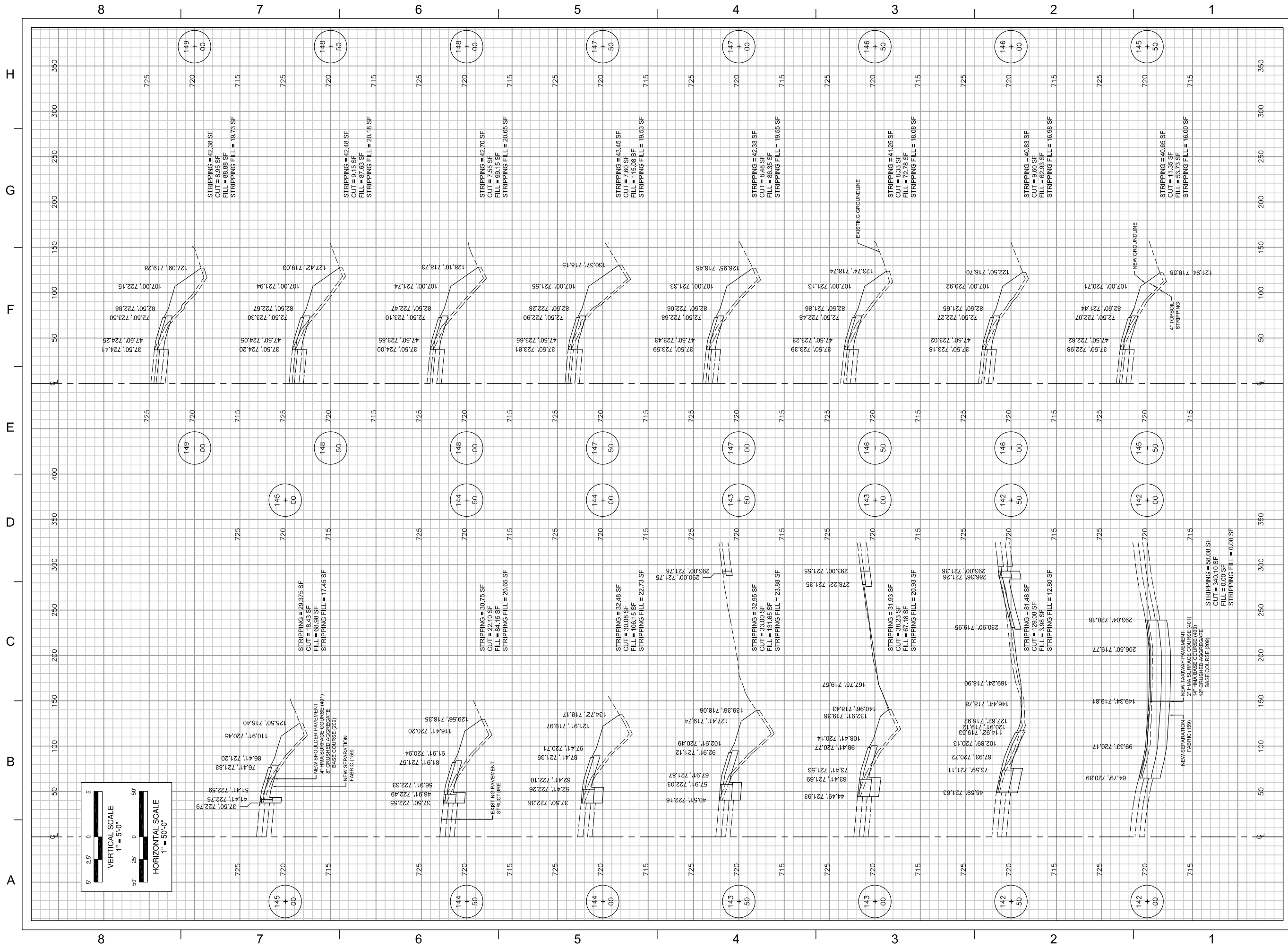
APPROVED AS WORKING PLAN		
REV	DATE	DESCRIPTION

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
CROSS SECTIONS  
TXYS F, K & M - SHEET 1

DESIGNED: SMS	DRAWN: CMT	CHECKED: DLP
PROJECT NO.: 11258-03-00/06	DATE: APRIL 20, 2012	

SHEET NO. <b>XSC1</b>	REVISION
SHEET 45 OF 70 SHEETS	





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
CROSS SECTIONS SHEET 2  
TXYS F, K & M

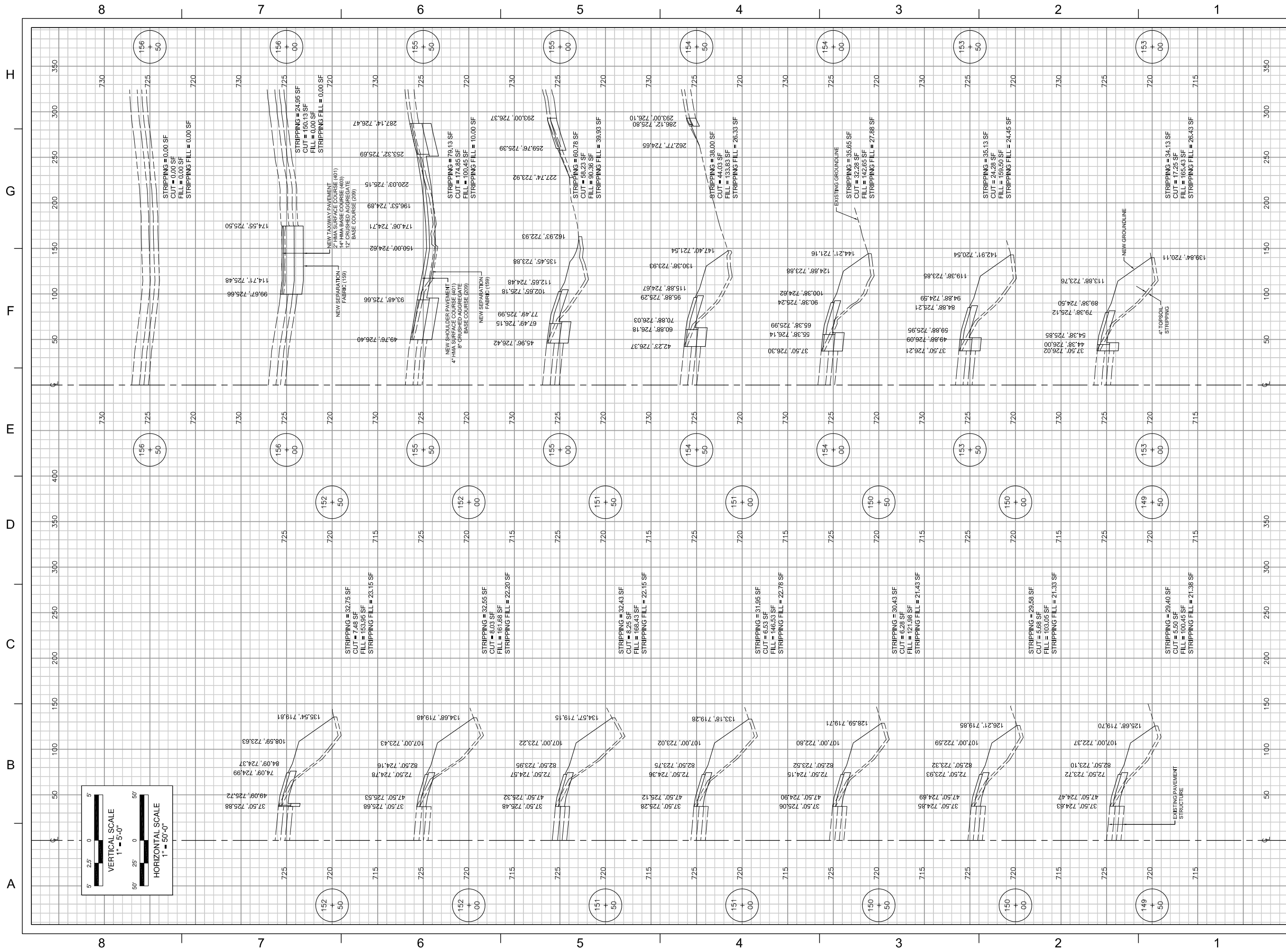
DESIGNED: SMS      DRAWN: CMT      CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.      REVISION

**XSC2**

SHEET 46 OF 70 SHEETS







MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV	DATE	DESCRIPTION

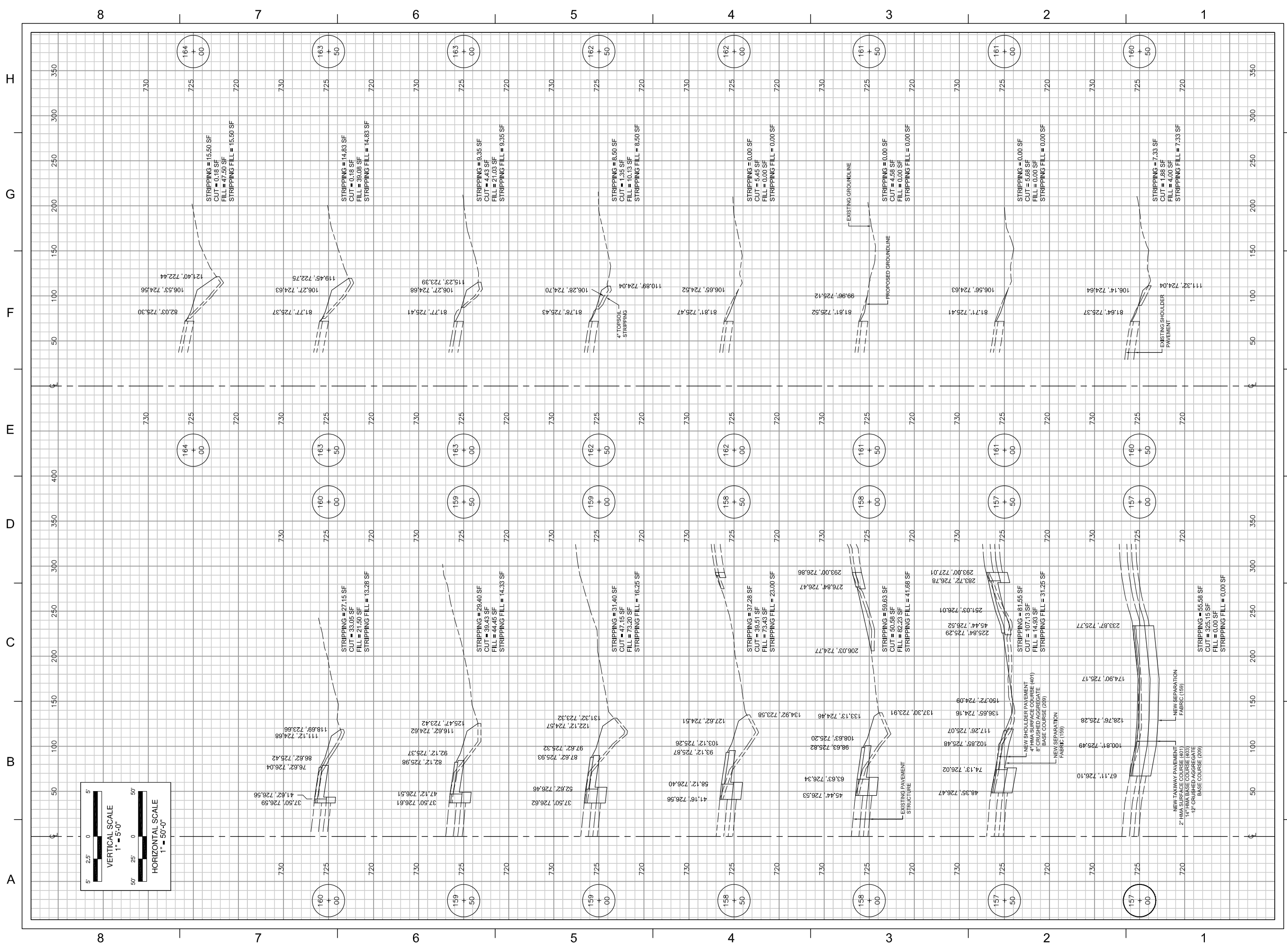
PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**CROSS SECTIONS SHEET 3  
TXYS F, K & M**

DESIGNED: SMS      DRAWN: CMT      CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **XSC3**      REVISION  
SHEET 47 OF 70 SHEETS





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV	DATE	DESCRIPTION

PROJECT NAME:

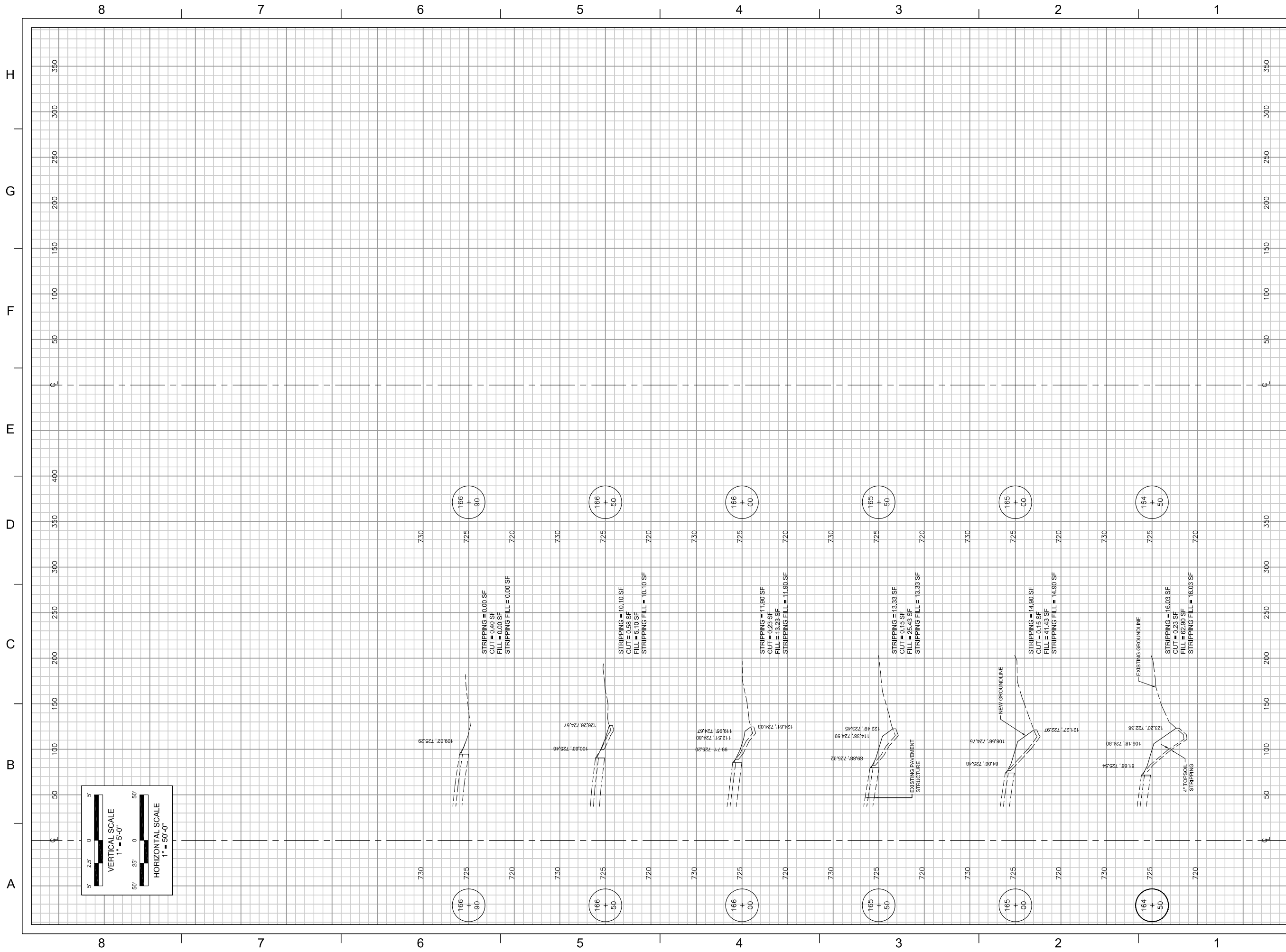
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**CROSS SECTIONS SHEET 4  
TXYS F, K & M**

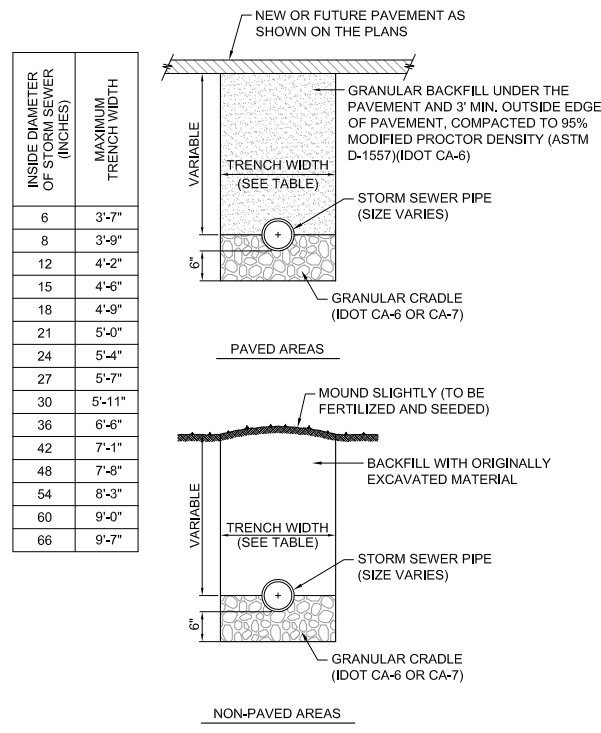
DESIGNED: SMS      DRAWN: CMT      CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

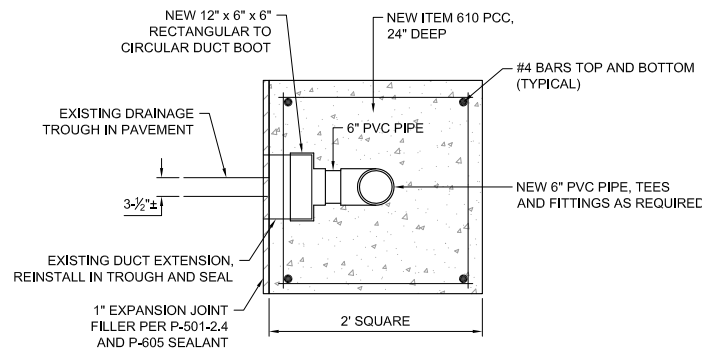
SHEET NO. **XSC4** REVISION  
SHEET 48 OF 70 SHEETS



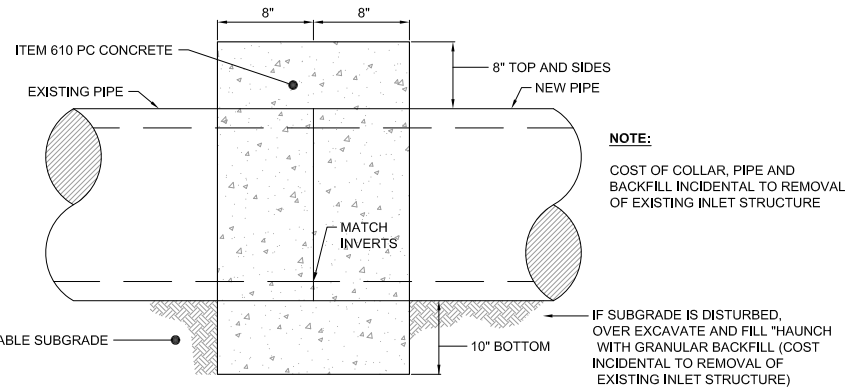
# RO020



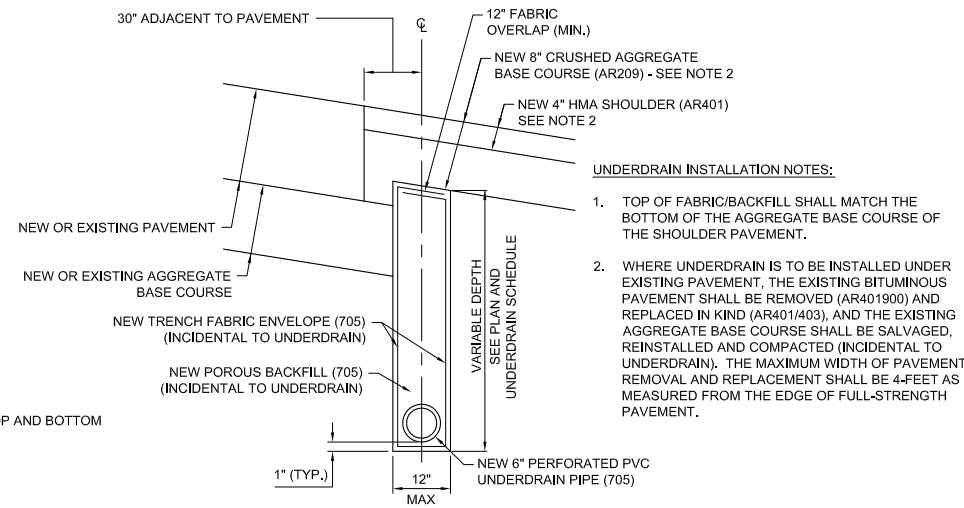
TRENCH DETAILS - STORM SEWER  
NOT TO SCALE



COLLECTION STRUCTURE DETAIL  
NOT TO SCALE



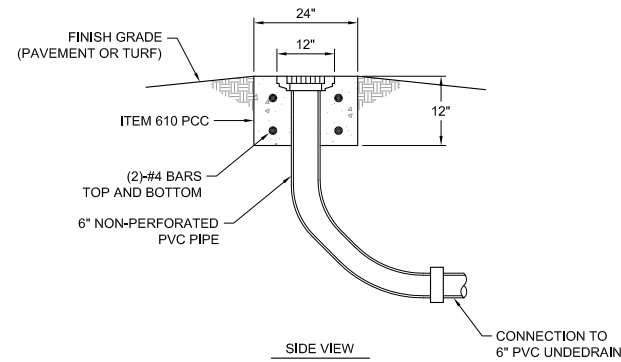
CONCRETE COLLAR - INLET REMOVAL  
NOT TO SCALE



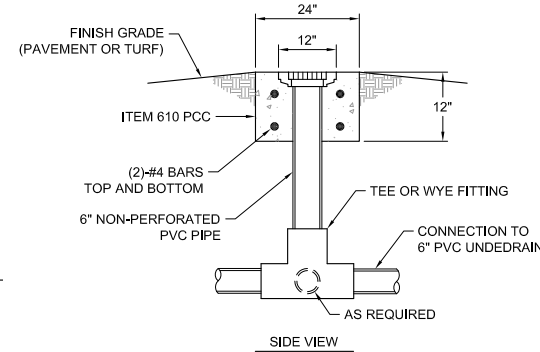
UNDERDRAIN DETAIL  
EDGE OF PAVEMENT AREAS  
NOT TO SCALE

- UNDERDRAIN INSTALLATION NOTES:**
1. TOP OF FABRIC/BACKFILL SHALL MATCH THE BOTTOM OF THE AGGREGATE BASE COURSE OF THE SHOULDER PAVEMENT.
  2. WHERE UNDERDRAIN IS TO BE INSTALLED UNDER EXISTING PAVEMENT, THE EXISTING BITUMINOUS PAVEMENT SHALL BE REMOVED (AR401900) AND REPLACED IN KIND (AR401403), AND THE EXISTING AGGREGATE BASE COURSE SHALL BE SALVAGED, REINSTALLED AND COMPACTED (INCIDENTAL TO UNDERDRAIN). THE MAXIMUM WIDTH OF PAVEMENT REMOVAL AND REPLACEMENT SHALL BE 4- FEET AS MEASURED FROM THE EDGE OF FULL-STRENGTH PAVEMENT.

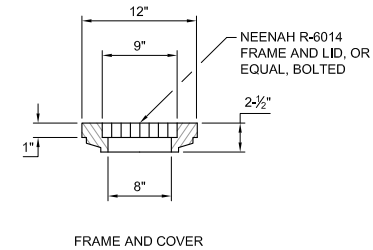
- COLLECTION STRUCTURE INSTALLATION NOTES:**
1. CARE SHALL BE TAKEN DURING THE REMOVAL OF THE EXISTING COLLECTION STRUCTURE TO NOT DAMAGE THE EXISTING DUCT EXTENSION OR TROUGH.
  2. REINSTALL DUCT EXTENSION USING REGLETS TO ENSURE PROPER SEATING. SEAL ALL AIR GAPS WITH SILICONE SEALANT.
  3. THE DUCT BOOT SHALL BE ANY COMMERCIALY AVAILABLE DUCT BOOT OF GOOD QUALITY. THE CONTRACTOR SHALL ENDEAVOR TO FIND A PRODUCT MADE IN THE USA PER THE CONTRACT REQUIREMENTS. IF A DUCT BOOT OF THE PROPER SIZE IS NOT AVAILABLE, THE CONTRACTOR SHALL FASHION ONE OF SHEET METAL.
  4. ALL DUCTING AND PVC FITTINGS SHALL FIT SNUG AND BE HELD IN PLACE TO PREVENT SHIFTING DURING PCC PLACEMENT.
  5. THE CONTRACTOR SHALL DEMONSTRATE THAT THE ASSEMBLED COLLECTION STRUCTURE FUNCTIONS BY POURING WATER INTO THE TROUGH AND COLLECTING THE RUNOFF THROUGH THE OUTLET PIPE. THIS DEMONSTRATION SHALL BE ACCOMPLISHED PRIOR TO AND AFTER PCC PLACEMENT.



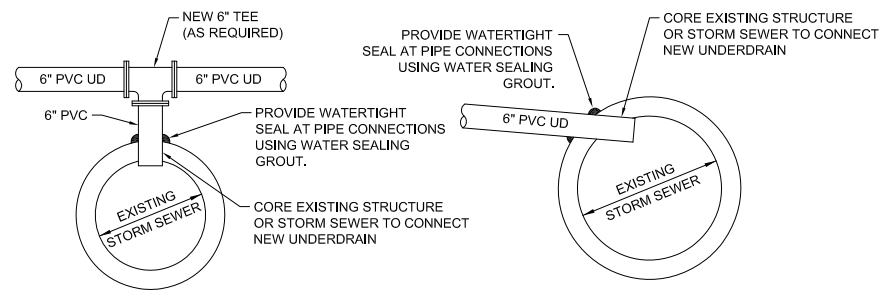
UNDERDRAIN CLEAN-OUT DETAIL - TYPE 1  
NOT TO SCALE



UNDERDRAIN CLEAN-OUT DETAIL - TYPE 2  
NOT TO SCALE



UNDERDRAIN CLEAN-OUT DETAILS  
NOT TO SCALE



UNDERDRAIN CONNECTIONS DETAILS  
NOT TO SCALE

- NOTES:**
1. UNDERDRAIN CONNECTIONS AND FITTINGS, TEES, ELBOWS AND RISERS USED FOR CONNECTIONS TO NEW STRUCTURES / EXISTING STORM SEWERS SHALL BE CONSIDERED INCIDENTAL TO THE NEW UNDERDRAIN.
  2. UNDERDRAIN CONNECTIONS AND FITTINGS, TEES, ELBOWS AND RISERS USED FOR CONNECTIONS TO NEW/EXISTING UNDERDRAIN SHALL BE CONSIDERED INCIDENTAL TO THE NEW UNDERDRAIN.

**DRAINAGE AND UNDERDRAIN  
DETAILS (SSD1)**



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION  
PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

**DRAINAGE AND  
UNDERDRAIN DETAILS**

DESIGNED: SMS DRAWN: CMT CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **SSD1** REVISION

SHEET 49 OF 70 SHEETS



RO020



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

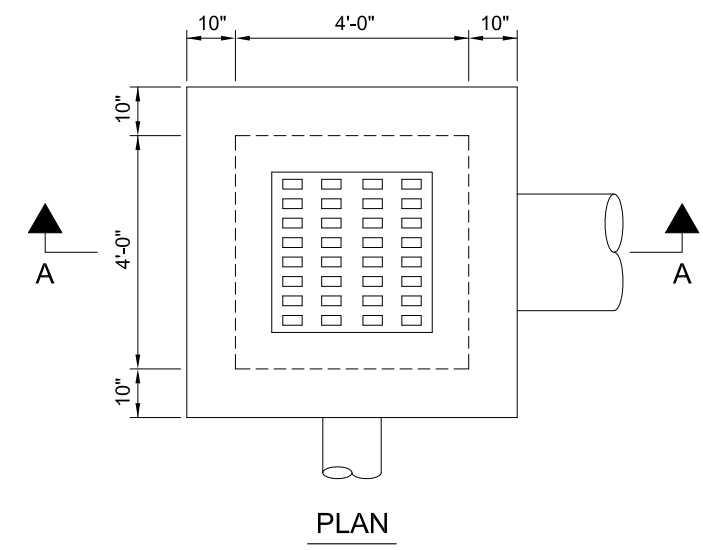
SHEET TITLE:  
**STORM SEWER INLET  
ADJUSTMENT DETAIL**

DESIGNED: SMS     DRAWN: CMT     CHECKED: DLP

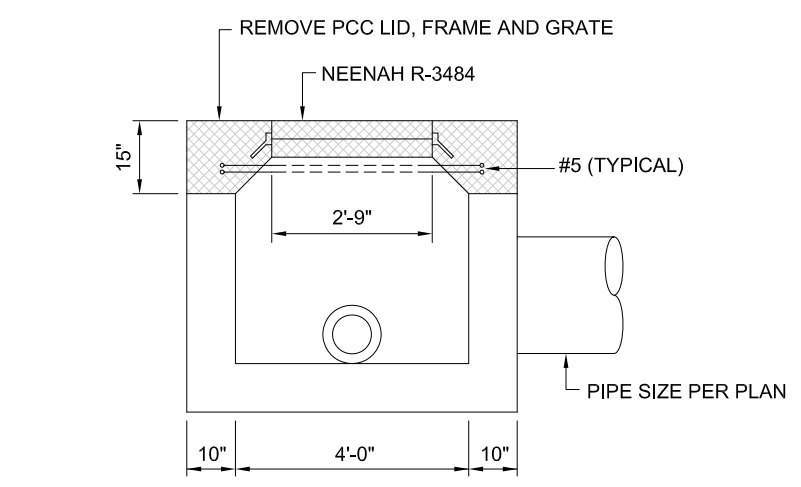
PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **SSD2**     REVISION

SHEET 50 OF 70 SHEETS

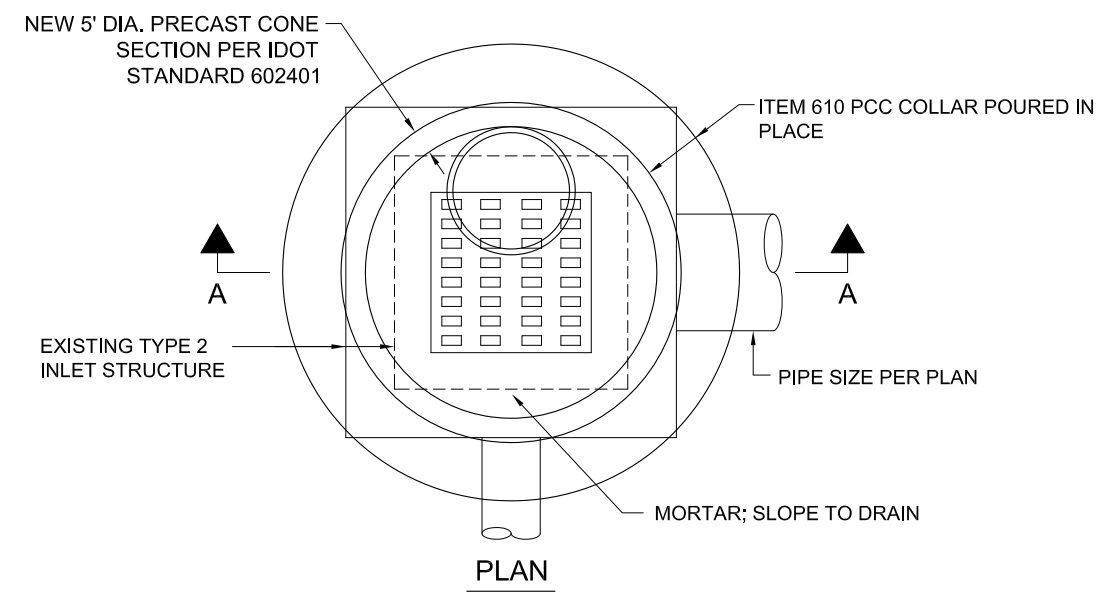


PLAN

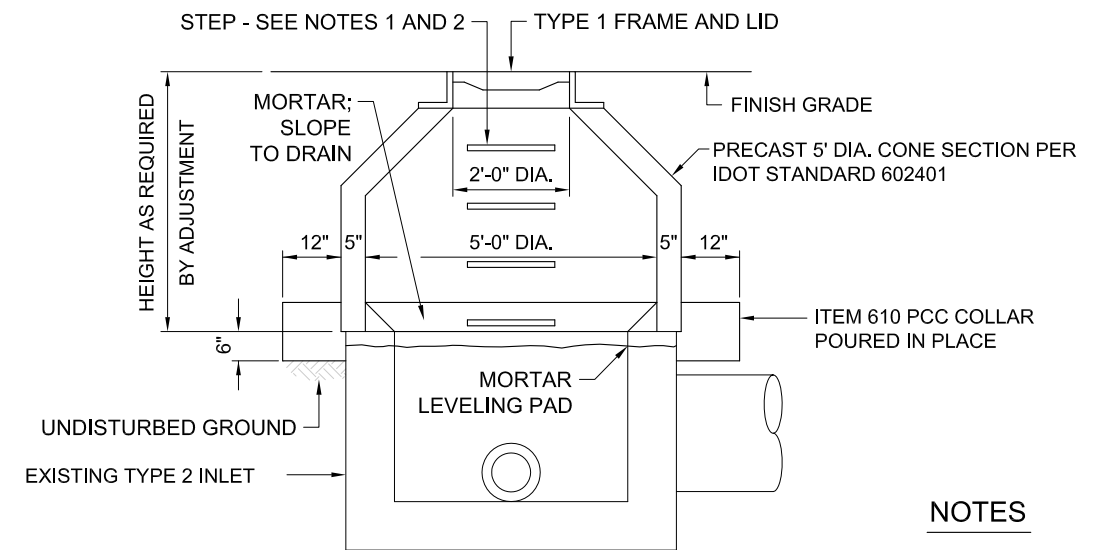


SECTION A-A

**ADJUST TYPE 2 INLET  
STEP 1**  
NOT TO SCALE



PLAN



SECTION A-A

**ADJUST TYPE 2 INLET  
STEP 2**  
NOT TO SCALE

**NOTES**

1. PROVIDE STEPS ON 12" CENTERS IN CONE SECTION.
2. IF REQUIRED PER 12" C-C SPACING OF STEPS, PROVIDE ADDITIONAL STEP(S) IN EXISTING STRUCTURE.

**STORM SEWER INLET  
ADJUSTMENT DETAIL (SSD2)**

8      7      6      5      4      3      2      1

8      7      6      5      4      3      2      1

H  
G  
F  
E  
D  
C  
B  
A

H  
G  
F  
E  
D  
C  
B  
A



MIKE DUNN  
DIRECTOR

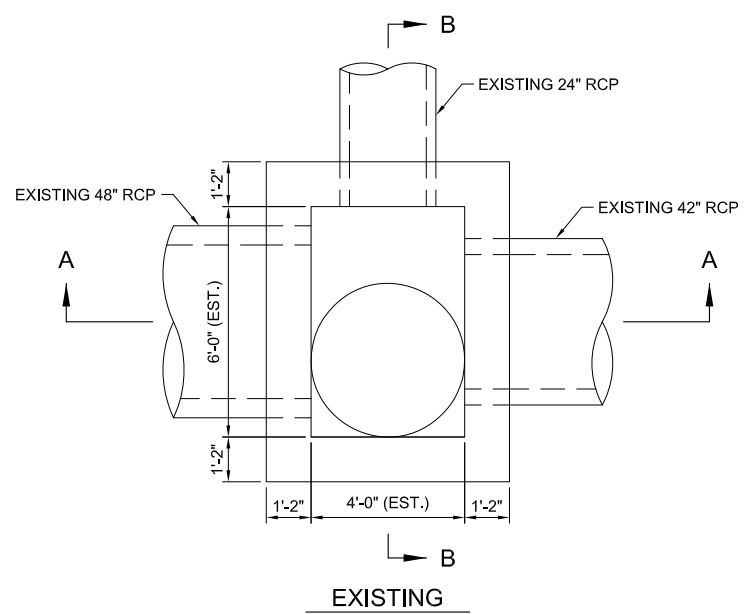


Chicago Rockford  
International Airport  
Rockford, Illinois

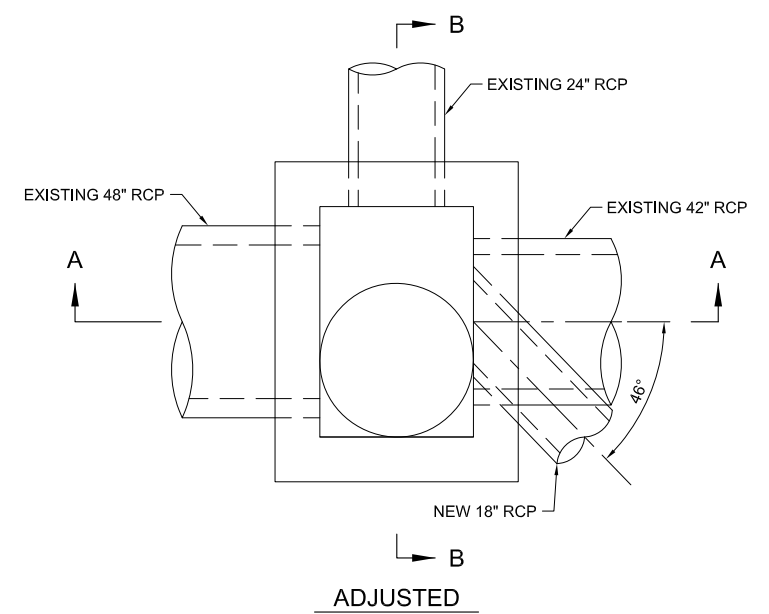
Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

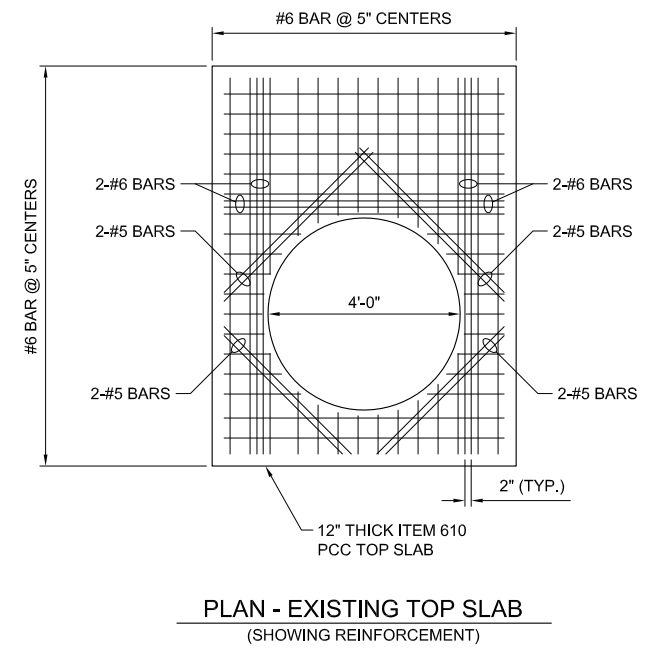
**RO020**



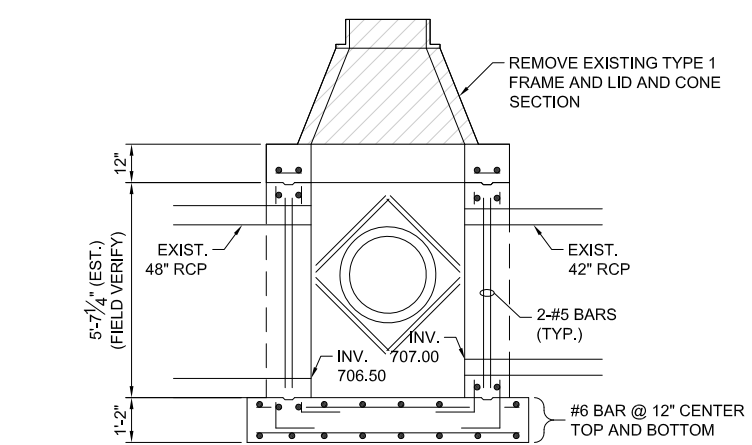
PLAN VIEW



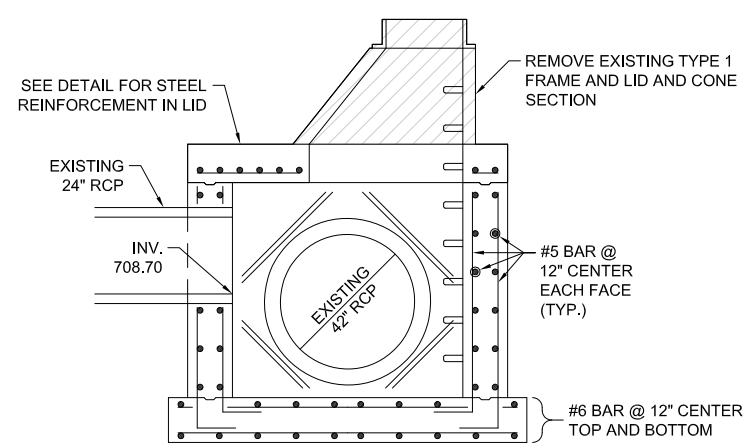
ADJUSTED



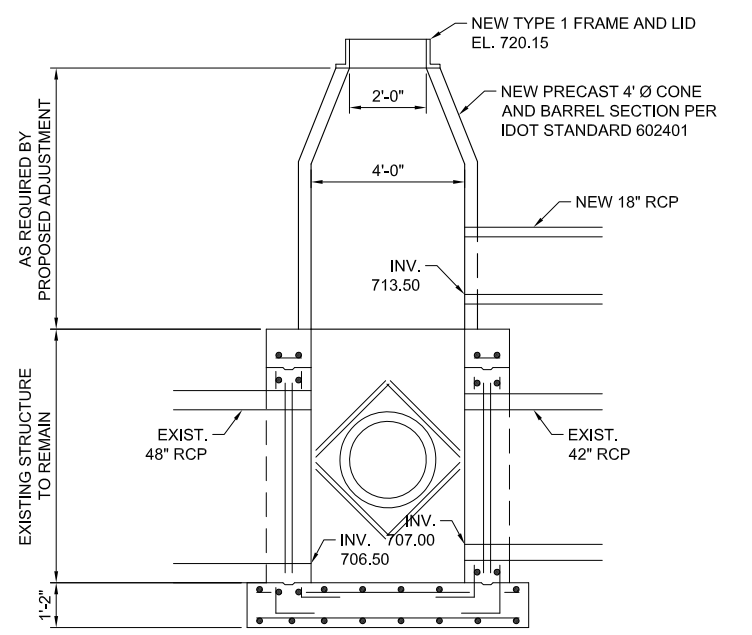
PLAN - EXISTING TOP SLAB  
(SHOWING REINFORCEMENT)



EXISTING SECTION A-A

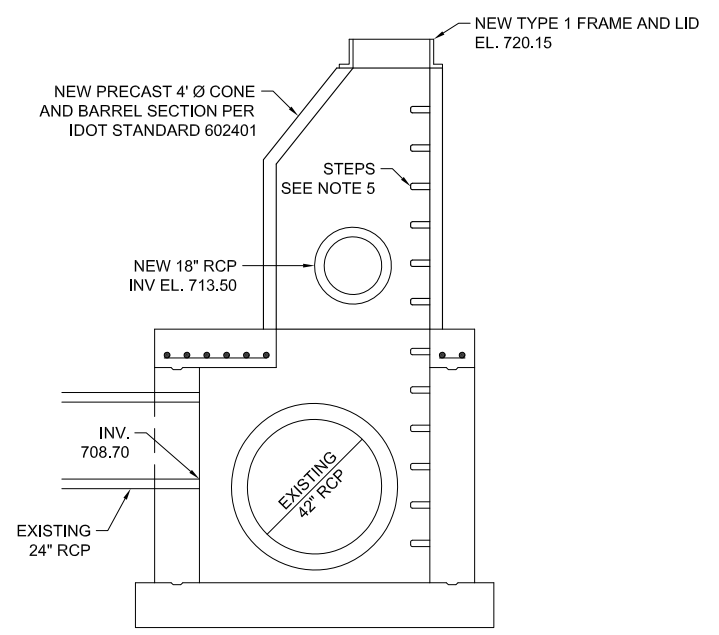


EXISTING SECTION B-B



ADJUSTED SECTION A-A

SECTION VIEWS



ADJUSTED SECTION B-B

**MANHOLE ADJUSTMENT DETAILS**  
(N.T.S.)

NOTES:

1. NEW RCP TO BE PAID UNDER ITEM AR701518.
2. THE NEW TYPE 1 FRAME AND LID SHALL BE SECURED TO THE NEW CONE SECTION TO PREVENT MOVEMENT DURING BACKFILL OPERATIONS.
3. THE NEW CONE AND BARREL SECTIONS SHALL BE SECURED TO THE TOP SLAB TO PREVENT MOVEMENT DURING BACKFILL OPERATIONS.
4. THE INVERT OF THE NEW RCP SHALL BE ADJUST AS REQUIRED TO ENSURE THAT THE NEW PIPE PENETRATES THE ADJUSTED STRUCTURE WITHIN THE NEW BARREL SECTION.
5. STEPS SHALL BE PROVIDED AT A MINIMUM IN THE ADJUSTMENT SECTION, TO BE SPACED FROM 12 TO 15 INCHES ON CENTER. THE DISTANCE BETWEEN THE LAST STEP IN THE ADJUSTED SECTION AND THE FIRST STEP IN THE EXISTING SECTION SHALL NOT EXCEED 15 INCHES.
6. ADJUSTING RINGS SHALL NOT BE USED.
7. PRIOR TO ORDERING THE BARREL AND CONE SECTION, THE CONTRACTOR SHALL DETERMINE THE ELEVATION OF THE EXISTING TOP SLAB SUCH THAT THE PROPER BARREL HEIGHT CAN BE DETERMINED.
8. CONCRETE FOR COLLARS AND BENCHES SHALL MEET THE REQUIREMENTS OF ITEM 610.
9. THE DIMENSIONS OF THE EXISTING STRUCTURE ARE APPROXIMATE BASED ON AVAILABLE RECORD DRAWINGS. NO ADDITIONAL COMPENSATION SHALL BE GRANTED FOR MINOR VARIATIONS IN ACTUAL SIZE ENCOUNTERED.

**STORM SEWER MANHOLE  
ADJUSTMENT DETAIL (SSD3)**

APPROVED AS WORKING PLAN		
BY:		
REV	DATE	DESCRIPTION
PROJECT NAME:		

**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**STORM SEWER MANHOLE  
ADJUSTMENT DETAIL**

DESIGNED: SMS	DRAWN: CMT	CHECKED: DLP
------------------	---------------	-----------------

PROJECT NO.:	11258-03-00/06
DATE:	APRIL 20, 2012

SHEET NO.	SSD3	REVISION
SHEET 51 OF 70 SHEETS		

RO020



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

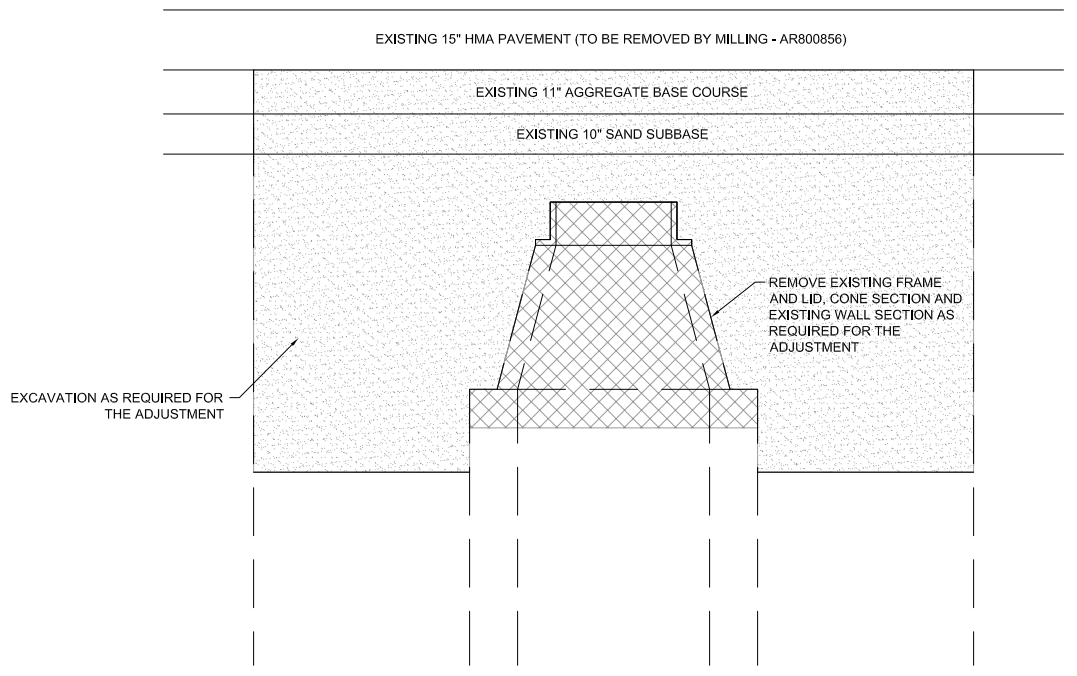
SHEET TITLE:  
**SANITARY SEWER MANHOLE  
ADJUSTMENT DETAIL**

DESIGNED: ATI      DRAWN: CMT      CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **SND1** REVISION

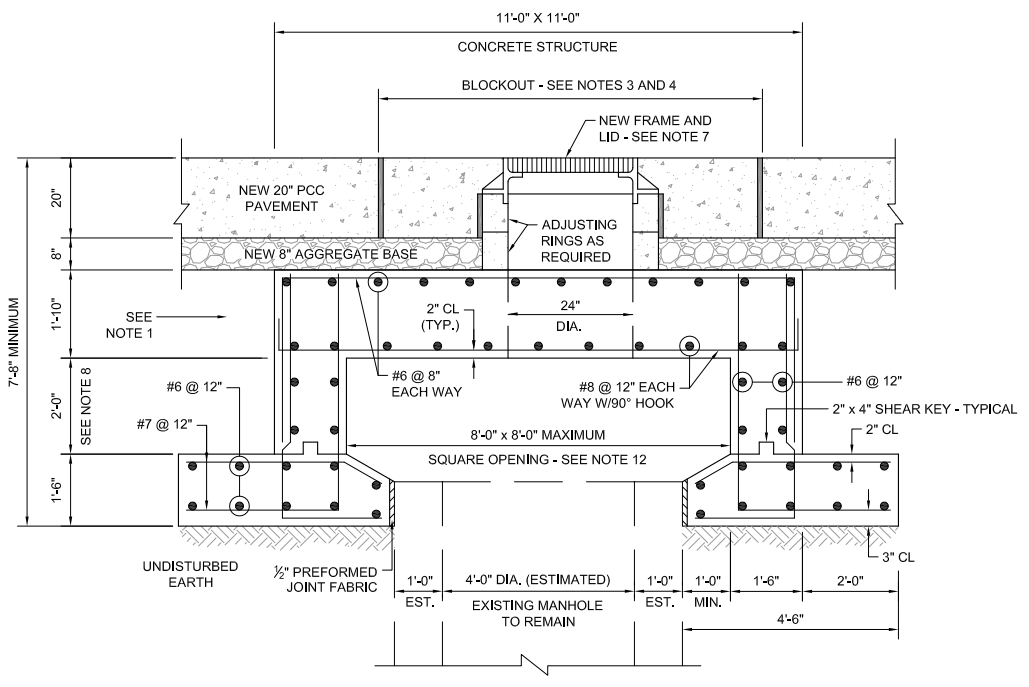
SHEET 52 OF 70 SHEETS



**RRWRD SANITARY MANHOLE ADJUSTMENT DETAIL - DEMOLITION**  
NOT TO SCALE

**STRUCTURE DEMOLITION NOTES:**

- SEE PAVING PLAN AND PROFILE SHEETS FOR THE LOCATION OF THE STRUCTURE TO BE ADJUSTED.
- THE EXISTING STRUCTURE IS ESTIMATED TO BE 12-INCHES BELOW THE BOTTOM OF THE SAND SUBBASE. THE AGGREGATE BASE COURSE, SAND SUBBASE AND SUBGRADE MATERIALS SHALL BE REMOVED TO A DEPTH AT LEAST 92-INCHES (7'-8") BELOW THE TOP OF THE PROPOSED PAVEMENT SECTION.
- THE PORTION OF THE EXISTING STRUCTURE EXPOSED DURING EXCAVATION SHALL BE REMOVED AND DISPOSED OF OFF AIRPORT PROPERTY.
- THE EXISTING STRUCTURE IS ASSUMED TO BE MADE OF BRICK AND PRECAST BARREL AND CONE SECTIONS WITH AN INSIDE DIAMETER OF 4'-0", WITH 12-INCH THICK WALLS. THE DESIGNER SHALL BE NOTIFIED IN THE EVENT THAT THE EXISTING STRUCTURE IS LARGER THAN ASSUMED.
- CARE SHALL BE TAKEN TO NOT DAMAGE THE EXISTING STRUCTURE TO REMAIN.
- ALL EXCAVATION REQUIRED TO LOCATE THE EXISTING STRUCTURE AND TO REACH THE PROPOSED FOUNDATION DEPTH SHALL BE CONSIDERED INCIDENTAL TO THE ADJUSTMENT.
- THE EXISTING STRUCTURE SHALL BE SAWED AT THE REMOVAL LIMITS. ALL REMOVAL AND DISPOSAL OF THE EXISTING STRUCTURE SHALL BE CONSIDERED INCIDENTAL TO THE ADJUSTMENT.
- AFTER THE EXISTING STRUCTURE IS LOCATED AND EXAMINED, AT THE CONTRACTOR'S OPTION, A SOLUTION USING PRECAST MATERIALS MAY BE PROPOSED, PROVIDED THE CONTRACTOR PROVIDES SUFFICIENT STRUCTURAL DESIGN CALCULATIONS FOR REVIEW. THE ENGINEER SHALL HAVE A MINIMUM OF 5 WORKING DAYS TO REVIEW THE PROPOSED ALTERNATIVE. THE PROPOSED ALTERNATIVE SHALL BEAR EXCLUSIVELY ON A FOUNDATION SEPARATE FROM THE EXISTING STRUCTURE.



**RRWRD SANITARY MANHOLE ADJUSTMENT DETAIL - ADJUSTED**  
NOT TO SCALE

**ADJUSTED STRUCTURE NOTES:**

- AREAS EXCAVATED UNDER THE PROPOSED RUNWAY PAVEMENT SHALL BE BACKFILLED WITH CRUSHED AGGREGATE BASE COURSE MEETING THE REQUIREMENTS OF AR209 AND COMPACTED TO 95% OF MODIFIED PROCTOR (ASTM D-1557). AGGREGATE BACKFILL SHALL BE CONSIDERED INCIDENTAL TO THE ADJUSTMENT.
- AN 8-INCH LIFT OF CRUSHED AGGREGATE BASE COURSE SHALL BE PLACED AND COMPACTED OVERTOP OF THE PROPOSED ADJUSTED MANHOLE PRIOR TO PCC PAVING. AGGREGATE BASE COURSE SHALL BE CONSIDERED INCIDENTAL TO THE ADJUSTMENT.
- COORDINATE BLOCKOUT IN FIELD WITH RESIDENT ENGINEER BASED ON FIELD LOCATION OF ADJUSTED MANHOLE.
- EXPANSION MATERIAL SHALL BE PLACED AROUND THE ADJUSTING RINGS AND AT THE EDGES OF THE BLOCKOUT. EXPANSION JOINT MATERIAL SHALL BE PER THE EXPANSION JOINT DETAIL ON SHEET JDT1. EXPANSION MATERIAL SHALL ALSO BE PLACED AROUND THE EXISTING STRUCTURE TO REMAIN, FULL DEPTH OF THE PROPOSED FOUNDATION.
- NO MORE THAN TWO ADJUSTING SHALL BE ALLOWED WITHOUT THE PERMISSION OF THE RESIDENT ENGINEER.
- WRAPS AND SEALS PER RRWRD REQUIREMENTS SHALL BE USED TO MAKE WATER TIGHT SEALS.
- NEW FRAME AND LID SHALL BE NEENAH R-3493-A OR APPROVED EQUAL. LID SHALL BE BOLTED TO THE FRAME.
- THE HEIGHT OF THE WALL SECTION IS DEPENDENT UPON THE DEPTH OF THE EXISTING STRUCTURE. THE WALL SHALL BE A MINIMUM OF 2'-0" TALL.
- THE WALL SECTION AND FLATTOP LID SHALL BEAR EXCLUSIVELY ON THE PROPOSED FOUNDATION. NO STRESSES SHALL BE IMPARTED ON THE EXISTING STRUCTURE TO REMAIN.
- CONCRETE SHALL MEET THE REQUIREMENTS OF ITEM 610 EXCEPT THAT THE CONCRETE SHALL HAVE A COMPRESSIVE STRENGTH OF 4,000 PSI AT 14 DAYS.
- STEEL REINFORCEMENT SHALL BE GRADE 60 OF THE SIZES SHOWN.
- IF, BASED ON FIELD CONDITIONS, THE OPENING IS REQUIRED TO BE LARGER THAN 8'-0" x 8'-0", THE DESIGNER SHALL BE IMMEDIATELY NOTIFIED. THE PROPOSED STRUCTURE SHALL NOT BE CONSTRUCTED UNTIL THE DESIGNER HAS REVIEWED THE CHANGE IN CONDITION.

**SANITARY SEWER MANHOLE  
ADJUSTMENT DETAIL (SND1)**





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN BY:		
REV	DATE	DESCRIPTION
PROJECT NAME:		

**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**ELECTRICAL DETAILS - 1  
(ELD1)**

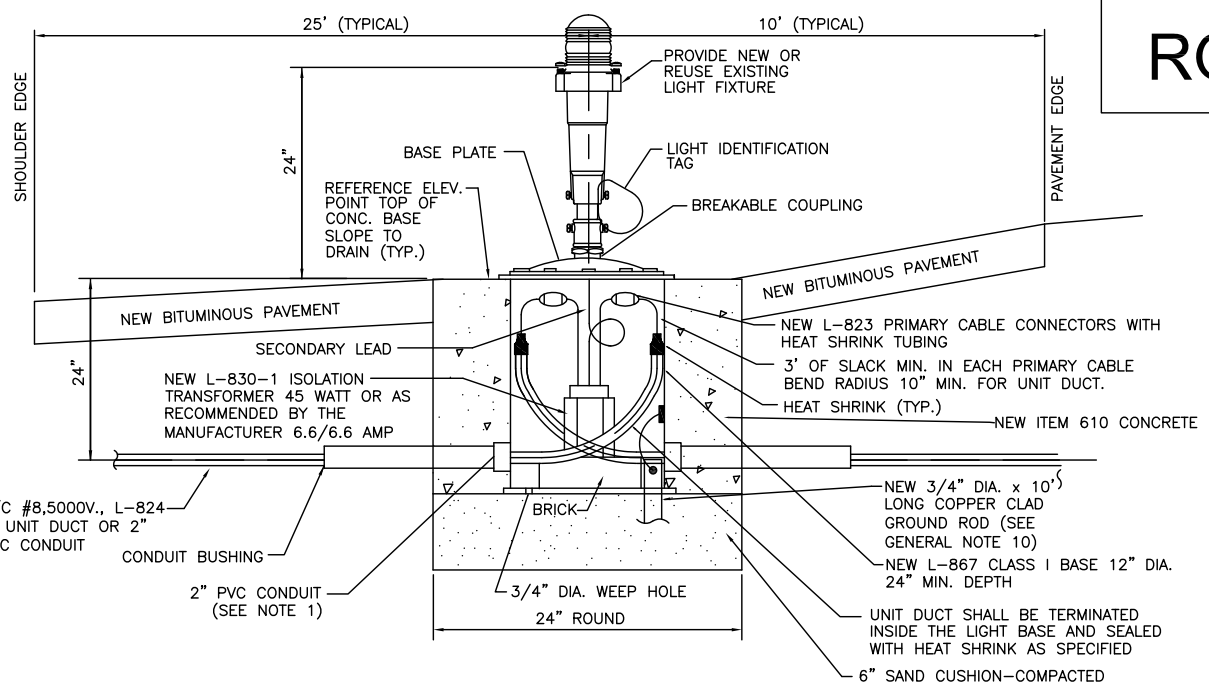
DESIGNED: CMT	DRAWN: CMT	CHECKED: DLP
------------------	---------------	-----------------

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.	<b>ELD1</b>	REVISION
-----------	-------------	----------

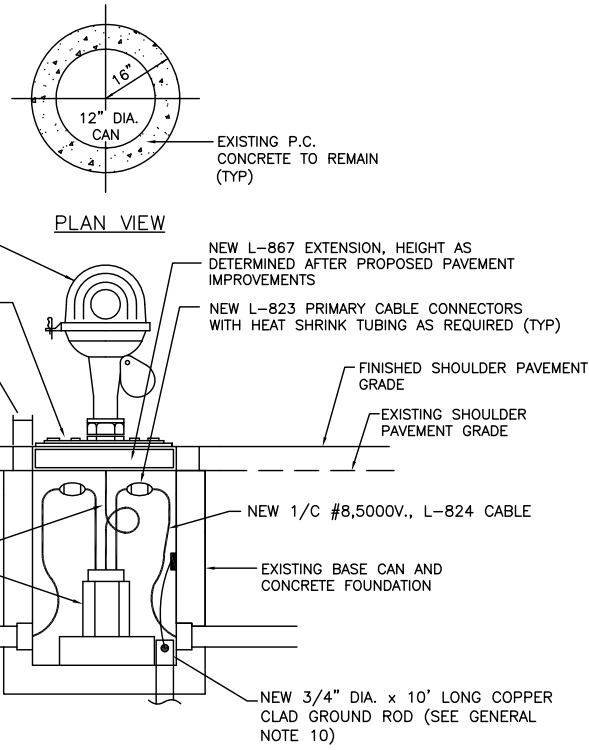
SHEET 53 OF 70 SHEETS

**RO020**



**NEW OR RELOCATED HIRL/MITL BASE MOUNTED LIGHT**  
NOT TO SCALE

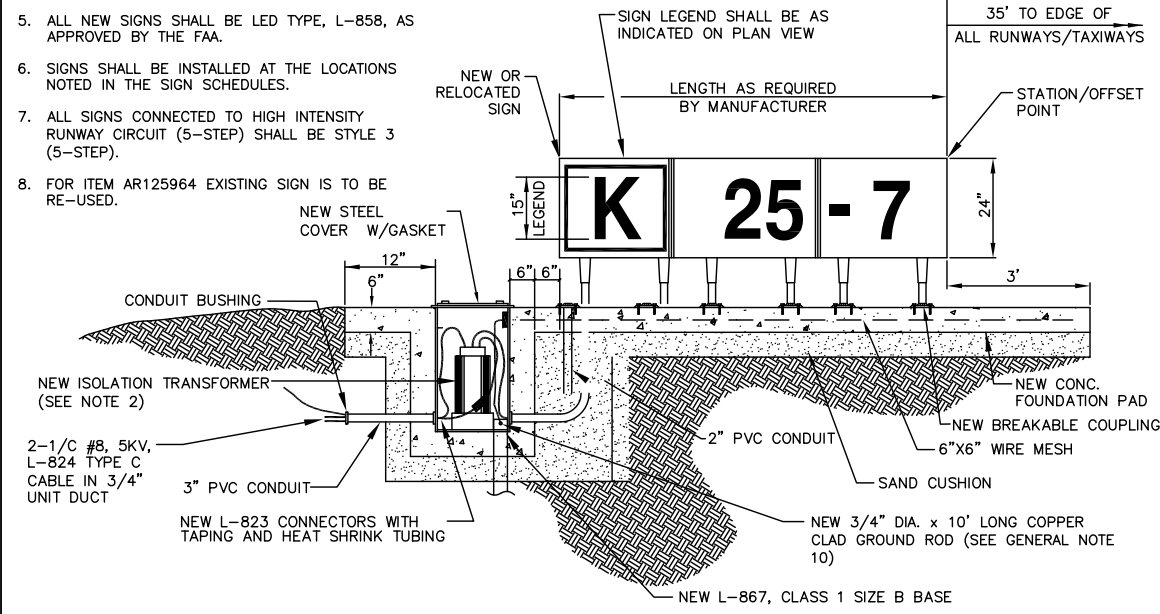
- INSTALL 2" PVC TO GRS ADAPTER WHERE PVC CONCRETE ENCASED DUCT WILL BE INSTALLED UNDER PAVED SHOULDER.
- RELOCATED LIGHTS SHALL INCLUDE NEW CONCRETE FOUNDATION, NEW ISOLATION TRANSFORMER, NEW GROUNDING, AND NEW CONNECTORS. ONLY THE FIXTURE SHALL BE REUSED FOR A RELOCATED LIGHT. BRICKS MAY BE REUSED IF IN SATISFACTORY CONDITION AND APPROVED BY THE RESIDENT ENGINEER.
- PAVEMENT REMOVAL AND REPLACEMENT FOR INSTALLATION OF A NEW OR RELOCATED BASE MOUNTED LIGHT IN EXISTING PAVEMENT SHALL BE PAID UNDER ITEM AR401910, REMOVE AND REPLACE BITUMINOUS PAVEMENT.



**ADJUST BASE MOUNTED LIGHT**  
NOT TO SCALE

INSTALL 2" PVC TO GRS ADAPTER WHERE PVC CONCRETE WILL BE INSTALLED UNDER PAVED SHOULDER.

- NOTES**
- ALL SIGNS ARE 2 - SIDED SIGNS.
  - TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER
  - LIGHTED SIGNS SHALL BE BASE MOUNTED ONLY.
  - UNIT DUCT SHALL BE TERMINATED IN THE CAN AND SEALED TO THE CABLE WITH HEAT SHRINK AS SPECIFIED.
  - ALL NEW SIGNS SHALL BE LED TYPE, L-858, AS APPROVED BY THE FAA.
  - SIGNS SHALL BE INSTALLED AT THE LOCATIONS NOTED IN THE SIGN SCHEDULES.
  - ALL SIGNS CONNECTED TO HIGH INTENSITY RUNWAY CIRCUIT (5-STEP) SHALL BE STYLE 3 (5-STEP).
  - FOR ITEM AR125964 EXISTING SIGN IS TO BE RE-USED.



**NEW / RELOCATED TAXI GUIDANCE SIGN, X CHARACTER  
HOLD LINE/TAXIWAY GUIDANCE SIGN LED L-858, SIZE 2, STYLE 2/3, CLASS 2**  
NO SCALE (TYPICAL)

CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWING INCLUDING SIGN, COLOR, SIZE, PROPOSED LEGEND, IN ENOUGH DETAIL AND DETERMINE NEW SPACING AND OTHER INFORMATION REQUIRED BY SPECIAL PROVISIONS. CONTRACTOR TO VERIFY NEW SIGN LOCATIONS AND ORIENTATIONS WITH RESIDENT ENGINEER PRIOR TO INSTALLATION. SIGN SHALL BE FAA APPROVED LED TYPE SIGN.

**GENERAL NOTES:**

- THE CONCRETE BASE FOR BASE MTD. LIGHTS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (610).
- TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL SIGNS, LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT DIRECTOR, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE AT NO ADDITIONAL COST.
- TAXIWAY LIGHTS SHALL HAVE A BLUE LENS, RUNWAY LIGHTS SHALL HAVE A CLEAR OR 180° AMBER/CLEAR LENS AS DESIGNATED ON PLANS.
- DUCT MARKERS SHALL BE INSTALLED AT EVERY NEW DUCT AND AT EVERY EXISTING DUCT USED FOR THIS PROJECT.
- CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.
- INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD. INSTALL GROUND LUG FOR EXISTING CANS, IF REQUIRED.
- PROVIDE MINIMUM OF ONE LOOP OF CABLE SLACK FOR EACH CABLE INSIDE LIGHT BASE/HANDHOLE.
- EXISTING FIXTURES SHALL BE USED FOR ALL ADJUSTED AND RELOCATED GUIDANCE SIGNS, TAXIWAY AND RUNWAY LIGHTS EXCEPT WHERE NOTED.

**ELECTRICAL DETAILS - 1  
(ELD1)**

RO020



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
ELECTRICAL DETAILS - 2  
(ELD2)

DESIGNED: CMT DRAWN: CMT CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. ELD2 REVISION

SHEET 54 OF 70 SHEETS

TEMPORARILY INSTALL STEEL COVER  
UNTIL LIGHT FIXTURE IS TO BE  
INSTALLED - TEMPORARY STEEL  
COVERS SHALL BE HEAVY DUTY  
3/4" THICKNESS. ALL OTHER  
TEMPORARY COVERS SHALL BE  
STANDARD THICKNESS.

NEW L-868 BASE, 12"  
DIAMETER, 24" DEEP

ITEM 610 CONCRETE  
ENCASEMENT (TYP)

COMPACTED SUBGRADE

NEW OR EXISTING CRUSHED  
AGGREGATE BASE COURSE

STEP 1

CORE DRILL 14" +/- DIAMETER  
TO ACCOMMODATE ADJUSTING  
RING AND SEALANT

AFTER CORING IS COMPLETE,  
INSTALL SPACER RINGS AS  
REQUIRED AND INSTALL NEW OR  
EXISTING FIXTURE. SEE DETAIL A.

NEW HMA PAVEMENT  
TO BE PAID AS  
AR401/403 OR  
AR401910

NEW OR EXISTING CRUSHED  
AGGREGATE BASE COURSE

STEP 2

NEW/RELOCATED IN-PAVEMENT LIGHT  
INSTALLATION IN EXISTING/NEW BITUMINOUS PAVEMENT

NO SCALE

REMOVE EXISTING LIGHT  
FIXTURE AND ADJUSTING  
RINGS. COST SHALL BE  
CONSIDERED INCIDENTAL TO  
AR125942

REMOVE 5/8" PLYWOOD AND REBOLT  
1/8" MUD PLATE PRIOR TO PAVING  
TOP OF NEW PAVEMENT

1-3/8" FROM TOP OF MUD  
PLATE TO SURFACE

PROP. PCC PAVEMENT

EXISTING L-868 BASE

EXISTING 610 CONCRETE

STEP 1

VERIFY ACTUAL  
THICKNESS

INSERT "HOOP" TOOL IMMEDIATELY  
FOLLOWING CONCRETE PAVING. WORK  
INTO WET CONCRETE TO AT LEAST  
1" BELOW MUD PLATE

2" GRS CONDUIT (TYP)

- ① REMOVE WET CONCRETE INSIDE HOOP, REMOVE MUD PLATE AND CLEAN TOP FLANGE OF LIGHT BASE. REINSTALL MUD PLATE.
- ② FINISH EDGE AROUND HOOP TOOL WITH A CURVED TROWEL TO PROVIDE A RADIUS AROUND THE EDGE.

STEP 2

ADJUST IN-PAVEMENT LIGHT  
INSTALLATION IN NEW PCC PAVEMENT

NO SCALE

AFTER CONCRETE HAS SET REMOVE  
HOOP TOOL AND MUD PLATE.  
INSTALL SPACER RING(S) AS REQUIRED.  
(SEE DETAIL A)

TEMPORARILY INSTALL STEEL COVER  
UNTIL LIGHT FIXTURE IS TO BE  
INSTALLED - TEMPORARY STEEL COVERS  
SHALL BE HEAVY DUTY 3/4" THICKNESS.  
ALL OTHER TEMPORARY COVERS SHALL  
BE STANDARD THICKNESS.

NEOPRENE GROMMET  
3/4" PVC DRAINPIPE  
FILTER FABRIC

STEP 3

REMOVE EXISTING LIGHT  
FIXTURE AND ADJUSTING  
RINGS. COST SHALL BE  
CONSIDERED INCIDENTAL TO  
AR125942.

TEMPORARILY INSTALL STEEL COVER  
UNTIL LIGHT FIXTURE IS TO BE  
INSTALLED - TEMPORARY STEEL  
COVERS SHALL BE HEAVY DUTY  
3/4" THICKNESS. ALL OTHER  
TEMPORARY COVERS SHALL BE  
STANDARD THICKNESS.

CORE DRILL 14" +/- DIAMETER  
TO ACCOMMODATE ADJUSTING  
RING AND SEALANT

AFTER CORING IS COMPLETE  
INSTALL SPACER RINGS AS  
REQUIRED AND REINSTALL  
EXISTING FIXTURE. SEE DETAIL A.

NEW HMA  
PAVEMENT

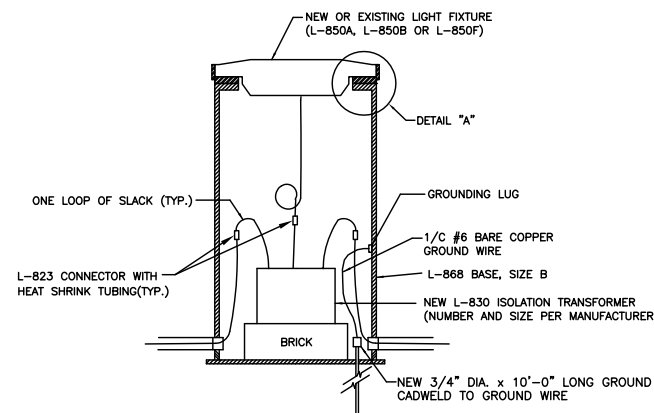
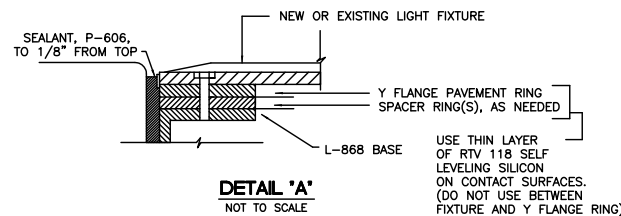
EXISTING BASE CAN

STEP 1

STEP 2

ADJUST IN-PAVEMENT LIGHT  
INSTALLATION IN NEW BITUMINOUS PAVEMENT

NO SCALE



INSTALLATION IN PCC PAVEMENT NOTES

- EXCAVATE TO PROPER DEPTH TO ALLOW 6" CONCRETE ENCASEMENT UNDER NEW BASES AND 6" UNDER NEW CONDUIT. CLEAN CUT EDGES AND COMPACT BOTTOM OF EXCAVATION.
- USE MANUFACTURERS SETTING JIG (OR OTHER DEVICE APPROVED BY THE ENGINEER FOR PROPERLY ALIGNING NEW L-868 BASES. SECURE SETTING JIG TO PREVENT MOVEMENT DURING CONCRETE ENCASEMENT. ALL CONDUITS TO BE SUPPORTED DURING CONSTRUCTION AS REQUIRED.
- ALL LIGHT BASES SHALL BE PROPERLY POSITIONED AND ALIGNED AND CONDUIT CONNECTING THE BASES PROPERLY SECURED IN PLACE BEFORE POURING CONCRETE. ENSURE PROPER ALIGNMENT AFTER CONCRETE ENCASEMENT OF NEW BASE BEFORE CONCRETE SETS. TIGHT CONNECTIONS MUST BE ASSURED TO PREVENT CONCRETE FROM ENTERING BASE OR CONDUIT.
- AFTER INSTALLATION OF THE L-868 BASE AND WHILE PCC PAVING AT THE FIXTURE LOCATION IS WET, INSERT "HOOP" TOOL TO AT LEAST 1" BELOW MUD PLATE. REMOVE WET CONCRETE FROM INSIDE HOOP AND CLEAN TOP FLANGE SURFACE. FINISH THE CONCRETE AROUND THE HOOP TOOL WITH A CURVED RADIUS TROWEL. USE CAUTION TO AVOID AGGREGATE SEGREGATION DURING THIS PROCEDURE.
- AFTER CONCRETE HAS SET, REMOVE HOOP TOOL AND MUD PLATE. INSTALL FLANGE AND SPACER RINGS AS REQUIRED AND LIGHT ASSEMBLY. A TEMPORARY STEEL COVER MAY BE INSTALLED IF LIGHT FIXTURE IS TO BE INSTALLED LATER.
- AFTER FIXTURE INSTALLATION, FILL THE ANNULAR SPACE BETWEEN THE FIXTURE BASE AND SURROUNDING PAVEMENT WITH P-606 SEALANT. ANNULAR SPACE SHALL BE NO MORE THAN 3/4" WIDE.
- INSTALL BASE AND RINGS SO THAT OUTER EDGE OF LIGHT FIXTURE WILL BE AT SAME ELEVATION AS THE FINISHED PAVEMENT SURFACE TO (+) 0" (-) 1/16" TOLERANCE. THE TOTAL THICKNESS OF THE SPACER/FLANGE RINGS SHALL BE NO LARGER THAN 3/4" MAX. NO MORE THAN 3 RINGS SHALL BE USED.

ELECTRICAL DETAILS - 2  
(ELD2)

RO020



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN BY:		

REHABILITATE  
RUNWAY 1/19  
PHASE 2

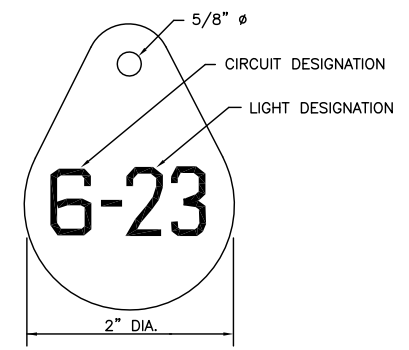
SHEET TITLE:  
**ELECTRICAL DETAILS - 3  
(ELD3)**

DESIGNED: CMT      DRAWN: CMT      CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **ELD3** REVISION

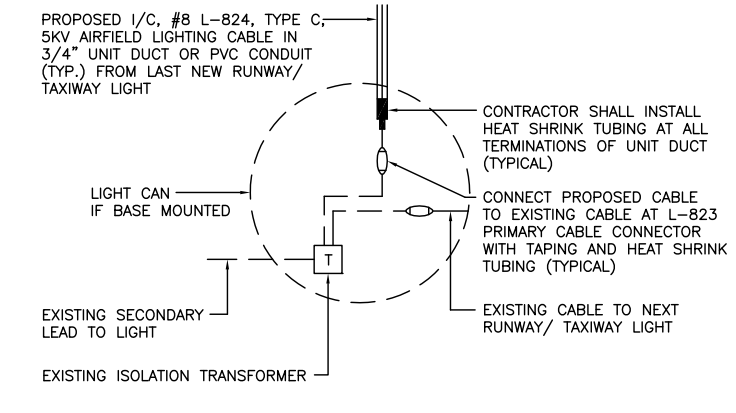
SHEET 55 OF 70 SHEETS



**LIGHT IDENTIFICATION DETAIL**  
NOT TO SCALE

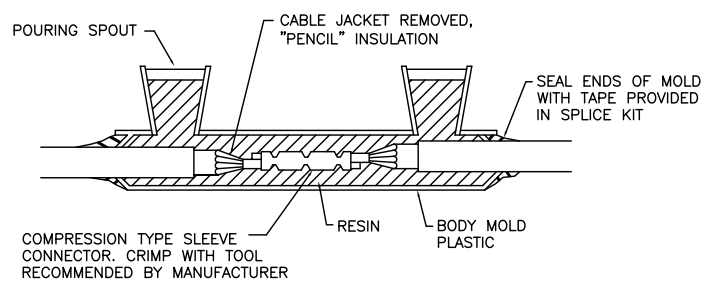
NOTES:

1. INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
2. NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (NEW OR RELOCATED LIGHTS) SHALL BE RETAGGED.
3. COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



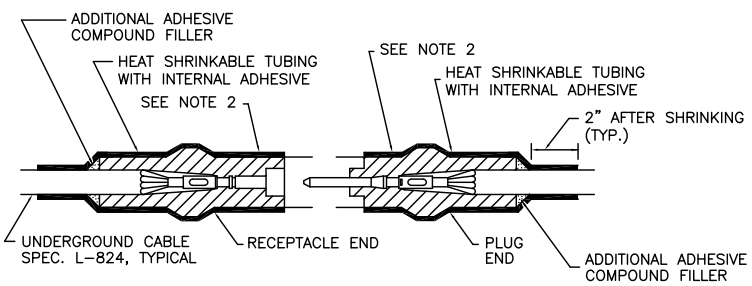
**RUNWAY/TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL**  
NOT TO SCALE

**ELECTRICAL DETAILS - 3  
(ELD3)**



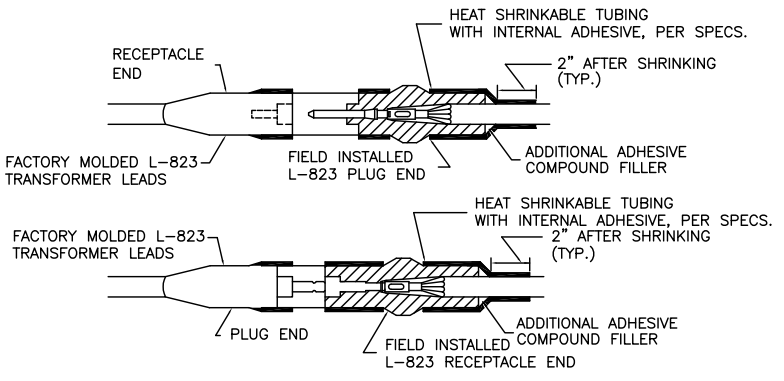
**TYPE A - CABLE SPLICE**

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY  
N.T.S.



**TYPE B - CABLE SPLICE**

FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT  
N.T.S.

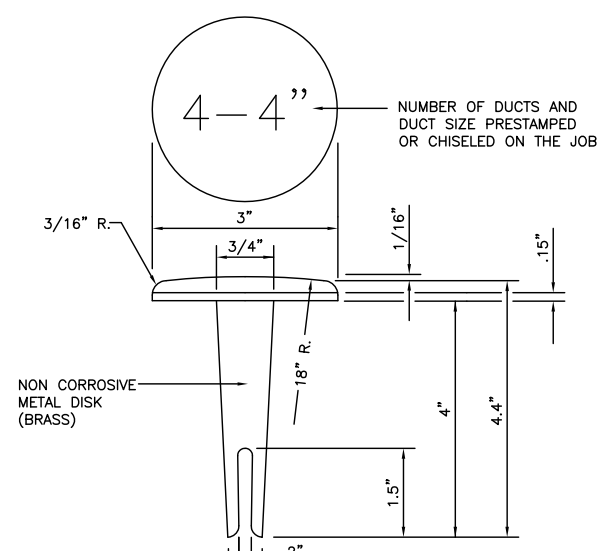


**TYPE C AND D - CABLE SPLICE**

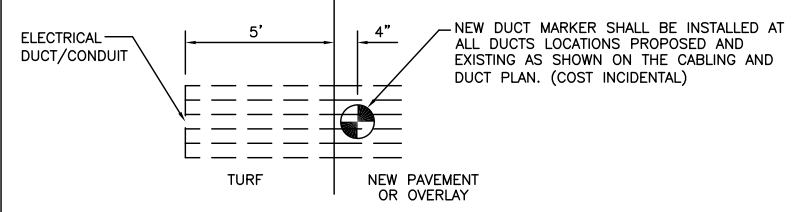
FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS  
N.T.S.

NOTES

1. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
2. WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
3. THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
4. THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

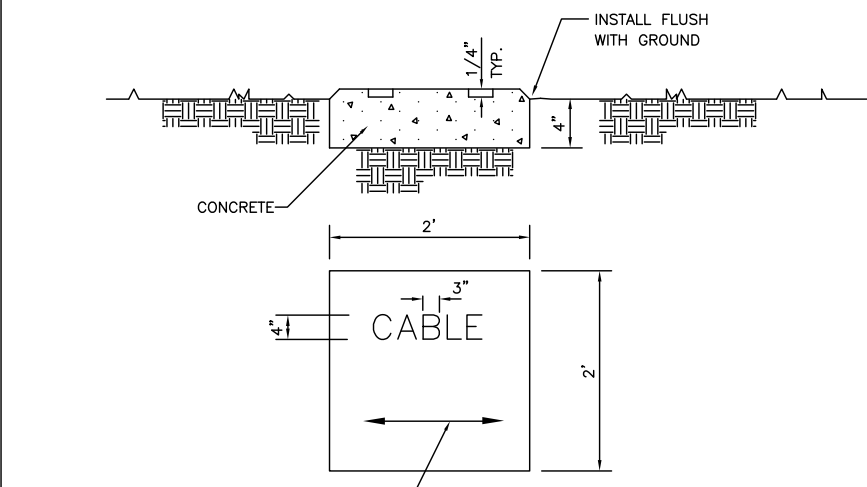


**DUCT/CONDUIT MARKER DETAIL**  
NOT TO SCALE



**TURF CABLE MARKER DETAIL**  
NOT TO SCALE

- NOTES:
1. CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUN.
  2. ITEM 610 CONCRETE SHALL BE USED.
  3. ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
  4. THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
  5. 0.049 CU. YD. CONCRETE PER MARKER.



**TURF CABLE MARKER DETAIL**  
NOT TO SCALE





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
**ELECTRICAL DETAILS - 4  
(ELD4)**

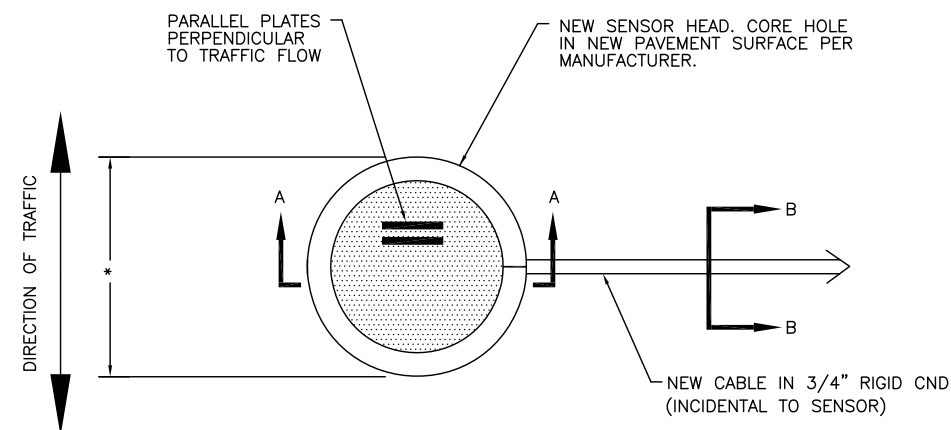
DESIGNED: CMT	DRAWN: CMT	CHECKED: DLP
------------------	---------------	-----------------

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

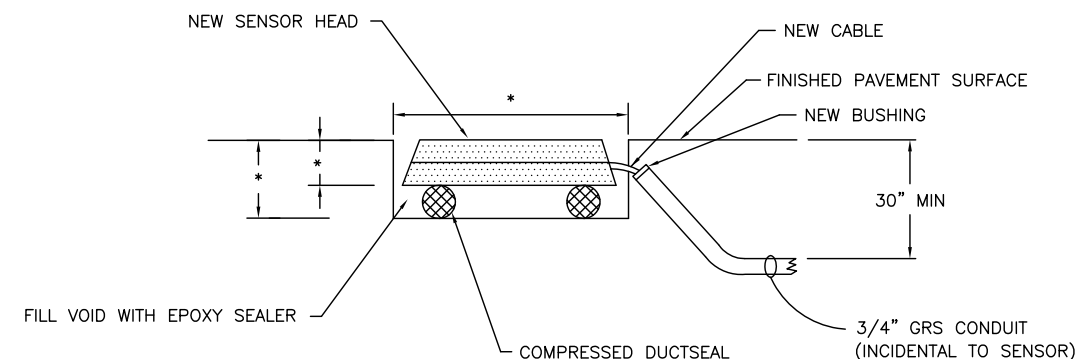
SHEET NO.	REVISION
ELD4	

SHEET 56 OF 70 SHEETS

RO020



PLAN

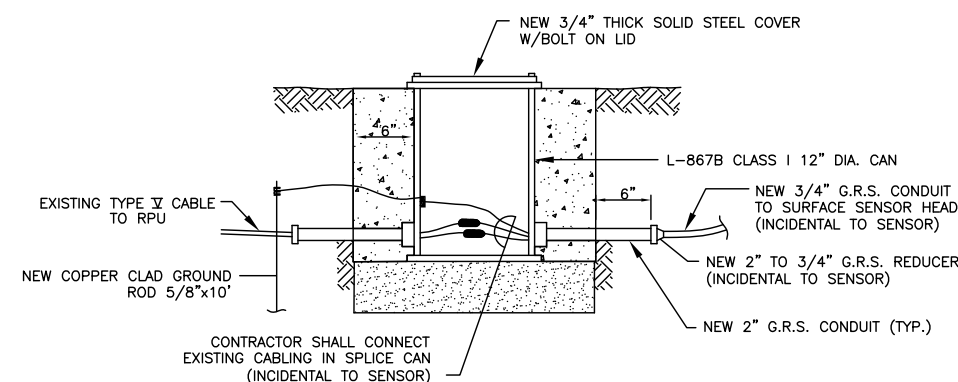


SECTION A-A

**SURFACE SENSOR HEAD DETAIL**

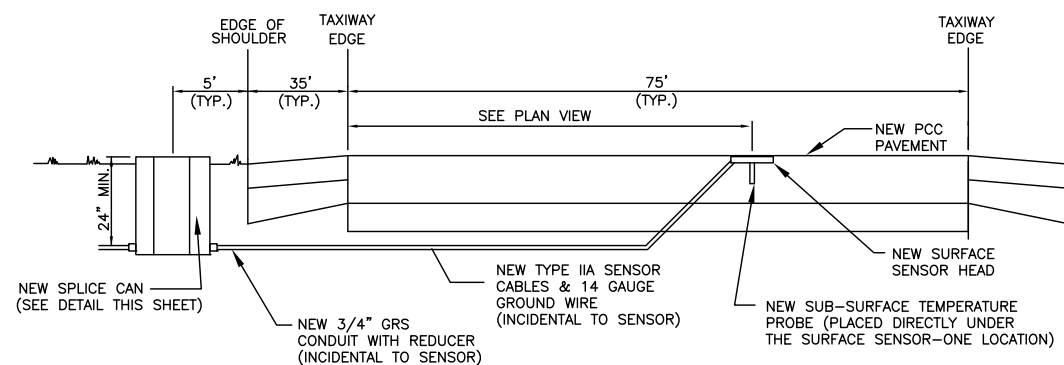
N.T.S.

\* DIMENSIONS AS REQUIRED BY SENSOR MANUFACTURER.



**NEW PAVEMENT SENSOR SPLICE CAN DETAIL**

N.T.S.



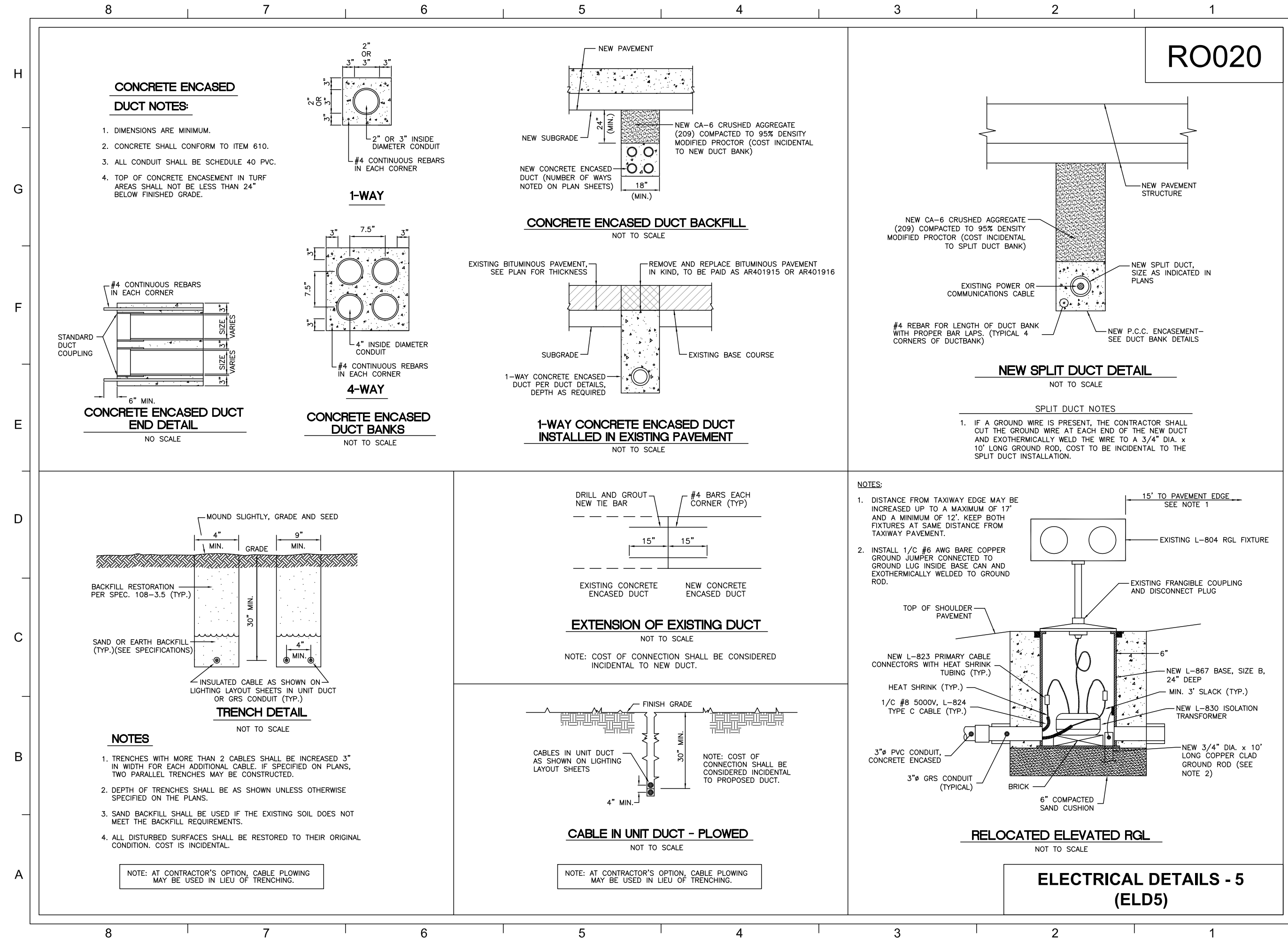
**SURFACE SENSOR INSTALLATION OVERVIEW**

N.T.S.

**NOTES FOR NEW SURFACE SENSOR SYSTEM**

- EXISTING SURFACE SENSOR SYSTEM IS SCAN, MANUFACTURED BY SURFACE SYSTEMS, INC. (QUIXOTE). NEW SENSOR SHALL BE CONFIRMED BY THE CONTRACTOR AS BEING COMPATIBLE WITH THE EXISTING SYSTEM.
- INSTALL NEW (FP2000 TYPE) SURFACE SENSOR IN PAVEMENT AT PROXIMITY OF EXISTING SENSOR LOCATION AS SHOWN IN THE PLANS. INSTALL NEW SPLICE CAN AT EDGE OF THE PAVEMENT TO INTERCEPT EXISTING SENSOR HOMERUN CABLE. INSTALL NEW SENSOR CABLE IN NEW CONDUIT TO SPLICE CAN. SPLICE TO EXISTING SENSOR HOMERUN CABLE IN SPLICE CAN.
- SPLICE CAN SHALL BE PAID FOR SEPARATELY.

**ELECTRICAL DETAILS - 4  
(ELD4)**



RO020



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN BY:		
REV	DATE	DESCRIPTION
PROJECT NAME:		

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
ELECTRICAL DETAILS - 5  
(ELD5)

DESIGNED:	DRAWN:	CHECKED:
CMT	CMT	DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.	ELD5	REVISION
SHEET 57 OF 70 SHEETS		



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

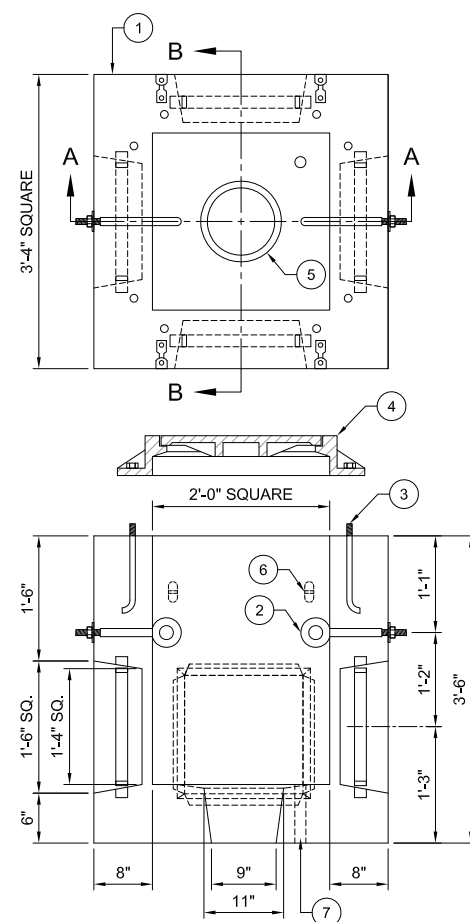
RO020

NOTES:

- CONCRETE SHALL MEET THE REQUIREMENTS OF ITEM 610.
- REBAR MEETING ASTM A-706, GRADE 60, SHALL BE INSTALLED ON 8-INCH CENTERS BOTH WAYS. REBAR SHALL BE MINIMUM #4 BARS. 3" CLEARANCE SHALL BE MAINTAINED BETWEEN THE REBAR AND THE FINISHED FACE OF THE CONCRETE.
- A 6-INCH THICK BED OF CA-6 SHALL BE PLACED BENEATH THE HANDHOLE AND COMPACTED TO NOT LESS THAN 95% MODIFIED PROCTOR (ASTM D-1557).
- THE HANDHOLE DRAIN SHALL BE CONNECTED TO THE PROPOSED UNDERDRAIN SYSTEM AT NO ADDITIONAL COST TO THE CONTRACT.
- ANCHOR BOLTS SHALL BE SET TO MATCH THE ANCHOR BOLT PATTERN ON THE FRAME.

KEYED NOTES:

- PRECAST CONCRETE HANDHOLE (1)
- PULLING IRONS - MINIMUM 1" Ø STEEL, HOT DIPPED GALVANIZED (MIN. 2)
- ANCHOR BOLTS - MINIMUM ¾" Ø STEEL, 16" LONG WITH 2" HOOK (PER FRAME MFR.)
- FRAME AND COVER - NEENAH R-3492 OR APPROVED EQUAL, LID SHALL BOLT TO FRAME AND READ "ELECTRIC" (1)
- MINIMUM 6" DRAIN WITH COVER - NEENAH R-4937 OR APPROVED EQUAL (1)
- LIFTING ANCHORS (4)
- 1-1/2" Ø GROUND ROD HOLE (1)



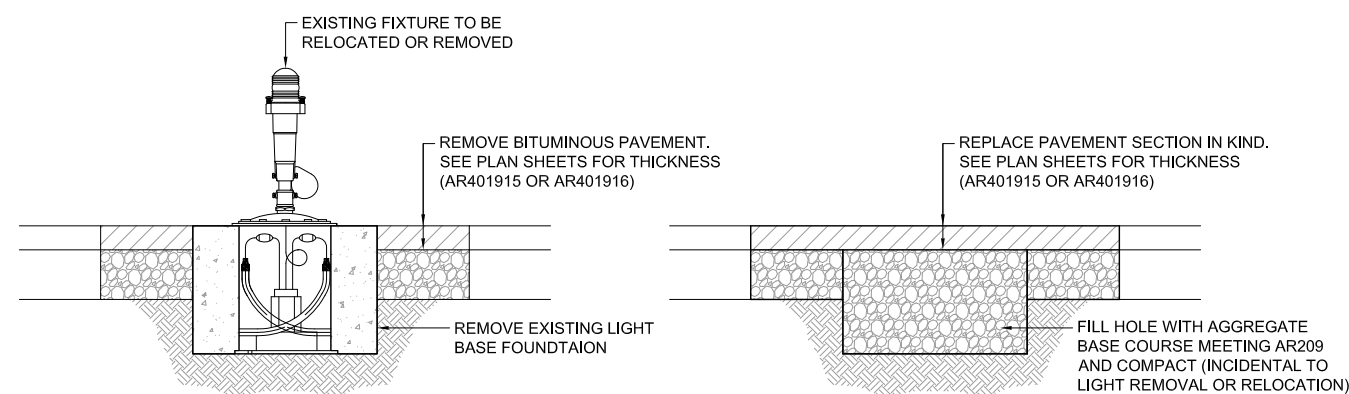
AIRFIELD ELECTRICAL HANDHOLE

NOT TO SCALE

PLAN VIEW

NOTES:

- SEE PLAN SHEETS FOR THICKNESS OF BITUMINOUS PAVEMENT TO BE REMOVED AND REPLACED.
- PAVEMENT REMOVED IN EXCESS OF THE AREA SHOWN WITHOUT PRIOR APPROVAL OF THE RESIDENT ENGINEER SHALL NOT BE CONSIDERED FOR PAYMENT, AND THE PAVEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
- WHEN A NEW FOUNDATION IS TO BE INSTALLED AT THE SAME LOCATION AS THE REMOVAL, THE NEW FOUNDATION SHALL BE INSTALLED PRIOR TO PAVEMENT REPLACEMENT.
- LIGHT BASE REMOVAL TO BE PAID FOR UNDER AR125902 WHEN THE FIXTURE AND FOUNDATION ARE TO BE REMOVED. SEE PLAN SHEETS FOR SPECIFIC LOCATIONS.
- LIGHT BASE REMOVAL TO BE PAID FOR UNDER AR125962, RELOCATE BASE MOUNTED LIGHT, OR AR125963, RELOCATE INPAVEMENT LIGHT WHEN THE FIXTURE IS TO BE REUSED. SEE PLAN SHEETS FOR SPECIFIC LOCATIONS.
- LIGHT BASE REMOVAL FOR LIGHTS TO BE RELOCATED SHALL BE CONSIDERED INCIDENTAL TO THE RELOCATED LIGHT PAY ITEM.
- WHEN NO NEW FOUNDATION OR FIXTURE WILL BE INSTALLED AT THE SAME LOCATION AS THE REMOVAL, THE HOLE SHALL BE FILLED WITH AGGREGATE BASE COURSE (AR209) AND COMPACTED TO 95% MODIFIED PROCTOR (ASTM D-1557) IN LIFTS NOT EXCEEDING 8-INCH LOOSE MEASURE.
- WHEN A NEW OR RELOCATED FIXTURE WILL BE INSTALLED AT THE SAME LOCATION AS THE REMOVAL, THE INSTALLATION SHALL BE COMPLETED IN ACCORDANCE WITH THE DETAILS AS SHOWN FOR A NEW OR RELOCATED BASE MOUNTED OR IN-PAVEMENT LIGHT. SEE PLAN FOR INSTALLATION TYPE AND LOCATIONS.



SECTION A-A

LIGHT BASE REMOVAL IN  
BITUMINOUS PAVEMENT TO REMAIN

NOT TO SCALE

ELECTRICAL DETAILS - 6  
(ELD6)

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

SHEET TITLE:  
**ELECTRICAL DETAILS - 6  
(ELD6)**

DESIGNED: CMT	DRAWN: CMT	CHECKED: DLP
------------------	---------------	-----------------

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. <b>ELD6</b>	REVISION
--------------------------	----------

SHEET 58 OF 70 SHEETS





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION  
PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
MARKING DETAILS - 1

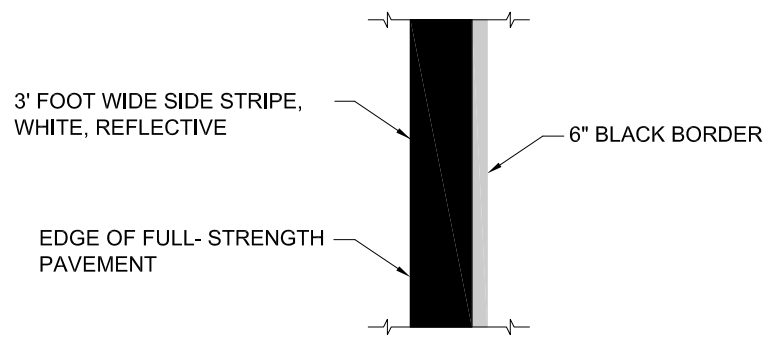
DESIGNED: CMT DRAWN: CMT CHECKED: DLP

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

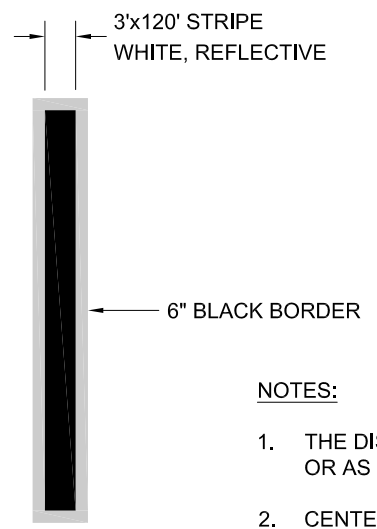
SHEET NO. MRD1 REVISION

SHEET 59 OF 70 SHEETS

RO020

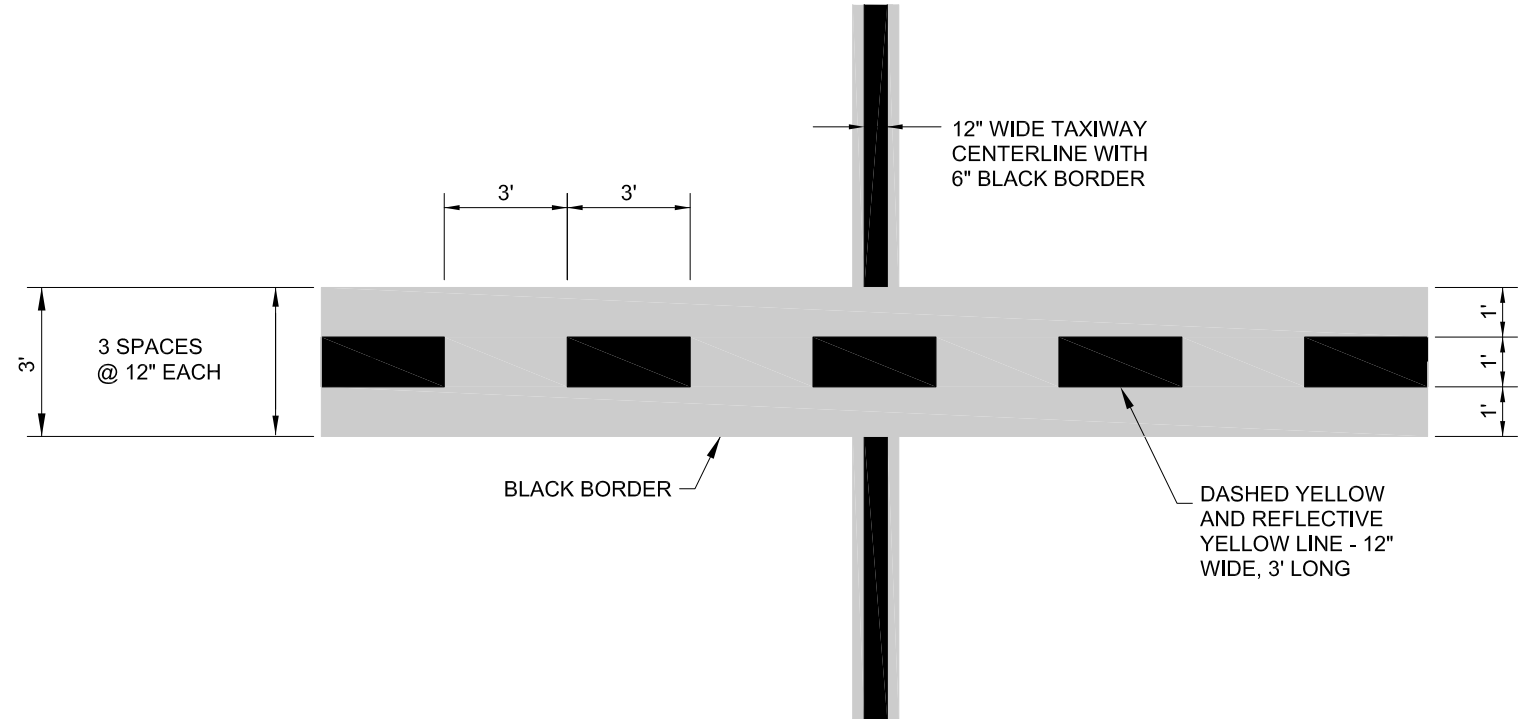


**RUNWAY EDGE MARKINGS,  
CONTINUOUS**  
NO SCALE



**RUNWAY CENTERLINE,  
DASHED**  
NO SCALE

- NOTES:
1. THE DISTANCE BETWEEN STRIPES SHALL BE 80'  
OR AS NOTED ON THE PLAN DRAWINGS.
  2. CENTERLINE STRIPES ARE CENTERED ABOUT  
THE RUNWAY CENTERLINE.



**INTERMEDIATE HOLD POSITION MARKING**  
NO SCALE

**MARKING DETAILS - 1  
(MRD1)**



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

MARKING DETAILS - 2

DESIGNED: CMT DRAWN: CMT CHECKED: DLP

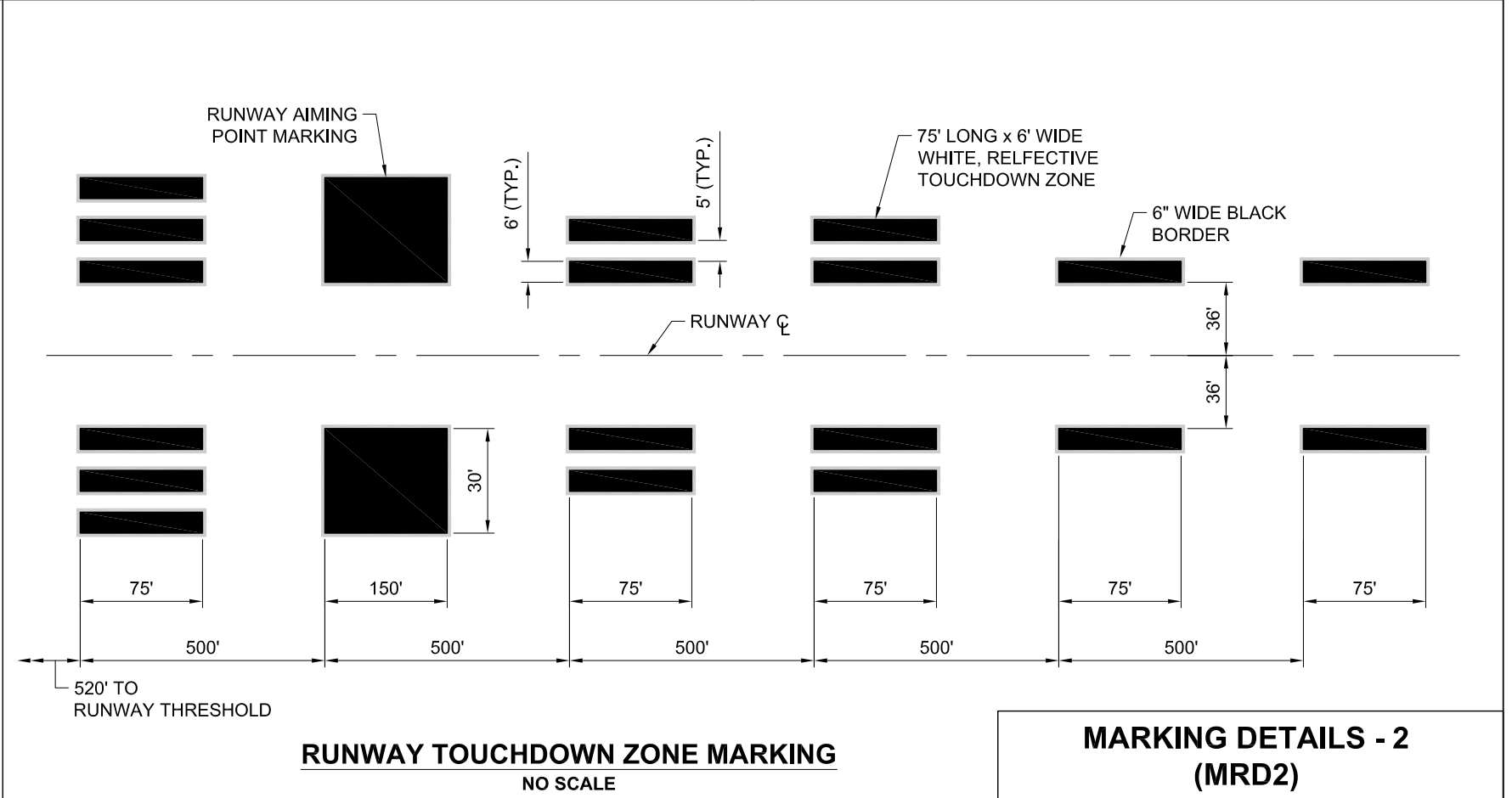
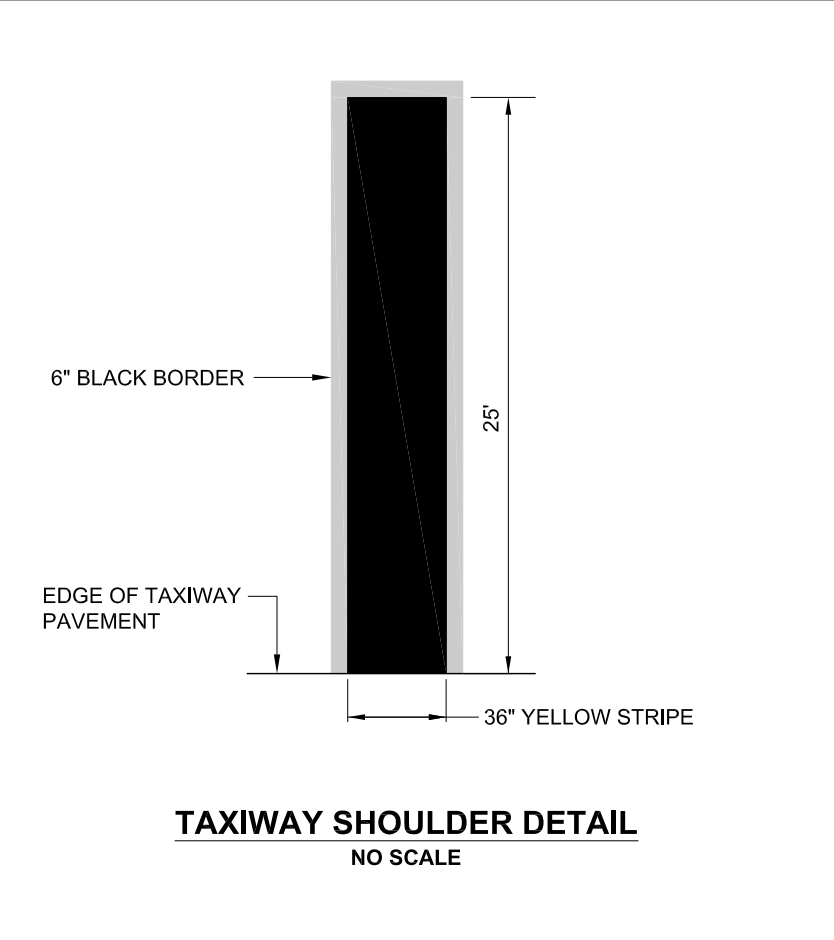
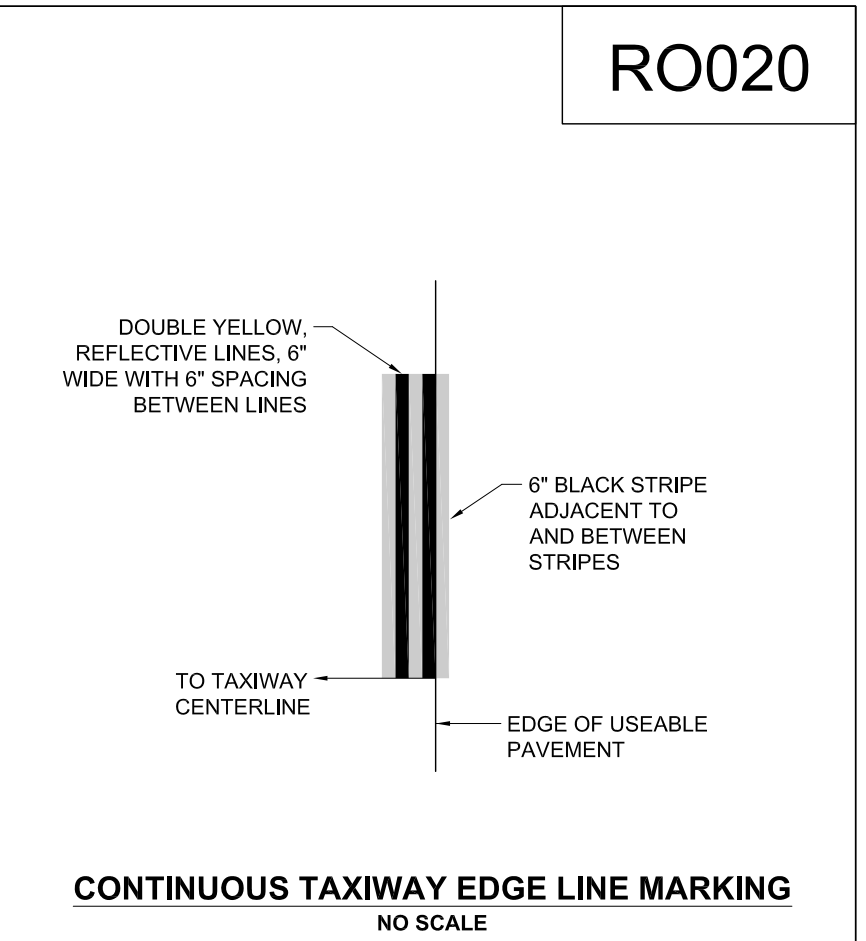
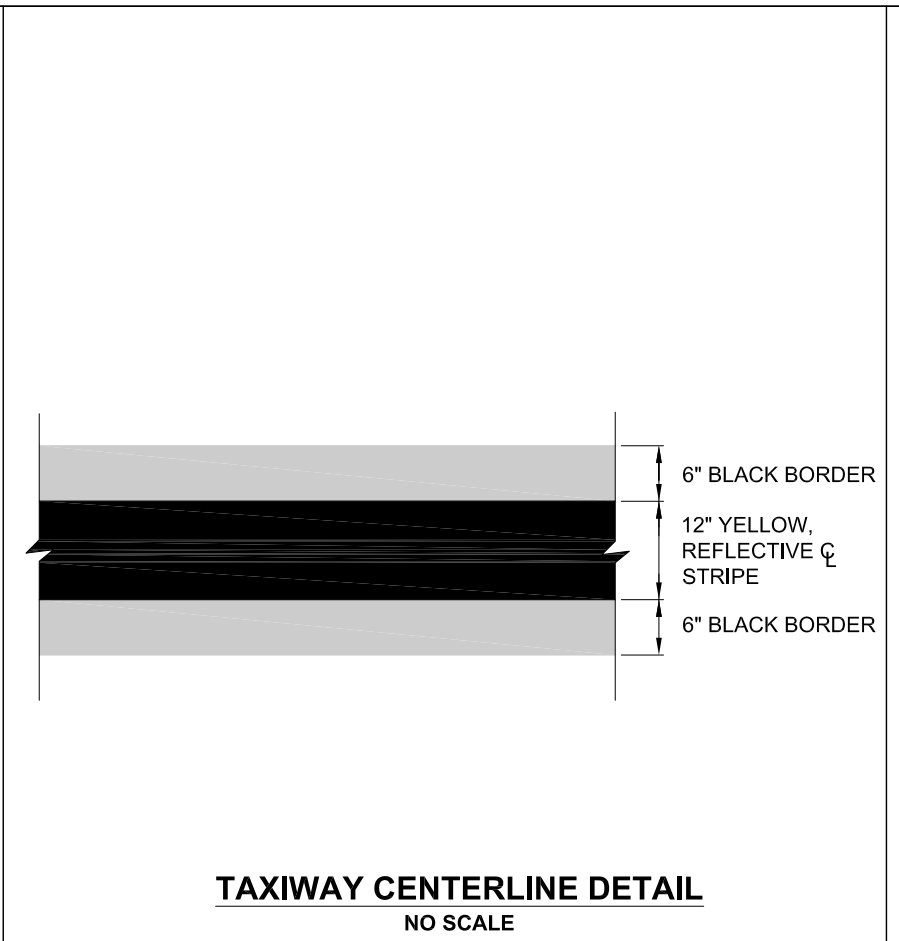
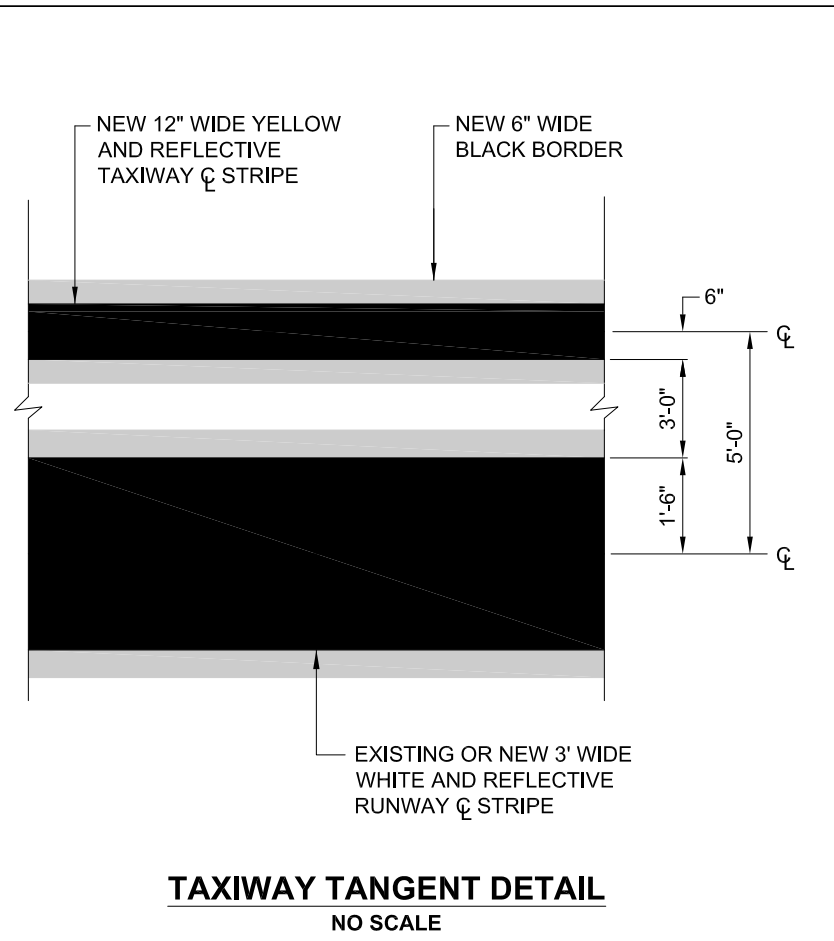
PROJECT NO.: 11258-03-00/06

DATE: APRIL 20, 2012

SHEET NO. MRD2 REVISION

SHEET 60 OF 70 SHEETS

RO020



MARKING DETAILS - 2  
(MRD2)



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN		
BY:		
REV	DATE	DESCRIPTION
PROJECT NAME:		

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
MARKING DETAILS - 3

DESIGNED: CMT	DRAWN: CMT	CHECKED: DLP
------------------	---------------	-----------------

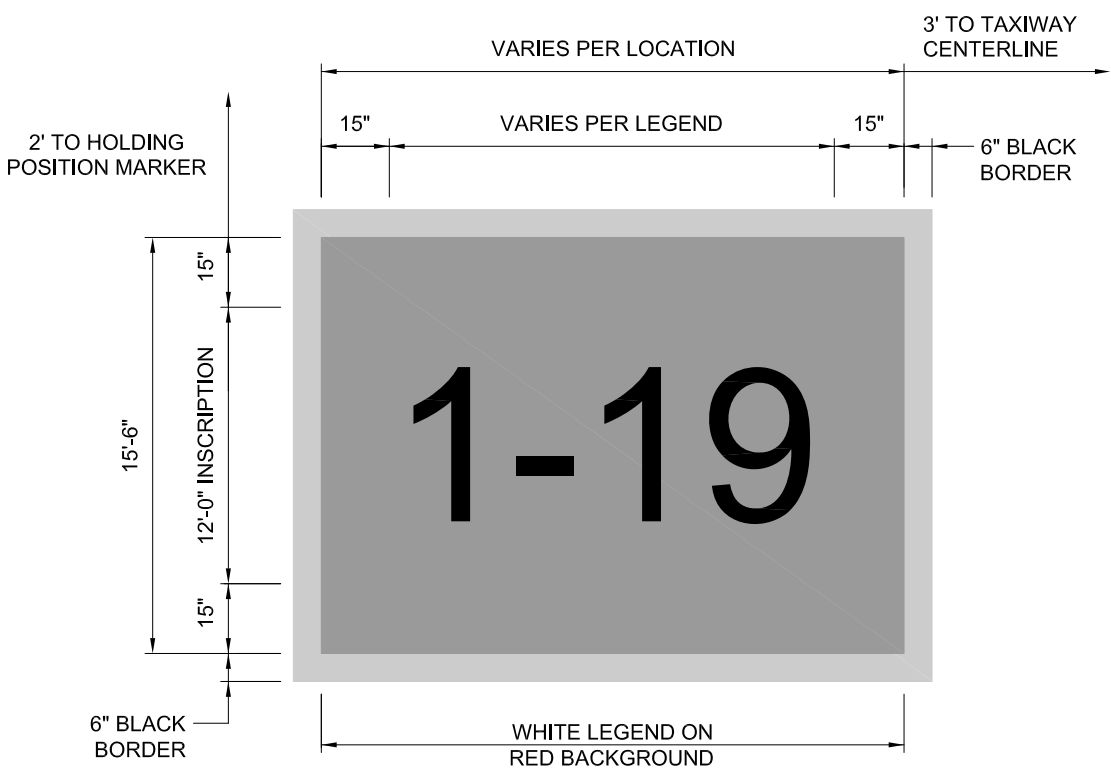
PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. <b>MRD3</b>	REVISION
--------------------------	----------

SHEET 61 OF 70 SHEETS

RO020

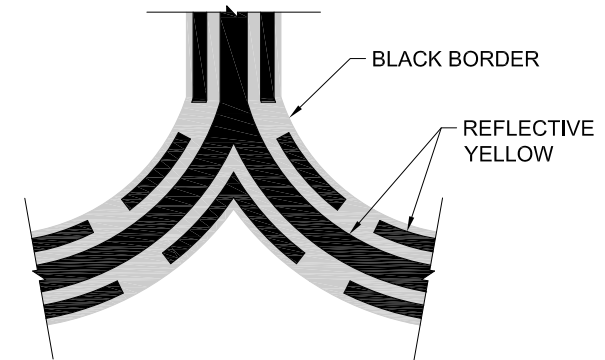
\* THE LAST DASH SHALL BE 6' LONG.



**SURFACE PAINTED RUNWAY HOLDING POSITION SIGN MARKING**  
NO SCALE

NOTES:

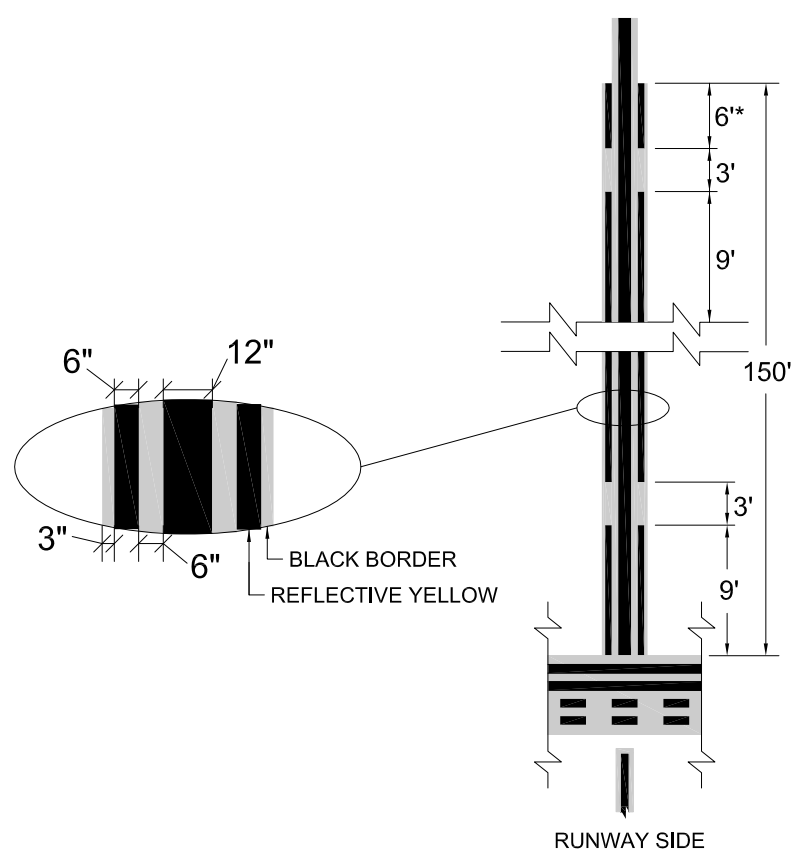
1. ALL SURFACE PAINTED RUNWAY HOLDING POSITION SIGNS SHALL HAVE A RED BACKGROUND WITH A WHITE INSCRIPTION.
2. ALL SURFACE PAINTED SIGNS SHALL BE OUTLINED WITH A 6" BLACK BORDER.
3. LEGENDS SHALL BE AS SHOWN ON THE PAVEMENT MARKING PLAN SHEETS.
4. ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FIGURES A-1 THROUGH A-5 IN FAA ADVISORY CIRCULAR 150/5340-1K (LATEST EDITION), APPENDIX 1.



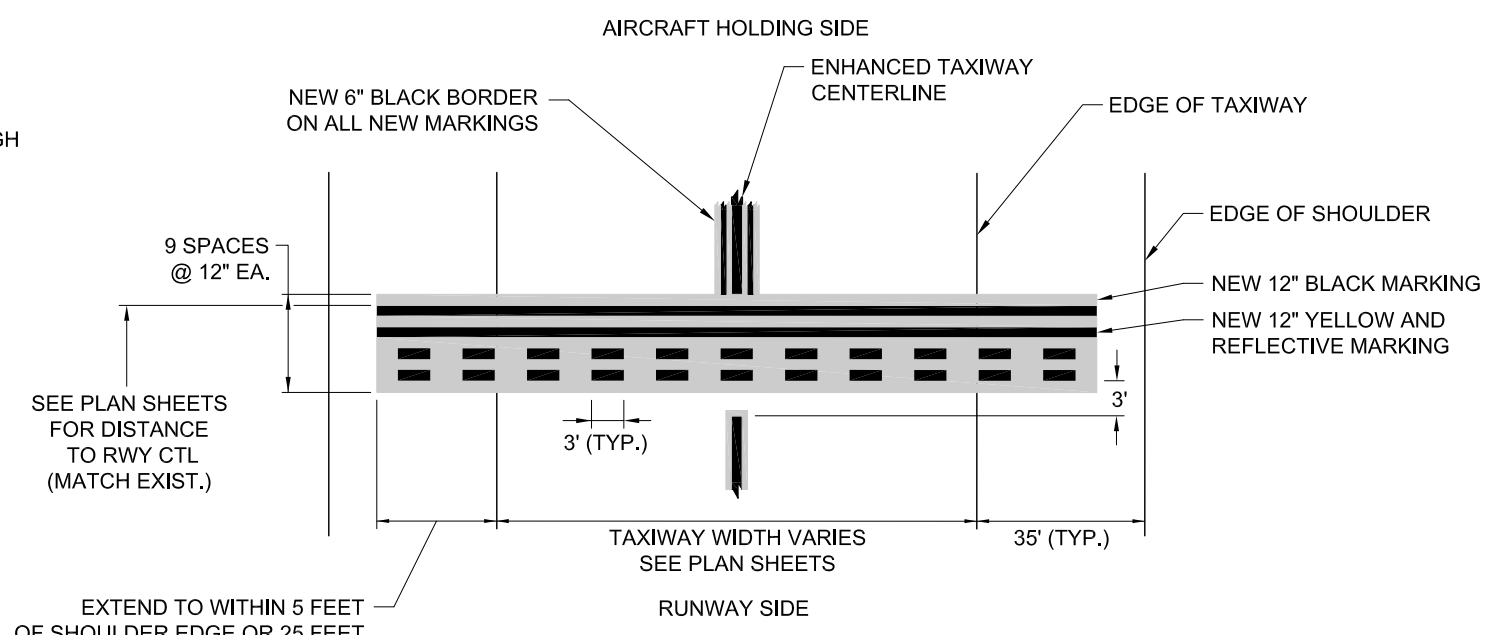
**ENHANCED TAXIWAY CENTERLINE - CONVERGENCE SECTION**  
NO SCALE

NOTE:

THERE MUST BE NO PARTIAL DASHED LINES LESS THAN 9- FEET AT THE POINT OF CONVERGENCE. THE FIRST INSIDE DASHED LINES MUST BE ALIGNED WITH THE OUTSIDE DASHED LINES - STARTING AND STOPPING WITH THE DASHED LINES ON THE OUTSIDE, AS SHOWN.



**ENHANCED TAXIWAY CENTERLINE MARKING**  
NO SCALE



**ENHANCED RUNWAY HOLDING POSITION MARKING**  
NO SCALE

**MARKING DETAILS - 3 (MRD3)**





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

SITE PLAN  
AND CONTROL

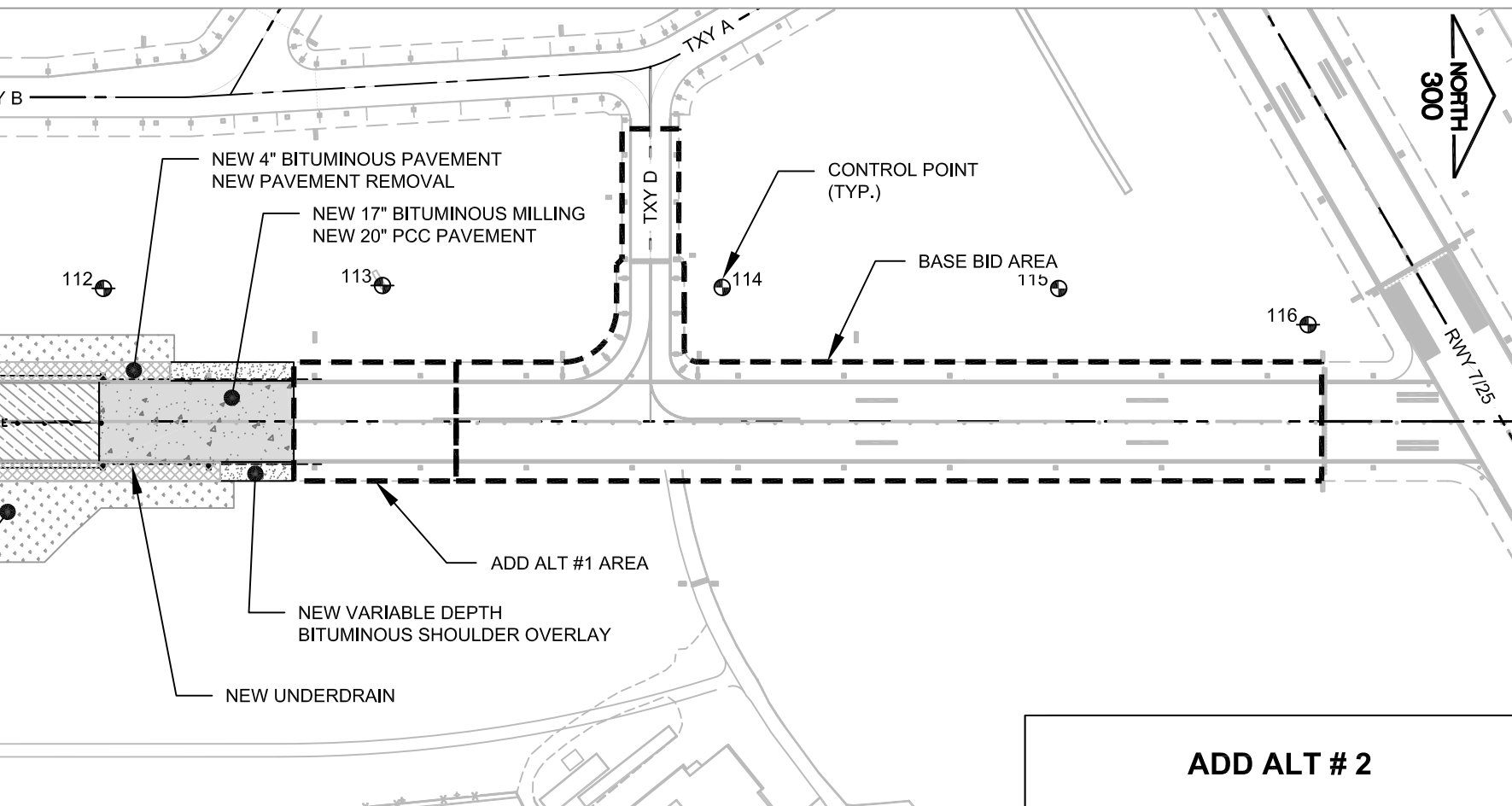
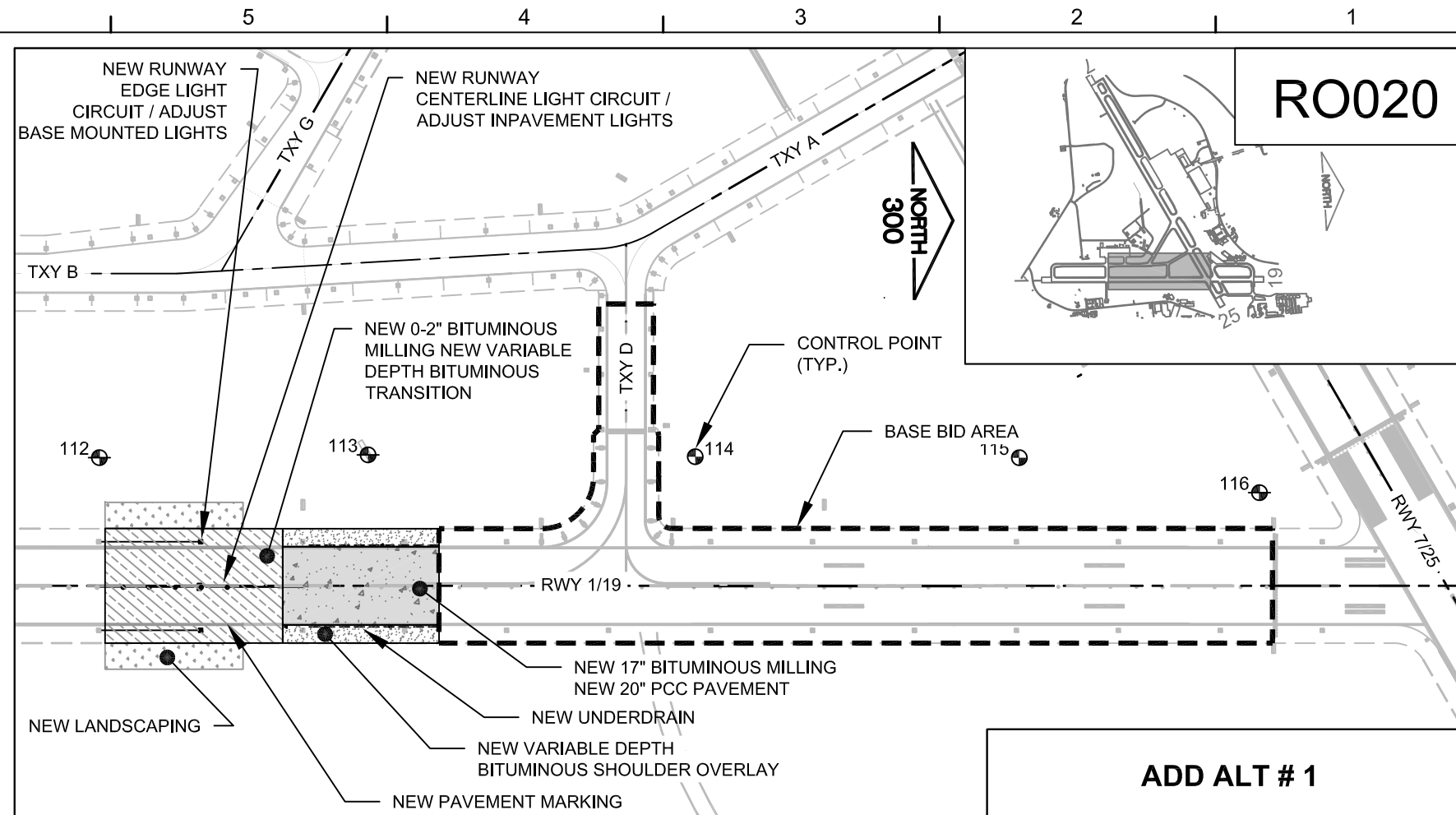
DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO.    REVISION

ASP1

SHEET 62 OF 70 SHEETS

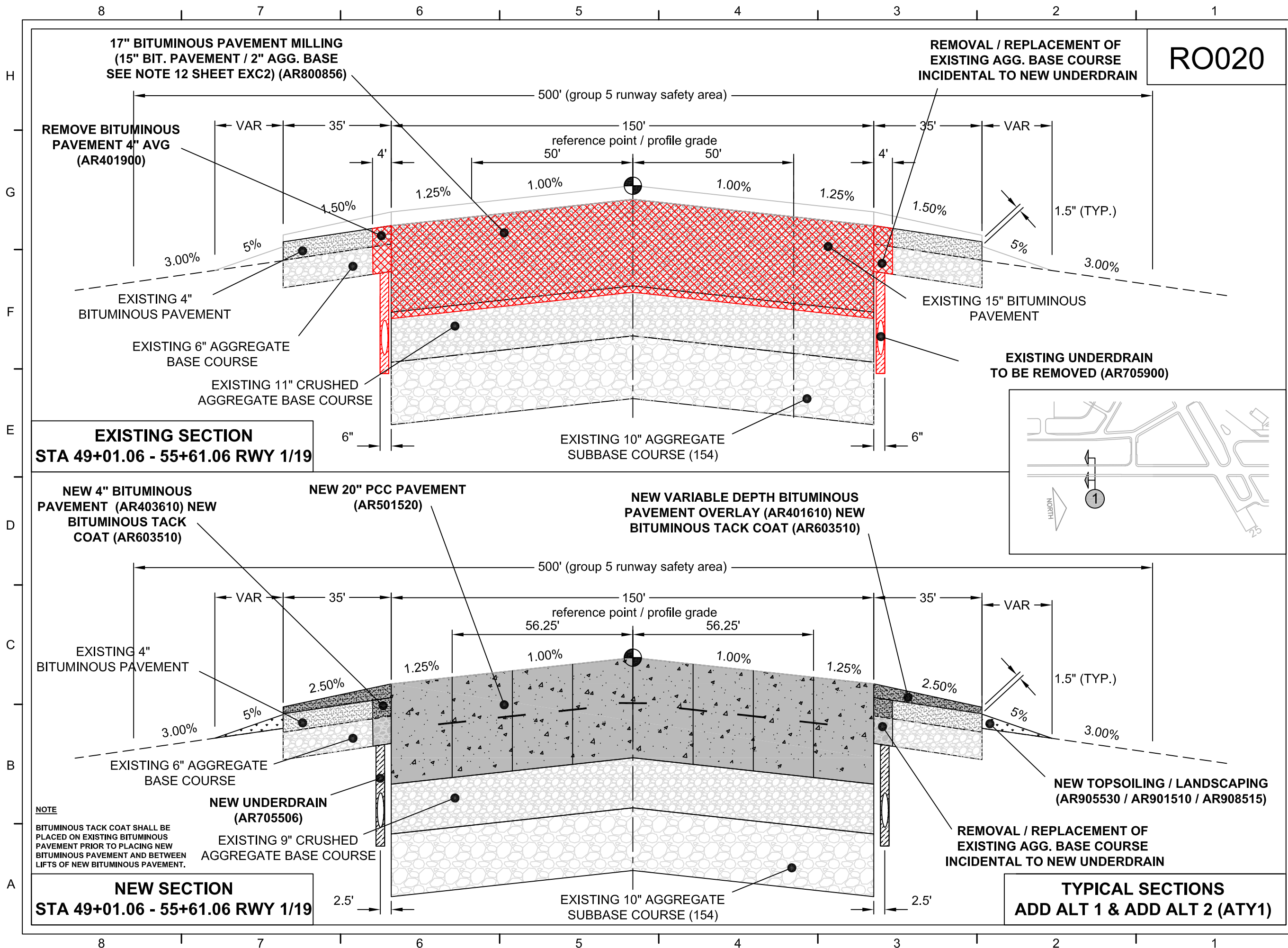


**LEGEND**

- NEW 20" PCC PAVEMENT INLAY AND 17" BITUMINOUS PAVEMENT MILLING REMOVAL
- NEW 4" BITUMINOUS PAVEMENT AND NEW 4" BITUMINOUS PAVEMENT REMOVAL
- NEW VARIABLE DEPTH BITUMINOUS SHOULDER OVERLAY
- NEW VARIABLE DEPTH BITUMINOUS TRANSITION AND 0-2" BITUMINOUS PAVEMENT MILLING
- NEW LANDSCAPING (TOPSOILING / SEEDING / MULCHING)

HORIZONTAL AND VERTICAL CONTROL

POINT NO.	STATION / OFFSET	NORTHING	EASTING	ELEVATION	DESCRIPTION
36	43+45.50 / 245.27' LT	2014374.51	2588014.19	725.90	IRON PIN
112	49+09.12 / 245.95' LT	2014936.28	2588059.78	730.45	IRON PIN
113	54+24.89 / 251.27' LT	2015450.75	2588096.82	729.09	IRON PIPE
114	60+52.91 / 247.75' LT	2016076.36	2588151.89	729.26	IRON PIN
115	66+75.05 / 245.78' LT	2016696.24	2588204.92	729.63	IRON PIN
116	71+36.03 / 178.73' LT	2017150.16	2588309.59	730.96	IRON PIN



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
TYPICAL SECTION  
RWY 1/19 ADD ALT 1 &  
ADD ALT 2

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **ATY1** REVISION





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

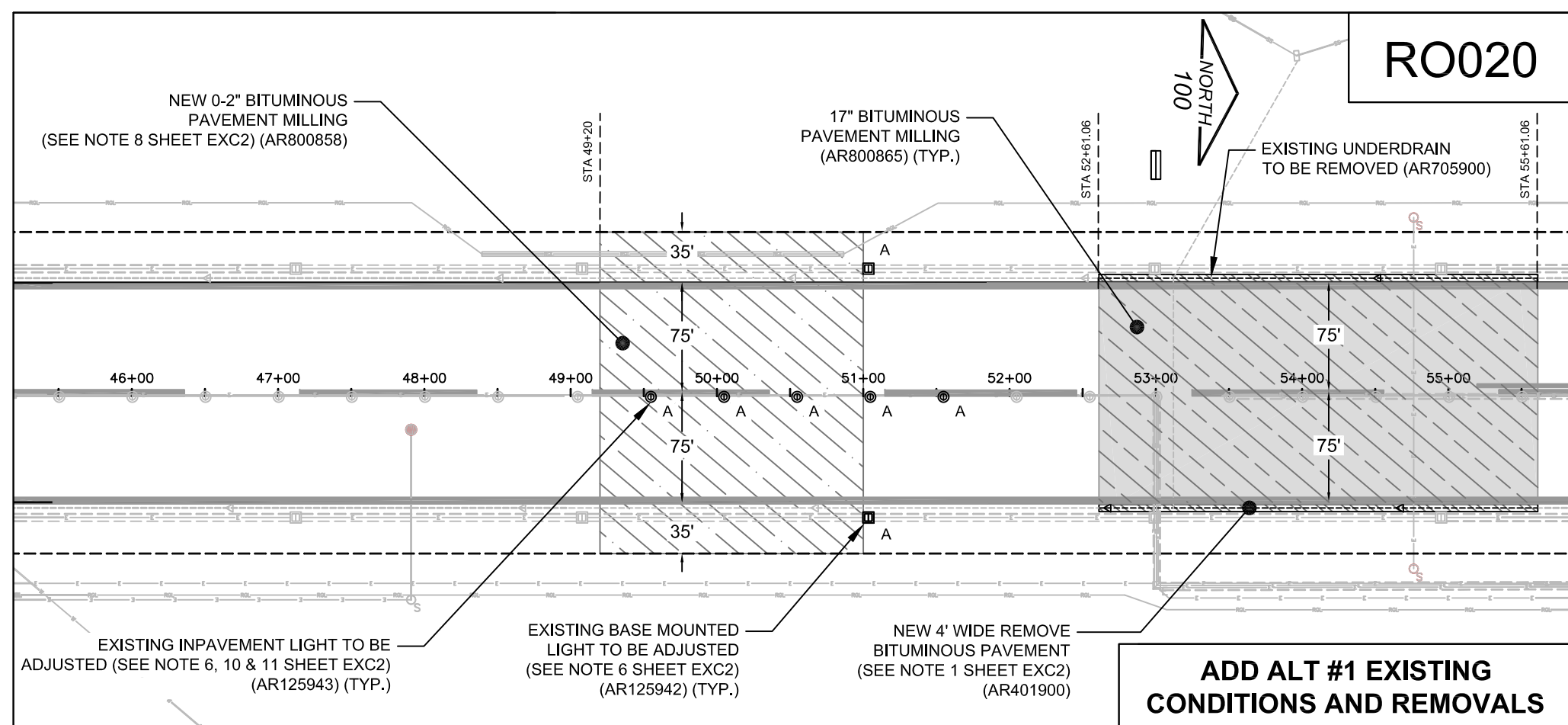
SHEET TITLE:  
**ADD ALT #2 EXISTING  
CONDITIONS AND REMOVALS**

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

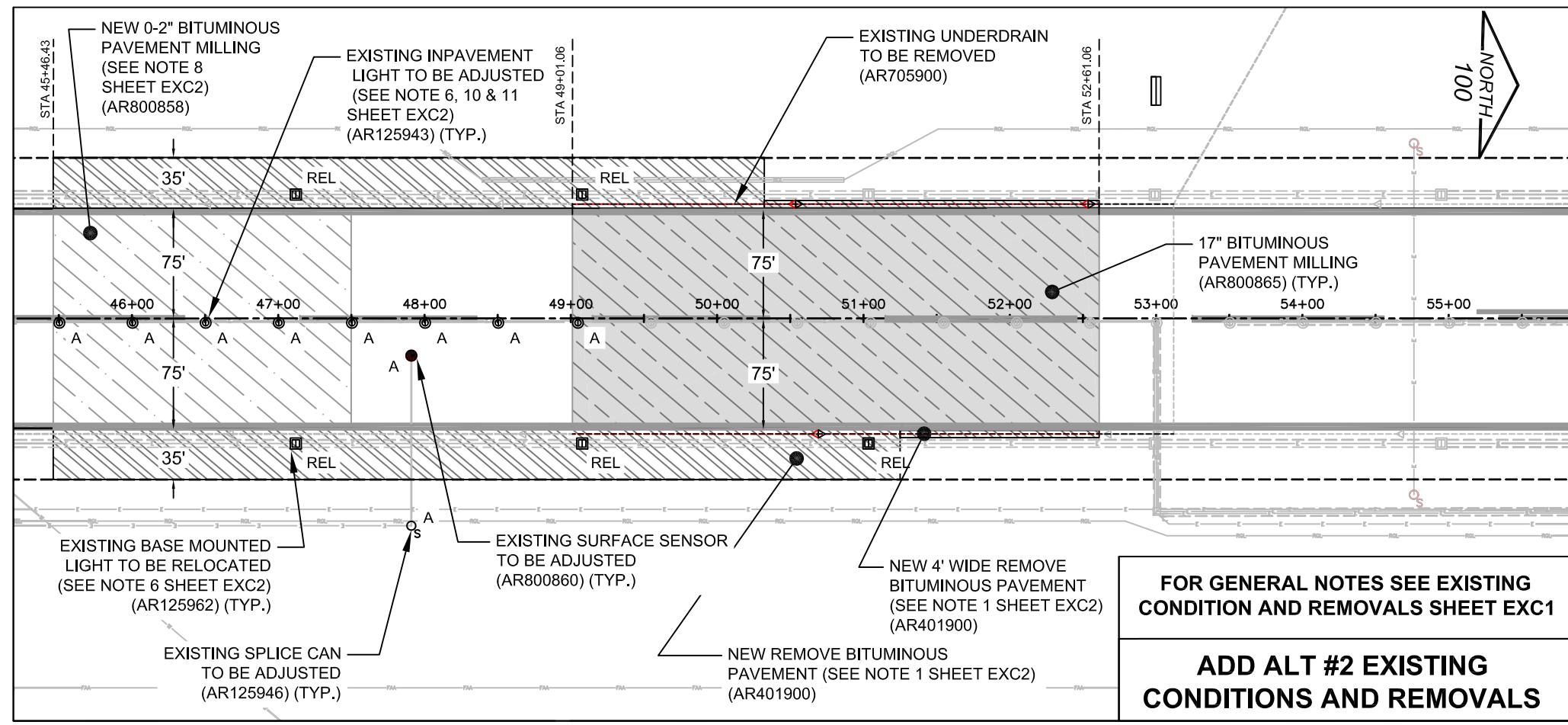
PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **AEX1** REVISION

SHEET 64 OF 70 SHEETS



**ADD ALT #1 EXISTING  
CONDITIONS AND REMOVALS**

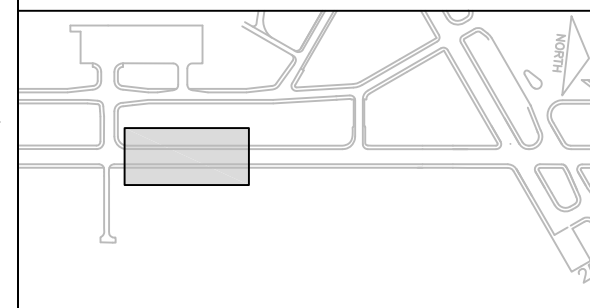


**FOR GENERAL NOTES SEE EXISTING  
CONDITION AND REMOVALS SHEET EXC1**

**ADD ALT #2 EXISTING  
CONDITIONS AND REMOVALS**

**LEGEND**

- NEW 17" BIT. PAVEMENT MILLING (AVG.)
- NEW 0-2" BITUMINOUS PAVEMENT MILLING (AVG)
- NEW 2" BIT. PAVEMENT MILLING (AVG.)
- REMOVE AND REPLACE BIT. PAVEMENT TYPE A (4" AVG)
- REMOVE AND REPLACE BIT. PAVEMENT TYPE B (10-12" AVG)
- REMOVE BITUMINOUS PAVEMENT
- EXISTING SANITARY LINE
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING WATERMAIN
- EXISTING DUCT
- EXISTING RWY GUIDANCE LIGHT CIRCUIT
- EXISTING AIRFIELD CIRCUIT
- EXISTING SURFACE SENSOR CIRCUIT
- EXISTING MITL, BASE MOUNTED
- EXISTING HIRL, BASE MOUNTED
- EXISTING ELECTRICAL HANDHOLE
- EXISTING RGL
- EXISTING TAXIWAY GUIDANCE SIGN
- EXISTING DRAINAGE INLET
- EXISTING MANHOLE
- EXISTING ITEM TO BE ADJUSTED
- EXISTING ITEM TO BE RELOCATED
- EXISTING ITEM TO BE REMOVED
- EXISTING PAVEMENT SENSOR
- EXISTING IN-PAVEMENT LIGHT
- EXISTING VASI
- EXISTING SPLICE CAN



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

8 7 6 5 4 3 2 1

H  
G  
F  
E  
D  
C  
B  
A



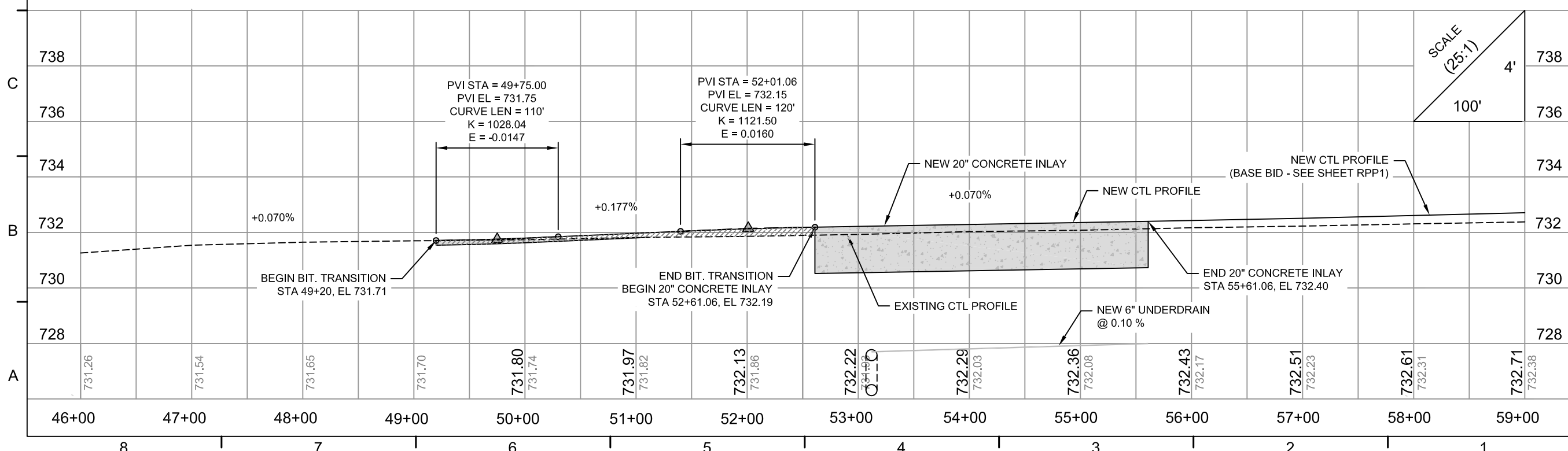
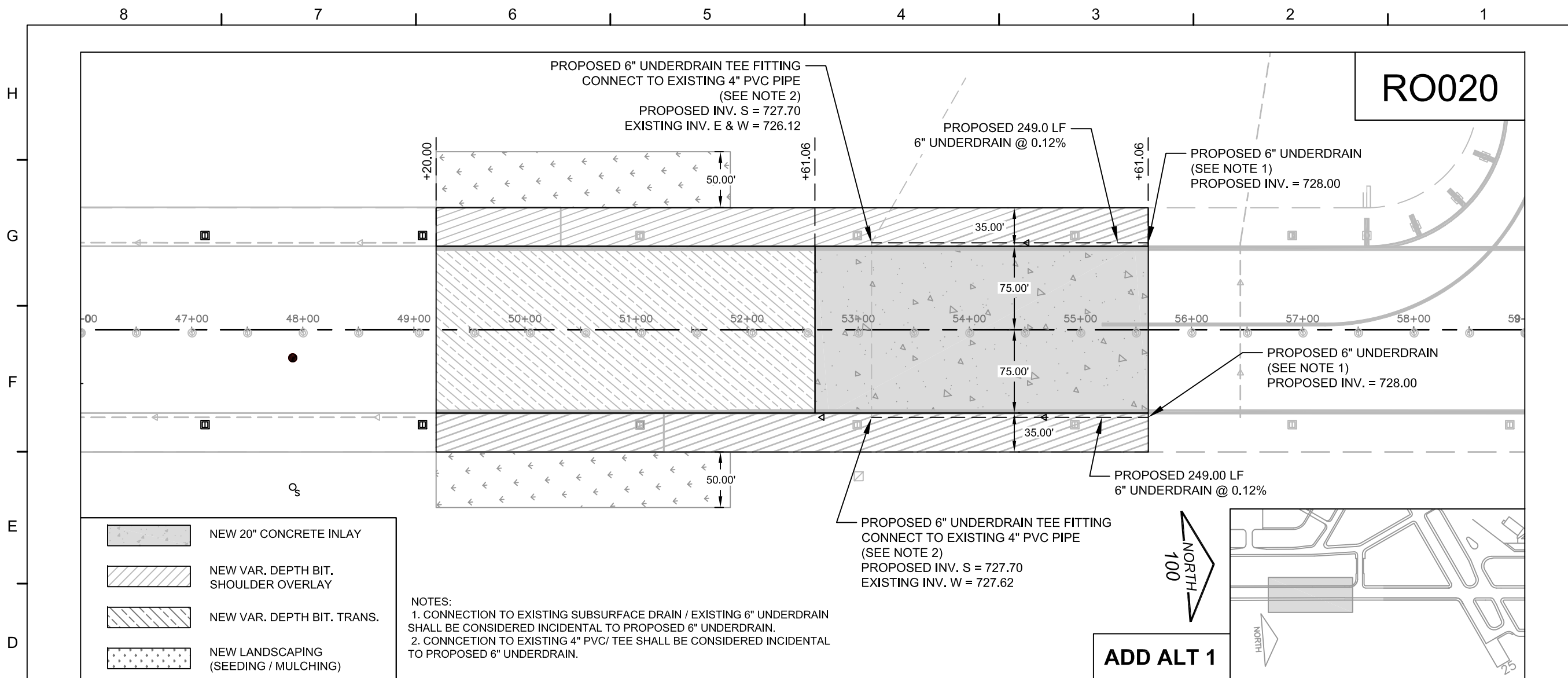
MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183



APPROVED AS WORKING PLAN BY:		
REV	DATE	DESCRIPTION

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
**RUNWAY 1/19  
PLAN/PROFILE  
ADD ALT 1**

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. <b>EXC5</b>	REVISION
SHEET 65 OF 70 SHEETS	



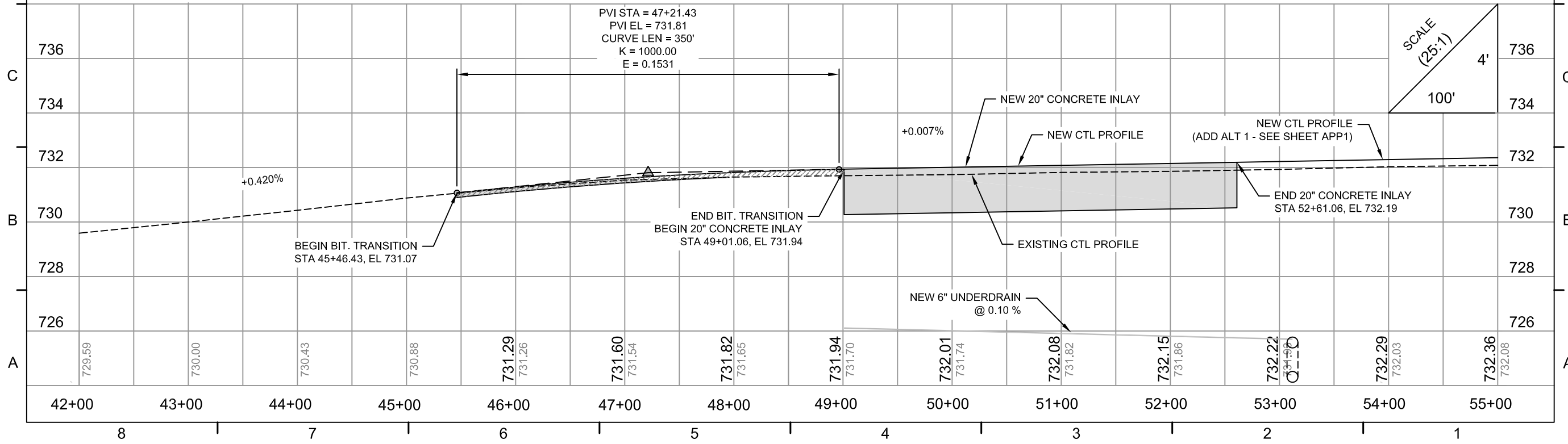
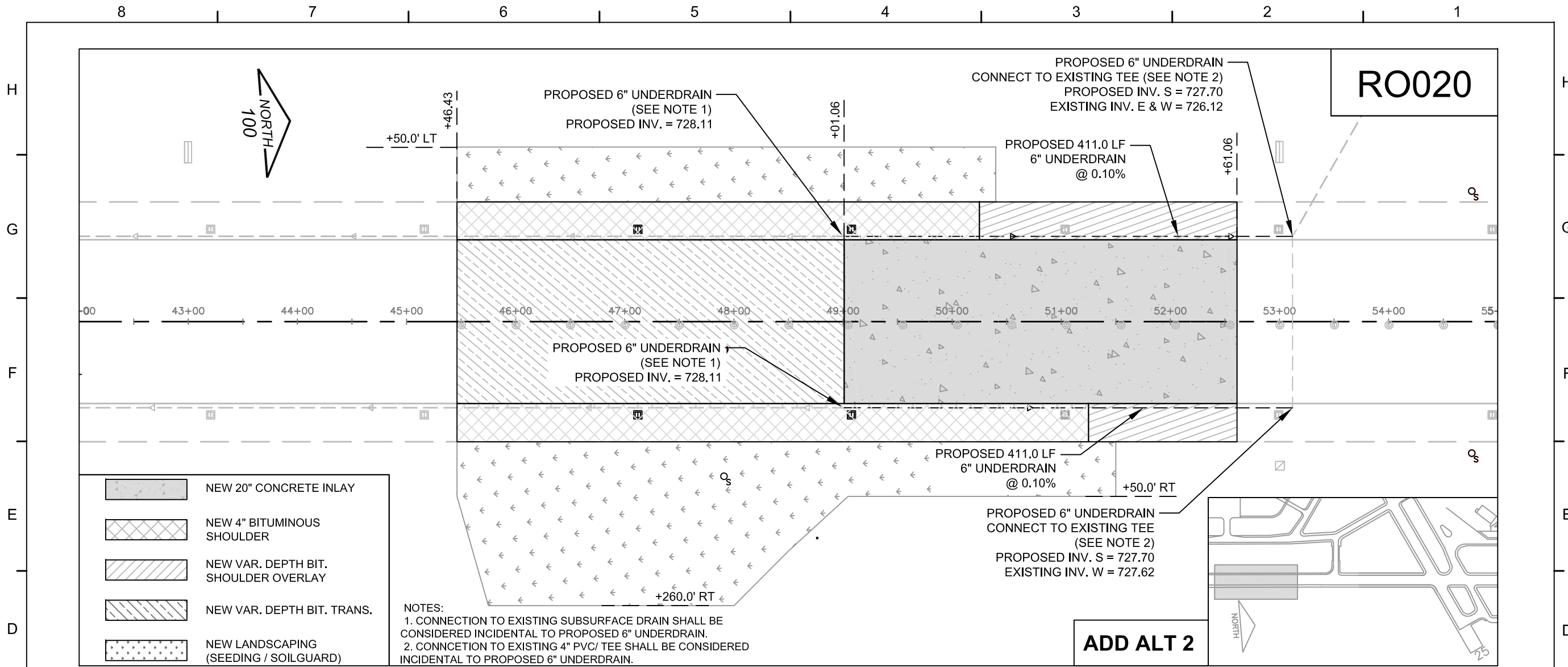
MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183



APPROVED AS WORKING PLAN BY: [Signature Box]

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:  
RUNWAY 1/19  
PLAN/PROFILE  
ADD ALT 2

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **APP2** REVISION



MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:  
**REHABILITATE  
RUNWAY 1/19  
PHASE 2**

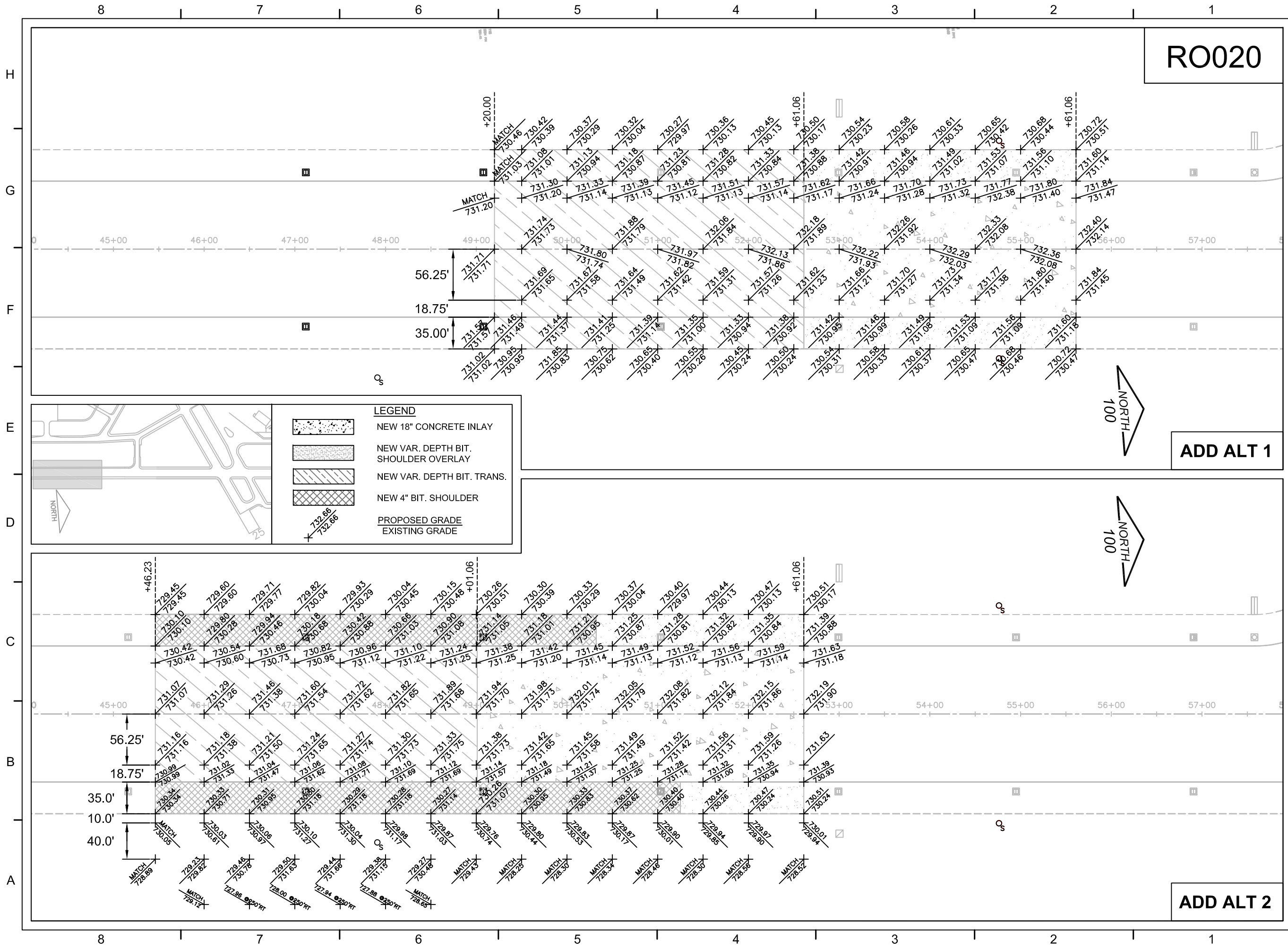
SHEET TITLE:  
**RUNWAY 1/19 GRADING  
PLAN ADD ALT 1 &  
ADD ALT 2**

DESIGNED: CMT-RFD      DRAWN: CMT-RFD      CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **AGP1**      REVISION

SHEET 67 OF 70 SHEETS



RO020

ADD ALT 1

ADD ALT 2





MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-XXXX

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

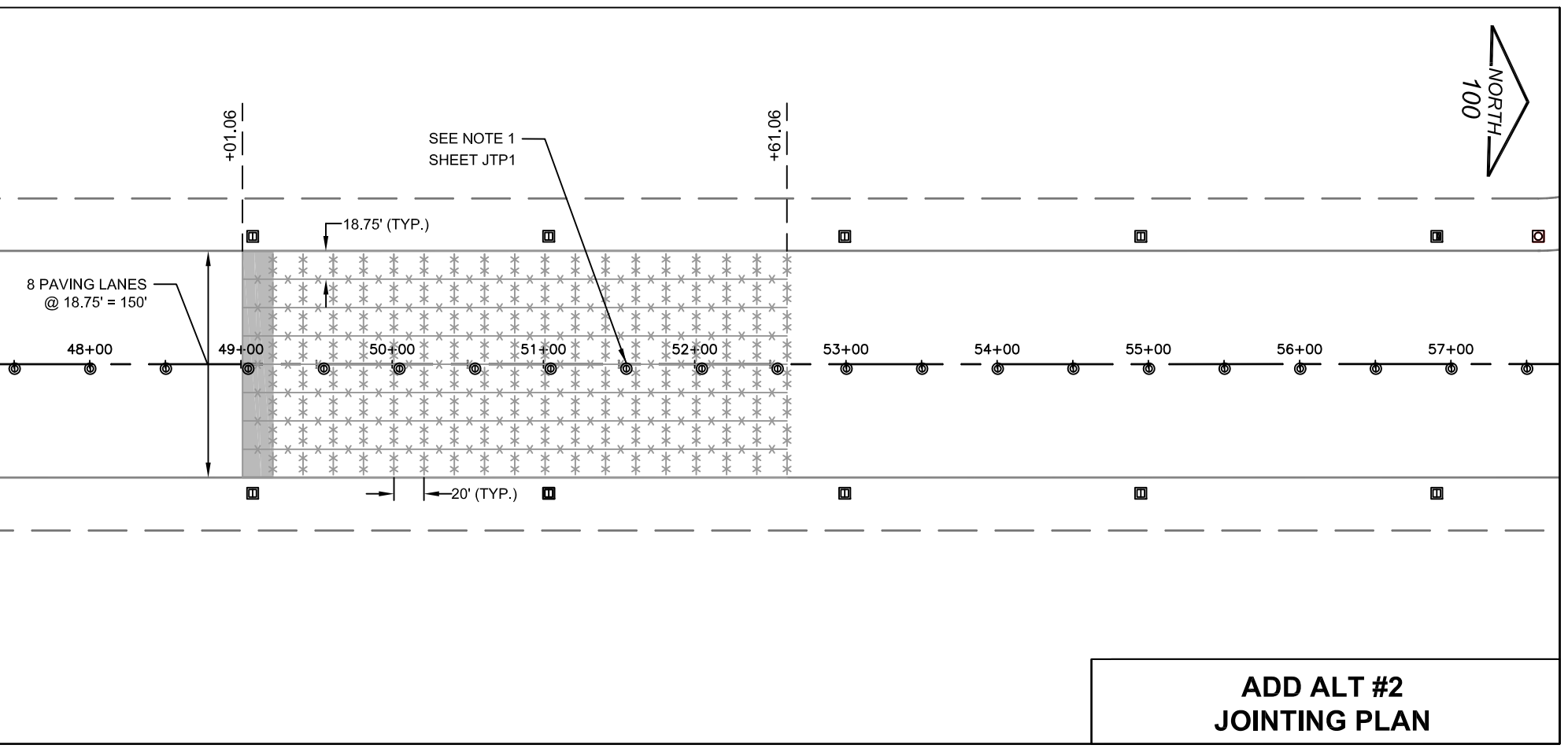
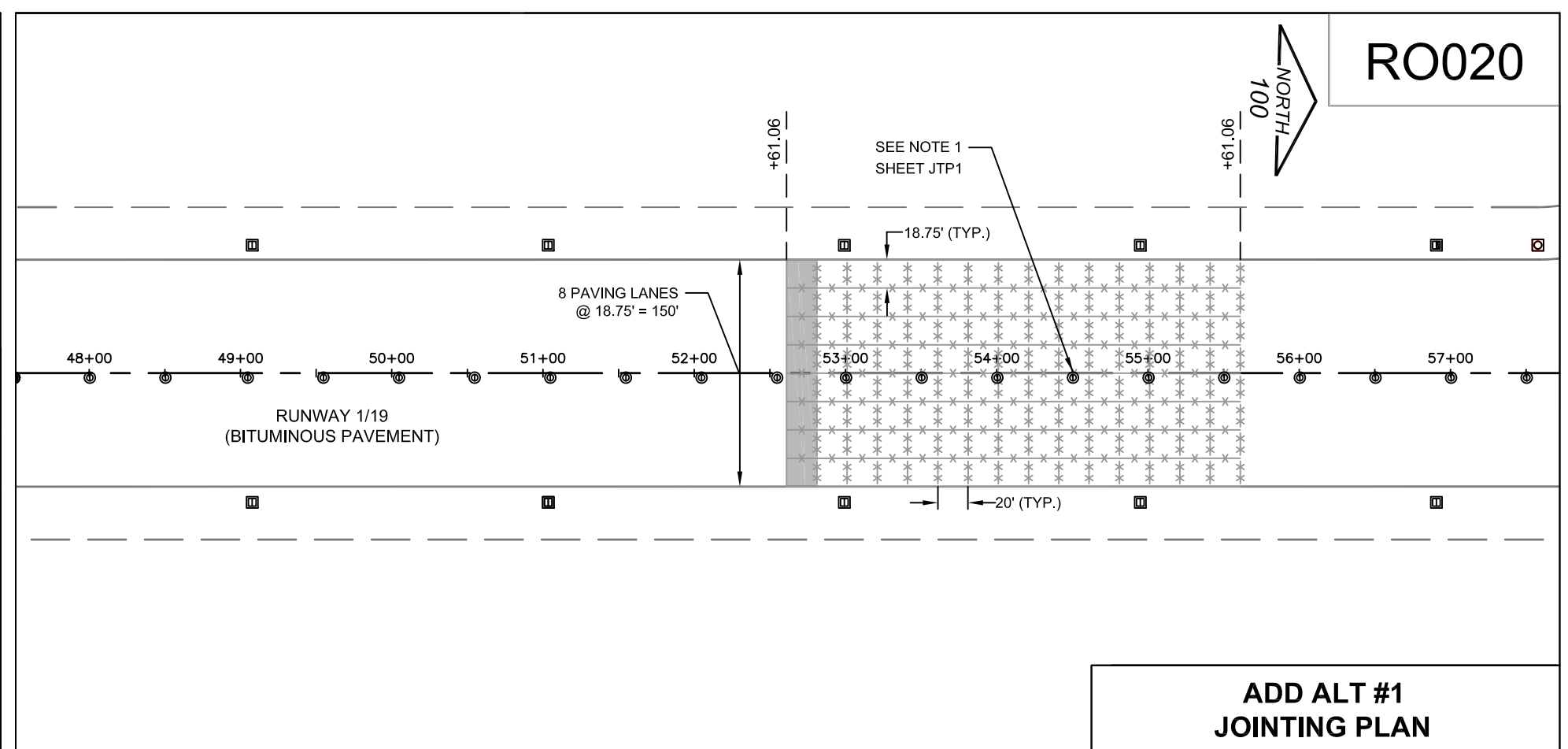
SHEET TITLE:  
**ADD ALT  
JOINTING PLAN**

DESIGNED: CMT-RFD    DRAWN: CMT-RFD    CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **AJT1**    REVISION

SHEET 68 OF 70 SHEETS

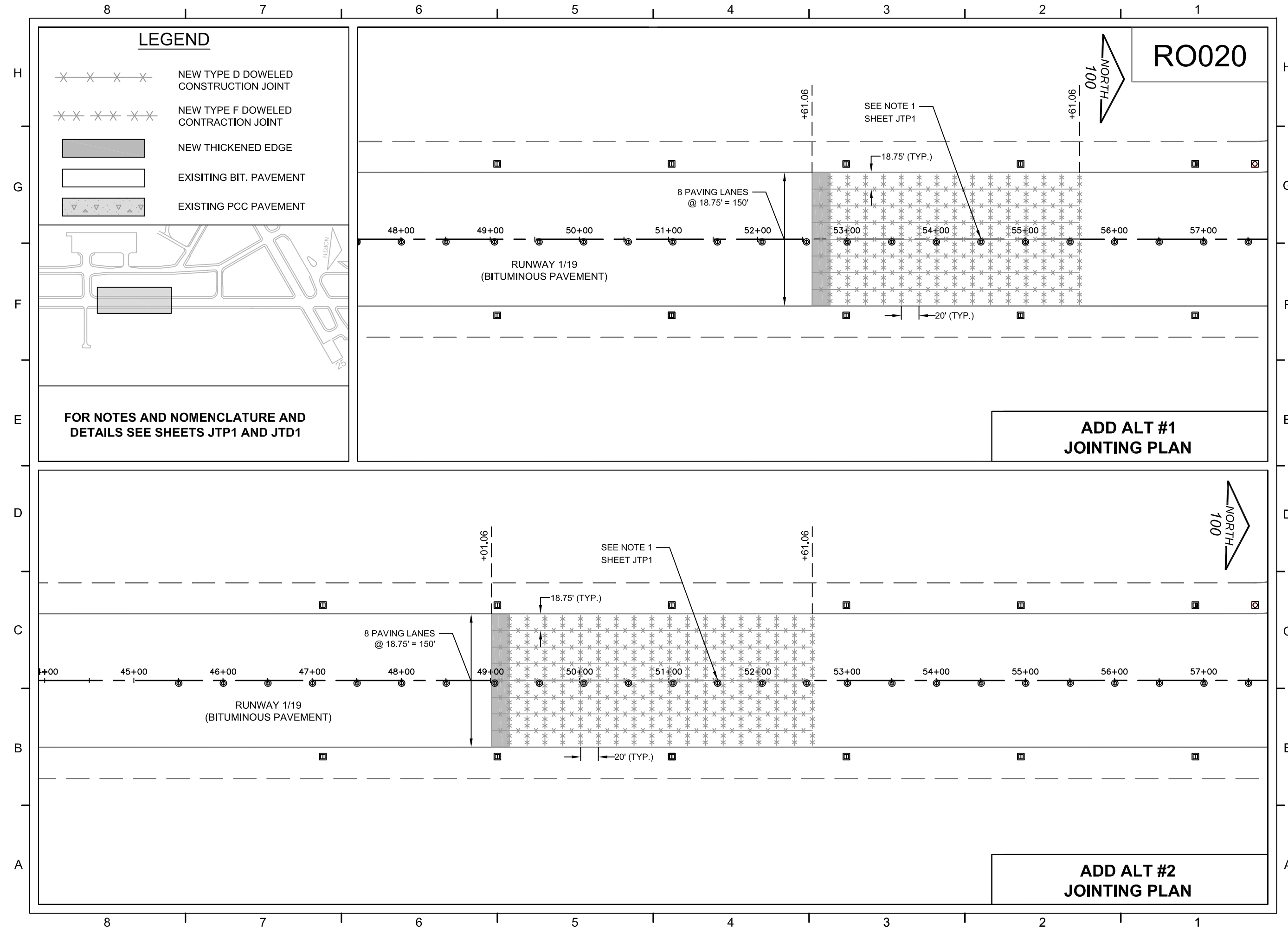


**LEGEND**

- NEW TYPE D DOWELED CONSTRUCTION JOINT
- NEW TYPE F DOWELED CONSTRUCTION JOINT
- NEW THICKENED EDGE
- EXISTING BIT. PAVEMENT
- EXISTING PCC PAVEMENT

**FOR NOTES AND NOMENCLATURE AND DETAILS SEE SHEETS JTP1 AND JTD1**

**FOR NOTES AND NOMENCLATURE AND DETAILS SEE SHEETS JTP1 AND JTD1**







MIKE DUNN  
DIRECTOR



Chicago Rockford  
International Airport  
Rockford, Illinois

Greater Rockford  
Airport Authority

AIP Proj: 3-17-0088-XX  
IL Proj: RFD-4183

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE  
RUNWAY 1/19  
PHASE 2

SHEET TITLE:

GROOVING PLAN AND  
DETAILS

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 11258-03-00/06  
DATE: APRIL 20, 2012

SHEET NO. **AGP1** REVISION

SHEET 70 OF 70 SHEETS

