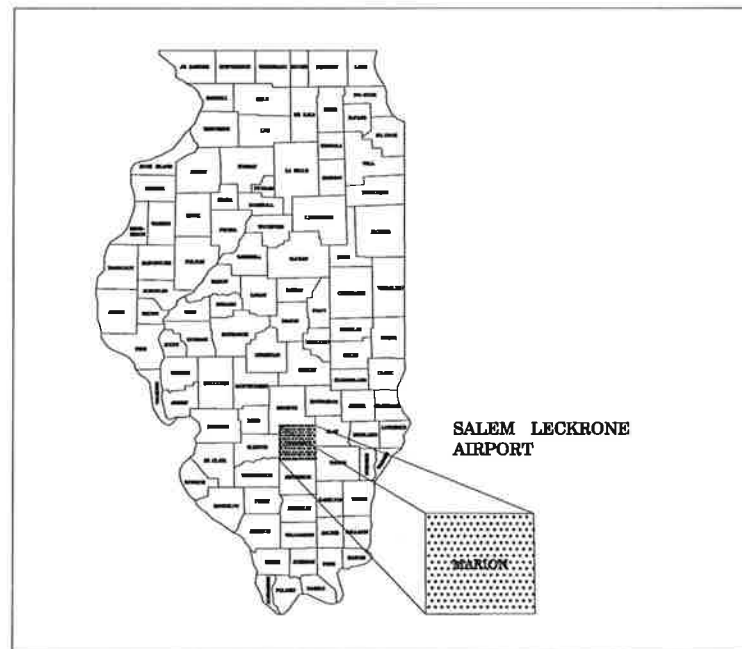


CONSTRUCTION PLANS FOR SALEM LECKRONE AIRPORT SALEM, MARION COUNTY, ILLINOIS

DRAINAGE IMPROVEMENTS

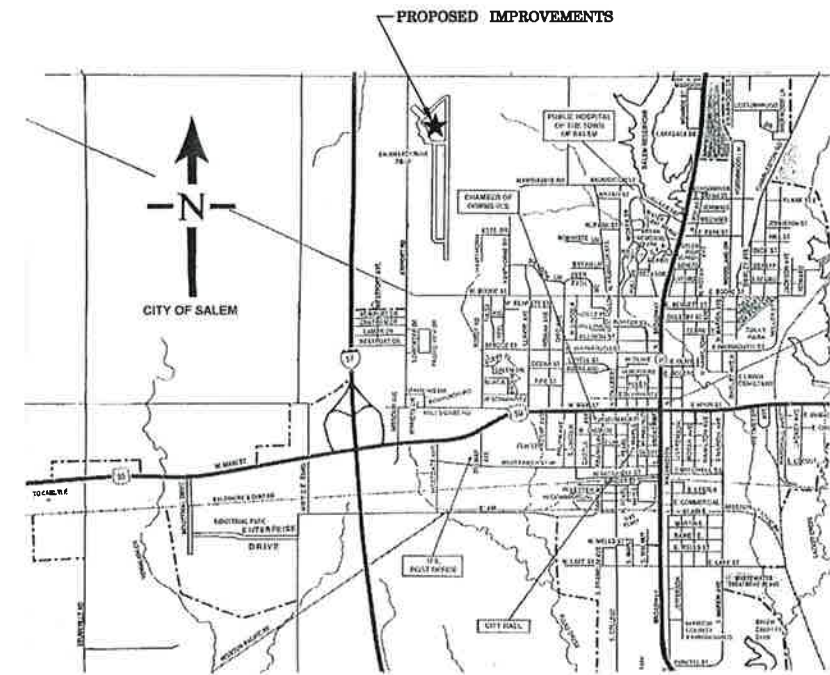
THIS PROJECT SHALL CONSIST OF THE REMOVAL AND REPLACEMENT OF STORM SEWER PIPES AND INLETS AND ALL OTHER INCIDENTAL WORK FOR SPECIFIED LOCATIONS



LOCATION MAP

ILLINOIS PROJECT - SLO-4151
AIP PROJECT NO - 3-17-0089-B20
DATE: MAY 11, 2012

LATITUDE: 38°38'34"
LONGITUDE: 88°57'51"
ELEVATION: 573 M.S.L.



VICINITY MAP

INDEX OF SHEETS

- 1 COVER
- 2 GENERAL NOTES AND SUMMARY OF QUANTITIES
- 3 SAFETY PLAN
- 4 KEY MAP
- 5-6 REMOVAL PLANS
- 7-13 STORM SEWER PLAN AND PROFILES
- 14-22 IDOT STANDARDS



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www.aecom.com

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AECOM Technical Services 184-000178

Kenneth M. Coulter 5-9-12
KENNETH M. COULTER
ILLINOIS REGISTERED PROFESSIONAL ENGINEER
No. 062-038839
EXPIRATION DATE 11/30/13

SALEM LECKRONE
AIRPORT
SALEM, ILLINOIS

APPROVED *[Signature]*
CHAIRMAN, SALEM AIRPORT AUTHORITY

DATE 5-8-12

UTILITIES – LOCATIONS/INFORMATION ON PLANS

The locations of existing utilities, as shown on the plans are based on the best information available, however they are not guaranteed. All utility locations shown on the plans are based on the approximate depth from previous construction plans. It shall be the Contractor's responsibility to ascertain their exact location. Neither the owner nor the engineer assumes any responsibility whatsoever in respect to the accuracy or sufficiency of the information and there is no guarantee either expressed or implied, that the conditions are representative of those to be encountered.

EXCESS WASTE

Excess waste products removed from the construction site shall be disposed of offsite as required by local and state laws. Excess earthen materials may be disposed of on airport property at a location determined by the Resident Engineer.

ORDERING LENGTH CONFIRMATION – DRAINAGE ITEMS

The Contractor shall verify exact lengths, types and size of the pipe culverts, storm sewers, and/or pipe drains required prior to ordering these items.

EXISTING DRAINAGE PIPES CONNECTED TO NEW STRUCTURES

The connecting of existing drain tiles, pipe culverts, or storm sewers to the proposed drainage system will not be paid for separately and shall be considered as included in the pay items provided.

STORM SEWER BACKFILL AND BACKFILL AT PIPE REMOVAL

Suitable material excavated during the project may be used as backfill for trenches in seeded areas, in accordance with Section 152. Additional material needed to backfill storm sewer or pipe/structure removal areas shall be provided by the contractor at no additional cost.

EARTHWORK

Minor earthwork and grading will be needed at the outlet of Pipe 1 and at various other locations, particularly at new inlets or manholes. All earthwork and grading will not be paid for separately but will be considered included in the cost of the project. All areas of construction shall be graded smooth, creating positive drainage, prior to seeding.

HIGHWAY STANDARDS

280001-06
542546-01
602401-03
602406-05
602601-02
604001-03
604036-02

TEMPORARY EROSION CONTROL SYSTEMS
FLUSH INLET BOX FOR MEDIAN
MANHOLE TYPE A
MANHOLE TYPE A 6' DIA.
PRECAST REINFORCED CONCRETE FLAT SLAB TOP
FRAME AND LIDS TYPE 1
GRATE TYPE 8

SUMMARY OF QUANTITIES

PAY ITEM NO.	PAY ITEM	UNIT	TOTAL QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1
AR150530	TRAFFIC MAINTENANCE	L SUM	1
AR156500	TEMPORARY EROSION CONTROL	L SUM	1
AR401910	REMOVE & REPLACE BIT. PAVEMENT	SQ YD	250
AR701415	15" RCP, CLASS III	L F	196
AR701418	18" RCP, CLASS III	L F	290
AR701424	24" RCP, CLASS III	L F	1880
AR701427	27" RCP, CLASS III	L F	410
AR701430	30" RCP, CLASS III	L F	59
AR701436	36" RCP, CLASS III	L F	268
AR701512	12' RCP, CLASS IV	L F	42
AR701518	18' RCP, CLASS IV	L F	524
AR701900	REMOVE PIPE	L F	2,379
AR751410	INLET	EACH	12
AR751540	MANHOLE 4'	EACH	2
AR751550	MANHOLE 5'	EACH	7
AR751560	MANHOLE 6'	EACH	1
AR751900	REMOVE INLET	EACH	5
AR751903	REMOVE MANHOLE	EACH	6
AR752430	PRECAST REINFORCED CONC. FES 30"	EACH	1
AR752903	REMOVE HEADWALL	EACH	1
AR801403	FILL EXISTING STORM SEWER	CU YD	144
AR801418	BORE AND JACK 18" PIPE CULVERT	L F	130
AR801427	BORE AND JACK 27" PIPE CULVERT	L F	130
AR801430	BORE AND JACK 30" PIPE CULVERT	L F	130
AR801436	BORE AND JACK 36" PIPE CULVERT	L F	130
AR901510	SEEDING	ACRES	4
AR908510	MULCHING	ACRES	4

CONTRACTOR'S RESPONSIBILITIES

1. THE CONTRACTOR'S EMPLOYEES WILL PARK IN THE EQUIPMENT PARKING AREA.
2. THE CONTRACTOR WILL FURNISH ALL EMPLOYEES WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THEM AS BEING PART OF THE CONSTRUCTION CREW.
3. WHEN THE CONTRACTOR'S VEHICLES ARE ON THE AIRPORT SITE THEY WILL BE PROPERLY MARKED WITH THE INTERNATIONAL ORANGE AND WHITE FLAG DISPLAYED IN FULL VIEW ABOVE THE VEHICLES.
4. CONTRACTOR MUST HAVE AERONAUTIC RADIO FOR EMERGENCY USE (122.8 MHZ)
5. THE CONTRACTOR WILL NOT BE ALLOWED ACCESS TO THE AIRPORT TERMINAL BUILDING & WILL NEED TO SUPPLY PORTABLE REST ROOMS ON SITE FOR CONSTRUCTION CREWS.
6. CONTRACTOR PERSONNEL SHALL BE LIMITED TO AREA REQUIRED TO CONSTRUCT IMPROVEMENTS. ACCESS NOT ALLOWED ACROSS AIRPORT PROPERTY.
7. BARRICADES SHALL BE PLACED IN SUCH A WAY AS TO PREVENT AIRCRAFT ACCESS TO THE CONTRACTOR'S WORK AREA.(MAXIMUM 10' SPACING)
8. THE CONTRACTOR SHALL HAVE ALL UTILITIES LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. J.U.L.I.E. WILL ONLY MARK THE UTILITIES TO THE METER LOCATION.

IT WILL BE THE CONTRACTORS RESPONSIBILITY TO LOCATE ON SITE UNDERGROUND UTILITIES, AND THE COST WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
9. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THE TAXIWAYS, TAXILANES, OR APRONS.
10. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORALLY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO AT NO ADDITIONAL COST TO THE PROJECT.
11. THE CONSTRUCTION WORK IS WITHIN AN OPERATING AIRPORT. THE CONTRACTOR SHALL EXERCISE CAUTION AND YIELD TO AIRCRAFT AT ALL TIMES.

FILE NAME = \$FILEL\$	USER NAME = \$USER\$	DESIGNED - REW	REVISED 5-4-12	SALEM LECKRONE AIRPORT SALEM, ILLINOIS	GENERAL NOTES AND SUMMARY OF QUANTITIES	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - KRS	CHECKED - KMC	REVISED -			IL. PROJ. NO. SLO-4151	MARION	22	2	
	PLOT SCALE = \$SCALE\$	DATE - 3-12-12	REVISED -			AIP PROJ. 3-17-0089-B20	ILLINOIS FED. AID PROJECT			
	PLOT DATE = \$DATE\$			SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.

SCOPE OF WORK

THIS PROJECT CONSISTS OF THE REMOVAL AND REPLACEMENT OF STORM SEWERS, MANHOLES, AND INLETS AND OTHER INCIDENTAL WORK. SOME STORM SEWERS ARE TO BE ABANDONED AND FILLED WITH CLSM.

RUNWAY SAFETY AREA

THE CONTRACTOR OR HIS EMPLOYEES WILL NOT PROCEED INTO THE RUNWAY SAFETY AREA (RSA) WITHOUT FIRST CLOSING THE AFFECTED RUNWAY. THE CONTRACTOR WILL NOTIFY THE AIRPORT MANAGER TWO DAYS IN ADVANCE OF A RUNWAY CLOSING. ANY RUNWAY CLOSURE MUST BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO ANY CONSTRUCTION. WHEN CONSTRUCTION ACTIVITIES ARE WITHIN 75' OF THE RUNWAY CENTERLINE, THE RUNWAY WILL BE CLOSED BY PLACING YELLOW CROSSES OVER THE NUMBERS AT THE END OF THE RUNWAY. COST OF CONSTRUCTING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE RUNWAY WILL BE CLOSED ONLY DURING THE CONSTRUCTION DAY. AT THE END OF EACH CONSTRUCTION DAY, THE CONTRACTOR WILL SMOOTH GRADE ALL AREAS WITHIN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER AND REOPEN THE RUNWAY. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

OBJECT FREE AREA (OFA) - TAXIWAYS / TAXILANES / APRONS

THE CONTRACTOR WILL NOT PROCEED INTO THE OBJECT FREE AREA WITHOUT FIRST CLOSING THE TAXIWAY. BARRICADES, SAFETY CONES AND CROSSES SHALL BE POSITIONED TO PROPERLY CLOSE THE AREA AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

WHEN RUNWAY 18-36 IS OPEN AND EXIT TAXIWAYS ARE CLOSED FOR MORE THAN 3 CALENDAR DAYS, YELLOW PAINTED "X" BETWEEN THE RUNWAY EDGE AND BARRICADES WILL BE REQUIRED. THE TEMPORARY PAINT SHALL BE 'SEYMOUR TEMPORARY MARKER' PART NO. 20-636 OR OTHER EQUIVALENT PAINT EASILY REMOVED WITH WATER WITHOUT DEFACING THE PAVEMENT.

ELECTRIC CABLE / UNDERDRAIN

LOCATION OF EXISTING AIRPORT UNDERGROUND CABLES AND UNDERDRAINS MUST BE VERIFIED BY THE CONTRACTOR. THE CONTRACTOR SHALL PROTECT AND SUPPORT EXPOSED CABLES AND UNDERDRAIN, BOTH TO PROTECT THE UTILITY AND WORKERS IN THE AREA.

THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT MANAGER TO SHUT-OFF ELECTRICITY AND DISCONNECT CIRCUITS NEAR THE WORK AREA.

REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE MADE IN ACCORDANCE WITH THE SPECIFICATION, OR AS DIRECTED BY THE OWNER OF THE CABLE, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM POINT TO POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF AN FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS, IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.

RUNWAY LIGHTING

PROPER CARE SHALL BE TAKEN WHEN WORKING AROUND THE RUNWAY/TAXIWAY LIGHTING TO AVOID ANY DAMAGE. DAMAGE TO THE LIGHTS SHALL BE REPAIRED OR REPLACED AT THE CONTRACTORS' EXPENSE.

EROSION CONTROL

THE CONTRACTOR IS RESPONSIBLE FOR ALL EROSION CONTROL WORK AND ANY REQUIRED PERMITS.

HAUL ROAD AND EQUIPMENT PARKING AREA

CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT PARKING AREA AS SHOWN ON THIS SHEET, OR AS DIRECTED BY THE ENGINEER. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE AIRPORT SECURITY AT THE HAUL ROUTE ENTRANCE, AND RESTORE THESE AREAS TO THEIR ORIGINAL CONDITION AFTER CONSTRUCTION IS COMPLETED. ANY DAMAGE WILL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE. THE COST OF THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

TAXIWAY CROSSING

A DESIGNATED CROSSING IS SHOWN ON THE PLANS, THE LOCATION MAY BE ALTERED AS APPROVED BY THE RESIDENT ENGINEER. DAMAGE TO ANY PAVED AREAS CAUSED BY THE CONTRACTORS' EQUIPMENT OR TRUCKS SHALL BE REPAIRED OR REPLACED AT THE CONTRACTORS' EXPENSE.

CONSTRUCTION EQUIPMENT HEIGHT

MAXIMUM HEIGHT OF CONSTRUCTION EQUIPMENT SHALL NOT EXCEED 25'.

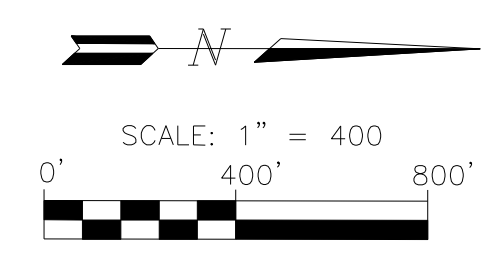
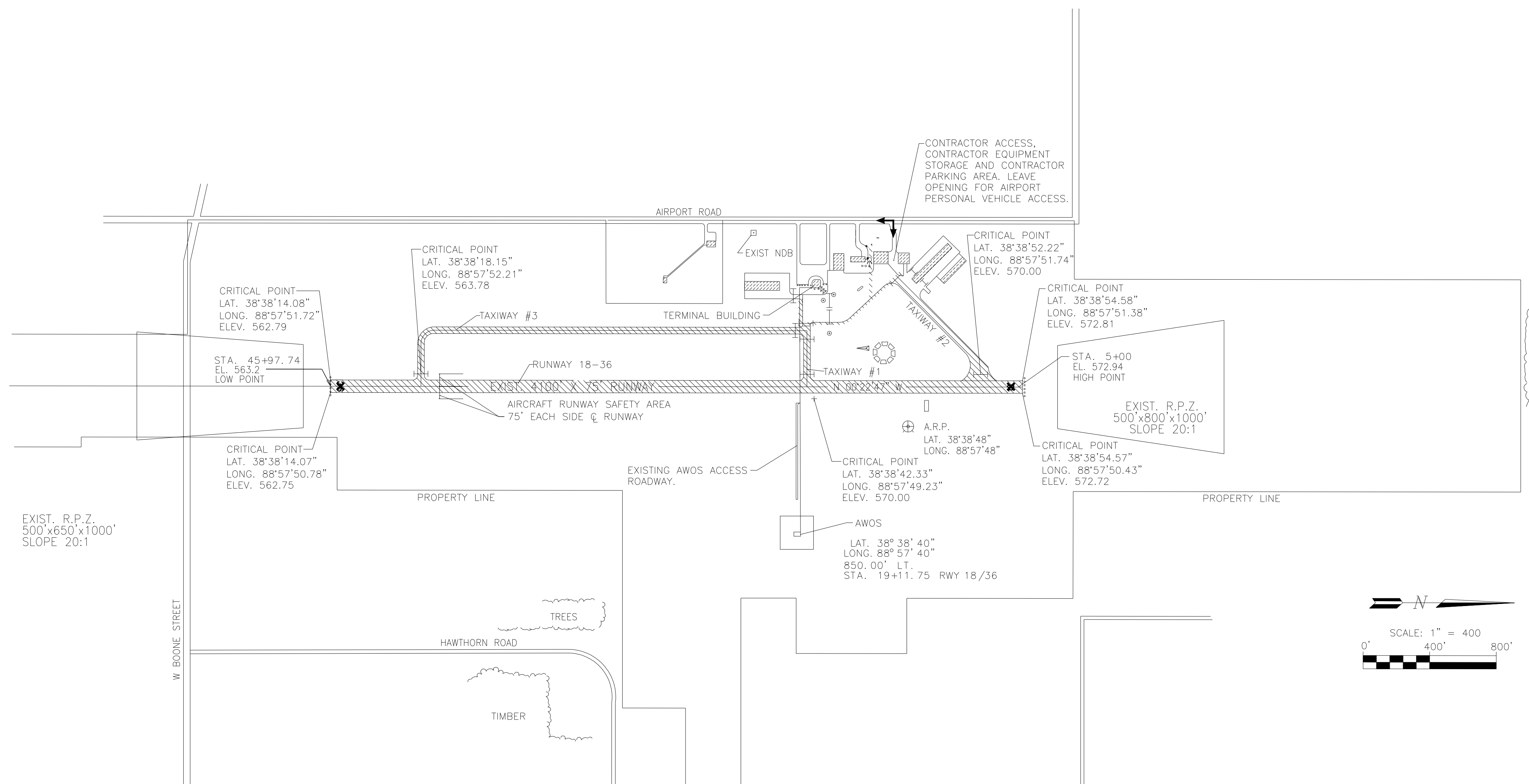
CONSTRUCTION STAGING

THE CONTRACTOR SHALL PLACE BARRICADES TO CLOSE INDIVIDUAL WORK AREAS AND TO ALLOW ACCESS FROM THE APRON AND/OR T-HANGARS TO THE RUNWAY AS DIRECTED BY THE RESIDENT ENGINEER. NO CONTRACTOR PERSONNEL, EQUIPMENT OR BARRICADES WILL BE ALLOWED WITHIN 66' OF AN ACTIVE TAXIWAY OR TAXILANE CENTERLINE, UNLESS OTHERWISE APPROVED.

BARRICADES REQUIRED FOR CLOSURES SHALL BE IDOT TY. 1 OR TY. 2 BARRICADES WITH ONE FLASHING OR STEADY BURNING RED LIGHT MOUNTED TO EACH BARRICADE.

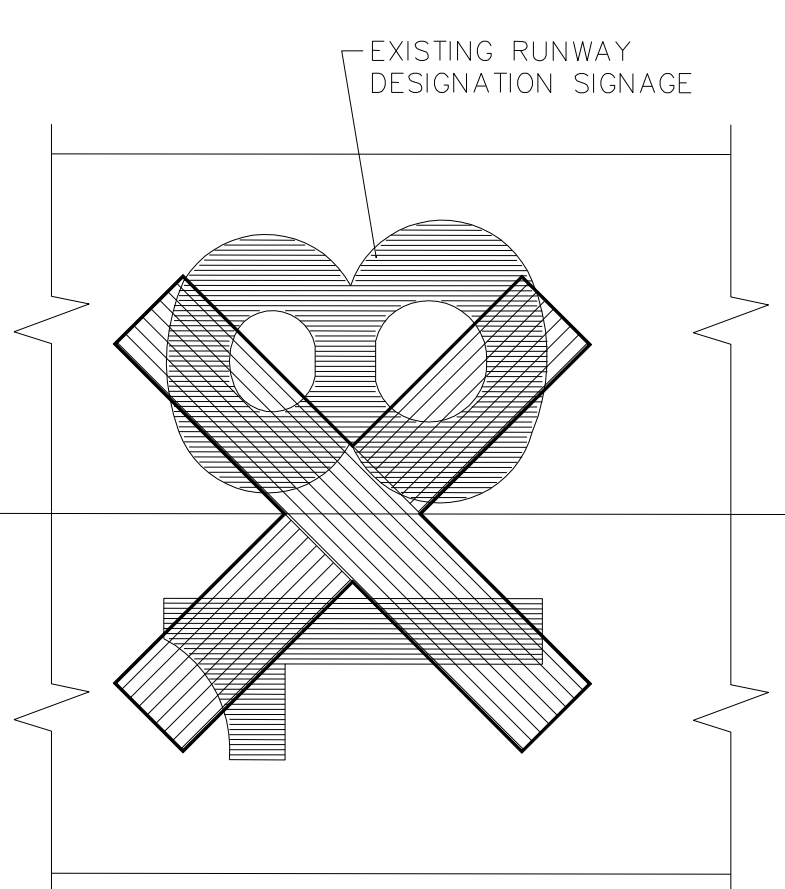
OPEN TRENCHES

BEFORE REOPENING RUNWAYS OR TAXIWAYS, ALL OPEN TRENCHES AND EXCAVATED MATERIAL WITHIN THE RUNWAY RSA OR TAXIWAY OFA SHALL BE BACKFILLED AND SMOOTH GRADED AS APPROVED BY THE RESIDENT ENGINEER.

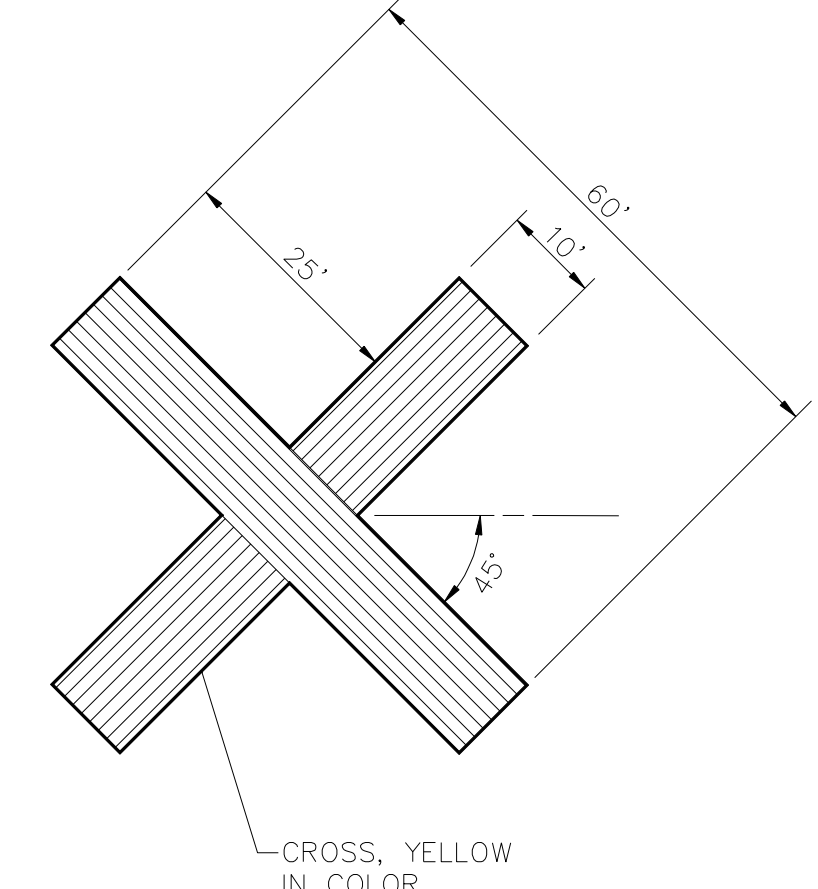


LEGEND

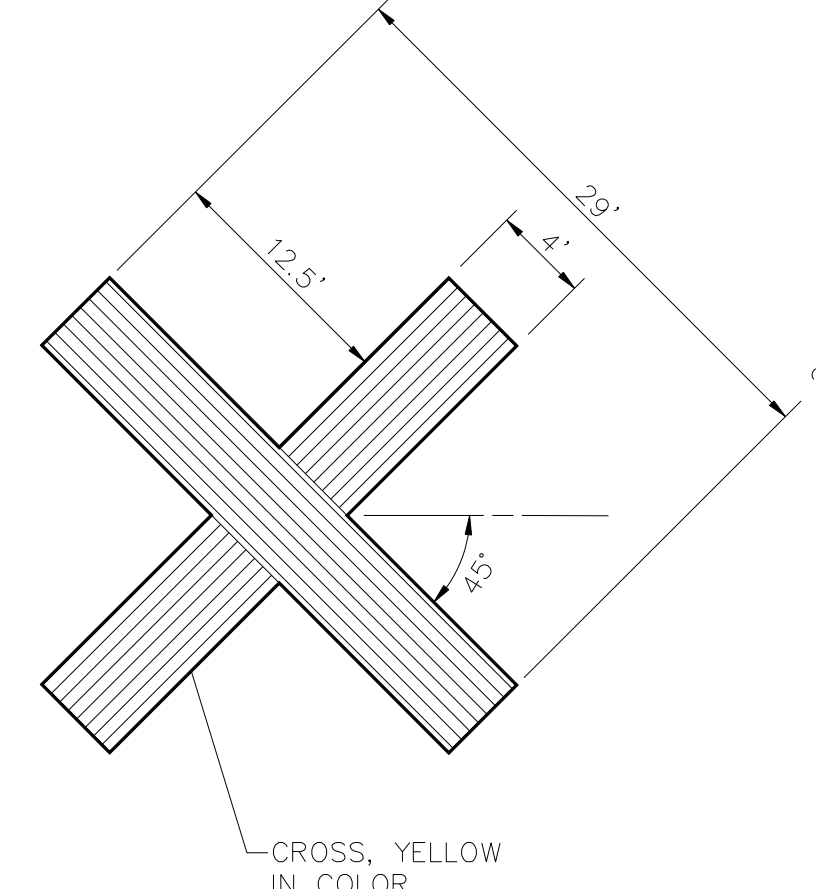
- EXISTING NON-DIRECTIONAL BEACON (NDB)
- EXISTING AIRPORT BOUNDARY
- EXISTING ROTATING BEACON
- EXISTING INTERNALLY LIGHTED WIND CONE
- EXISTING THRESHOLD LIGHT - GREEN
- EXISTING RUNWAY PROTECTION ZONE (RPZ)
- FUTURE SEGMENTED CIRCLE
- SECURITY FENCE
- EXISTING AIRPORT BUILDINGS
- EXISTING RUNWAY END IDENTIFIER LIGHT (REIL)
- BARRICADES
- DESIGNATED HAUL ROUTE
- PROPOSED LIGHTING IMPROVEMENTS



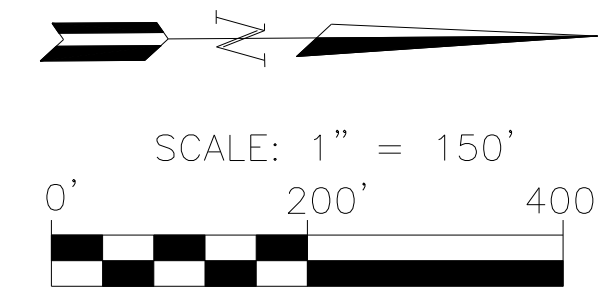
DETAIL OF CROSS FOR CLOSED RUNWAY



DETAIL OF CROSS FOR CLOSED TAXIWAY



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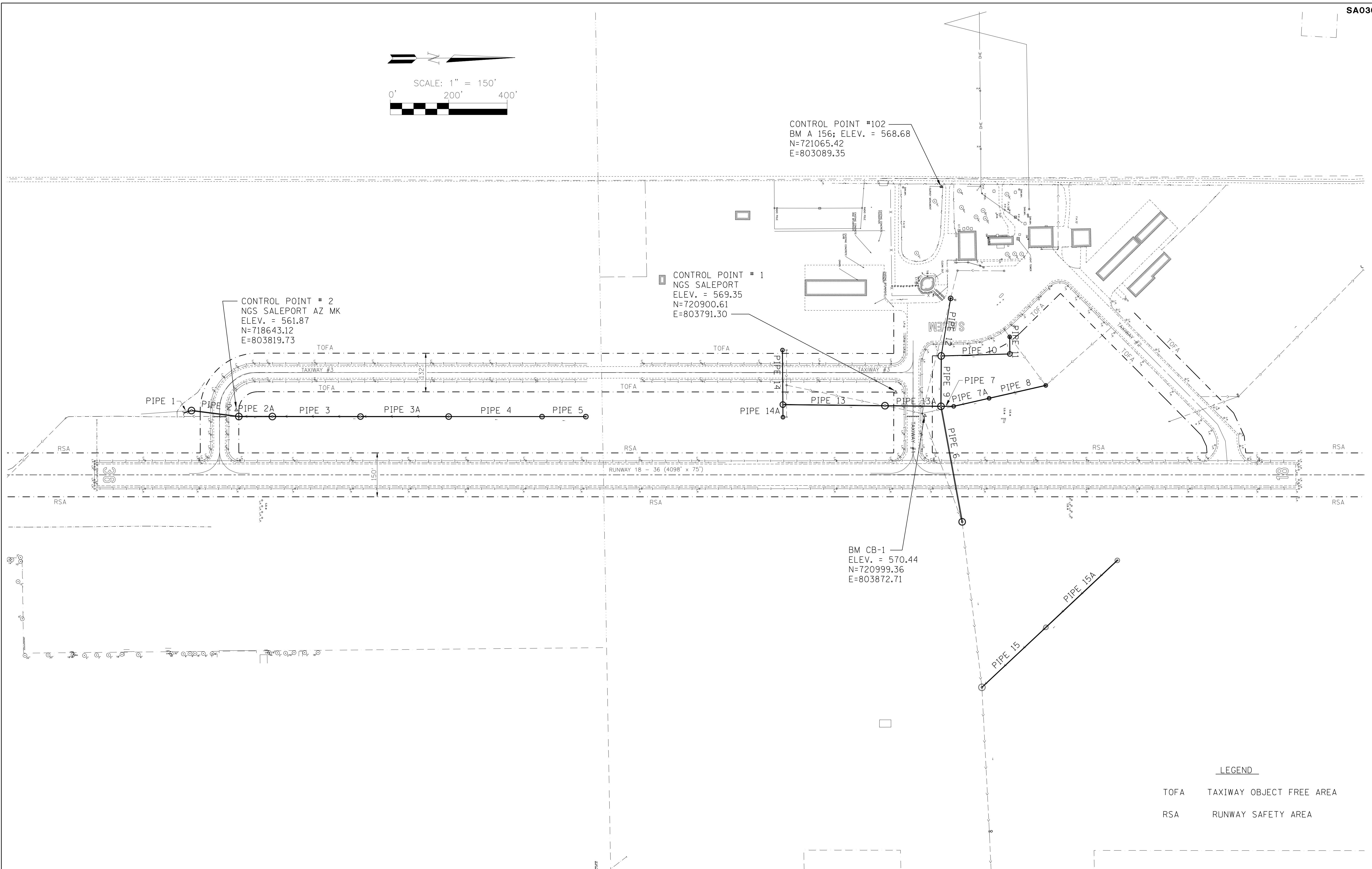


CONTROL POINT #102
 BM A 156; ELEV. = 568.68
 N=721065.42
 E=803089.35

CONTROL POINT # 2
 NGS SALEPORT AZ MK
 ELEV. = 561.87
 N=718643.12
 E=803819.73

CONTROL POINT # 1
 NGS SALEPORT
 ELEV. = 569.35
 N=720900.61
 E=803791.30

BM CB-1
 ELEV. = 570.44
 N=720999.36
 E=803872.71



LEGEND

- TOFA TAXIWAY OBJECT FREE AREA
- RSA RUNWAY SAFETY AREA

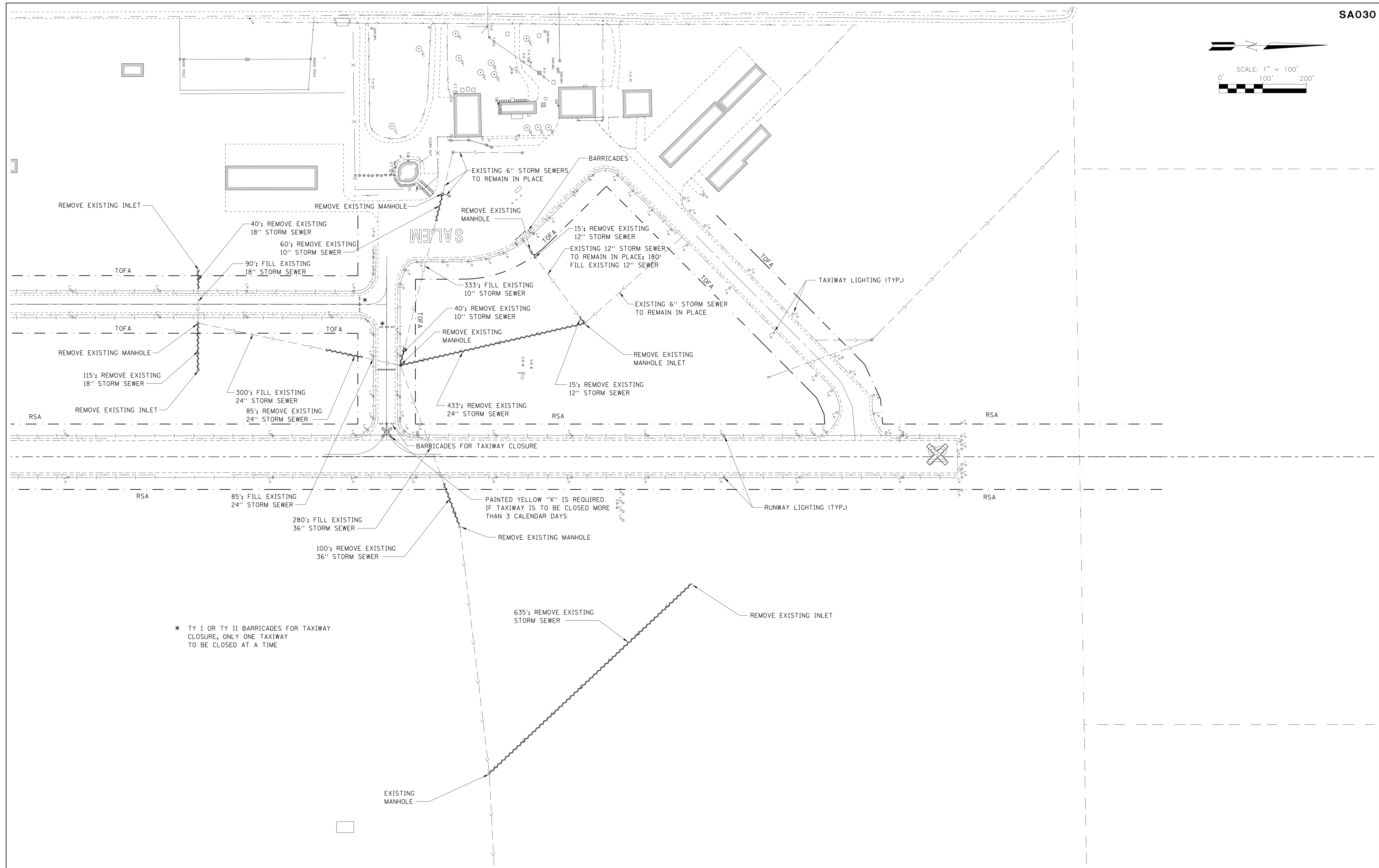
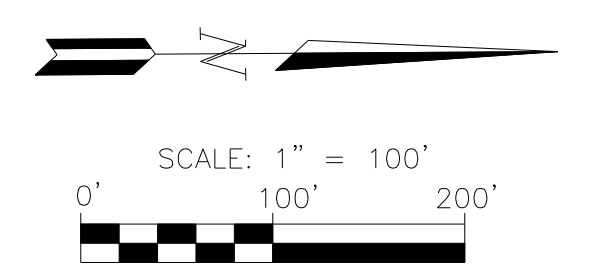
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	PLOT DATE = \$DATE\$	DATE - 3-12-12	REVISED -

**SALEM LECKRONE AIRPORT
 SALEM, ILLINOIS**

**KEY MAP
 PROPOSED DRAINAGE SYSTEM**

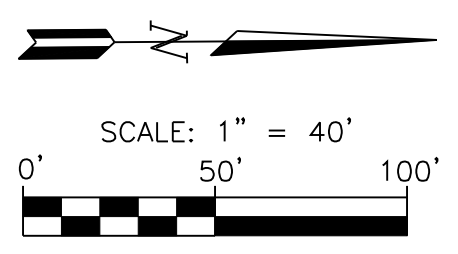
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ILLINOIS FED. AID PROJECT				

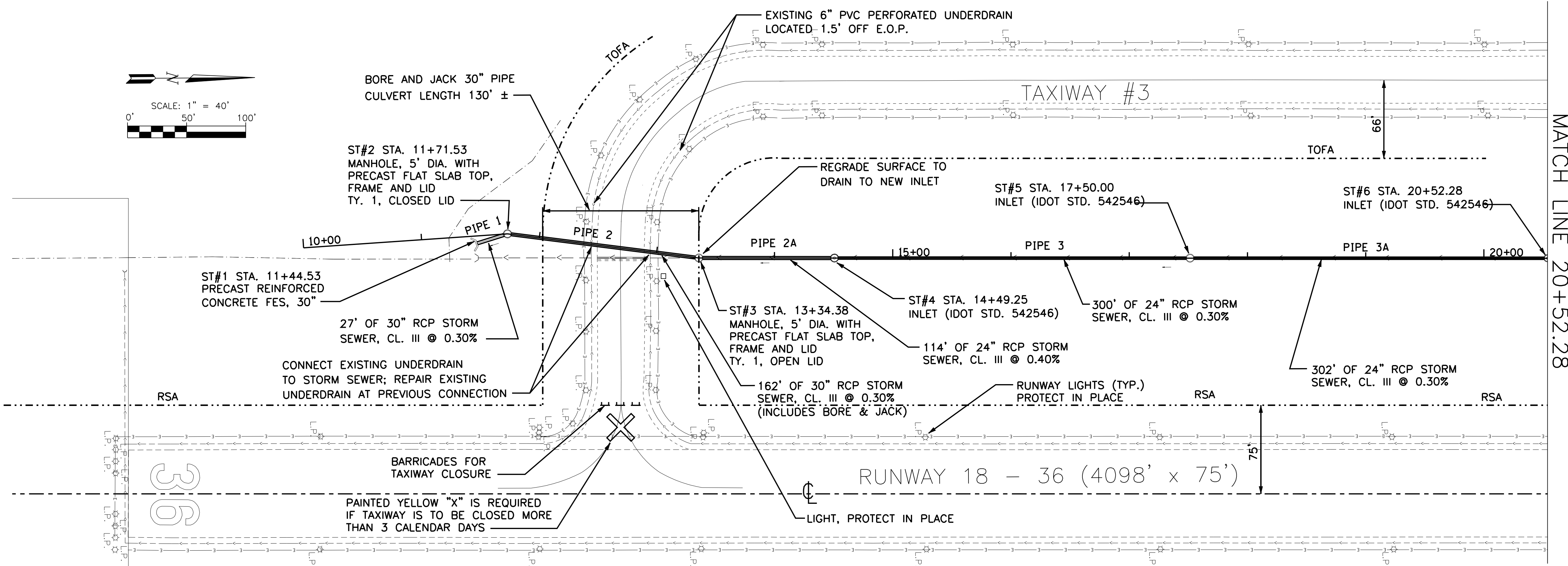


* TY I OR TY II BARRICADES FOR TAXIWAY CLOSURE, ONLY ONE TAXIWAY TO BE CLOSED AT A TIME

FILE NAME = \$FILEL\$	USER NAME = \$USER\$	DESIGNED - REW	REVISED 5-4-12	SALEM LECKRONE AIRPORT SALEM, ILLINOIS	REMOVAL PLAN NORTHERN STORM SEWER				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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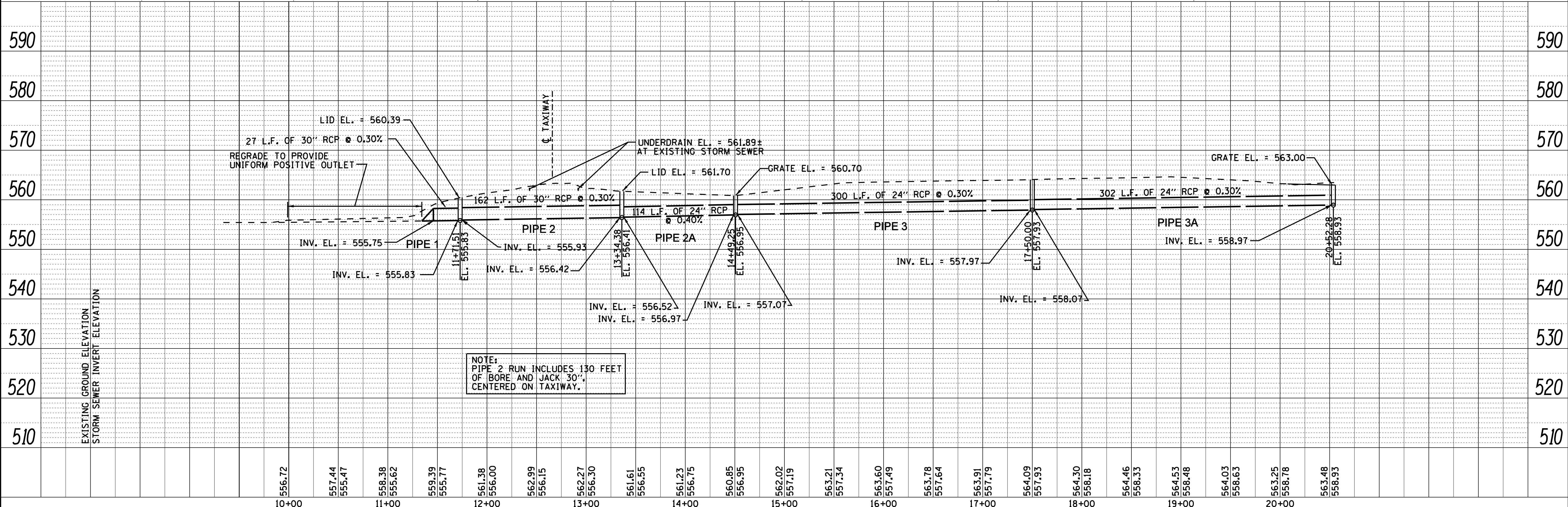
PLAN	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
	CHECKED	
	AT	
	FILE NAME	



MATCH LINE 20+52.28

ST#	STATION	NORTHING	EASTING
1	11+44.63	718463.26	803827.45
2	11+71.51	718489.96	803819.42
3	13+34.38	718651.55	803842.04
4	14+49.25	718766.23	803843.65
5	17+50.00	719066.95	803847.80
6	20+52.28	719369.19	803851.99

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	GRADES CHECKED	BY
	STRUCTURE	
	NOT AT THIS CHFD	



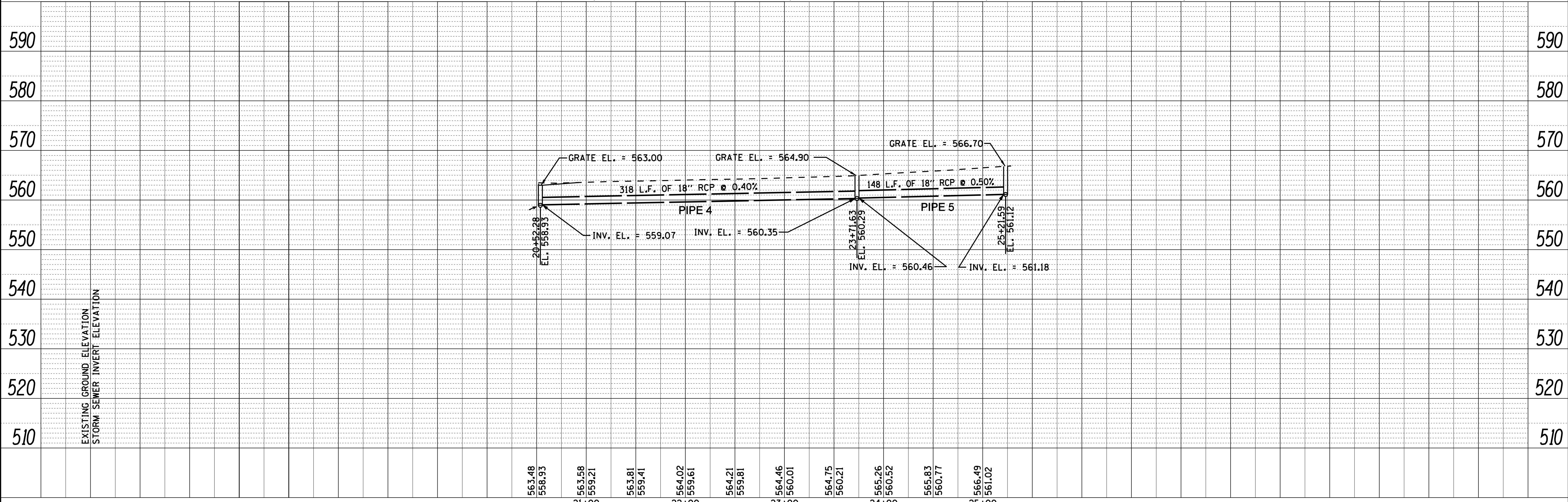
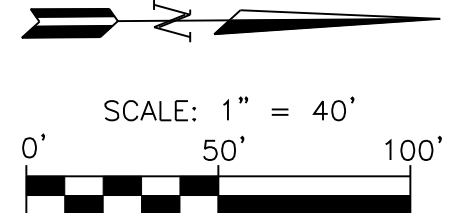
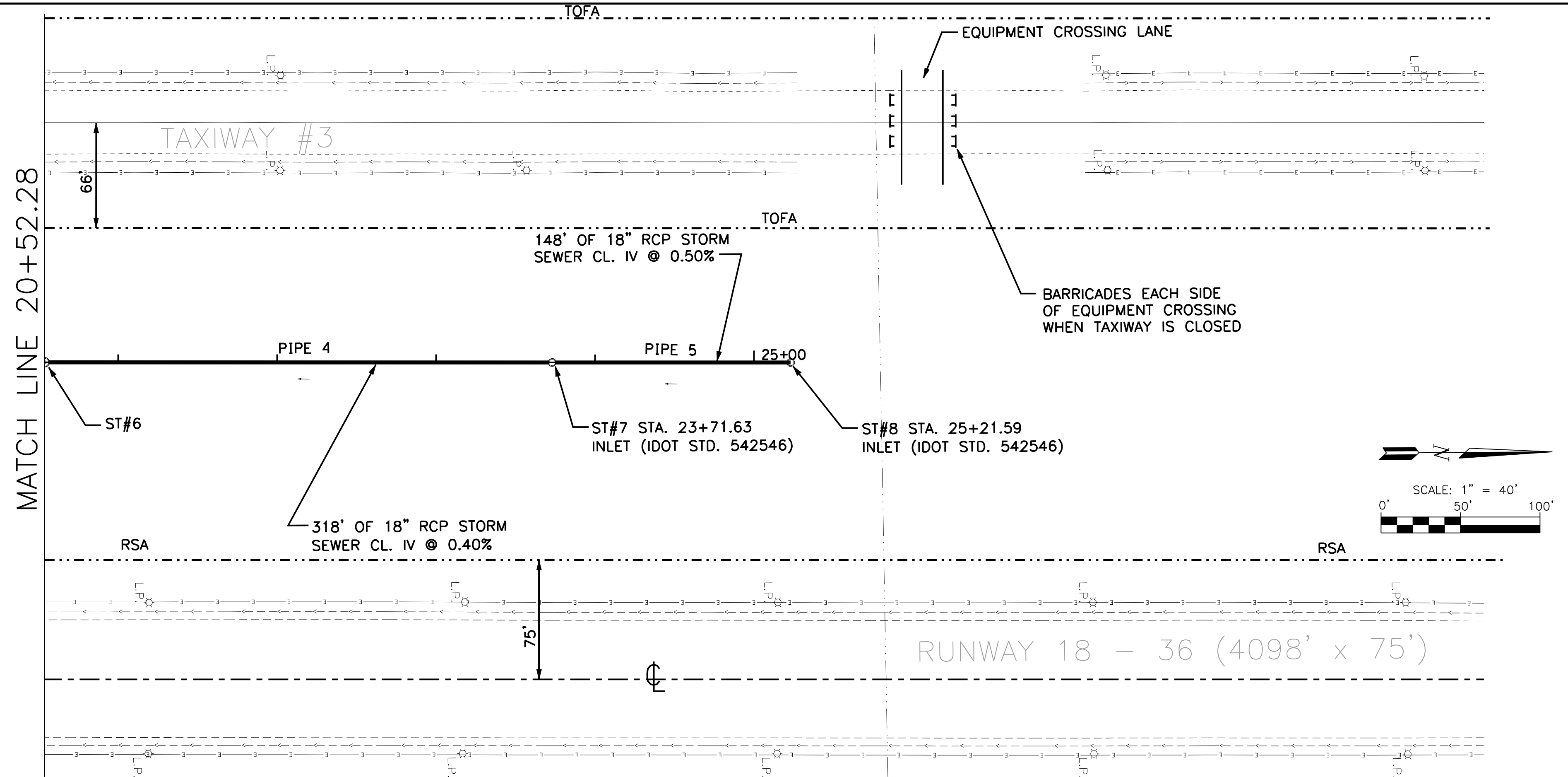
NOTE:
PIPE 2 RUN INCLUDES 130 FEET
OF BORE AND JACK 30",
CENTERED ON TAXIWAY.

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		DATE - 3-12-12	REVISED -													

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	PLOTTED		
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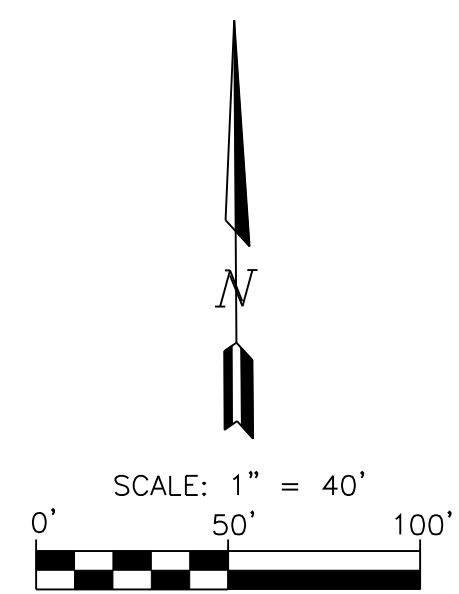
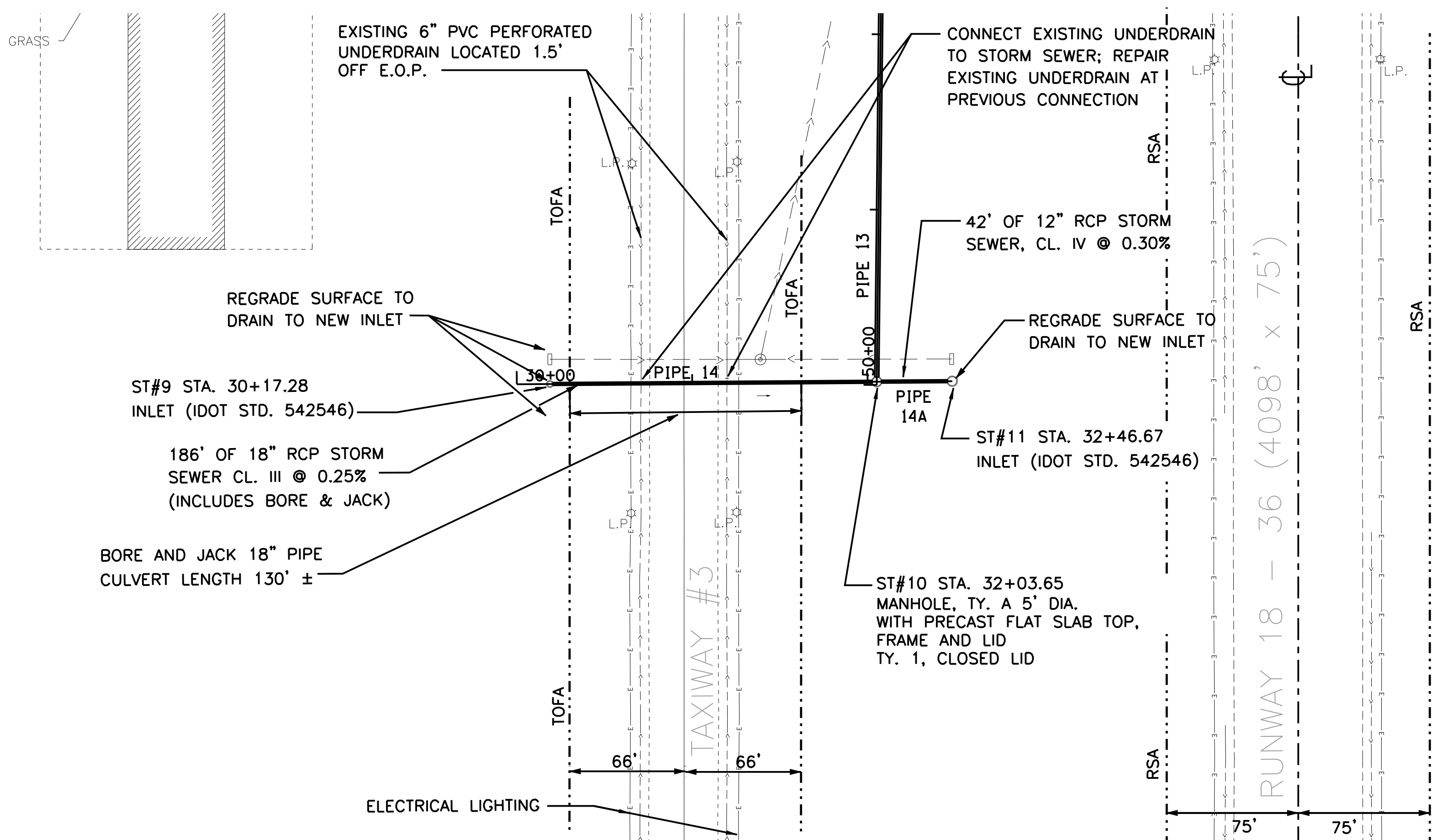
ST#	STATION	NORTHING	EASTING
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7	23+71.63	719688.52	803856.45
8	25+21.59	719838.47	803858.48



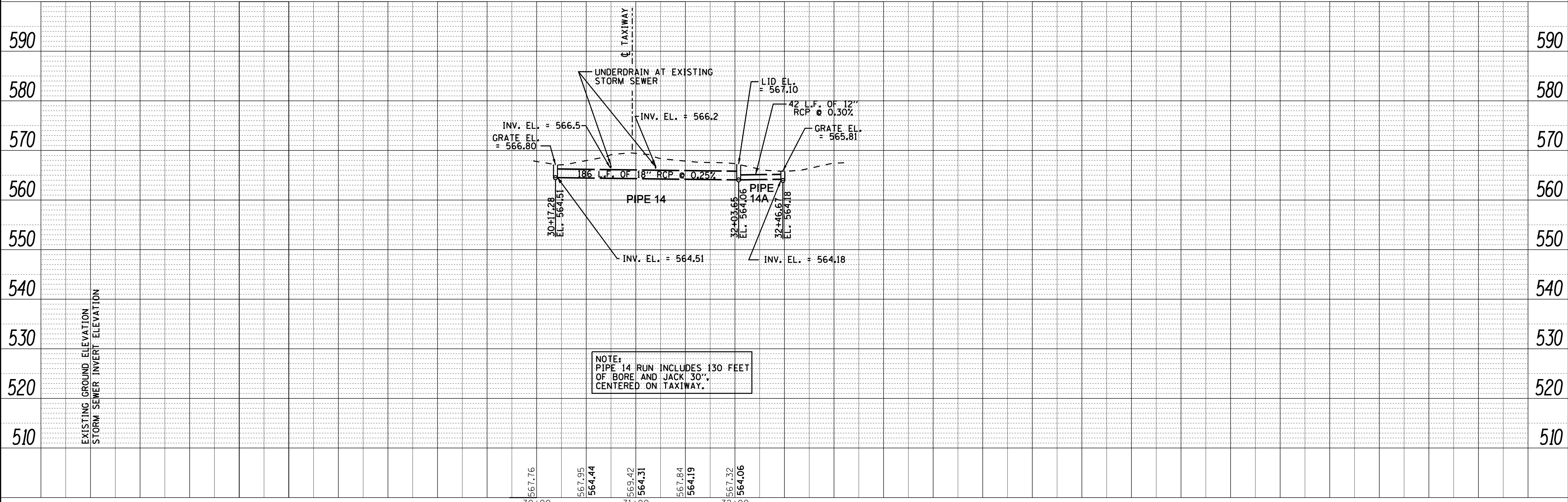
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		DATE - 3-12-12	REVISED -			ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	CADD FILE NAME	

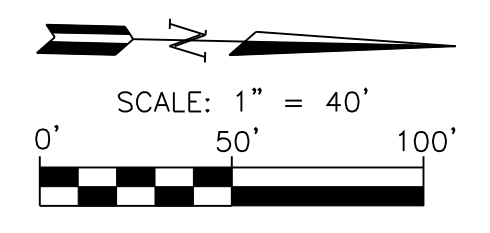
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	STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	CADD FILE NAME	



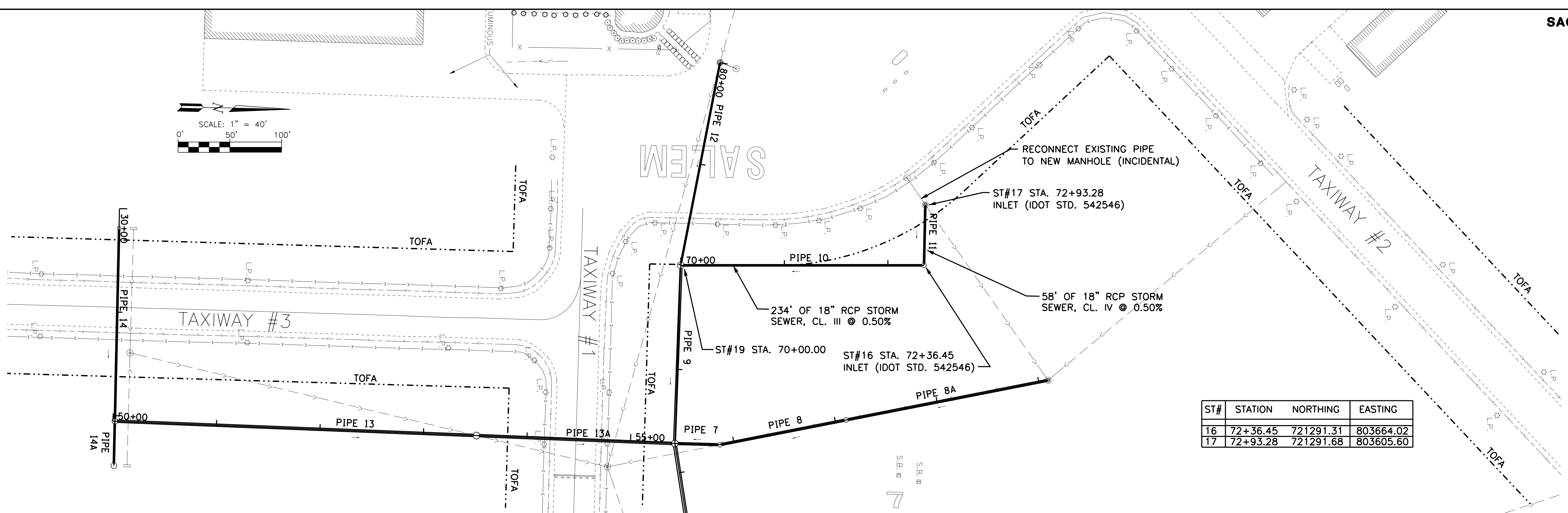
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10	32+03.65	720512.70	803826.54
11	32+46.67	720512.41	803869.56



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		DATE - 3-12-12	REVISED -										

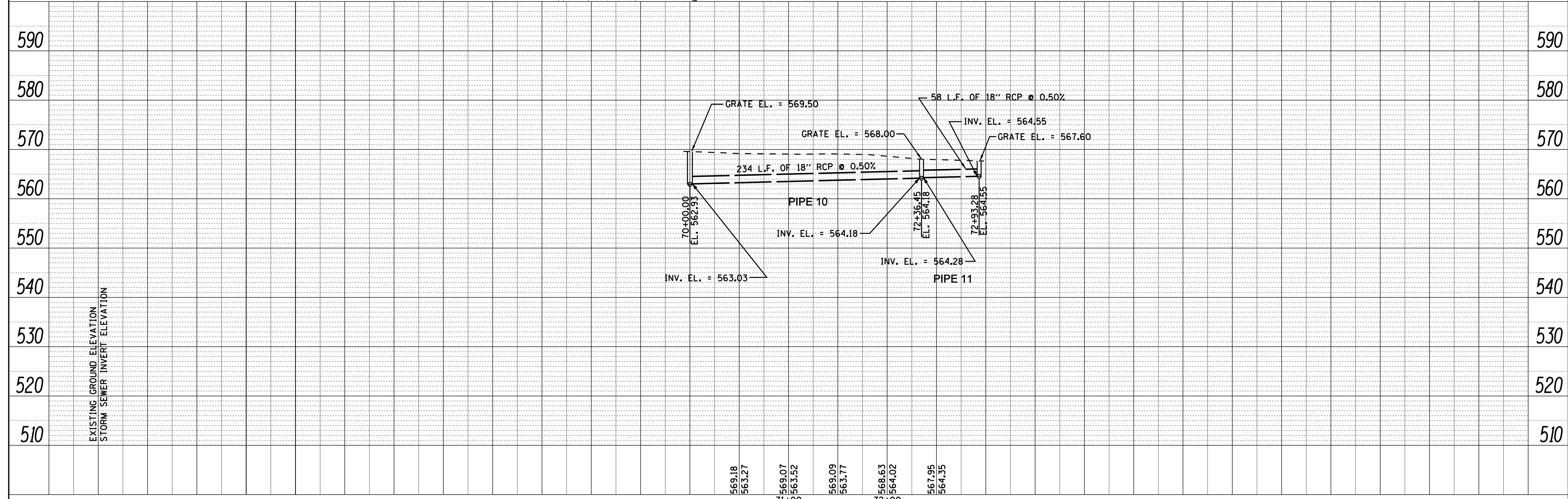


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	CHECKED		
	AT		
	CADD FILE NAME		
	NO.		

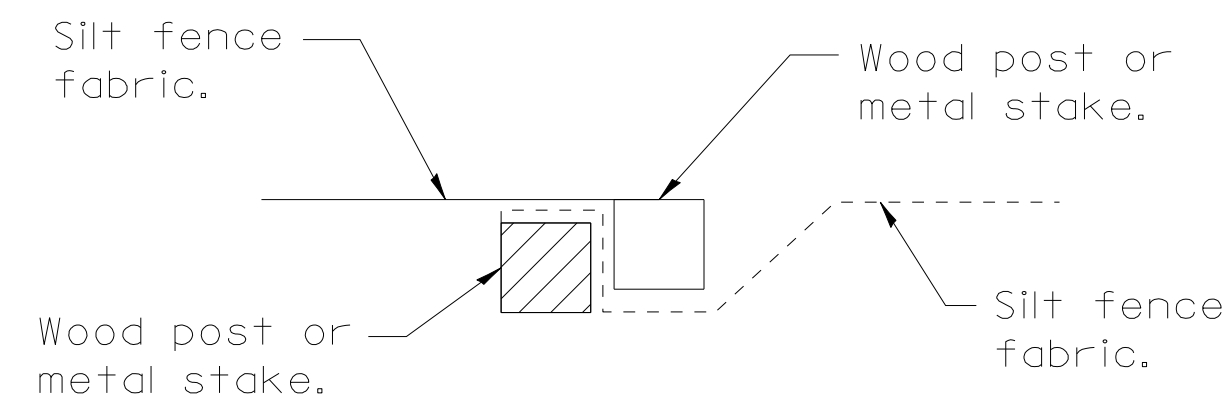


ST#	STATION	NORTHING	EASTING
16	72+36.45	721291.31	803664.02
17	72+93.28	721291.68	803605.60

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	AT		
	STRUCTURE		
	NOT AT THIS CHFD		
	NO.		

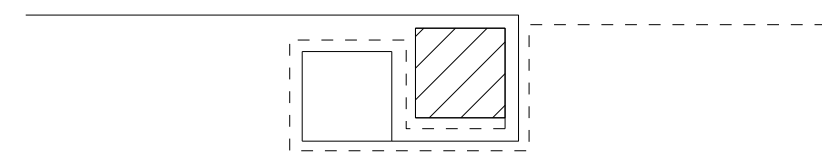


FILE NAME =	USER NAME = \$USER\$	DESIGNED - REW	REVISED 5-4-12	SALEM LECKRONE AIRPORT SALEM, ILLINOIS	PROPOSED STORM SEWER PIPES 10 AND 11				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
*FILEL\$		DRAWN - KRS	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	ILLINOIS	MARION	22	11
		CHECKED - KMC	REVISED -								IL. PROJ. NO. SL0-4151	AIP PROJ.	3-17-0089-B20	
		DATE - 3-12-12	REVISED -								ILLINOIS FED. AID PROJECT			



Place end-post (stake) of first silt fence adjacent to end-post (stake) of second silt fence with fabric positioned as shown.

STEP 1

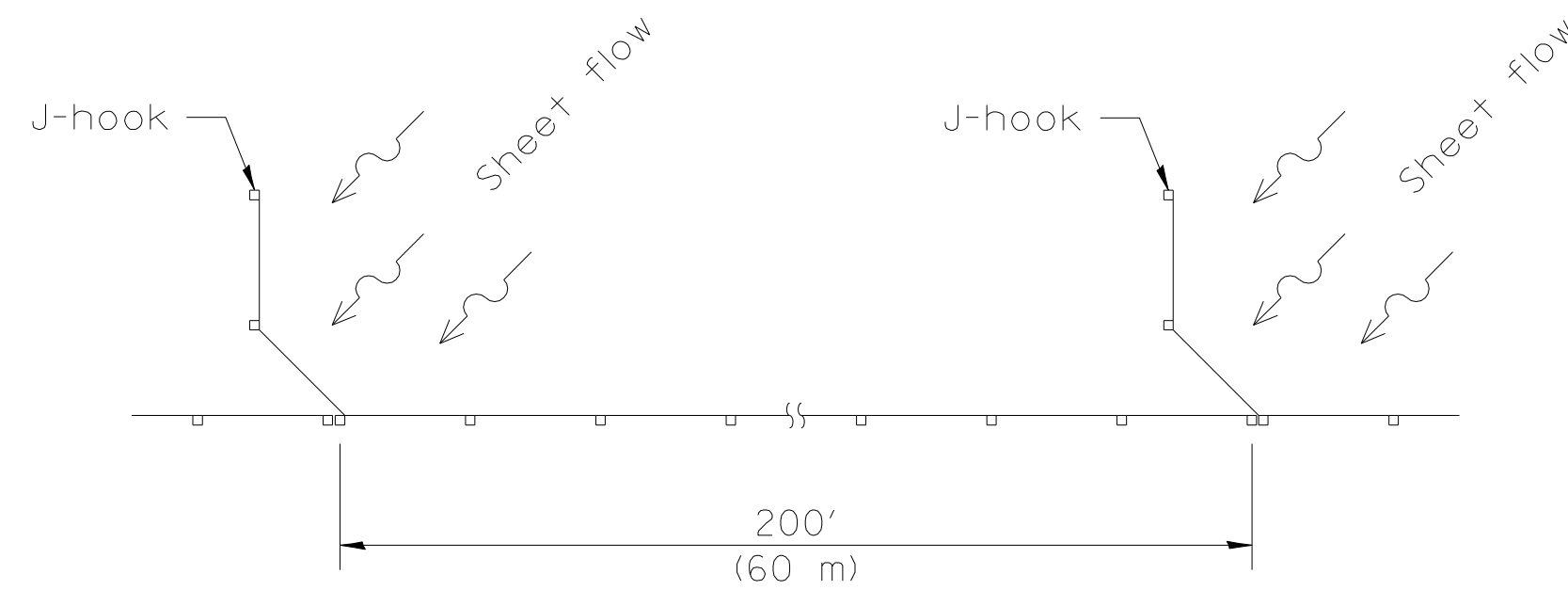


Rotate posts (stakes) together 180° clockwise and drive both posts (stakes) 18 (450) into ground.

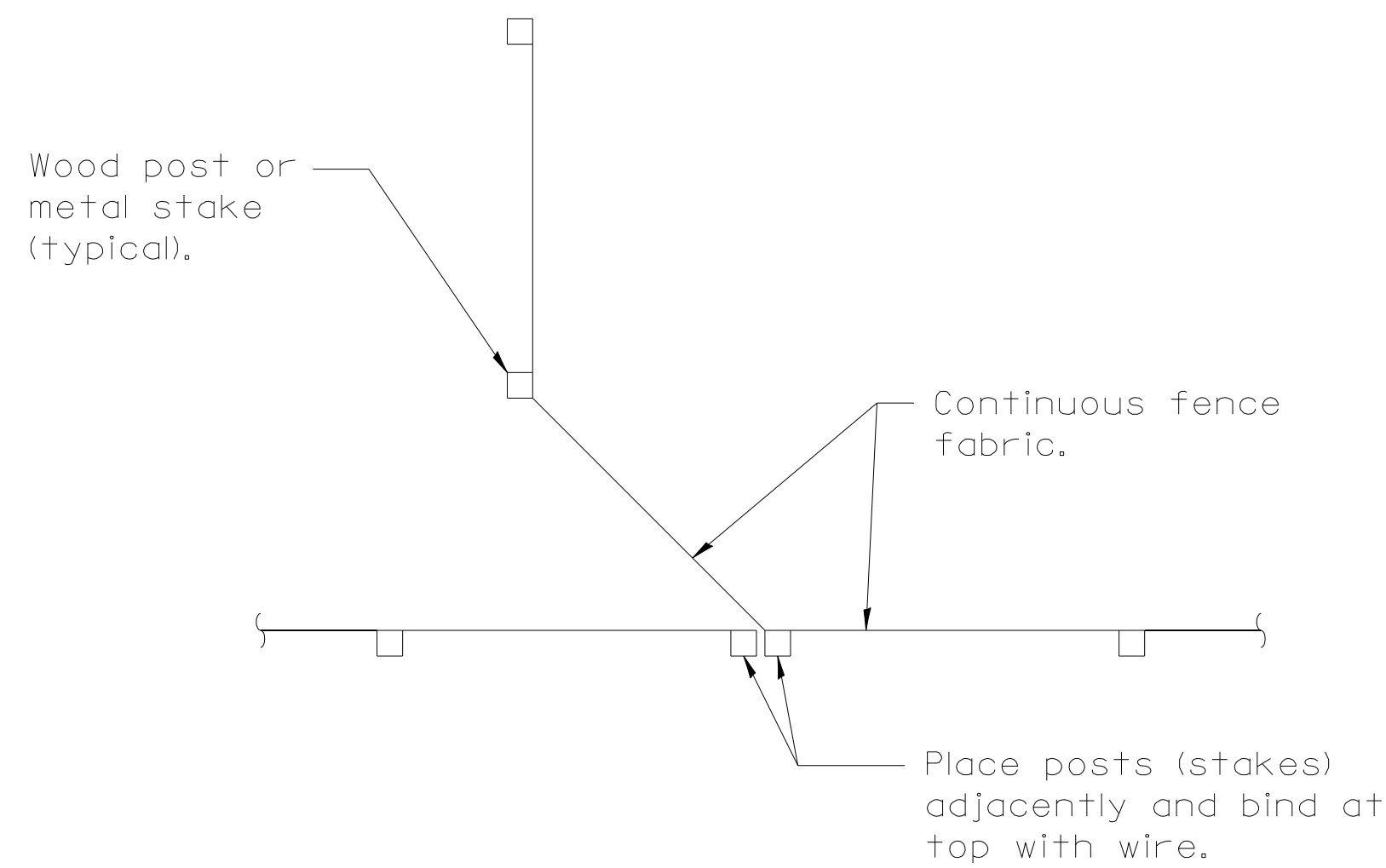
STEP 2

ATTACHING TWO SILT FILTER FENCES

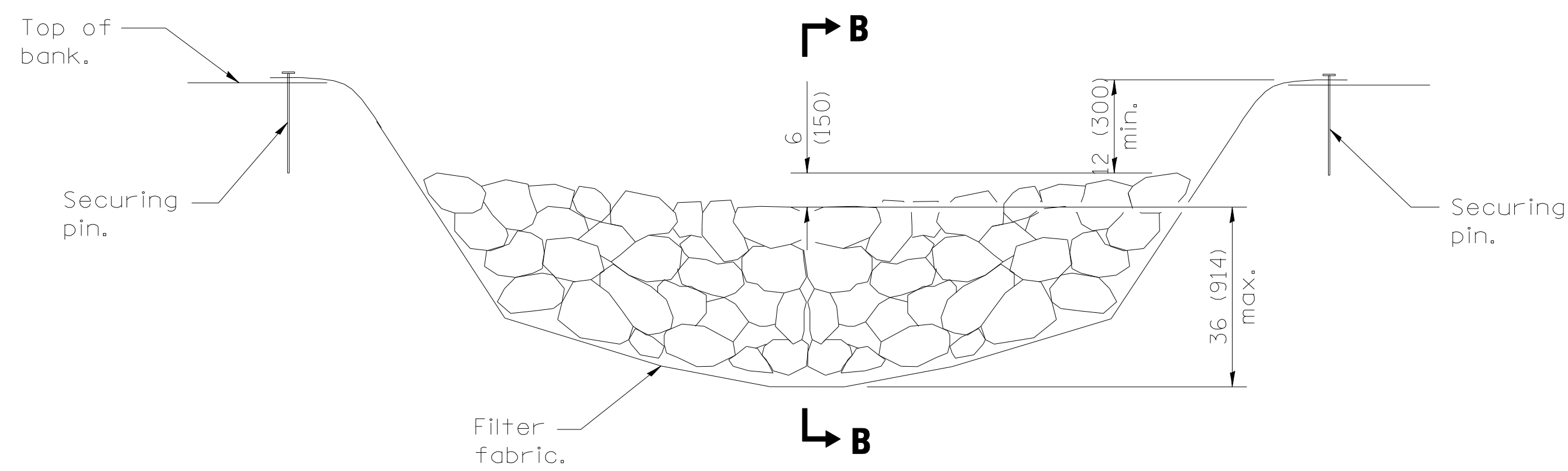
(Not applicable for J-hooks)



SILT FILTER J-HOOK PLACEMENT

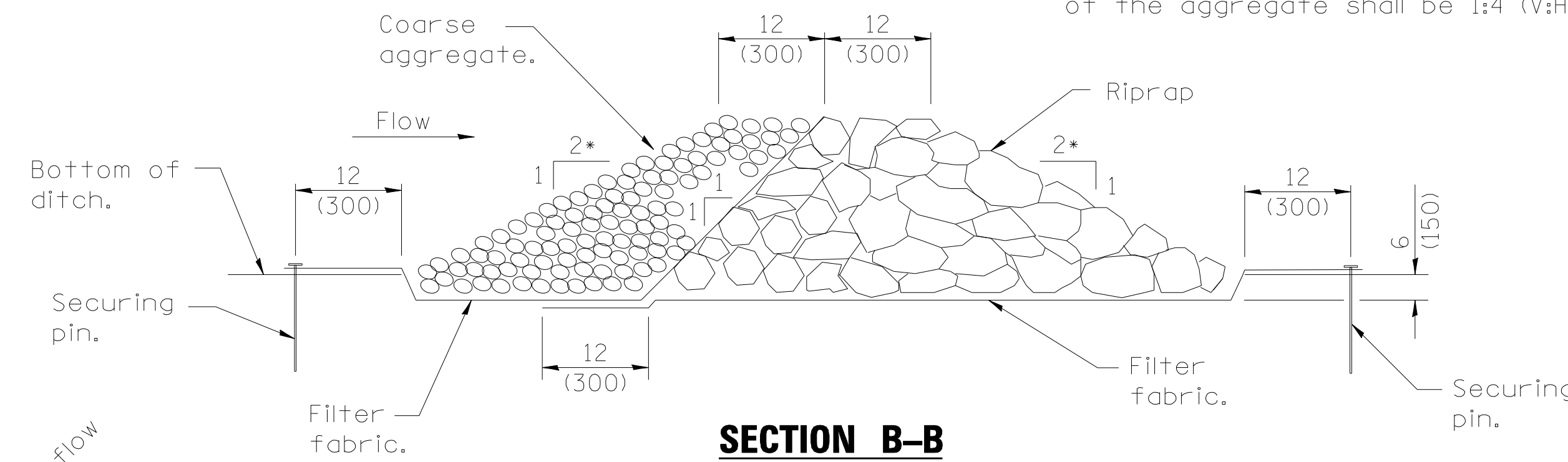


J-HOOK



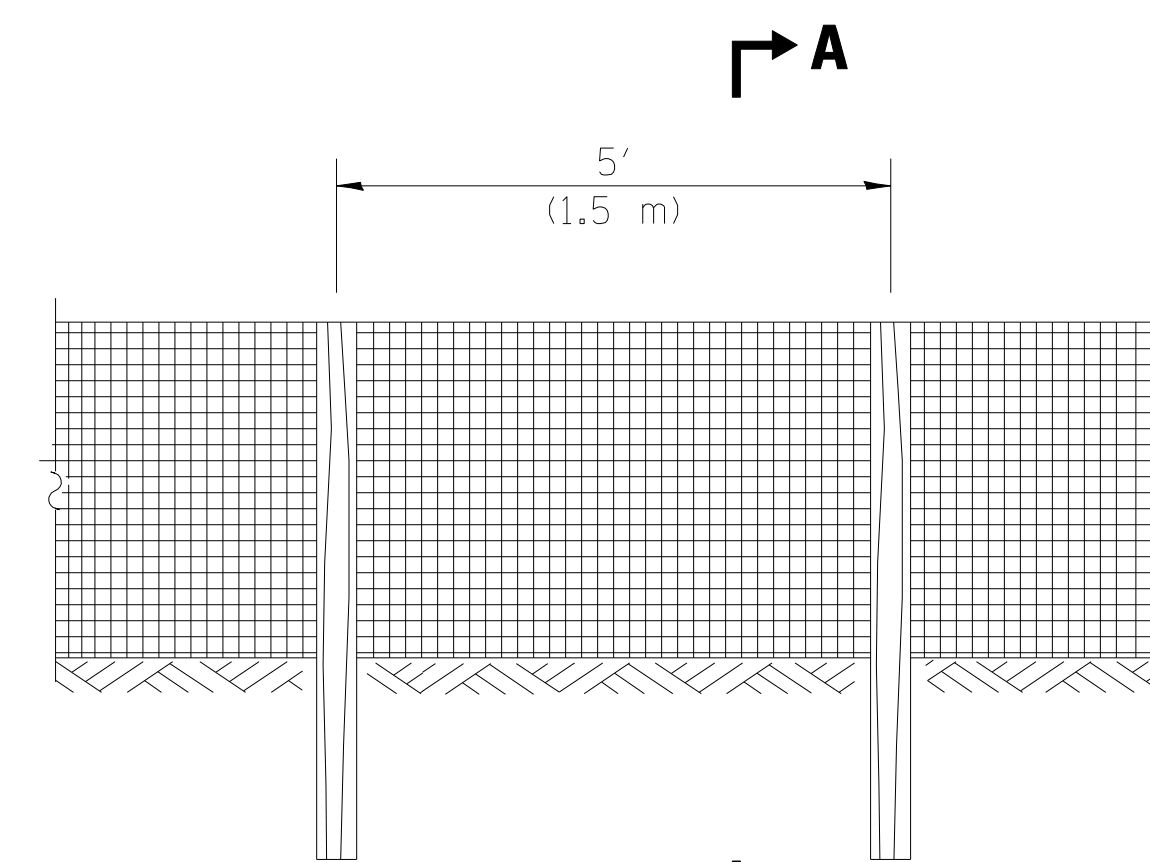
ELEVATION

* When the ditch check is within the clear zone and the road is open to traffic, the traffic approach slope of the aggregate shall be 1:4 (V:H).



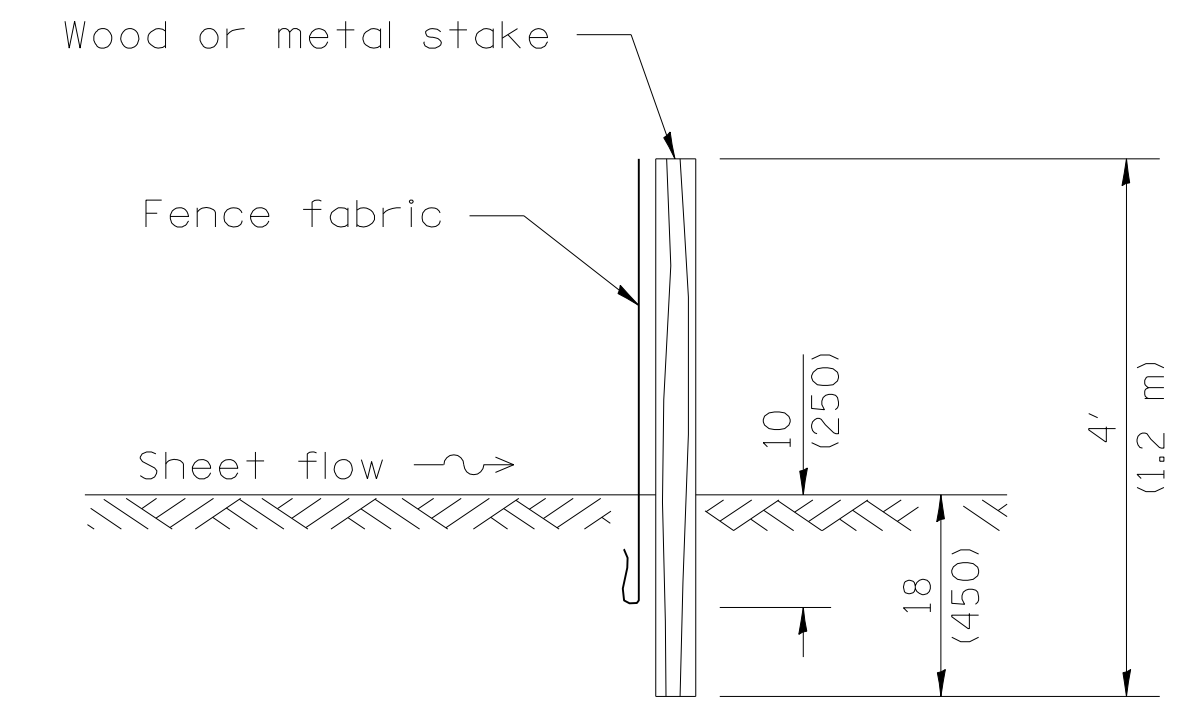
SECTION B-B

AGGREGATE DITCH CHECK

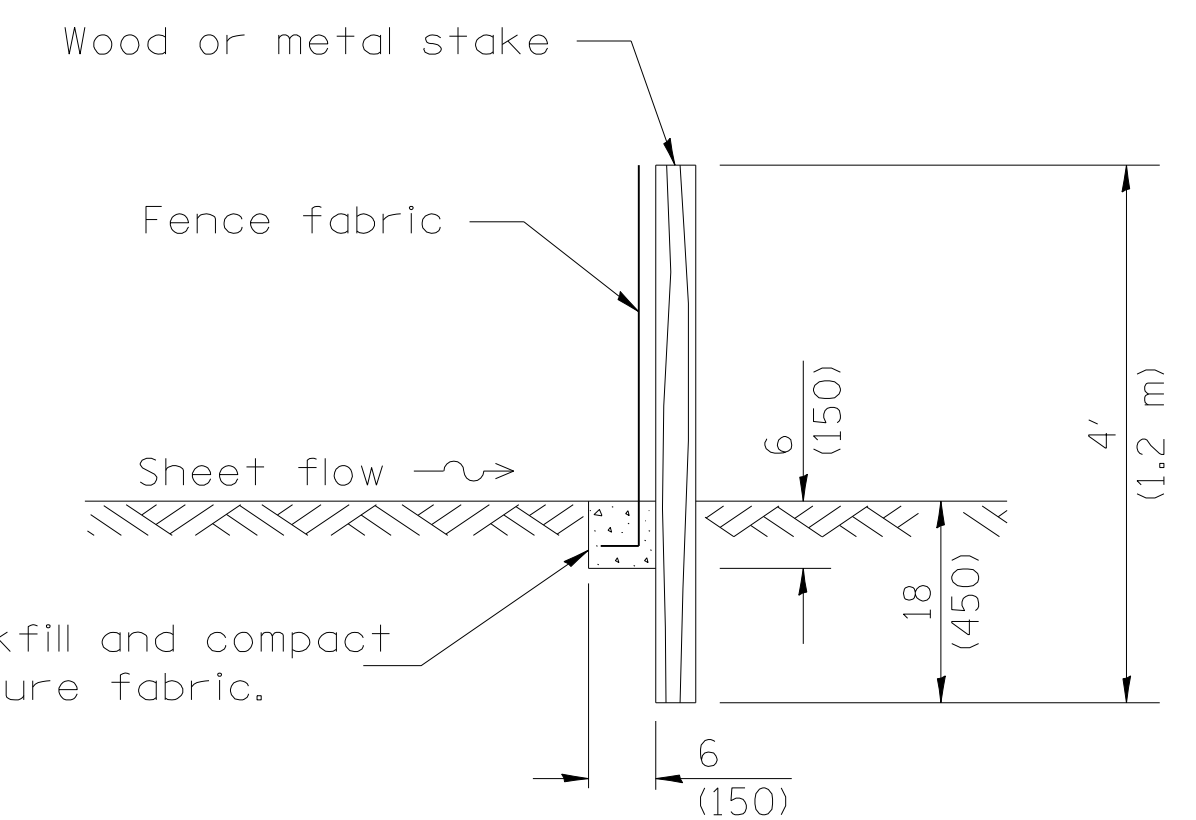


ELEVATION

SILT FILTER FENCE AS A PERIMETER EROSION BARRIER



SLICE METHOD



TRENCH METHOD

SECTION A-A

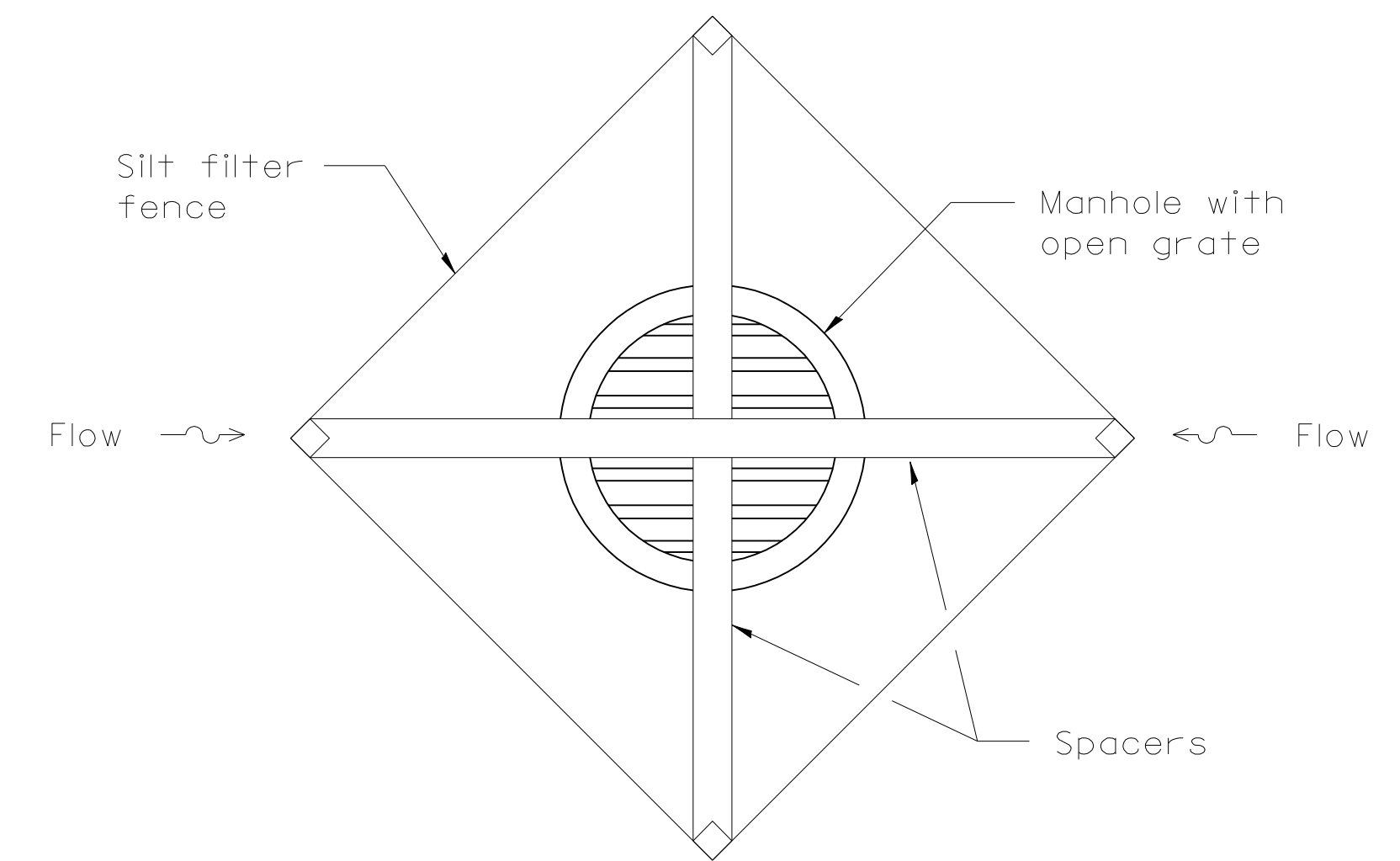
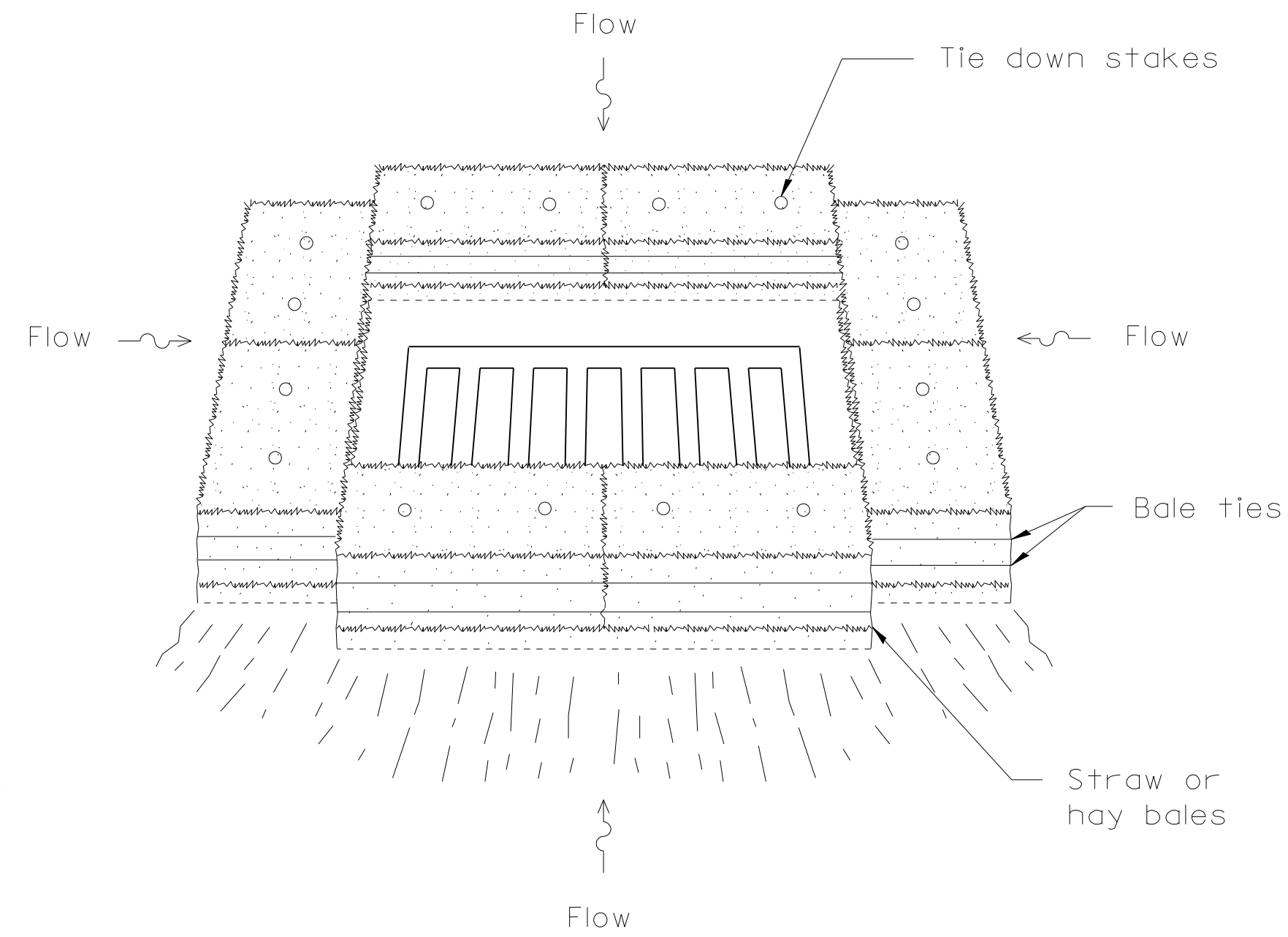
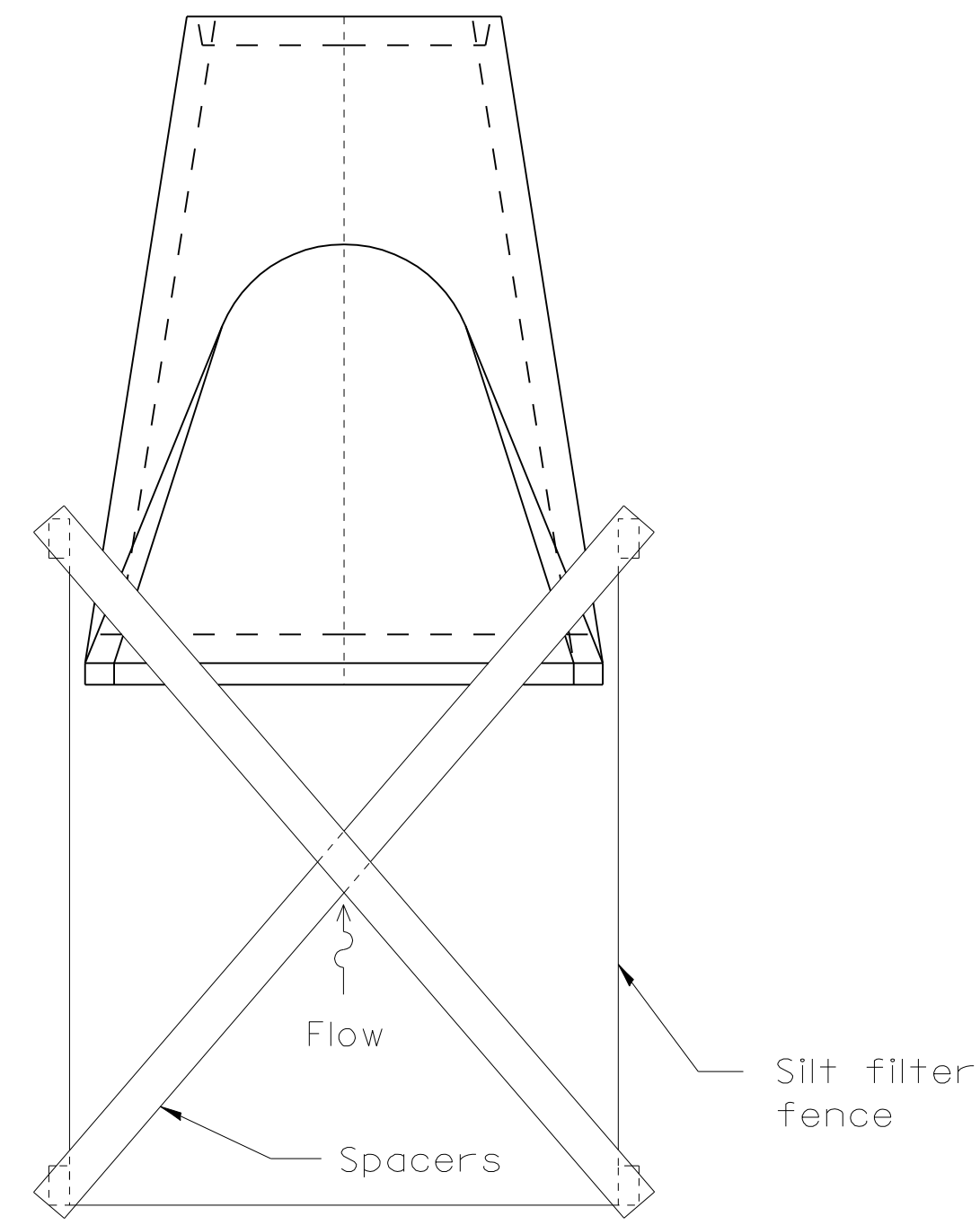
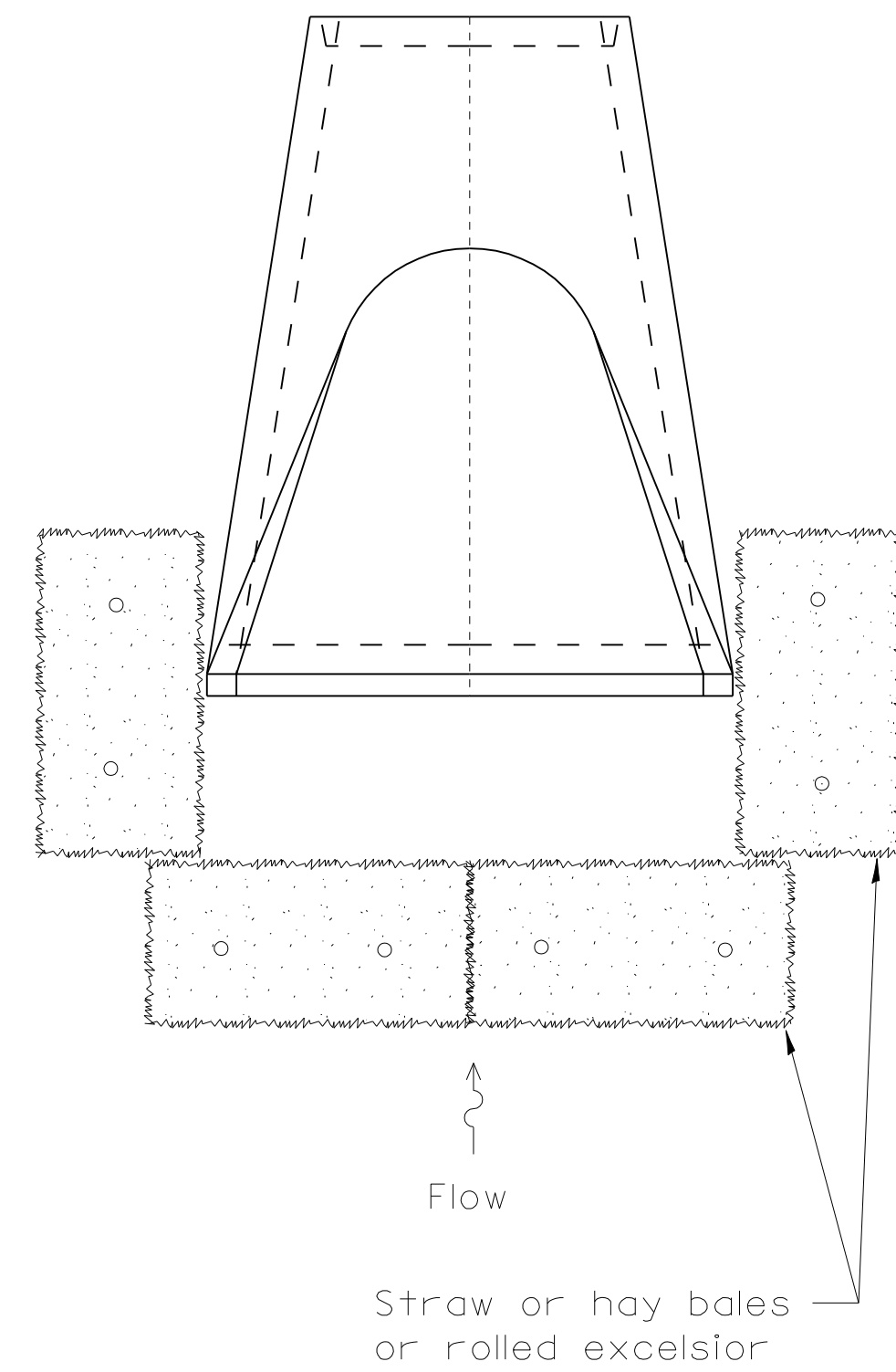
GENERAL NOTES

The installation details and dimensions shown for perimeter erosion barriers shall also apply for inlet and pipe protection.

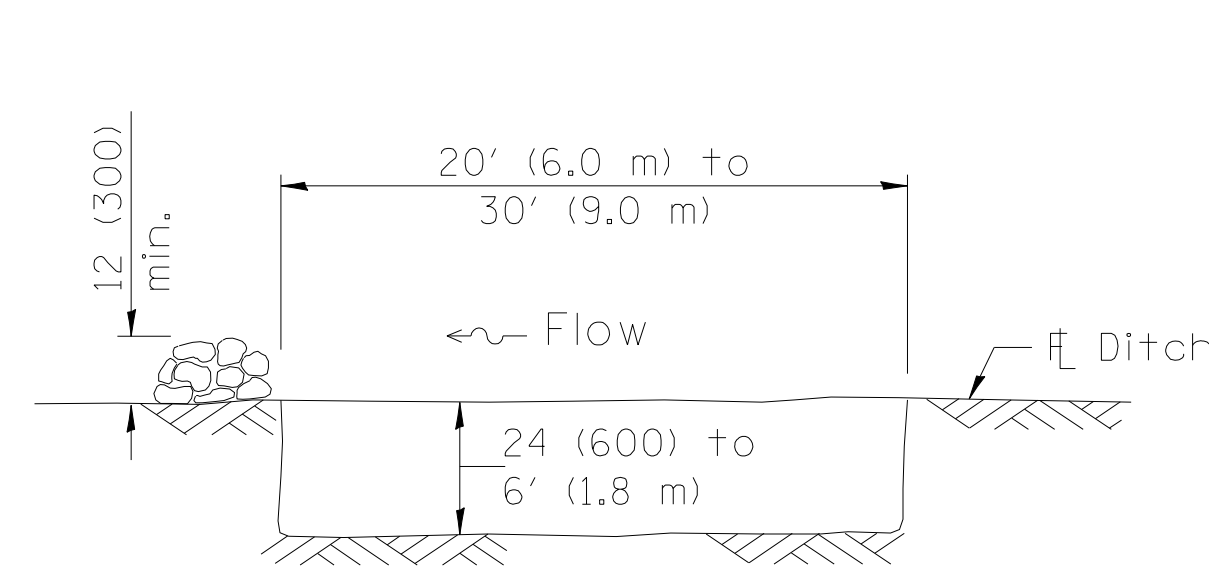
All dimensions are in inches (millimeters) unless otherwise shown.

TEMPORARY EROSION CONTROL SYSTEMS

(Sheet 1 of 2)

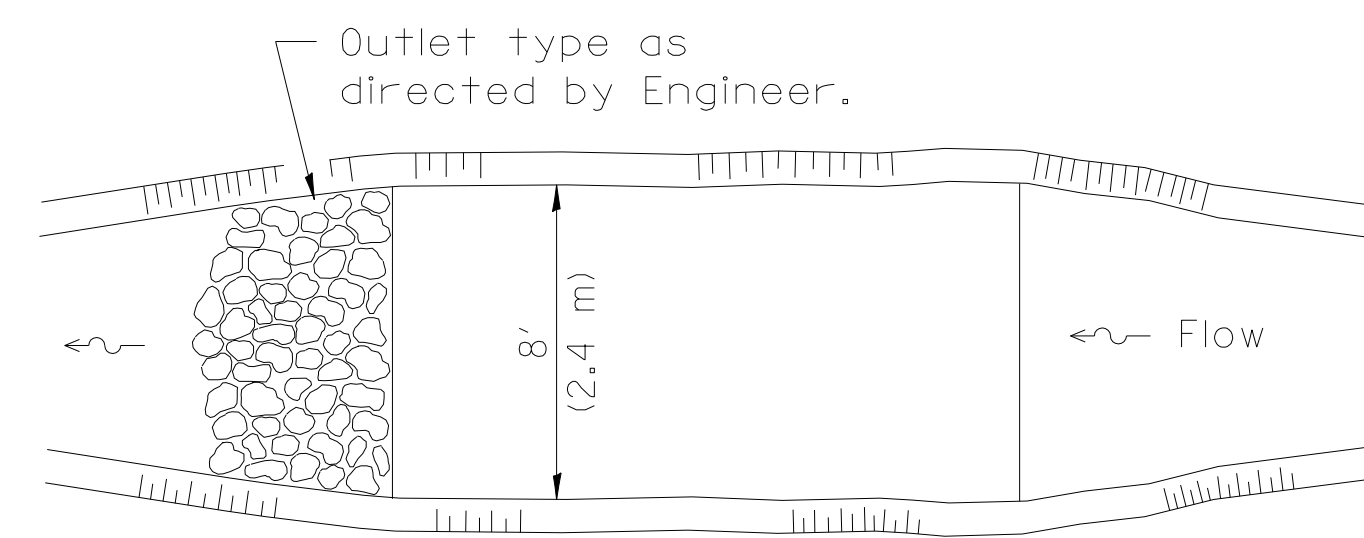


INLET AND PIPE PROTECTION



The performance of the basin will improve if put into a series.

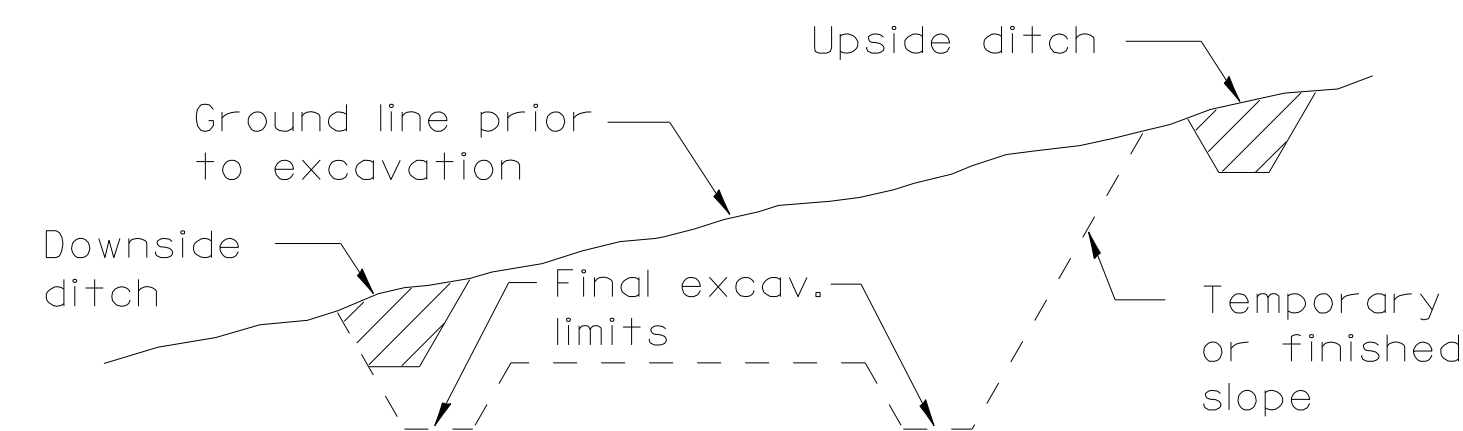
ELEVATION



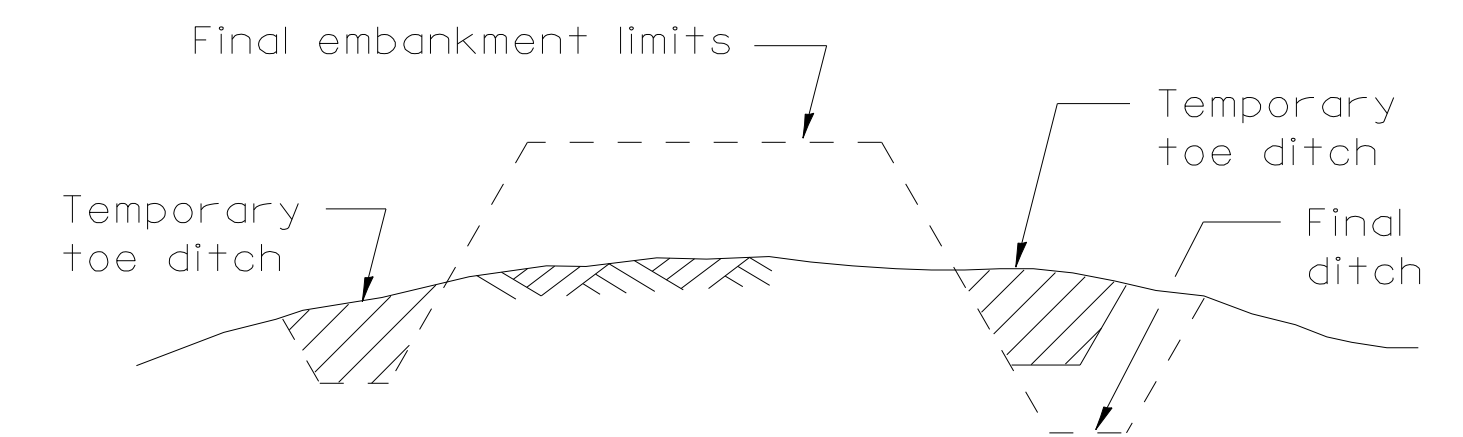
The long dimension should be parallel with the direction of the flow. Accumulated silt shall be removed anytime the basins become 75% filled.

PLAN

SEDIMENT BASIN



TYPICAL CUT CROSS-SECTION

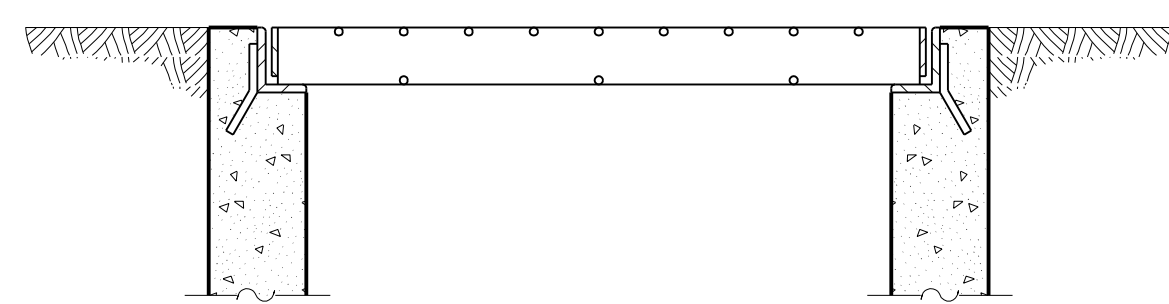
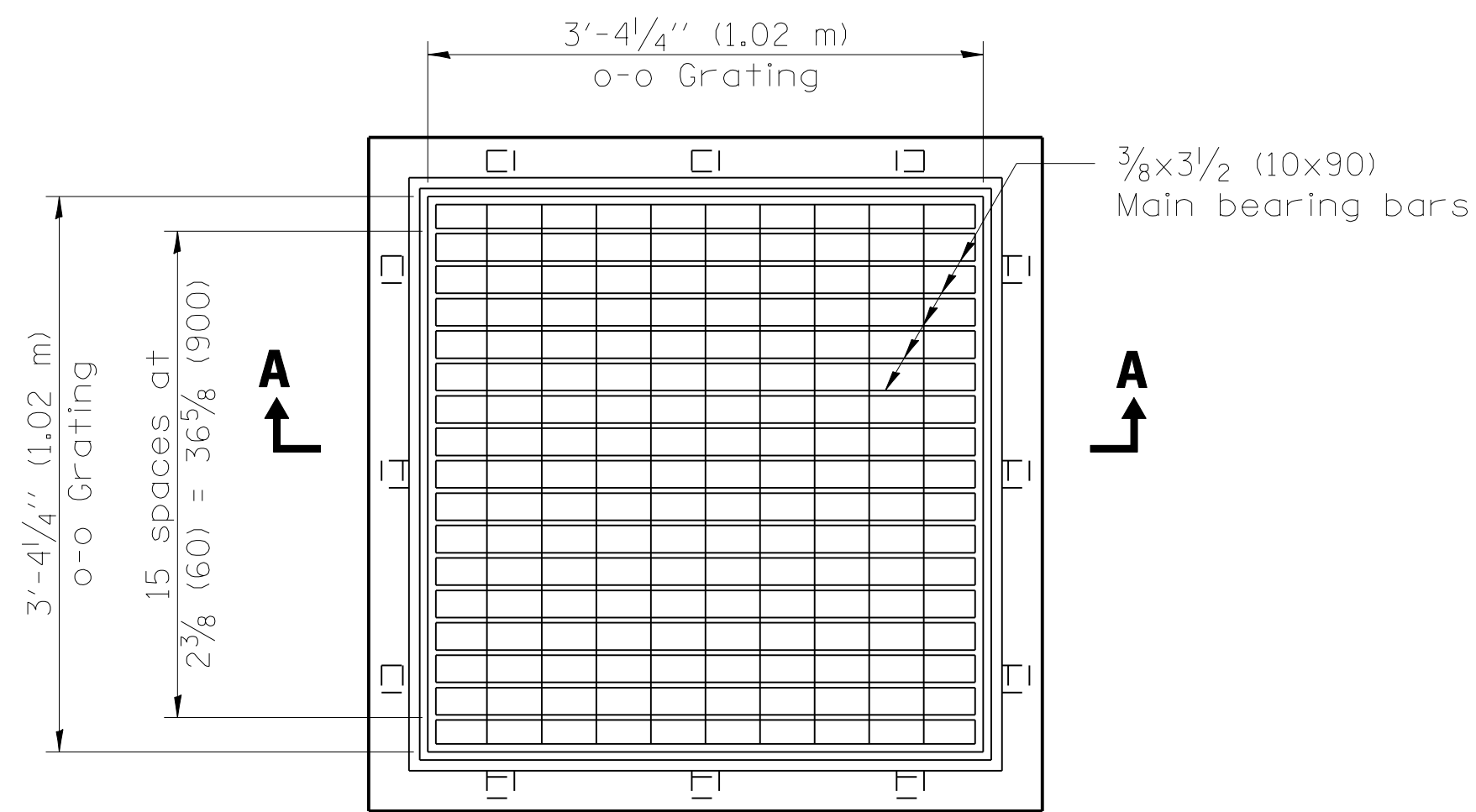


TYPICAL FILL CROSS-SECTION

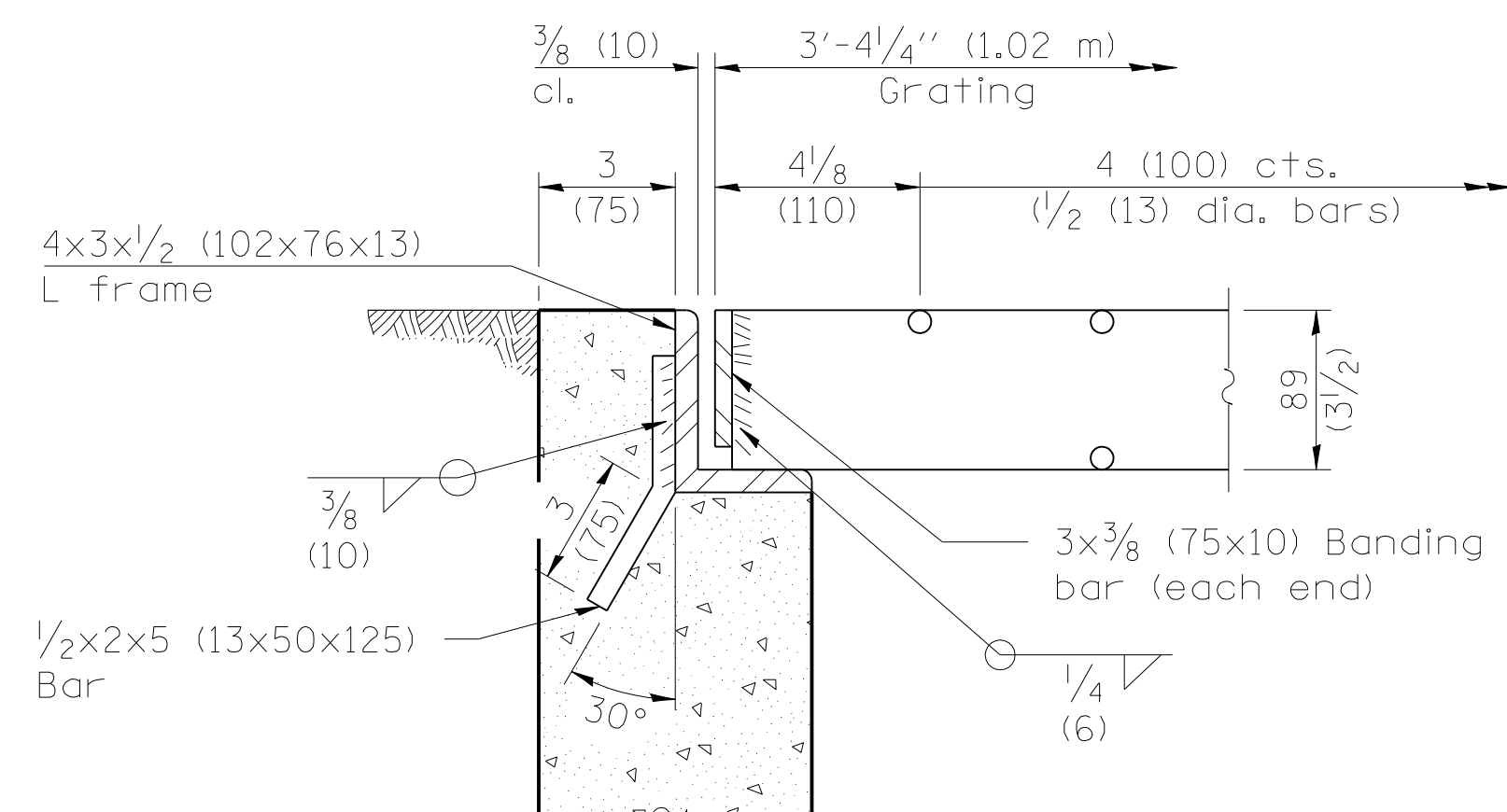
TEMPORARY DITCHES FOR CUT & FILL SECTIONS

TEMPORARY EROSION CONTROL SYSTEMS

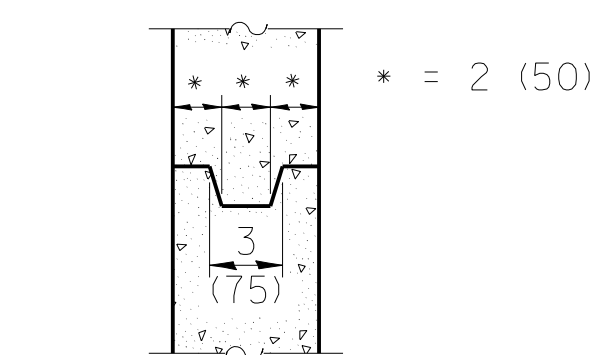
(Sheet 2 of 2)



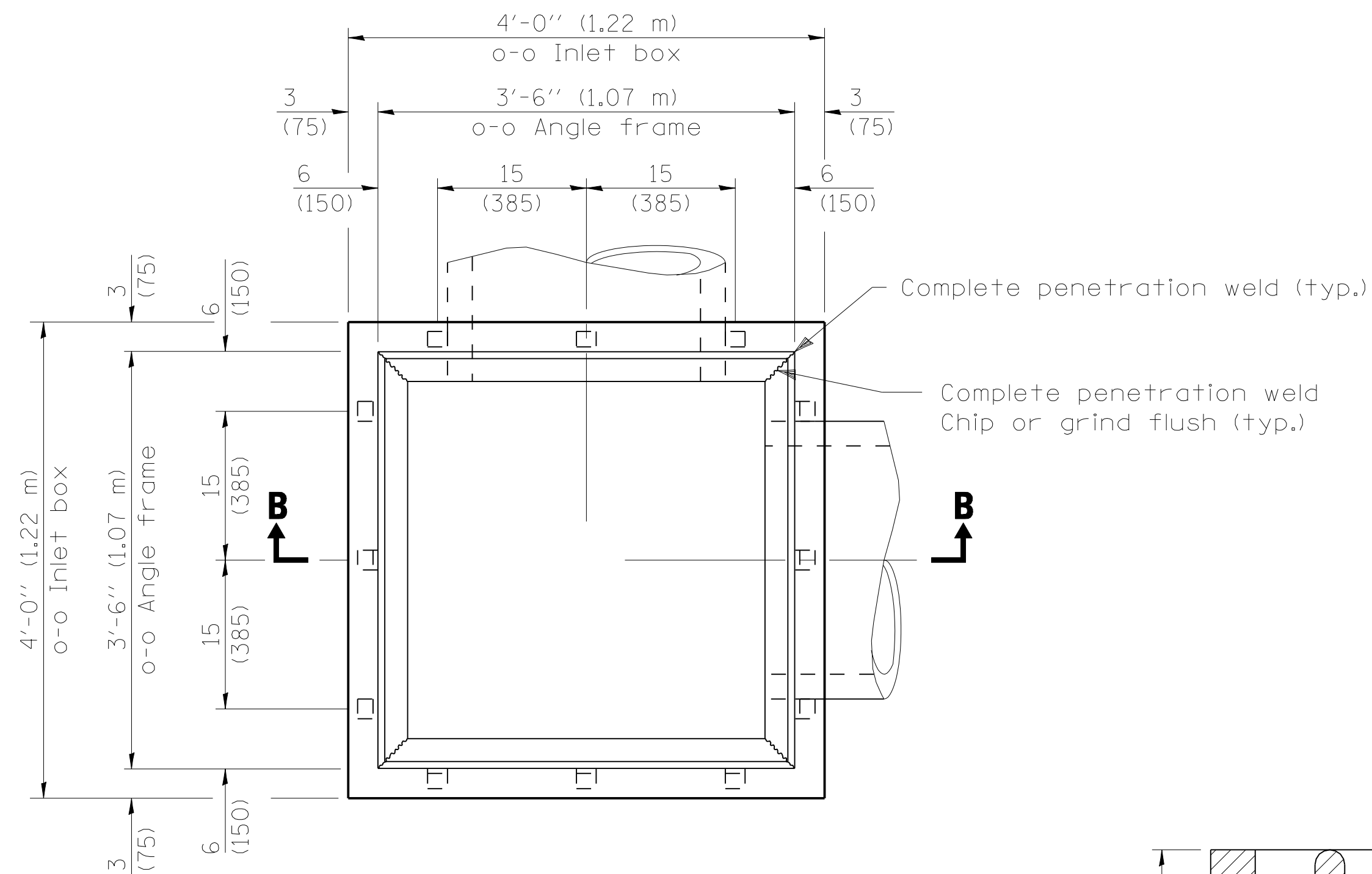
SECTION A-A



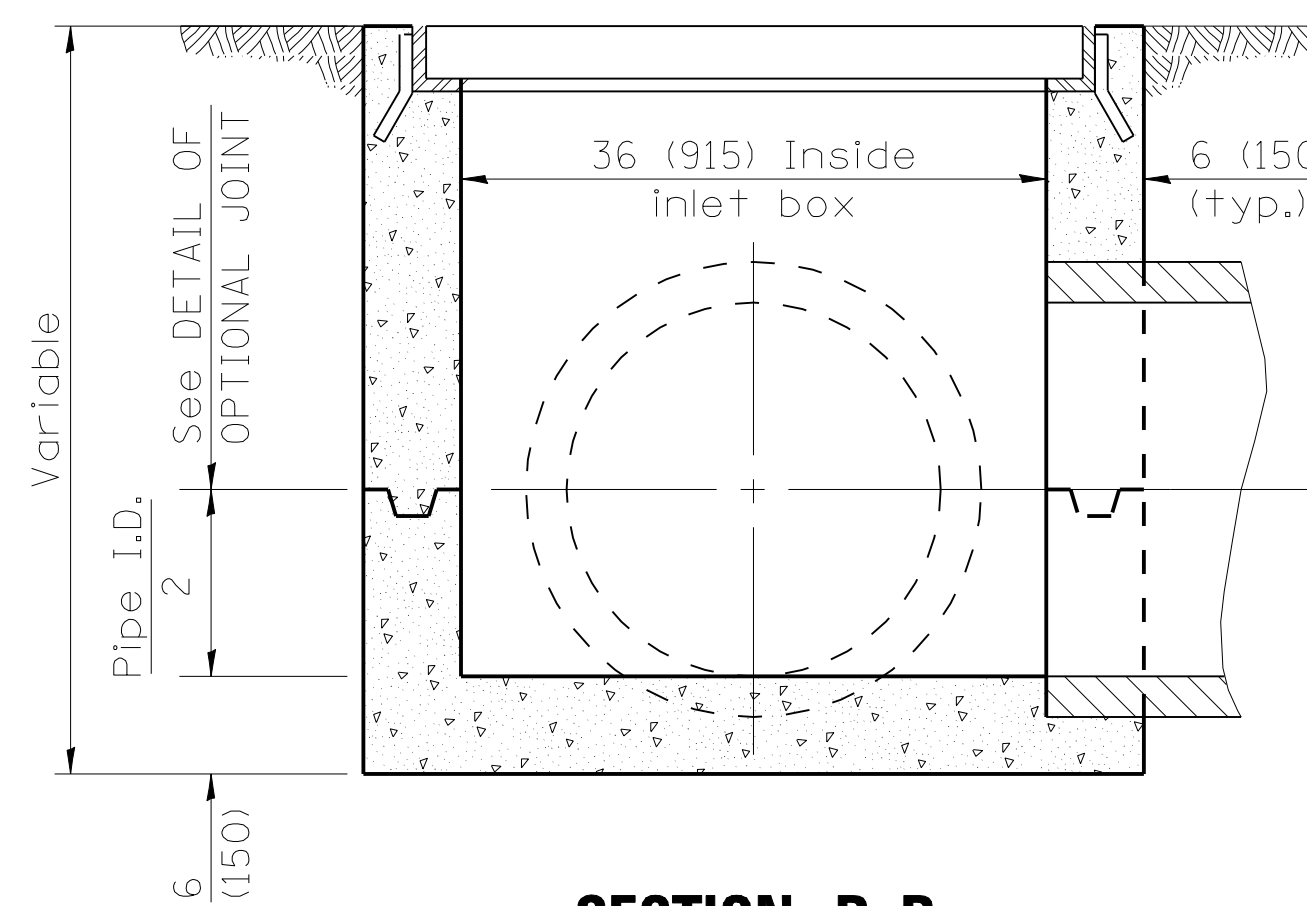
STEEL FRAME & GRATE



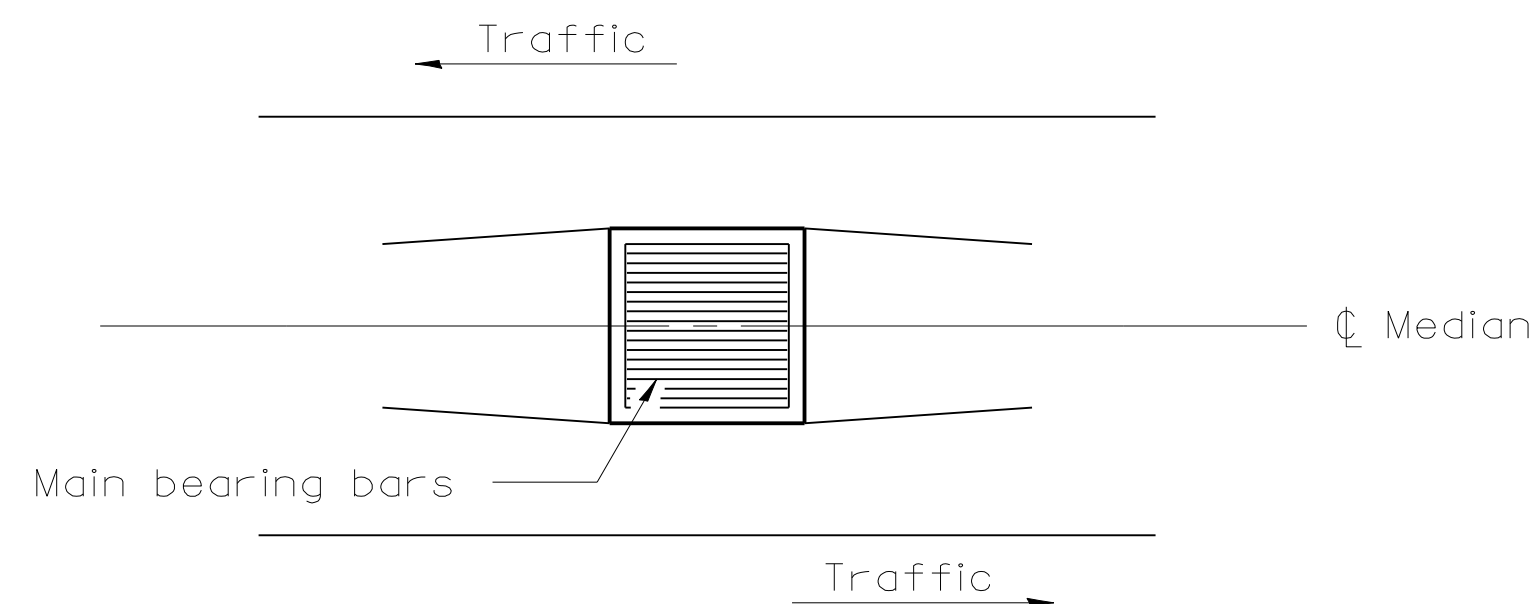
**DETAIL of
OPTIONAL JOINT**



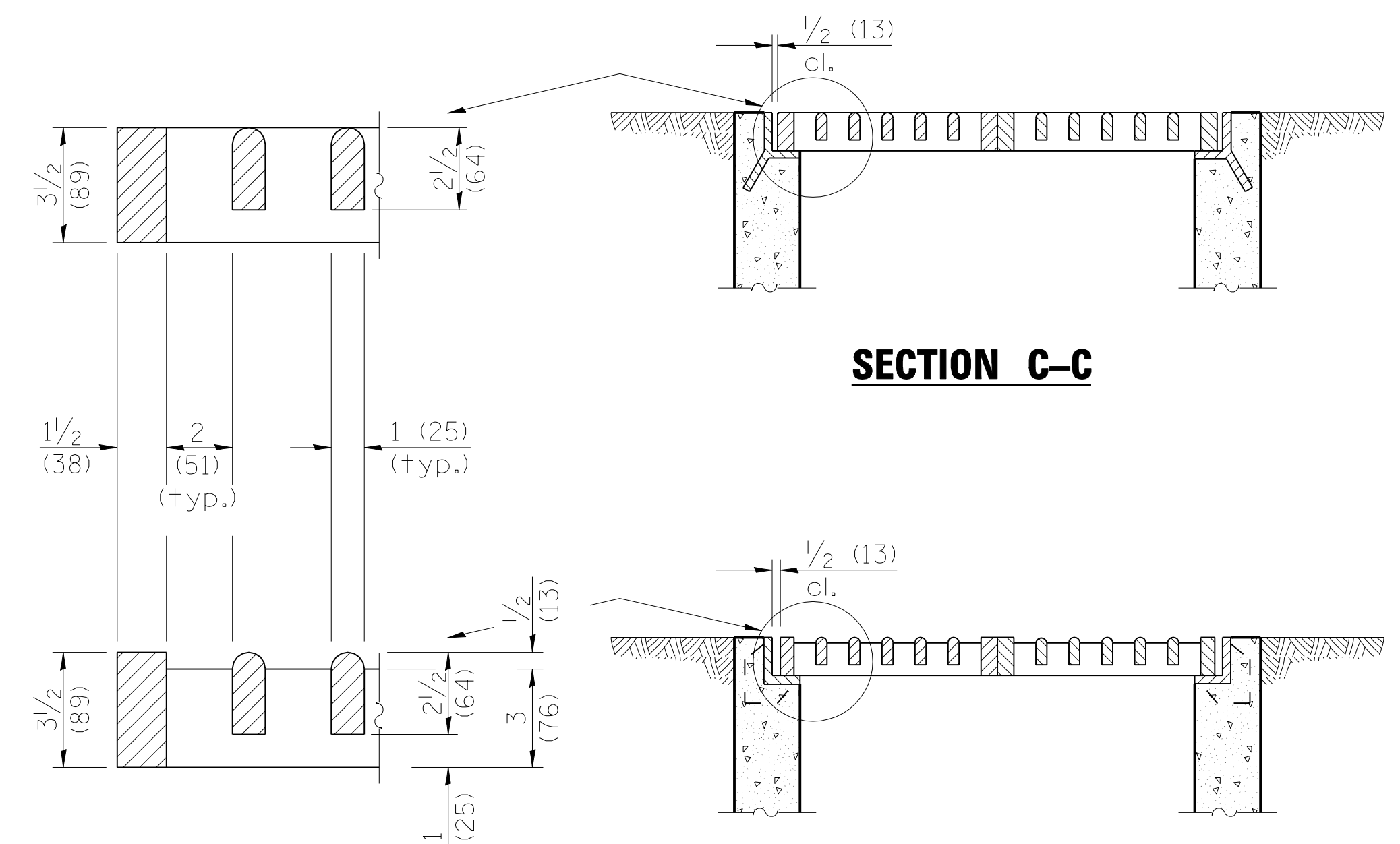
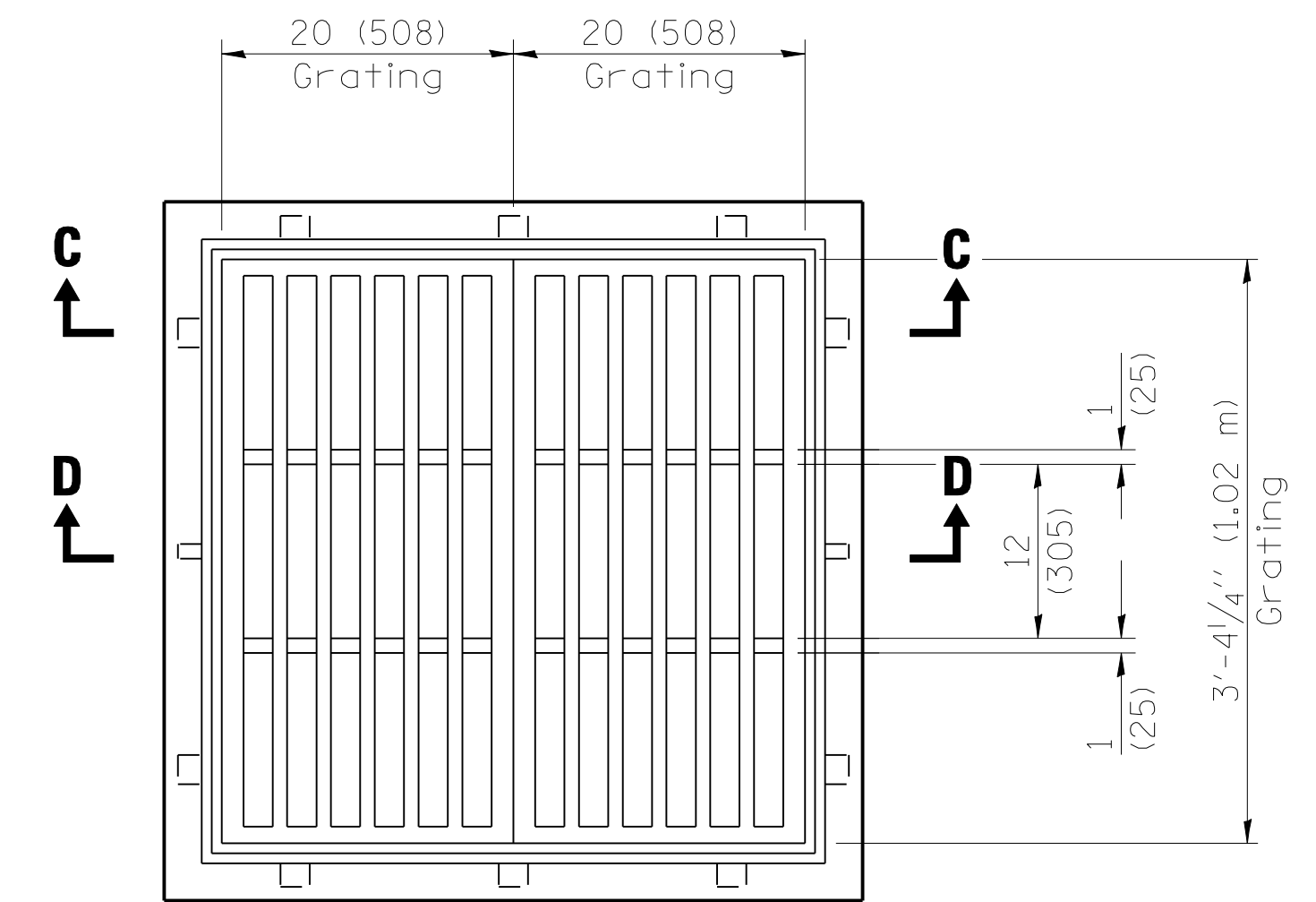
PLAN
(Grating omitted for clarity)



SECTION B-B



Sketch showing location and direction of
main bearing bars in relation to Median

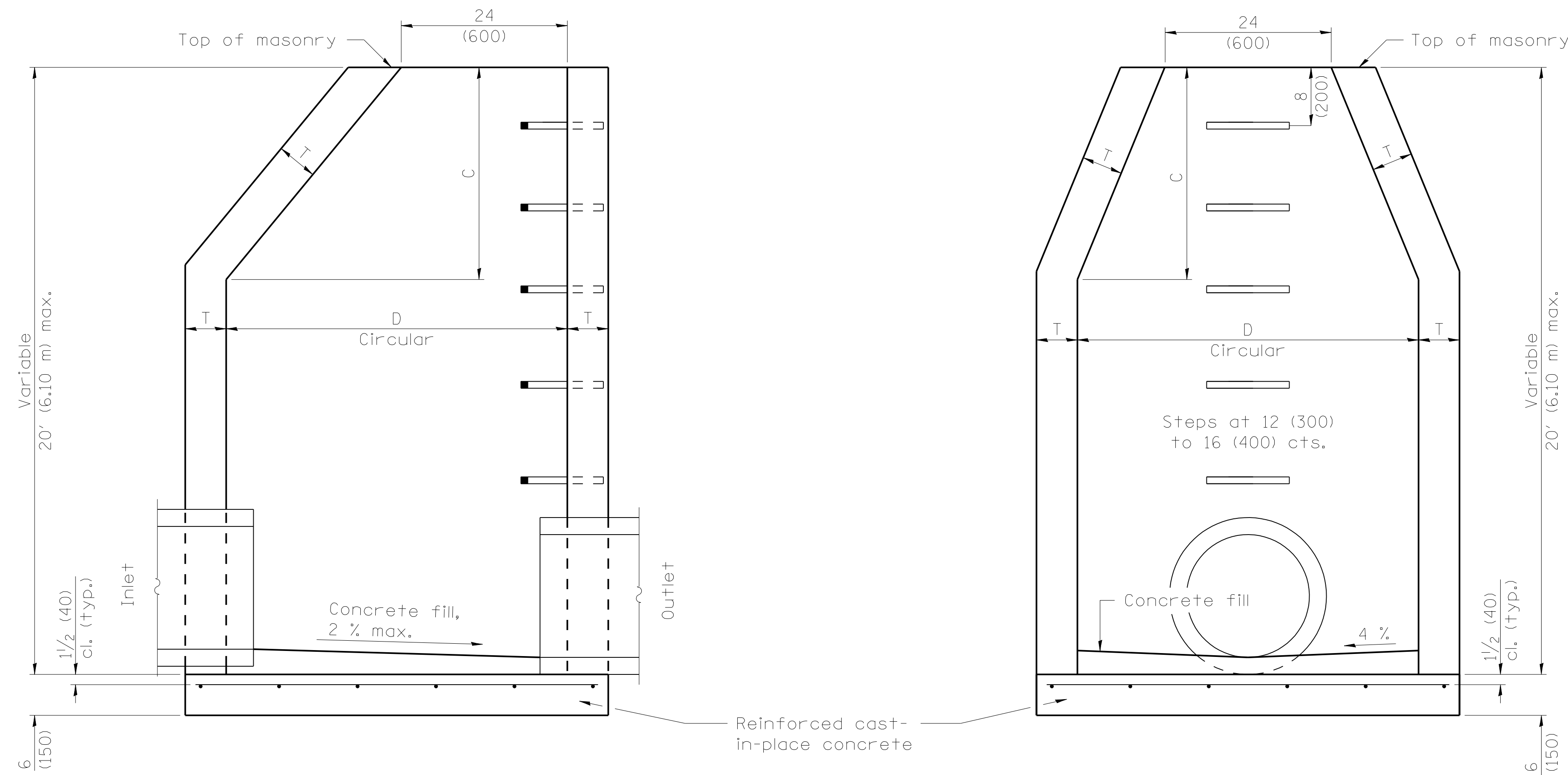


SECTION C-C

**SECTION D-D
CAST FRAME & GRATE**

All dimensions are in inches (millimeters)
unless otherwise shown.

**FLUSH INLET BOX
FOR MEDIAN**

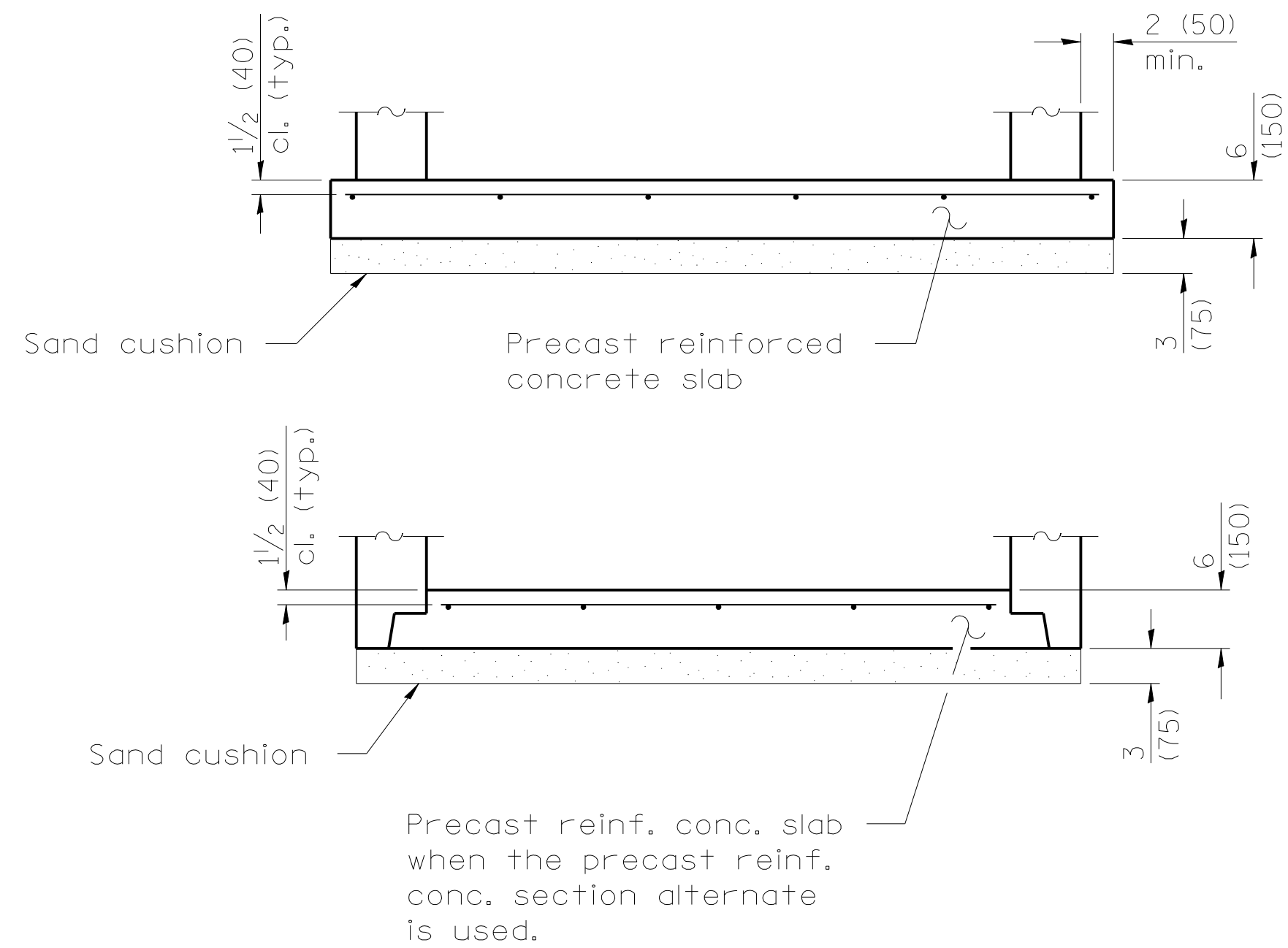


ELEVATION - ECCENTRIC

ELEVATION - CONCENTRIC

ALTERNATE MATERIALS FOR WALLS	D	C*	T (min.)
Concrete Masonry Unit	4'-0" (1.2 m)	30 (750)	5 (125)
	5'-0" (1.5 m)	3'-9" (1.15 m)	5 (125)
Brick Masonry	4'-0" (1.2 m)	30 (750)	8 (200)
	5'-0" (1.5 m)	3'-9" (1.15 m)	8 (200)
Precast Reinforced Concrete Section	4'-0" (1.2 m)	30 (750)	4 (100)
	5'-0" (1.5 m)	3'-9" (1.15 m)	5 (125)
Cast-in-place Concrete	4'-0" (1.2 m)	30 (750)	6 (150)
	5'-0" (1.5 m)	3'-9" (1.15 m)	6 (150)

* For precast reinforced concrete sections, dimension "C" may vary from the dimension given to plus 6 (150).



ALTERNATE BOTTOM SLAB

GENERAL NOTES

Bottom slabs shall be reinforced with a minimum of 0.31 sq. in./ft. (660 sq. mm/m) in both directions with a maximum spacing of 12 (300).

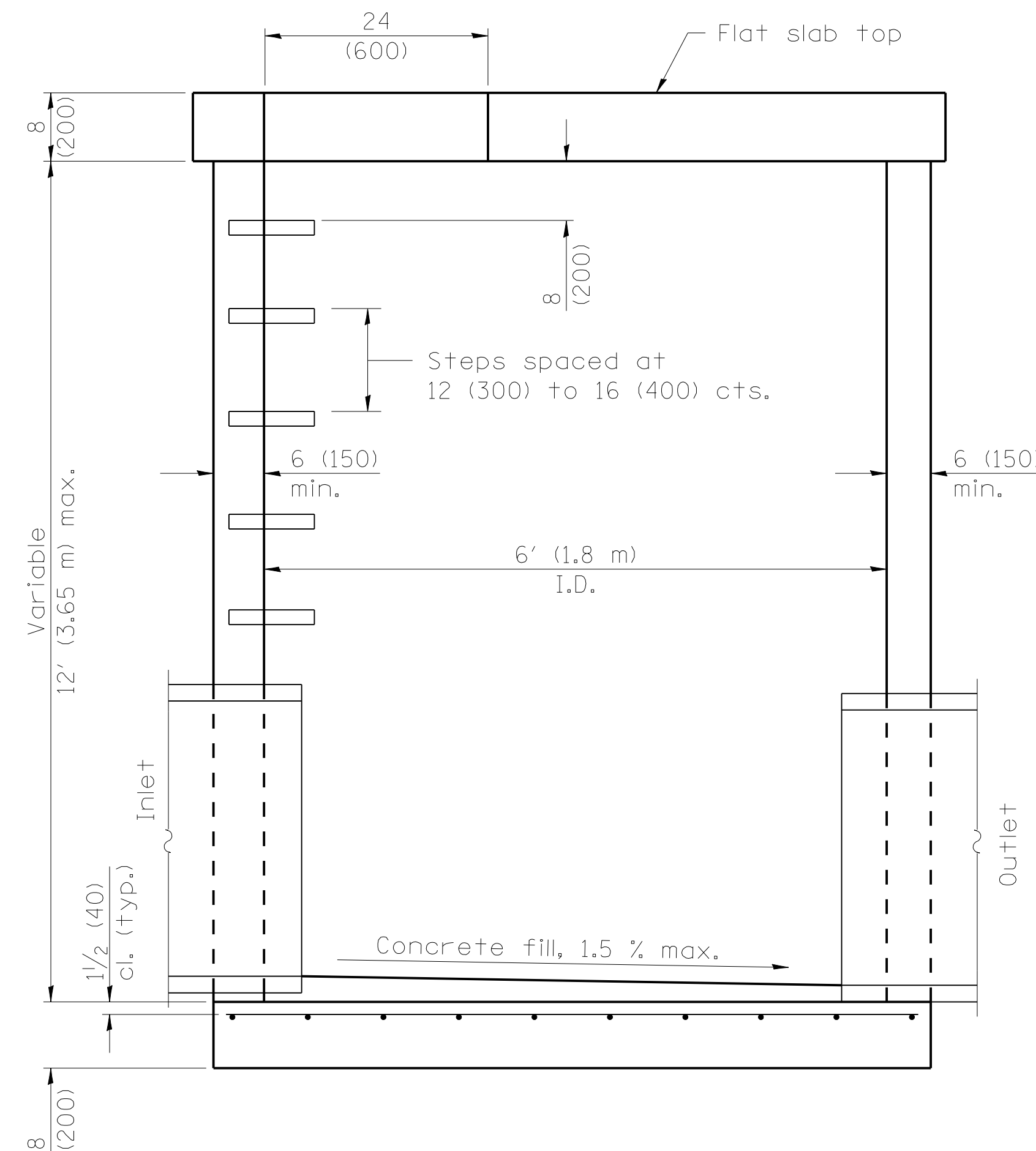
Bottom slabs may be connected to the riser as determined by the fabricator; however, only a single row of reinforcement around the perimeter may be utilized.

See Standard 602701 for details of steps.

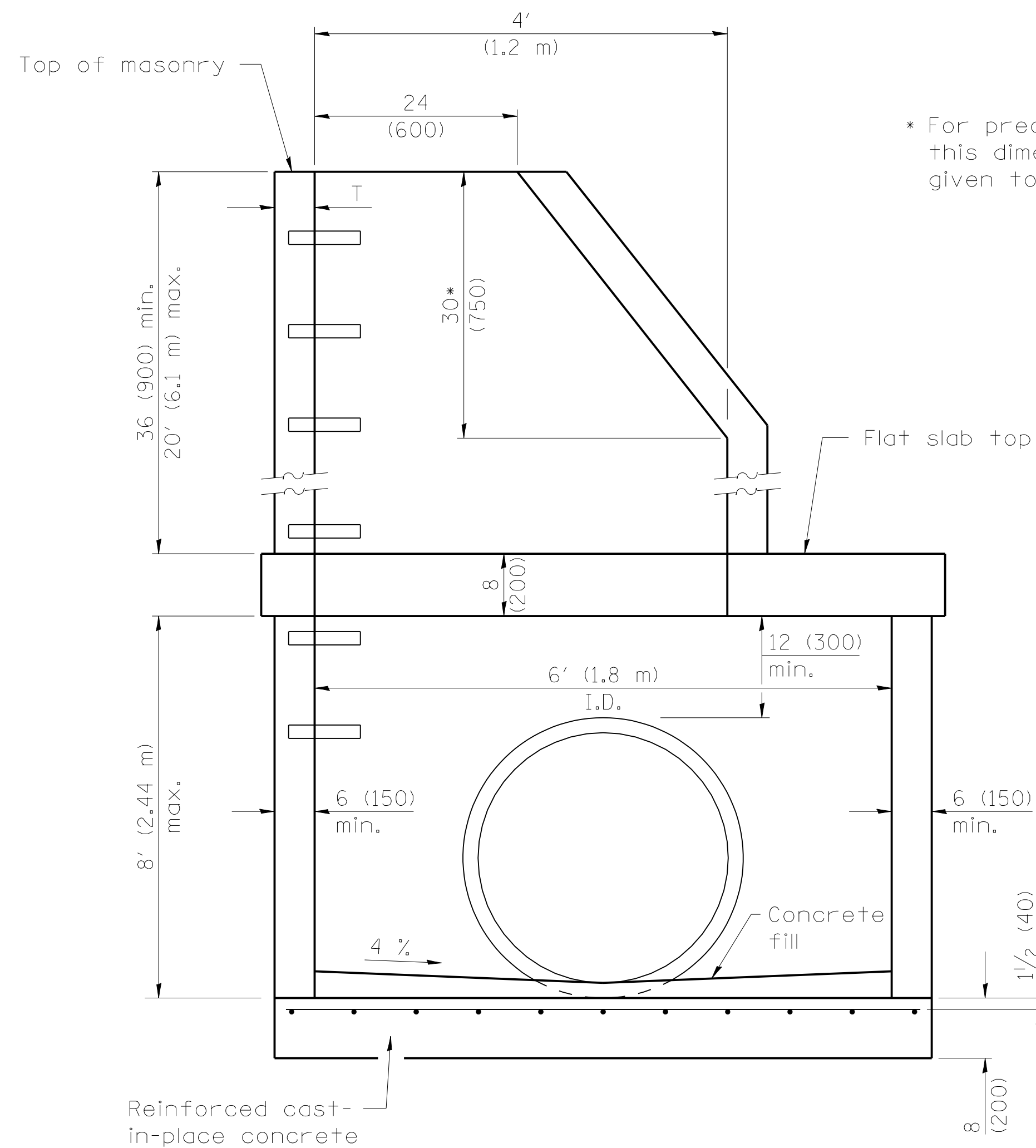
See Standard 602601 for optional Precast Reinforced Concrete Flat Slab Top.

All dimensions are in inches (millimeters) unless otherwise shown.

MANHOLE TYPE A

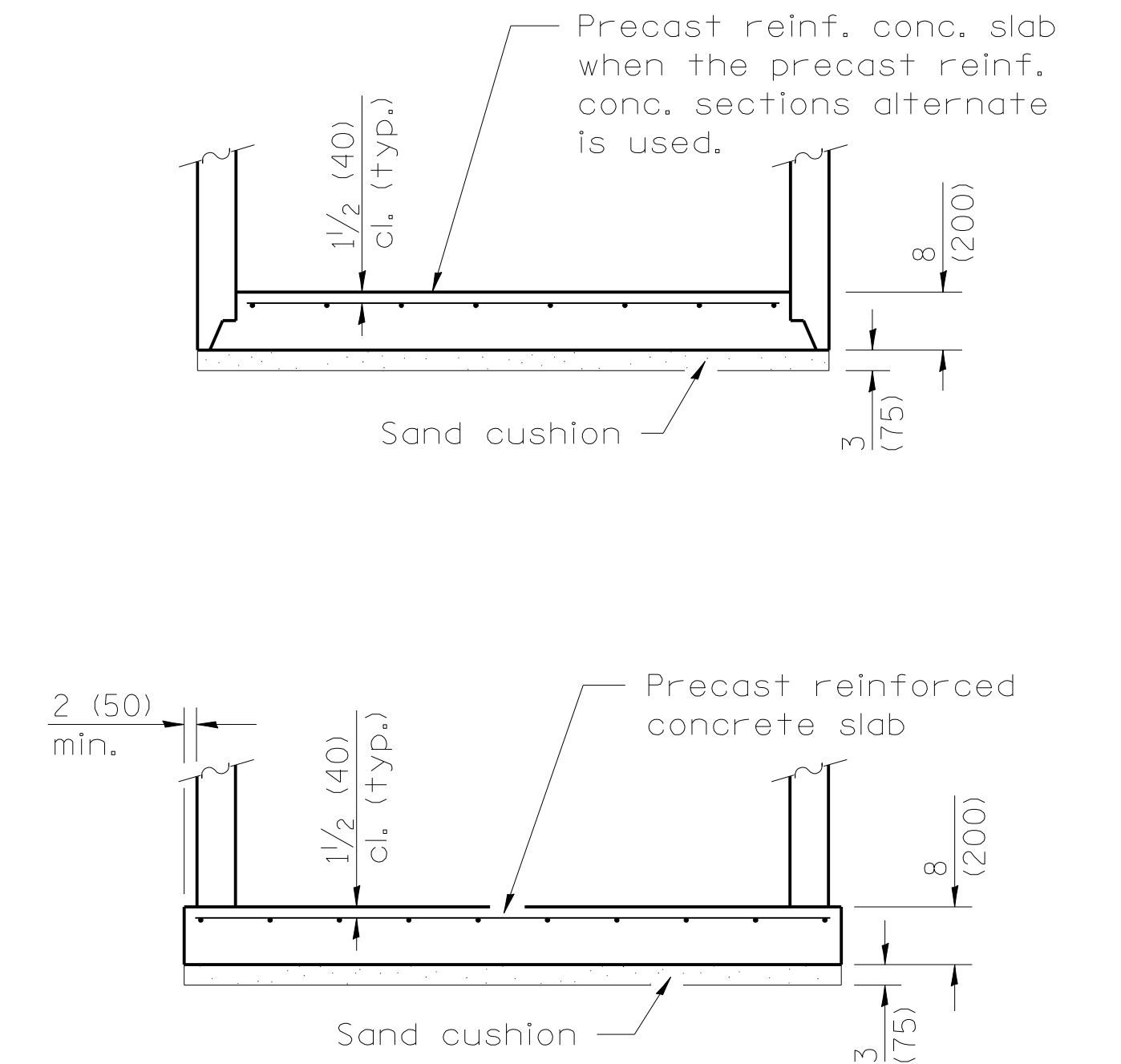


ELEVATION
(with flat slab top only)

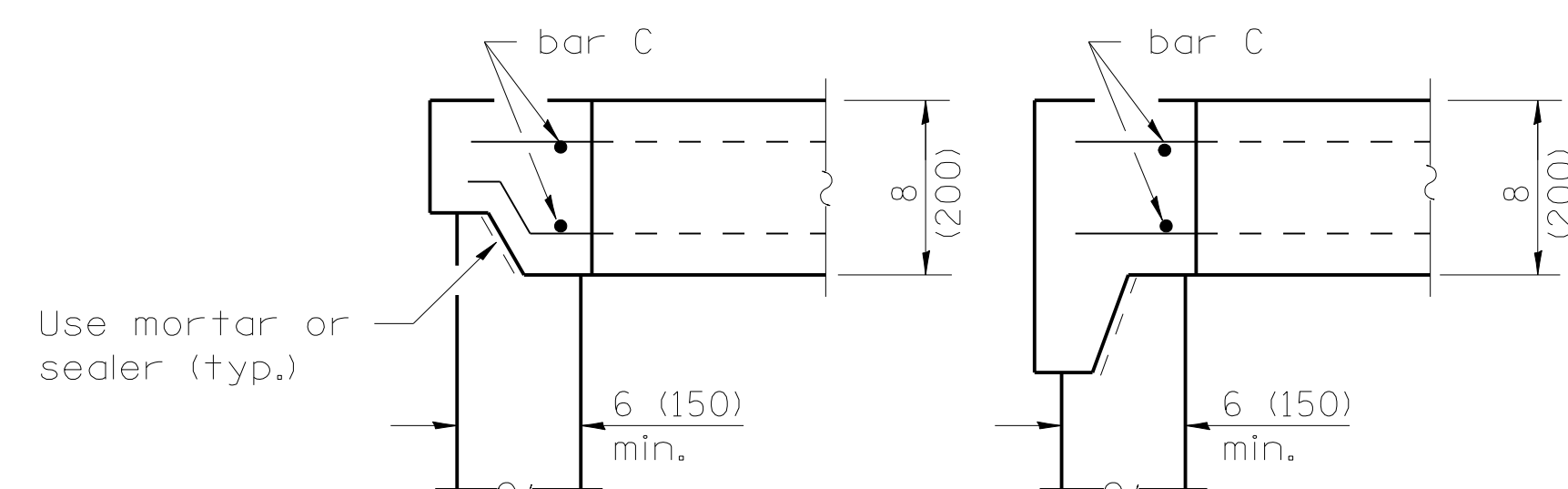


ELEVATION
(with flat slab top and riser)

* For precast reinforced concrete sections, this dimension may vary from the dimension given to plus 6 (150).



ALTERNATE BOTTOM SLABS



ALTERNATE JOINT CONFIGURATIONS

ALTERNATE MATERIALS FOR WALLS	T (min)
Concrete Masonry Units	5 (125)
Precast Reinforced Concrete Sections	4 (100)
Cast-in-Place Concrete	6 (150)

GENERAL NOTES

Joint configuration and dimensions of flat slab top shall match and fit the riser joint detail.

Lifting devices shall be approved by the Engineer.

Bottom slabs shall be reinforced with a minimum of 0.37 sq. in./ft. (780 sq. mm /m) in both directions with a maximum spacing of 10 (250)

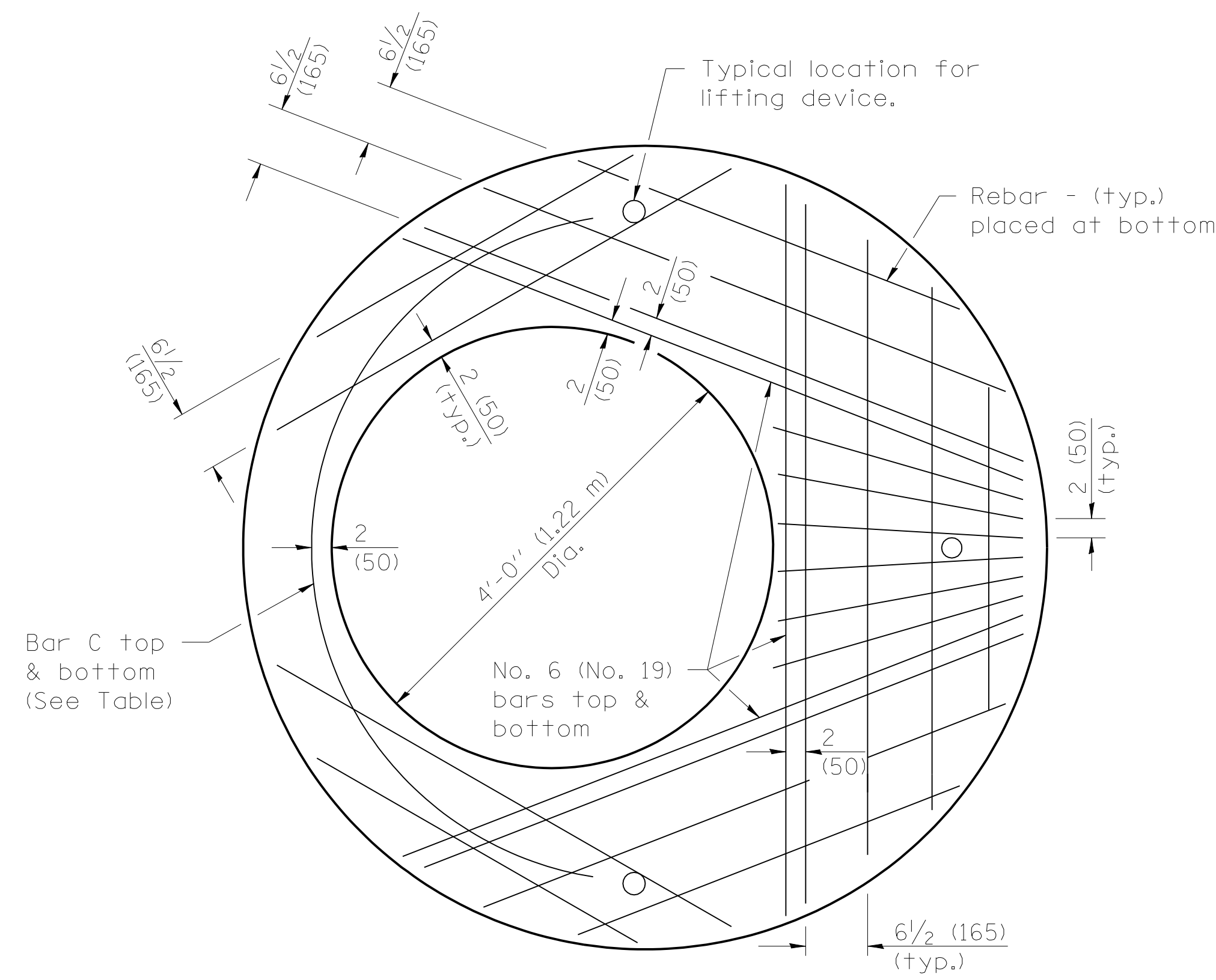
Bottom slabs may be connected to the riser as determined by the fabricator; however, only a single row of reinforcement around the perimeter may be utilized.

See Standard 602701 for details of manhole steps.

All dimensions are in inches (millimeters) unless otherwise shown.

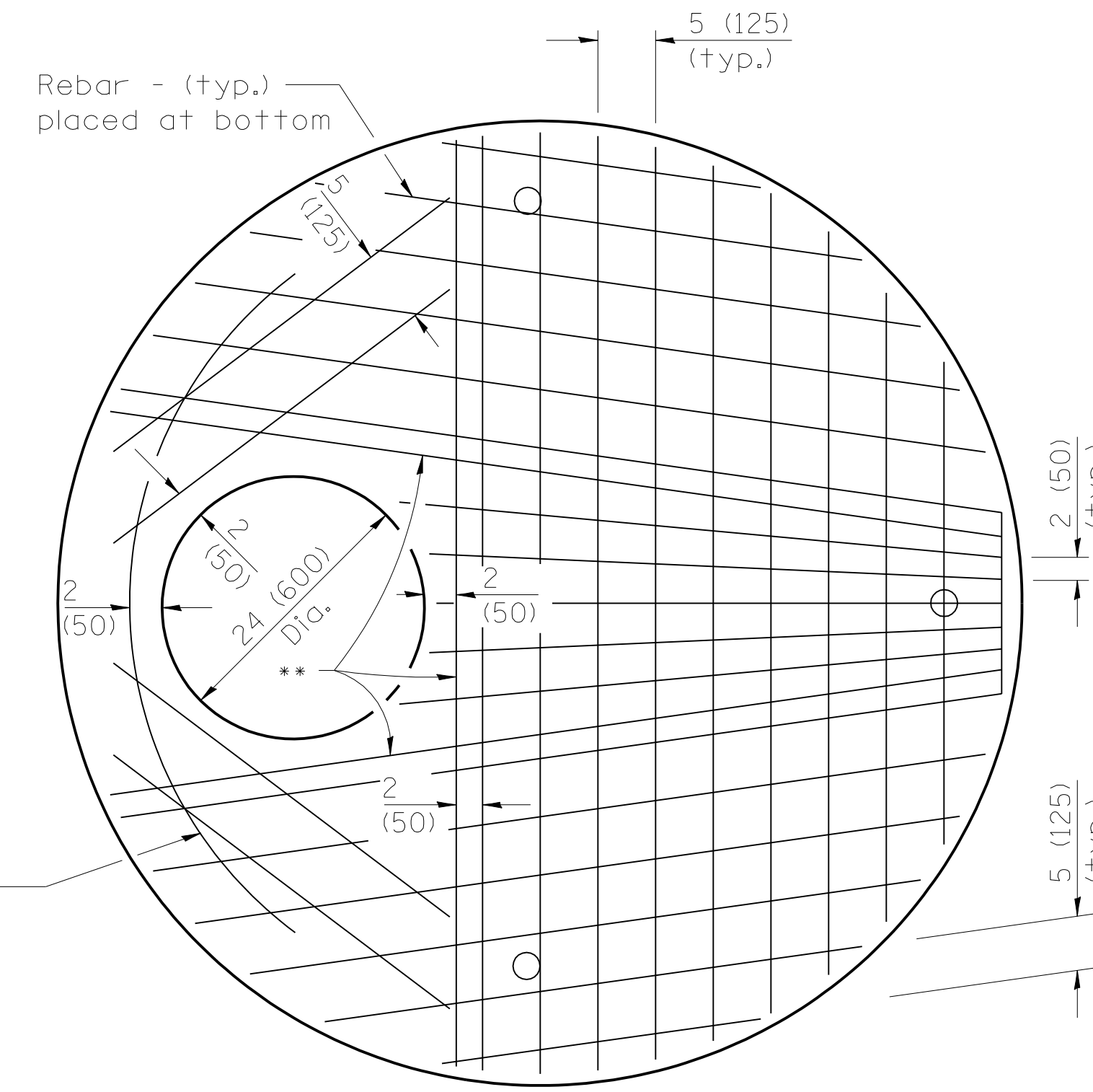
MANHOLE TYPE A
6' (1.8 m) DIAMETER

(Sheet 1 of 2)



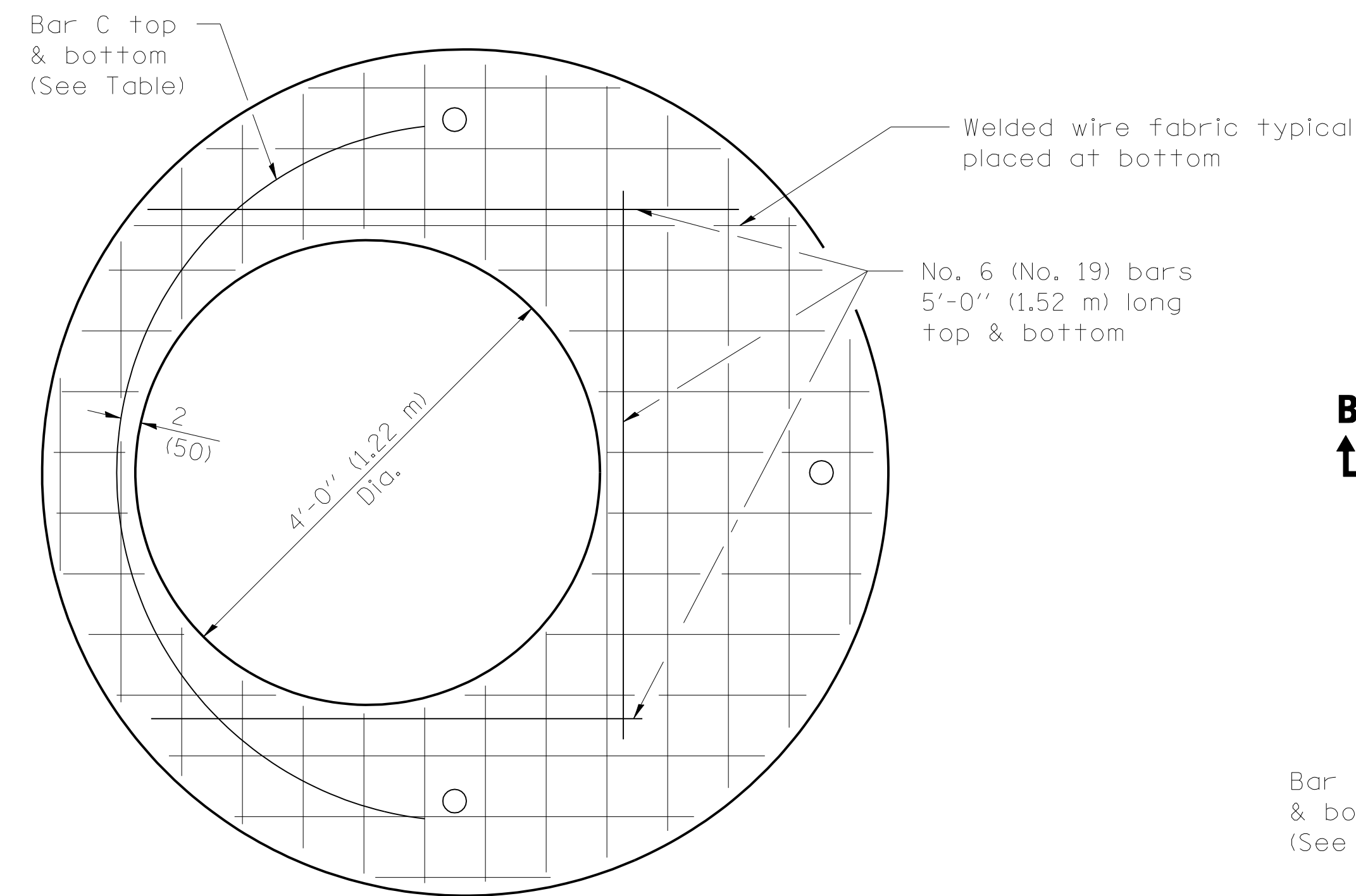
PLAN

Showing Rebar Reinforcement



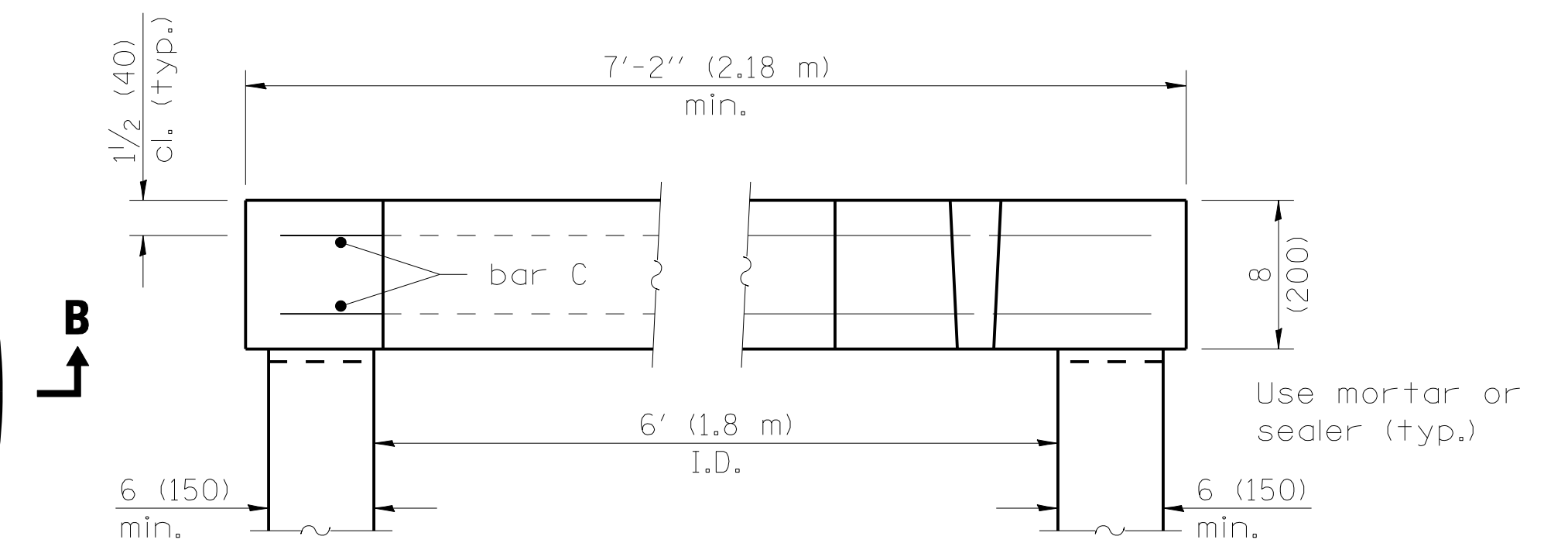
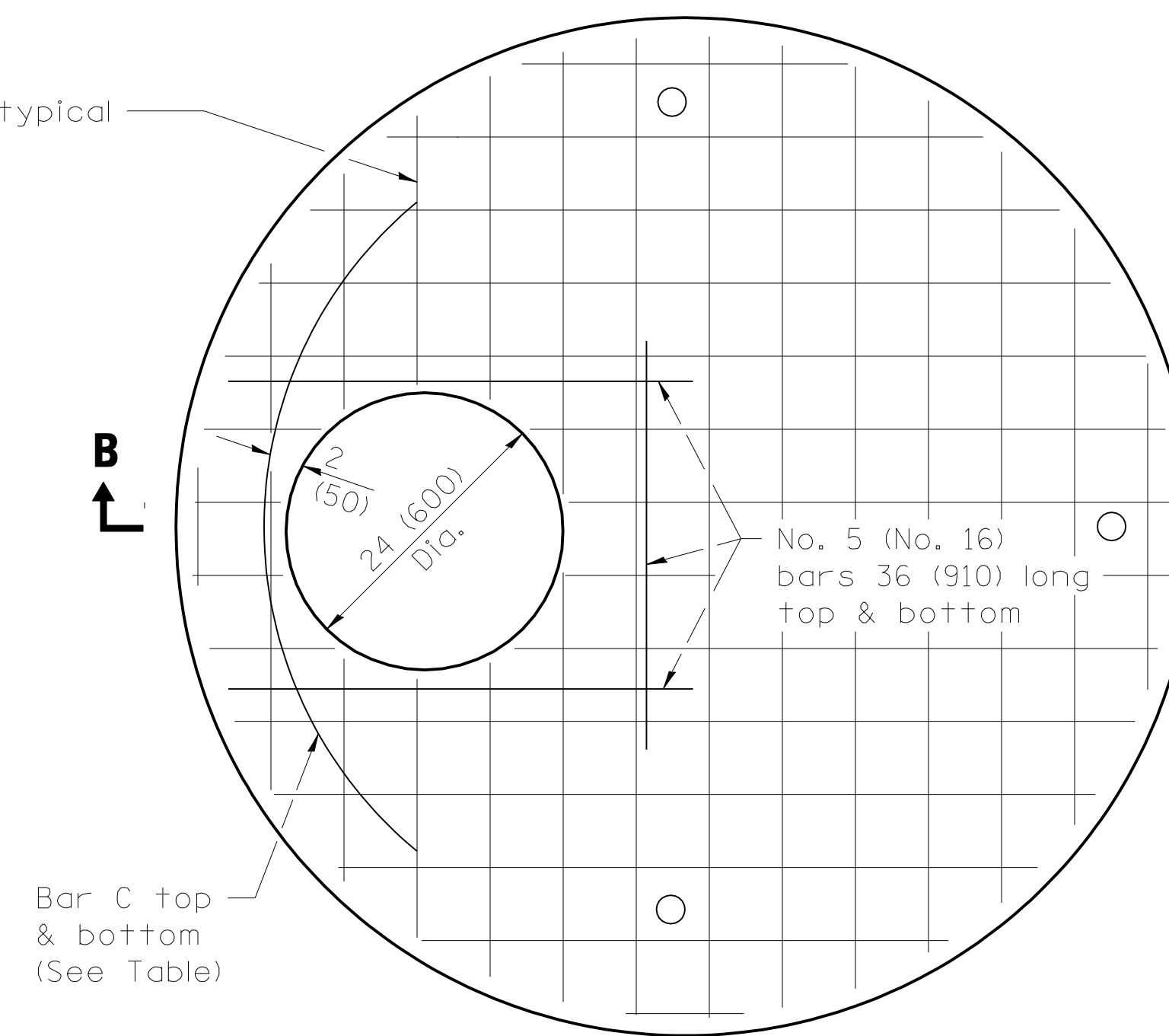
** No. 6 (No. 19) bars top & bottom

Diameter of opening	Thickness	Reinforcement "As" WWF Each direction	Bar Size	No. 4 (No. 13) Bar C	
				Length	Radius
24 (600)	8 (200)	1.06 sq. in./ft. (2244 sq. mm/m)	No. 6 (No. 19)	6'-0" (1.83 m)	38 (965)
4'-0" (1.2 m)	8 (200)	0.82 sq. in./ft. (1736 sq. mm/m)	No. 6 (No. 19)	9'-0" (2.74 m)	38 (965)



PLAN

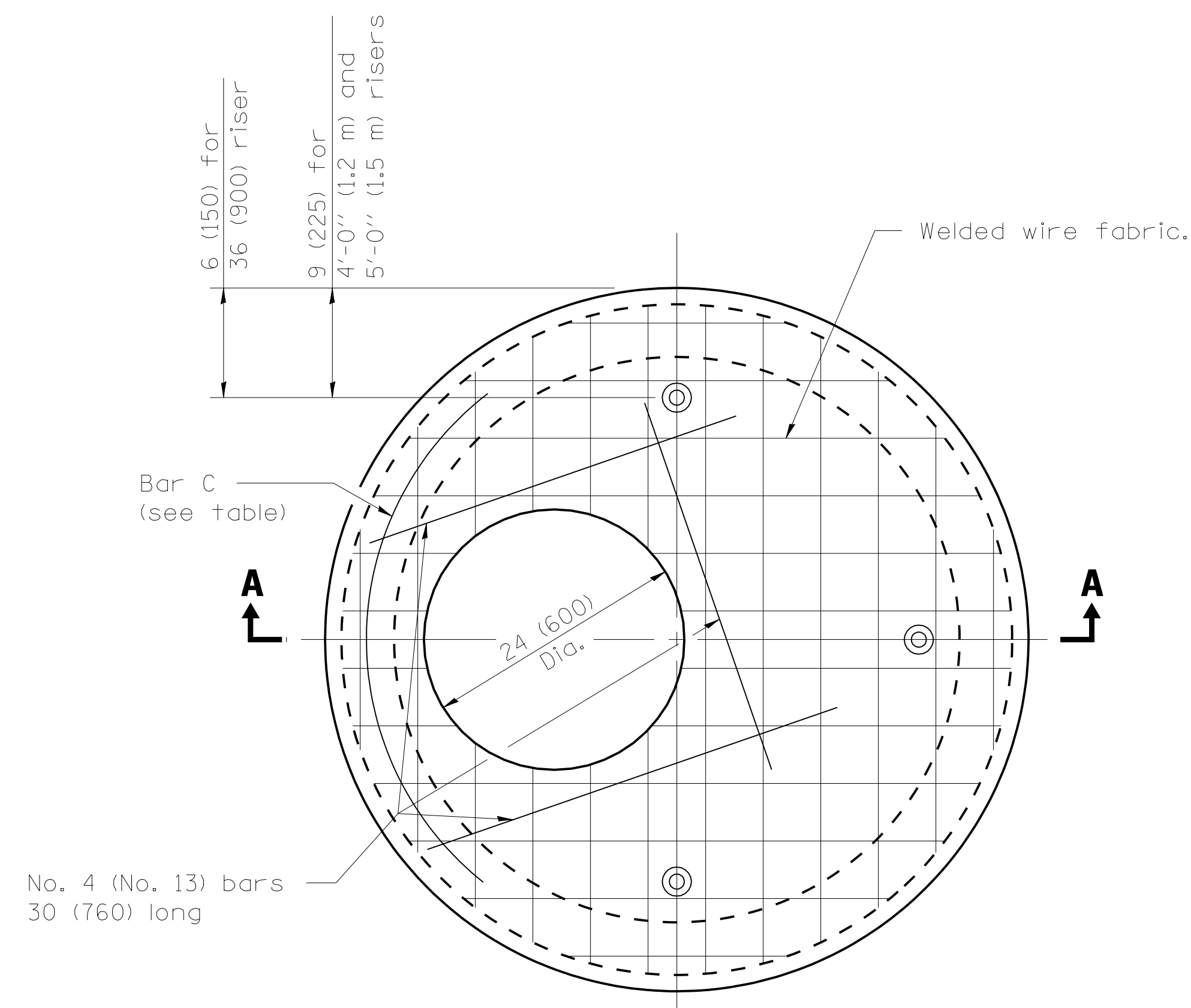
Showing Welded Wire Fabric Reinforcement



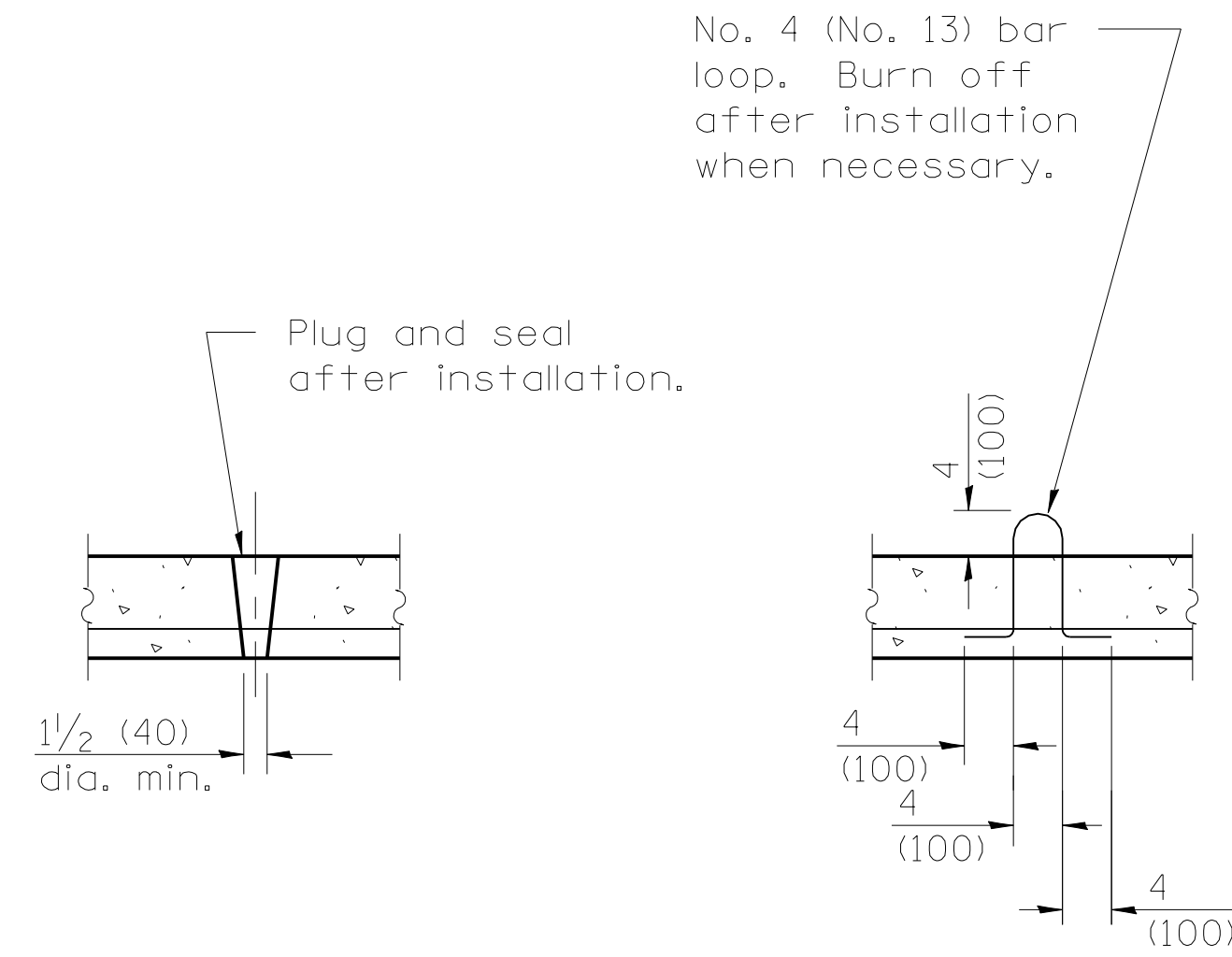
SECTION B-B

**MANHOLE TYPE A
6' (1.8 m) DIAMETER**

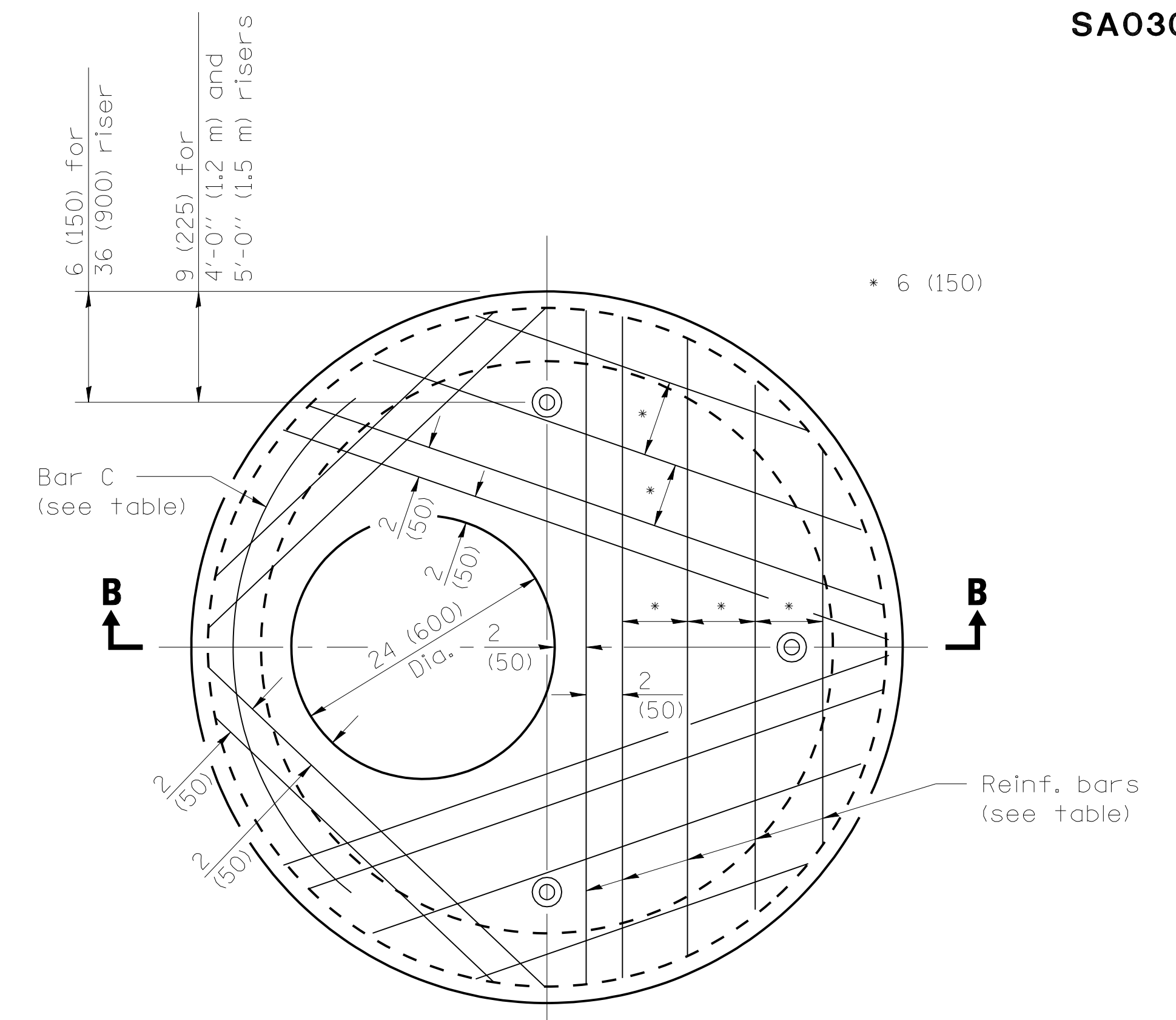
(Sheet 2 of 2)



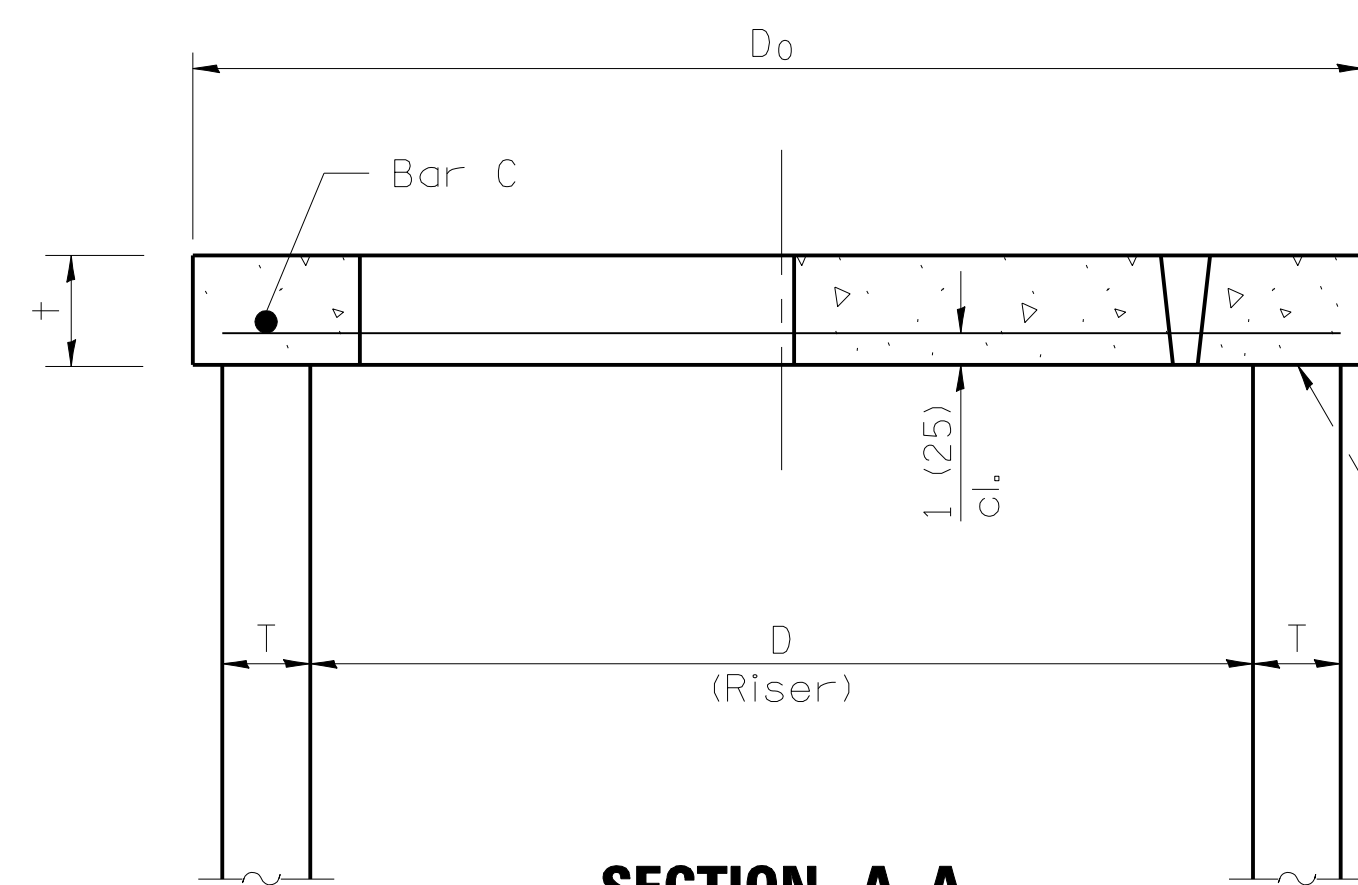
PLAN
(WELDED WIRE FABRIC)



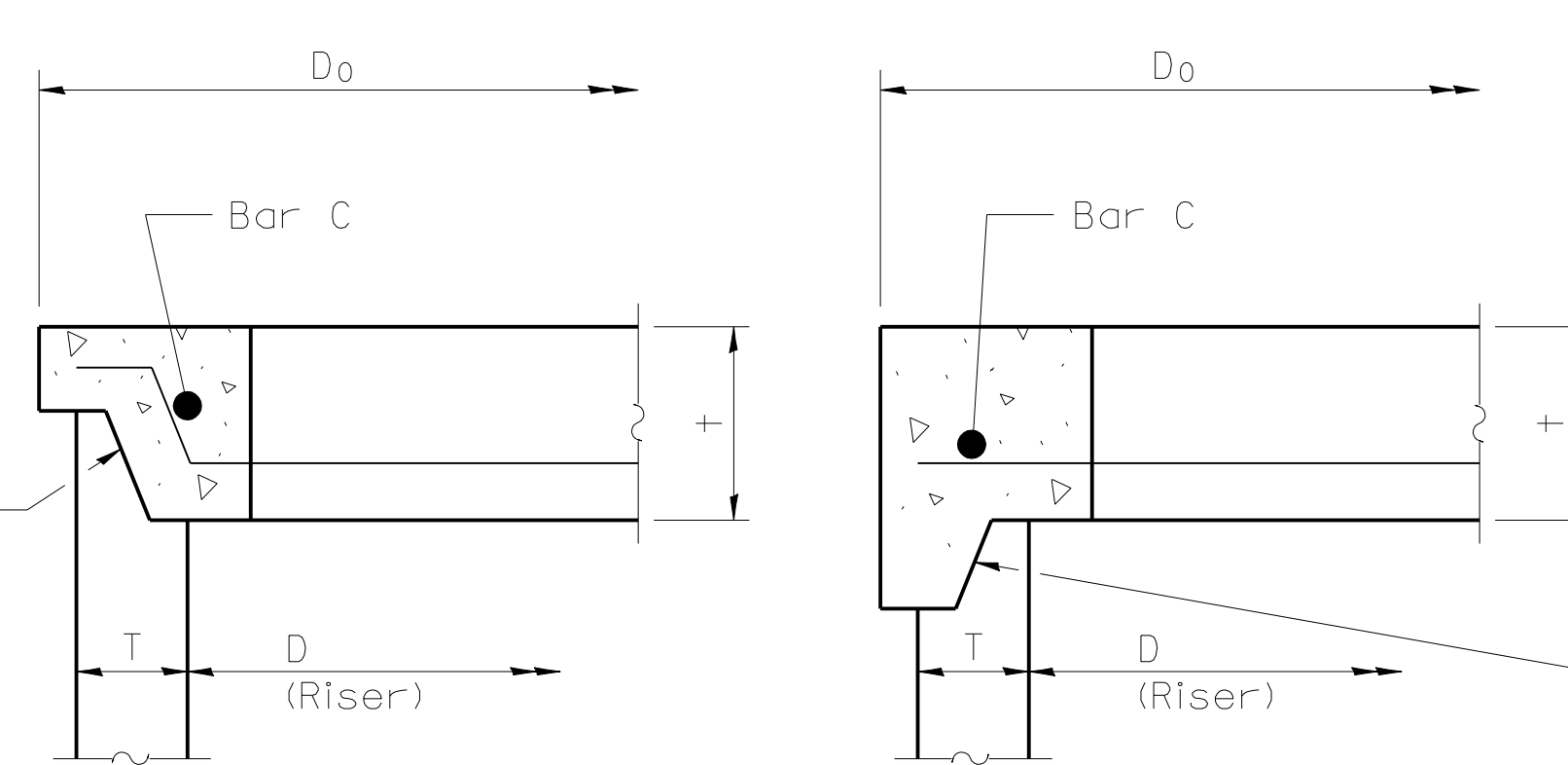
LIFTING HOLE OR LIFTING LOOP
TYPICAL
(3 required per slab)



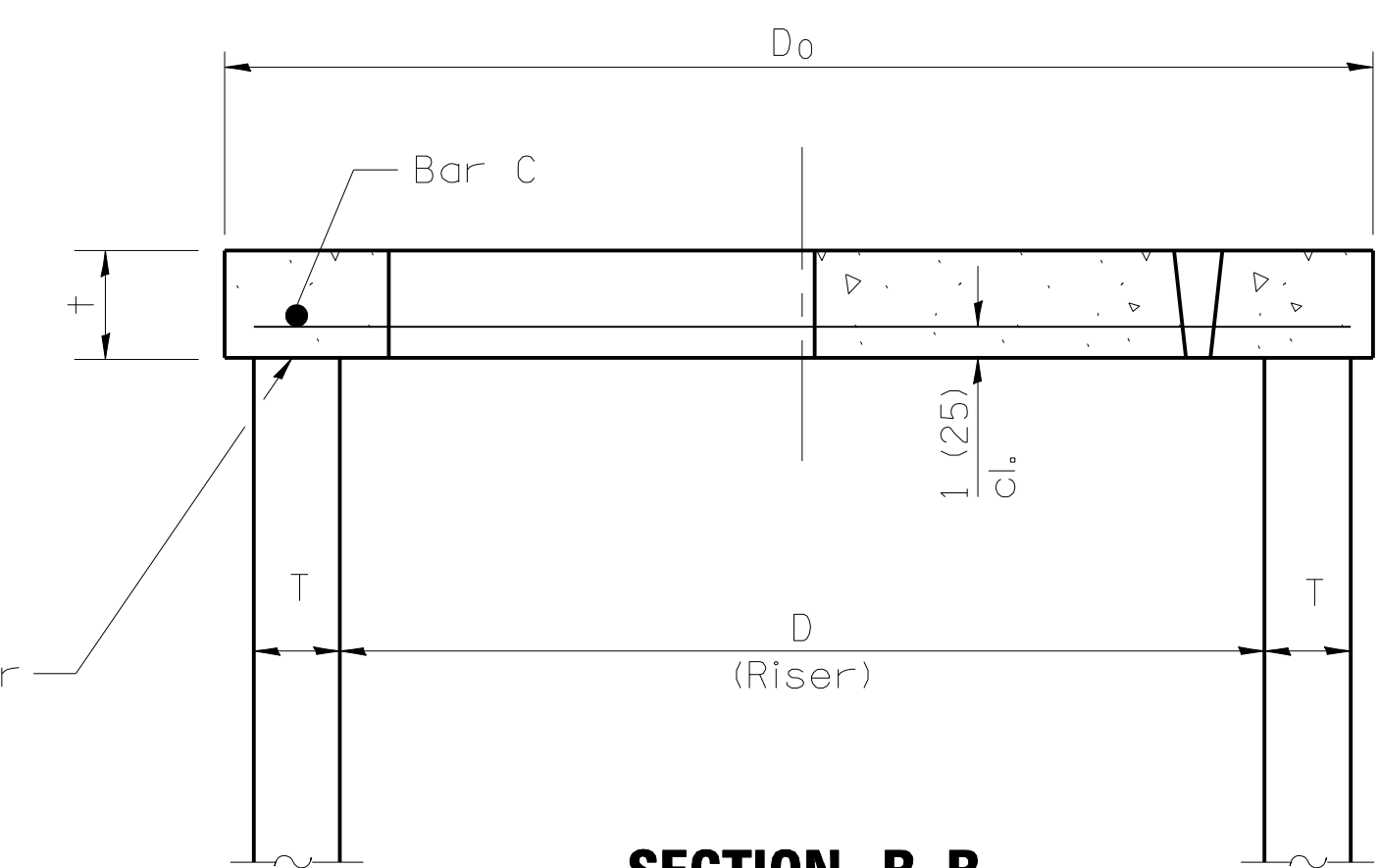
PLAN
(REINFORCEMENT BARS)



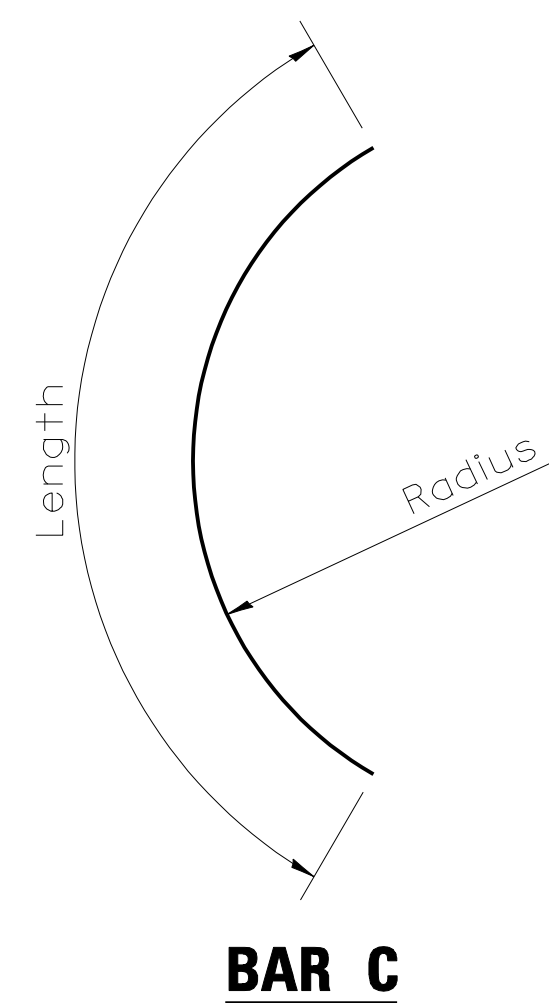
SECTION A-A



ALTERNATE JOINT CONFIGURATIONS



SECTION B-B



BAR C

TABLE

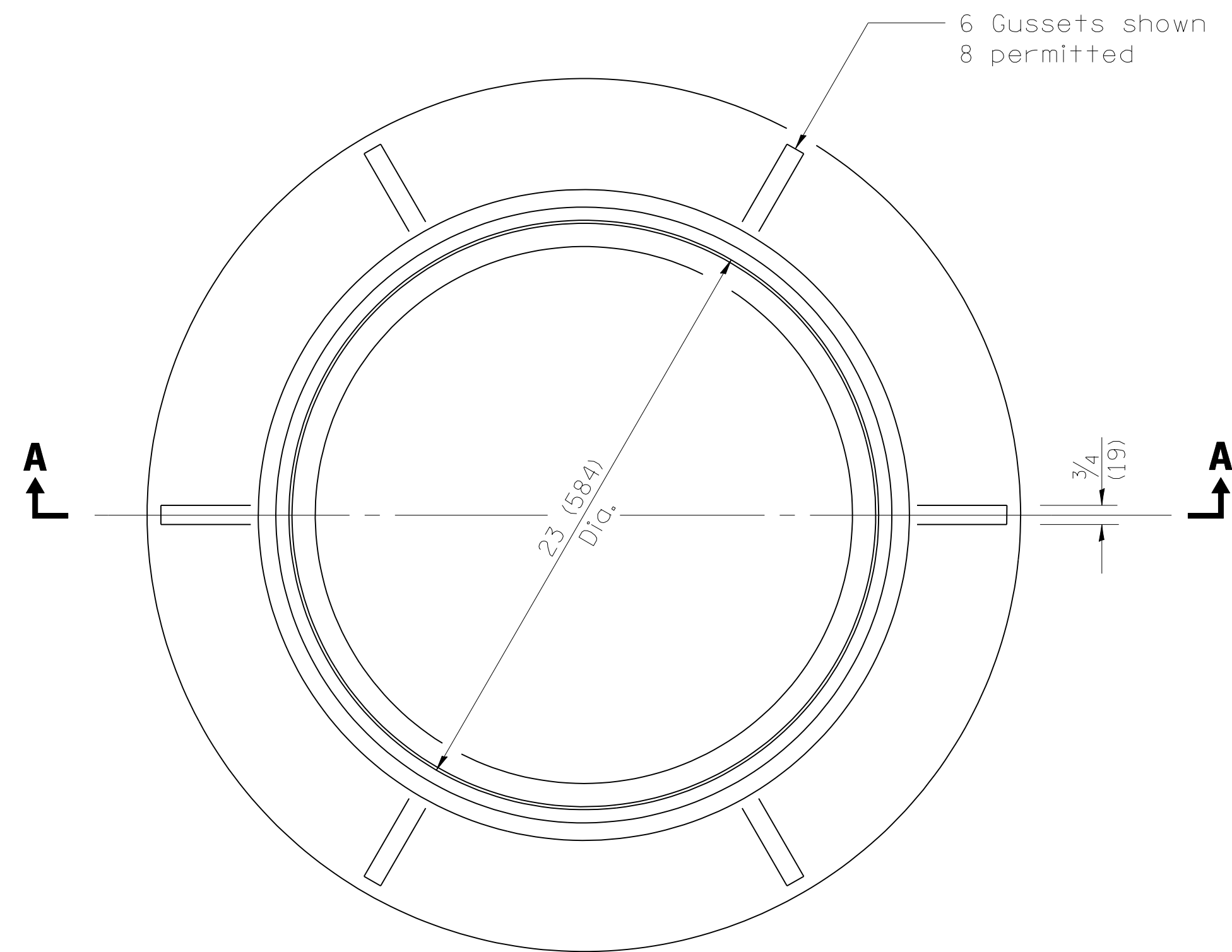
D	T	D ₀ (min.)	t	Reinforcement		No. 4 (No. 13) Bar C	
				"A _s " W.W.F. each direction	QR Bar size	Length	Radius
36 (900)	See applicable Standards	D + 2T	6 (150)	0.20 sq. in./ft. (425 sq. mm/m)	No. 4 (No. 13)	4'-0" (1.2 m)	19 (480)
4'-0" (1.2 m)			6 (150)	0.35 sq. in./ft. (740 sq. mm/m)	No. 5 (No. 16)	4'-6" (1.35 m)	26 (660)
5'-0" (1.5 m)			8 (200)	0.35 sq. in./ft. (740 sq. mm/m)	No. 5 (No. 16)	5'-0" (1.5 m)	32 (810)

GENERAL NOTES

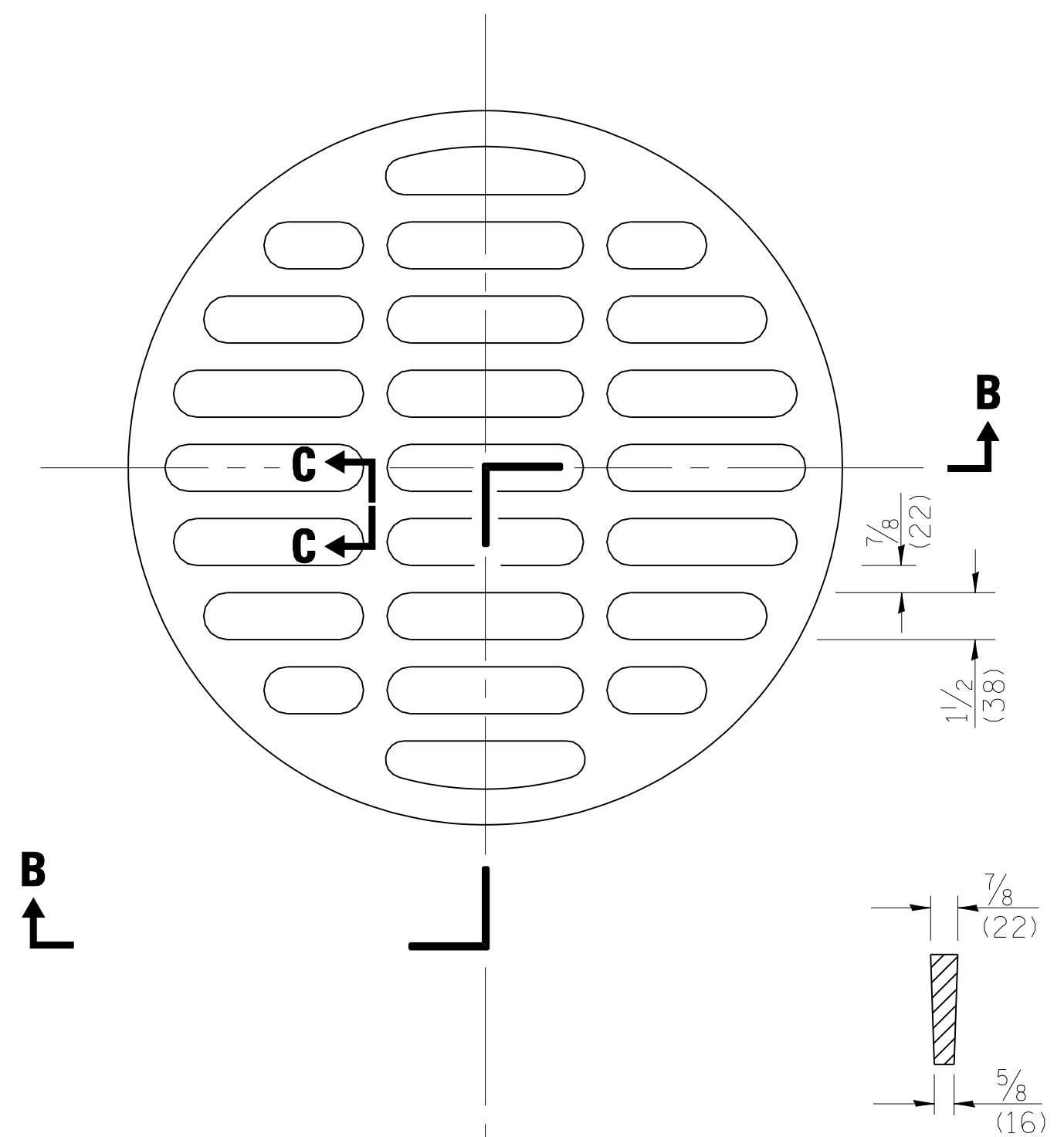
The flat slab top may be used in lieu of the tapered tops shown on Standards 602001, 602011, 602016, 602306, 602401, or 602501 at the option of the Contractor or when field conditions prohibit the use of tapered tops.

All dimensions are in millimeters (inches) unless otherwise shown.

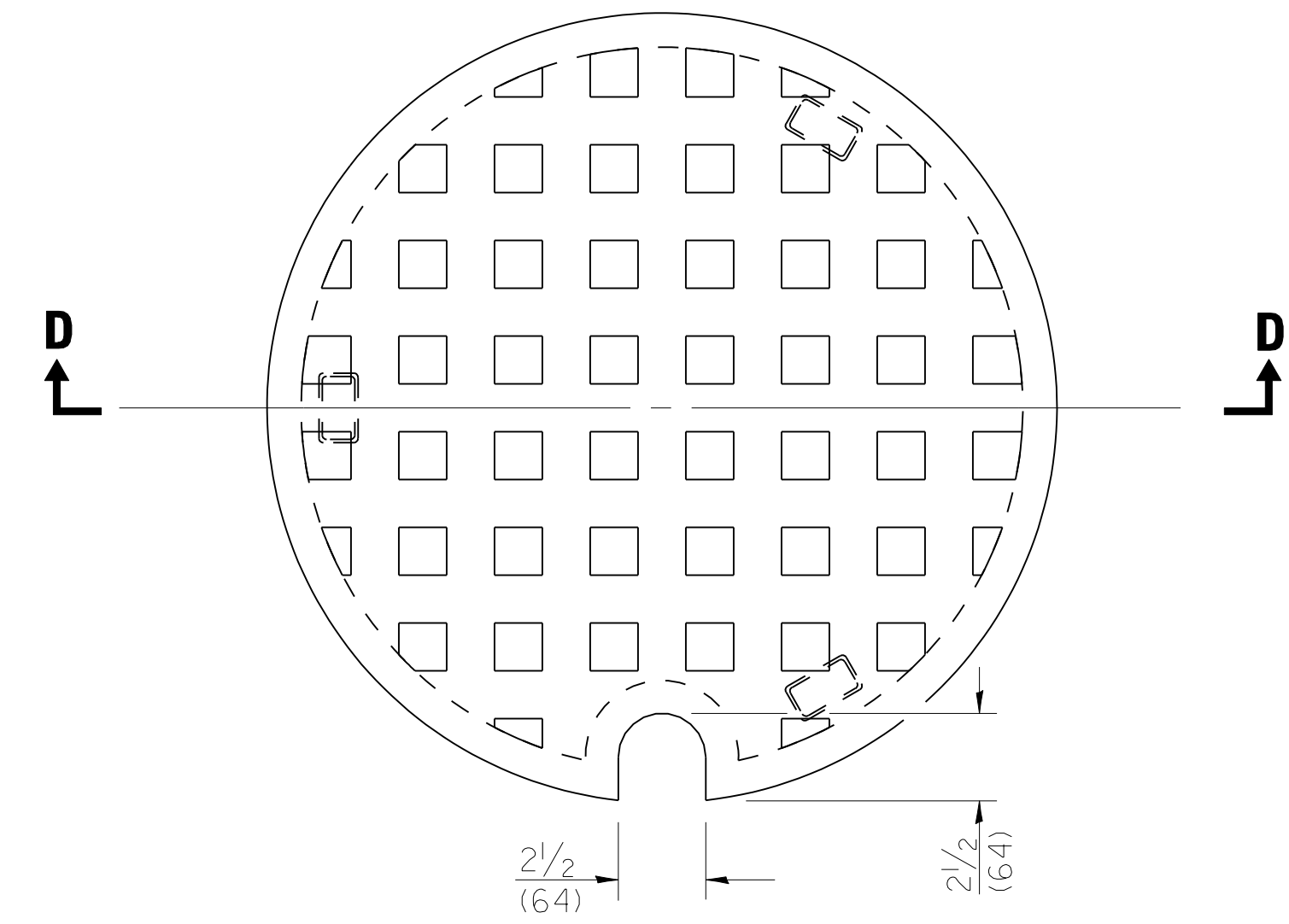
**PRECAST REINFORCED
CONCRETE FLAT SLAB TOP**



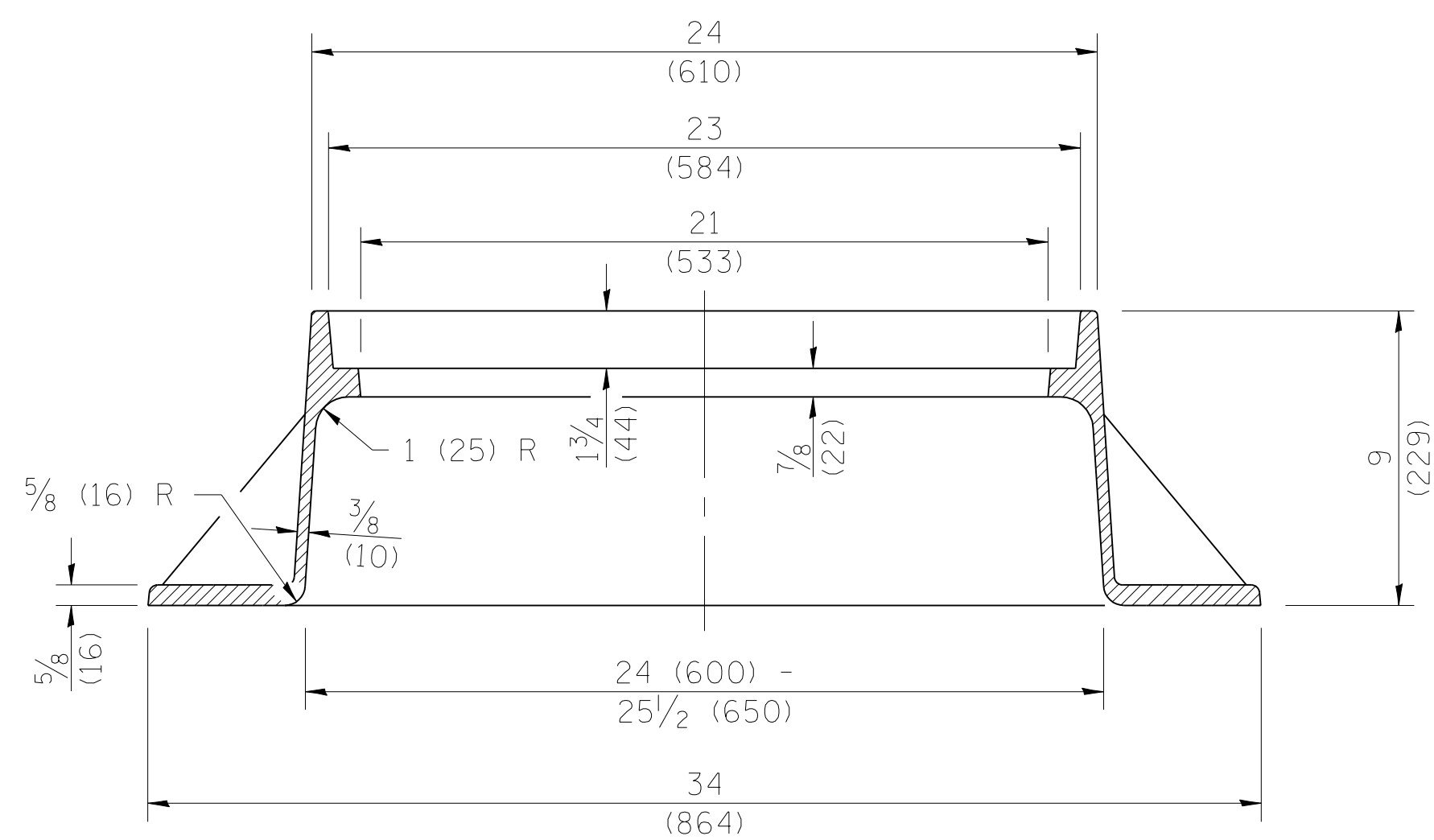
CAST FRAME



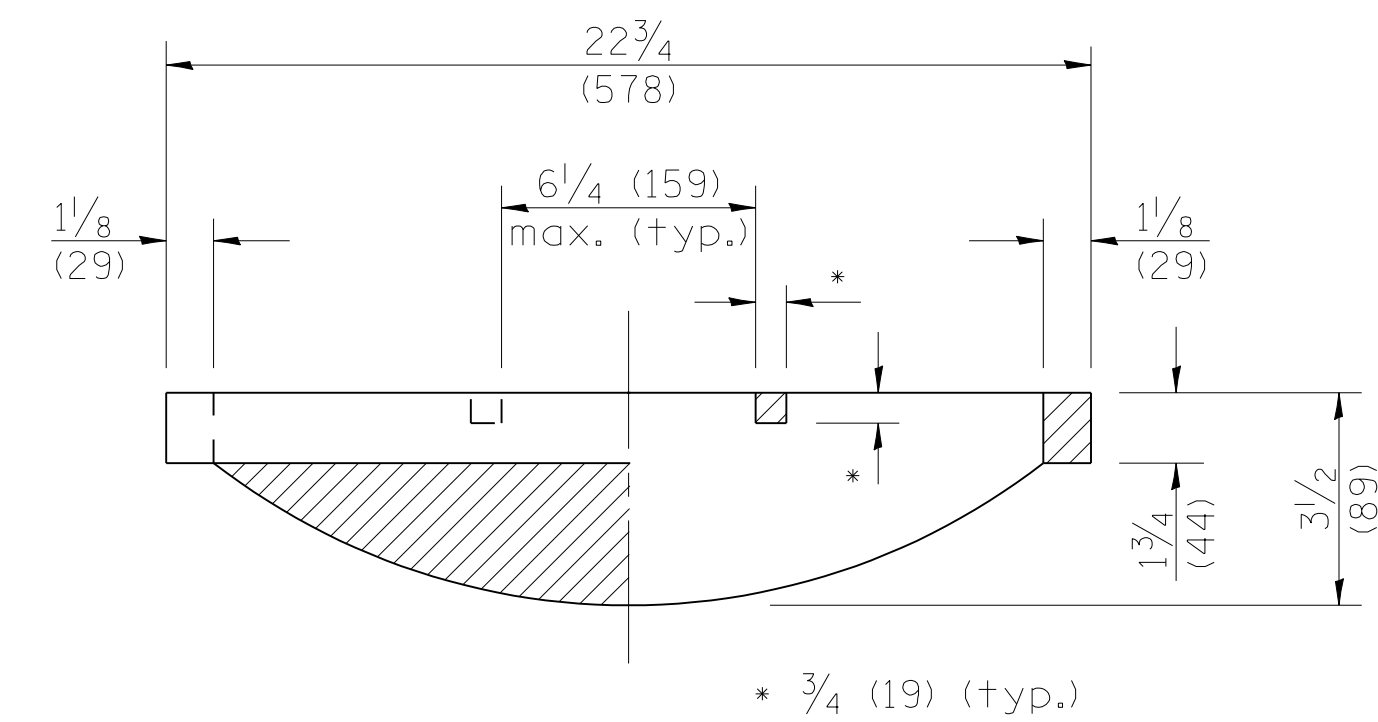
SECTION C-C



SECTION D-D

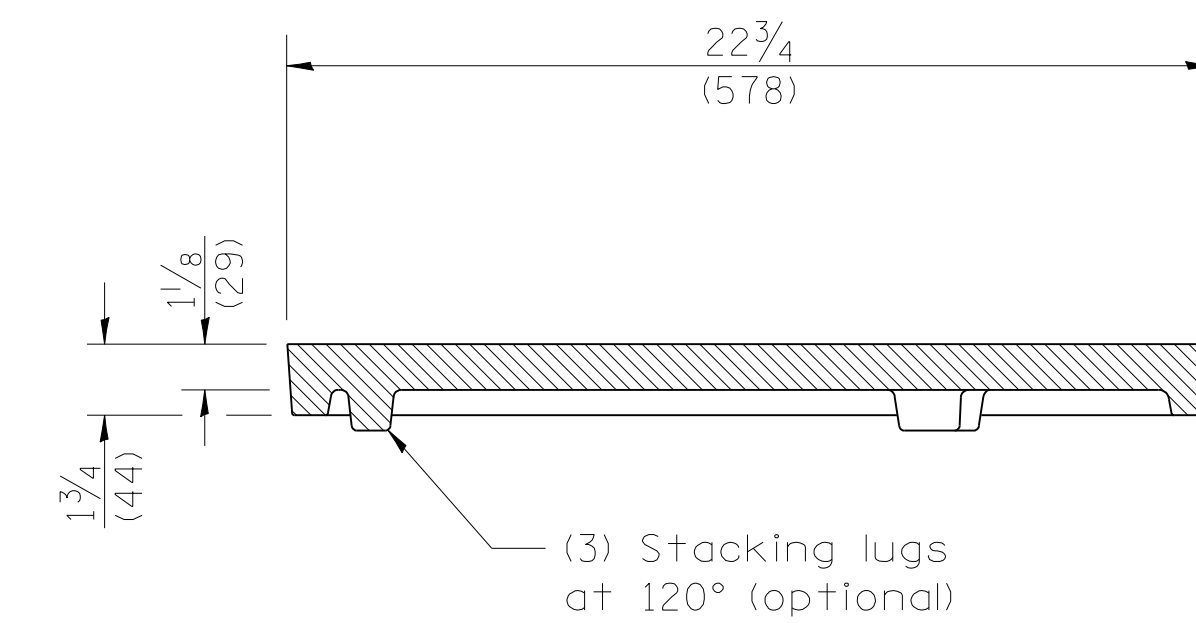


SECTION A-A
Gray Iron



SECTION B-B

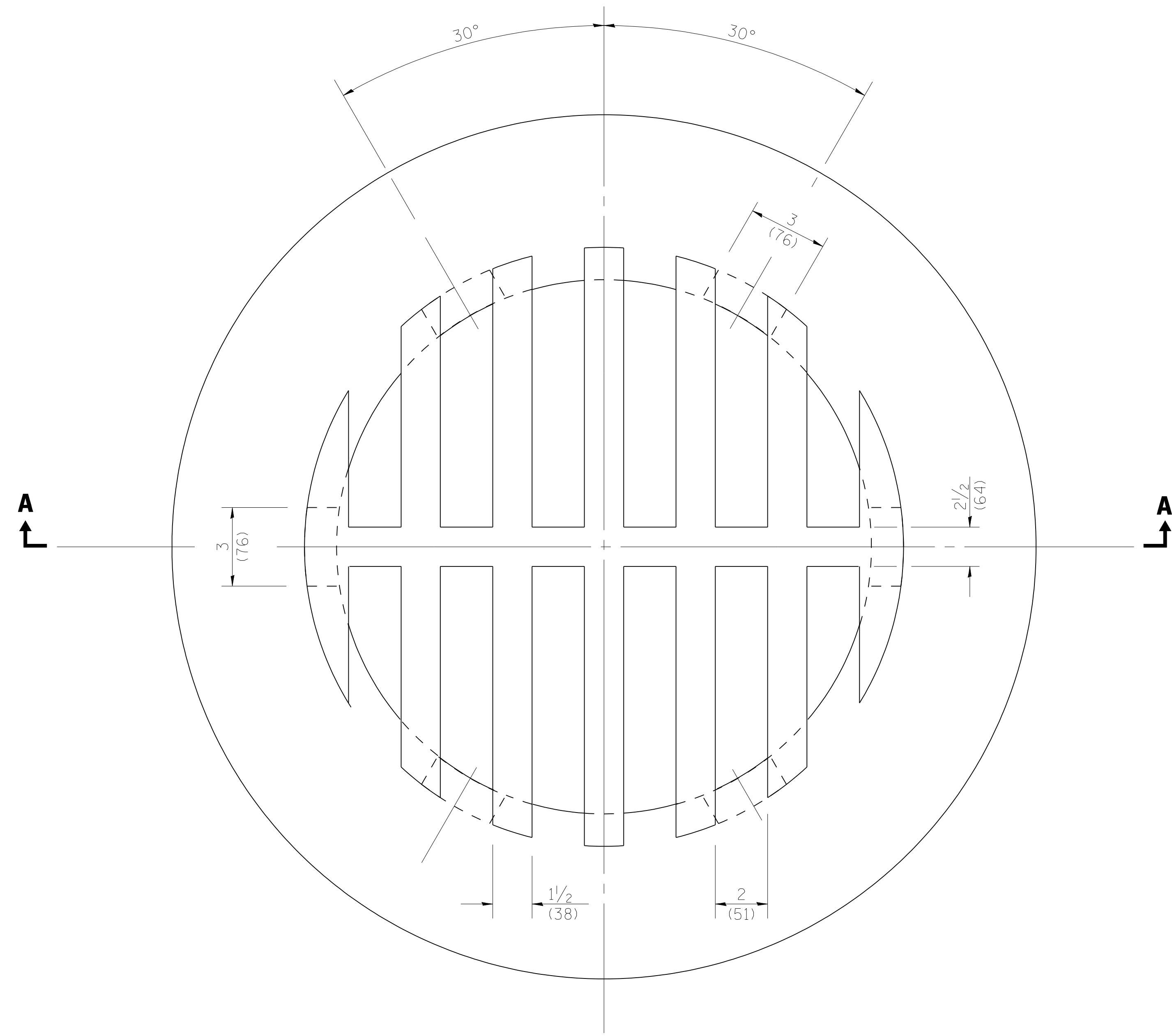
CAST OPEN LID



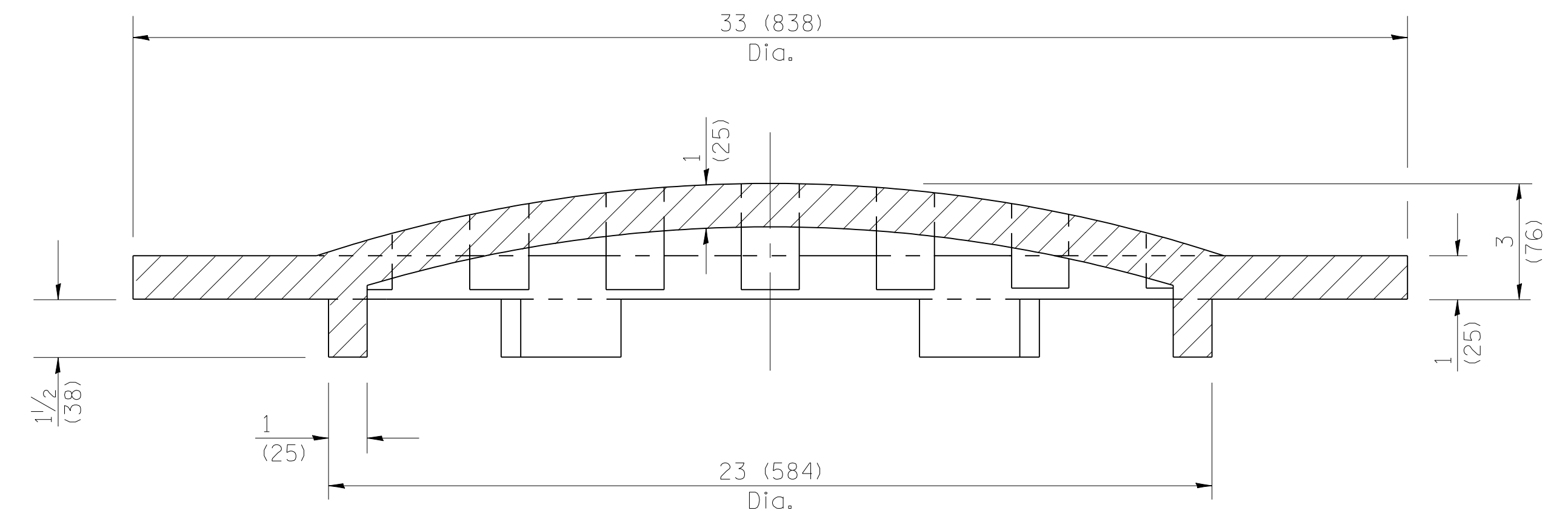
CAST CLOSED LID
Gray Iron Lid

All dimensions are in inches (millimeters) unless otherwise shown.

**FRAME AND LIDS
TYPE 1**



CAST GRATE



SECTION A-A

All dimensions are in inches (millimeters) unless otherwise shown.

GRATE TYPE 8