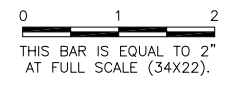


UN051

REVISIONS		
NUMBER	BY	DATE



WILLARD AIRPORT
UNIVERSITY OF ILLINOIS

NEW AIRFIELD LIGHTING VAULT
ELECTRICAL DETAILS 1

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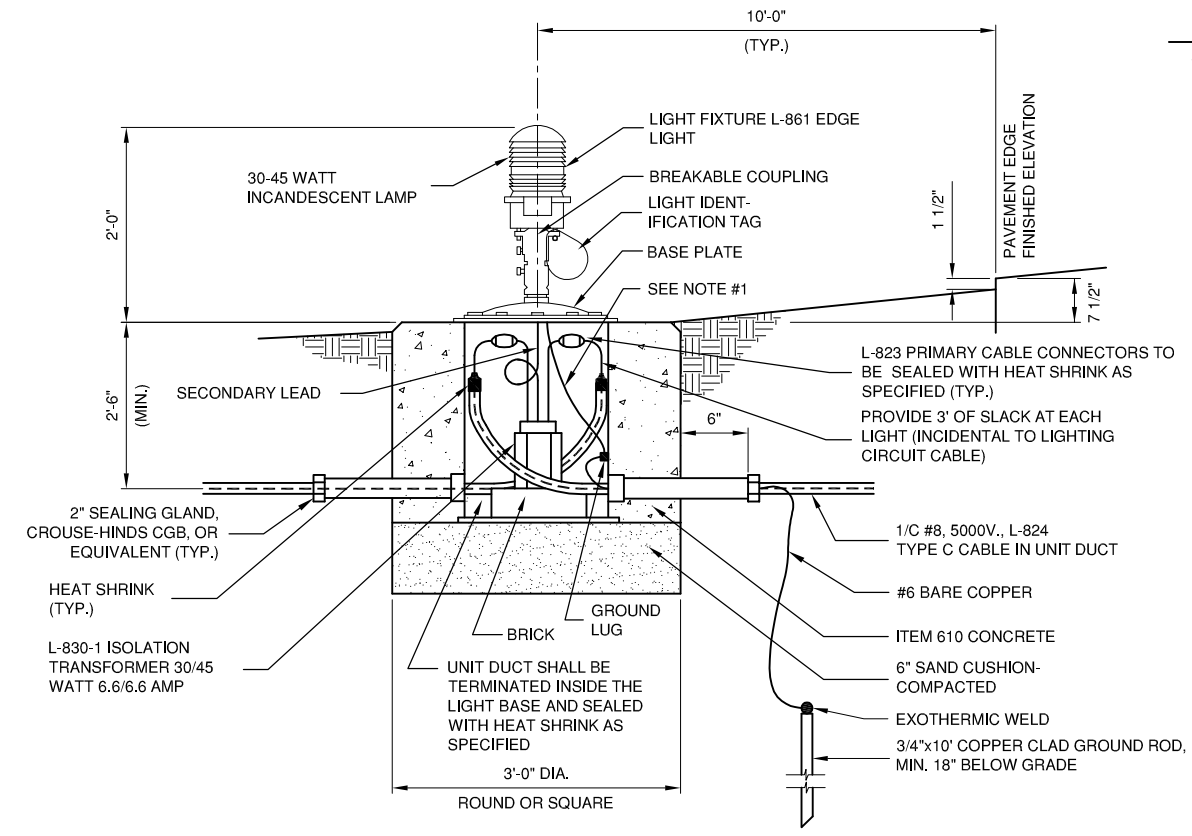
CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613



DESIGN BY:	CBG/WDP
DRAWN BY:	CMT
CHECKED BY:	CET
APPROVED BY:	CET
DATE:	APRIL 20, 2012
JOB No:	11059-03
IL. PROJ. NO. CMI-4100 AIP PROJ. NO. 3-17-0016-XX	
SHEET 23 OF 60 SHEETS	

LIGHT NOTES

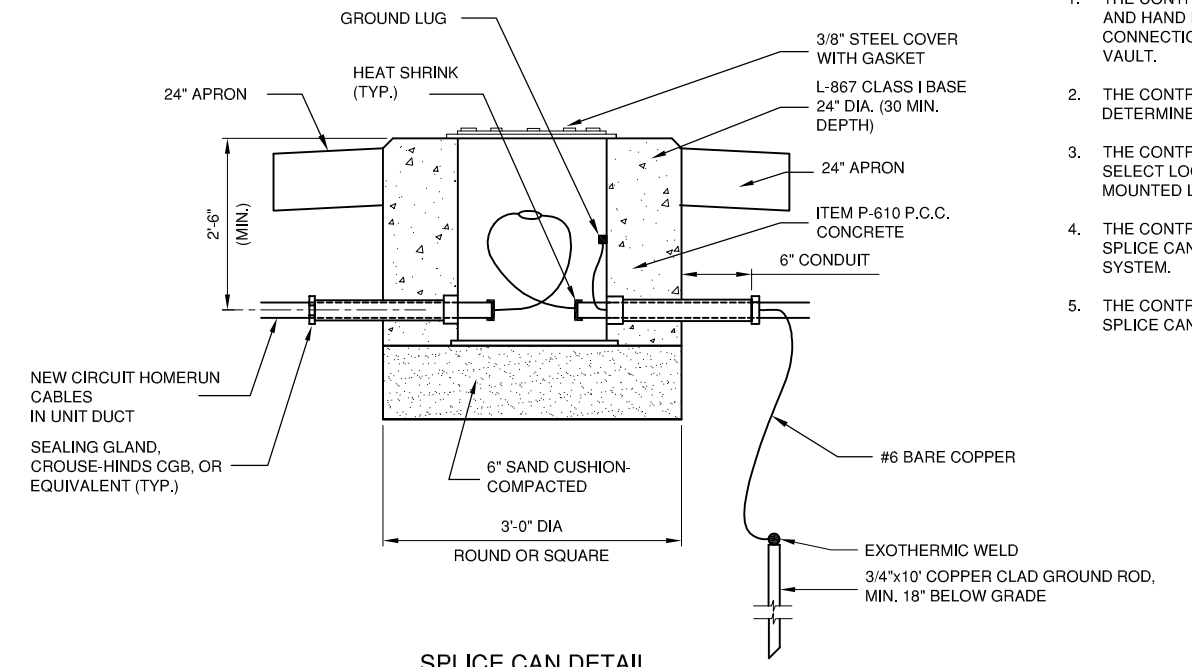
1. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.
2. LIGHTING DETAIL SHOWN FOR HOMERUN CONNECTIONS AT RUNWAY 14L/32R, RUNWAY 4/22, CENTER TAXIWAY AND TAXIWAY D CIRCUITS. INSTALL NEW SPLICE CAN AND REINSTALL EXISTING LIGHT COMPONENTS.



BASE MOUNTED MEDIUM INTENSITY LIGHTS
N.T.S.

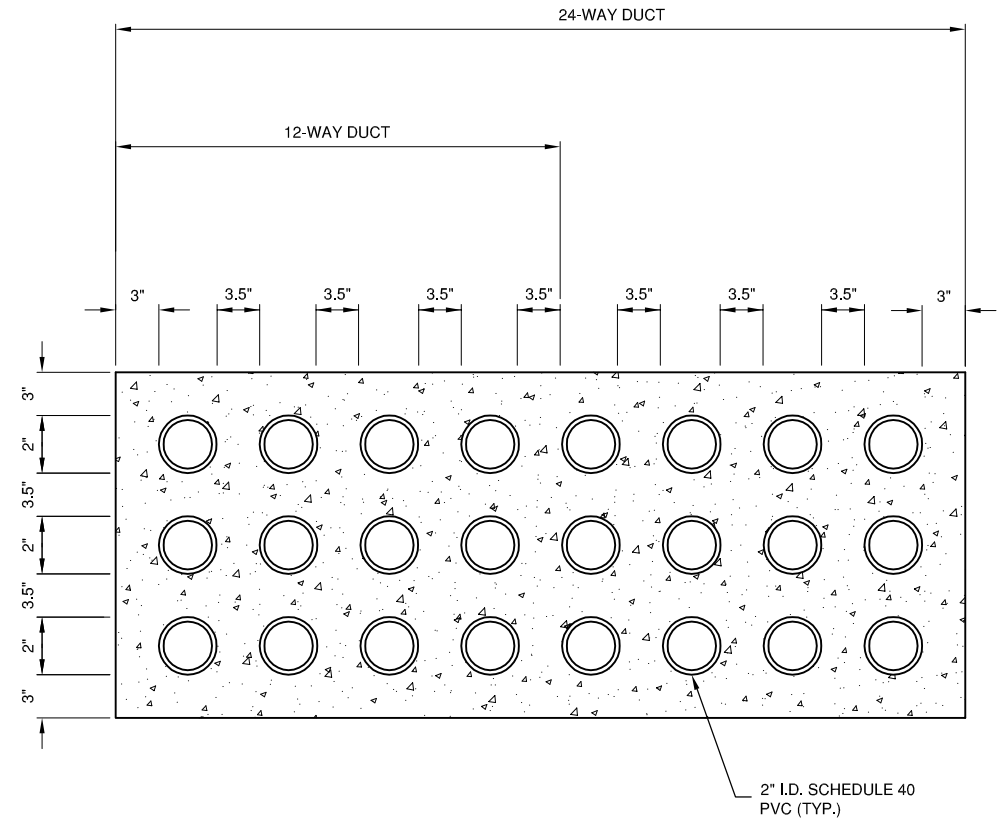
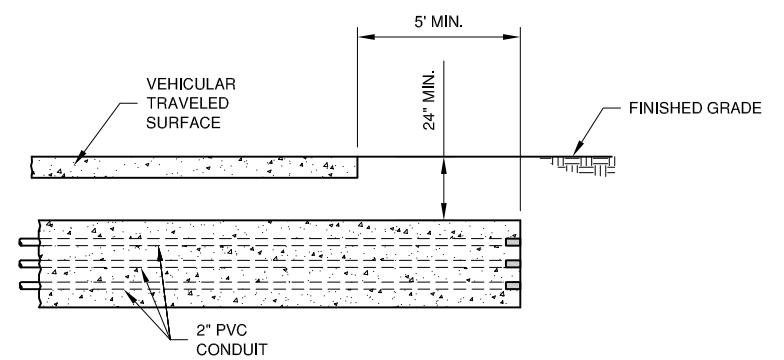
HOMERUN SWITCH OVER NOTES

1. THE CONTRACTOR SHALL FIELD LOCATE EXISTING AIRFIELD CIRCUITS AND HAND EXCAVATE AT THE LOCATION SELECTED AS THE CONNECTION POINT FOR THE NEW CIRCUIT HOMERUN TO THE NEW VAULT.
2. THE CONTRACTOR SHALL EXPOSE SUFFICIENT CONDUCTORS TO DETERMINE CABLE SIZE AND TYPE.
3. THE CONTRACTOR SHALL INSTALL A SPLICE CAN AS DETAILED. (AT SELECT LOCATIONS THE SWITCH OVER MAY BE AT AN EXISTING BASE MOUNTED LIGHT, THESE LOCATIONS ARE SHOWN IN THE PLANS.)
4. THE CONTRACTOR SHALL FEED THE EXISTING CONDUCTORS INTO THE SPLICE CAN AS NECESSARY TO PROVIDE SUFFICIENT SLACK IN THE SYSTEM.
5. THE CONTRACTOR SHALL FEED THE NEW CONDUCTORS INTO THE SPLICE CAN AND CONNECT TO THE EXISTING CONDUCTORS.



SPLICE CAN DETAIL
N.T.S.

UNIT DUCT SHALL BE TERMINATED AT THE L-823 CONNECTORS AND SEALED WITH HEAT SHRINK AS SPECIFIED.



DUCT BANK DETAILS
N.T.S.

DUCT BANK NOTES

1. DIMENSIONS SHOWN ARE MINIMUM.
2. TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 24" BELOW FINISHED GRADE.
3. DUCT CONCRETE TO BE ITEM 610 STRUCTURAL P.C.C. CONCRETE.
4. ALL 12-WAY AND 24-WAY DUCT WILL BE 2" INSIDE DIAMETER.
5. LOCATIONS SHOWN ARE APPROXIMATE. DUCT BANKS TO BE INSTALLED AT LOCATIONS DESIGNATED BY THE ENGINEER.
6. CONTRACTOR WILL INSTALL DUCT BANKS AT A DEPTH WHICH WILL NOT CONFLICT WITH ELEVATION SENSITIVE UTILITIES.
7. A PULL WIRE MUST BE PROVIDED IN EACH UNUSED CONDUIT. UNUSED DUCT MUST BE SEALED AT CAN PLAZA OR HANDHOLE TO THE SATISFACTION OF THE RESIDENT ENGINEER.
8. COUNTERPOISE (NOT SHOWN) SHALL BE INSTALLED PER SECTION 108-3.6 OF THE SPECIAL PROVISIONS. COSTS FOR THE INSTALLATION OF THE COUNTERPOISE SHALL BE INCIDENTAL TO THE DUCT PAY ITEM.