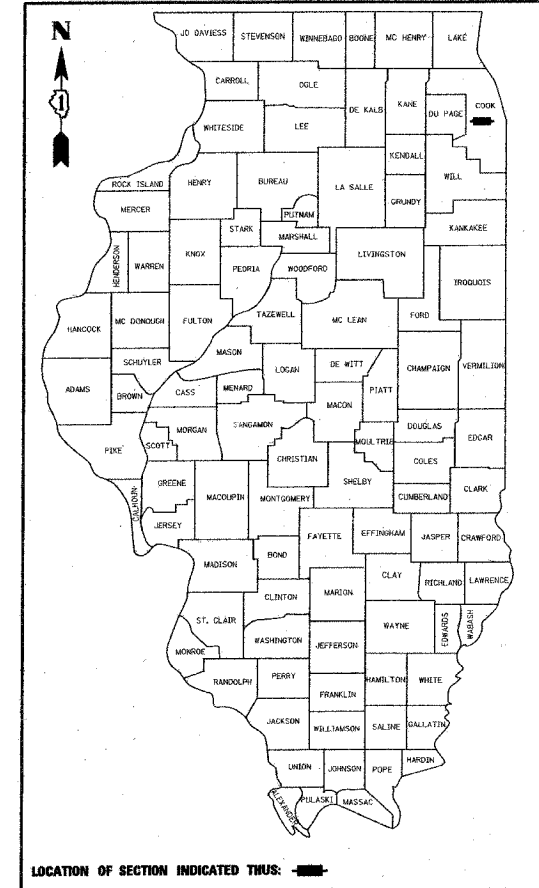


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1693	05-00071-00-RS	COOK	14	1
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				
CONTRACT NO: 83848				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**
F.A.U. ROUTE 1693 (EAST QUINCY STREET)
RIVERSIDE ROAD TO F.A.P. 348 (IL 43 / HARLEM AVENUE)
ROADWAY RESURFACING
SECTION 05-00071-00-RS
PROJECT: M-8003 (515)
JOB# C-91-199-05
VILLAGE OF RIVERSIDE
COOK COUNTY



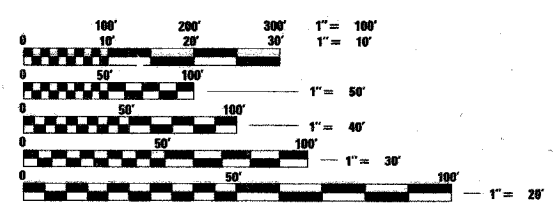
INDEX OF SHEETS

NO.	TITLE
1	TITLE SHEET
2	SUMMARY OF QUANTITIES, IDOT STANDARDS, GENERAL NOTES AND BENCHMARKS
3	TYPICAL SECTIONS
4-7	PLAN SHEETS
8-9	CONSTRUCTION DETAILS
10	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT DETAILS
11	BUTT JOINT AND BITUMINOUS TAPER DETAILS
12	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
14	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

**PROJECT LOCATED IN
VILLAGE OF RIVERSIDE**

DESIGN SPEED 30 mph
SPEED LIMIT = 25 mph
ADT = 2,000 (2030)

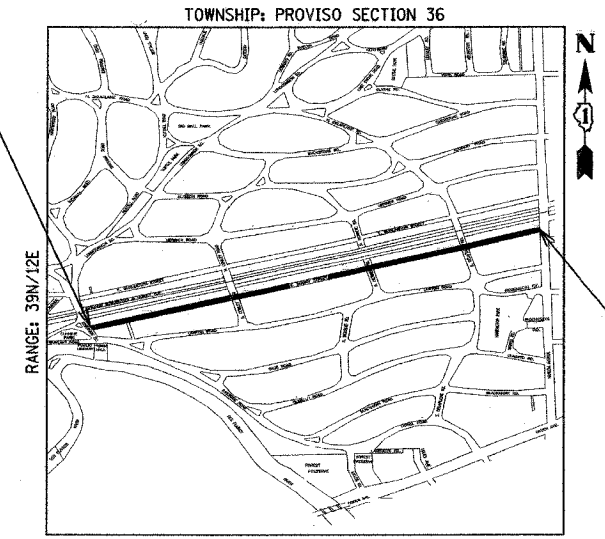
DESIGN DESIGNATION: COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

PROJECT BEGINS - EAST QUINCY STREET
AT RIVERSIDE ROAD
STA. 100+01



PROJECT ENDS - EAST QUINCY STREET
AT HARLEM AVENUE (IL 43)
STA. 145+95

LOCATION MAP
GROSS LENGTH OF PROJECT: 4594 FT. (0.870 MI.)
39N/12E PROVISO TOWNSHIP SECTION 36
SCALE: NTS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 2/24/06 [Signature]
VILLAGE MANAGER, VILLAGE OF RIVERSIDE

PASSED APRIL 12 2006 [Signature] CHRIS HOLT
ENGINEER OF LOCAL ROADS AND STREETS

SIGNED April 12 2006
Diane M. O'Keefe [Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER



James D. Miedema, P.E.
Exp. 11-30-07



JAMES J. BENES & ASSOCIATES
CONSULTING ENGINEERS
950 WARRENVILLE ROAD, SUITE 101
LISLE, IL 60532

IDOT FEDERAL AID DESIGN ENGINEER: KEVIN STALLWORTH (847) 705-4169

CONTRACT NO. 83848

COUNTY: COOK SECTION: F.A.U. ROUTE: 1693

SUMMARY OF QUANTITIES

GENERAL NOTES	
1.	ACCESS TO LOCAL RESIDENCES AND BUSINESSES SHALL BE MAINTAINED DURING CONSTRUCTION.
2.	THE CONTRACTOR SHALL GIVE THE MUNICIPALITY AND JAMES J. BENES AND ASSOCIATES, INC. THREE (3) WORKING DAYS NOTICE PRIOR TO THE COMMENCEMENT OF WORK. JAMES J. BENES AND ASSOCIATES, INC.: (630) 719-7570
3.	ALL ELEVATIONS ARE ON U.S.G.S. DATUM.
4.	NEITHER THE ENGINEER, NOR THE OWNER, SHALL ASSUME ANY OF THE RESPONSIBILITIES OF THE CONTRACTOR'S SUPERINTENDENT OR OF SUBCONTRACTORS. ADDITIONALLY, NEITHER THE ENGINEER, NOR THE OWNER, SHALL ADVISE ON, OR ISSUE DIRECTIONS CONCERNING, ASPECTS OF CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, OR SAFETY PRECAUTIONS AND/OR PROGRAMS IN CONNECTION WITH THE WORK.
5.	THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THEIR ACCURACY IS NOT GUARANTEED. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION AND ELEVATION OF ALL UTILITIES. THE CONTRACTOR SHALL REPORT ANY ENCOUNTERED DISCREPANCIES TO THE ENGINEER AT ONCE. THE CONTRACTOR SHALL TAKE DUE CARE IN ALL PHASES OF CONSTRUCTION TO PROTECT ANY UTILITIES WHICH MAY BE AFFECTED BY THE WORK. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
6.	THE CONTRACTOR SHALL KEEP THE CONSTRUCTION AREA FREE OF DEBRIS AND/OR OBJECTIONABLE MATERIALS DURING CONSTRUCTION.
7.	THE CONTRACTOR SHALL MAINTAIN ALL EXISTING DRAINAGE FACILITIES DURING CONSTRUCTION AND SHALL REPAIR ANY DRAINAGE FACILITIES DAMAGED DURING CONSTRUCTION. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT AND WILL NOT BE PAID FOR SEPARATELY.
8.	THE CONTRACTOR SHALL VERIFY THE ELEVATIONS OF EXISTING STORM SEWERS PRIOR TO THE CONSTRUCTION OF PROPOSED STORM SEWER.
9.	BEFORE STARTING AND EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
10.	UNLESS OTHERWISE SHOWN, TRANSITIONS OF 10' SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTERS AND MEDIANS IN THE FIELD. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
11.	THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF RIVERSIDE.
12.	BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I AND TYPE II BARRICADE USED - ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.
13.	WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
14.	BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINTS AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

ITEM CODE NUMBER	ITEM NO.	ITEM DESCRIPTION	UNIT	QTY	CONSTRUCTION CODE NUMBER
X4021000	1	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	27	1-000
X4066414	2	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIXTURE C, N50	TON	1,245	1-000
X4067100	3	POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50	TON	400	1-000
XX004005	4	PORTLAND CEMENT CONCRETE SIDEWALK, 5", SPECIAL (PEA GRAVEL MIX)	SQ FT	3,250	1-000
XX004740	5	PORTLAND CEMENT CONCRETE SIDEWALK, 7", SPECIAL (PEA GRAVEL MIX)	SQ FT	1,250	1-000
XX006561	6	PORTLAND CEMENT CONCRETE SIDEWALK, 5", SPECIAL (SWANEE MIX)	SQ FT	300	1-000
XX004833	7	STABILIZED DRIVEWAY SUPERPAVE, 7"	SQ YD	220	1-000
XX000406	8	BRICK PAVER REMOVAL AND REPLACEMENT	SQ FT	325	1-000
20201200	9	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	366	1-000
20700420	10	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	75	1-000
21001000	11	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	200	1-000
21101615	12	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	2,420	1-000
25000400	13	NITROGEN FERTILIZER NUTRIENT	POUND	30	1-000
25000500	14	PHOSPHORUS FERTILIZER NUTRIENT	POUND	30	1-000
25000600	15	POTASSIUM FERTILIZER NUTRIENT	POUND	30	1-000
25200100	16	SODDING	SQ YD	2,420	1-000
25200200	17	SUPPLEMENTAL WATERING	UNIT	20	1-000
40600200	18	BITUMINOUS MATERIALS (PRIME COAT)	TON	11	1-000
40600300	19	AGGREGATE (PRIME COAT)	TON	24	1-000
40600400	20	MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS	TON	12	1-000
40600895	21	CONSTRUCTING TEST STRIP	EACH	1	1-000
40600980	22	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SQ YD	115	1-000
40600990	23	TEMPORARY RAMP	SQ YD	40	1-000
42001300	24	PROTECTIVE COAT	SQ YD	725	1-000
42300300	25	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7"	SQ YD	250	1-000
XX006562	26	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT (SCORED PATTERN), 7"	SQ YD	45	1-000
42400200	27	PORTLAND CEMENT CONCRETE SIDEWALK, 5"	SQ FT	1,000	1-000
42400800	28	DETECTABLE WARNINGS	SQ FT	250	1-000
44000006	29	BITUMINOUS SURFACE REMOVAL, 1 1/2"	SQ YD	8,873	1-000
44000007	30	BITUMINOUS SURFACE REMOVAL, 2"	SQ YD	4,460	1-000
44000200	31	DRIVEWAY PAVEMENT REMOVAL	SQ YD	500	1-000
44000600	32	SIDEWALK REMOVAL	SQ FT	5,800	1-000
44001700	33	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,200	1-000
56500600	34	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	15	1-000
60252800	35	CATCH BASINS TO BE RECONSTRUCTED	EACH	5	1-000
60257900	36	MANHOLES TO BE RECONSTRUCTED	EACH	21	1-000
60262700	37	INLETS TO BE RECONSTRUCTED	EACH	1	1-000
60266600	38	VALVE BOXES TO BE ADJUSTED	EACH	7	1-000
60300105	39	FRAMES AND GRATES TO BE ADJUSTED	EACH	1	1-000
60300305	40	FRAMES AND LIDS TO BE ADJUSTED	EACH	37	1-000
60406000	41	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	5	1-000
60406100	42	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	11	1-000
70102620	43	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1-000
70300100	44	SHORT TERM PAVEMENT MARKINGS	FOOT	300	1-000
70301000	45	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	100	1-000
* 78000200	46	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1,250	1-000
* 78000400	47	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,000	1-000
* 78000650	48	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	150	1-000
XX006560	49	CLASS D PATCHES, SUPERPAVE, TYPE 1, 6 INCHES	SQ YD	25	1-000
XX004870	50	CLASS D PATCHES, SUPERPAVE, TYPE 2, 6 INCHES	SQ YD	50	1-000
XX004871	51	CLASS D PATCHES, SUPERPAVE, TYPE 3, 6 INCHES	SQ YD	75	1-000
XX004872	52	CLASS D PATCHES, SUPERPAVE, TYPE 4, 6 INCHES	SQ YD	100	1-000
XX004915	53	CLASS D PATCHES, SUPERPAVE, TYPE 1, 10 INCHES	SQ YD	50	1-000
XX004916	54	CLASS D PATCHES, SUPERPAVE, TYPE 2, 10 INCHES	SQ YD	100	1-000
XX004917	55	CLASS D PATCHES, SUPERPAVE, TYPE 3, 10 INCHES	SQ YD	150	1-000
XX004918	56	CLASS D PATCHES, SUPERPAVE, TYPE 4, 10 INCHES	SQ YD	200	1-000
67100100	57	Mobilization	L SUM	1	1-000

* DENOTES AS SPECIALTY ITEMS

IDOT HIGHWAY STANDARDS

000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
420001-06	PAVEMENT JOINTS
424001-04	CURB RAMPS FOR SIDEWALKS
442201-01	CLASS C AND D PATCHES
602601	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-02	FRAMES AND LIDS, TYPE 1
606001-02	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701501-03	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
702001-06	TRAFFIC CONTROL DEVICES
780001-01	TYPICAL PAVEMENT MARKINGS

BENCH MARKS

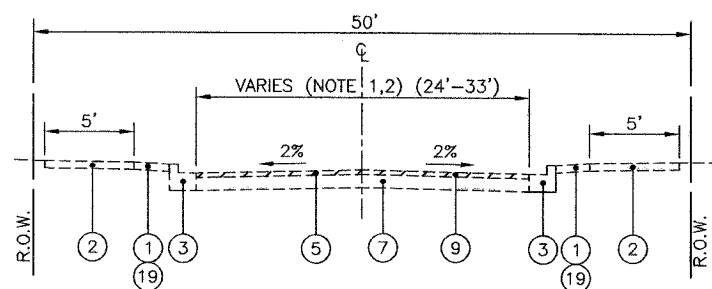
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ELEVATION = 616.69
- NE FLANGE BOLT ON TOP FLANGE OF FIRE HYDRANT AT #48 EAST QUINCY STREET.
ELEVATION = 615.36
- NE FLANGE BOLT ON TOP FLANGE OF FIRE HYDRANT AT #165 EAST QUINCY STREET.
ELEVATION = 613.61
- NE FLANGE BOLT ON TOP FLANGE OF FIRE HYDRANT AT SOUTHEAST CORNER OF HERBERT ROAD AND EAST QUINCY STREET.
ELEVATION = 612.46
- NE FLANGE BOLT ON TOP FLANGE OF FIRE HYDRANT AT SOUTHEAST CORNER OF DELAPLAINE ROAD AND EAST QUINCY STREET.
ELEVATION = 612.50
- NE FLANGE BOLT ON TOP FLANGE OF FIRE HYDRANT AT SOUTHWEST CORNER OF HARLEM AVENUE AND EAST QUINCY STREET.
ELEVATION = 613.65

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.U. 1693 EAST QUINCY STREET
NAME	DATE	
△ IDOT	12/22/05	SUMMARY OF QUANTITIES, IDOT STANDARDS, GENERAL NOTES AND BENCH MARKS

SCALE: NTS
DATE: _____

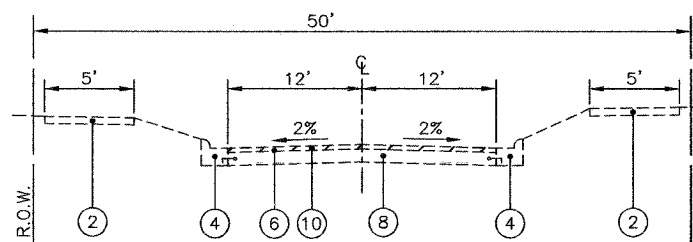
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CHECKED BY: JDM

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1693	05-00071-00-RS	COOK	14	3
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83848				



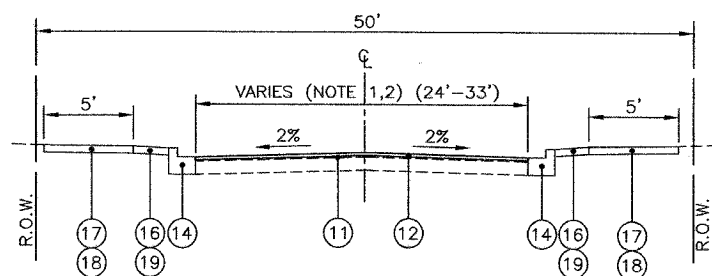
EXISTING TYPICAL SECTION
EAST QUINCY STREET

RIVERSIDE ROAD TO STA. 114+70



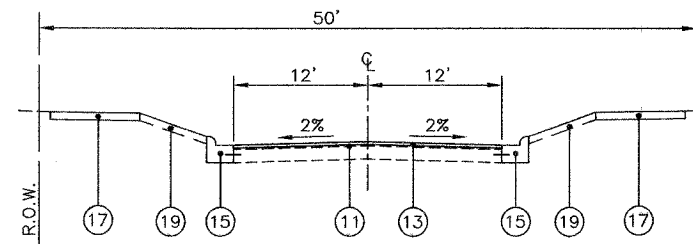
EXISTING TYPICAL SECTION
EAST QUINCY STREET

STA. 114+70 TO HARLEM AVENUE (IL. 43)



PROPOSED TYPICAL SECTION
EAST QUINCY STREET

RIVERSIDE ROAD TO STA. 114+70



PROPOSED TYPICAL SECTION
EAST QUINCY STREET

STA. 114+70 TO HARLEM AVENUE (IL. 43)

LEGEND

- ① EXISTING PCC SIDEWALK (NOTE 1)
- ② EXISTING PCC SIDEWALK (PEA GRAVEL MIX) (NOTE 2)
- ③ EXISTING CURB AND GUTTER, TYPE B-6.18 (NOTE 1)
- ④ EXISTING CURB AND GUTTER, TYPE M-4.18 (NOTE 2)
- ⑤ EXISTING BITUMINOUS CONCRETE SURFACE COURSE (APPROX. 2" THICK)
- ⑥ EXISTING BITUMINOUS CONCRETE SURFACE COURSE, (APPROX. 1-1/2" THICK)
- ⑦ EXISTING BITUMINOUS BASE COURSE, (APPROX. 6" THICK)
- ⑧ EXISTING PCC BASE COURSE (APPROX. 10" THICK)
- ⑨ PROPOSED BITUMINOUS SURFACE REMOVAL, 2"
- ⑩ PROPOSED BITUMINOUS SURFACE REMOVAL, 1-1/2" (CONTRACTOR SHALL REMOVE ALL BITUMINOUS FROM EXISTING CONCRETE BASE. COST INCLUDED WITH REMOVAL)
- ⑪ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50, 0"-1/2"
- ⑫ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50, 2"
- ⑬ PROPOSED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50, 1-1/2"
- ⑭ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (NOTE 1) (AS DIRECTED BY THE ENGINEER)
- ⑮ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.18 (NOTE 2) (AS DIRECTED BY THE ENGINEER)
- ⑯ PROPOSED PCC SIDEWALK, 5 INCH (NOTE 1,3)
- ⑰ PROPOSED PCC SIDEWALK, 5 INCH (PEA GRAVEL MIX), (NOTE 2,3) OR PROPOSED PCC SIDEWALK, 7 INCH (PEA GRAVEL MIX). (NOTE 2,3)
- ⑱ PROPOSED PCC SIDEWALK, 5 INCH (SWANEE MIX) (NOTE 1,3)
- ⑲ TOPSOIL FURNISH AND PLACE 4" SEEDING (AS DIRECTED BY THE ENGINEER)

NOTE 1: RIVERSIDE ROAD TO STA. 105+60 (33' E-E)
NOTE 2: STA. 105+60 TO HARLEM AVENUE (24' E-E)
NOTE 3: MINIMUM 2 INCH CA-6 SUBBASE REQUIRED (COST INCLUDED WITH SIDEWALK)

PAVEMENT DESIGN CALCULATIONS			
DATE:	November 15, 2005		
IMPROVEMENT TYPE:	FLEXIBLE PAVEMENT CROSS SECTION (RESURFACING)		
LOCATION:	EAST QUINCY STREET - Village of Riverside FAU 1693 Riverside Road to South Cowley Road		
CLASSIFICATION OF ROADWAY:	CLASS II ROADWAY (ADT > 2000)		
TRAFFIC FACTOR = DP(((0.073*PV)+(44.530*SU)+(156.403*MU))/1000000)			
DESIGN LANE VOLUME % OF ADT	= 50 % TRUCKS	50 % PASS. VEHICLES	
DESIGN PERIOD, YEARS (DP)	= 20 YEARS		
% OF PASSENGER VEHICLES (PV)	= 98.50 %		
% OF SINGLE UNIT TRUCKS (SU)	= 1.40 %		
% OF MULTI UNIT TRUCKS (MU)	= 0.10 %		
AVERAGE DAILY TRAFFIC	= 2000	TRAFFIC FACTOR.....	= 0.017035
DESIGN LANE VOLUME	= 1000		
NO. OF PASSENGER VEHICLES	= 985	LB.R.....	= 2.5
NO. OF SINGLE UNIT TRUCKS	= 14		
NO. OF MULTI UNIT TRUCKS	= 1	STRUCTURAL NUMBER (D _s)	= 2.30
PROPOSED PAVEMENT CROSS SECTION			
MATERIAL THICKNESS	STRUCTURAL MATERIAL	COEFFICIENT	D _t
2.00	BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX C, N50	0.40	0.80
0.50	POLYMERIZED LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50	0.33	0.17
6.00	EXISTING BITUMINOUS BASE COURSE	0.23	1.38
4.00	EXISTING AGGREGATE BASE COURSE	0.11	0.44
TOTAL D _t PROVIDED =			2.79

PAVEMENT DESIGN CALCULATIONS			
DATE:	November 15, 2005		
IMPROVEMENT TYPE:	COMPOSITE PAVEMENT CROSS SECTION (RESURFACING)		
LOCATION:	EAST QUINCY STREET - Village of Riverside FAU 1693 South Cowley Road to Harlem Avenue (IL RT 43)		
CLASSIFICATION OF ROADWAY:	CLASS II ROADWAY (ADT > 2000)		
TRAFFIC FACTOR = DP(((0.073*PV)+(44.530*SU)+(156.403*MU))/1000000)			
DESIGN LANE VOLUME % OF ADT	= 50 % TRUCKS	50 % PASS. VEHICLES	
DESIGN PERIOD, YEARS (DP)	= 20 YEARS		
% OF PASSENGER VEHICLES (PV)	= 98.50 %		
% OF SINGLE UNIT TRUCKS (SU)	= 1.40 %		
% OF MULTI UNIT TRUCKS (MU)	= 0.10 %		
AVERAGE DAILY TRAFFIC	= 2000	TRAFFIC FACTOR.....	= 0.017035
DESIGN LANE VOLUME	= 1000		
NO. OF PASSENGER VEHICLES	= 985	LB.R.....	= 2.5
NO. OF SINGLE UNIT TRUCKS	= 14		
NO. OF MULTI UNIT TRUCKS	= 1	STRUCTURAL NUMBER (D _s)	= 2.30
PROPOSED PAVEMENT CROSS SECTION			
MATERIAL THICKNESS	STRUCTURAL MATERIAL	COEFFICIENT	D _t
1.50	BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX C, N50	0.40	0.60
0.50	POLYMERIZED LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50	0.33	0.17
10.00	EXISTING PCC BASE COURSE	0.26	2.60
4.00	EXISTING AGGREGATE BASE COURSE	0.11	0.44
TOTAL D _t PROVIDED =			3.81

BITUMINOUS MIXTURE REQUIREMENT

ITEM	AC TYPE	VOIDS	MAX RAP %
POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE IL-4.75 N50	SBS/SBR PG 76-28	2.5%@50 GYR	0
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX C, N50	PG 64-22	4%@50 GYR	15
BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE IL 19.0, N70 (CLASS D PATCHES, 6" & 10")	PG 64-22	4%@70 GYR	15
BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE IL 19.0, N50 (DRIVEWAY BASE, 5")	PG 58-22	4%@50 GYR	25
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX C, N50 (DRIVEWAY SURFACE, 2")	PG 64-22	4%@50 GYR	15

UNIT WEIGHT FOR ALL BITUMINOUS SURFACE MIX IS 112 LBS/SY/IN

REVISIONS	
NO.	DATE
1	12/22/05

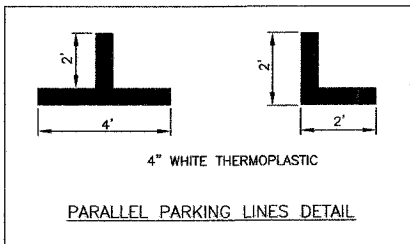
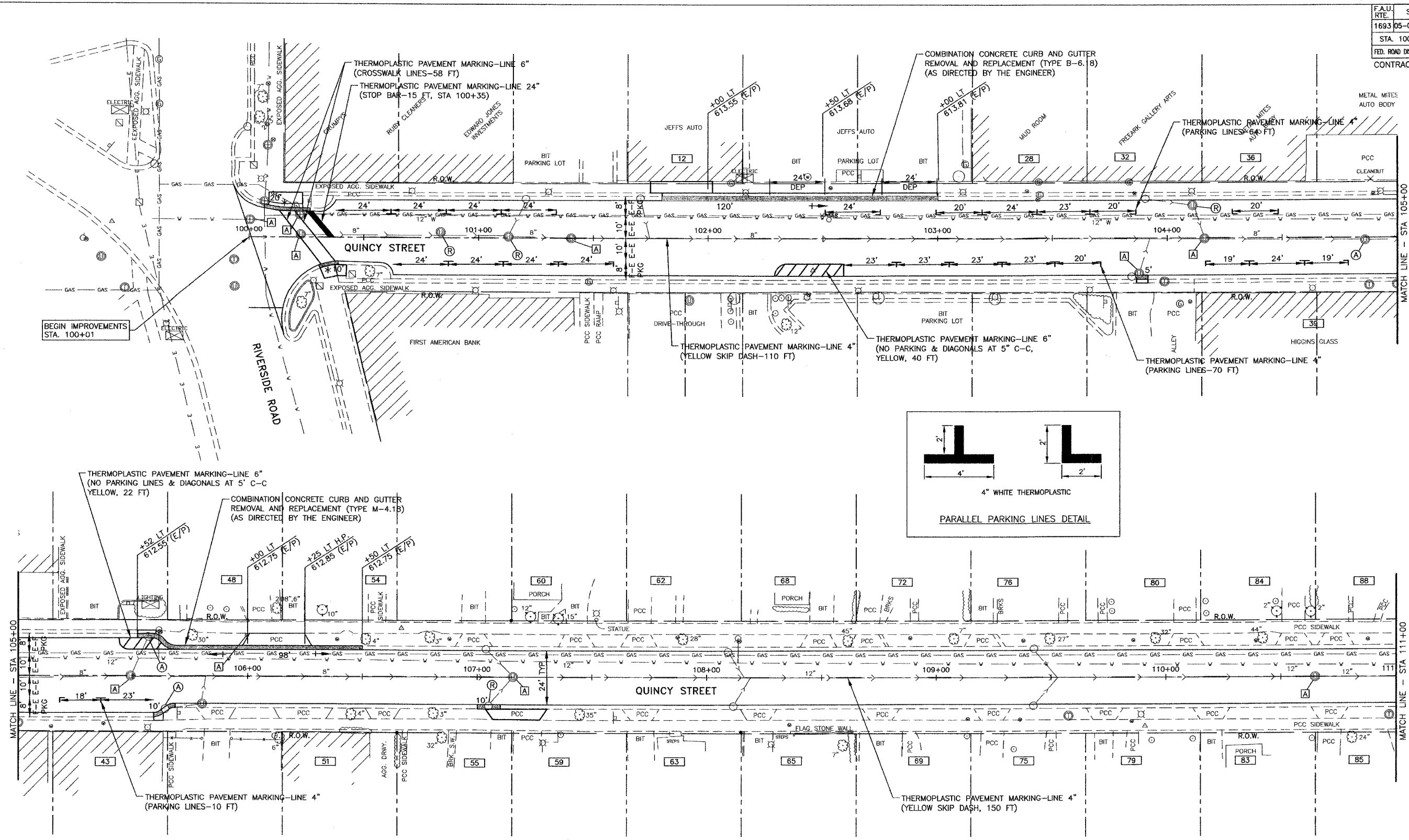
ILLINOIS DEPARTMENT OF TRANSPORTATION
FAU 1693
EAST QUINCY STREET

TYPICAL SECTIONS

SCALE: NTS
DATE:

DRAWN BY: SMP
CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1693 05-00071-00-RS		COOK	14	4
STA. 100+01 TO STA. 111+00				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 83848		



LEGEND

*	HANDICAP SIDEWALK DEPRESSION
[A]	STRUCTURE TO BE ADJUSTED
(A)	STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
[R]	STRUCTURE TO BE RECONSTRUCTED
(R)	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID

REVISIONS

NO.	NAME	DATE
1	IDOT	12/22/09

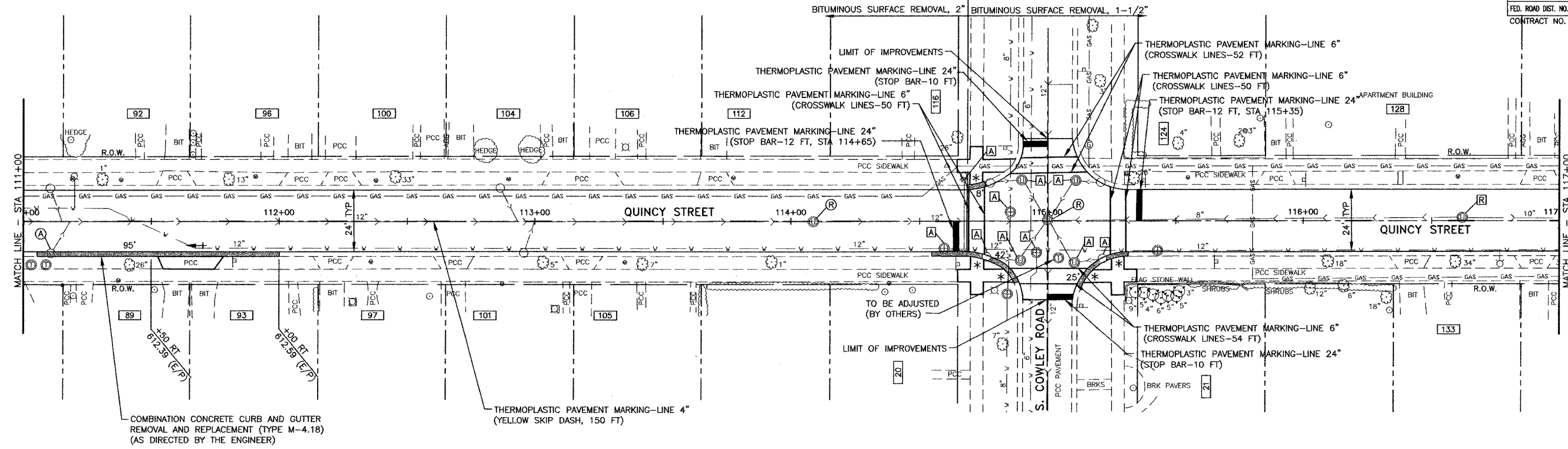
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 1693
 QUINCY STREET

PLAN SHEET
 STA. 100+01 TO STA. 111+00

JJB JAMES J. BENES & ASSOCIATES, INC.
 950 Warrenville Road, Suite 101, Lisle, Illinois 60532
 Tel. (630) 719-7570 • Fax (630) 719-7589

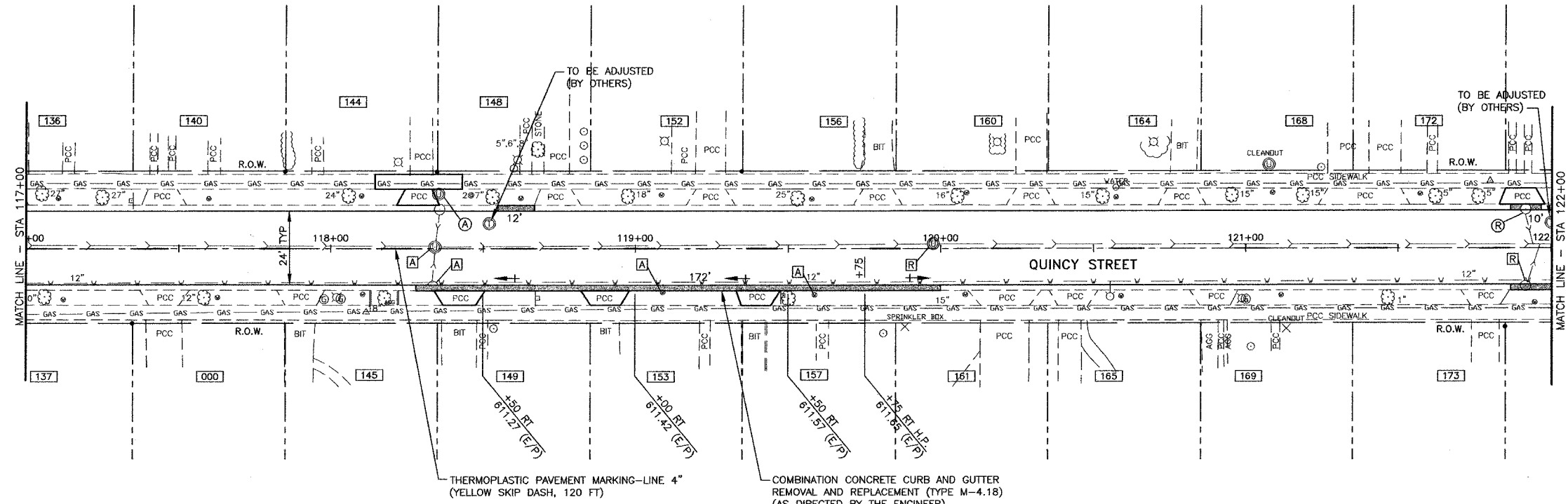
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 DATE: _____ DRAWN BY: SMP
 CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1693	05-00071-00-RS	COOK	14	5
STA. 111+00 TO STA. 122+00				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
CONTRACT NO. 83848				



COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (TYPE M-4.18) (AS DIRECTED BY THE ENGINEER)

THERMOPLASTIC PAVEMENT MARKING-LINE 4" (YELLOW SKIP DASH, 150 FT)



THERMOPLASTIC PAVEMENT MARKING-LINE 4" (YELLOW SKIP DASH, 120 FT)

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (TYPE M-4.18) (AS DIRECTED BY THE ENGINEER)

LEGEND

*	HANDICAP SIDEWALK DEPRESSION
A	STRUCTURE TO BE ADJUSTED
(A)	STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
R	STRUCTURE TO BE RECONSTRUCTED
(R)	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID

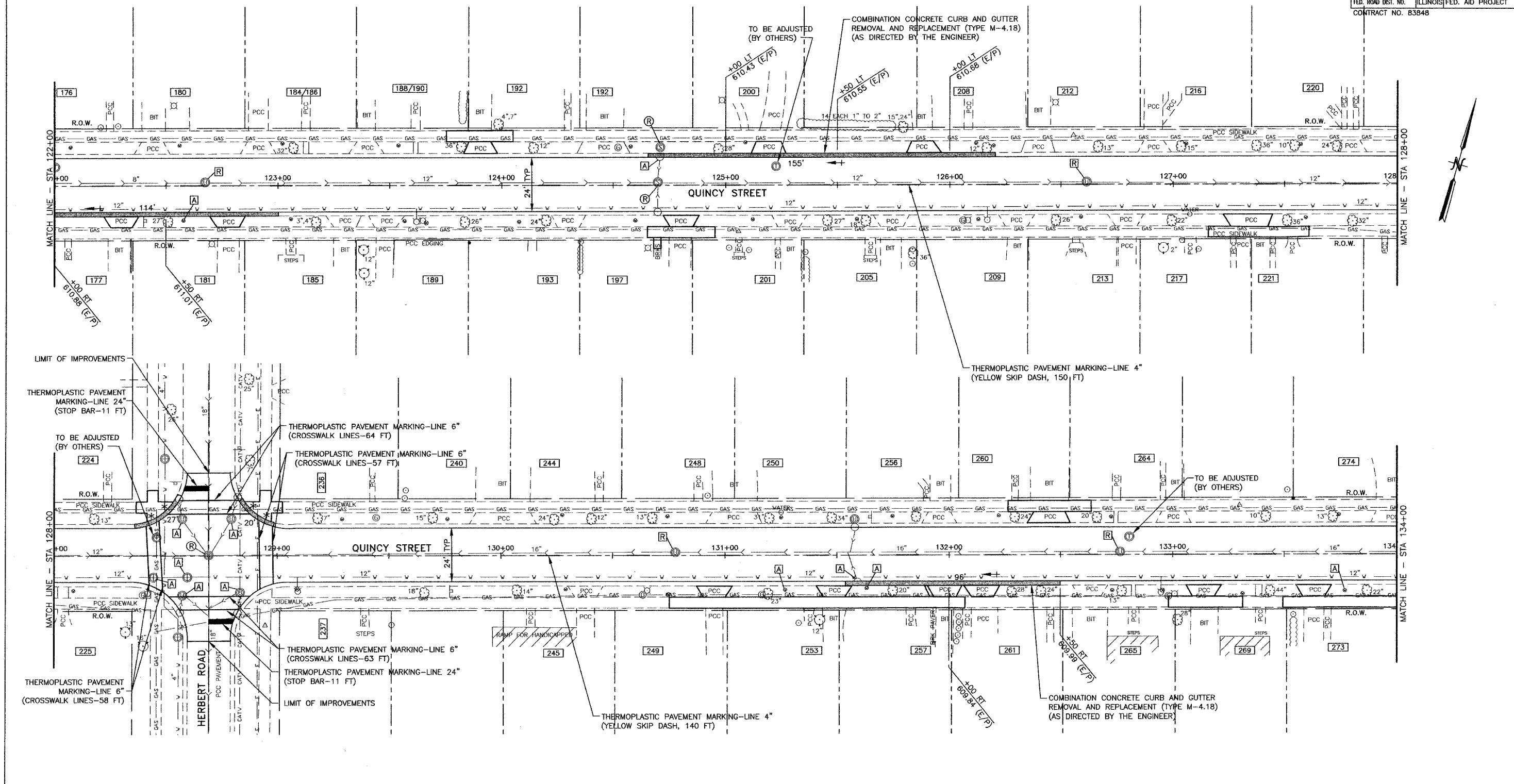
REVISIONS	
NAME	DATE
JDOT	12/22/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 1693
 QUINCY STREET
 PLAN SHEET
 STA. 111+00 TO STA. 122+00

JJB JAMES J. BENES & ASSOCIATES, INC.
 950 Warrenville Road, Suite 101, Lisle, Illinois 60532
 Tel. (630) 719-7570 • Fax (630) 719-7589

SCALE: 1"=20'
 DATE: DRAWN BY: SMP
 CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1693	05-00071-00-RS	COOK	14	6
STA. 122+00 TO STA. 134+00				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
CONTRACT NO. 83848				



LEGEND

*	HANDICAP SIDEWALK DEPRESSION
[A]	STRUCTURE TO BE ADJUSTED
(A)	STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
[R]	STRUCTURE TO BE RECONSTRUCTED
(R)	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID

REVISIONS	
NAME	DATE
ID01	12/22/05

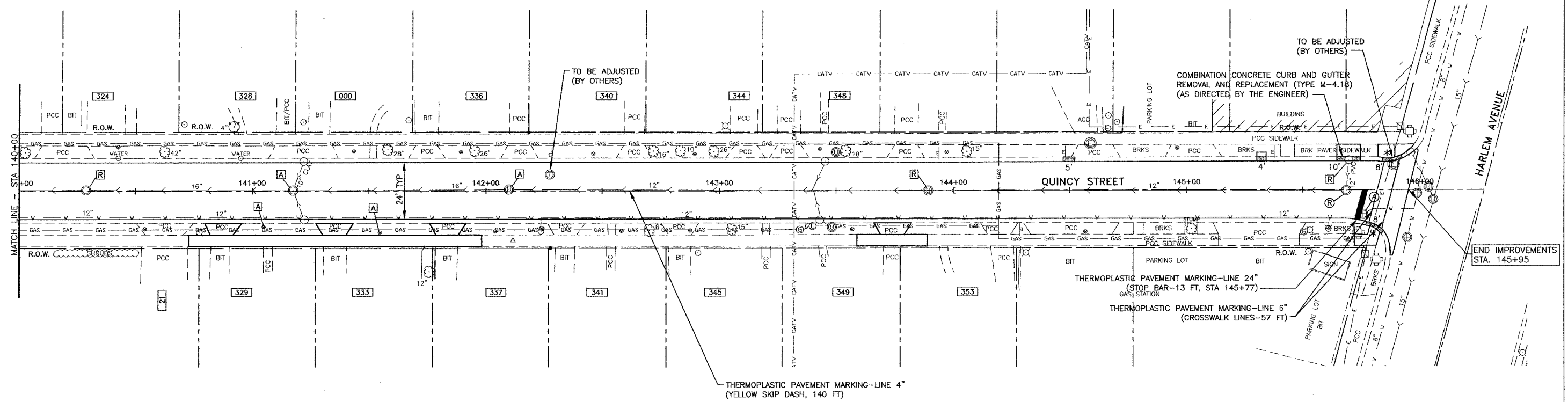
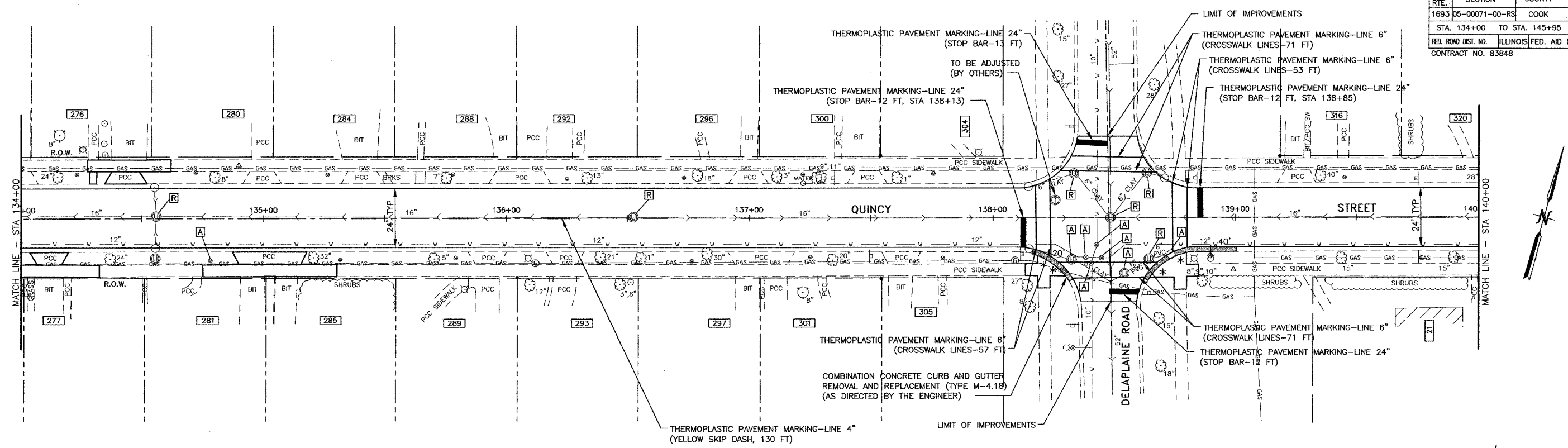
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 1693
 QUINCY STREET

PLAN SHEET
 STA. 122+00 TO STA. 134+00

JJB JAMES J. BENES & ASSOCIATES, INC.
 950 Warrenville Road, Suite 101, Lisle, Illinois 60532
 Tel. (630) 719-7570 • Fax (630) 719-7589

SCALE: 1"=20'
 DATE: _____
 DRAWN BY: SMP
 CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1693 05-00071-00-RS	COOK		14	7
STA. 134+00 TO STA. 145+95				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
CONTRACT NO. 83848				



LEGEND

*	HANDICAP SIDEWALK DEPRESSION
[A]	STRUCTURE TO BE ADJUSTED
(A)	STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
[R]	STRUCTURE TO BE RECONSTRUCTED
(R)	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID

REVISIONS

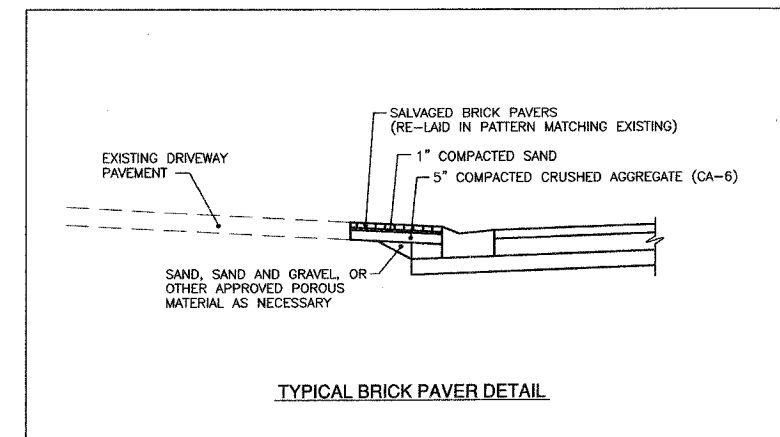
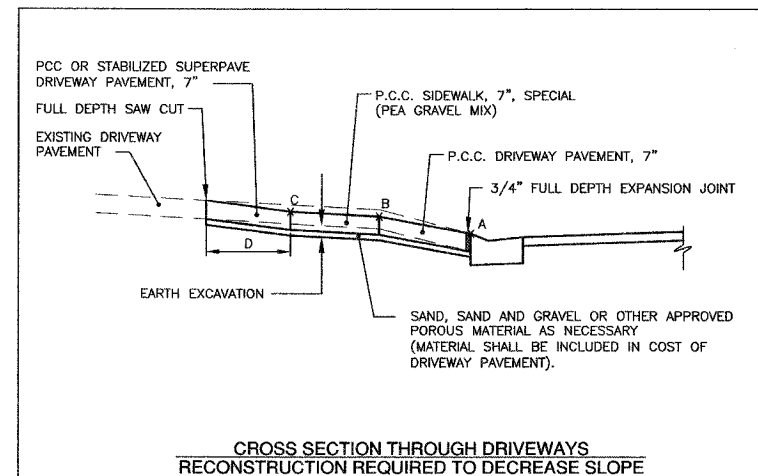
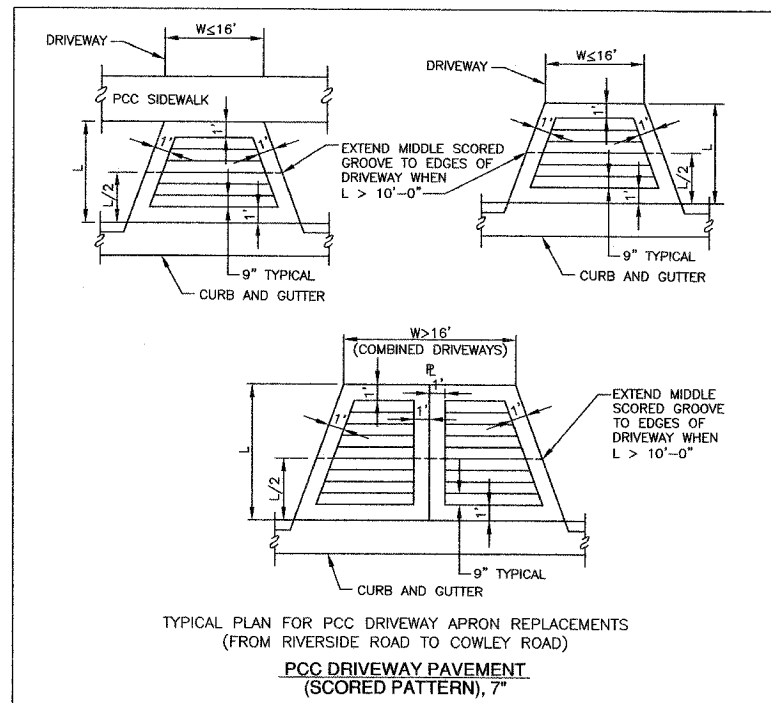
NO.	NAME	DATE
1	IDOT	12/22/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 1693
 QUINCY STREET

PLAN SHEET
 STA. 134+00 TO STA. 145+95

SCALE: 1"=20'
 DATE: _____
 DRAWN BY: SMP
 CHECKED BY: JDM

JJB JAMES J. BENES & ASSOCIATES, INC.
 950 Warrenville Road, Suite 101, Lisle, Illinois 60532
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Driveway and Sidewalk Elevations

House #	A	B	C	D (ft)
# 144	611.18	611.61	611.71	10.0
# 192	610.90	611.31	611.41	10.0
# 197	610.22	610.62	610.72	15.0
# 217/221	610.57	611.01	611.11	10.0
# 249	609.99	610.38	610.48	15.0
# 253	609.78	610.17	610.27	15.0
# 257	609.86	610.27	610.37	15.0
# 260	610.24	610.64	610.74	10.0
# 269	610.10	610.50	610.60	10.0
# 273	609.92	610.35	610.45	15.0
# 277	609.57	609.98	610.08	15.0
# 276	609.58	609.98	610.08	10.0
# 281	609.57	609.98	610.08	15.0
# 285	609.57	609.98	610.08	15.0
# 329	609.85	610.24	610.34	15.0
# 333	609.82	610.21	610.31	10.0
# 337	609.93	610.32	610.42	10.0
# 349	609.79	610.19	610.29	15.0

NOTE 1

NOTE 1: 10' UNLESS NOTED OTHERWISE ON THE PLANS (MAXIMUM WIDTHS ARE DETERMINED BY VILLAGE ORDINANCE)

DRIVEWAY SCHEDULE MODIFICATIONS

MWRD GENERAL NOTES

- THE MWRD LOCAL SEWER PERMIT SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF WORK (CALL 708-588-4055.)
- ELEVATION DATUM IS U.S.G.S.
- ALL FLOOR DRAINS SHALL DISCHARGE TO THE SANITARY SEWER SYSTEM.
- ALL DOWNSPOUTS AND FOOTING DRAINS SHALL DISCHARGE TO THE STORM SEWER SYSTEM.
- ALL SANITARY SEWER PIPE MATERIALS AND JOINTS (AND STORM SEWER PIPE MATERIALS AND JOINTS IN A COMBINED SEWER AREA) SHALL CONFORM TO:

Pipe Material Spec.	Joint Spec.	Pipe Material Spec.	Joint Spec.
<u>Vitrified Clay Pipe</u>		<u>PVC Gravity Sewer Pipe</u>	
VCP C-700	C-425	6"-15" dia. SDR 26	D-3212 or D-2855
VCP (No-Bell) C-700	C-425	D-3034	
Joint Collar	D-1784		
<u>Concrete Pipe C-14</u>		18"-27" dia. F/dy=46	D-3212 or D-2855
RCP C-76	C-443	F-679	
ACP C-428	D-1869		
<u>ABS Sewer Pipe</u>		CISP A-74	C-564
Solid Wall 6" dia. SDR 23.5		DIP A-21.51	A-21.11
ABS D-2751	D-2751		
<u>ABS Composite/Truss Pipe</u>			
8"-15" dia.			
ABS D-2680	D-2680		

- ALL SANITARY SEWER CONSTRUCTION, (AND STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS), REQUIRES STONE BEDDING WITH STONE 1/4" TO 1" IN SIZE, WITH MINIMUM BEDDING THICKNESS EQUAL TO 1/4 THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR (4) INCHES NOR MORE THAN EIGHT (8) INCHES. MATERIAL SHALL BE CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12" ABOVE THE TOP OF THE PIPE WHEN USING PVC.

- "BAND-SEAL" OR SIMILAR FLEXIBLE-TYPE COUPLINGS SHALL BE USED IN THE CONNECTION OF SEWER PIPES OF DISSIMILAR MATERIALS.
- WHEN CONNECTING TO AN EXISTING SEWER MAIN BY MEANS OTHER THAN AN EXISTING WYE, TEE, OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:
 - CIRCULAR SAW-CUT OF SEWER MAIN BY PROPER TOOLS ("SEWER-TAP" MACHINE OR SIMILAR) AND PROPER INSTALLATION OF HUB-WYE SADDLE OR HUB-TEE SADDLE.
 - REMOVE AN ENTIRE SECTION OF PIPE (BREAKING ONLY THE TOP OF ONE BELL) AND REPLACE WITH A WYE OR TEE BRANCH SECTION.
 - WITH PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING, USING A "BAND-SEAL" OR SIMILAR COUPLINGS TO HOLD IT FIRMLY IN PLACE.
- WHENEVER A SANITARY/COMBINED SEWER CROSSES UNDER A WATERMAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATERMAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATERMANS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATERMAIN LOCATED AT THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18 INCH VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CAN NOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATERMAIN, THE SEWER SHALL BE CONSTRUCTED TO WATERMAIN STANDARDS.
- ALL EXISTING SEPTIC SYSTEMS SHALL BE ABANDONED. ABANDONED TANKS SHALL BE FILLED WITH GRANULAR MATERIAL OR REMOVED.
- ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES, AND SHALL BE CAST IN PLACE OR PRE-CAST REINFORCED CONCRETE.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1693
EAST QUINCY STREET

CONSTRUCTION DETAILS

SCALE: NTS
DATE:

DRAWN BY: SMP
CHECKED BY: JDM

STRUCTURE RECONSTRUCTION SCHEDULE				
STATION	O/S (FT)	LT / RT	DESCRIPTION	TYPE 1 FRAME
100+83	2	LT	MANHOLES TO BE RECONSTRUCTED	C/L
101+13	1	LT	MANHOLES TO BE RECONSTRUCTED	C/L
104+11	15	LT	CATCH BASINS TO BE RECONSTRUCTED	C/L
107+04	12	RT	CATCH BASINS TO BE RECONSTRUCTED	O/L
114+08	0	CL	MANHOLES TO BE RECONSTRUCTED	C/L
115+00	0	CL	MANHOLES TO BE RECONSTRUCTED	C/L
116+62	1	LT	MANHOLES TO BE RECONSTRUCTED	
119+98	2	LT	MANHOLES TO BE RECONSTRUCTED	
121+92	12	LT	INLETS TO BE RECONSTRUCTED	O/L
121+92	12	RT	CATCH BASINS TO BE RECONSTRUCTED	
122+68	1	LT	MANHOLES TO BE RECONSTRUCTED	
124+70	1	LT	MANHOLES TO BE RECONSTRUCTED	C/L
124+71	17	LT	CATCH BASINS TO BE RECONSTRUCTED	C/L
126+61	1	LT	MANHOLES TO BE RECONSTRUCTED	
128+70	0	CL	MANHOLES TO BE RECONSTRUCTED	C/L
130+78	1	LT	MANHOLES TO BE RECONSTRUCTED	
132+76	1	LT	MANHOLES TO BE RECONSTRUCTED	
134+56	1	LT	MANHOLES TO BE RECONSTRUCTED	
136+53	0	CL	MANHOLES TO BE RECONSTRUCTED	
138+34	18	LT	MANHOLES TO BE RECONSTRUCTED	
138+49	0	CL	MANHOLES TO BE RECONSTRUCTED	
138+63	18	LT	MANHOLES TO BE RECONSTRUCTED	
138+65	17	RT	MANHOLES TO BE RECONSTRUCTED	
140+29	0	CL	MANHOLES TO BE RECONSTRUCTED	
143+89	0	CL	MANHOLES TO BE RECONSTRUCTED	
145+69	0	CL	MANHOLES TO BE RECONSTRUCTED	C/L
145+70	12	LT	CATCH BASINS TO BE RECONSTRUCTED	

STRUCTURE ADJUSTMENT SCHEDULE				
STATION	O/S (FT)	LT / RT	DESCRIPTION	TYPE 1 FRAME
100+01	8	LT	FRAMES AND LIDS TO BE ADJUSTED	
100+23	1	LT	FRAMES AND LIDS TO BE ADJUSTED	
100+23	10	LT	FRAMES AND LIDS TO BE ADJUSTED	
101+40	0	CL	FRAMES AND LIDS TO BE ADJUSTED	
103+88	15	RT	FRAMES AND LIDS TO BE ADJUSTED	
104+16	0	CL	FRAMES AND LIDS TO BE ADJUSTED	
104+86	1	RT	FRAMES AND LIDS TO BE ADJUSTED	C/L
105+49	0	CL	FRAMES AND LIDS TO BE ADJUSTED	
105+57	17	LT	FRAMES AND LIDS TO BE ADJUSTED	O/L
105+64	15	RT	FRAMES AND LIDS TO BE ADJUSTED	O/L
107+16	0	CL	FRAMES AND LIDS TO BE ADJUSTED	
110+65	0	CL	FRAMES AND LIDS TO BE ADJUSTED	
111+11	12	RT	FRAMES AND LIDS TO BE ADJUSTED	O/L
114+76	14	RT	FRAMES AND GRATES TO BE ADJUSTED	
114+86	3	LT	FRAMES AND LIDS TO BE ADJUSTED	
114+90	16	LT	FRAMES AND LIDS TO BE ADJUSTED	
114+90	15	RT	FRAMES AND LIDS TO BE ADJUSTED	
114+96	12	RT	FRAMES AND LIDS TO BE ADJUSTED	
115+10	16	LT	FRAMES AND LIDS TO BE ADJUSTED	
115+11	16	RT	FRAMES AND LIDS TO BE ADJUSTED	
115+21	14	RT	FRAMES AND LIDS TO BE ADJUSTED	
118+33	12	RT	FRAMES AND LIDS TO BE ADJUSTED	
118+34	0	CL	FRAMES AND LIDS TO BE ADJUSTED	
118+36	18	LT	FRAMES AND LIDS TO BE ADJUSTED	C/L
124+70	12	LT	FRAMES AND LIDS TO BE ADJUSTED	
128+45	10	RT	FRAMES AND LIDS TO BE ADJUSTED	
128+58	16	LT	FRAMES AND LIDS TO BE ADJUSTED	
128+58	18	RT	FRAMES AND LIDS TO BE ADJUSTED	
128+60	10	RT	FRAMES AND LIDS TO BE ADJUSTED	
128+80	16	LT	FRAMES AND LIDS TO BE ADJUSTED	
128+84	16	RT	FRAMES AND LIDS TO BE ADJUSTED	
131+59	12	RT	FRAMES AND LIDS TO BE ADJUSTED	
138+32	17	RT	FRAMES AND LIDS TO BE ADJUSTED	
138+54	23	RT	FRAMES AND LIDS TO BE ADJUSTED	
138+73	15	RT	FRAMES AND LIDS TO BE ADJUSTED	
141+17	0	CL	FRAMES AND LIDS TO BE ADJUSTED	
142+10	1	LT	FRAMES AND LIDS TO BE ADJUSTED	
145+75	10	RT	FRAMES AND LIDS TO BE ADJUSTED	

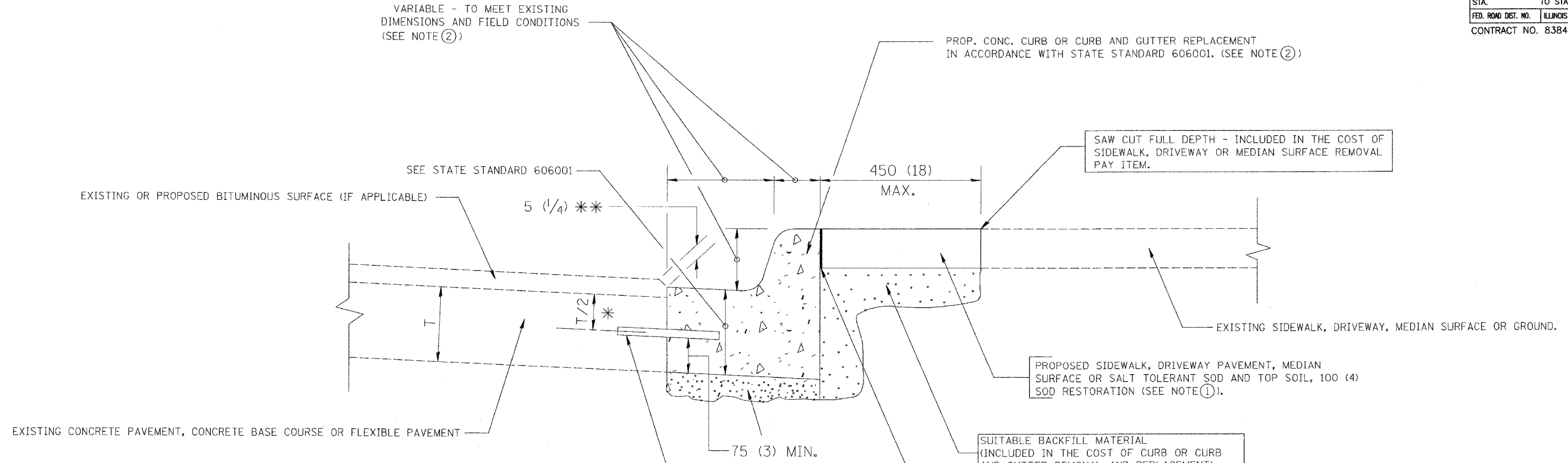
DRIVEWAY REPLACEMENT SCHEDULE								
STATION / OFFSET	ADDRESS NO.	PROPOSED WIDTH AT SIDEWALK (FOOT)	PROPOSED WIDTH AT BACK / CURB (FOOT)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	EARTH * EXCAVATION (CU YD)	STABILIZED DRIVEWAYS SUPERPAVE, 7" (SQ YD)	P.C.C. DRIVEWAY PVMT, 7" (SQ YD)	
106+13	12' LT	# 48/54	24	30	16.5		16.5 S.P.	
107+17	12' LT	# 55/59	20	26	11.5		11.5 S.P.	
111+64	12' RT	# 89/93	20	26	12.8		12.8 S.P.	
118+29	12' LT	# 144	9	14	14.7	4.28	14.7	
118+42	12' RT	# 149	11	17	7.0		7.0	
118+90	12' RT	# 153	10	16	6.5		6.5	
119+40	12' RT	# 157	10	14	6.0		6.0	
121+90	12' LT	# 176	10	16	8.0		8.0	
123+91	12' LT	# 192	10	15	19.8	3.01	12.2	7.6
124+80	12' RT	# 197	10	16	17.1	5.01	10.6	6.5
125+19	12' LT	# 200	11	16	8.3		8.3	
125+88	12' LT	# 208	10	16	7.2		7.2	
127+33	12' RT	# 217/221	17	22	28.6	4.74	8.9	19.7
130+95	12' RT	# 249	10	16	19.7	4.73		19.7
131+49	12' RT	# 253	10	16	23.2	8.21	16.7	6.5
131+98	12' RT	# 257	10	15	20.3	5.75	13.3	7.0
132+45	12' LT	# 260	14	19	28.7	4.24	19.5	9.2
133+14	12' RT	# 269	10	16	21.1	4.28	13.9	7.2
133+63	12' RT	# 273	10	16	22.2	6.73		22.2
134+11	12' RT	# 277	10	16	27.2	8.34	20.0	7.2
134+43	12' LT	# 276	12	18	25.0	4.28	16.7	8.3
135+03	12' RT	# 281/285	26	31	48.3	9.73	32.5	15.8
140+88	12' RT	# 329	10	16	21.4	5.13	14.2	7.2
141+35	12' RT	# 333	10	16	18.3	8.89	11.1	7.2
141+84	12' RT	# 337	10	16	17.8	4.72	10.6	7.2
143+74	12' RT	# 349	10	16	19.7	5.19		19.7
TOTALS				477	97.26	200	277	

* THE EARTH EXCAVATION TOTAL INCLUDES REGRADING PARKWAYS ADJACENT TO DRIVEWAYS AND SIDEWALKS AND IS MEASURED AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
S.P. = SCORED PATTERN

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.U. 1693 EAST QUINCY STREET
NAME	DATE	
		CONSTRUCTION DETAILS

SCALE: N.T.S. DRAWN BY: SMP
DATE: CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1693	05-00071-00-RS	COOK	14	10
STA. TO STA.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 83848				



* 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SALT TOLERANT SOD AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

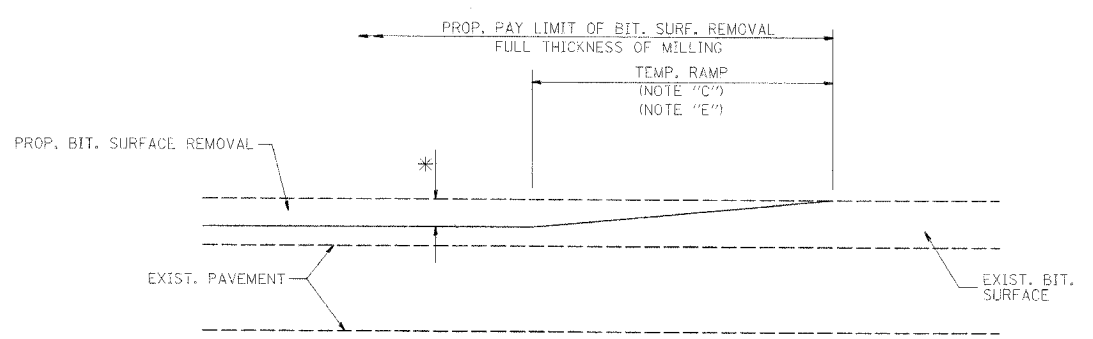
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

REVISIONS	
NAME	DATE
M. DE YONG	05/28/91
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01

ILLINOIS DEPARTMENT OF TRANSPORTATION
**CURB OR
 CURB AND GUTTER
 REMOVAL AND REPLACEMENT**

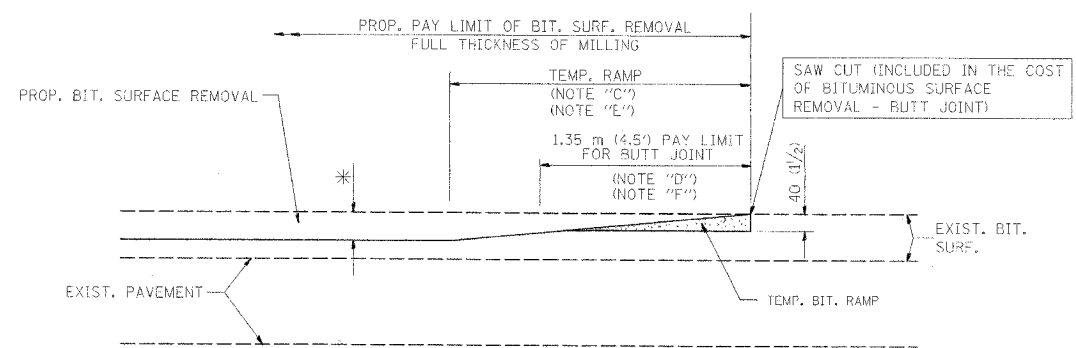
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 DRAWN BY
 CHECKED BY
 BD600-06 (B0-24)

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1693	05-00071-00-RS	COOK	14	11
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83848				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

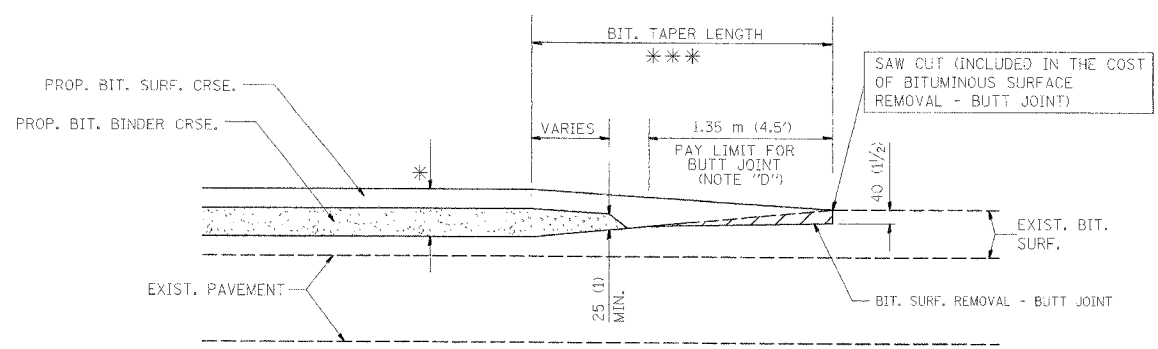
OPTION 1



BITUMINOUS CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

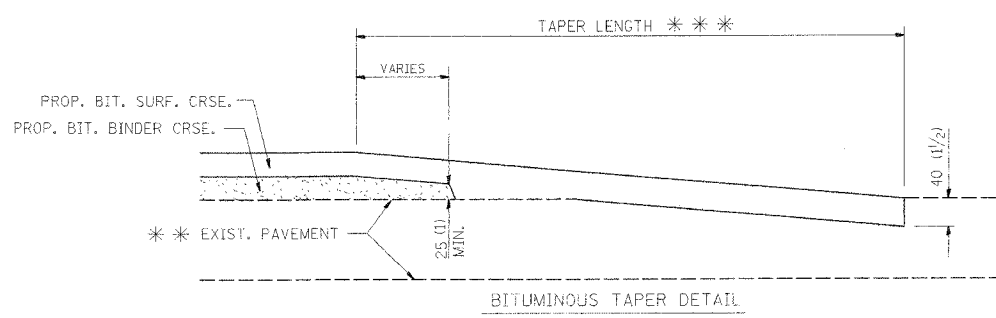
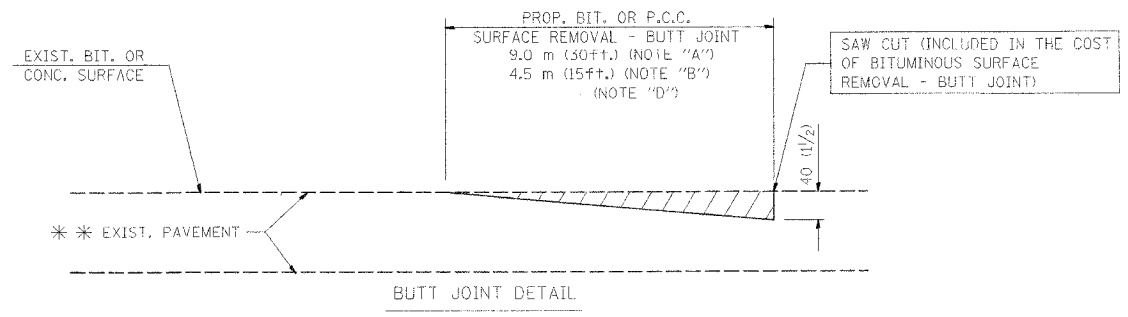
OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT AND BITUMINOUS TAPER

TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR RESURFACING ONLY

*** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 FT.) PER INCH OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
 - G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND BITUMINOUS TAPER DETAILS

BASIS OF PAYMENT:

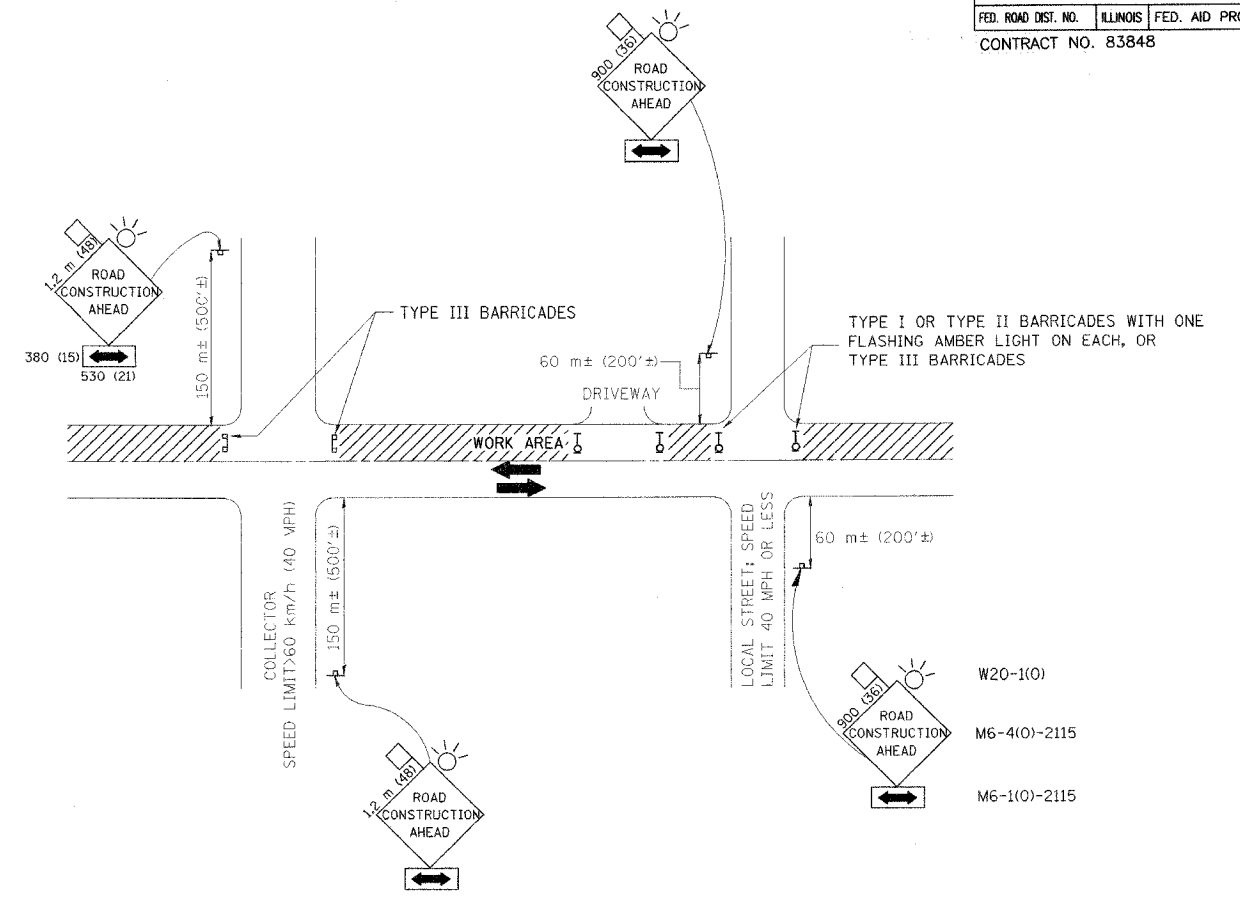
THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97

SCALE: NONE
DATE: **DATE**

DRAWN BY
CHECKED BY

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1693	05-00071-00-RS	COOK	14	12
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83848				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

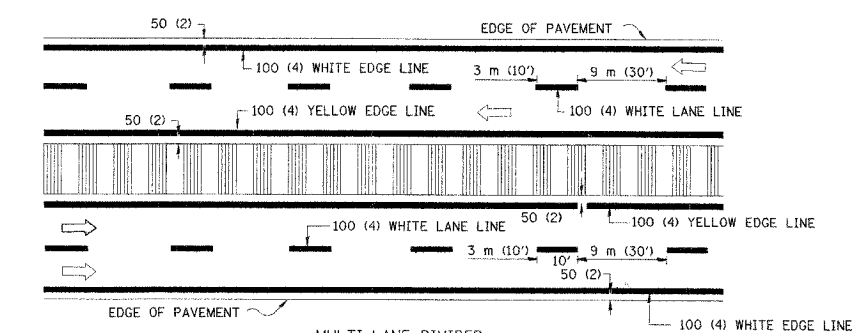
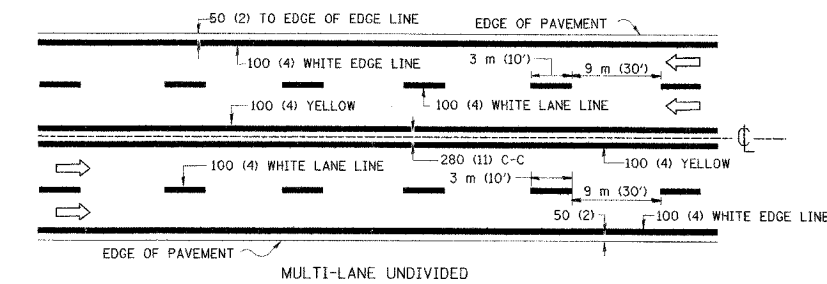
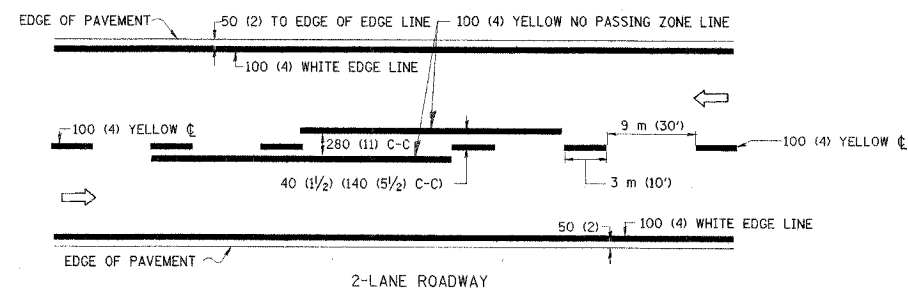
- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

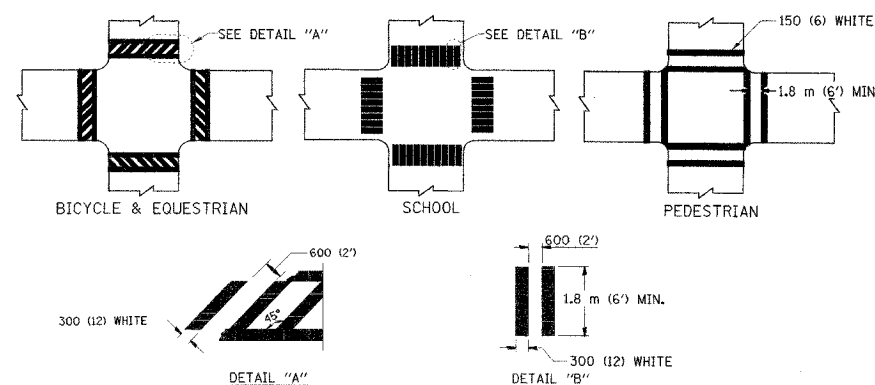
REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96

SCALE: NONE
 DATE: ##DATE##
 DRAWN BY
 CHECKED BY

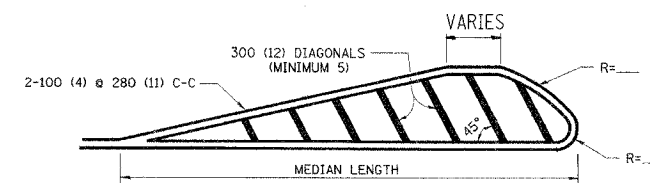
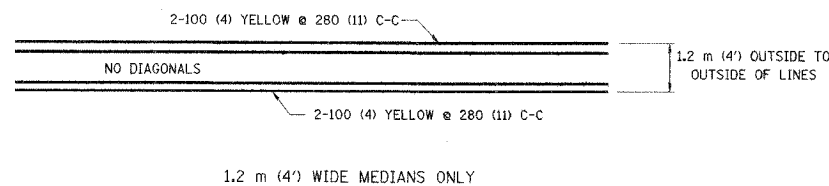


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



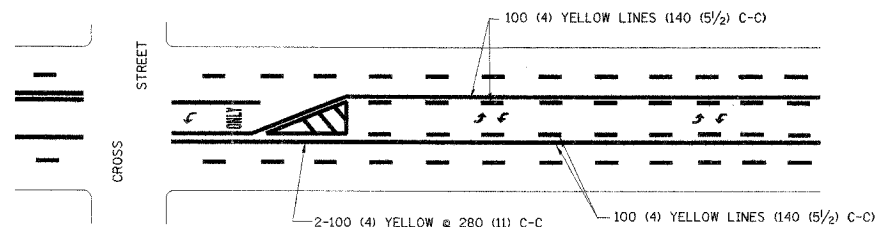
TYPICAL CROSSWALK MARKING



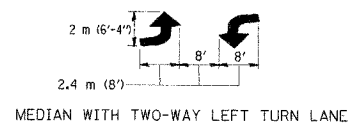
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 15 m (50') C-C (LESS THAN 50 km/h (30 MPH))
 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH))
 45 m (150') C-C (MORE THAN 70 km/h (45 MPH))

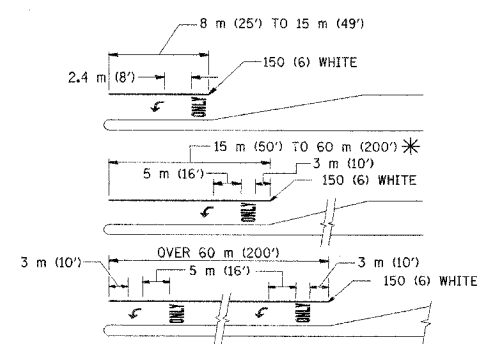
MEDIANS OVER 1.2 m (4') WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



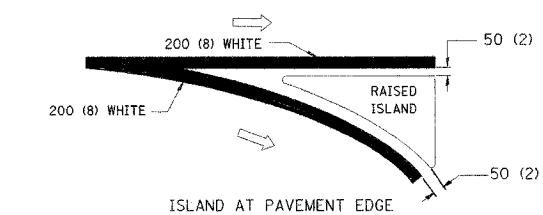
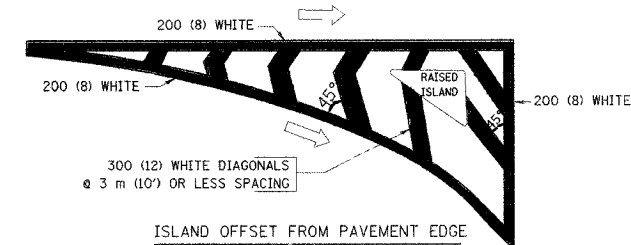
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT.
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.40 m² (4.3 SQ. FT.) EACH "X"=5.0 m² (54.0 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 1997 AND STATE STANDARD 780001.

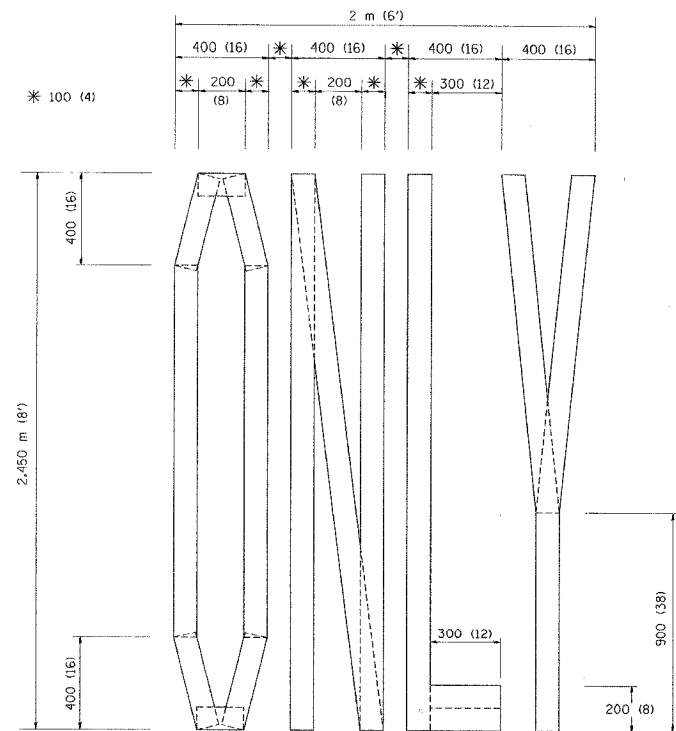
All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 TYPICAL PAVEMENT MARKINGS

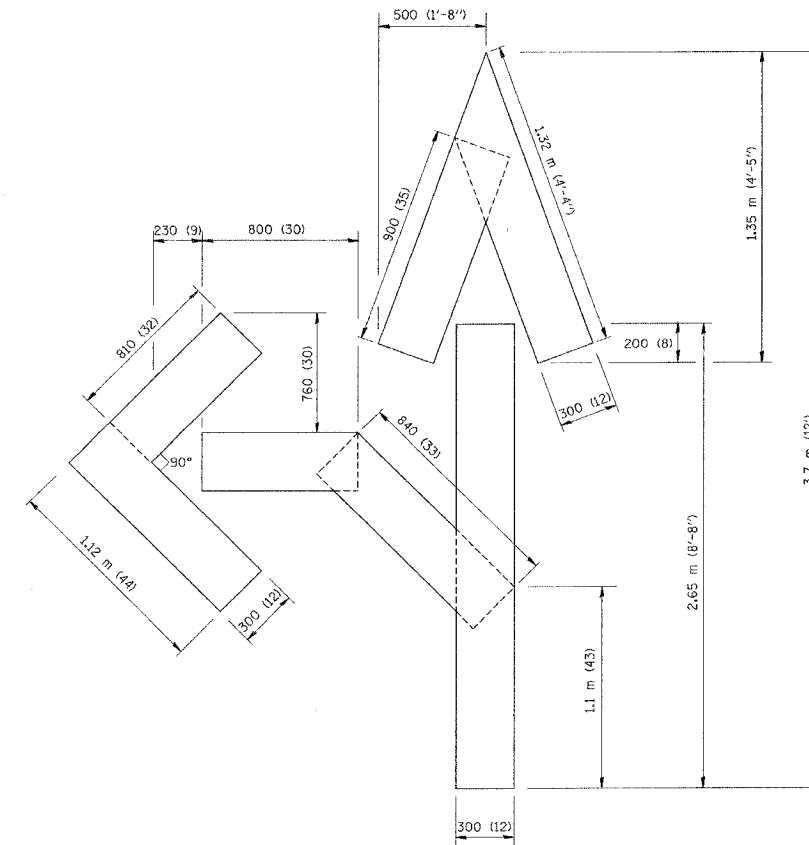
REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96

SCALE: NONE
 DATE **DATE**
 DRAWN BY CADD
 CHECKED BY

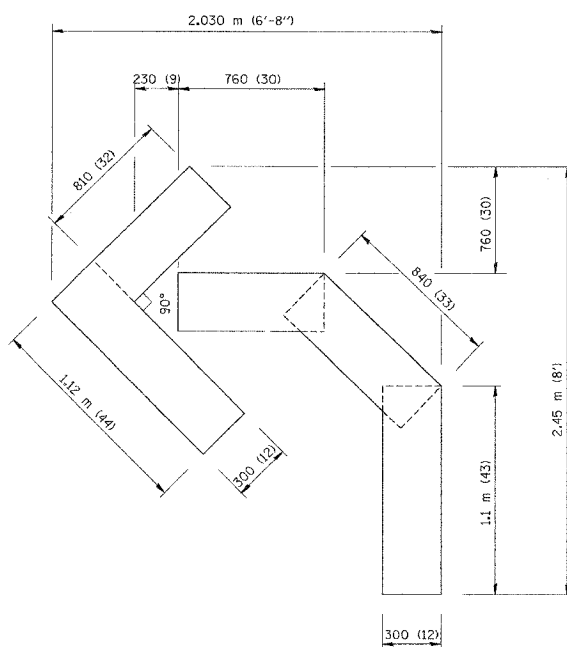
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1893	05-00071-00-RS	COOK	14	14
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 83848				



QUANTITY
 100 (4) LINE = 19.7 m (64.1 ft.)
 1.97 sq. m (21.1 sq. ft.)



QUANTITY
 100 (4) LINE = 25.3 m (82.5 ft.)
 2.53 sq. m (27.5 sq. ft.)



QUANTITY
 100 (4) LINE = 13.9 m (45.5 ft.)
 1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

SCALE: NONE
 DATE 10/18/2002

DRAWN BY CADD
 CHECKED BY TC-16

REVISION DATE: 08/28/00