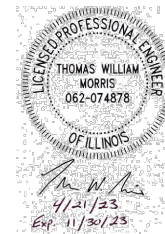


CONSTRUCTION PLANS FOR



DESIGN PROFESSIONALS OF RECORD



CIVIL ENGINEER:
 THOMAS W. MORRIS
 ILLINOIS PE 062-074878
 RESPONSIBLE FOR: GI100-GI104
 GC001-GC004
 GC101-GC105
 GC501-GC503
 LG101-LG102
 LG501
 CS101
 CH101
 CI501
 CP101
 CP501
 CP801
 CG101-CG102
 CG301-CG303
 CG501-CG507
 CG701-CG706



CIVIL ENGINEER:
 FRANCIS P. BAUER
 ILLINOIS PE 062-074654
 RESPONSIBLE FOR: CD101
 CM101
 CM501-502



ELECTRICAL ENGINEER:
 ASAD M. BAJWA
 ILLINOIS PE 062-055662
 RESPONSIBLE FOR: EL101
 EL501-EL506

BLV PROJECT NO. 2023-09
 ILLINOIS PROJECT NO. BLV-5101
 AIP PROJECT NO. 3-17-0146-TBD

TERMINAL APRON EXPANSION - PROJECT 1
 BID ISSUE
 APRIL 21, 2023

* CONTRACTOR SHALL IDENTIFY SPECIFIC LOCATIONS
 WHEN SUBMITTING JULIE REQUEST.

811 Know what's below. Call before you dig. J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

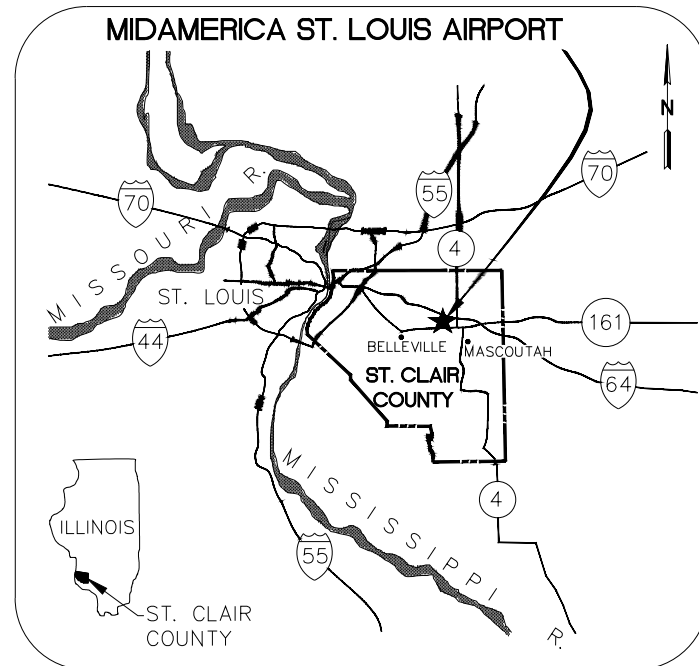
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

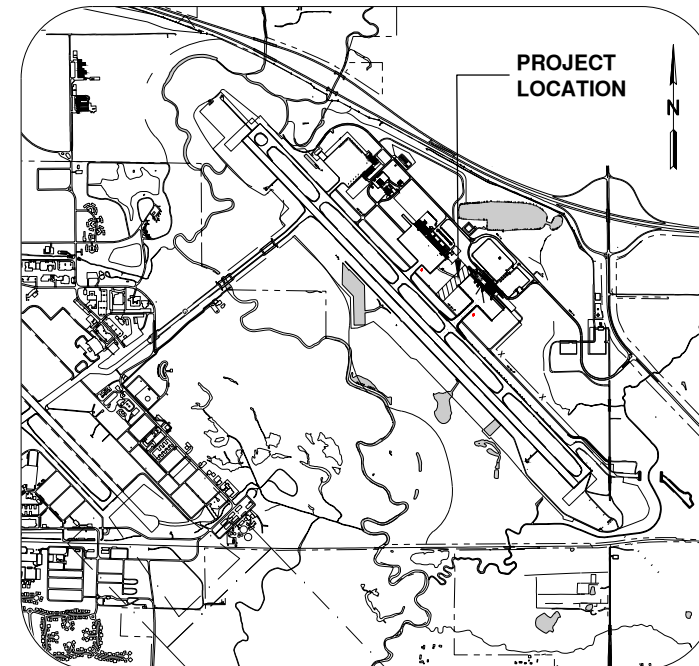
DESIGN INFORMATION

GEOMETRIC CRITERIA

AIRCRAFT APPROACH CATEGORY (AAC):	C
AIRPLANE DESIGN GROUP (ADG):	III
TAXIWAY DESIGN GROUP (TDG):	3
TAXIWAY SAFETY AREA (TSA):	118'
TAXIWAY OBJECT FREE AREA (TOFA):	171'
TAXILANE OBJECT FREE AREA (TLOFA):	158'
RUNWAY SAFETY AREA (RSA):	500'
RUNWAY OBJECT FREE AREA (ROFA):	800'



LOCATION MAP



SITE PLAN

MIDAMERICA ST. LOUIS AIRPORT
 AIRPORT SPONSOR
 APPROVED: *[Signature]*
 BRYAN E. JOHNSON, A.A.E., AIRPORT DIRECTOR
 DATE: APRIL 21, 2023

CMT
 ■ AURORA, IL ■ CHICAGO, IL ■ COLUMBUS, OH
 ■ EDWARDSVILLE, IL ■ INDIANAPOLIS, IN ■ PEORIA, IL
 ■ ROCKFORD, IL ■ SPRINGFIELD, IL ■ ST. LOUIS, MO
 ■ SPRINGFIELD, MO ■ KANSAS CITY, MO
 SUBMITTED BY: *[Signature]*
 DATE: APRIL 21, 2023
 CMT JOB NUMBER: 22001186--00



License No. 184-000613

CONSULTANTS

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3	GI102	SUMMARY OF QUANTITIES
4	GI103	SITE PLAN
5	GI104	ILS CRITICAL AREAS
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7	GC002	CONSTRUCTION ACTIVITY PLAN NOTES 2
8	GC003	CONSTRUCTION ACTIVITY PLAN NOTES 3
9	GC004	CONSTRUCTION ACTIVITY PLAN NOTES 4
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11	GC102	CONSTRUCTION ACTIVITY PLAN - PHASE 1
12	GC103	CONSTRUCTION ACTIVITY PLAN - PHASE 1B
13	GC104	CONSTRUCTION ACTIVITY PLAN - PHASE 2
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37	CG505	DRAINAGE DETAILS 5
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BID ISSUE
APRIL 21, 2023

TERMINAL APRON EXPANSION -
PROJECT 1



MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

MARK | DATE | DESCRIPTION

BLV PROJECT NO. 2022-14
IL PROJECT NO. BLV-5087
CMT PROJECT NO: 22001186.00
CAD DWG FILE: 22001186 - GI100.DWG
DESIGNED BY: CMT
DRAWN BY: %%U
CHECKED BY: CMT
APPROVED BY: CMT
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SHEET TITLE

SHEET INDEX

SHEET 2 OF 60
GI101



License No. 184-000613

CONSULTANTS

SUMMARY OF QUANTITIES BASE BID - TERMINAL APRON EXPANSION - PROJECT 1					
ITEM #	DESCRIPTION	UNIT	NORTH EXPANSION QUANTITY	TOTAL PROJECT QUANTITY	RECORDED QUANTITY
AW108040	#4/0 XLP-USE CABLE	FOOT	200.0	200.0	
AW108086	#6 XLP-USE CABLE	FOOT	4,200.0	4,200.0	
AW108088	#8 XLP-USE CABLE	FOOT	95.0	95.0	
AW108090	#10 XLP-USE CABLE	FOOT	4,000.0	4,000.0	
AW108092	#12 XLP-USE CABLE	FOOT	9,900.0	9,900.0	
AW108108	1/C #8 5 KV UG CABLE	FOOT	350.0	350.0	
AW108706	1/C #6 COUNTERPOISE	FOOT	175.0	175.0	
AW108960	REMOVE CABLE	FOOT	710.0	710.0	
AW110201	1" PVC DUCT, DIRECT BURY	FOOT	2,325.0	2,325.0	
AW110202	2" PVC DUCT, DIRECT BURY	FOOT	875.0	875.0	
AW110502	2-WAY CONCRETE ENCASED DUCT	FOOT	370.0	370.0	
AW110610	ELECTRICAL HANDHOLE	EACH	4.0	4.0	
AW125415	MILT-BASE MOUNTED	EACH	3.0	3.0	
AW125902	REMOVE BASE MOUNTED LIGHT	EACH	1.0	1.0	
AW150510	ENGINEER'S FIELD OFFICE	L SUM	1.0	1.0	
AW150520	MOBILIZATION	L SUM	1.0	1.0	
AW152410	UNCLASSIFIED EXCAVATION	CU YD	11,846.0	11,846.0	
AW155540	BY-PRODUCT LIME	TON	300.0	300.0	
AW155612	SOIL PROCESSING-12"	SQ YD	7,094.0	7,094.0	
AW156510	SILT FENCE	FOOT	1,507.0	1,507.0	
AW156511	DITCH CHECK	EACH	6.0	6.0	
AW156520	INLET PROTECTION	EACH	1.0	1.0	
AW156531	EROSION CONTROL BLANKET	SQ YD	2,000.0	2,000.0	
AW161515	TEMPORARY CLASS C FENCE	FOOT	1,150.0	1,150.0	
AW161516	TEMPORARY CLASS C FENCE WITH JERSEY BARRIER	FOOT	445.0	445.0	
AW161601	TEMPORARY GATE	EACH	1.0	1.0	
AW162508	CLASS E FENCE 8'	FOOT	265.0	265.0	
AW209609	CRUSHED AGG. BASE COURSE- 9"	SQ YD	6,910.0	6,910.0	
AW209611	CRUSHED AGGREGATE BASE COURSE- 11"	SQ YD	184.0	184.0	
AW302611	ASPHALT TREATED PERMEABLE SUBBASE	SQ YD	6,313.0	6,313.0	
AW302630	ATPS TEST SECTION	EACH	1.0	1.0	
AW401610	BITUMINOUS SURFACE COURSE	TON	292.0	292.0	
AW401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	1,613.0	1,613.0	
AW501509	9" PCC PAVEMENT	SQ YD	513.0	513.0	
AW501516	16" PCC PAVEMENT	SQ YD	5,573.0	5,573.0	
AW501530	PCC TEST BATCH	EACH	1.0	1.0	
AW602510	BITUMINOUS PRIME COAT	GALLON	255.0	255.0	
AW603510	BITUMINOUS TACK COAT	GALLON	218.0	218.0	
AW620520	PAVEMENT MARKING-WATERBORNE	SQ FT	3,600.0	3,600.0	
AW620525	PAVEMENT MARKING-BLACK BORDER	SQ FT	1,200.0	1,200.0	
AW620900	PAVEMENT MARKING REMOVAL	SQ FT	650.0	650.0	
AW701524	24" RCP, CLASS IV	FOOT	360.0	360.0	
AW701536	36" RCP, CLASS IV	FOOT	185.0	185.0	
AW701900	REMOVE PIPE	FOOT	74.0	74.0	
AW705526	6" PERFORATED UNDERDRAIN W/SOCK	FOOT	1,087.0	1,087.0	
AW705635	UNDERDRAIN COLLECTION STRUCTURE	EACH	4.0	4.0	
AW705640	UNDERDRAIN CLEANOUT	EACH	9.0	9.0	
AW751416	TYPE 1 INLET	EACH	1.0	1.0	
AW752424	PRECAST REINFORCED CONC. FES 24"	EACH	2.0	2.0	
AW752436	PRECAST REINFORCED CONC. FES 36"	EACH	1.0	1.0	
AW752900	REMOVE END SECTION	EACH	2.0	2.0	
AW754610	PAVED DITCH	FOOT	490.0	490.0	
AW770508	8" SANITARY SEWER	FOOT	394.0	394.0	
AW770704	SANITARY MANHOLE 4'	EACH	1.0	1.0	
AW770900	REMOVE SANITARY SEWER	FOOT	178.0	178.0	
AW801966	3" GRS CONDUIT	FOOT	50.0	50.0	
AW801967	RECEPTACLE PEDESTAL	EACH	5.0	5.0	
AW801968	STRUT FRAMING AND PEDESTAL	L SUM	1.0	1.0	
AW801969	UTILITY SERVICE INSTALLATION	L SUM	1.0	1.0	
AW801971	4' X 20' TRENCH DRAIN	EACH	1.0	1.0	
AW801972	8" VALVE & ACTUATOR	EACH	1.0	1.0	
AW801973	CONTROL SWITCHES	L SUM	1.0	1.0	
AW801980	50KVA DISTRIBUTION TRANSFORMER, 480-120/240V, 1PH, 3W	EACH	1.0	1.0	
AW801981	DISTRIBUTION PANEL BOARD, 200A, 480/277V, 3PH, 4W, NEMA 3R, 30C	EACH	1.0	1.0	
AW801985	POWER PANEL, 200A, 120/240V, 1PH, 3W, NEMA 3R	EACH	1.0	1.0	
AW801992	YIELD SIGN AND POST	EACH	1.0	1.0	
AW801994	8" SANITARY FORCEMAIN	FOOT	178.0	178.0	
AW801996	REMOVE FENCE	FOOT	265.0	265.0	
AW801997	FIBER OPTIC CABLE	FOOT	400.0	400.0	
AW801998	NON-FUSIBLE SERVICE DISCONNECT, 200A, 600V, 2-POLE, NEMA 3R	EACH	1.0	1.0	
AW801999	#14 XLP-USE CABLE	FOOT	12,100.0	12,100.0	
AW901510	SEEDING	ACRE	4.0	4.0	
AW904510	SODDING	SQ YD	350.0	350.0	
AW908510	MULCHING	ACRE	4.0	4.0	
AW910915	REMOVE ROADWAY SIGN	EACH	1.0	1.0	

SUMMARY OF QUANTITIES ALTERNATE 1 - TERMINAL APRON EXPANSION - PROJECT 1					
ITEM #	DESCRIPTION	UNIT	NORTH EXPANSION AA QUANTITY	TOTAL AA QUANTITY	RECORDED QUANTITY
AX108088	#8 XLP-USE CABLE	FOOT	580.0	580.0	
AX108090	#10 XLP-USE CABLE	FOOT	800.0	800.0	
AX110201	1" PVC DUCT, DIRECT BURY	FOOT	175.0	175.0	
AX110202	2" PVC DUCT, DIRECT BURY	FOOT	175.0	175.0	
AX801474	LIGHTING CONTROLLER IN NEMA 3R ENCLOSURE	L SUM	1.0	1.0	
AX801993	60' APRON LIGHT POLE W/FIXTURES	EACH	2.0	2.0	
AX801995	PASSENGER BOARDING RAMP	EACH	1.0	1.0	
AX801997	FIBER OPTIC CABLE	FOOT	300.0	300.0	

BID ISSUE
APRIL 21, 2023

TERMINAL APRON EXPANSION -
PROJECT 1



MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

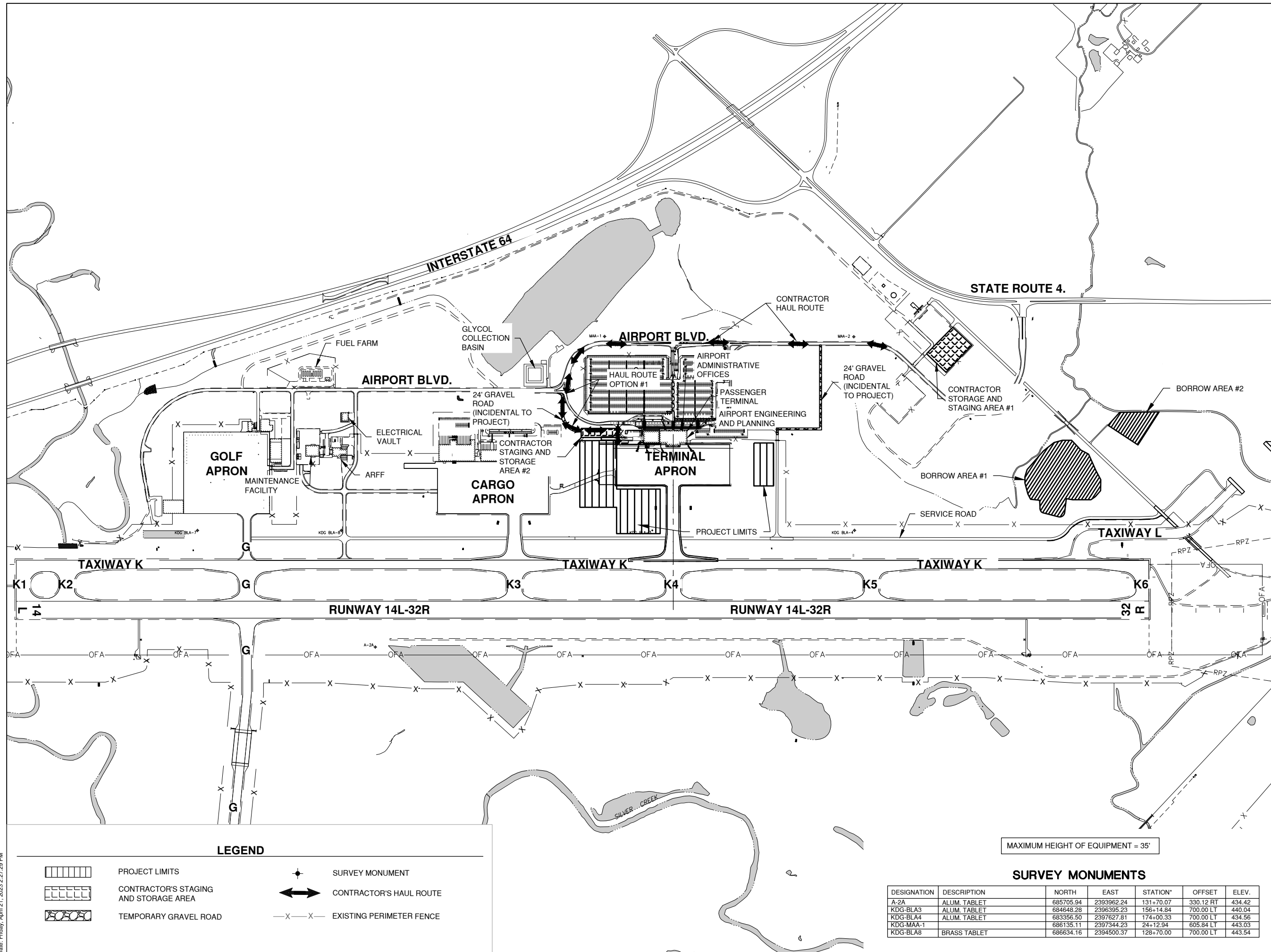
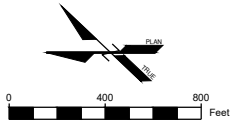
MARK | DATE | DESCRIPTION

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 IL PROJECT NO. BLV-5087
 CMT PROJECT NO: 22001186.00
 CAD DWG FILE: 22001186 - G1100 UPDATED.DWG
 DESIGNED BY: CMT
 DRAWN BY: %%U
 CHECKED BY: CMT
 APPROVED BY: CMT
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SHEET TITLE

SUMMARY OF
QUANTITIES

SHEET 3 OF 60
G1102



BID ISSUE
APRIL 21, 2023

TERMINAL APRON EXPANSION -
PROJECT 1



MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION
		BLV PROJECT NO. 2022-14
		IL PROJECT NO. BLV-5087
		CMT PROJECT NO: 22001186.00
		CAD DWG FILE: 22001186 - G1100.DWG
		DESIGNED BY: CMT
		DRAWN BY: %%U
		CHECKED BY: CMT
		APPROVED BY: CMT
		COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2021

SHEET TITLE

SITE PLAN

LEGEND

- PROJECT LIMITS
- CONTRACTOR'S STAGING AND STORAGE AREA
- TEMPORARY GRAVEL ROAD
- SURVEY MONUMENT
- CONTRACTOR'S HAUL ROUTE
- EXISTING PERIMETER FENCE

MAXIMUM HEIGHT OF EQUIPMENT = 35'

SURVEY MONUMENTS

DESIGNATION	DESCRIPTION	NORTH	EAST	STATION*	OFFSET	ELEV.
A-2A	ALUM. TABLET	685705.94	2393962.24	131+70.07	330.12 RT	434.42
KDG-BLA3	ALUM. TABLET	684648.28	2396395.23	156+14.84	700.00 LT	440.04
KDG-BLA4	ALUM. TABLET	683356.50	2397627.81	174+00.33	700.00 LT	434.56
KDG-MAA-1		686135.11	2397344.23	24+12.94	605.84 LT	443.03
KDG-BLA8	BRASS TABLET	686634.16	2394500.37	128+70.00	700.00 LT	443.54

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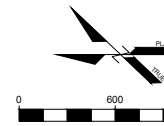
LEGEND

- RSA — RUNWAY SAFETY AREA
- ROFA — RUNWAY OBJECT FREE AREA
- TSA — TAXIWAY SAFETY AREA
- TOFA — TAXIWAY OBJECT FREE AREA
- //// ILS CRITICAL AREA
- ▨ PROJECT LIMITS
- ↔ HAUL ROUTE
- SURVEY MONUMENT

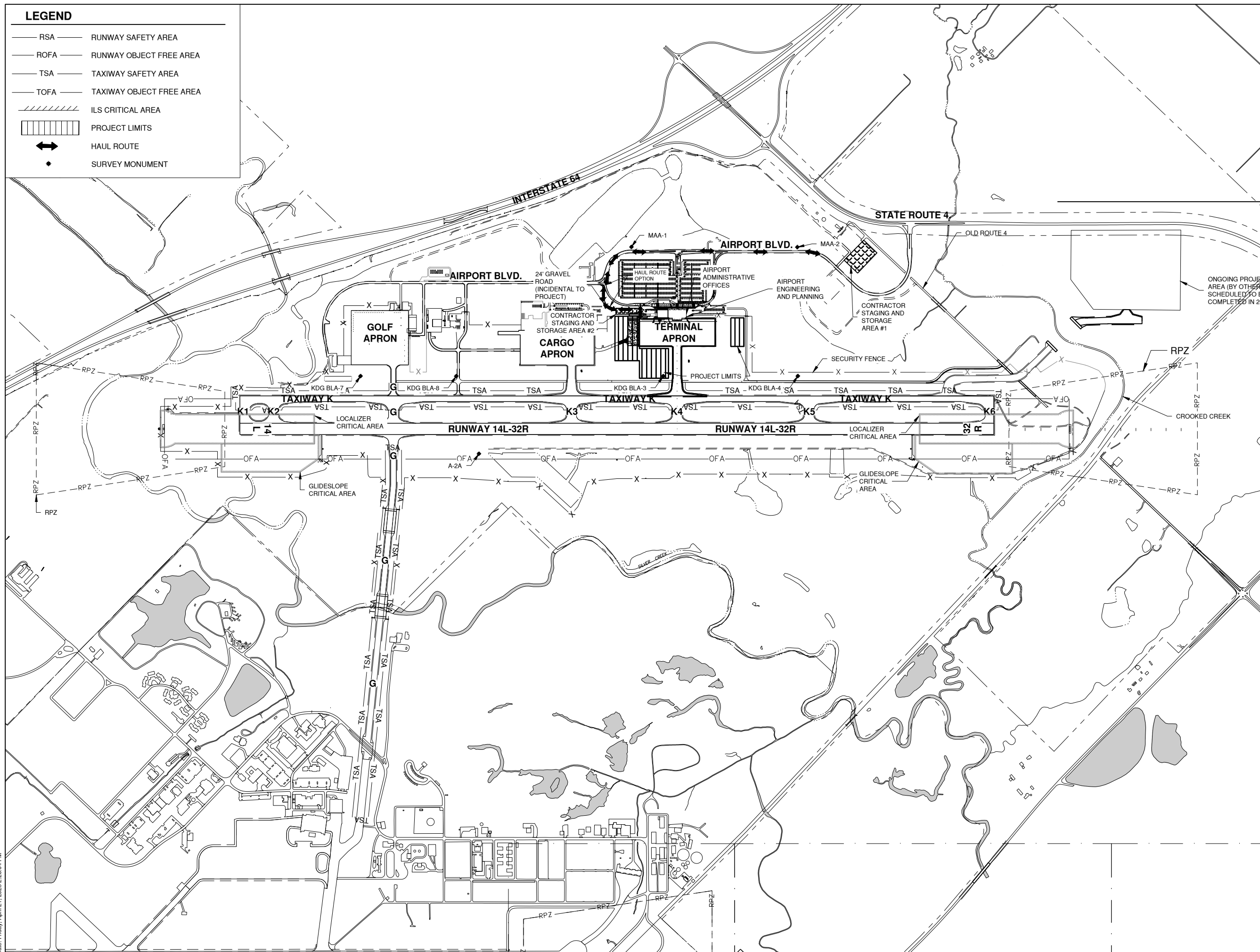


License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)



BID ISSUE
APRIL 21, 2023

TERMINAL APRON EXPANSION - PROJECT 1



MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION
		BLV PROJECT NO. 2021-38
		IL PROJECT NO. BLV-4911
		CMT PROJECT NO: 22001186.00
		CAD DWG FILE: CRITICAL AREAS.DWG
		DESIGNED BY: CMT
		DRAWN BY: %%U
		CHECKED BY: CMT
		APPROVED BY: CMT
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SHEET TITLE

ILS CRITICAL AREAS

SHEET 5 OF 60
G1104

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(NOTES CONTINUED FROM SHEET GC001)

- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING THE WORK. NO ADDITIONAL COMPENSATION WILL BE MADE FOR THIS WORK.
- THE CONTRACTOR SHALL ENSURE ALL VEHICLE AND EQUIPMENT OPERATORS UTILIZED ON THE PROJECT ARE PROPERLY TRAINED ON THE USE AND OPERATION OF THE VEHICLE OR EQUIPMENT.
- THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITIES WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.
- THE AIRPORT RESERVES THE RIGHT TO RESTRICT ACCESS TO CERTAIN AREAS OF THE AIRPORT OR AIRFIELD AT TIME DUE TO OPERATIONAL REQUIREMENTS.

WILDLIFE MANAGEMENT:

- THE CONTRACTOR SHALL MAINTAIN THE CONSTRUCTION SITES, HAUL ROUTES AND STORAGE AREAS IN COMPLIANCE WITH INDUSTRY BEST MANAGEMENT PRACTICES TO AVOID CREATING WILDLIFE ATTRACTANTS OR HAZARDS AT THE AIRPORT.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO MITIGATE ANY STANDING WATER CAUSED BY ANY CONSTRUCTION OR CONTRACTOR ACTIVITIES WITHIN 24 HOURS OF AN EVENT.
- NO FOOD OR FOOD RELATED DEBRIS ARE TO BE LEFT OR STORED WITHIN THE AIRPORT AIR OPERATIONS AREA INCLUDING ANY ALTERNATIVE, AOA CONSTRUCTION STAGING. AT THE CONTRACTORS STAGING AND STORAGE AREA OUTSIDE THE AOA, ALL DRUMS OR CONTAINERS USED TO HOLD TRASH AND DEBRIS SHALL BE CLEARLY LABELED " TRASH" AND BE EMPTIED REGULARLY.
- THE CONTRACTOR SHALL IMMEDIATELY REPORT ANY DAMAGE TO GATES OR FENCES. ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRS TO ANY GATES OR FENCES CAUSED BY NEGLIGENCE OF THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT IMMEDIATELY OF ANY WILDLIFE SIGHTINGS.

FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT & DUST CONTROL:

- THE CONTRACTOR SHALL BECOME FAMILIAR WITH THE REQUIREMENTS OF AIRFIELD WORK AND FOREIGN OBJECT AND DEBRIS (FOD) MANAGEMENT INCLUDING DUST CONTROL. THIS ITEM SHALL BE SPECIFICALLY ADDRESSED IN THE CONTRACTORS SPCD AS DETAILED IN THE PROJECT DOCUMENTATION. REFERENCE FAA AC 150/1510-24, FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT FOR FURTHER INSTRUCTION.
- THE CONTRACTOR SHALL NOT PLACE WASTE AND LOOSE MATERIAL IN ACTIVE MOVEMENT AREAS. MATERIALS TRACKED ON THESE AREAS SHALL IMMEDIATELY BE REMOVED.
- PROPERLY STOCKPILED LOOSE MATERIAL CAPABLE OF BEING DISPLACED MUST BE CONSTRAINED TO PREVENT ITS MOVEMENT AS A RESULT OF AIRCRAFT JET BLAST OR WIND CONDITIONS.
- THE CONTRACTOR SHALL ENSURE ALL LOADS ARE SECURED AND/OR COVERED DURING TRANSPORT AND BE LOADED SUCH THAT NO SPILLAGE OCCURS DURING TRANSIT.
- HAUL ROUTES, ACCESS ROADS, AND ANY PART OF ACTIVE APRONS, OR TAXIWAYS USED BY CONSTRUCTION TRAFFIC SHALL BE KEPT CONTINUOUSLY CLEAN AT ALL TIMES. A MINIMUM OF ONE (1) EACH, MECHANICAL SWEEPER AND VACUUM TRUCK SHALL BE KEPT ON SITE AT ALL TIMES DURING THE LIFE OF THIS CONTRACT PER THE PROJECT DOCUMENTATION. THE CONTRACTOR SHALL ALSO SHOW EVIDENCE OF AVAILABILITY OF REPLACEMENT EQUIPMENT MEETING THE SAME WITHIN TWO (2) HOURS OF AN EQUIPMENT BREAKDOWN OR TO SUPPLEMENT STAGED EQUIPMENT SHOULD IT BE DEEMED NECESSARY BY AIRPORT OPERATIONS FOR CONTINUED CONTRACTOR OPERATIONS.
- ALL SWEEPER OPERATORS SHALL BE TRAINED AND BADGED FOR UNESCORTED ACCESS TO THE AOA. ALL PERSONNEL WHO WILL OPERATE EQUIPMENT INSIDE THE AOA SHALL RECEIVE DRIVER TRAINING FROM AIRPORT OPERATIONS PRIOR TO OPERATING ON THE AIRFIELD. SWEEPER CREWS SHALL MONITOR ALL ATCT COMMUNICATIONS AND BE ATTENTIVE OF ALL AIRPORT ACTIVITIES AND AIRCRAFT MOVEMENTS.
- THE CONTRACTOR SHALL MAINTAIN CONSTRUCTION AREAS, INCLUDING HAUL ROADS, STAGING AREAS, AND ADJACENT AIRFIELD PAVEMENTS IN A CLEAN CONDITION AND SHALL NOT ALLOW ANY SIZABLE ACCUMULATION OF DEBRIS IN THE CONSTRUCTION AREA. IN ADDITION TO THE MECHANICAL SWEEPER AND VACUUM, THE CONTRACTOR SHALL UTILIZE WHATEVER OTHER EQUIPMENT AND MEANS NECESSARY TO KEEP THESE ROUTES FREE AND CLEAR OF DUST, DEBRIS, MUD, ETC.
- IN SUCH A SITUATION WHERE A SIGNIFICANT AMOUNT OF DEBRIS IS DEPOSITED ON ACTIVE PAVEMENTS, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE AIRPORT AND ENGINEER.
- AIRPORT OPERATIONS RESERVES THE RIGHT TO SUSPEND CONTRACTOR OPERATIONS WHEN AT ITS DISCRETION THE CONTRACTOR'S DUST CONTROL AND FOD MANAGEMENT BECOMES INEFFECTIVE.

HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT:

- THE CONTRACTOR SHALL DEVELOP A HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT AND RESPONSE PLAN. COPIES OF THIS PLAN SHALL BE MAINTAINED ON THE JOBSITE.

- THE CONTRACTOR SHALL ALSO DEVELOP A HAZMAT COMMUNICATION PLAN. THE PLAN SHALL LIST AND INCLUDE COPIES OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL HAZARDOUS MATERIALS BEING HANDLED ON THE JOBSITE. COPIES OF THIS PLAN SHALL BE MAINTAINED AT ALL STAGING AND STORAGE AREAS AND ON THE JOBSITE. COPIES OF THE PLAN SHALL ALSO BE SUBMITTED TO AIRPORT OPERATIONS.
- THE CONTRACTOR SHALL NOT REFUEL EQUIPMENT WITHIN THE AOA. THE CONTRACTOR SHALL MAINTAIN ON HAND A SPILL RESPONSE KIT TO EXPEDITIOUSLY CONTAIN AND CLEAN-UP SPILLS RESULTING FROM FUEL OR HYDRAULIC FLUID LEAKS, CONSISTENT WITH THEIR HAZMAT MANAGEMENT AND RESPONSE PLAN.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT OPERATIONS IMMEDIATELY IN THE EVENT A RELEASE OF HAZARDOUS MATERIAL OCCURS OR IF SIGNS OF POTENTIAL CONTAMINATION BY HAZARDOUS MATERIALS ARE ENCOUNTERED DURING EXCAVATION OR OTHER CONSTRUCTION ACTIVITIES.

NOTIFICATION OF CONSTRUCTION ACTIVITIES:

- ALL WORK WHICH MAY IMPACT AIRPORT OPERATIONS SHALL BE COORDINATED THROUGH THE CONSTRUCTION MANAGEMENT TEAM WITH AIRPORT OPERATIONS.
- THE CONTRACTOR SHALL SUPPLY A 24-HOUR CONTACT LIST FOR AT LEAST TWO (2) REPRESENTATIVES WHO SHALL BE AUTHORIZED TO CORRECT ANY UNSAFE CONDITIONS WHICH MAY ARISE DURING OFF HOURS, IN A TIMELY MANNER. THE LIST SHALL BE SUPPLIED TO ALL PARTIES PRIOR TO CONSTRUCTION.
- CONTACTS FOR THIS PROJECT ARE AS FOLLOWS:

PUBLIC SAFETY
 MANAGER - AIRPORT PUBLIC SAFETY - JOHN WHISENANT (618) 566-5227
 ASSISTANT AIRPORT DIRECTOR - DARREN JAMES (618) 566-5241

ENGINEERING & PLANNING
 ENGINEERING & PLANNING - DAN TRAPP, P.E. (618) 566-5322
 AIRPORT ENGINEER - ETHAN SISK (618) 566-5359
 AIRPORT OPERATIONS CENTER (AOC) (618) 566-5233

AIRPORT MAINTENANCE
 AIRPORT MAINTENANCE BRIAN GIESEKING (618) 566-5211

ENGINEER
 PROJECT MANAGER - TY SANDER, P.E. (314) 571-9066
 PROJECT ENGINEER - TOM MORRIS, P.E. (314) 571-9080

- THE CONTRACTOR SHALL PROVIDE A MINIMUM 72-HOUR NOTICE TO AIRPORT OPERATIONS PRIOR TO COMMENCING ANY WORK AFFECTING OPERATIONS SO THAT PROPER NOTAM'S MAY BE ISSUED BY THE AIRPORT.
- ANY DEACTIVATION OF WATER LINES OR HYDRANTS, REROUTING OF ACCESS ROUTES, OR USE OF HAZARDOUS MATERIALS ON THE AIRFIELD SHALL BE COORDINATED AND APPROVED BY THE AIRPORT'S ARFF PERSONNEL PRIOR TO EXECUTION OF SUCH ACTIVITIES.
- AIRPORT OPERATIONS SHALL BE RESPONSIBLE FOR COORDINATION WITH LOCAL ATO/TECHNICAL OPERATIONS PERSONNEL. SHUTDOWN OF ANY NAVID (AIRPORT OR FAA OWNED) SHALL BE COORDINATED WITH THE FAA ATO 45 DAYS PRIOR TO THE PROPOSED SHUTDOWN. THE CONTRACTOR SHALL PROVIDE AN ADDITIONAL SEVEN DAYS ADVANCE NOTICE TO THE AIRPORT TO COORDINATE WITH THE FAA ATO TECH OPS OFFICE RESPONSIBLE FOR THE FAA FACILITIES.
- NOTAMS WILL BE ISSUED AND UPDATED BY AIRPORT OPERATIONS WITH THE LATEST AIRPORT CONSTRUCTION INFORMATION. BEFORE BEGINNING ANY CONSTRUCTION ACTIVITY, THE CONTRACTOR MUST, THROUGH AIRPORT OPERATIONS, GIVE NOTICE USING THE NOTAM SYSTEM OF PROPOSED LOCATION, TIME AND DATE OF COMMENCEMENT OF CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE ALL WORK THROUGH THE CONSTRUCTION MANAGEMENT TEAM WITH AIRPORT OPERATIONS. COMPLIANCE WITH AIRPORT NOTAM SHALL BE ADDRESSED WITHIN THE CONTRACTOR'S SPCD. THE NOTAM SYSTEM WILL BE SUPPLEMENTED WITH AN APPROPRIATE AERONAUTICAL CONSTRUCTION NOTICE DISSEMINATED TO THE ATC, ALL TENANTS AND ARFF PERSONNEL. UPON COMPLETION OF WORK AND RETURN OF ALL SUCH AREAS TO STANDARD CONDITIONS, THE CONTRACTOR MUST, THROUGH AIRPORT OPERATIONS, VERIFY THE CANCELLATION OF ALL ASSOCIATED NOTICES ISSUED VIA THE NOTAM SYSTEM.
- AIRPORT OPERATIONS IS RESPONSIBLE FOR COORDINATION AND NOTIFICATION TO THE FAA UNDER 14 CFR PART 77 AND 157. THE AIRPORT THROUGH THE ENGINEER HAS SUBMITTED FOR REVIEW, THE CRITICAL POINTS AND EQUIPMENT HEIGHTS FOR VARIOUS ROUTES AND AREAS AFFECTED BY THIS WORK UNDER THE FAA'S 7460 AIRSPACE REVIEW AND OBSTRUCTION EVALUATION PROCEDURES.
- THE MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT IN THE WORK AREAS IS RESTRICTED TO 25-FEET UNLESS OTHERWISE NOTED IN THE PLANS. THE MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT ON THE ACCESS AND HAUL ROUTES IS RESTRICTED TO 25-FEET UNLESS OTHERWISE NOTED IN THE PLANS. ANY EQUIPMENT IN EXCESS OF THE RESTRICTED HEIGHTS NECESSARY TO COMPLETE THE WORK WILL REQUIRE ADDITIONAL AIRSPACE REVIEW.
- AN FAA AIRSPACE REVIEW WILL REQUIRE THE CONTRACTOR TO SUBMIT FOR REVIEW AT LEAST 45-DAYS PRIOR TO ANTICIPATED USE A SEPARATE FAA FORM 7460-1 FOR EACH PIECE OF EQUIPMENT THAT EXCEEDS THE MAXIMUM HEIGHT AND FOR EACH WORK AREA AS NOTED IN THE CAP SHEETS FOR THE AREAS THE EQUIPMENT WILL OCCUPYING.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL FIRST CALL 911, THEN IMMEDIATELY NOTIFY THE ENGINEER AND AIRPORT MANAGER.

INSPECTION REQUIREMENTS:

- ALL CONSTRUCTION ACTIVITY WITHIN THE AIR OPERATIONS AREA (AOA) WILL BE SUBJECT TO CONTINUOUS MONITORING BY AIRPORT OPERATIONS PERSONNEL WHO ARE FULLY TRAINED IN PART 139, AIRFIELD CONSTRUCTION MANAGEMENT REQUIREMENTS AND AIRFIELD OPERATING PROCEDURES.
- THE CONTRACTOR SHALL CONDUCT INSPECTIONS AT LEAST DAILY, BUT MORE FREQUENTLY WHEN CONDITIONS DICTATE, A TEMPLATE CHECKLIST FOR INSPECTIONS IS PROVIDED IN APPENDIX D OF AC 150/5370-2G. THIS DOCUMENT IS ALSO INCLUDED AS APPENDIX B IN THIS CSPP. INSPECTIONS SHALL INCLUDE, BUT NOT BE LIMITED TO, BARRICADE LOCATIONS, BARRICADE LIGHTING, LIGHT AND SIGN OPERATION, RUNWAY AND TAXIWAY CLOSURE MARKERS, COVERS ON TAXIWAY GUIDANCE SIGNS, AND FOD (FOREIGN OBJECT DEBRIS) POTENTIAL AFFECTING ACTIVE AIRFIELD PAVEMENTS.
- THE AIRPORTS CONSTRUCTION MANAGEMENT TEAM IS RESPONSIBLE FOR ALL CONSTRUCTION INSPECTION REGARDING CONTRACT COMPLIANCE. ALL CONSTRUCTION AREAS WILL BE INSPECTED BY AIRPORT OPERATIONS AND THE ENGINEER PRIOR TO ACCEPTANCE OF THE WORK.
- RE-OPENING OF PAVEMENTS AFTER A CONSTRUCTION ACTIVITY OR OTHER CLOSURE WILL ONLY BE APPROVED BY AIRPORT OPERATIONS AND WILL ONLY BE APPROVED ON THE BASIS THAT THE CONTRACTOR HAS MET THE REQUIREMENT FOR OPENING AIRFIELD PAVEMENTS AS OUTLINED IN THE CONTRACT. THE CONTRACTOR SHALL INCLUDE SUFFICIENT TIME WITHIN THE ALLOWABLE SCHEDULED CLOSURE REQUIREMENTS TO ACCOMMODATE INSPECTION OF PAVEMENTS TO BE OPENED BY AIRPORT OPERATIONS.

UNDERGROUND UTILITIES:

- THE CONTRACTOR IS REQUIRED TO COMPLY WITH ALL AIRPORT SAFETY PROVISIONS, AND PERMIT AND CERTIFICATION REQUIREMENTS SUCH AS UNDERGROUND UTILITY MARK-OUT, ELECTRICAL CIRCUIT SHUTDOWN, WELDING, CONFINED-SPACE, ETC.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE A FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT ITS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS BORNE BY THE CONTRACTOR.
- ALL WORK MUST BE COORDINATED WITH AIRPORT OPERATIONS PERSONNEL INCLUDING DATES OF CONSTRUCTION AND PROPOSED CONSTRUCTION METHODS FOR DETERMINING EXISTING CABLE AND UTILITY LOCATIONS.
- THE CONTRACTOR IS RESPONSIBLE FOR LOCATING, MARKING, AND PROTECTING ALL UTILITIES, IMPACTED BY CONSTRUCTION ACTIVITIES. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY, THE AIRPORT, AND OTHER AIRPORT TENANTS OF IT'S OPERATIONAL PLANS AND MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. THE LIST OF UTILITY COMPANIES TO CONTACT CAN BE FOUND IN THE PROJECT DOCUMENTATION. THIS PROCEDURE SHALL BE FOLLOWED FOR ALL CONSTRUCTION ACTIVITIES IN ALL AREAS.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATION ON AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES.
- THE CONTRACTOR SHALL TAKE CARE WHEN WORKING IN THE VICINITY OF EXISTING AIRPORT LIGHTING SYSTEMS. SHOULD THE CONTRACTOR DAMAGE ANY AIRPORT LIGHTING SYSTEM, THE CONTRACTOR SHALL IMMEDIATELY CONTACT AIRPORT OPERATIONS AND THE ENGINEER. THE CONTRACTOR SHALL IMMEDIATELY REPAIR OR REPLACE THE DAMAGED SYSTEM. ANY REPAIRS OR REPLACEMENT SHALL BE TO THE SATISFACTION OF THE AIRPORT.
- THE CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING IN AREAS NEAR FAA UNDERGROUND CABLES. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS PERSONNEL A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION IN THESE AREAS TO ENSURE NO FAA UTILITIES WILL BE IMPACTED. ANY FAA EQUIPMENT OR UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AT NO EXPENSE TO THE OWNER OR THE FAA.
- DAMAGED FAA CABLES/EQUIPMENT SHALL BE REPAIRED AS APPROVED BY FAA TECHNICAL OPERATIONS PERSONNEL. AIRPORT OPERATIONS WILL NOTIFY THE LOCAL FAA TECHNICAL OPERATIONS PERSONNEL, FAA 139 INSPECTOR AND THE FAA STATE AIRPORT ENGINEER IN THE EVENT OF AN UNSCHEDULED LIGHTING INTERRUPTION.

PENALTIES:

- THE AIRPORT RESERVES THE RIGHT TO TEMPORARILY SUSPEND CONTRACTOR ACTIVITIES FOR AIRPORT OPERATIONS AND EMERGENCIES AT NO ADDITIONAL COST TO THE AIRPORT. THE CONTRACTOR SHALL COMPLY WITH ALL AIRPORT REQUIREMENTS AND DIRECTION PROVIDED BY AIRPORT PERSONNEL IN THE EVENT OF AN URGENT OPERATIONAL NEED OR EMERGENCY.
- INCURSION ONTO OR ACROSS AN ACTIVE RUNWAY, SAFETY AREA, APPROACH AREA, OR TAXIWAY SAFETY AREA WITHOUT PRIOR APPROVAL OF AIRPORT OPERATIONS IS A SERIOUS VIOLATION THAT WILL SUBJECT THE CONTRACTOR TO THE MAXIMUM FINE ALLOWED BY THE FEDERAL AVIATION ADMINISTRATION. ANY VIOLATIONS OF FAA RULES AND REGULATIONS MAY DIRECTLY SUBJECT THE CONTRACTOR TO THOSE FINES IMPOSED BY THE FAA.

- AT NO TIME SHALL CONTRACTOR PERSONNEL OR CONTRACTOR VEHICLES CROSS ANY BARRICADE LINE WITHOUT PRIOR AIR TRAFFIC CONTROL TOWER (ATCT) PERMISSION.
- THE AIRPORT RESERVES THE RIGHT TO SUSPEND WORK IN THE EVENT THE CONTRACTOR VIOLATES AIRPORT REQUIREMENTS, CONSTRUCTION SAFETY AND PHASING REQUIREMENTS, THE CSPP, OR THE SPCD; AND WILL ONLY ALLOW THE CONTRACTOR TO PROCEED WITH THE WORK WHEN THE CONTRACTOR TAKES DOCUMENTED CORRECTIVE ACTION TO PREVENT FUTURE VIOLATIONS.

- FAILURE OF THE CONTRACTOR (INCLUDING EMPLOYEES) OR ANY OF HIS SUBCONTRACTORS (INCLUDING EMPLOYEES) TO COMPLY WITH AIRPORT INSTRUCTIONS, THE CSPP, THE SPCD, OR ANY OF THE OTHER REQUIREMENTS OF THE AIRPORT WHILE OPERATING ON AIRPORT PROPERTY, SHALL BE SUBJECT TO THE FOLLOWING:

FIRST OFFENSE - THE VEHICLE OPERATOR WILL RECEIVE A LOSS OF DRIVING PRIVILEGES ON THE AIRPORT. IN ADDITION, ANY FINES OR PENALTIES IMPOSED ON THE AIRPORT AS A RESULT OF THE INCIDENT WILL BE ASSESSED TO THE CONTRACTOR.

SECOND OFFENSE - THE VEHICLE OPERATOR WILL RECEIVE A LOSS OF DRIVING PRIVILEGES ON THE AIRPORT. IN ADDITION, ANY FINE OR PENALTIES IMPOSED ON THE AIRPORT AS A RESULT OF THE INCIDENT WILL BE ASSESSED TO THE CONTRACTOR.

THIRD OFFENSE - WORK WILL BE SUSPENDED. THE CONTRACTOR (INCLUDING EMPLOYEES) AND ANY OF HIS SUBCONTRACTORS (INCLUDING EMPLOYEES) WHO WILL OPERATE GROUND VEHICLES ON THE AIRPORT SHALL SUCCESSFULLY COMPLETE, FOR A SECOND TIME, FORMALIZED AIRPORT SAFETY TRAINING, TO BE CONDUCTED BY AIRPORT STAFF. WHEN THE CONTRACTOR'S EMPLOYEES HAVE COMPLETED AIRPORT SAFETY TRAINING TO THE SATISFACTION OF THE OWNER, WORK MAY CONTINUE AT THE DISCRETION OF THE OWNER.

SPECIAL CONDITIONS:

- AIRPORT RUNWAYS, TAXIWAYS AND RAMP AREAS SHALL REMAIN IN USE BY AIRCRAFT TO THE MAXIMUM EXTENT POSSIBLE DURING THE PROJECT. THE CONTRACTOR'S OPERATION SHALL BE CONTROLLED TO MINIMIZE DISTURBANCE TO AIRCRAFT AREA INCLUDING DUST CONTROL PROCEDURES AND DEBRIS CONTROL.
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN LIGHTING, AND/OR SIGNS ADJACENT TO THE WORK AREA OR ANY LIGHTING AND SIGNAGE AFFECTED BY ELECTRICAL WORK RELATED TO THE CONTRACT WORK.
- FLAGGERS ARE REQUIRED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY DRAWINGS. FLAGGERS SHALL CONTROL CONSTRUCTION TRAFFIC IN THE DESIGNATED AREAS AND ENSURE THAT THE AIRCRAFT MOVEMENT AREAS REMAIN CLEAR OF CONSTRUCTION DEBRIS. IN THE EVENT THAT CLEANING OPERATIONS ARE REQUIRED TO CLEAR CONSTRUCTION DEBRIS, THE FLAGGER SHALL IMMEDIATELY NOTIFY THE AIRPORT OPERATIONS STAFF TO CONTACT ATCT FOR THE PURPOSE OF CLOSING THE PAVEMENT UNTIL CLEANING OPERATIONS ARE COMPLETED.
- THE CONTRACTOR, CONSTRUCTION EMPLOYEES, SUB-CONTRACTORS, DELIVERY AND HAUL OPERATORS, OR ANY OTHER SUPPORT WORKERS REQUIRED TO ENTER THE AIRCRAFT OPERATIONS AREA RELATED TO CONTRACT WORK SHALL WEAR A REFLECTIVE SAFETY VEST, DAY OR NIGHT.
- THE CONTRACTOR SHALL PROVIDE ADEQUATE LIGHTING TO THE WORK AREA. ANY ADDITIONAL OR SPECIAL REQUIREMENTS FOR NIGHT-TIME WORK WILL BE ADDRESSED DURING THE PRE-CONSTRUCTION MEETING.
- THE CONTRACTOR SHALL COORDINATE ALL ARTIFICIAL LIGHTING ACTIVITIES AND PROPOSED LOCATIONS WITH THE AIRPORT OPERATIONS PRIOR TO THE IMPLEMENTATION OF ANY ARTIFICIAL LIGHTING ON THE PROJECT. CARE SHALL BE TAKEN TO SHIELD THE AIR TRAFFIC CONTROL TOWER FROM ANY HIGH INTENSITY SITE LIGHTING DURING NIGHTTIME OR OTHER WORK ACTIVITIES UTILIZING ARTIFICIAL LIGHTING.
- IN THE EVENT OF AN UNEXPECTED WEATHER EVENT THE CONTRACTOR IS REQUIRED - PRIOR TO LEAVING THE AREA - TO ENSURE THE SITE IS SECURE AND FREE FROM EQUIPMENT OR MATERIAL HAVING THE POTENTIAL OF BEING DISLODGED CONTRIBUTING TO A FOD/DEBRIS HAZARD TO AIRCRAFT OR PERSONNEL AND EQUIPMENT OPERATING ON THE AIRPORT. ADDITIONALLY, THE CONTRACTOR SHALL ENSURE ALL BARRICADES AND HAZARD LIGHTING ARE IN PLACE PRIOR TO VACATING THE SITE.
- FOLLOWING A WEATHER EVENT THE CONTRACTOR SHALL POLICE THE WORK AREA(S) RESTORING BARRICADES, HAZARD LIGHTING, AND OTHER SAFETY MEASURES. IN ADDITION, THE CONTRACTOR SHALL MITIGATE ANY POTENTIAL HAZARDS INCLUDING STANDING WATER TO THE SATISFACTION OF AIRPORT OPERATIONS AND THE ENGINEER.
- IN THE EVENT OF AN EMERGENCY, AIRPORT OPERATIONS MAY REQUEST THE CONTRACTOR TEMPORARILY SUSPEND WORK ACTIVITIES, AND MAY REQUEST THE CONTRACTOR VACATE THE WORK SITE, UNTIL THE EMERGENCY EVENT IS SATISFACTORY ADDRESSED. UPON RECEIVING SUCH NOTIFICATION, THE CONTRACTOR SHALL IMMEDIATELY SUSPEND WORK OPERATIONS AND REMAIN OR RELOCATE AS DIRECTED BY AIRPORT OPERATIONS.

(NOTES CONTINUE ON SHEET GC003)



License No. 184-000613

CONSULTANTS

BID ISSUE
 APRIL 21, 2023

TERMINAL APRON EXPANSION -
 PROJECT 1



MIDAMERICA
 ST. LOUIS AIRPORT
 ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION
		BLV PROJECT NO. 2022-14
		IL PROJECT NO. BLV-5087
		CMT PROJECT NO: 22001186.00
		CAD DWG FILE: 22001186 - GC000.DWG
		DESIGNED BY: CMT
		DRAWN BY: %%JU
		CHECKED BY: CMT
		APPROVED BY: CMT
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SHEET TITLE

**CONSTRUCTION
 ACTIVITY PLAN
 NOTES 2**

GC002

SHEET **7** OF **60**

(NOTES CONTINUED FROM SHEET GC002)

RUNWAY AND TAXIWAY VISUAL AIDS:

- 1. EXISTING AIRFIELD LIGHTING SYSTEMS SHALL BE KEPT IN OPERATION DURING THE CONSTRUCTION UNLESS OTHERWISE SPECIFIED OR WITH PRIOR APPROVAL FROM AIRPORT OPERATIONS. THE CONTRACTOR SHALL PROVIDE A PLAN FOR TEMPORARY SPLICES OF PRIMARY CABLE TO THE CONSTRUCTION MANAGEMENT TEAM PRIOR TO INTERRUPTING ANY CIRCUIT. COORDINATE WITH CONSTRUCTION MANAGEMENT AND ELECTRICAL MAINTENANCE PRIOR TO THE START OF ANY WORK.
2. THE CONTRACTOR SHALL COVER ELEVATED LIGHTS IF LIGHTING CIRCUIT MUST BE MAINTAINED FOR ACTIVE SURFACES OUTSIDE OF THE WORK AREA.
3. THE CONTRACTOR SHALL DE-ENERGIZE OR COVER LIGHTS FOR ALL CLOSED TAXIWAYS DURING PAVEMENT CLOSURES. TEMPORARILY COVERING OR DE-ENERGIZING AIRFIELD SIGNAGE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS SHALL BE IMPLEMENTED IMMEDIATELY UPON THE INITIATION OF EACH PROJECT PHASE.
4. ALL TEMPORARY OR PERMANENT RUNWAY AND TAXIWAY VISUAL AIDS SHALL CONFORM TO THE REQUIREMENTS OF THE MOST RECENT EDITION OF FAA AC 150/5340-1.
5. THE CONTRACTOR SHALL INSTALL TAXIWAY CLOSURE MARKERS AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS. THE TAXIWAY CLOSURE MARKERS SHALL BE PLACED AT ALL RUNWAY/TAXIWAY OR TAXIWAY/TAXIWAY INTERSECTIONS AT THE ENTRANCE TO THE CLOSED TAXIWAYS. AT INTERSECTING TAXIWAYS, THE CONTRACTOR SHALL PLACE BARRICADES AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS. THE BARRICADES SHALL BE LOCATED OUTSIDE THE SAFETY AREA. LOW PROFILE BARRICADES WITH FLASHERS SHALL BE PLACED AT THE SPACING SHOWN ON THE CONSTRUCTION ACTIVITY PLANS FOR EACH PHASE.
6. ALL RUNWAY HOLD SIGNS SHALL REMAIN OPERATIONAL AND UNCOVERED AT ALL TIMES THROUGHOUT THE CONSTRUCTION.

MARKING AND SIGNS FOR ACCESS ROUTE:

- 1. CONTRACTOR SHALL CLEARLY MARK ALL ACCESS AND HAUL ROUTES. SIGNS WILL BE IN ACCORDANCE WITH AC 150/5340-18. THE CONTRACTOR SHALL SET UP A LATHE LINE AS NOTED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS FOR EACH PARTICULAR PHASE AND WORK AREA TO INDICATE AREAS OFF-LIMITS FOR CONTRACTOR'S VEHICLES AND PERSONNEL. LATHE LINES SHALL BE OFFSET A MINIMUM OF 107 FEET FROM TAXIWAY CENTERLINES AND 250 FEET FROM RUNWAY CENTERLINES UNLESS NOTED OTHERWISE ON THE CONSTRUCTION ACTIVITY PLANS. THE CONTRACTOR SHALL INCLUDE FLAG ROPE STRUNG BETWEEN EACH LATHE.

HAZARD MARKING AND LIGHTING:

- 1. THE CONTRACTOR SHALL PLACE AND MAINTAIN BARRICADES MARKING THE CLOSURE OF VARIOUS WORK AREAS AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS. EACH BARRICADE MUST BE EQUIPPED WITH BATTERY OPERATED RED FLASHING LIGHTS.
2. MARKING AND LIGHTING OF CLOSED, DECEPTIVE, OR HAZARDOUS AREAS IS REQUIRED AS DIRECTED BY THE AIRPORT INCLUDING PROMINENTLY MARKING OPEN TRENCHES AND EXCAVATIONS AT THE CONSTRUCTION SITE AND LIGHTING THESE OBSTACLES DURING HOURS OF RESTRICTED VISIBILITY AND DARKNESS IN ACCORDANCE WITH FAA AC/150-5370-2G.
3. THE CONTRACTOR SHALL SUBMIT TO THE CONSTRUCTION MANAGEMENT TEAM AND THE AIRPORT A NAME AND PHONE NUMBER OF TWO INDIVIDUALS WHO WILL BE AVAILABLE ON A 24-HOUR CALL BASIS FOR EMERGENCY BARRICADE AND BARRICADE LIGHTING MAINTENANCE AS WELL AS MAINTENANCE AND REPAIR OF TEMPORARY ELECTRICAL SYSTEMS AND WIRING.
4. ALL AIR OPERATIONS AREAS (AOA) NOT IN COMPLIANCE WITH FAA PART 139 SHALL BE CLOSED, SECURED AND APPROPRIATE NOTIFICATION DISSEMINATED. ACCESS TO HAZARDOUS AREAS SHALL BE RESTRICTED AND ALL HAZARDS SHALL BE MARKED WITH BARRICADES AND FLASHING RED LIGHTS IN ORDER TO MAKE THE HAZARD OBVIOUS TO AIRCRAFT, PERSONNEL, AND VEHICLES AS SHOWN IN THE CONSTRUCTION DRAWINGS.
5. PAVEMENT MARKINGS LEADING INTO CLOSED AREAS AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS SHALL BE REMOVED OR OBFUSCURED BY THE CONTRACTOR DURING ALL PHASES. PAVEMENT MARKING SHALL BE REPLACED AS SHOWN ON THE PLANS.
6. HAZARDS SUCH AS OPEN UTILITY COVERS, AREAS UNDER REPAIR AND STOCKPILED MATERIALS SHALL BE MARKED AND LIGHTED WITH CONES OR BARRICADES.
7. BARRICADES AND SIGNS THAT ARE AFFIXED TO THE PAVEMENT SURFACE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW AS POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND. NON-FRANGIBLE HAZARD BARRICADES SUCH AS CONCRETE BARRIERS, 12X12 RAILROAD TIES AND/OR METAL-DRUM-TYPE BARRICADES ARE NOT TO BE USE IN AIRCRAFT MOVEMENT AREAS.
8. THE CONTRACTOR PERSONNEL SHALL BE EDUCATED AS TO THE DANGERS OF JET BLAST. THE CONTRACTOR SHALL ENSURE ADEQUATE DISTANCE FOR AIRCRAFT ENGINE BLAST PROTECTION WITHIN THE WORK SITE IS PROVIDED.
9. ALL BARRICADES MUST BE WEIGHTED TO THE PAVEMENT SURFACE TO PREVENT DISPLACEMENT FROM PROP WASH, JET BLAST, WING VORTEX, OR OTHER SURFACE WIND CURRENTS.
10. BARRICADES AND RED FLASHING LIGHTS ARE REQUIRED FOR TAXIWAY CLOSURES

AS SHOWN IN CONSTRUCTION DRAWINGS.

- 11. CONSTRUCTION DRAWINGS MAY DIRECT THE CONTRACTOR TO SUPPLY AND PLACE SIGNS AND OTHER CONSTRUCTION BARRICADES TO COMPLETELY ENCLOSE A STAGE OR CONSTRUCTION AREA. ALL BARRICADES, TEMPORARY MARKERS, AND OTHER OBJECTS PLACED ADJACENT TO ACTIVE SAFETY AREAS SHALL NOT EXCEED 18" FOR OTHER AREAS IN THE OBJECT FREE AREA (OFA) A 5' VERTICAL AND HORIZONTAL CLEARANCE MUST BE MAINTAINED. SAFETY AREAS ASSOCIATED WITH ANY OPEN RUNWAY, TAXIWAY, OR TAXILANE MUST BE AS LOW AS POSSIBLE TO THE GROUND; OF LOW MASS; EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS. BARRICADE LOCATIONS ARE SHOWN ON THE DRAWINGS.
12. THE CONTRACTOR SHALL SUPPLY AND PLACE FLASHING RED LIGHTS AND BARRICADES MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE STRIPES AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
13. ANY ADVISORY SIGN WARNING EMPLOYEES OF AN ACTIVE MOVEMENT AREA MUST BE LOW MASS AND COLLAPSIBLE. IF INSTALLED WITHIN AN ACTIVE PAVEMENT OBJECT FREE AREA, IT MUST BE LESS THAN 18 INCHES.

WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION:

- 1. NO CONSTRUCTION ACTIVITY IS PERMITTED WITHIN 250 FEET OF AN OPEN RUNWAY CENTERLINE, OR 107 FEET OF AN OPEN TAXIWAY CENTERLINE, UNLESS NOTED OTHERWISE ON THE CONSTRUCTION ACTIVITY PLANS. WORK WITHIN THESE AREAS CAN ONLY BE PERFORMED DURING CLOSURE OF THE SUBJECT RUNWAY OR TAXIWAY.
2. ALL WORK THAT IS WITHIN THE RSA OR TSA SHALL BE COORDINATED THROUGH THE CONSTRUCTION MANAGEMENT TEAM WITH AIRPORT OPERATIONS IN ORDER TO MAINTAIN REQUIRED CLEARANCE, CLOSURES AND/OR ISSUE THE APPROPRIATE NOTAM(S). ALL WORK INSIDE THE RSA OR TSA SHALL REQUIRE A RUNWAY OR TAXIWAY CLOSURE.
3. TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN THE SAFETY AREAS OF OPEN RUNWAYS OR TAXIWAYS. WORK WITHIN THESE AREAS CAN ONLY BE PERFORMED DURING CLOSURE OF THE SUBJECT RUNWAY OR TAXIWAY AND MUST BE COORDINATED THROUGH THE CONSTRUCTION MANAGEMENT TEAM WITH AIRPORT OPERATIONS.
4. THERE ARE NO PROVISIONS TO PLATE OR COVER TRENCHES OR EXCAVATIONS WITHIN ANY RUNWAY OR TAXIWAY SAFETY AREA. NO WORK IS PERMITTED WITHIN 250 FEET OF AN OPEN RUNWAY CENTERLINE OR 107 FEET OF AN OPEN TAXIWAY CENTERLINE, UNLESS NOTED OTHERWISE ON THE CONSTRUCTION ACTIVITY PLANS. INSTEAD, ALL WORK IN THESE AREAS IS ACCOMPLISHED DURING THE ADJACENT TAXIWAY CLOSURE. THE AREA MUST BE BACKFILLED, GRADED AND COMPACTED TO SUPPORT THE PASSAGE OF AN AIRCRAFT OR ARFF EQUIPMENT.
5. IF ANY RUNWAY OR TAXIWAY MUST BE RE-OPENED WHILE GROUND DISTURBANCES REMAIN IN THE RSA OR TSA, THE AREA WITHIN THE RSA OR TSA MUST BE REPAIRED SO THAT THERE ARE NO SLOPES GREATER THAN 5% OR DROP-OFFS GREATER THAN 3" FROM WITHIN THE SAFETY AREA IN ORDER TO OPEN THE RUNWAY OR TAXIWAY.
6. RUNWAY AND TAXIWAY SAFETY AREAS DISTURBED BY CONSTRUCTION WORK OR ACTIVITY MUST BE GRADED AND CONTINUOUS MEASURES MUST BE TAKEN TO PREVENT EROSION BY WATER, WIND OR BLAST, TO INCLUDE TIME AFTER PAVEMENT IS RE-OPENED PRIOR TO FULLY ESTABLISHED TURF.
7. EQUIPMENT, VEHICLES AND MATERIALS NOT BEING UTILIZED IN THE IMMEDIATE FUTURE IN THE WORK AREA MUST BE REMOVED AND STORED AT THE DESIGNATED CONTRACTORS STAGING AREAS.
8. ALL CONTAINERS, EQUIPMENT AND/OR SUPPLIES SHALL BE LOCATED IN DESIGNATED AND DEFINED CONTRACTOR STAGING AREA CLEAR OF ALL AIRPORT SAFETY AREAS, OBJECT/OBSTACLE FREE ZONES, OBSTRUCTION RESTRICTIONS, MOVEMENT AREAS, AND CLEAR OF AIRCRAFT ENGINE BLAST CRITERIA AREAS. ALL STORAGE CONTAINERS MUST HAVE COMPANY NAME AND 24-HOUR CONTACT NUMBER CLEARLY DISPLAYED.
9. ALL WORK IN A RUNWAY OR TAXIWAY OBJECT FREE AREA SHALL BE COORDINATED WITH AIRPORT OPERATIONS. APPROPRIATE NOTAM(S) AND NOTIFICATIONS WILL BE MADE.
10. MAXIMUM HEIGHT LIMITS OF CONSTRUCTION EQUIPMENT INSIDE THE WORK AREA IS 25 FEET, THUS PREVENTING PERSONNEL, MATERIAL, AND/OR EQUIPMENT, FROM PENETRATING THE OBSTACLE FREE ZONE (OFZ) AS DEFINED IN THE FEDERAL AVIATION ADMINISTRATION ADVISORY CIRCULAR AC 150/5300-13A AIRPORT DESIGN, PARAGRAPH 308, " OBSTACLE FREE ZONE (OFZ) ".
11. CONTRACTOR PERSONNEL, WHETHER PEDESTRIANS OR VEHICLES, MAY NOT ENTER ANY ACTIVE SAFETY AREA UNLESS UNDER ESCORT AND AS APPROVED BY THE BLV ATCT. UNAUTHORIZED ENTRY INTO A SAFETY AREA IS CONSIDERED A VEHICLE OR PEDESTRIAN DEVIATION (VPD INCIDENT) THAT COULD RESULT IN PENALTIES FOR THE OFFENDER. REFERENCE SECTION 216 PENALTIES.
12. ANY ADVISORY SIGN WARNING EMPLOYEES OF AN ACTIVE MOVEMENT AREA MUST BE LOW MASS AND COLLAPSIBLE. IF INSTALLED WITHIN AN ACTIVE PAVEMENT OBJECT FREE AREA, IT MUST BE LESS THAN 18 INCHES.

PROTECTION OF AREAS AND SURFACES:

- 1. THE CONTRACTOR MUST COMPLY WITH ALL SAFETY REQUIREMENTS AND SPECIFIC AIRPORT REQUIREMENTS INCLUDING PERMITS FOR OPEN-FLAME WELDING OR TORCH CUTTING OPERATIONS AND AIRFIELD LIGHTING OR ELECTRICAL POWER INTERRUPTIONS.
2. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE

CONTRACTOR SHALL ABIDE BY ALL INSTRUCTIONS FROM AIRPORT OPERATIONS.

- 3. ELECTRICAL BLASTING CAPS ARE NOT PERMITTED WITHIN 1,000 FEET OF THE AIRPORT PROPERTY. FLARE POTS ARE NOT PERMITTED WITHIN THE AIR OPERATIONS AREA.
4. ALL DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.
5. WHEN MODIFICATIONS TO THE SECURITY FENCING ARE REQUIRED THE CONTRACTOR SHALL SEQUENCE THE WORK IN ORDER TO MAINTAIN A SECURED PERIMETER AT ALL TIMES. PRIOR TO INITIATING THE WORK, THE CONTRACTOR SHALL SUBMIT A PHASING PLAN FOR REVIEW AND APPROVAL BY THE AIRPORT INDICATING HOW A FULLY SECURED PERIMETER WILL BE MAINTAINED. WHEN POSSIBLE, ALL NEW SECURITY FENCING SHALL BE INSTALLED AND APPROVED BY THE AIRPORT PRIOR TO THE REMOVAL OF EXISTING SECURITY FENCING. IF IT IS NOT POSSIBLE TO SEQUENCE THE FENCING TO MAINTAIN A FULLY SECURED PERIMETER THE CONTRACTOR SHALL PROVIDE A 24-HOUR SECURITY GUARD, UTILIZING THE AIRPORT'S APPROVED SECURITY CONTRACTOR TO MONITOR ANY TEMPORARY OPENINGS IN THE SECURITY FENCING. TEMPORARY OPENINGS THAT ARE MONITORED BY A SECURITY CONTRACTOR SHALL NOT EXCEED 30' IN WIDTH.

BLV BADGING REQUIREMENTS:

I. APPLICABILITY:

A. ALL REQUIREMENTS NOTED BELOW ARE APPLICABLE TO THE SELECTED BIDDER ONLY.

B. BADGE APPLICATIONS ARE TO BE OBTAINED AND COMPLETED ONLY BY THE SELECTED BIDDER AFTER AWARD OF THE CONTRACT

II. AIRPORT SECURITY:

A. PROJECT REQUIREMENTS FOR SECURED ACCESS AND ACCESS TO THE AIRPORT OPERATIONS AREA (AOA) OR MOVEMENT CONTROL AREA (MCA).

- 1. WORK FOR THIS PROJECT WILL BE WITHIN A SECURED AREAS OF THE AIRPORT. THE PROJECT WILL REQUIRE SECURITY IDENTIFICATION DISPLAY AREA (SIDA) BADGE ACCESS.
2. WORK FOR THIS PROJECT WILL REQUIRE ACCESS TO THE AIRPORT OPERATIONS AREA (AOA) OF THE AIRPORT. THE PROJECT WILL REQUIRE THAT THE CONTRACTOR DRIVE ON THE AOA.

B. CONTRACTOR SHALL ENSURE THE WORKSITE AND RESTRICTED AREAS ARE MAINTAINED IN A SECURE MANNER AT ALL TIMES TO PREVENT ENTRY INTO THE RESTRICTED AREAS BY UNAUTHORIZED PERSONS. CONTRACTOR SHALL HAVE AN ADEQUATE NUMBER OF SUPERVISORS AND/OR EMPLOYEES OBTAIN AIRPORT SECURITY BADGES FROM MIDAMERICA ST. LOUIS AIRPORT PRIOR TO COMMENCING WORK ON THE PROJECT TO PROVIDE WORKSITE SECURITY AND ESCORT SERVICE DURING THE TERM OF THE PROJECT.

C. ALL WORK WITHIN A RESTRICTED AREA SHALL BE SUBJECT TO AIRPORT SECURITY REGULATIONS. ANY VIOLATION OF AIRPORT SECURITY RULES IS GROUNDS FOR IMMEDIATE TERMINATION OF CONTRACT AND RELEVANT FINES.

III. SECURITY BADGE REQUIREMENTS:

A. ALL PERSONS WORKING WITHIN A RESTRICTED AREA AND THE SECURITY IDENTIFICATION DISPLAY AREA (SIDA) OF MIDAMERICA ST. LOUIS AIRPORT (BLV) MUST BE UNDER AUTHORIZED ESCORT OR DISPLAY AN AIRPORT ISSUED IDENTIFICATION BADGE FOR UNESCORTED ACCESS TO ANY RESTRICTED AREA. ANY PERSON NOT UNDER PROPER ESCORT OR IN ANY RESTRICTED AREA WITHOUT AN AIRPORT ISSUED IDENTIFICATION BADGE IS SUBJECT TO ARREST AND CRIMINAL AND/OR CIVIL PROSECUTION.

B. EACH BADGED PERSON WHO HAS ESCORT AUTHORITY ANNOTATED ON THEIR MEDIA ID MAY ESCORT PERSONNEL IN ACCORDANCE WITH BLV RULES AND AS LONG AS ALL ESCORTED PERSONS ARE UNDER THE DIRECT AND IMMEDIATE CONTROL OF THE PERSON PERFORMING ESCORT DUTIES. THE BADGE APPROVAL PROCESS AND ALL FEES ARE SUBJECT TO CHANGE AT ANY TIME. VIOLATION OF AIRPORT RULES AND REGULATIONS REGARDING AIRPORT ISSUED IDENTIFICATION AND SECURITY PROCEDURES WILL RESULT IN REVOCATION OF THE BADGE AND POSSIBLE CRIMINAL AND/OR CIVIL PENALTIES. NO EXTENSION OF TIME OR OTHER ACCOMMODATION WILL BE MADE TO THE CONSTRUCTION CONTRACT DUE TO LOSS OF UNESCORTED PRIVILEGES FOR SECURITY VIOLATIONS.

IV. BADGE PROCESS:

A. APPLICANTS MUST OBTAIN A BADGING PACKET FROM THE MIDAMERICA ST LOUIS AIRPORT PUBLIC SAFETY OFFICE (PSO) LOCATED AT 8849 AIR SERVICE DRIVE, MASCOUTAH, IL (PHONE 618-566-5227).

B. APPLICANTS MUST COMPLETE FINGERPRINTING AT THE PUBLIC SAFETY OFFICE. PLEASE SCHEDULE TIME IN ADVANCE FOR FINGERPRINTING 618-566-5257 OR JASON.BRUNS@FLYMIDAMERICA.COM.

C. ALL PAPERWORK MUST BE COMPLETED PRINTED CLEARLY AND LEGIBLE. ANY ILLEGIBLE PAPERWORK WILL BE RETURNED AND WILL DELAY ISSUANCE OF THE BADGE.

D. APPLICANTS MUST SUBMIT A COMPLETE PACKET TO THE PUBLIC SAFETY OFFICE WHICH INCLUDES:

- 1. INITIAL BADGING FEE (SEE BADGE FEES BELOW)
2. AUTHORIZED SIGNATORY DESIGNATION LETTER COMPLETED AND SIGNED BY APPLICANT.
3. APPLICATION FOR AIRPORT ACCESS MEDIA/ID SIGNED BY APPLICANT AND AUTHORIZED SIGNATORY (REFER TO YOUR BLV CONTACT TO DETERMINE THE ANSWERS TO QUESTIONS REGARDING THE DRIVERS PERMIT, ESCORT

PRIVILEGES, KEYS AND ACCESS REQUIRED.)

- 4. APPLICANT MUST PRESENT TWO FORMS OF ID AS DENOTED IN THE PACKETS.
5. APPLICANT MUST COMPLETE APPLICATION FOR AOA DRIVER PERMIT IF NECESSARY.

E. TRAINING:

- 1. ONCE THE APPLICANT'S FINGERPRINT RESULTS HAVE BEEN RETURNED TO THE BADGING OFFICE, THE BADGING OFFICE WILL CONTACT THE AUTHORIZED SIGNATORY TO SCHEDULE AN ONLINE CLASS. ONCE THE ONLINE CLASS(ES) HAVE BEEN COMPLETED, THE BADGING OFFICE WILL SCHEDULE IN-PERSON TRAINING AND BADGE PICKUP WITH THE AUTHORIZED SIGNATORY.
2. ANY APPLICANT WHO WILL BE DESIGNATED AS A DRIVER ON THE AOA SHALL ALSO ATTEND THE PASS THE MIDAMERICA ST. LOUIS AIRPORT DRIVERS TRAINING PROGRAM AND CONDUCT AIRPORT FAMILIARIZATION AND DRIVE ON THE AOA. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON MIDAMERICA ST. LOUIS AIRPORT (BLV).

V. BADGE FEES:

A. GENERAL:

- 1. CHECKS ARE PAYABLE TO MIDAMERICA AIRPORT.
2. YOUR COMPANY IS RESPONSIBLE FOR THESE FEES.
3. ALL FEES ARE SUBJECT TO CHANGE AT ANY TIME.
4. FEES MAY BE WAIVED AND/OR TIMEFRAMES ADJUSTED AT THE DISCRETION OF THE AIRPORT DIRECTOR.

B. INITIAL BADGING FEE - \$90.00

- 1. COVERS THE COST OF FINGERPRINTING AND SUBMISSION, SECURITY CLASS/BADGING, AND BACKGROUND CHECKS THROUGH FBI AND TSA (STA AND CHRC).

C. YEARLY STA - \$10.00

- 1. REQUIRED YEARLY BY TSA - STA (SECURITY THREAT ASSESSMENT)

D. EVERY TWO YEARS - \$60.00

- 1. FINGERPRINT RESUBMITTAL - REQUIRED BY TSA FOR BIENNIAL CHRC (CRIMINAL HISTORY RECORDS CHECK) THROUGH THE FBI.

E. BROKEN/DAMAGED FEE - \$60.00

- 1. COST TO REPLACE A BROKEN OR DAMAGED BADGE.

F. LOST BADGE FEE - \$150.00

- 1. ANY BADGE NOT RETURNED WITHIN 30 DAYS OF PUNCHLIST COMPLETION WILL BE ASSESSED THE LOST BADGE FEE AND MAY ANY INCUR A TSA FEE FOR NOT RETURNING THE SIDA BADGE.

VI. ESCORTING:

A. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE ESCORT SERVICES FOR ALL EMPLOYEES, SUB-CONTRACTORS, AND VENDORS/SUPPLIERS REQUIRING ACCESS TO THE CONSTRUCTION SITE.

B. THE INDIVIDUAL PROVIDING THE ESCORT WILL POSSESS A VALID AIRPORT ID BADGE AND MUST STAY WITHIN VISUAL AND VERBAL CONTROL OF THE ESCORTED INDIVIDUAL. THE INDIVIDUAL PROVIDING ESCORT MUST BE FAMILIAR WITH THE AIRPORT RULES AND REGULATIONS AND THE VEHICLE DRIVERS MANUAL.

C. THE CONTRACTOR SHALL PROVIDE AN ESCORT VEHICLE, PROPERLY MARKED, WITH DRIVER TO ESCORT DELIVERY VEHICLES AND CONSTRUCTION EQUIPMENT BETWEEN THE WORKSITE AND THE IDENTIFIED ENTRY GATE TO THE WORK AREA. ESCORT DRIVER SHALL HAVE A VALID AIRPORT IDENTIFICATION BADGE WITH THE APPROPRIATE PRIVILEGES (AOA OR MCA). ESCORT DRIVER SHALL BE TRAINED BY THE BADGING OFFICE OR DESIGNATED OFFICIAL AND CONFORM TO ALL RULES AND REGULATIONS OF MIDAMERICA ST. LOUIS AIRPORT. ADDITIONAL ESCORT VEHICLES MAY BE NEEDED DEPENDING UPON VOLUME OF VEHICLES.

D. THE PUBLIC SAFETY OFFICE OR DESIGNEE WILL INSPECT ALL VEHICLES ENTERING THE AOA. NO MORE THAN 3 VEHICLES MAY BE ESCORTED AT ONE TIME. VEHICLES MUST REMAIN TOGETHER AND MAY NOT SEPARATE AT ANY TIME FROM THE ESCORT CARAVAN. CONTRACTOR SHALL ENSURE A COMMUNICATION SYSTEM SO THAT DELIVERY DRIVERS MAY CONTACT THE ESCORT DRIVER WHEN WAITING FOR ESCORT AT THE AIRFIELD GATE. DELIVERY VEHICLES SHALL NOT BLOCK TRAFFIC AT THE ENTRANCE GATE(S) WHILE WAITING FOR ESCORT. VEHICLES MAY BE DIRECTED OFF SITE IF PROBLEMS ARE CREATED. ALL DELIVERY VEHICLES MUST BE ESCORTED TO AND FROM THE WORKSITE. NO EXCEPTIONS. ANY VIOLATION OF THESE PROVISIONS OR AIRPORT REGULATIONS MAY RESULT IN PERMANENT REVOCATION OF SECURITY BADGES OF THOSE OFFENDERS.

(NOTES CONTINUE ON SHEET GC004)



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APRIL 21, 2023

TERMINAL APRON EXPANSION -
PROJECT 1



MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

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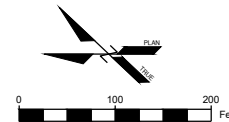
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SHEET TITLE

CONSTRUCTION
ACTIVITY PLAN
NOTES 3



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THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)

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APRIL 21, 2023

TERMINAL APRON EXPANSION -
PROJECT 1



MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

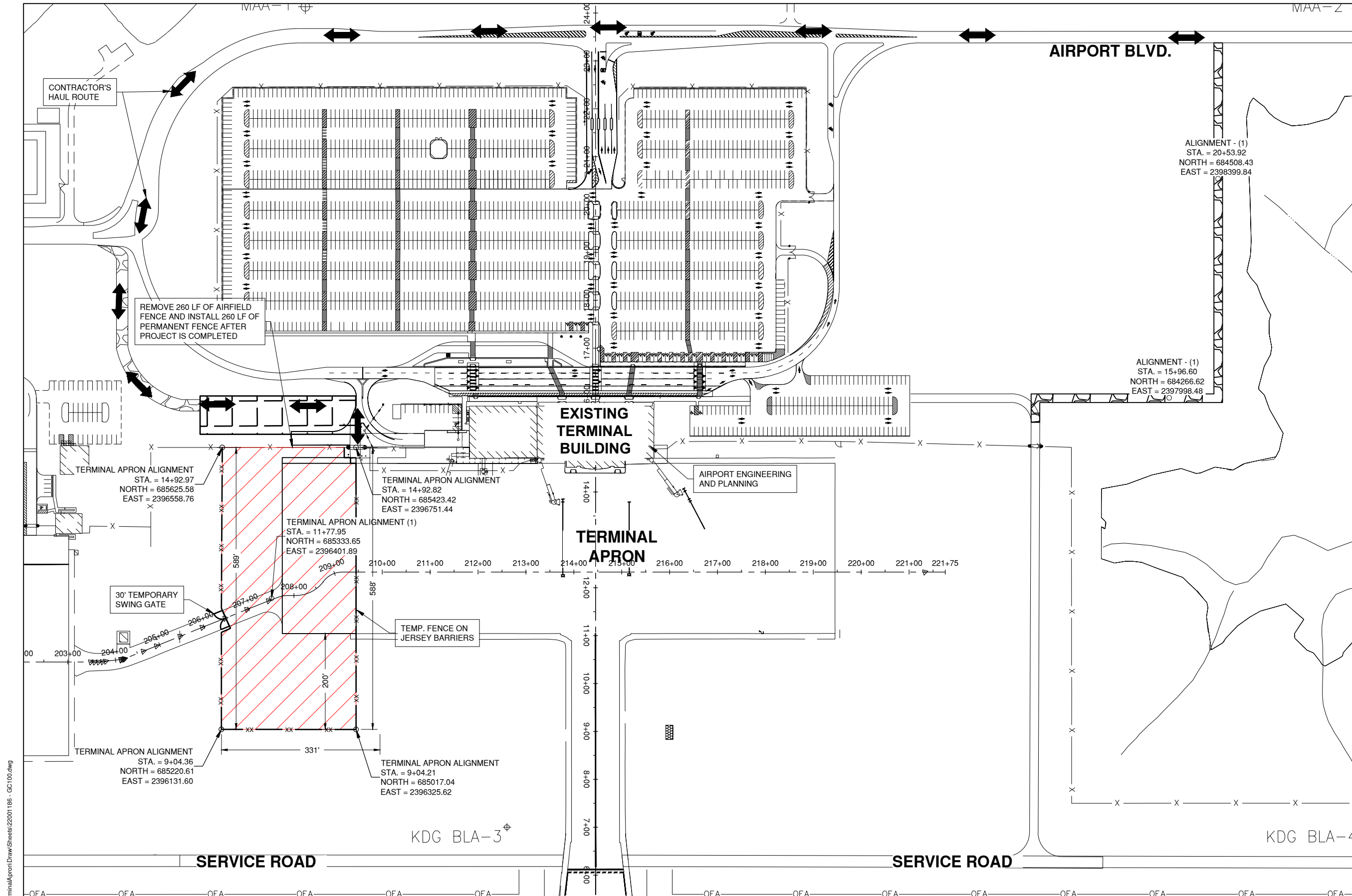
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SHEET TITLE

TEMPORARY FENCING
PLAN

SHEET 14 OF 60



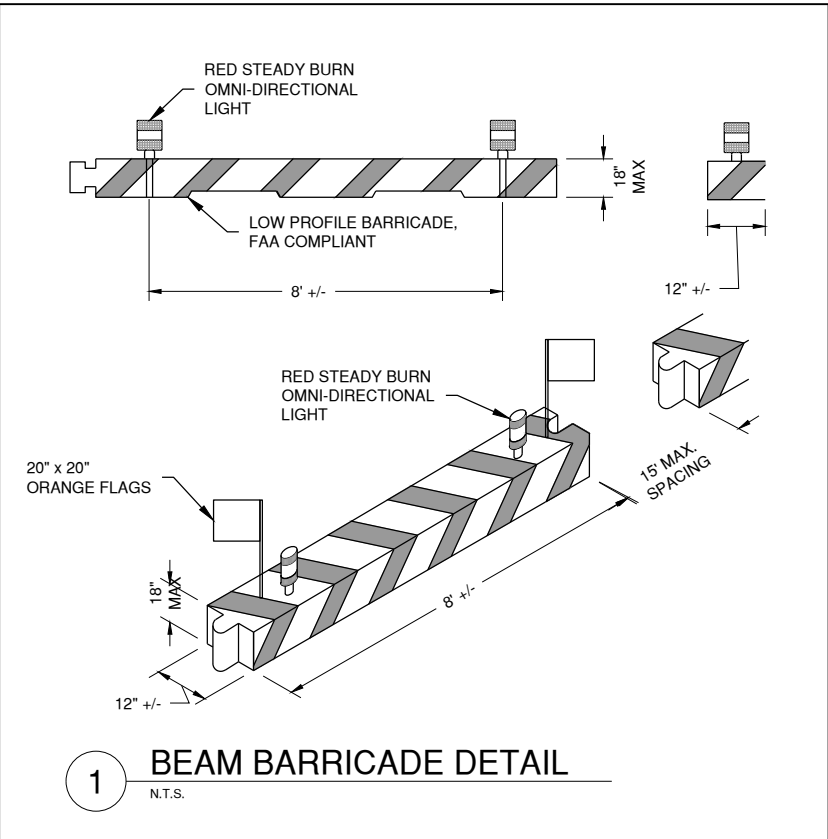
LEGEND

- PHASE 1 WORK AREA
- EXISTING AIRPORT PERIMETER FENCING
- PROPOSED TEMPORARY FENCING
- CONTRACTOR'S HAUL ROUTE
- 30' TEMPORARY GATE

TEMPORARY FENCING NOTES:

1. ALL TEMPORARY FENCE SHALL BE GROUNDED AT 500' INTERVALS. THE COST SHALL BE INCIDENTAL TO THE TEMPORARY FENCE.
2. EXISTING PERMANENT FENCE SHALL NOT BE REMOVED UNTIL TEMP. FENCE HAS BEEN ACCEPTED BY THE AIRPORT. A SECURE PERIMETER MUST BE KEPT AT ALL TIMES.
3. FINAL PERMANENT FENCE SHALL BE INSTALLED AND ACCEPTED BY THE AIRPORT BEFORE TEMP. FENCE CAN BE REMOVED.

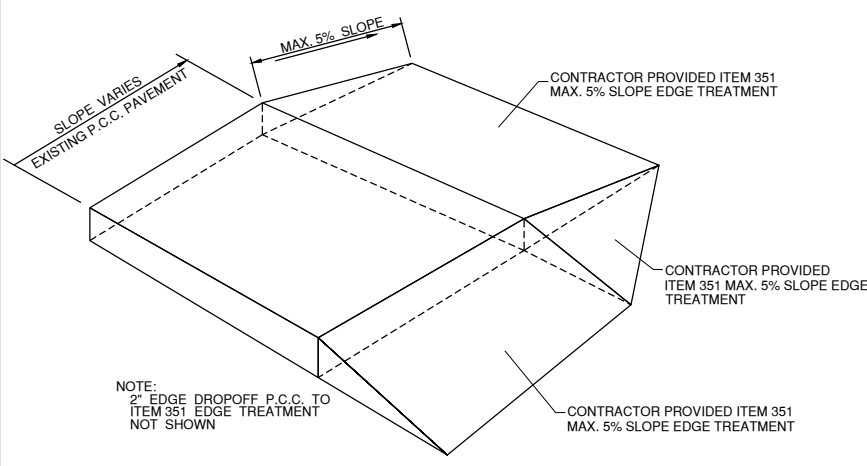
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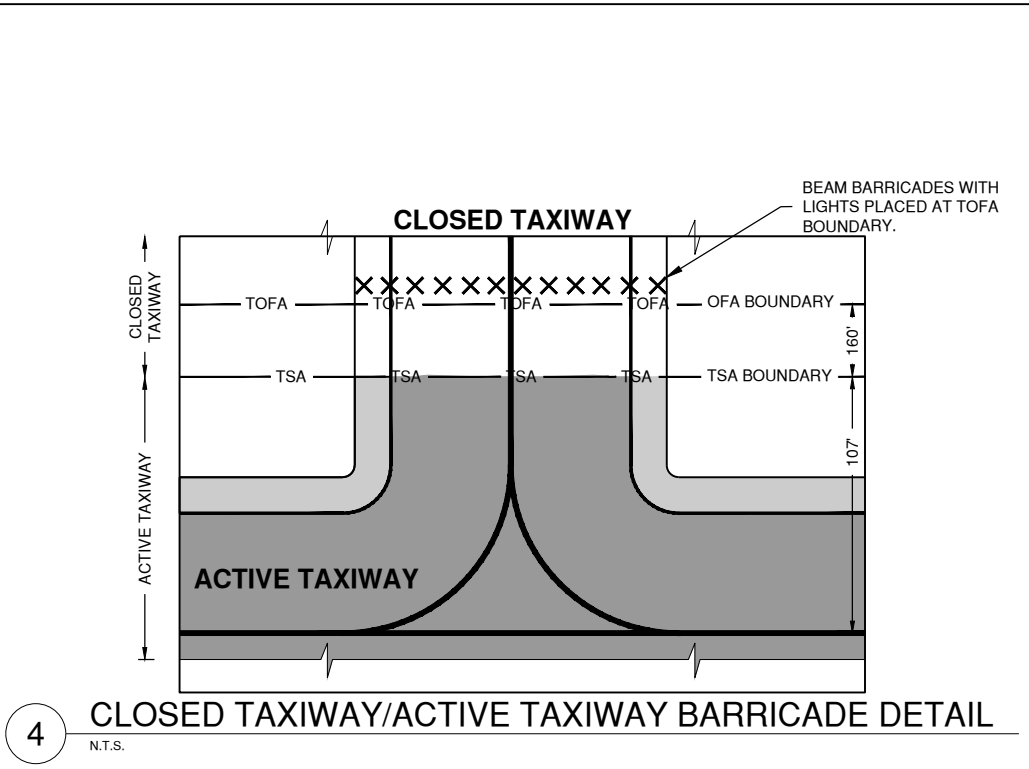
1 BEAM BARRICADE DETAIL
N.T.S.

BEAM BARRICADE NOTES

1. BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY WIND, JET OR PROP BLAST.
2. BARRICADE MUST BE OF LOW MASS AND EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AS SHOWN IN PLANS AND AS DIRECTED BY THE ENGINEER.
5. BARRICADES SHALL BE COMPLIANT WITH FAA AC 150/5370-2 (LATEST VERSION).
6. THE CONTRACTOR MAY USE THE LIMITED NUMBER OF BLV SUPPLIED BARRICADES BUT MUST MAINTAIN THE LIGHTS AND FLAGS REQUIRED.



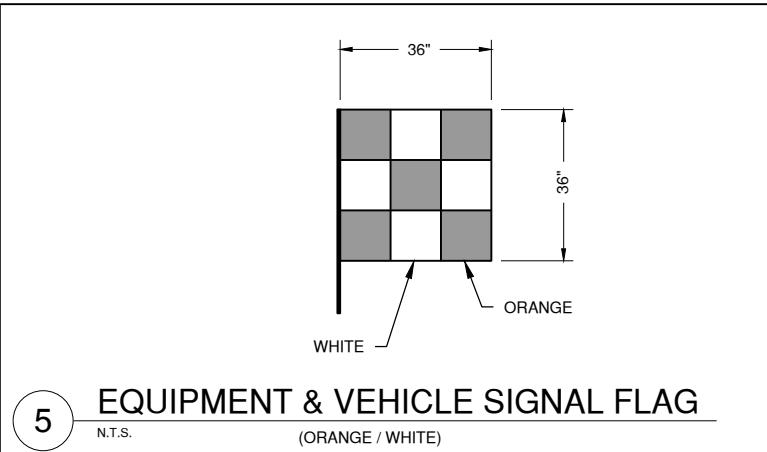
2 EDGE TREATMENT
N.T.S.



4 CLOSED TAXIWAY/ACTIVE TAXIWAY BARRICADE DETAIL
N.T.S.

EDGE TREATMENT NOTES

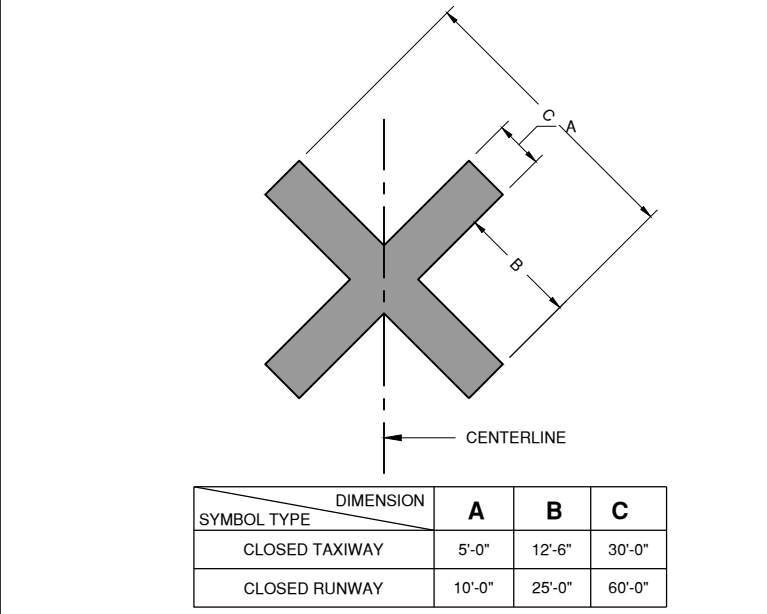
1. EDGE TREATMENT MUST BE IN PLACE ANYTIME TAXIWAY IS TO BE OPEN TO AIRCRAFT TRAFFIC AT THE END OF EACH WORK PERIOD. THIS INCLUDES THE TIME TO COMPLETE ALL CLEAN UP AND OBTAIN INSPECTION AND APPROVAL BY AIRPORT OPERATIONS.
2. EDGE TREATMENT MUST BE USED ON ALL PHASES OF CONSTRUCTION DONE WITHIN THE PHASE 4 WORK LIMITS.
3. CONTRACTOR SHALL MAINTAIN AREA WITHIN WORK LIMITS SUCH THAT NO DROPOFFS OF GREATER THAN 3" EXISTING WHEN TAXIWAY IS OPENED TO AIRCRAFT TRAFFIC.
4. MAXIMUM SLOPES SHALL NOT EXCEED 5 PERCENT WHEN THE TAXIWAY IS OPEN TO AIRCRAFT TRAFFIC.
5. EDGE TREATMENT SHALL BE COMPACTED ACCORDING TO ITEM 351.
6. ALL COST ASSOCIATED WITH EDGE TREATMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTACT.
7. ANY AGGREGATE SURFACES MUST BE COVERED WITH A SECURED BLANKET OR OTHER APPROVED MATERIAL PRIOR TO OPENING THE TAXIWAY TO AIRCRAFT OPERATIONS. MATERIAL AND METHOD OF SECURING SHALL BE APPROVED BY AIRPORT OPERATIONS PRIOR TO THE RE-OPENING TIME.



5 EQUIPMENT & VEHICLE SIGNAL FLAG
N.T.S. (ORANGE / WHITE)

SIGNAL FLAG NOTES

1. ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG.
2. WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS.
3. CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.



6 NON-LIGHTED CLOSURE MARKER
N.T.S.

NOTES

1. CLOSURE MARKERS SHALL BE SOLID YELLOW.
2. MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA.
3. MARKERS SHALL BE PLACED ON RUNWAYS TO COVER THE NUMERALS ON BOTH ENDS.
4. MARKERS MAY BE CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
5. MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT.



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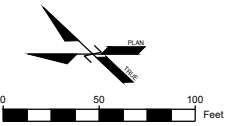


MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

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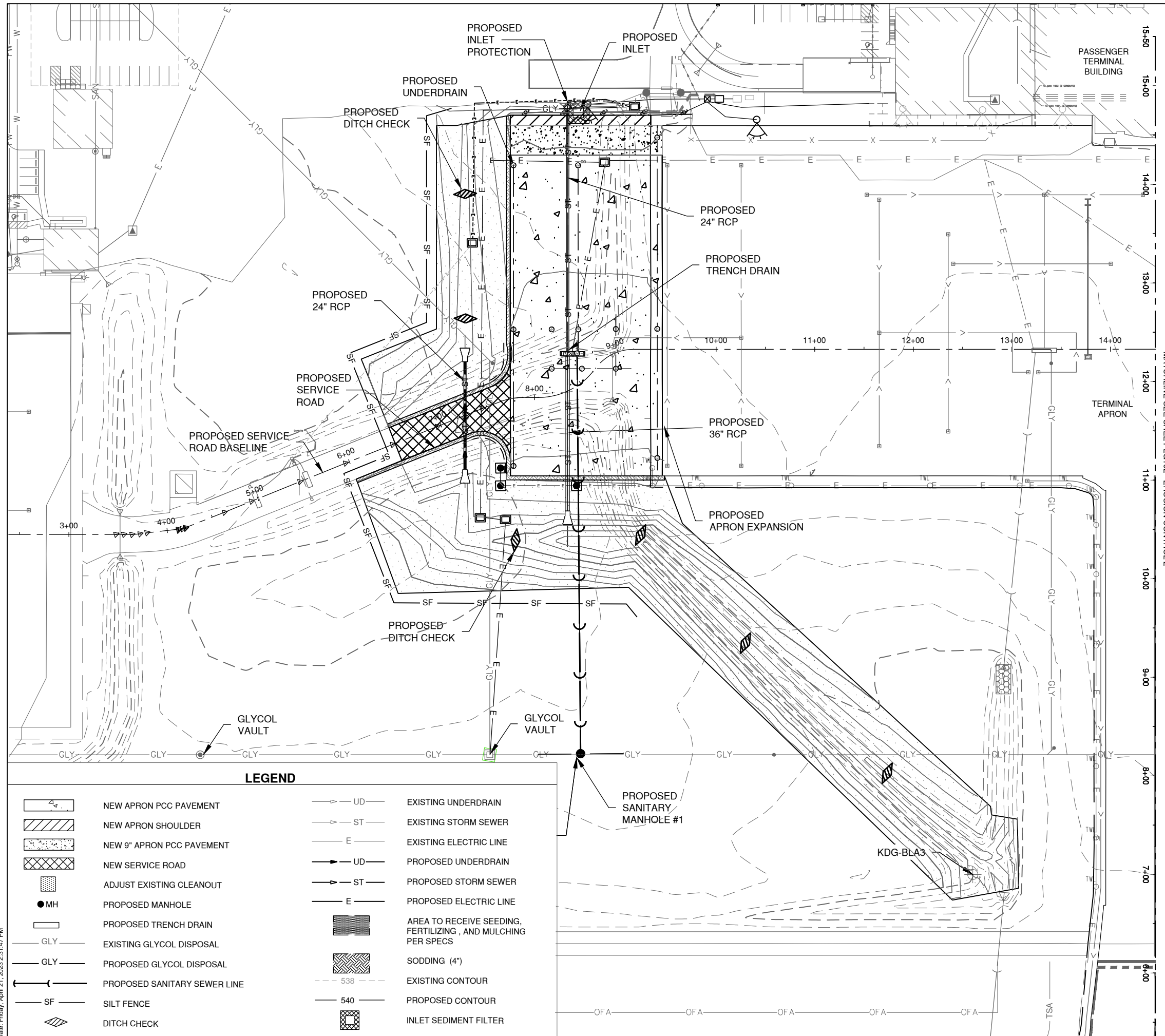
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MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

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		IL PROJECT NO. BLV-5087
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		DESIGNED BY: CMT
		DRAWN BY: %%U
		CHECKED BY: CMT
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**EROSION CONTROL
PLAN 1**



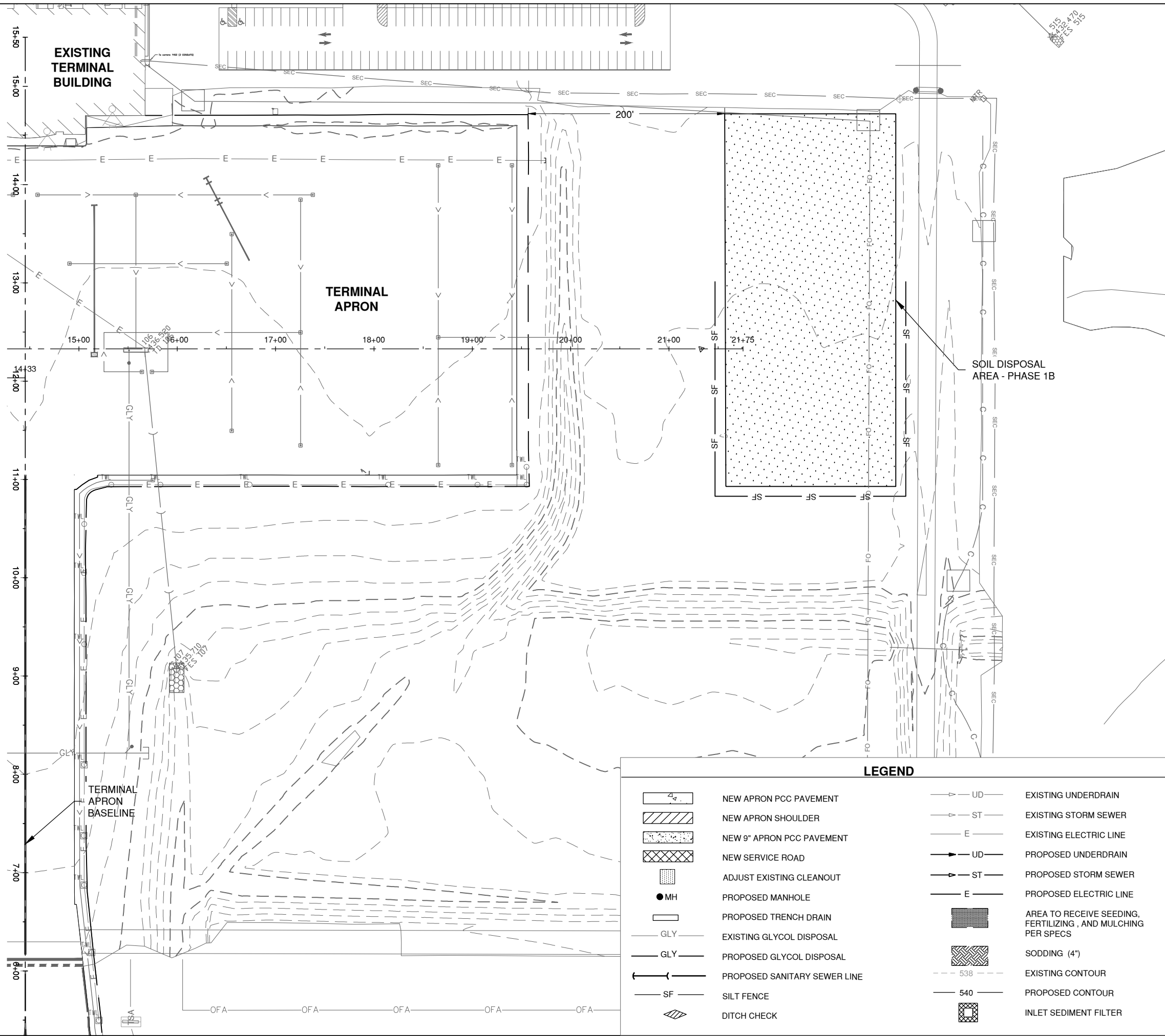
LEGEND

	NEW APRON PCC PAVEMENT		EXISTING UNDERDRAIN
	NEW APRON SHOULDER		EXISTING STORM SEWER
	NEW 9" APRON PCC PAVEMENT		EXISTING ELECTRIC LINE
	NEW SERVICE ROAD		PROPOSED UNDERDRAIN
	ADJUST EXISTING CLEANOUT		PROPOSED STORM SEWER
	PROPOSED MANHOLE		PROPOSED ELECTRIC LINE
	PROPOSED TRENCH DRAIN		AREA TO RECEIVE SEEDING, FERTILIZING, AND MULCHING PER SPECS
	EXISTING GLYCOL DISPOSAL		SODDING (4")
	PROPOSED GLYCOL DISPOSAL		EXISTING CONTOUR
	PROPOSED SANITARY SEWER LINE		PROPOSED CONTOUR
	SILT FENCE		INLET SEDIMENT FILTER
	DITCH CHECK		

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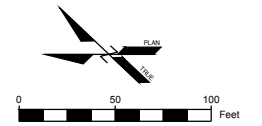
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MATCHLINE SHEET LG102 - EROSION CONTROL 1



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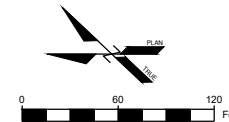
MIDAMERICA
 ST. LOUIS AIRPORT
 ST. CLAIR COUNTY, IL

LEGEND

- | | | | |
|--|------------------------------|--|--|
| | NEW APRON PCC PAVEMENT | | EXISTING UNDERDRAIN |
| | NEW APRON SHOULDER | | EXISTING STORM SEWER |
| | NEW 9" APRON PCC PAVEMENT | | EXISTING ELECTRIC LINE |
| | NEW SERVICE ROAD | | PROPOSED UNDERDRAIN |
| | ADJUST EXISTING CLEANOUT | | PROPOSED STORM SEWER |
| | PROPOSED MANHOLE | | PROPOSED ELECTRIC LINE |
| | PROPOSED TRENCH DRAIN | | AREA TO RECEIVE SEEDING, FERTILIZING, AND MULCHING PER SPECS |
| | EXISTING GLYCOL DISPOSAL | | SODDING (4") |
| | PROPOSED GLYCOL DISPOSAL | | EXISTING CONTOUR |
| | PROPOSED SANITARY SEWER LINE | | PROPOSED CONTOUR |
| | SILT FENCE | | INLET SEDIMENT FILTER |
| | DITCH CHECK | | |

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**EROSION CONTROL
 PLAN 2**



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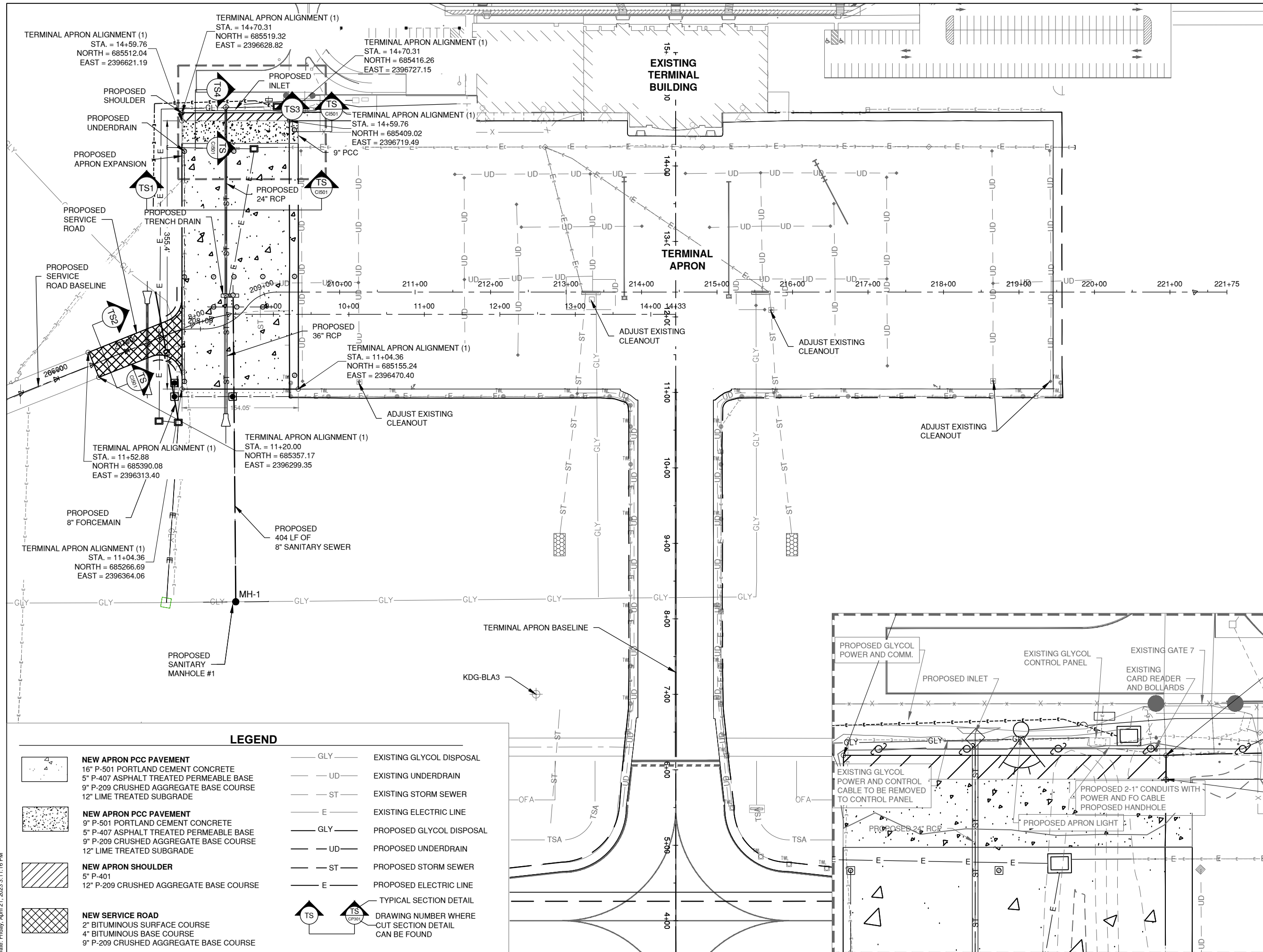
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PROPOSED
IMPROVEMENTS

CS101
SHEET 22 OF 60



LEGEND

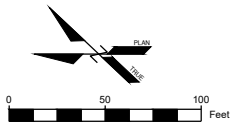
- NEW APRON PCC PAVEMENT**
16" P-501 PORTLAND CEMENT CONCRETE
5" P-407 ASPHALT TREATED PERMEABLE BASE
9" P-209 CRUSHED AGGREGATE BASE COURSE
12" LIME TREATED SUBGRADE
- NEW APRON PCC PAVEMENT**
9" P-501 PORTLAND CEMENT CONCRETE
5" P-407 ASPHALT TREATED PERMEABLE BASE
9" P-209 CRUSHED AGGREGATE BASE COURSE
12" LIME TREATED SUBGRADE
- NEW APRON SHOULDER**
5" P-401
12" P-209 CRUSHED AGGREGATE BASE COURSE
- NEW SERVICE ROAD**
2" BITUMINOUS SURFACE COURSE
4" BITUMINOUS BASE COURSE
9" P-209 CRUSHED AGGREGATE BASE COURSE
- GLY — EXISTING GLYCOL DISPOSAL
- UD — EXISTING UNDERDRAIN
- ST — EXISTING STORM SEWER
- E — EXISTING ELECTRIC LINE
- GLY — PROPOSED GLYCOL DISPOSAL
- UD — PROPOSED UNDERDRAIN
- ST — PROPOSED STORM SEWER
- E — PROPOSED ELECTRIC LINE
- TYPICAL SECTION DETAIL
- DRAWING NUMBER WHERE CUT SECTION DETAIL CAN BE FOUND

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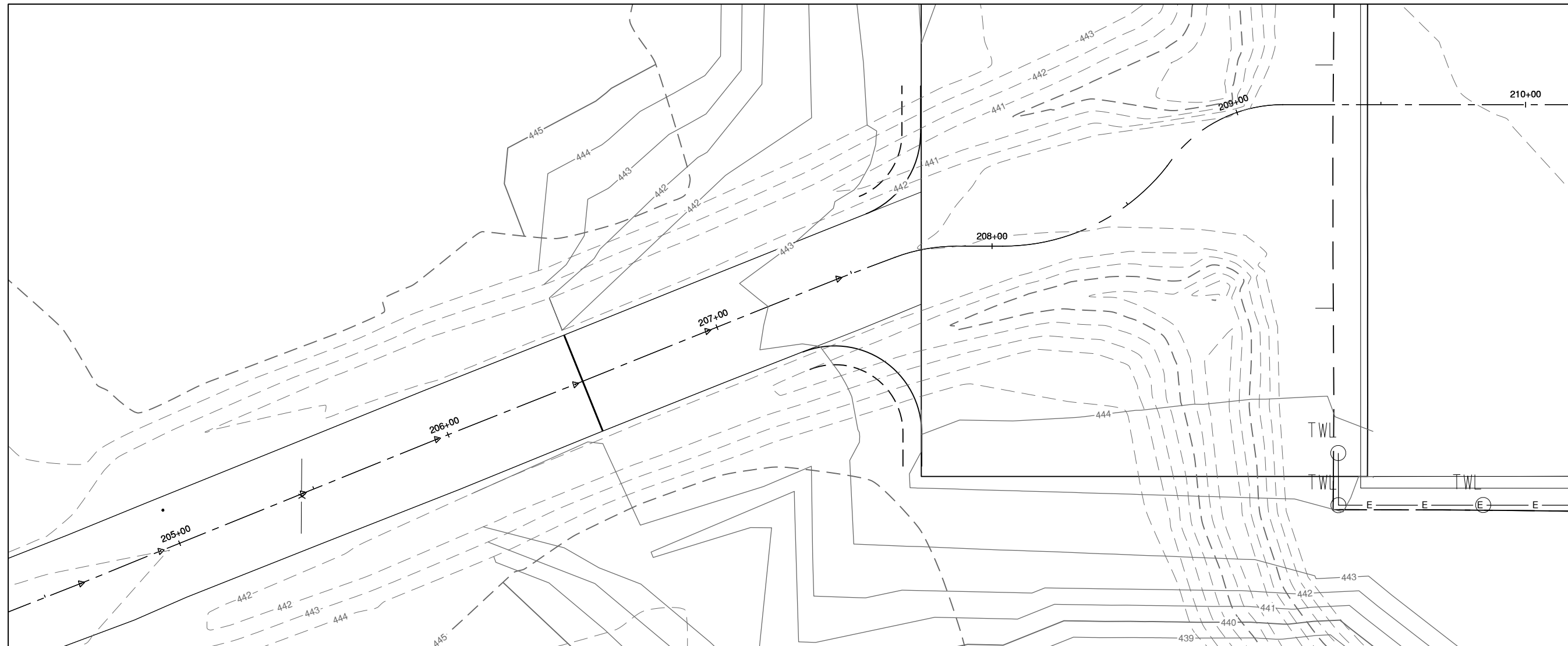
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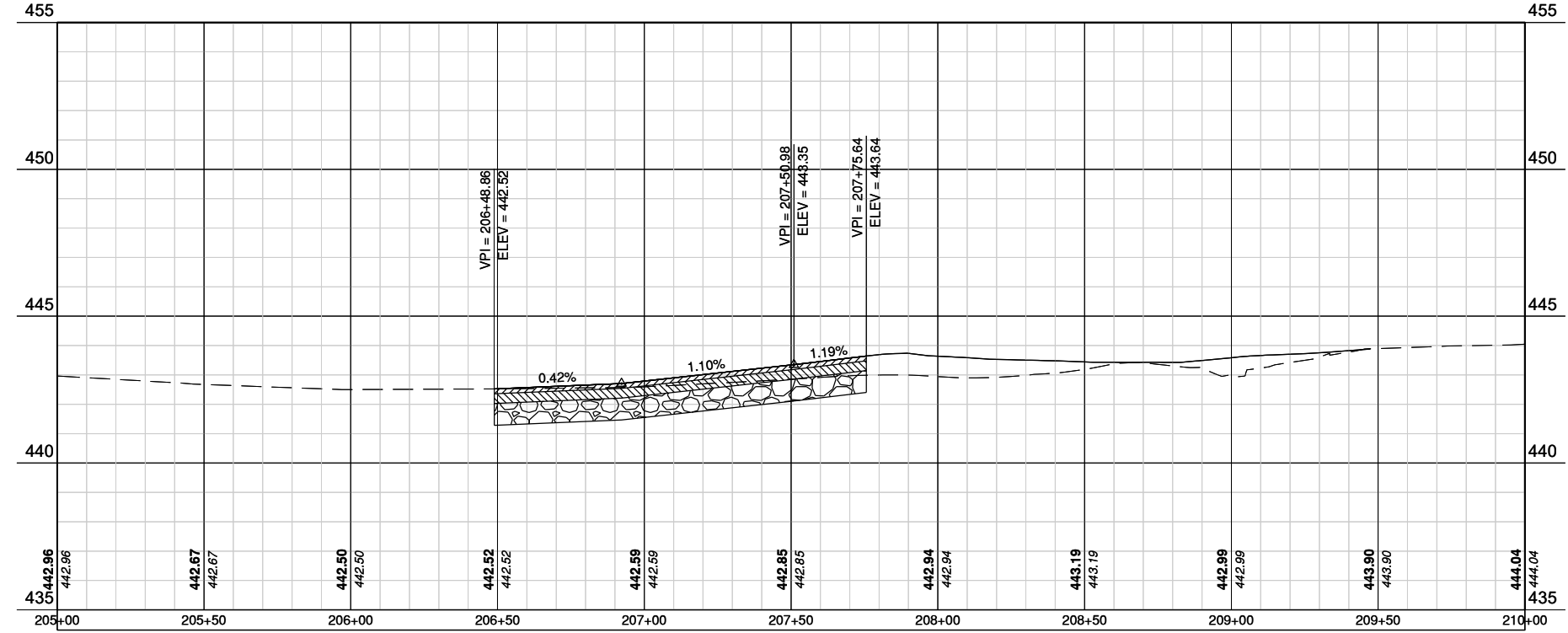
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**SERVICE ROAD PLAN
& PROFILE SHEET**

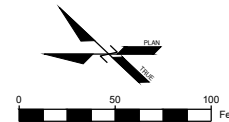
SHEET 23 OF 60



SERVICE ROAD PROFILE



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APRIL 21, 2023

TERMINAL APRON EXPANSION -
PROJECT 1



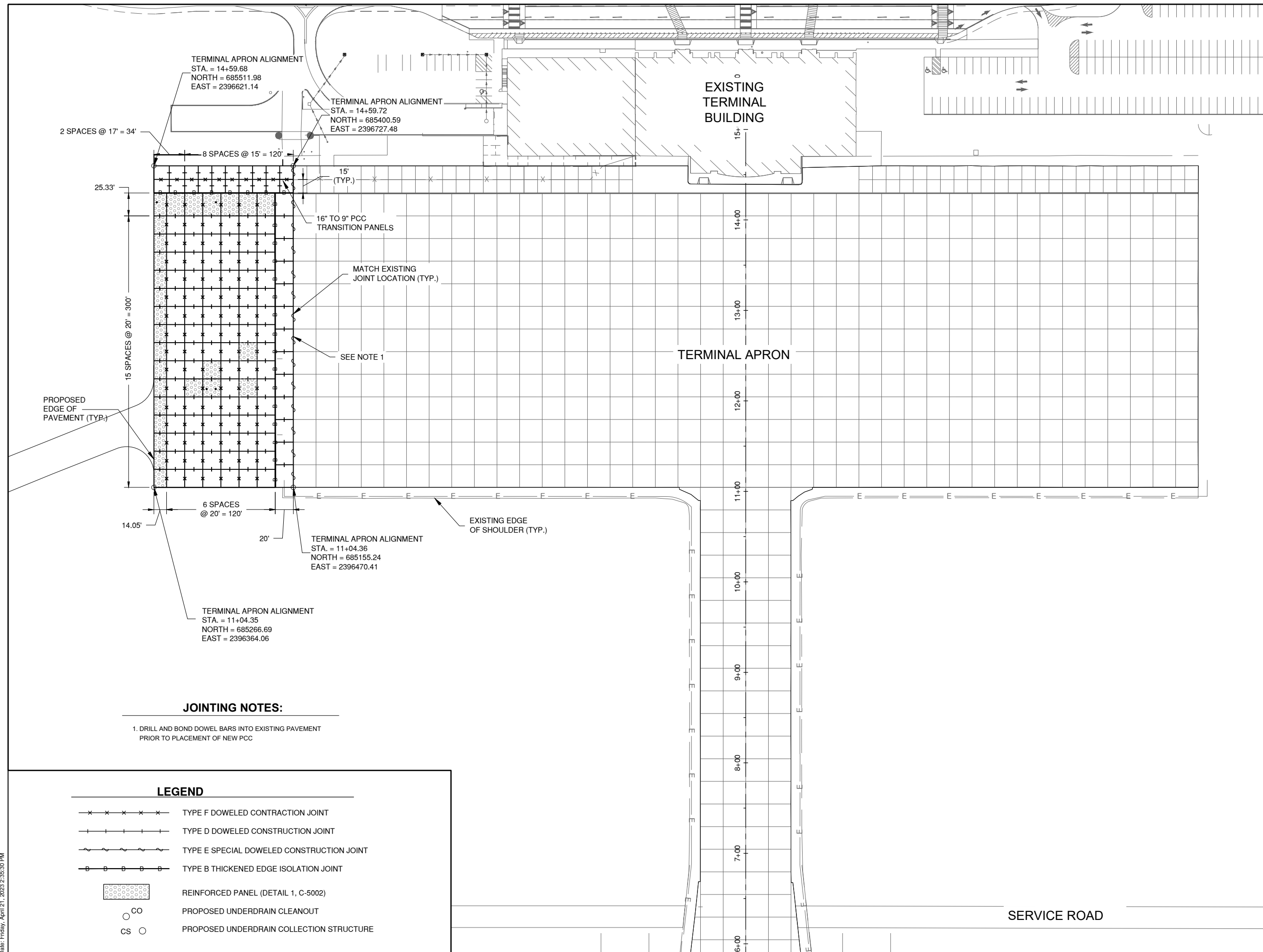
MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION
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		IL PROJECT NO. BLV-5087
		CMT PROJECT NO: 22001186.00
		CAD DWG FILE: 22001186 - CP100.DWG
		DESIGNED BY: CMT
		DRAWN BY: %%UJ
		CHECKED BY: CMT
		APPROVED BY: CMT
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SHEET TITLE

JOINTING PLAN

SHEET 25 OF 60
CP101



JOINTING NOTES:

1. DRILL AND BOND DOWEL BARS INTO EXISTING PAVEMENT PRIOR TO PLACEMENT OF NEW PCC

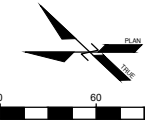
LEGEND

- TYPE F DOWELED CONTRACTION JOINT
- TYPE D DOWELED CONSTRUCTION JOINT
- TYPE E SPECIAL DOWELED CONSTRUCTION JOINT
- TYPE B THICKENED EDGE ISOLATION JOINT
- REINFORCED PANEL (DETAIL 1, C-5002)
- PROPOSED UNDERDRAIN CLEANOUT
- PROPOSED UNDERDRAIN COLLECTION STRUCTURE



License No. 184-000613

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APRIL 21, 2023

TERMINAL APRON EXPANSION -
PROJECT 1



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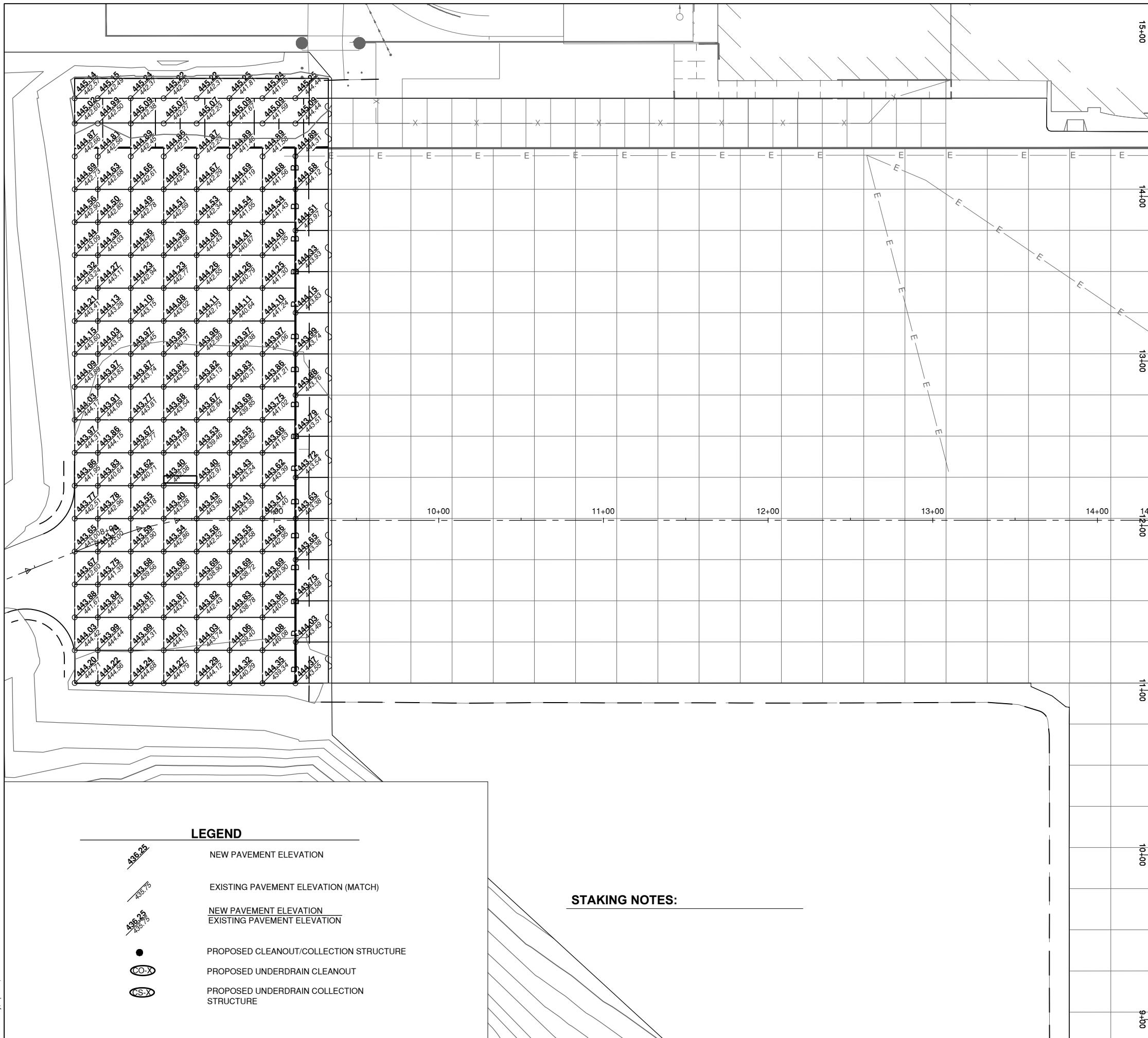
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CMT PROJECT NO.	22001186.00
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DESIGNED BY:	###
DRAWN BY:	%%U
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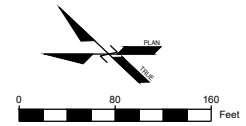
SHEET TITLE

STAKING PLAN 1

SHEET 27 OF 60
CP801



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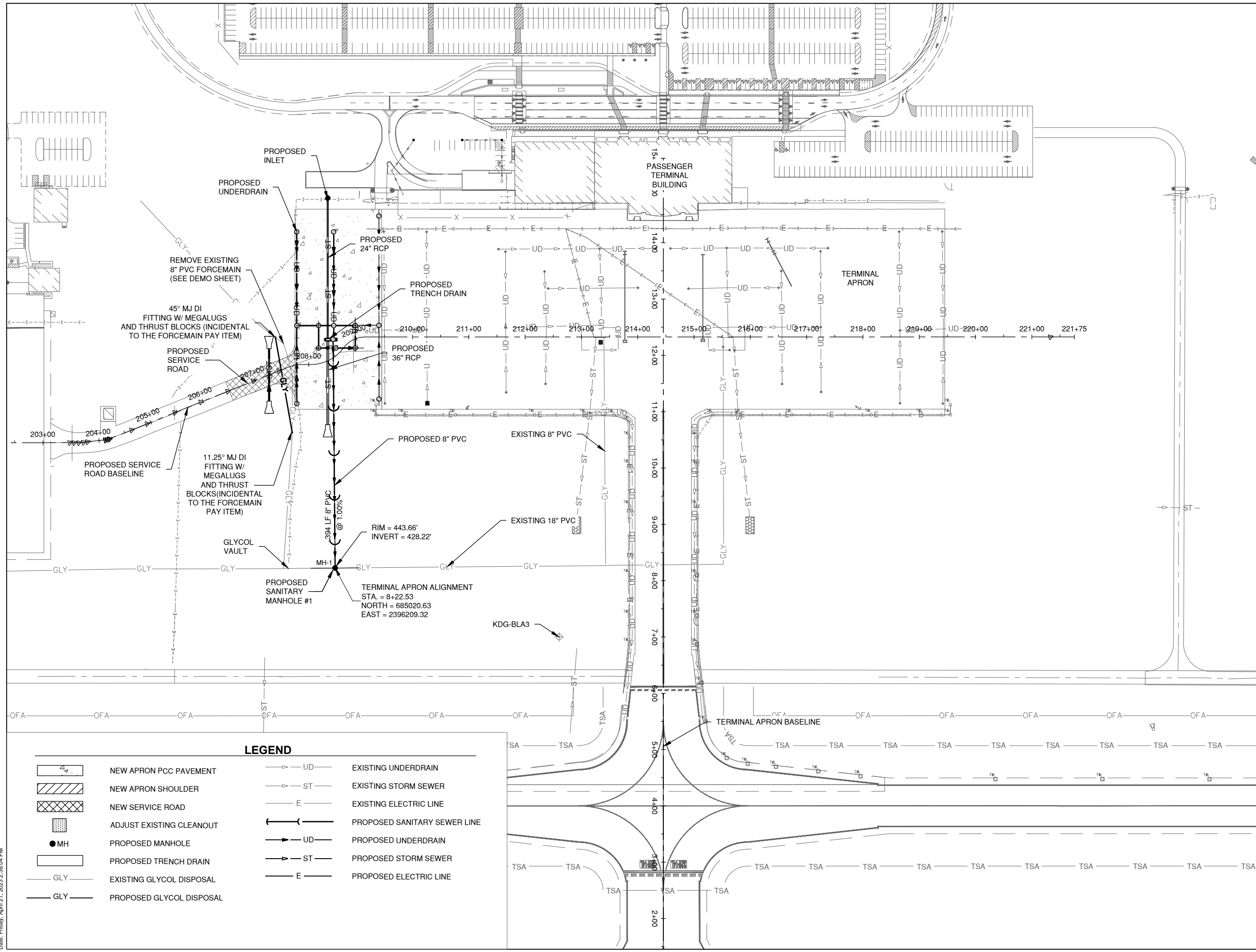
BID ISSUE
APRIL 21, 2023
TERMINAL APRON EXPANSION -
PROJECT 1



MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION
		BLV PROJECT NO. 2022-14
		IL PROJECT NO. BLV-5087
		CMT PROJECT NO: 22001186.00
		CAD DWG FILE: 22001186 - GLYCOL.DWG
		DESIGNED BY: CMT
		DRAWN BY: %%U
		CHECKED BY: CMT
		APPROVED BY: CMT
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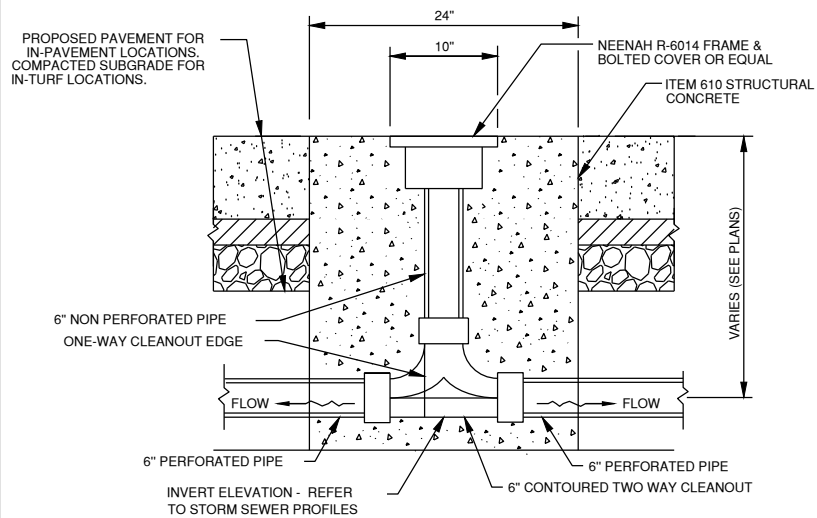
SHEET TITLE
**GLYCOL
IMPROVEMENTS**



LEGEND

- | | | | |
|--|--------------------------|--|------------------------------|
| | NEW APRON PCC PAVEMENT | | EXISTING UNDERDRAIN |
| | NEW APRON SHOULDER | | EXISTING STORM SEWER |
| | NEW SERVICE ROAD | | EXISTING ELECTRIC LINE |
| | ADJUST EXISTING CLEANOUT | | PROPOSED SANITARY SEWER LINE |
| | PROPOSED MANHOLE | | PROPOSED UNDERDRAIN |
| | PROPOSED TRENCH DRAIN | | PROPOSED STORM SEWER |
| | EXISTING GLYCOL DISPOSAL | | PROPOSED ELECTRIC LINE |
| | PROPOSED GLYCOL DISPOSAL | | |

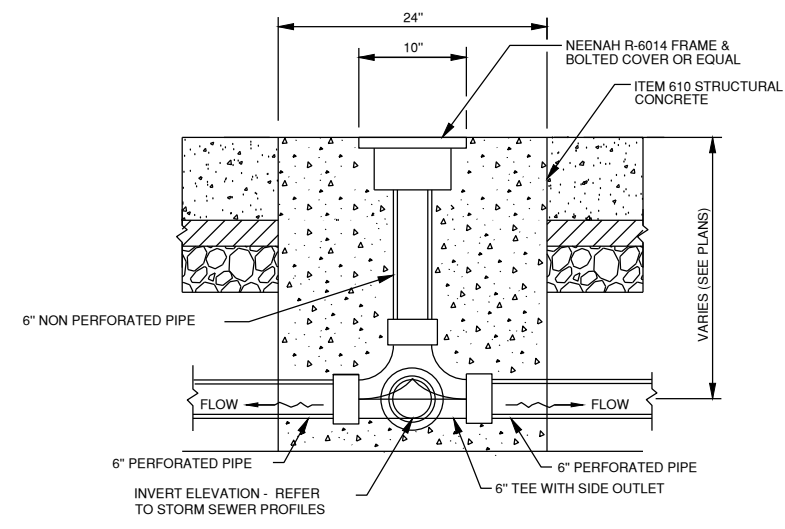
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ONE-WAY OR TWO-WAY CLEANOUT ELEVATION
N.T.S.

UNDERDRAIN CLEANOUT NOTES

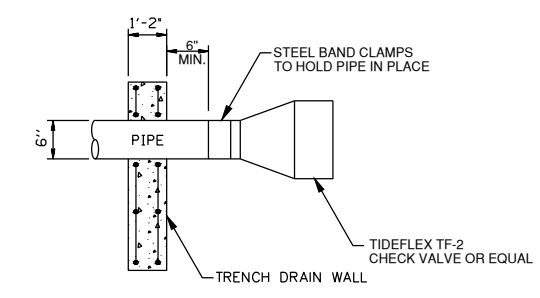
- SEQUENCE OF CONSTRUCTION/PLACEMENT OF CLEANOUTS SHALL BE AS FOLLOWS:
 - PAVE ASPHALT
 - CORE THROUGH ASPHALT
 - SET CLEANOUT WITH PCC COLLAR
 - PAVE CONCRETE APRON
- FINISHED GRADE OF CLEANOUTS AND COLLECTION STRUCTURES SHALL BE AT OR JUST BELOW PCC PAVEMENT SURFACE ELEVATION. ANY STRUCTURE EXTENDING ABOVE THE FINISHED PCC PAVEMENT ELEVATION SHALL BE ADJUSTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.



UNDERDRAIN COLLECTION STRUCTURE
N.T.S.

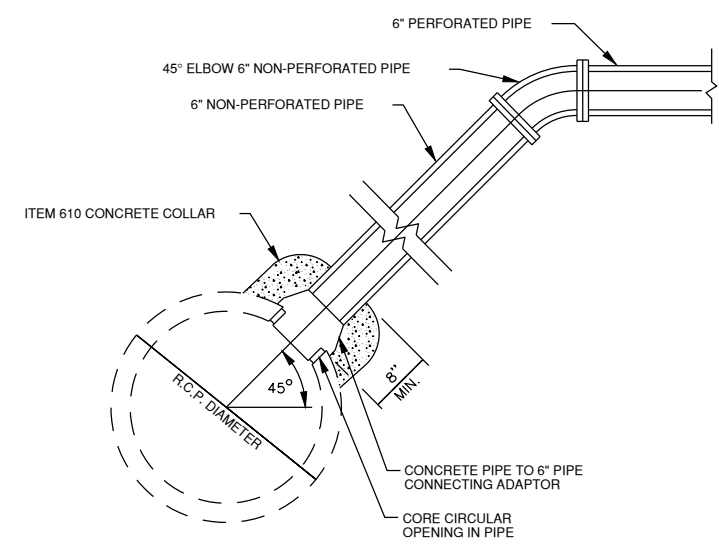
UNDERDRAIN COLLECTION STRUCTURE NOTES

- SEQUENCE OF CONSTRUCTION/PLACEMENT OF CLEANOUTS SHALL BE AS FOLLOWS:
 - PAVE ASPHALT
 - CORE THROUGH ASPHALT
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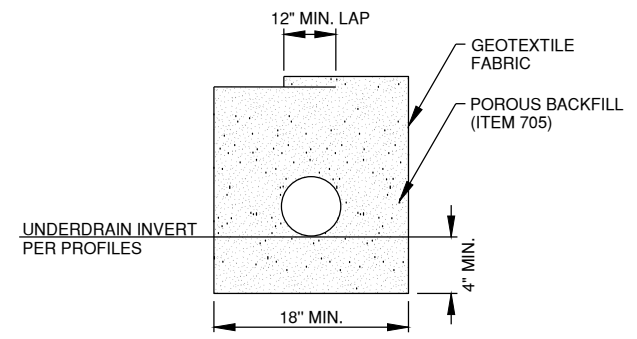


UNDERDRAIN BACKFLOW DETAIL
N.T.S.

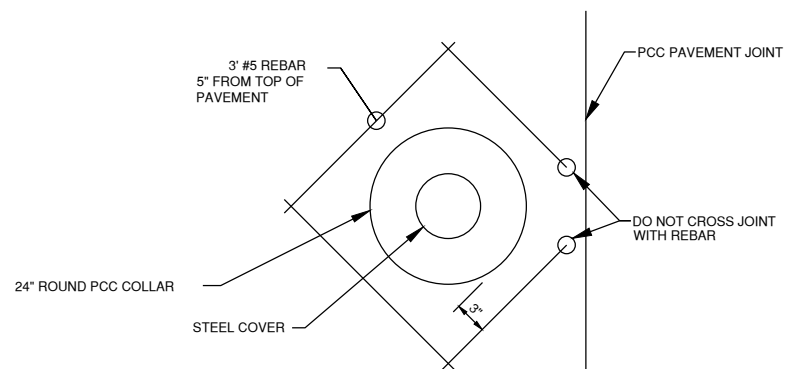
NOTE: CHECK VALVE SHALL BE INCIDENTAL TO UNDERDRAIN WORK.



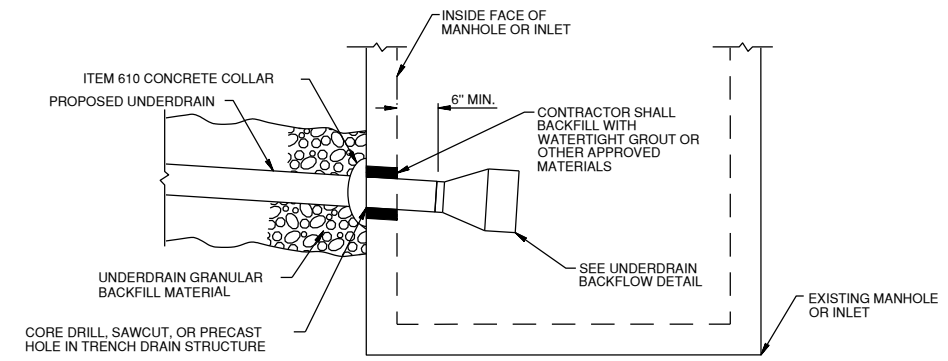
UNDERDRAIN DIRECT CONNECTION AT RCP
N.T.S.



UNDERDRAIN TRENCH DETAIL
N.T.S.



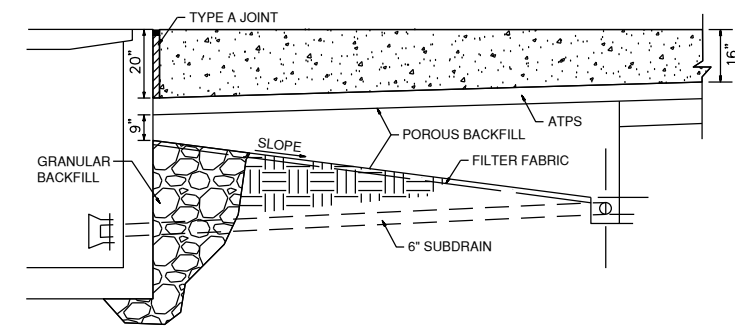
UNDERDRAIN PLAN VIEW
N.T.S.



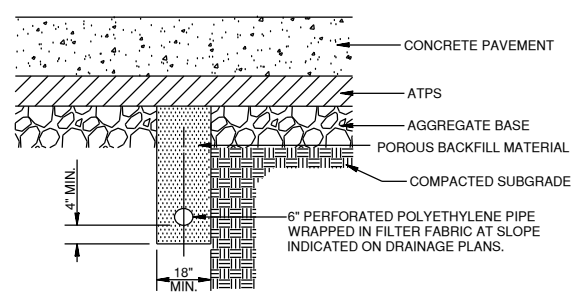
DIRECT CONNECTION DETAIL
N.T.S.

DIRECT CONNECTION NOTES

- HOLE IN STRUCTURE SHALL BE AT LEAST 1" WIDER THAN UD PIPE.
- FILL SPACE BETWEEN UNDERDRAIN AND STRUCTURE WITH NON-SHRINK GROUT OR ITEM 610 PCC COLLAR.



SUBDRAIN DETAIL AT TRENCH DRAIN
N.T.S.



UNDERDRAIN DETAIL
N.T.S.

MARK	DATE	DESCRIPTION
		BLV PROJECT NO. 2022-14
		IL PROJECT NO. BLV-5087
		CMT PROJECT NO: 22001186.00
		CAD DWG FILE: 22001186 - CG500.DWG
		DESIGNED BY: CMT
		DRAWN BY: %%LU
		CHECKED BY: CMT
		APPROVED BY: CMT
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CONSULTANTS



0 50 100 Feet
THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)



GONZALEZ COMPANIES, LLC
525 WEST MAIN STREET
SUITE 125
BELLEVILLE, IL 62220
PHONE: (618) 222-2221
WWW.GONZALEZCOS.COM
ILLINOIS PROFESSIONAL DESIGN FIRM 184.004564

100% DESIGN SUBMITTAL
APRIL 21, 2023

TERMINAL APRON EXPANSION
PROJECT 1

OWNER



MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION

BLV PROJECT NO. 2022-14

IL PROJECT NO. BLV-5087

CMT PROJECT NO: 22001186 GONZALEZ PROJECT NO: 22-1031

CAD DWG FILE: 22001186-CM500.DWG

DESIGNED BY: FPB

DRAWN BY: FPB

CHECKED BY: JMG

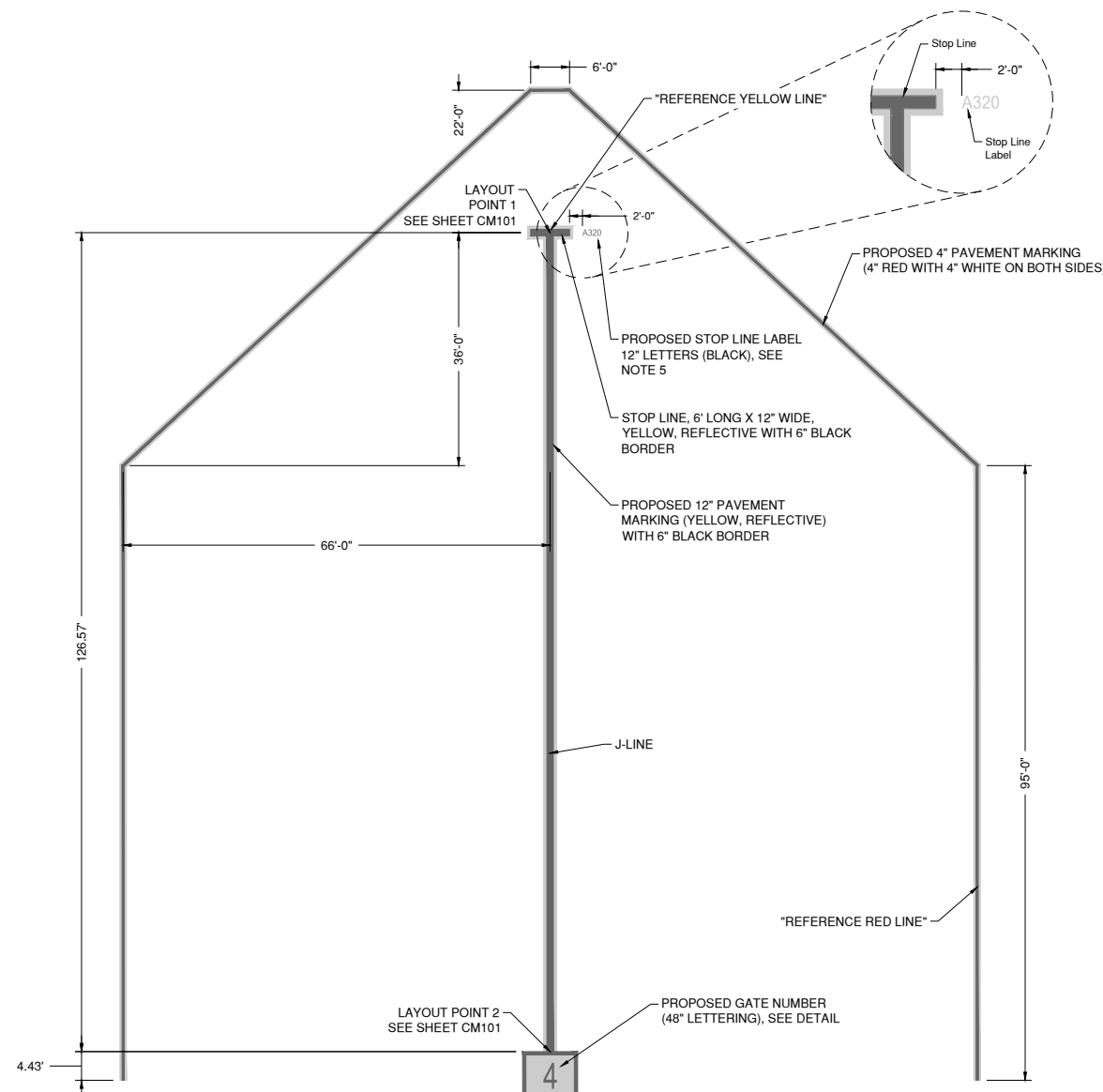
APPROVED BY: TAS

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SHEET TITLE

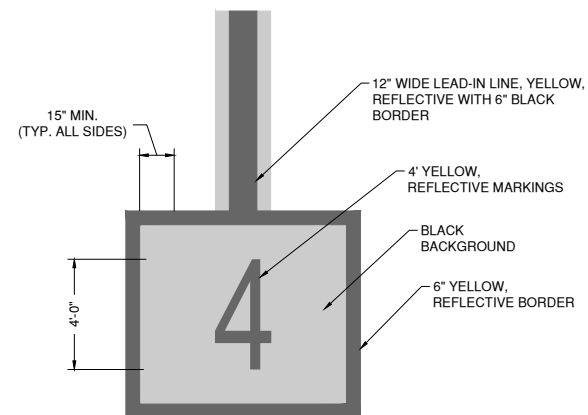
**APRON MARKING
DETAILS**

SHEET **41** OF **60**
CM501



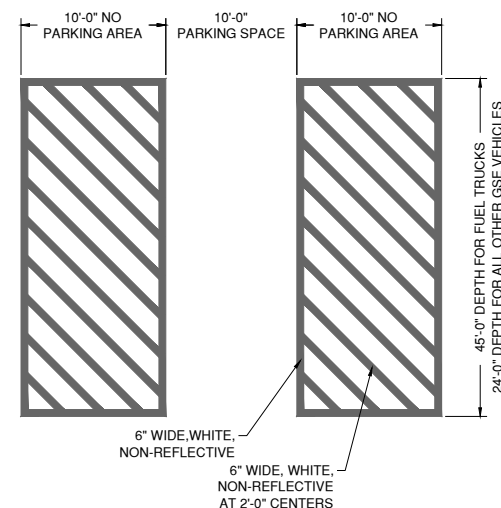
**GATE PARKING AND
SAFETY ZONE MARKING DETAIL**
NOT TO SCALE

- DETERMINE LOCATION NOSE WHEEL STOP LINE (REFERENCE YELLOW LINE).
- MEASURE AND MARK A POINT FROM FIRST NOSE WHEEL STOP LINE TO TOP OF SAFETY ZONE (22' FROM END OF LINE).
 - FROM THIS POINT, MEASURE AND MARK TWO POINTS PERPENDICULAR TO THE J-LINE (3')
- MEASURE AND NOTE THE POSITION FROM NOSE WHEEL STOP LINE BACK TOWARDS THE MAIN LANDING GEAR POSITION (36' FROM BEGINNING OF J-LINE).
 - FROM THIS NOTED POSITION, MEASURE AND MARK TWO POINTS, PERPENDICULAR TO THE J-LINE AT 66'.
 - FROM THESE TWO POINTS, MEASURE AND MARK TWO POINTS, PARALLEL TO THE J-LINE AT 95'.
- CONNECT POINTS (REFERENCE RED LINE).
- STOP LINE LABELS SHALL BE BLACK WITH 12" HIGH INSCRIPTION IN CONFORMANCE WITH THE DETAILS PROVIDED IN FAA ADVISORY CIRCULAR 150/5340-1M, APPENDIX A (OR LATEST REVISION). BEGIN THE FIRST CHARACTER 24-INCHES TO THE RIGHT OF THE END OF THE STOP LINE.



GATE NUMBERING DETAIL
NOT TO SCALE

- ALL MARKINGS 4" YELLOW AND REFLECTIVE ON BLACK BACKGROUND WITH 15" OFFSET FROM NUMERALS TO BORDER. BORDER IS 6" YELLOW AND REFLECTIVE.
- MARKINGS BASED ON VARIOUS GUIDANCE. ACRP REPORT RECOMMENDED YELLOW ON BLACK AND 2" LETTER HEIGHT. 4" LETTER HEIGHT BASED ON OTHER AIRPORT'S MARKINGS.



**GROUND SERVICE EQUIPMENT (GSE)
PARKING SPACE MARKING**
NOT TO SCALE

- ALL MARKINGS WHITE AND NON-REFLECTIVE.
- NUMBER OF PARKING SPACES SHALL BE AS NOTED ON PLANS OR AS DIRECTED BY THE ENGINEER OR OWNER.



TAXILANE CENTERLINE DETAIL
NOT TO SCALE
(ITEM AR620520 AND ITEM AR620525)

CONSULTANTS



0 50 100 Feet
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GONZALEZ COMPANIES, LLC
525 WEST MAIN STREET
SUITE 125
BELLEVILLE, IL 62220
PHONE: (618) 222-2221
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ILLINOIS PROFESSIONAL DESIGN FIRM 184.004564

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APRIL 21, 2023

TERMINAL APRON EXPANSION
PROJECT 1

OWNER



MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

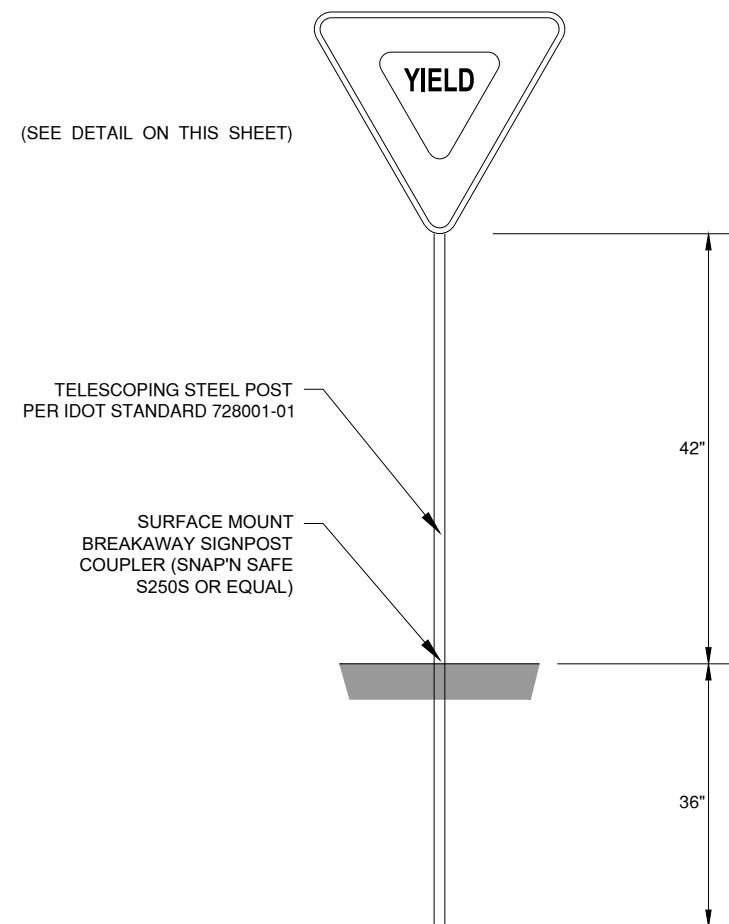
MARK	DATE	DESCRIPTION

BLV PROJECT NO. 2022-14
IL PROJECT NO. BLV-5087
CMT PROJECT NO: 22001186 GONZALEZ PROJECT NO: 22-1031
CAD DWG FILE: 22001186-CM500.DWG
DESIGNED BY: FPB
DRAWN BY: FPB
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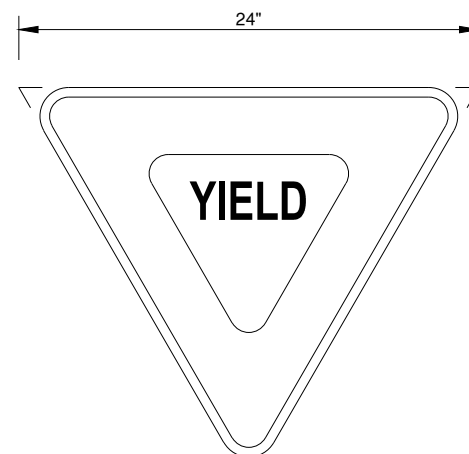
SHEET TITLE
**MARKING AND
SIGNAGE DETAILS**

SHEET **42** OF **60**
CM502

(SEE DETAIL ON THIS SHEET)



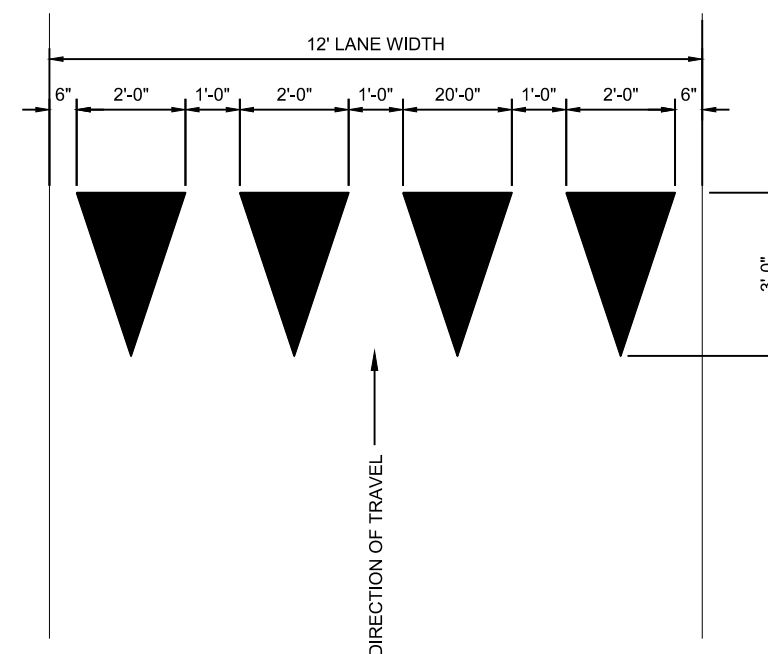
SERVICE ROAD YIELD SIGN DETAIL
N.T.S.



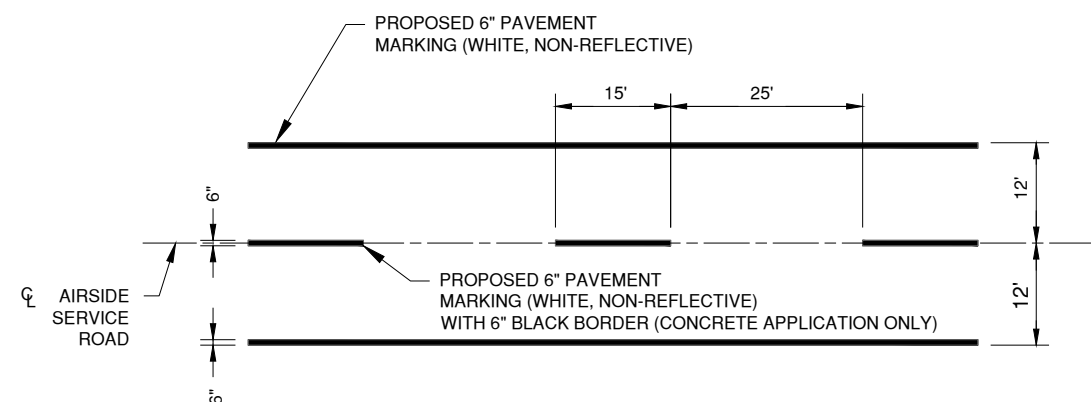
R1-2 YIELD SIGN
(24" ACROSS FLATS)
COLORS:
LEGEND - RED (RETROREFLECTIVE)
BACKGROUND - WHITE (RETROREFLECTIVE)

REF: MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD)
STANDARD HIGHWAY SIGNS 2009 (ENGLISH) EDITION

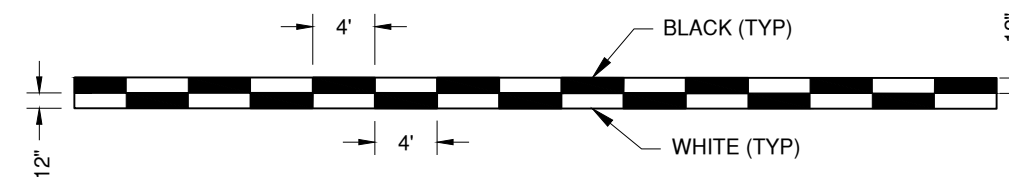
YIELD SIGN DETAIL
N.T.S.



YIELD LINE DETAIL
N.T.S.



VEHICULAR ROADWAY MARKING
N.T.S.



ROADWAY EDGE STRIPES, WHITE, ZIPPER STYLE
N.T.S.



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APRIL 21, 2023

TERMINAL APRON EXPANSION -
PROJECT 1



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ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

MARK | DATE | DESCRIPTION

BLV PROJECT NO. 2022-14
IL PROJECT NO. BLV-5087
CMT PROJECT NO: 22001186.00
CAD DWG FILE: 22001186 - EL500.DWG
DESIGNED BY: CMT
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SHEET TITLE

ELECTRICAL
DETAILS 6

EL506
SHEET 49 OF 60

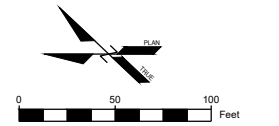
PANELBOARD SCHEDULE																	
PANEL DESIGNATION: PP-1				BOND NEUTRAL AND GROUND BAR: NO				POLE: 42									
LOCATION: NW OF TERMINAL BLDG				NEUTRAL BUS RATING: 100%				SHORT CIRCUIT RATING: 22KA									
MFR & TYPE:				SERVICE ENTRANCE RATED: NO				SERIES OR FULLY RATED: FULLY									
TVSS & DISCONNECT REQUIRED: NO																	
VOLTS: 120/240				MOUNTING: SURFACE				BUS RATING (AMPS): 225									
PHASE: 1				ENCL RATING: NEMA 3R				BUS: COPPER									
WIRE: 3				MAIN CIRCUIT BREAKER: 200/2													
CKT NO.	LOAD	BREAKER SIZE	LOAD AMPS	USAGE FACTOR	PHASE AMPS		POLE NO.	PHASE AMPS		USAGE FACTOR	LOAD AMPS	BREAKER SIZE	LOAD	CKT NO.			
1	240V RECEPTACLE OUTLET POST 1	30/2	12.5	0.5	6.25	B	1	6.25	0.5	12.5	30/2	240V RECEPTACLE OUTLET POST 2	2				
3	-	-	12.5	0.5	6.25	B	3	6.25	0.5	12.5	-	-	4				
5	120V RECEPTACLE OUTLET POST 1	20/1	15	0.5	7.5	A	5	7.5	0.5	15	20/1	120V RECEPTACLE OUTLET POST 2	6				
7	240V RECEPTACLE OUTLET POST 3	30/2	12.5	0.5	6.25	B	7	6.25	0.5	12.5	30/2	240V RECEPTACLE OUTLET POST 4	8				
9	-	-	12.5	0.5	6.25	B	9	6.25	0.5	12.5	-	-	10				
11	120V RECEPTACLE OUTLET POST 3	20/1	15	0.5	7.5	A	11	7.5	0.5	15	20/1	120V RECEPTACLE OUTLET POST 4	12				
13	240V RECEPTACLE OUTLET POST 5	30/2	12.5	0.5	6.25	B	13	6.25	0.5	12.5	30/2	APRON LIGHTPOLE RECEPTACLE	14				
15	-	-	12.5	0.5	6.25	B	15	6.25	0.5	12.5	20/1	APRON LIGHTPOLE RECEPTACLE	16				
17	120V RECEPTACLE OUTLET POST 5	20/1	15	0.5	7.5	A	17	7.5	0.5	15	20/1	-	18				
19	-	20/1	-	-	0	A	19	0	-	-	20/1	-	20				
21	-	20/1	-	-	0	A	21	0	-	-	20/1	-	22				
23	-	20/1	-	-	0	A	23	0	-	-	20/1	-	24				
25	-	20/1	-	-	0	A	25	0	-	-	20/1	-	26				
27	-	20/1	-	-	0	A	27	0	-	-	20/1	-	28				
29	-	20/1	-	-	0	A	29	0	-	-	20/1	-	30				
31	-	20/1	-	-	0	A	31	0	-	-	20/1	-	32				
33	-	20/1	-	-	0	A	33	0	-	-	20/1	-	34				
35	-	20/1	-	-	0	A	35	0	-	-	20/1	-	36				
37	-	20/1	-	-	0	A	37	0	-	-	20/1	-	38				
39	-	20/2	-	-	0	A	39	0	-	-	20/1	-	40				
SECTION TOTAL:					33.75	26.25		27.5	27.5								
MINIMUM MAIN CIRCUIT BREAKER AMPS: 90					PHASE TOTAL AMPS:				A B		TOTAL USAGE LOAD: 13800 VA						
									61.25 53.75		MIN. XFMR VA: 17250 VA						
									A B								
									7350 6450								

PANELBOARD SCHEDULE																	
PANEL DESIGNATION: DP-1				BOND NEUTRAL AND GROUND BAR: NO				POLE: 30									
LOCATION: NW OF TERMINAL BLDG				NEUTRAL BUS RATING: 100%				SHORT CIRCUIT RATING: 22KA									
MFR & TYPE:				SERVICE ENTRANCE RATED: NO				SERIES OR FULLY RATED: SERIES									
TVSS & DISCONNECT REQUIRED: YES																	
VOLTS: 480Y/277V				MOUNTING: SURFACE				BUS RATING (AMPS): 225									
PHASE: 3				ENCL RATING: NEMA 3R				BUS: COPPER									
WIRE: 4				MAIN CIRCUIT BREAKER: AMP/POLE 200/3													
CKT NO.	LOAD	BREAKER SIZE	LOAD AMPS	USAGE FACTOR	PHASE AMPS (USAGE)			POLE NO.	PHASE AMPS (USAGE)			USAGE FACTOR	LOAD AMPS	BREAKER SIZE	LOAD	CKT NO.	
1	120/240V XFMR/PANELBOARD	125/2	100	0.5	50	A	1	15	B	15	0	1	15	20/1	APRON FLOODLIGHT	2	
3	-	-	-	-	0	A	3	15	B	15	0	1	15	20/1	APRON FLOODLIGHT	4	
5	-	20/1	-	-	0	A	5	0	B	0	-	-	-	20/1	-	6	
7	-	20/1	-	-	0	A	7	0	B	0	-	-	-	20/1	-	8	
9	-	20/1	-	-	0	A	9	0	B	0	-	-	-	20/1	-	10	
11	-	20/1	-	-	0	A	11	0	B	0	-	-	-	20/1	-	12	
13	-	20/1	-	-	0	A	13	0	B	0	-	-	-	20/1	-	14	
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27	-	20/1	-	-	0	A	27	0	B	0	-	-	-	20/1	-	28	
29	-	20/1	-	-	0	A	29	0	B	0	-	-	-	20/1	-	30	
SECTION TOTAL:					50	0	0		15	15	0						
MINIMUM MAIN CIRCUIT BREAKER AMPS: 42					PHASE TOTAL AMPS:				A B C			TOTAL USAGE LOAD: 9600 VA					
									65 15 0			MIN. XFMR VA: 12000 VA					
									A B C								
									7800 1800 0								

Path: K:\MidAmerica\22001186-00_TerminalApron\Draw\Sheets\22001186 - EL500.dwg
Date: Friday, April 21, 2023 2:42:15 PM



License No.
CONSULTANTS



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BID ISSUE
APRIL 21, 2023

TERMINAL APRON EXPANSION -
PROJECT 1



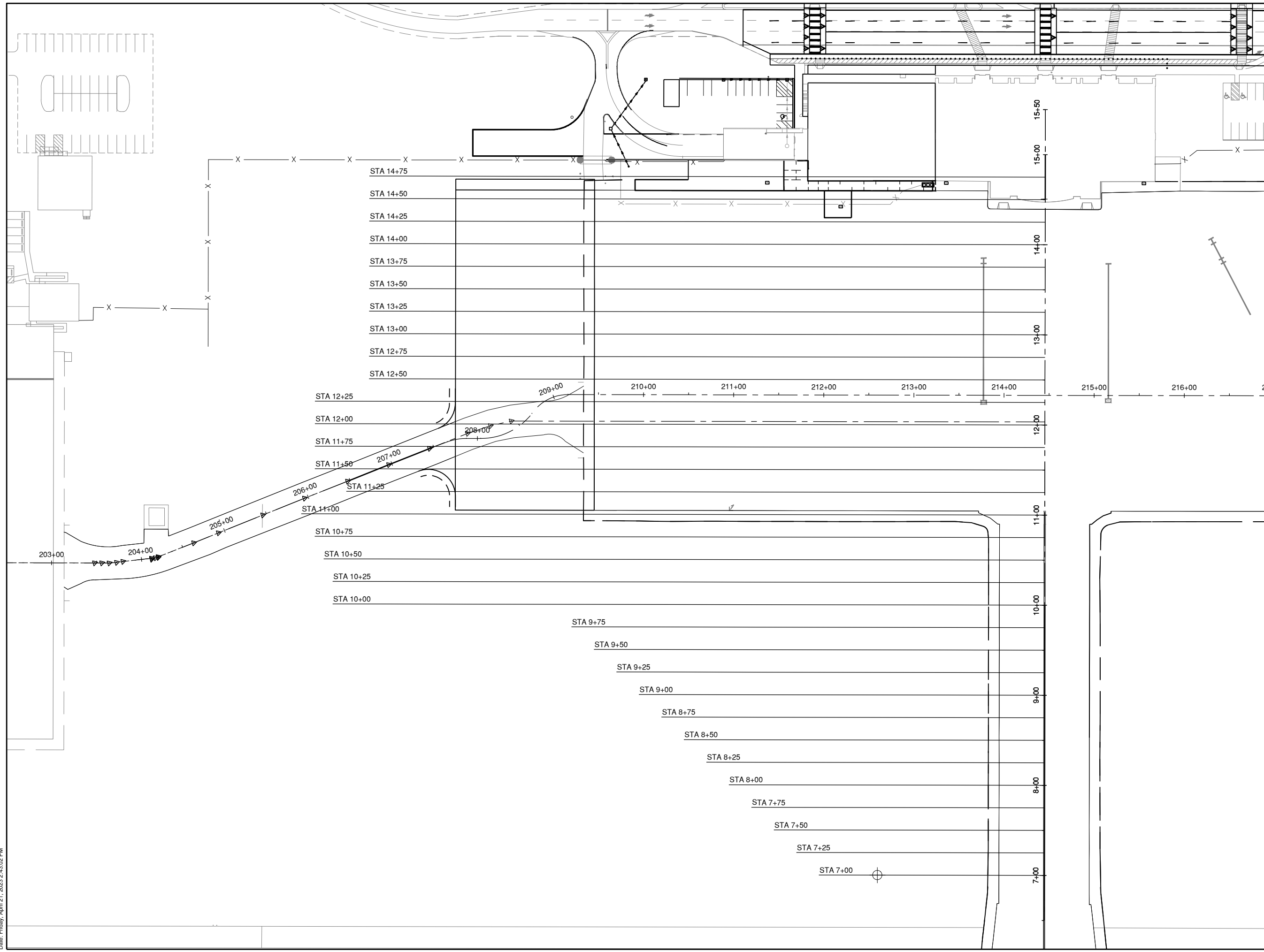
MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION
		BLV PROJECT NO. 2022-14
		IL PROJECT NO. BLV-5087
		CMT PROJECT NO: 22001186.00
		CAD DWG FILE: APRON XS SHEETS.DWG
		DESIGNED BY: CMT
		DRAWN BY: %%U
		CHECKED BY: CMT
		APPROVED BY: CMT
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SHEET TITLE

CROSS SECTION INDEX

CG700
SHEET 50 OF 60



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Date: Friday, April 21, 2023 2:43:02 PM



License No.

CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)

APRIL 21, 2023
BID ISSUE

TERMINAL APRON
EXPANSION - PROJECT 1



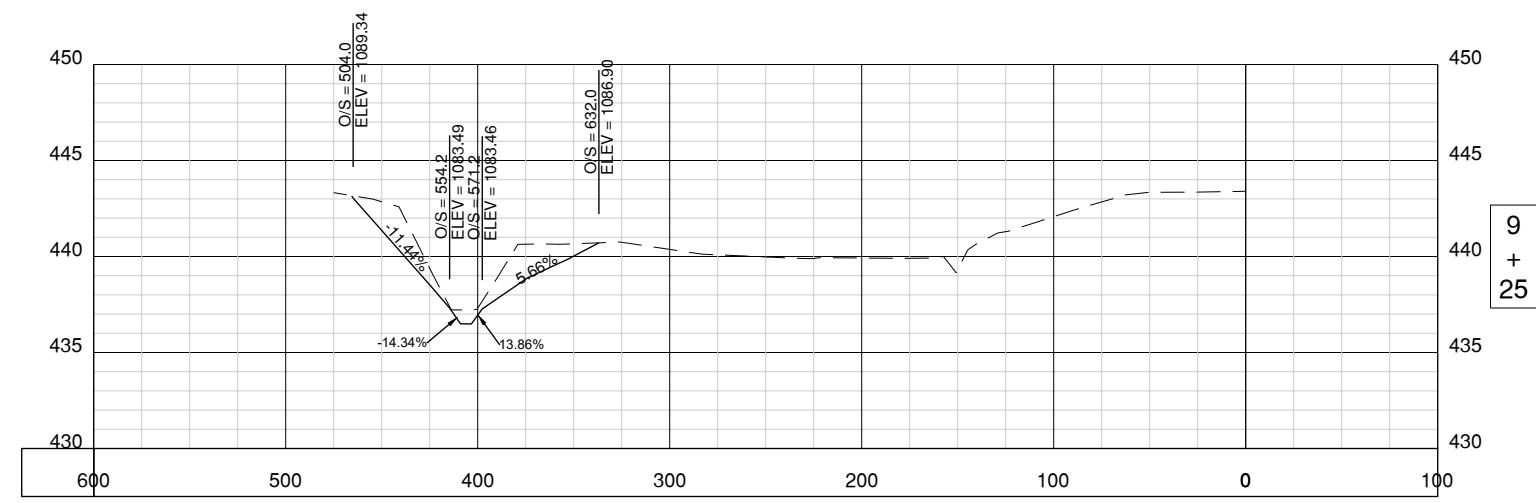
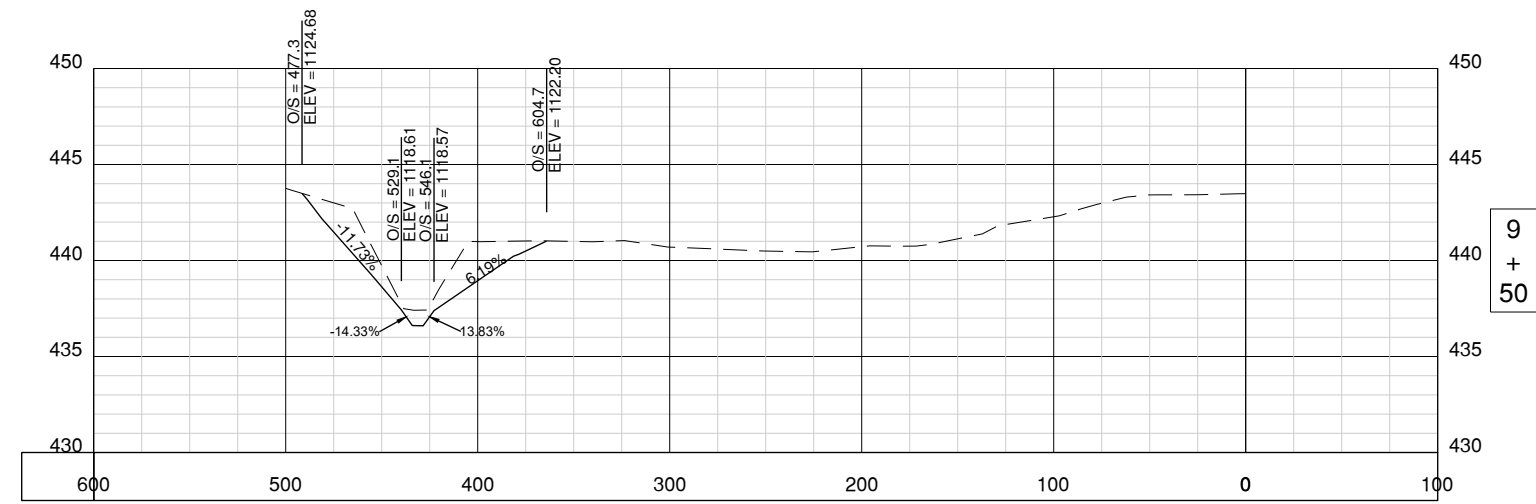
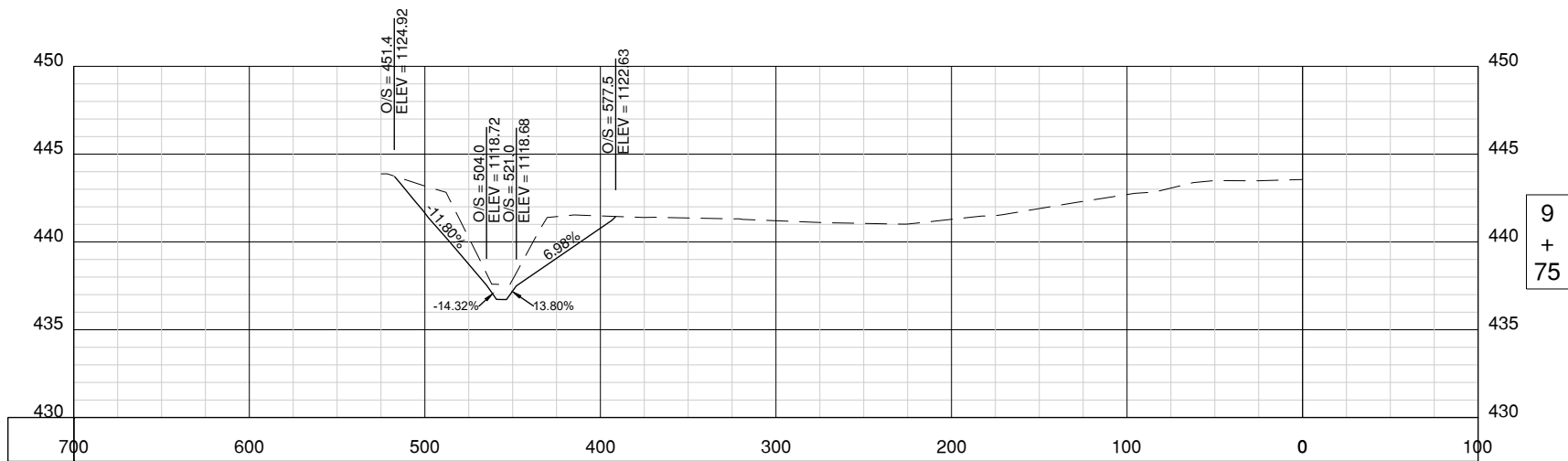
MIDAMERICA
ST. LOUIS AIRPORT
ST. CLAIR COUNTY, IL

MARK	DATE	DESCRIPTION

CMT PROJECT NO:22001186.00
 CAD DWG FILE: APRON XS SHEETS.DWG
 DESIGNED BY: DHC
 DRAWN BY: %%U
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SHEET TITLE
**APRON EXPANSION
 CROSS SECTIONS 3**

CG703
 SHEET 53 OF 60



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