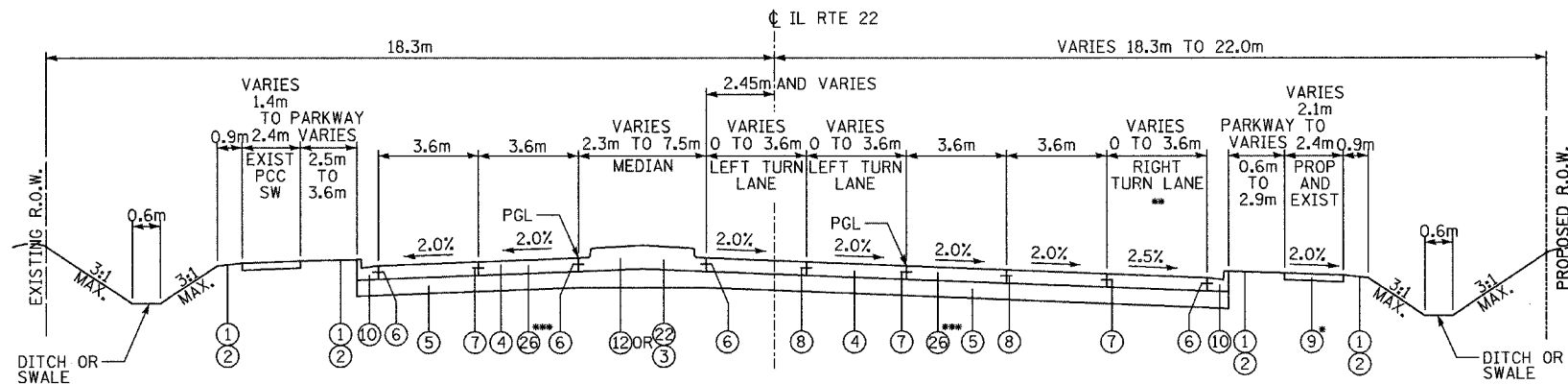


| | | | | |
|-------------------------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 337 | 20R-5 | LAKE | 562 | 19 |
| STA. 99+815.000 TO STA. 104+665.000 | | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| 0-91-552-99 | | | 60881 | |



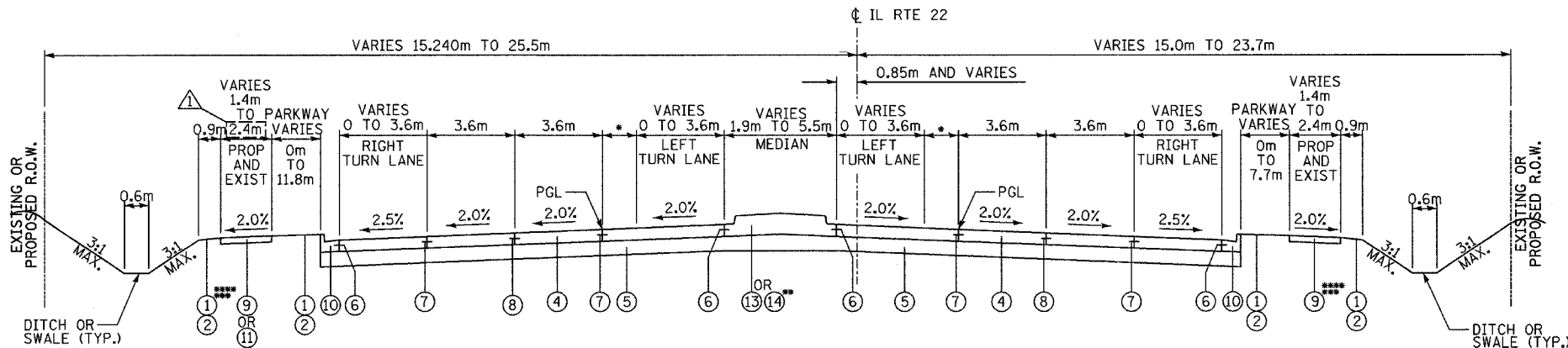
ILLINOIS ROUTE 22

* PROPOSED SIDEWALK
STA. 101+466.206 TO STA. 101+599.920 RT
EXISTING SIDEWALK TO REMAIN ALL OTHER AREAS

STA. 101+414.060 TO STA. 101+613.705

** RIGHT TURN LANE STA. 101+503.311 TO STA. 101+585.198

*** PAVEMENT FABRIC
STA. 101+400.000 TO STA. 101+585.000



ILLINOIS ROUTE 22

* PAINTED MEDIAN VARIES 0m TO 1.4m

*** PCC SIDEWALK, 125mm
STA. 101+128.726 TO STA. 101+244.000 LT
STA. 101+259.405 TO STA. 101+310.875 RT
STA. 102+852.140 TO STA. 103+030.189 RT & LT
STA. 103+086.182 TO STA. 103+212.212 RT & LT
STA. 103+212.212 TO STA. 102+275.477 RT
STA. 103+493.217 TO STA. 104+030.147 LT (BIT. SIDEPATH)
STA. 103+653.091 TO STA. 104+003.417 RT
STA. 104+026.582 TO STA. 104+102.000 RT (BIT. SIDEPATH)

STA. 100+273.045 TO STA. 100+415.388
STA. 100+608.157 TO STA. 100+749.980
STA. 101+128.726 TO STA. 101+414.060
STA. 102+321.945 TO STA. 102+464.469
STA. 102+550.000 TO STA. 102+703.306
STA. 102+735.576 TO STA. 103+030.189
STA. 103+086.182 TO STA. 104+003.147
STA. 104+026.582 TO STA. 104+102.000

*** EXISTING PCC SIDEWALK
STA. 101+259.000 TO STA. 101+414.060 LT
STA. 101+310.875 TO STA. 101+414.060 RT
STA. 102+321.945 TO STA. 102+464.467 LT & RT
STA. 102+550.000 TO STA. 102+703.306 RT
STA. 102+735.576 TO STA. 102+852.140 RT
STA. 103+212.212 TO STA. 103+493.217 LT
STA. 103+275.477 TO STA. 103+653.091 RT

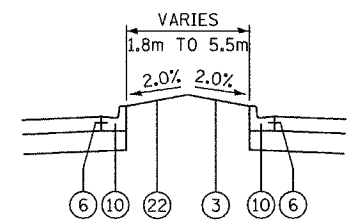
LEGEND

- TOPSOIL FURNISH AND PLACE, 100mm
- SODDING, SALT TOLERANT
- SEEDING (TYPE AS SPECIFIED IN PLANS)
- PORTLAND CEMENT CONCRETE PAVEMENT 250mm (JOINTED)
- AGGREGATE SUBGRADE, 300mm
- CONSTRUCTION JOINT WITH NO. 20 DEFORMED TIE BAR (EPOXY COATED), 600mm LONG @ 600mm CENTERS, DRILL & GROUT IN PLACE (INCLUDED IN COST OF CURB & GUTTER)
- CONSTRUCTION JOINT WITH NO. 25 DEFORMED TIE BAR (EPOXY COATED), 600mm LONG @ 600mm CENTERS, DRILL & GROUT IN PLACE (INCLUDED IN COST OF PCC PAVEMENT)
- SAWED LONGITUDINAL JOINT WITH NO. 20 DEFORMED TIE BAR (EPOXY COATED), 750mm LONG @ 750mm CENTERS (INCLUDED IN COST OF PCC PAVEMENT)
- PORTLAND CEMENT CONCRETE SIDEWALK 125mm
- COMBINATION CONCRETE CURB AND GUTTER, TYPE B-15.60
- BITUMINOUS SIDEPATH (SEE DETAIL)
- CONCRETE MEDIAN, TYPE SB-15.60
- CONCRETE MEDIAN, TYPE SB (SPECIAL)
- CORRUGATED MEDIAN
- BITUMINOUS SHOULDERS SUPERPAVE, 250mm
- COMBINATION CONCRETE CURB AND GUTTER, TYPE B15.30
- BITUMINOUS BASE COURSE, SUPERPAVE 250mm
- BITUMINOUS SURFACE REMOVAL
- BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N70, 38mm
- BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX D, N70, 38mm
- AGGREGATE SHOULDERS, TYPE B 250mm
- TOPSOIL, FURNISH AND PLACE, 600mm
- BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50, 38mm
- BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N70, 100mm
- PORTLAND CEMENT CONCRETE SHOULDERS, 250mm
- PAVEMENT FABRIC
- COMBINATION CONCRETE CURB AND GUTTER, TYPE M5.30
- INCIDENTAL BITUMINOUS SURFACING - 50mm
- BITUMINOUS CONCRETE BASE COURSE WIDENING, SUPERPAVE, 250mm
- BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50, 50mm
- POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX F, N90, 50mm

| | |
|--------------------------------------|---------|
| STA. 100+550.000 TO STA. 101+050.000 | - 150mm |
| STA. 101+500.000 TO STA. 101+640.000 | - 300mm |
| STA. 102+600.000 TO STA. 102+700.000 | - 300mm |
| STA. 103+370.000 TO STA. 103+650.000 | - 150mm |
| STA. 103+650.000 TO STA. 103+750.000 | - 300mm |

ILLINOIS ROUTE 22

| | |
|----------------------------------|---------------------|
| STRUCTURAL DESIGN TRAFFIC: | YEAR 2014 |
| PV = 31,188 | SU = 930 MU = 1,096 |
| ROAD/STREET CLASSIFICATION: | CLASS 1 |
| P = 32% | S = 45% M = 45% |
| TRAFFIC FACTOR: ACTUAL TF = 8.10 | AC TYPE = XX |
| | MINIMUM TF = 6.03 |



LANDSCAPE MEDIAN DETAIL

- LANDSCAPE MEDIAN WHEN THE MEDIAN WIDTH IS 1.8m OR GREATER.
- CONCRETE MEDIAN OF TYPE SPECIFIED WHEN THE MEDIAN WIDTH IS LESS THAN 1.8m

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION IL RTE 22 (FAP 337) IL RTE 83 TO US 45 / IL 21 (MILWAUKEE AVE) PROPOSED TYPICAL SECTIONS ILLINOIS RTE 22 |
|-----------|------|----------------------------------------------------------------------------------------------------------------------------------------------------------------|
| NAME | DATE | |
| | | SCALE: NONE DRAWN BY: JSM |
| | | DATE: 03-22-2004 CHECKED BY: JNR |
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| WESTBOUND SUPERELEVATION LIMITS |
|----------------------------------------------------|
| TRANSITION STA. 101+638.171 TO STA. 101+704.171 |
| 3.8% SE RIGHT STA. 101+704.171 TO STA. 101+829.160 |
| TRANSITION STA. 101+829.160 TO STA. 101+895.160 |
| NORMAL CROWN STA. 101+895.160 TO STA. 101+924.600 |
| TRANSITION STA. 101+924.600 TO STA. 101+964.599 |
| 3.6% SE LEFT STA. 101+964.599 TO STA. 102+100.740 |
| TRANSITION STA. 102+100.740 TO STA. 102+140.740 |
| NORMAL CROWN STA. 102+140.740 TO STA. 104+101.921 |
| TRANSITION STA. 104+101.921 TO STA. 104+167.921 |
| 2.2% SE RIGHT STA. 104+167.867 TO STA. 104+474.910 |
| TRANSITION STA. 104+474.910 TO STA. 104+540.108 |

| EASTBOUND SUPERELEVATION LIMITS |
|----------------------------------------------------|
| TRANSITION STA. 101+664.171 TO STA. 101+704.171 |
| 3.8% SE RIGHT STA. 101+704.171 TO STA. 101+829.160 |
| TRANSITION STA. 101+829.160 TO STA. 101+869.160 |
| NORMAL CROWN STA. 101+869.160 TO STA. 101+898.600 |
| TRANSITION STA. 101+898.600 TO STA. 101+964.599 |
| 3.6% SE LEFT STA. 101+964.599 TO STA. 102+100.740 |
| TRANSITION STA. 102+100.740 TO STA. 102+166.740 |
| NORMAL CROWN STA. 102+166.740 TO STA. 104+154.593 |
| TRANSITION STA. 104+154.593 TO STA. 104+167.921 |
| 2.2% SE RIGHT STA. 104+167.921 TO STA. 104+474.910 |
| TRANSITION STA. 104+474.910 TO STA. 104+540.108 |