

| FAP | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----|------------|--------|--------------|-----------|
| 326 | 106 S-RS-2 | KANE | 18 | 1 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

**F.A.P. ROUTE: 326/ILLINOIS ROUTE 47
SECTION: 106 S-RS-2
US 20 TO MANNING ROAD
RESURFACING (MAINTENANCE)
KANE COUNTY
C-91-250-04**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

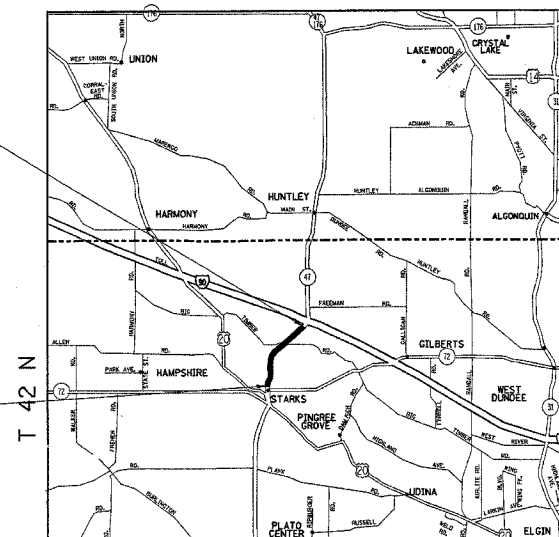
THE IMPROVEMENT IS LOCATED IN THE VILLAGE OF HAMPSHIRE



R 7 E

IMPROVEMENT BEGINS
STA. 827+78

IMPROVEMENT ENDS
STA. 956+68



RUTLAND TOWNSHIP



TRAFFIC DATA

2003 ADT = 9,700
POSTED SPEED LIMIT = 50 MPH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

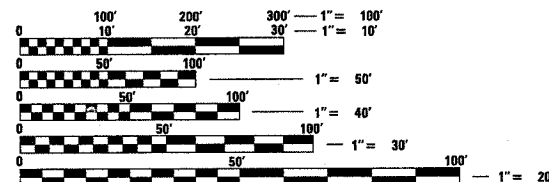
SUBMITTED April 18, 2005

Dina Okl
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 13, 2005
Mike Wine
ENGINEER OF DESIGN AND ENVIRONMENT

May 13, 2005
Victor Modesto
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

GROSS LENGTH OF IMPROVEMENT = 12,890 LINEAL FEET = 2.441 MILES
NET LENGTH OF IMPROVEMENT = 12,890 LINEAL FEET = 2.441 MILES

CONTRACT NO. 62782

DISTRICT ONE DESIGN PLAN PREPARATION ENGINEER:
KEN ENG/RAJENDRA SHAH (847)705-4437

*REF-
*REF-
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 326 | 106 S-RS-2 | KANE | 18 | 2 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

CONTRACT #62782

INDEX OF SHEETS

| SHEET NO. | DESCRIPTION |
|-----------|---|
| 1 | COVER SHEET |
| 2 | INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES |
| 3 | SUMMARY OF QUANTITIES |
| 4 | TYPICAL CROSS SECTIONS |
| 5 - 9 | ROADWAY AND PAVEMENT MARKING PLANS |
| 10 | PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT |
| 11 | BUTT JOINT AND BITUMINOUS TAPER DETAILS |
| 12 | DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER STABILIZATION AT TBT TY 1 SPL |
| 13 | METHOD OF FAGGING |
| 14 | TEMPORARY INFORMATION SIGNING |
| 15 | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS |
| 16 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) |
| 17 | DISTRICT ONE TYPICAL PAVEMENT MARKINGS |
| 18 | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING |

LIST OF STATE STANDARDS

| | |
|-----------|---|
| 000001-04 | STANDARD SYMBOLS ABBREVIATIONS AND PATTERNS |
| 442201-01 | CLASS C AND D PATCHES |
| 630301-03 | SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS |
| 635006-02 | REFLECTOR AND TERMINAL MARKER PLACEMENT |
| 701006-02 | OFF-ROAD OPERATIONS, MULTILANE, LESS THAN 4.5M (15') AWAY, FOR SPEEDS >= 45 MPH |
| 701306-01 | LANE CLOSURE 2L, 2W SLOW MOVING OPERATIONS-DAY ONLY FOR SPEEDS >= 45 MPH |
| 701311-02 | LANE CLOSURE 2L, 2W MOVING DAY ONLY OPERATIONS |
| 701336-04 | LANE CLOSURE 2L, 2W WORK AREA IN SERIES, FOR SPEEDS >=45 MPH |
| 702001-05 | TRAFFIC CONTROL DEVICES |
| 780001-01 | TYPICAL PAVEMENT MARKINGS |

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTORS SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE VILLAGE OF HAMPSHIRE.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- _____
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS OVER 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H)
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT DEBBIE HANLON, AREA TRAFFIC FIELD ENGINEER, AT (847)705-4413. A MINIMUM OF 72 HOURS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS
- TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT MODIFY OR REMOVE LANE CLOSURES OF CHANNELIZATION SHOW IN THE PLANS. THE CONTRACTOR SHALL MAKE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND WITHIN 30 MINUTES FROM THE TIME OF NOTIFICATION BY THE ENGINEER TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION
- PAVEMENT PATCHING SHALL BE COMPLETED PRIOR TO BITUMINOUS SURFACE REMOVAL OPERATIONS

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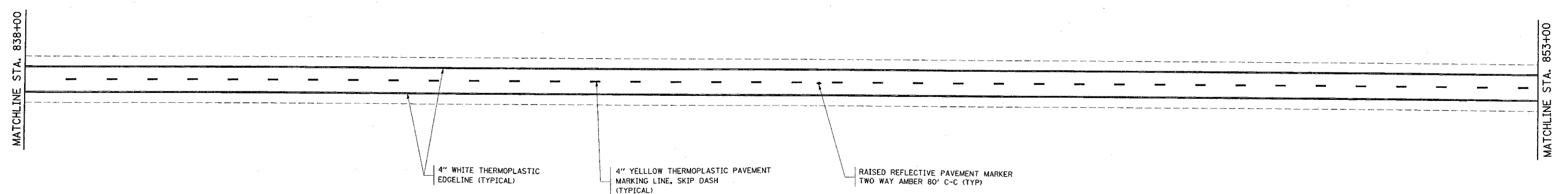
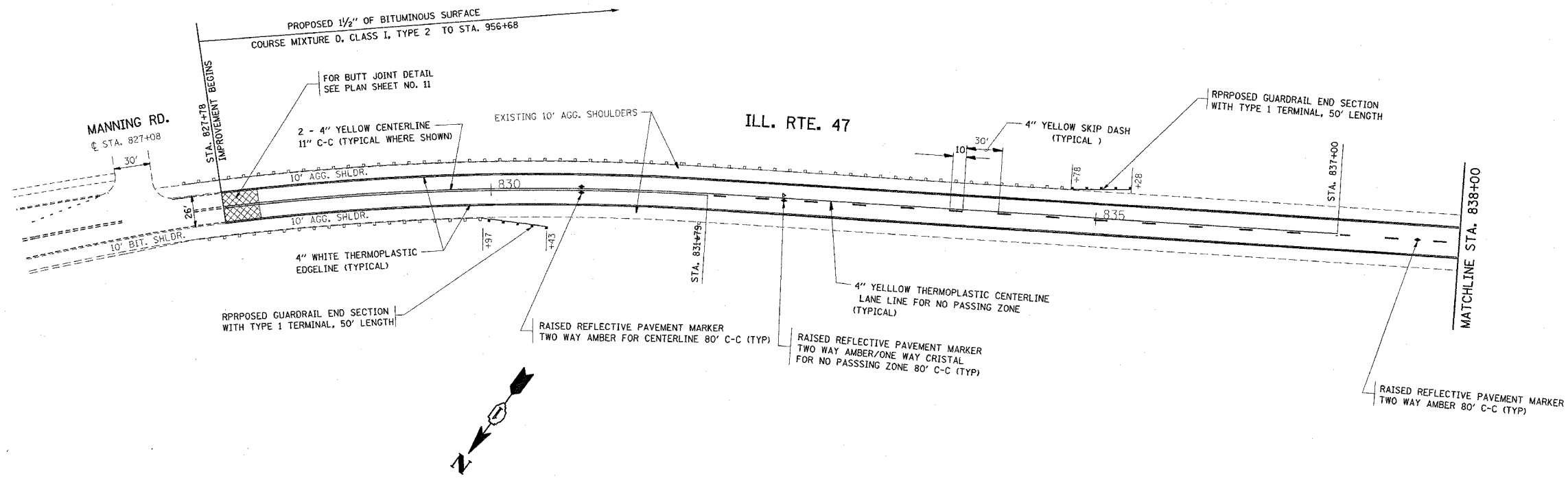
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.P. ROUTE 326
 ILLINOIS ROUTE 47
 INDEX OF SHEETS
 LIST OF STATE STANDARDS
 PLAN NOTES

SCALE: VERT.
 HORIZ.
 DATE 4/5/2005

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| F. A. P. EEL. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 326 | 106S-RS-2 | KANE | 18 | 5 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

CONTRACT NO. 62782



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ILLINOIS DEPARTMENT OF TRANSPORTATION

**ILL. RTE. 47
ROADWAY AND PAVEMEN
MARKING PLAN**

SCALE: VERT. 1"=50'
HORIZ. 1"=50'

DATE 4/5/2005

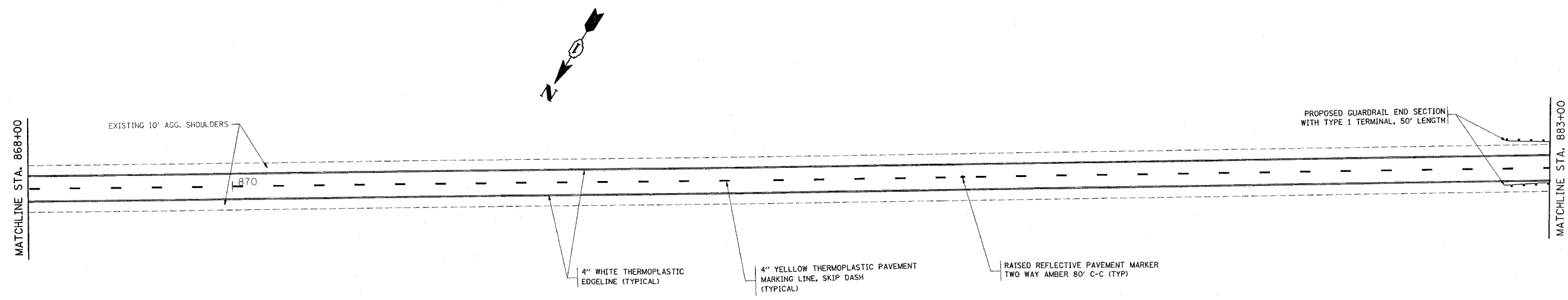
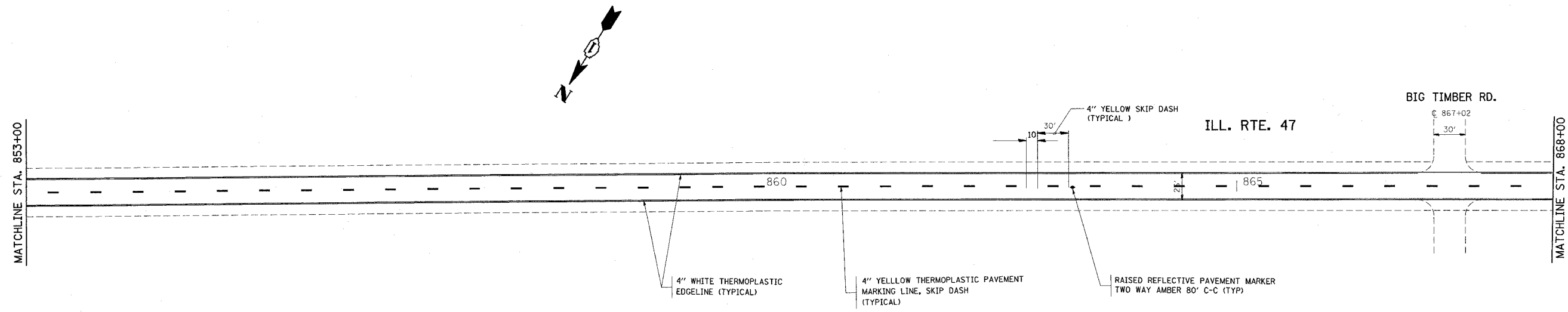
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CONTRACT NO. 62782



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ILLINOIS DEPARTMENT OF TRANSPORTATION

**ILL. RTE. 47
ROADWAY AND PAVEMENT
MARKING PLAN**

SCALE: VERT. 1"=50'
HORIZ. 1"=50'

DATE 4/5/2005

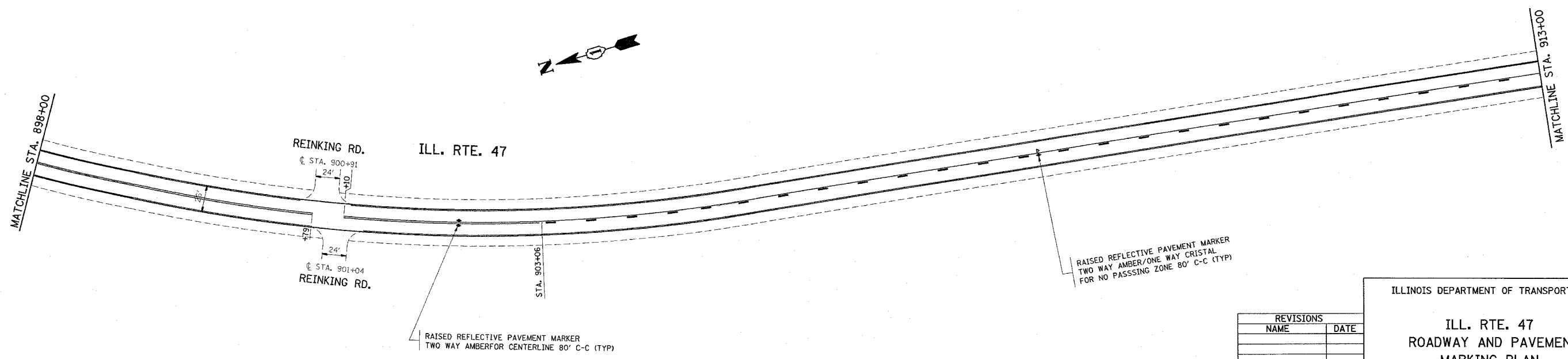
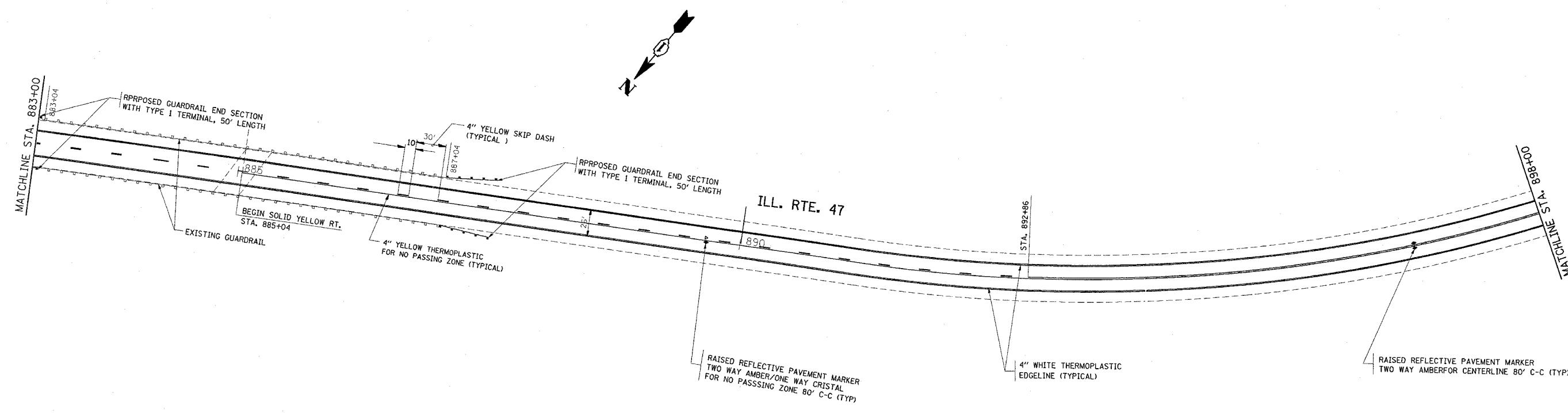
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CONTRACT NO. 62782



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ILLINOIS DEPARTMENT OF TRANSPORTATION

**ILL. RTE. 47
ROADWAY AND PAVEMENT
MARKING PLAN**

SCALE: VERT. 1"=50'
HORIZ. 1"=50'
DATE 4/5/2005

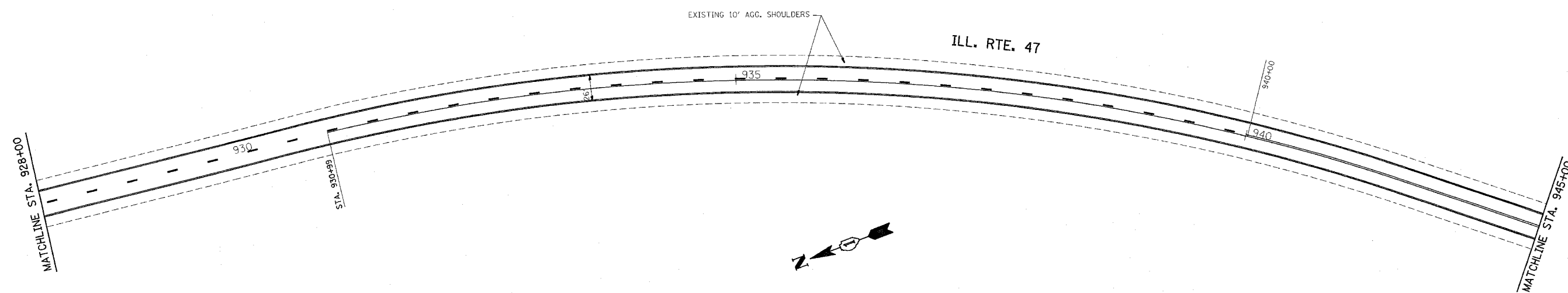
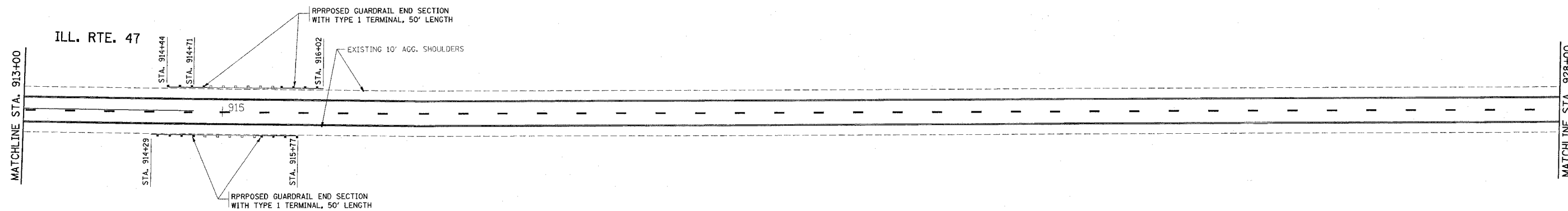
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ILLINOIS DEPARTMENT OF TRANSPORTATION

**ILL. RTE. 47
ROADWAY AND PAVEMENT
MARKING PLAN**

SCALE: VERT. 1"=50'
HORIZ. 1"=50'

DATE 4/5/2005

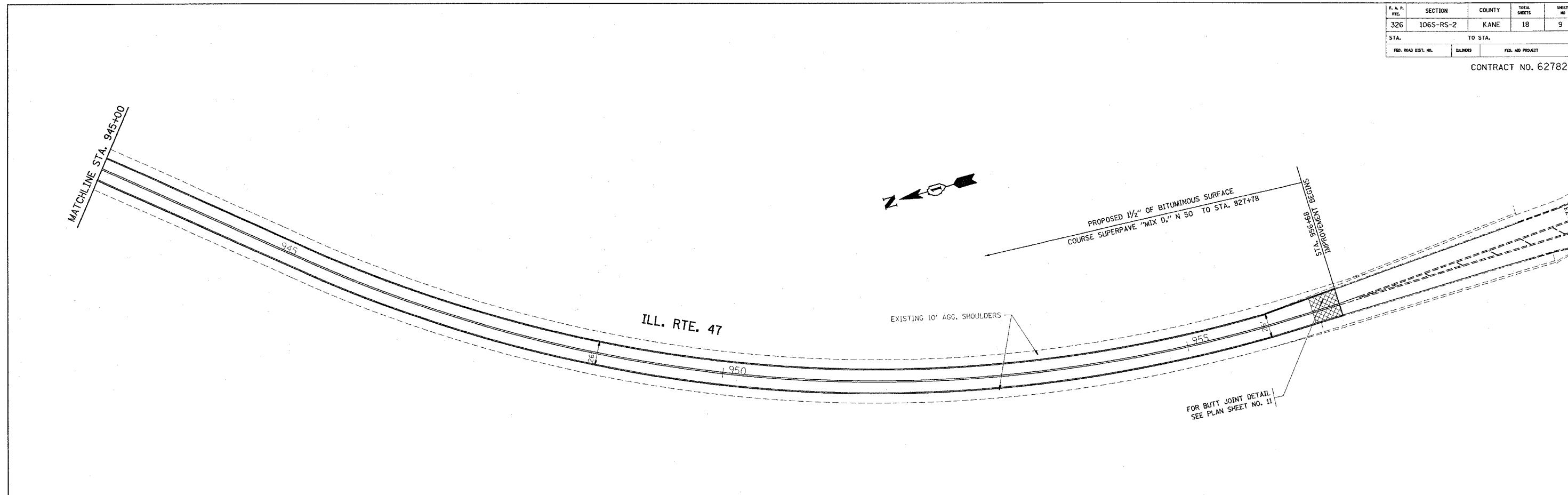
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| F. A. P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | BLINKS | FED. AID PROJECT | | |

CONTRACT NO. 62782



FOR BUTT JOINT DETAIL
SEE PLAN SHEET NO. 11

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ILLINOIS DEPARTMENT OF TRANSPORTATION

**ILL. RTE. 47
ROADWAY AND PAVEMEN
MARKING PLAN**

SCALE: VERT. 1"=50'
HORIZ. 1"=50'
DATE 4/5/2005

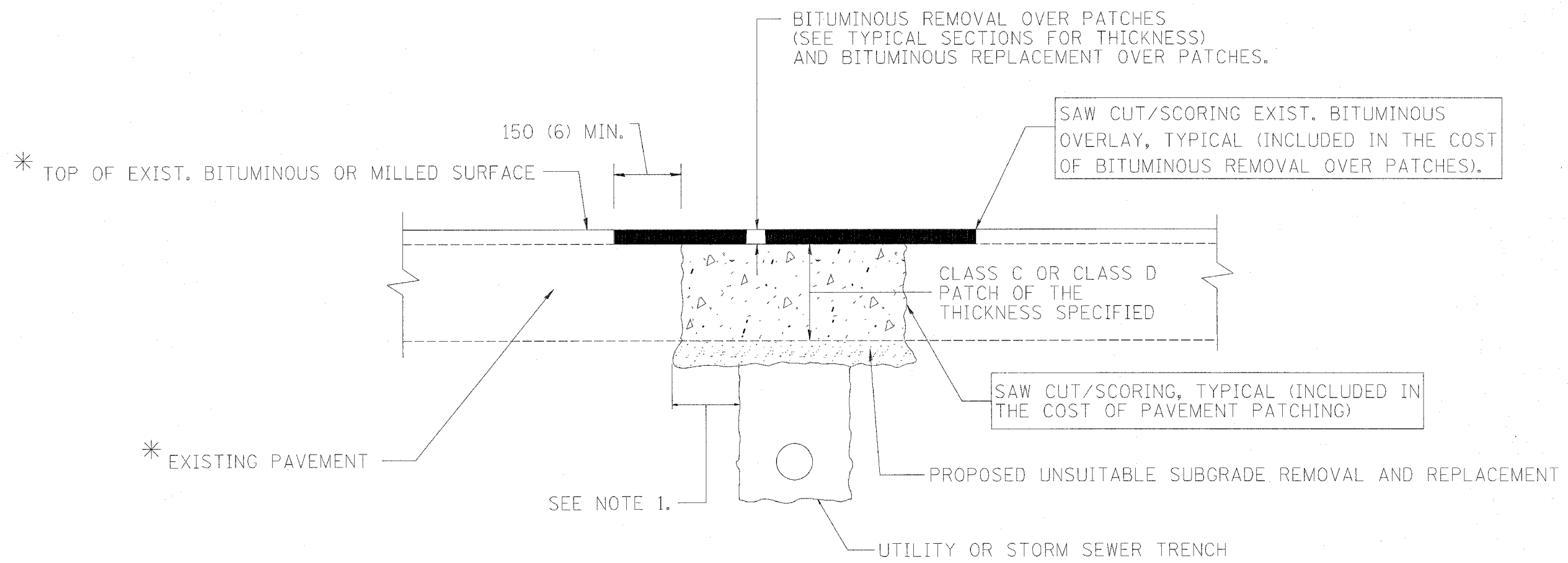
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| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 300 (12) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE SPECIAL PROVISION "PATCHING WITH BITUMINOUS OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE BITUMINOUS MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

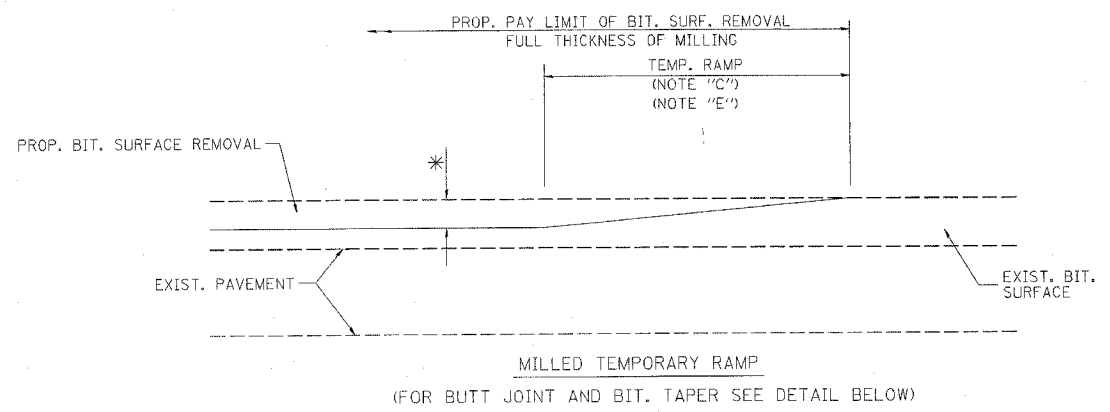
| REVISIONS | | REVISIONS | |
|-----------|----------|-----------|----------|
| NAME | DATE | NAME | DATE |
| R. SHAH | 10/25/94 | ART ABBAS | 04/27/98 |
| R. SHAH | 01/14/95 | | |
| R. SHAH | 03/23/95 | | |
| R. SHAH | 04/24/95 | | |
| A. HOUSEH | 03/15/96 | | |
| A. ABBAS | 03/21/97 | | |
| A. ABBAS | 01/20/98 | | |

PAVEMENT PATCHING FOR BITUMINOUS SURFACED PAVEMENT

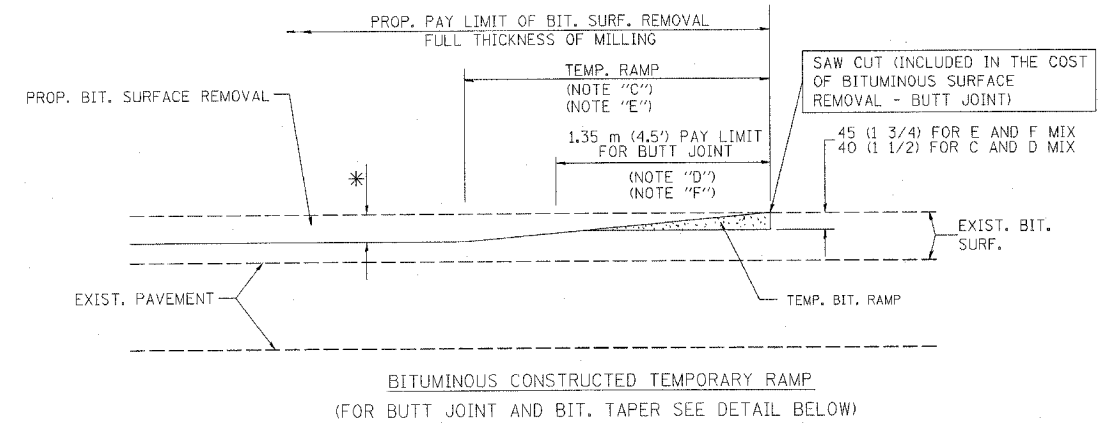
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 DATE: 10/18/2002
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 REVISION DATE: 04/27/98

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| F.A. SITE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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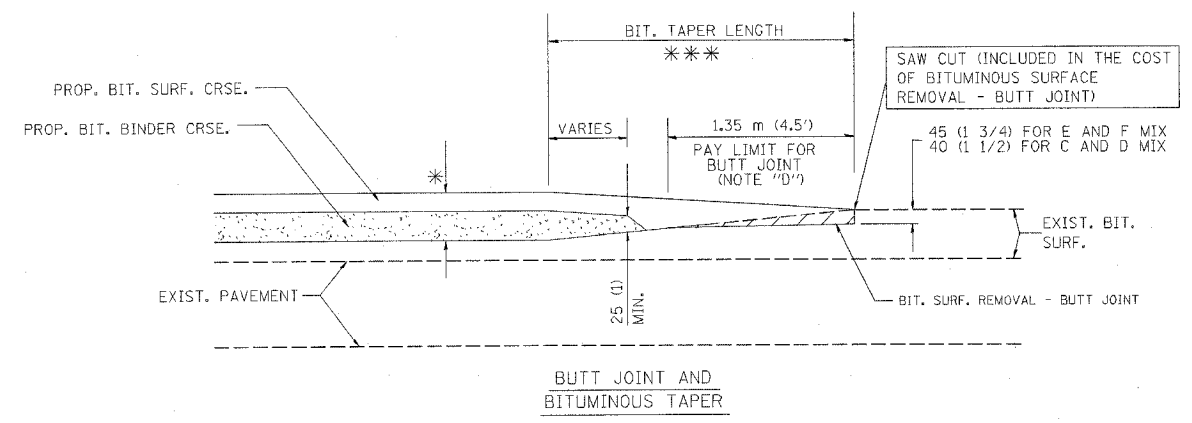


OPTION 1

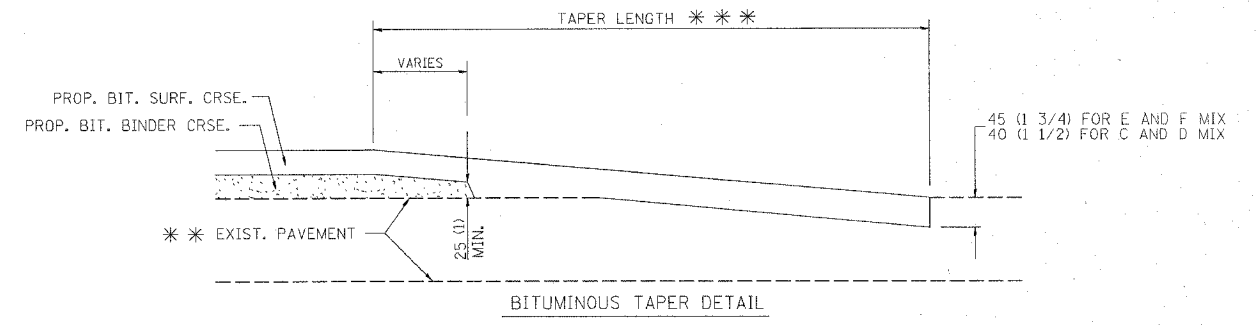
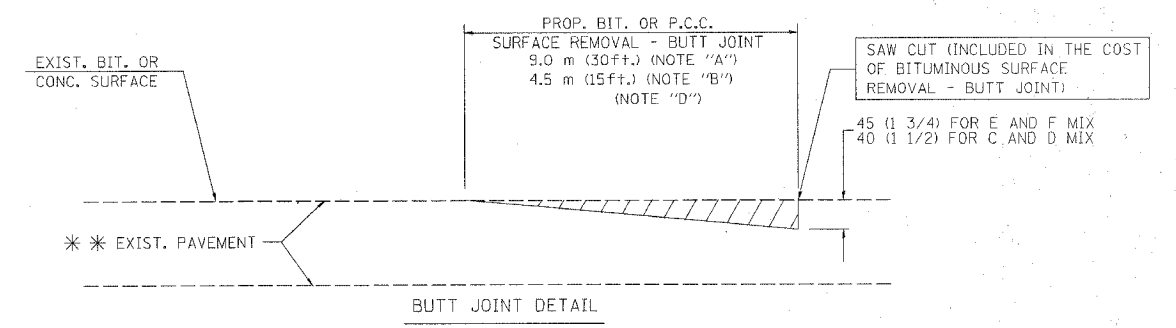


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR RESURFACING ONLY

*** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 ft.) PER INCH OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
 - G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND BITUMINOUS TAPER DETAILS

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

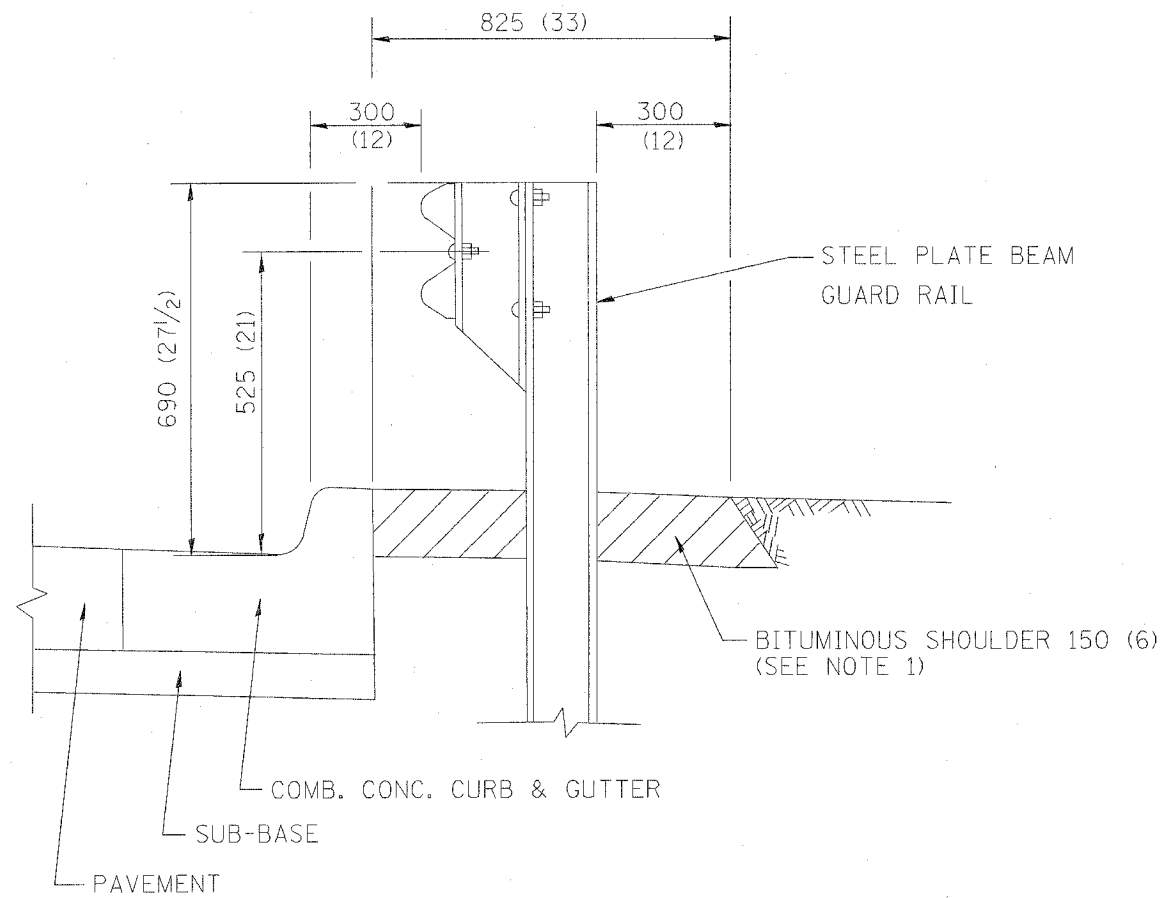
| REVISIONS | |
|------------|----------|
| NAME | DATE |
| M. DE YONG | 6-13-90 |
| M. DE YONG | 7-3-90 |
| M. DE YONG | 3-27-92 |
| R. SHAH | 09/09/94 |
| R. SHAH | 10/25/94 |
| A. ABBAS | 03/21/97 |
| M. GOMEZ | 04/06/01 |

SCALE: NONE
DATE PLOTTED: 10/18/2002

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REVISION DATE: 04/06

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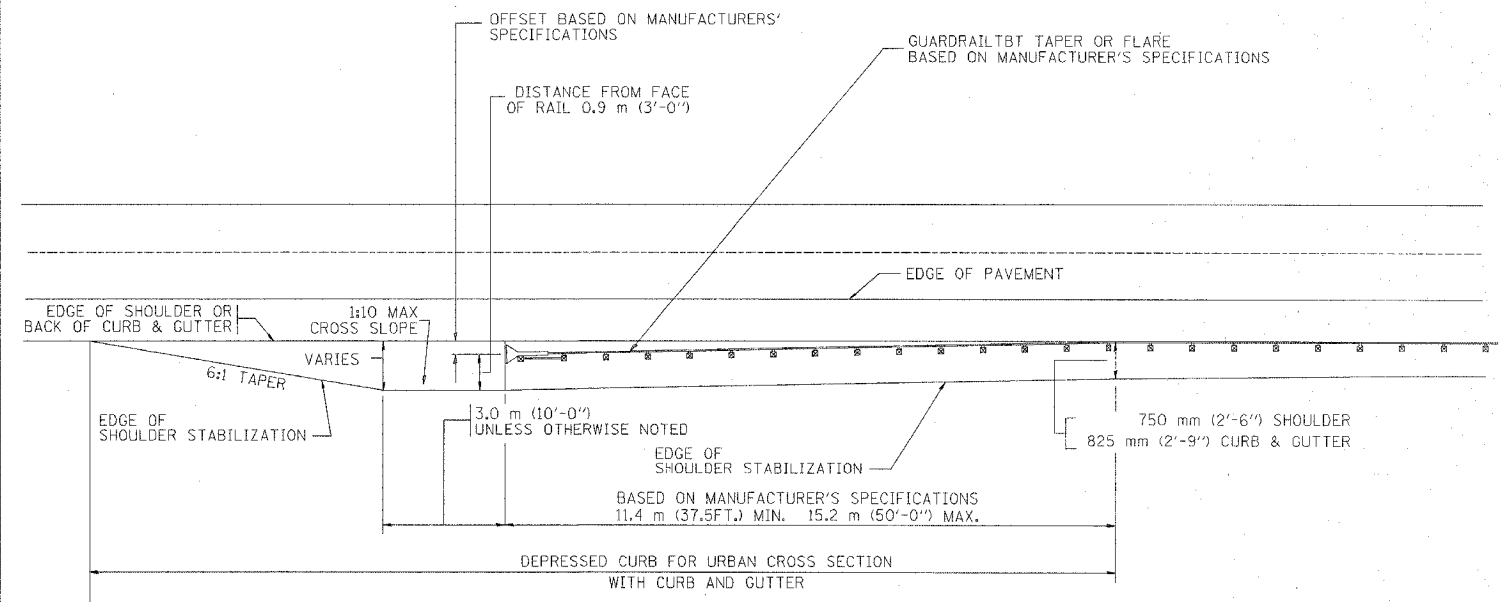


- NOTES: 1. THE BITUMINOUS SHOULDER SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL
2. GUARD RAIL MAY BE PLACED AT THE BACK OF CURB WHEN DIRECTED BY THE ENGINEER.

BASIS OF PAYMENT: BITUMINOUS SHOULDER 150 (6) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER m² (sq. yd.) AS "BITUMINOUS SHOULDER 150 (6)."

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER
 [FOR ROADWAY SPEED 60 kmh (35 MPH) TO 70 kmh (45 MPH)]



STABILIZATION AT TBT TY. 1 SPL.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER

STABILIZATION AT TBT TY 1 SPL.

| REVISIONS | |
|------------|----------|
| NAME | DATE |
| M. DE YONG | 09-22-90 |
| M. DE YONG | 07-14-92 |
| R. SHAH | 09/09/94 |
| R. SHAH | 10/25/94 |
| R. SHAH | 02/23/95 |
| A. ABBAS | 03/21/97 |
| E. GOMEZ | 08/28/00 |

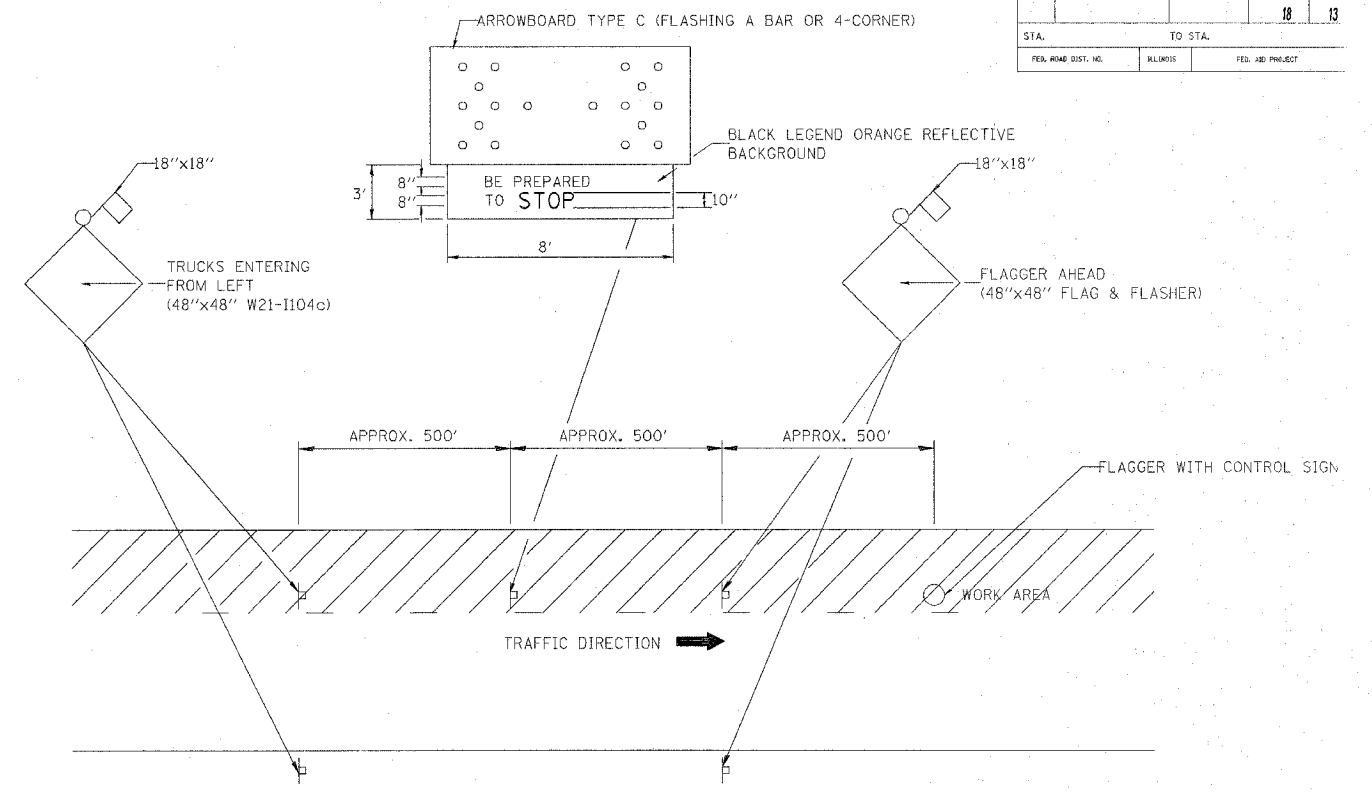
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BD600-10 (BD 34)
 REVISION DATE: 08/28/00

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| STA. TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. JOB PROJECT | |



METHOD OF FLAGGING

NOTE:

1. SIGNS SHALL BE MOUNTED AT A MINIMUM CLEARANCE HEIGHT OF 5 FEET
2. ALL SIGNS SHALL BE REMOVED WHEN THE FLAGGING OPERATION CEASES.
3. THIS CASE ALSO APPLIES WHEN THE WORK ZONE IS ON THE RIGHT. UNDER THESE CONDITIONS "TRUCKS ENTERING FROM RIGHT" SIGNS SHALL BE SUBSTITUTED FOR "TRUCKS ENTERING FROM LEFT" SIGNS. ALSO THE ARROWBOARD AND "BE PREPARED TO STOP" SIGNS SHALL BE RELOCATED TO THE RIGHT SIDE OF THE ROAD.
4. WORK ZONE ACCESS POINTS SHOULD BE A MINIMUM OF ONE HALF MILE APART. MEDIAN WORK ZONE ACCESS POINTS SHOULD NOT BE LOCATED OPPOSITE OF EACH OTHER.
5. NIGHTTIME FLAGGING OPERATIONS: THE FLAG STATION SHALL BE LIGHTED WITH ADDITIONAL LIGHTS OTHER THAN STREET LIGHTS. THE FLAGGER CONTROL SIGN AND THE FLAGGER'S VEST SHALL BE REFLECTORIZED. IN ADDITION, THE FLAGGER SHALL HAVE A FLASHLIGHT OR LIGHTED WAND.

ILLINOIS DEPARTMENT OF TRANSPORTATION

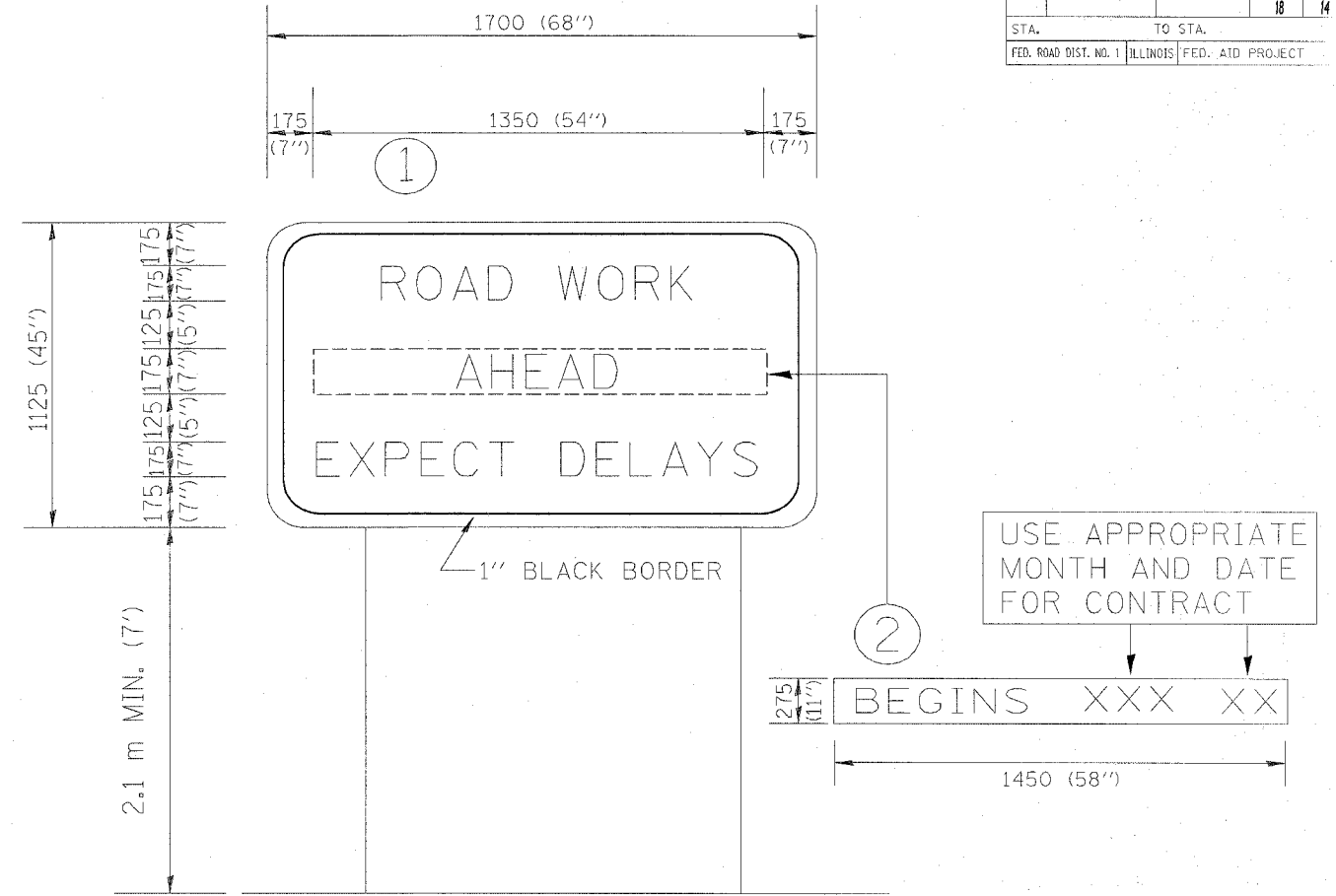
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| NAME | DATE |
| RAY RITCHIE | 5/10/00 |
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METHOD OF FLAGGING

SCALE: NOT TO SCALE
 DATE: 10/18/2002
 DRAWN BY: C.A.D.
 CHECKED BY: BM-14
 REVISION DATE: 05/10/00

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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| FED. ROAD DIST. NO. 1 | | ILLINOIS FED. AID PROJECT | | |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 2.3 SQ. M. (25.70 SQ. FT.)

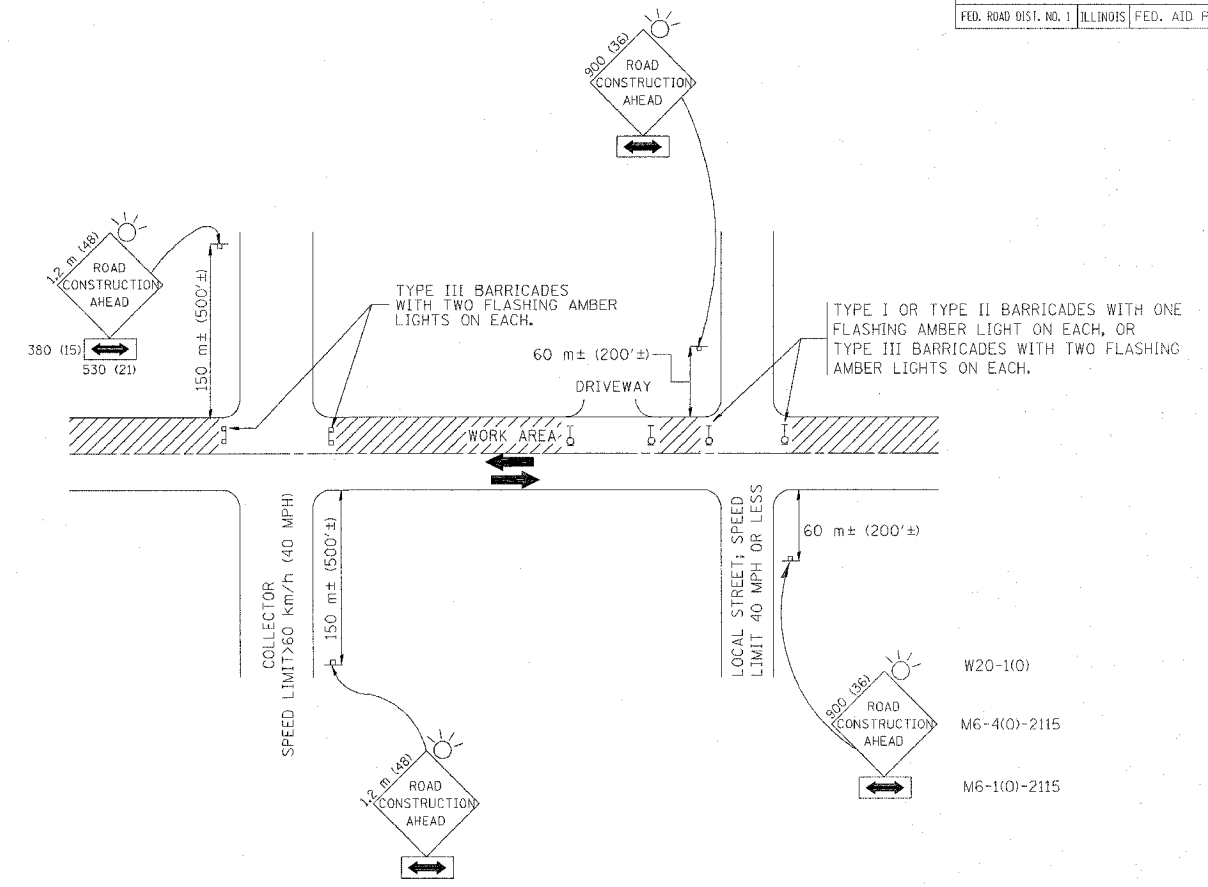
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION TEMPORARY INFORMATION SIGNING |
|--------------|---------|--|
| NAME | DATE | |
| R. MIRS | 9-15-97 | |
| R. MIRS | 2-11-97 | |
| T. RAMMACHER | 2-2-99 | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

SCALE: DATE 10/18/2002 DRAWN BY: BUR. OF DESIGN CHECKED BY:

62782

| | | | | |
|-----------------------|---------|----------|--------------|------------------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | 18 | 15 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | | FED. AID PROJECT |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| LHA | 6/89 |
| T. RAMMACHER | 09/08/94 |
| J. OBERLE | 10/18/95 |
| A. HOUSEH | 03/06/96 |
| A. HOUSEH | 10/15/96 |
| T. RAMMACHER | 01/06/00 |
| | |
| | |
| | |
| | |

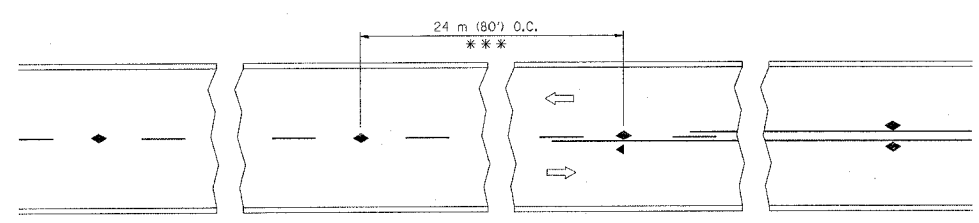
ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: VERT. _____
 HORIZ. _____
 DATE 10/18/2002

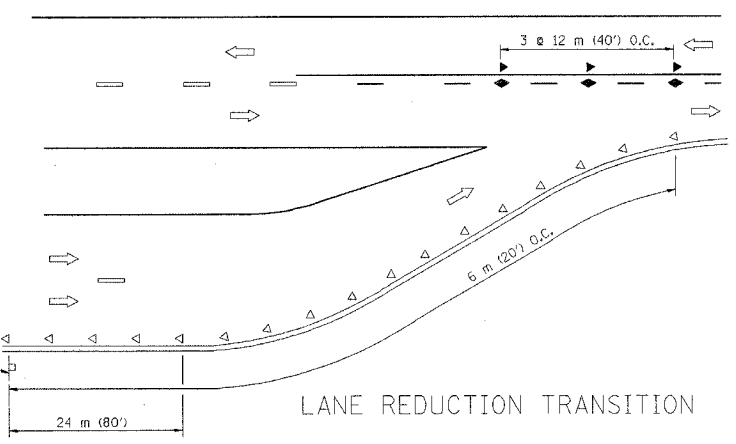
DRAWN BY _____
 CHECKED BY _____
 TC-10
 REVISION DATE: 01/06/00

62782

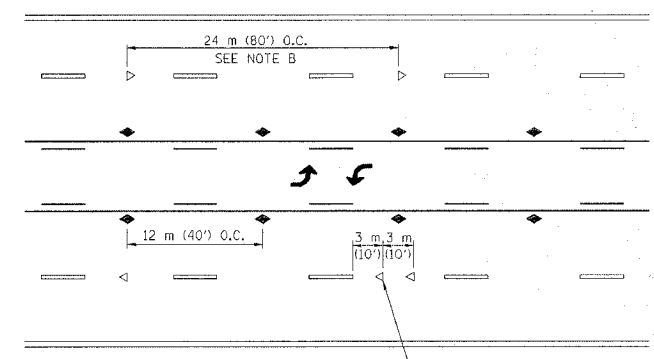
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|--------------|--------|---------------------|-----------|
| SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | 18 | 16 |
| STA. TO STA. | | FED. ROAD DIST. NO. | |
| | | FED. AID PROJECT | |



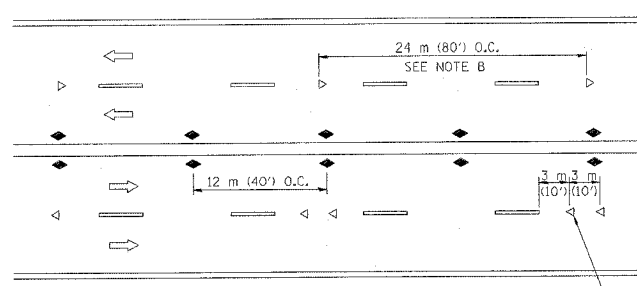
TWO-LANE/TWO-WAY



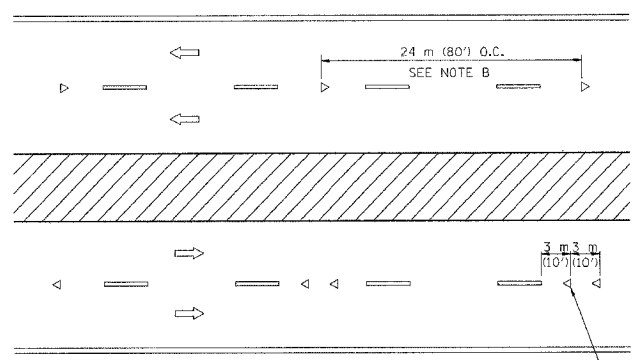
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

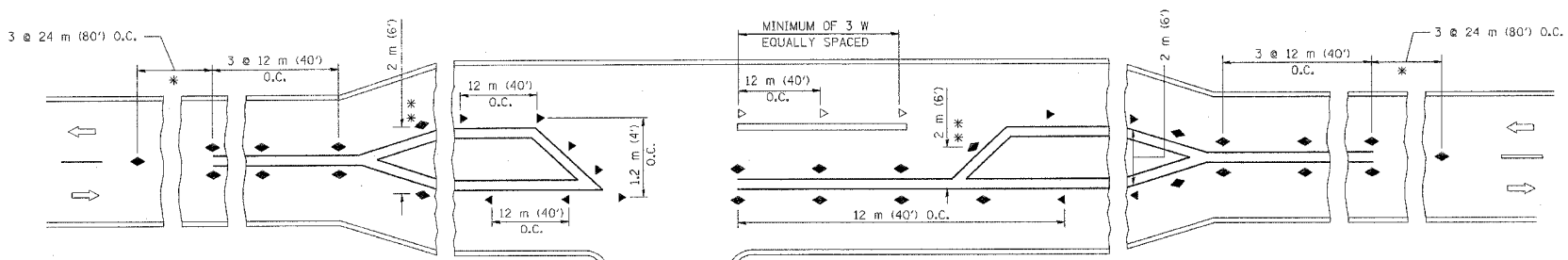
1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 50 TO 75 (2 TO 3) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 150 m (500') IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 12 m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 20 km/h (10 M.P.H.) LOWER THAN POSTED SPEEDS.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 2 m (6') OR LESS USE TWO-WAY MARKERS.

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS
 (SNOW-PLOW RESISTANT)

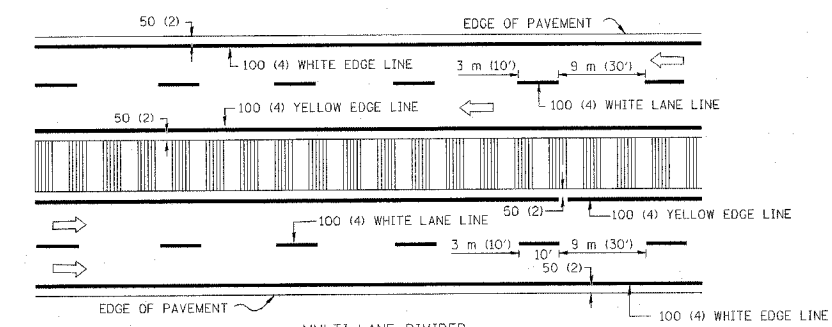
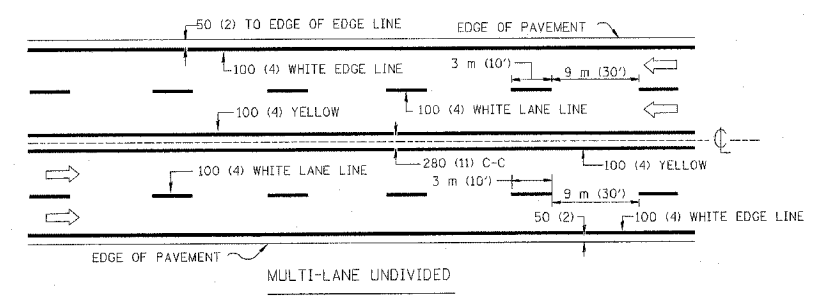
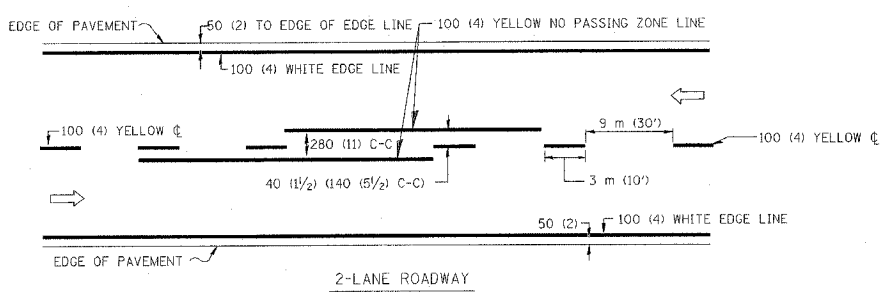
| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| T. RAMMACHER | 09-19-94 |
| T. RAMMACHER | 03-12-99 |
| T. RAMMACHER | 01-06-00 |
| | |
| | |

SCALE: NONE
 DATE: 10/18/2002

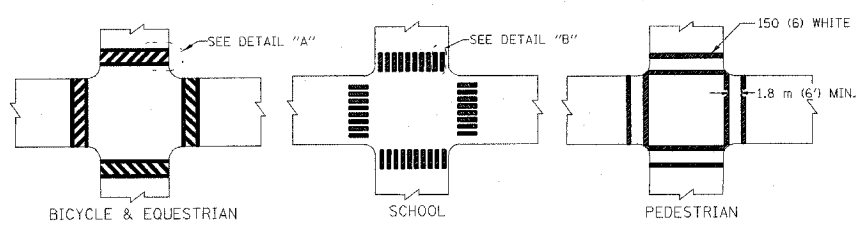
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 CHECKED BY TC-11

REVISION DATE: 01/06/00

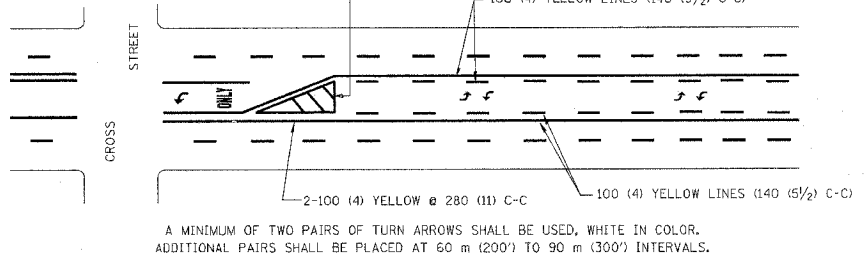
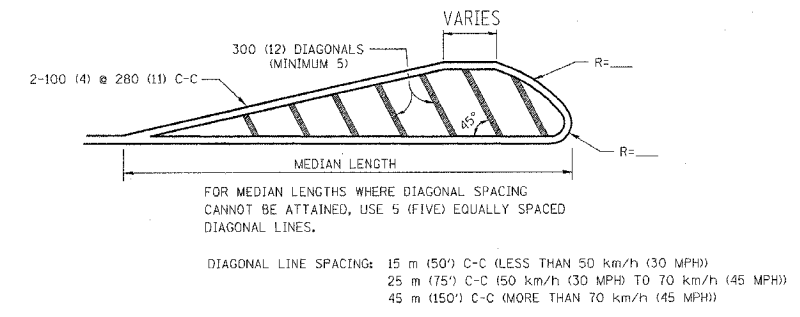
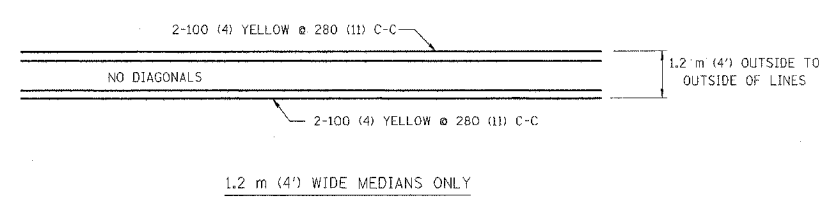
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|---------------------|---------|------------------|--------------|-----------|
| F. & S. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | 18 | 17 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | BLANKS | FED. AID PROJECT | | |



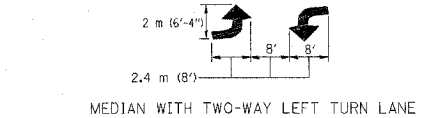
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



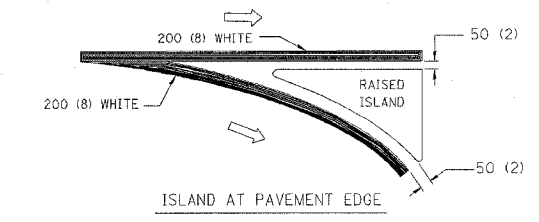
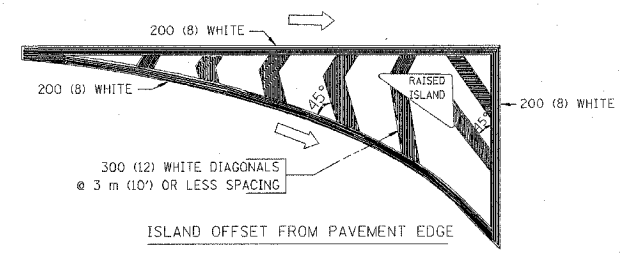
TYPICAL PAINTED MEDIAN MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|---|---------------------|--|--|
| CENTERLINE ON 2 LANE PAVEMENT | 100 (4) | SKIP-DASH | YELLOW | 3 m (10') LINE WITH 9 m (30') SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 100 (4) | SOLID | YELLOW | 280 (11) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION | 100 (4) | SOLID | YELLOW | 140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE |
| FOR BOTH DIRECTIONS | 2 @ 100 (4) | SOLID | YELLOW | 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 100 (4) 125 (5) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 3 m (10') LINE WITH 9 m (30') SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 600 (2') LINE WITH 1.8 m (6') SPACE |
| EDGE LINES | 100 (4) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8')) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 100 (4) EACH DIRECTION | SKIP-DASH AND SOLID | YELLOW | 3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE |
| | 2.4 m (8') LEFT ARROW | IN PAIRS | WHITE | SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 1.8 m (6') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 600 (24) | SOLID | WHITE | PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| CORE MARKING AND CHANNELIZING LINES | 200 (8) WITH 300 (12) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH)) |
| RAILROAD CROSSING | 600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=0.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.) |
| SHOULDER DIAGONALS | 300 (12) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

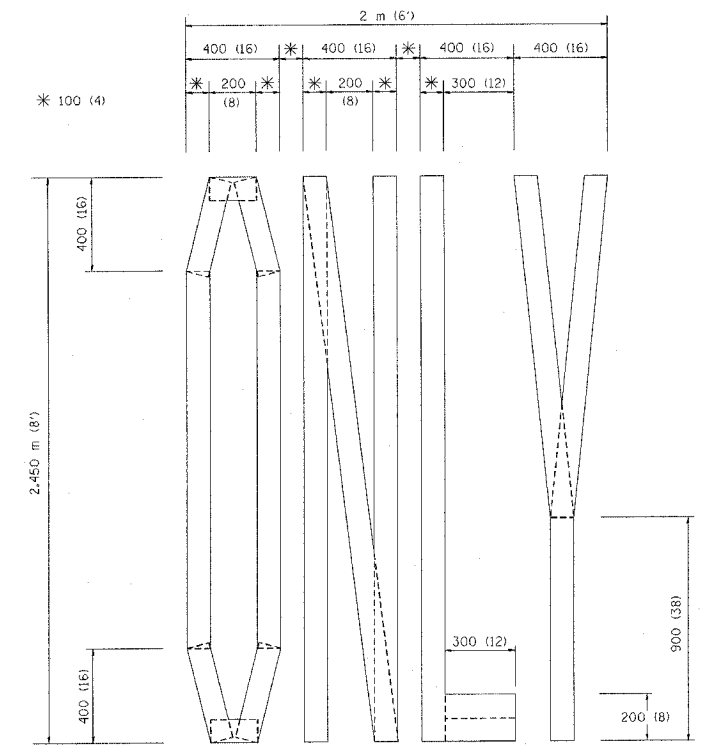
ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 TYPICAL PAVEMENT MARKINGS

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| EVERS | 03-19-90 |
| T. RAMMACHER | 10-27-94 |
| ALEX HOUSEH | 10-09-96 |
| ALEX HOUSEH | 10-17-96 |
| T. RAMMACHER | 01-06-00 |

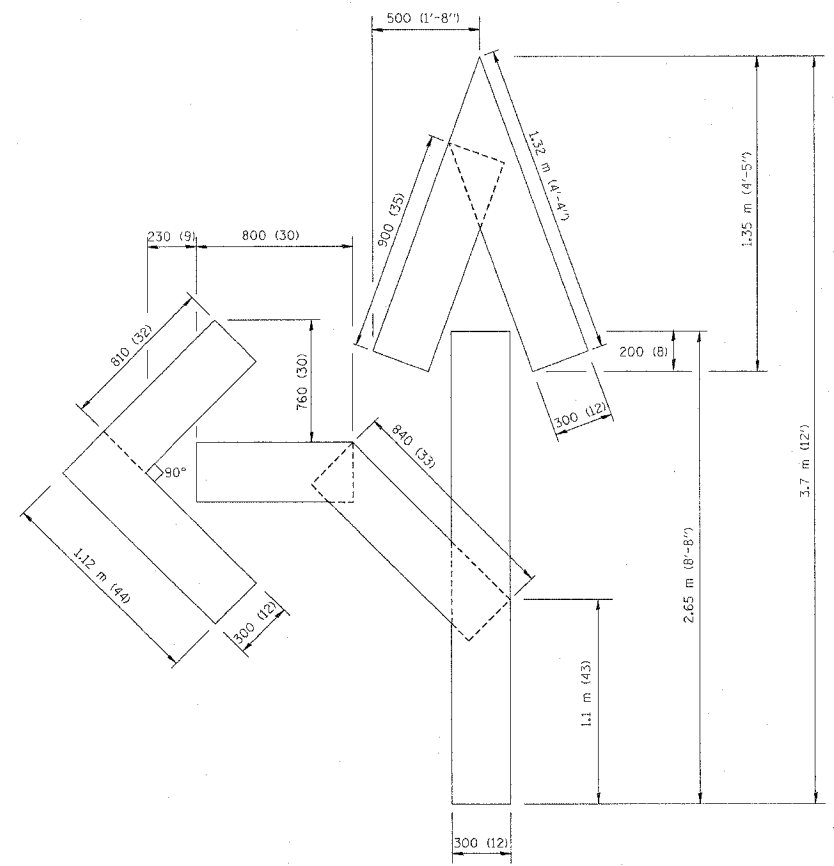
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 DATE 10/18/2002
 DRAWN BY CADD
 CHECKED BY TC-13
 REVISION DATE 01/06/00

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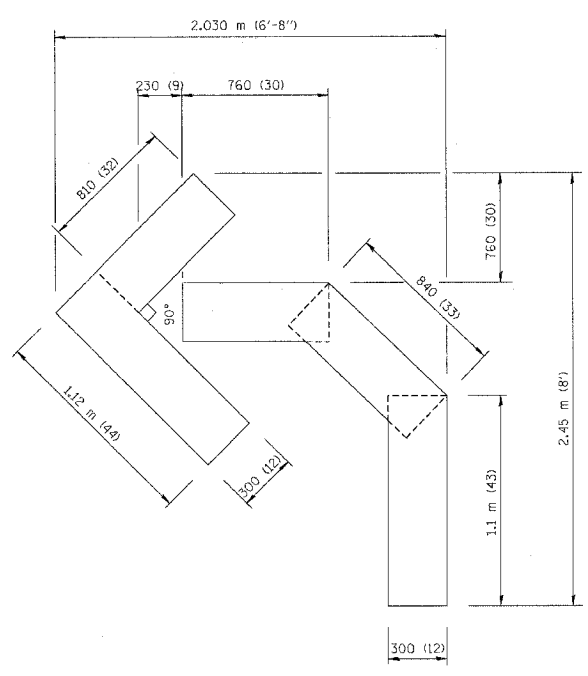
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|---------------------|----------|------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | 18 | 18 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



QUANTITY
 100 (4) LINE = 19.7 m (64.1 ft.)
 1.97 sq. m (21.1 sq. ft.)



QUANTITY
 100 (4) LINE = 25.3 m (82.5 ft.)
 2.53 sq. m (27.5 sq. ft.)



QUANTITY
 100 (4) LINE = 13.9 m (45.5 ft.)
 1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| T. RAMMACHER | 09/18/94 |
| J. OBERLE | 06/01/96 |
| T. RAMMACHER | 06/05/96 |
| T. RAMMACHER | 11/04/97 |
| T. RAMMACHER | 03/02/98 |
| E. GOMEZ | 08/28/00 |

SCALE: NONE
 DATE: 10/18/2002

DRAWN BY CADD
 CHECKED BY TC-16

REVISION DATE: 08/28/00