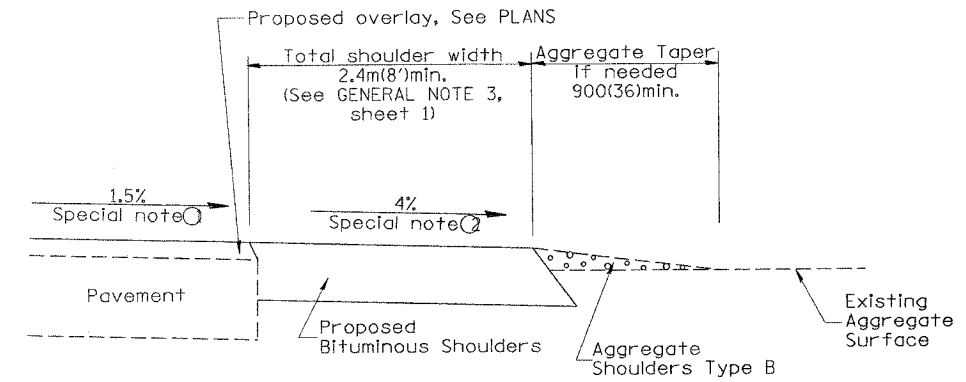
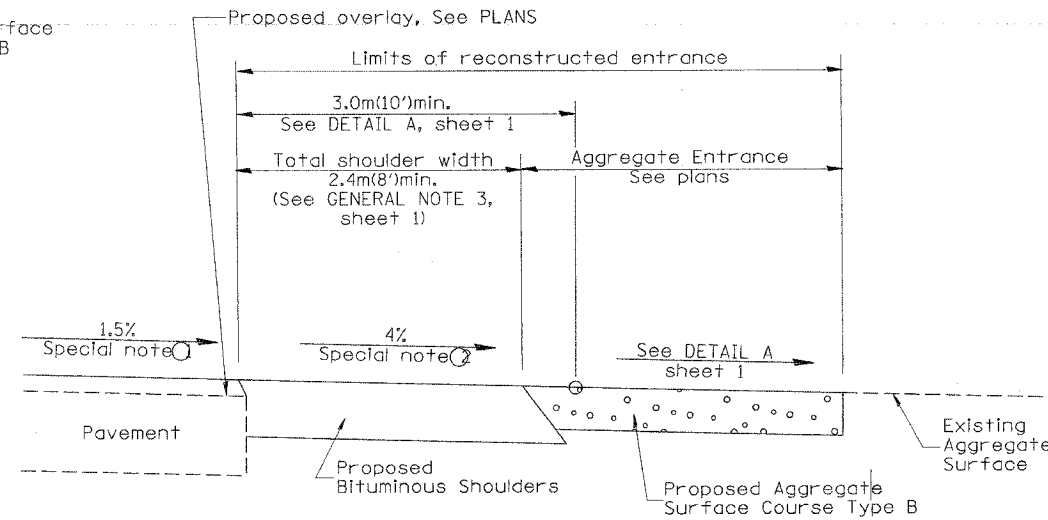
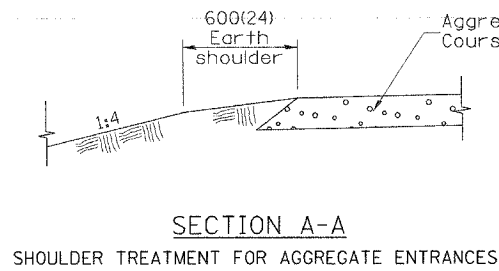
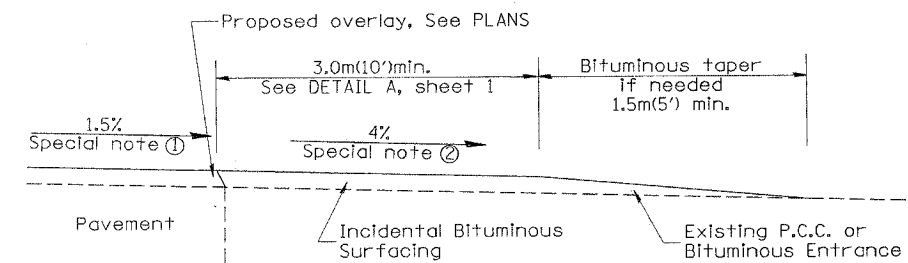
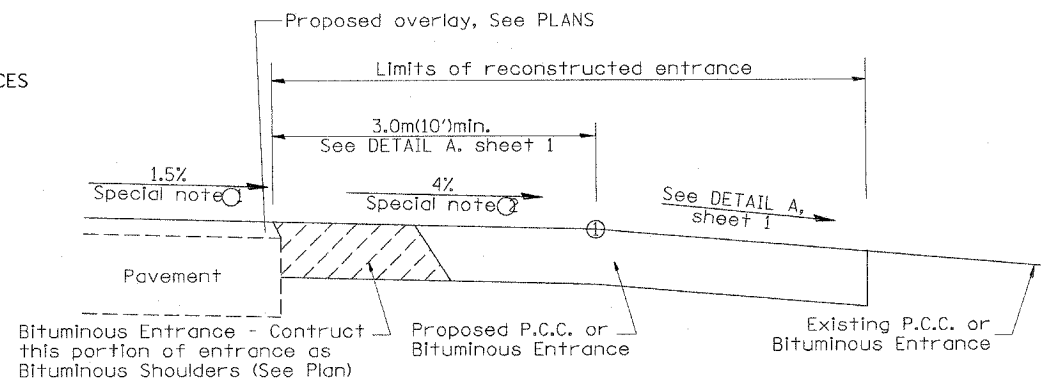
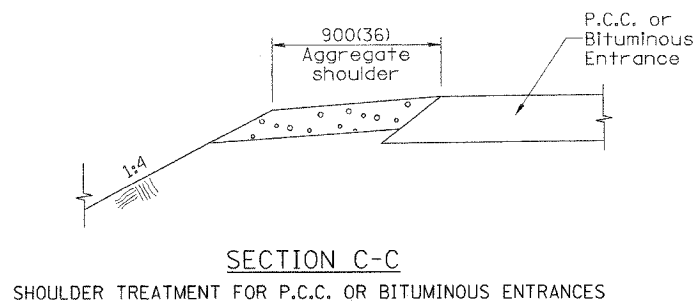


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|--|----------|------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 315/317 | *** | FULTON | 684 | 259 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| *** 181,188BRY&188BRY-188R,188-1 CONTRACT# 88753 | | | | |



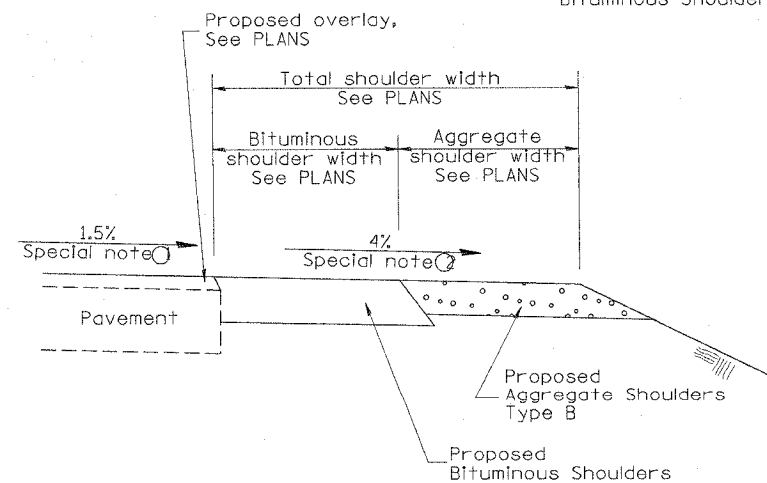
SECTION B-B
RECONSTRUCTED AGGREGATE ENTRANCE

SECTION B-B
EXISTING AGGREGATE ENTRANCE



SECTION D-D
RECONSTRUCTED P.C.C. OR BITUMINOUS ENTRANCE

SECTION D-D
EXISTING P.C.C. OR BITUMINOUS ENTRANCE



SECTION E-E
MAINLINE SHOULDER TREATMENT

SPECIAL NOTES

- ① The mainline pavement cross-slope is 1.5% for tangent alignment. See PLANS for cross-slope on super-elevated horizontal curves.
- ② The shoulder slope shall control the entrance profile for a distance of 3.0m(10') minimum from the pavement edge. The shoulder cross-slope is 4% for tangent alignment. Through super-elevated curves, the maximum pavement-shoulder breakover should not be greater than 10% for shoulders 1.8(6) and wider and 12% for shoulders 1.2m(4') and less. Where 300(12) paved shoulders are provided, the breakover should be at the edge of the paved shoulder rather than at the pavement edge.

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H). All dimensions are in millimeters (inches) unless otherwise noted.

| | |
|---------------------------------------|--------------------------|
| ILLINOIS DEPARTMENT OF TRANSPORTATION | |
| DISTRICT CADD STANDARD | |
| RURAL ENTRANCES FOR "3R" PROJECTS | |
| CADD STD NO. 406301-D4 | SHEET 2 OF 2 |
| SCALE: NOT DRAWN TO SCALE | DRAWN BY CADD |
| DATE **DATE** | CHECKED BY: T. PICKERING |

406301-D4(2)