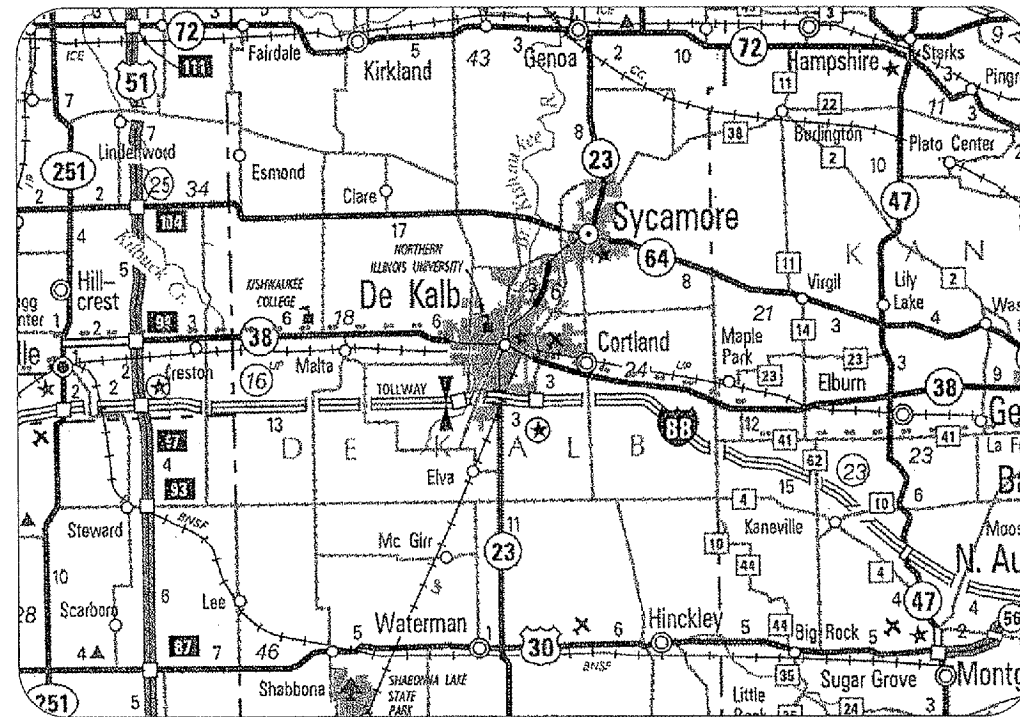


# CONSTRUCTION PLANS FOR DEKALB TAYLOR MUNICIPAL AIRPORT DEKALB, DEKALB COUNTY, ILLINOIS RECONSTRUCT WEST APRON

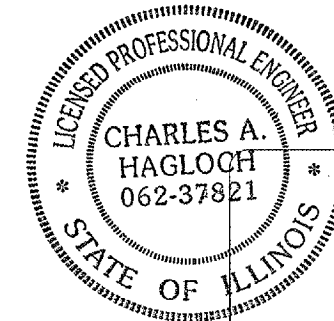
### SCOPE OF WORK

THIS PROJECT CONSISTS OF RECONSTRUCTION OF THE WEST AIRCRAFT PARKING APRON. ASSOCIATED WORK INCLUDES REMOVAL AND REPLACEMENT OF EXISTING PAVEMENT, POROUS FRICTION COURSE, TIE DOWNS AND TIE DOWN REMOVAL, MARKING AND MARKING REMOVAL, MANHOLE ADJUSTMENTS AND SODDING.



### LOCATION

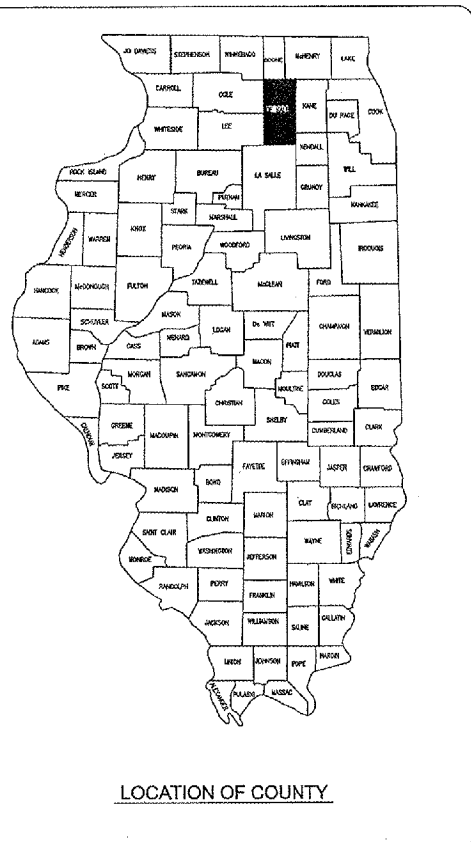
ILL. PROJ.: DKB-3467  
LATITUDE: 41° 55' 48"  
LONGITUDE: 88° 42' 48"  
ELEVATION: 911.0' M.S.L.  
DATE: MARCH 18, 2005



Submitted by: *Charles A. Hagloch* P.E.  
Date Submitted: 4-28-05  
Lic. Exp. Date: 11-30-05

CITY OF DEKALB

Approved: *[Signature]*  
Director Of Public Works  
Date: 4-6-05



| REVISION | DATE | BY |
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|----------------------------|-------------------|----------|-----|----------|
| HEL Project No. 803-05APND | FILENAME R-001CVR | LAYOUT   | JMB | 03/16/05 |
| Scale NOT TO SCALE         | Date 03/16/05     | DRAWN    | JMB | 03/16/05 |
|                            |                   | REVIEWED | CAH | ?        |



RECONSTRUCT WEST APRON  
COVER SHEET

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SUMMARY OF QUANTITIES

| ITEM NO. | DESCRIPTION                    | UNIT | AS AWARDED QUANTITIES | AS BUILT QUANTITIES |
|----------|--------------------------------|------|-----------------------|---------------------|
| AR108758 | 1/C #8 GROUND                  | L.F. | 375                   |                     |
| AR150510 | ENGINEER'S FIELD OFFICE        | L.S. | 1                     |                     |
| AR150540 | HAUL ROUTE                     | L.S. | 1                     |                     |
| AR152480 | SHOULDER ADJUSTMENT            | S.Y. | 390                   |                     |
| AR401610 | BITUMINOUS SURFACE COURSE      | TONS | 877                   |                     |
| AR401620 | BIT. SURFACE COURSE, LEVELING  | TONS | 1,398                 |                     |
| AR401655 | BUTT JOINT CONSTRUCTION        | S.Y. | 743                   |                     |
| AR401910 | REMOVE & REPLACE BIT. PAVEMENT | S.Y. | 450                   |                     |
| AR402622 | POROUS FRICTION COURSE, 0.10'  | S.Y. | 8,595                 |                     |
| AR510510 | TIE DOWN                       | EACH | 36                    |                     |
| AR510515 | GROUND ROD                     | EACH | 7                     |                     |
| AR510900 | REMOVE TIE DOWN                | EACH | 22                    |                     |
| AR603510 | BITUMINOUS TACK COAT           | GAL. | 7,395                 |                     |
| AR620520 | PAVEMENT MARKING - WATERBORNE  | S.F. | 1,472                 |                     |
| AR620900 | PAVEMENT MARKING REMOVAL       | S.F. | 116                   |                     |
| AR751943 | ADJUST MANHOLE                 | EACH | 3                     |                     |
| AR904510 | SODDING                        | S.Y. | 292                   |                     |

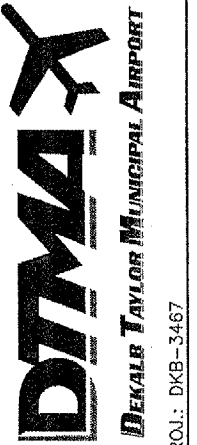
INDEX TO SHEETS

| SHEET NO. | DESCRIPTION                                       |
|-----------|---|
| 1         | COVER SHEET                                       |
| 2         | SUMMARY OF QUANTITIES, INDEX TO SHEETS            |
| 3         | PROPOSED SAFETY PLAN                              |
| 4         | PROPOSED CONSTRUCTION PLAN                        |
| 5         | PROPOSED TYPICAL SECTIONS                         |
| 6         | PROPOSED PAVEMENT PREPARATION PLAN                |
| 7         | PROPOSED STAKING PLAN                             |
| 8         | EXISTING MARKING AND TIE DOWN PLAN                |
| 9         | PROPOSED MARKING AND TIE DOWN PLAN                |
| 10        | PROPOSED MARKING AND TIE DOWN DETAILS             |
| 11        | PROPOSED CROSS-SECTIONS STA. 0+71.08 TO STA. 1+00 |
| 12        | PROPOSED CROSS-SECTIONS STA. 1+25 TO STA. 1+50    |
| 13        | PROPOSED CROSS-SECTIONS STA. 1+75 TO STA. 2+25    |
| 14        | PROPOSED CROSS-SECTIONS STA. 2+50 TO STA. 3+00    |
| 15        | PROPOSED CROSS-SECTIONS STA. 3+25 TO STA. 3+75    |
| 16        | PROPOSED CROSS-SECTIONS STA. 4+00 TO STA. 4+25    |
| 17        | PROPOSED CROSS-SECTIONS STA. 4+50 TO STA. 4+75    |
| 18        | PROPOSED CROSS-SECTIONS STA. 5+00 TO STA. 5+21    |
| 19        | PROPOSED CROSS-SECTIONS STA. 5+50 TO STA. 5+75    |
| 20        | PROPOSED CROSS-SECTIONS STA. 6+00 TO STA. 6+35    |
| 21        | PROPOSED CROSS-SECTIONS STA. 6+50 TO STA. 7+02.34 |

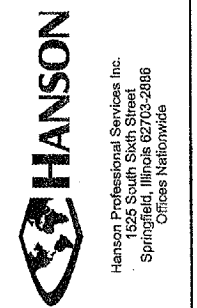
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| HEL Project No. 803-05APND | FILENAME R-002\I.P. | DATE 02/10/05 |
| Scale NOT TO SCALE         | DATE 02/10/05       |               |
| LAYOUT                     | JMB                 | 02/10/05      |
| DRAWN                      | JMB                 | 02/10/05      |
| REVIEWED                   | CAH                 |               |



RECONSTRUCT  
WEST APRON

SUMMARY OF QUANTITIES,  
INDEX TO SHEETS

ILL. PROJ.: DKB-3467

**SCOPE OF WORK**

THIS PROJECT WILL CONSIST OF RECONSTRUCTING THE WEST APRON. ASSOCIATED WORK WILL INCLUDE REMOVAL AND REPLACEMENT OF EXISTING PAVEMENT, POROUS FRICTION COURSE, TIE DOWNS, TIE DOWN REMOVAL, MARKING, MARKING REMOVAL, MANHOLE ADJUSTMENT AND SODDING.

**AIRPORT SECURITY NOTE**

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE AND LOCK THE EXISTING GATE IN THE HAUL ROUTE AT THE END OF EACH WORKING DAY.

**BARRICADES AND TRAFFIC CONES**

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS DIRECTED BY THE AIRPORT MANAGER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR STEADY BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**EROSION CONTROL**

THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND, THEREFORE NO N.P.D.E.S. PERMIT WILL BE REQUIRED.

**PROPOSED SAFETY PLAN**

GENERAL - THE DEKALB TAYLOR MUNICIPAL AIRPORT IS COMPRISED OF 2 RUNWAYS. THE PROPOSED CONSTRUCTION WILL NOT NECESSITATE CLOSING EITHER RUNWAY.

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (122.7 MHz.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE DEKALB TAYLOR MUNICIPAL AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

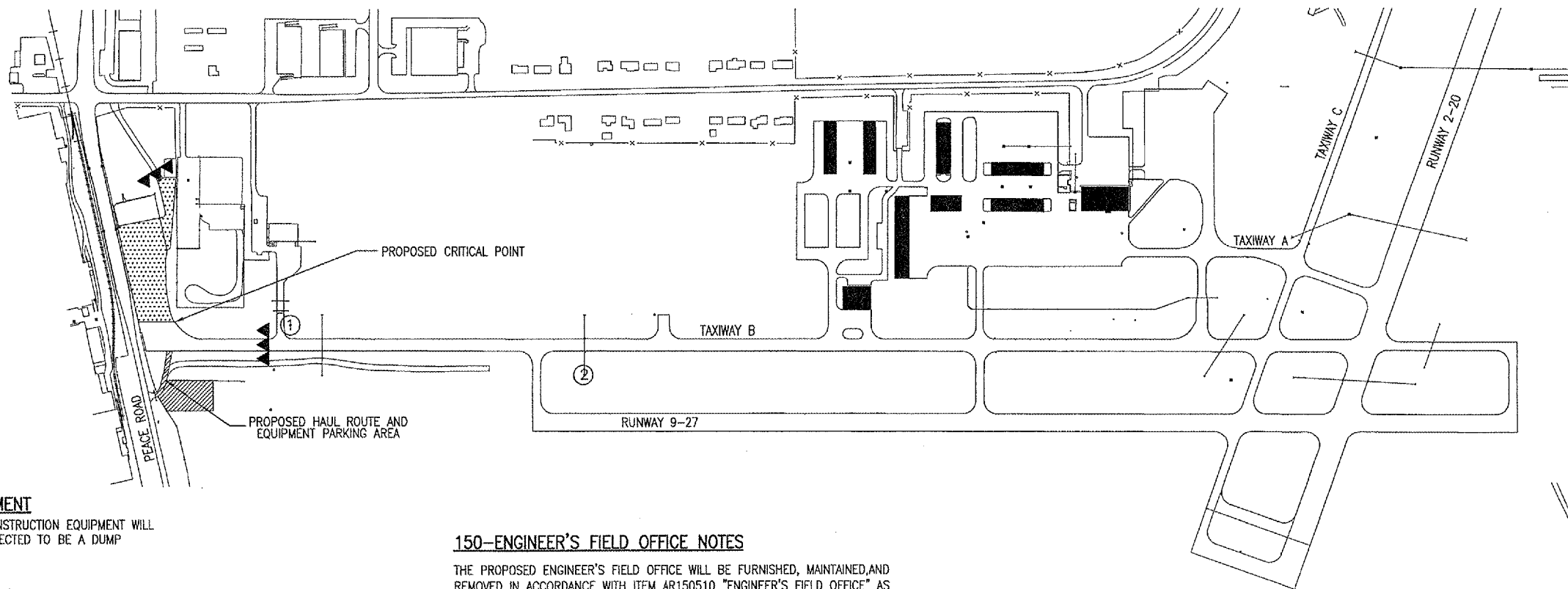
**UTILITY NOTE**

THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND AGENCIES WHICH HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) TO ACCOMPLISH THE ABOVE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY ALL UNDERGROUND NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UNDERGROUND IMPROVEMENTS WILL BE LOCATED AT THE CONTRACTOR'S OWN EXPENSE PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.

**J.U.L.I.E. INFORMATION**

COUNTY DEKALB  
 CITY DEKALB  
 TOWNSHIP CORTLAND  
 SECTION NO. 18 AND 19  
 ADDRESS DEKALB MUNICIPAL AIRPORT  
 3232 EAST PLEASANT STREET  
 DEKALB, ILLINOIS 60115  
 815-756-7525

| CRITICAL POINT DATA |         |           |                |                |           |
|---------------------|---------|-----------|----------------|----------------|-----------|
| NO.                 | STATION | OFFSET    | LATTITUDE      | LONGITUDE      | ELEVATION |
| 1                   | 0+47.08 | 109.3' RT | 41° 55' 48.25" | 88° 43' 27.59" | 907.56    |



**HEIGHT OF CONSTRUCTION EQUIPMENT**

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A DUMP TRUCK.

**HAUL ROUTE AND VEHICLE PARKING**

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND PARKING AREA AS SHOWN ON THIS SHEET. THE PROPOSED PARKING AREA WILL BE 200' X 200'. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL GRADE, FERTILIZE, SEED AND MULCH THE HAUL ROUTE AND PARKING AREA AS NEEDED TO RESTORE IT TO ITS' ORIGINAL STATE. CONSTRUCTION AND RESTORATION OF THE HAUL ROUTE AND PARKING AREA WILL BE PAID FOR UNDER ITEM:

AR150540 "HAUL ROUTE" 1 L.S.

**CONTRACTOR RESPONSIBILITIES**

THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THIS AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

ALL WORK PERFORMED SHALL BE DONE IN A ORDERLY AND EFFECTIVE MANNER TO MINIMIZE DISRUPTION OF AIRPORT ACTIVITY.

NO TRENCHES OR HOLES WILL REMAIN OPEN OVERNIGHT.

**150-ENGINEER'S FIELD OFFICE NOTES**

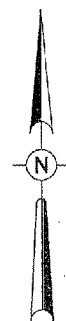
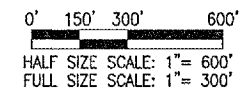
THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 168 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS:  
 AR150510 ENGINEER'S FIELD OFFICE 1 L.S.

| BENCHMARK DATA |  |        |
|----------------|--|--------|
| NO.            | DESCRIPTION                              | ELEV.  |
| 1              | NGS -- KALBPORT AZ MARKER                | 903.47 |
| 2              | CONC. NAIL AND WASHER IN TOP OF HEADWALL | 906.72 |



**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA
- PROPOSED BENCHMARK
- PROPOSED BARRICADES OR TRAFFIC CONES

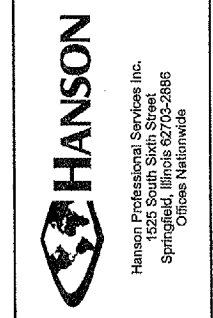
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| HEL Project No. 803-05APND | LAYOUT   | JMB | 03/16/05 |
| Filename R-003SEY          | DRAWN    | JMB | 03/16/05 |
| Scale 1" = 300'            | REVIEWED | CAH | ?        |
| Date 03/16/05              |          |     |          |



RECONSTRUCT WEST APRON  
 PROPOSED SAFETY PLAN

**POROUS FRICTION COURSE NOTES**

THE PROPOSED POROUS FRICTION COURSE WILL BE CONSTRUCTED IN ONE LAYER, HAVING A COMPACTED NOMINAL THICKNESS OF 0.10 FOOT.

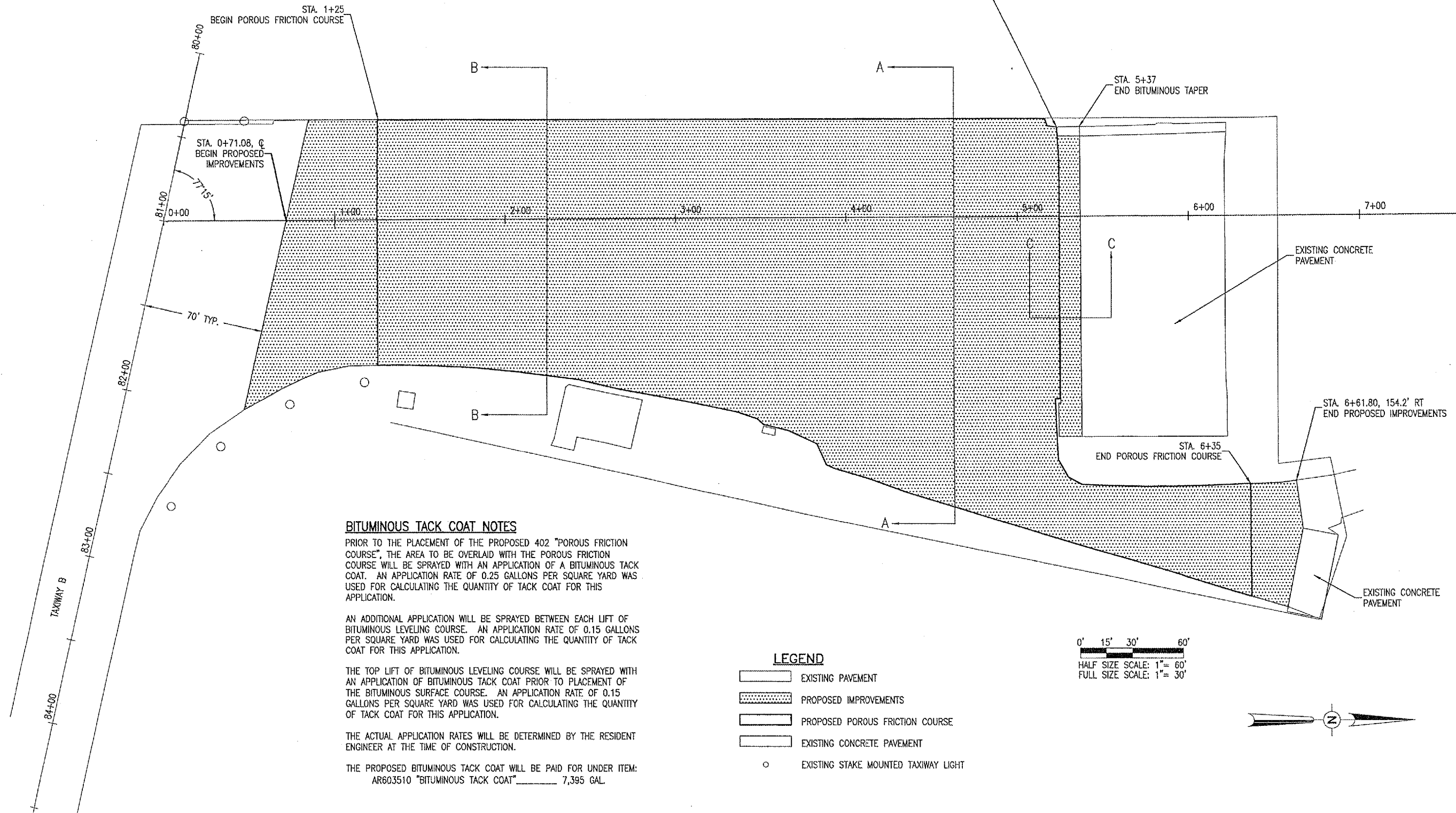
POROUS FRICTION COURSE SHALL BE PLACED ON A CLEAN AND PREPARED SURFACE ONLY AFTER THE APPROVAL OF THE RESIDENT ENGINEER.

POROUS FRICTION COURSE WILL BE CONSTRUCTED IN THE LOCATIONS SHOWN ON THIS SHEET OR AS DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

**VEGETATION REMOVAL**

ALL VEGETATION GROWING WITHIN THE PAVEMENT AREA TO BE OVERLAID WILL BE SPRAYED WITH A HERBICIDE (LASSO OR AN APPROVED EQUAL). THE HERBICIDE WILL BE APPLIED AT A RATE RECOMMENDED BY THE MANUFACTURER'S INSTRUCTIONS.

THE CONTRACTOR WILL BURN THE VEGETATION ONCE IS HAS DIED. THIS WORK WILL BE CONSIDERED AS INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



**BITUMINOUS TACK COAT NOTES**

PRIOR TO THE PLACEMENT OF THE PROPOSED 402 "POROUS FRICTION COURSE", THE AREA TO BE OVERLAID WITH THE POROUS FRICTION COURSE WILL BE SPRAYED WITH AN APPLICATION OF A BITUMINOUS TACK COAT. AN APPLICATION RATE OF 0.25 GALLONS PER SQUARE YARD WAS USED FOR CALCULATING THE QUANTITY OF TACK COAT FOR THIS APPLICATION.

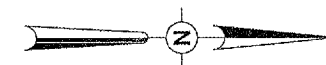
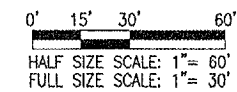
AN ADDITIONAL APPLICATION WILL BE SPRAYED BETWEEN EACH LIFT OF BITUMINOUS LEVELING COURSE. AN APPLICATION RATE OF 0.15 GALLONS PER SQUARE YARD WAS USED FOR CALCULATING THE QUANTITY OF TACK COAT FOR THIS APPLICATION.

THE TOP LIFT OF BITUMINOUS LEVELING COURSE WILL BE SPRAYED WITH AN APPLICATION OF BITUMINOUS TACK COAT PRIOR TO PLACEMENT OF THE BITUMINOUS SURFACE COURSE. AN APPLICATION RATE OF 0.15 GALLONS PER SQUARE YARD WAS USED FOR CALCULATING THE QUANTITY OF TACK COAT FOR THIS APPLICATION.

THE ACTUAL APPLICATION RATES WILL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

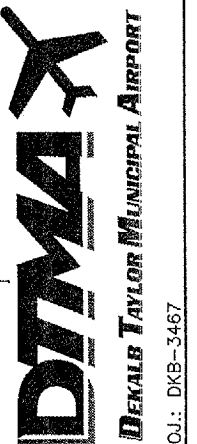
THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM: AR603510 "BITUMINOUS TACK COAT" \_\_\_\_\_ 7,395 GAL.

- LEGEND**
- EXISTING PAVEMENT
  - PROPOSED IMPROVEMENTS
  - PROPOSED POROUS FRICTION COURSE
  - EXISTING CONCRETE PAVEMENT
  - EXISTING STAKE MOUNTED TAXIWAY LIGHT

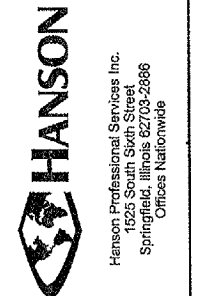


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| REL Project No. | B03-05APND   |
| File Name       | R-121CON     |
| Scale           | 1" = 30'     |
| Date            | 02/09/05     |
| LAYOUT          | JMB 02/09/05 |
| DRAWN           | JMB 02/09/05 |
| REVIEWED        | CAH ??/??/?? |



RECONSTRUCT WEST APRON  
PROPOSED CONSTRUCTION PLAN

**AR401611 BITUMINOUS SURFACE COURSE – METHOD 1**

THE BITUMINOUS SURFACE COURSE (401) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR401001 "BITUMINOUS SURFACE COURSE-METHOD 1" AS STATED ON PAGE 185 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING SUCCESSIVE LIFTS OF BITUMINOUS LEVELING COURSE (2-1/2 INCH MAXIMUM LIFTS) ON TOP OF THE PROPOSED POROUS FRICTION COURSE. A 1-1/2 INCH LIFT OF BITUMINOUS SURFACE COURSE WILL BE PLACED ON TOP OF THE FINAL LIFT OF BITUMINOUS LEVELING COURSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE QUALITY CONTROL IN THE PRODUCTION AND CONSTRUCTION OF THE BITUMINOUS SURFACE COURSE.

PRIOR TO STARTING THE AGGREGATE BITUMINOUS SURFACE COURSE OPERATION, THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER A DETAILED OUTLINE SHOWING AREAS AND ORDER OF PAVING WIDTHS OF PAVING LANES, AND REQUIRED OFFSETS FOR ELECTRONIC GRADE.

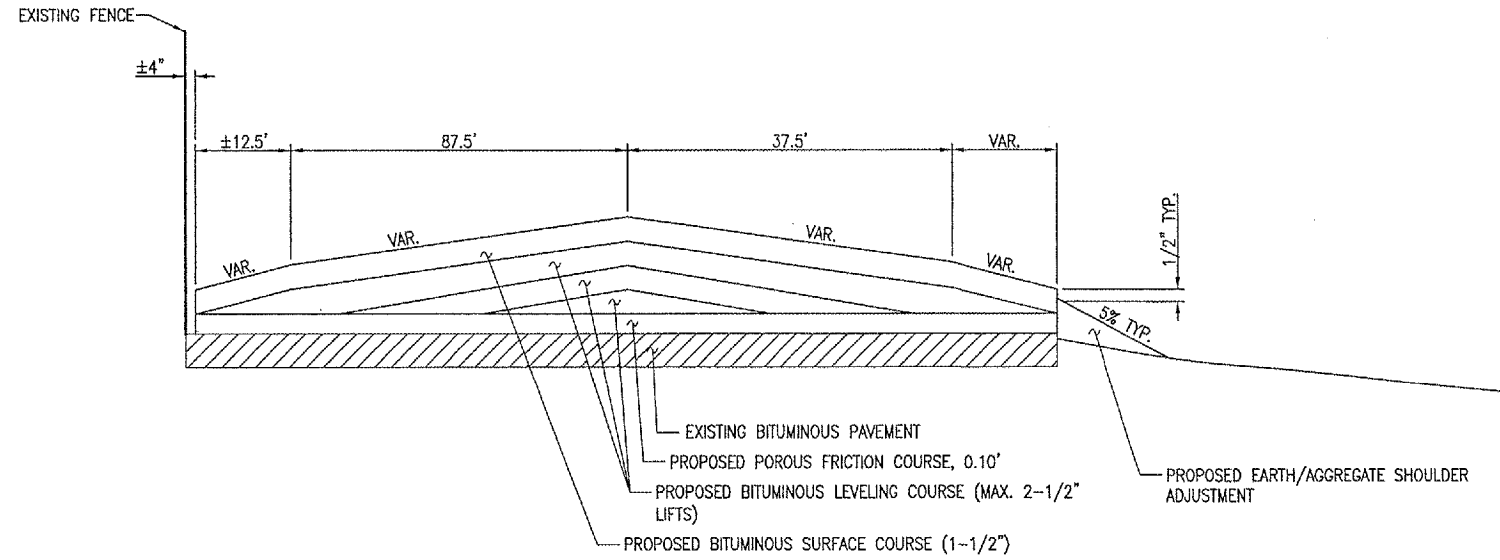
THE PROPOSED BITUMINOUS SURFACE COURSE WILL BE DESIGNED TO A MARSHALL DESIGN OF LESS THAN 60,000 POUNDS.

401-4.9 ADD THE FOLLOWING TO THIS SECTION:

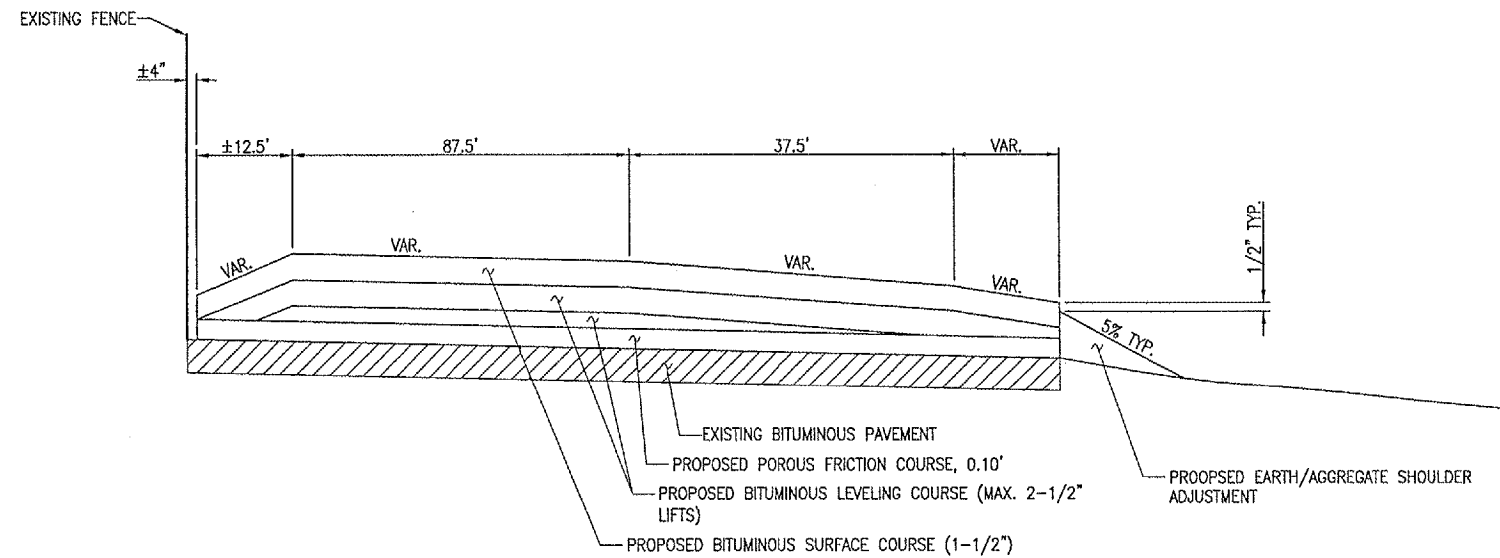
WHEN HAND SPREADING IS PERMITTED, THE MIXTURE WILL BE DISTRIBUTED AND SPREAD USING HAND TOOLS. WHEN THE WORK IS COMPLETED, THE LAYER WILL HAVE THE REQUIRED THICKNESS AND CONFORM TO THE GRADE AND SURFACE CONTOUR SHOWN ON THE PLANS.

401-4.12 ADD THE FOLLOWING TO THIS PARAGRAPH:

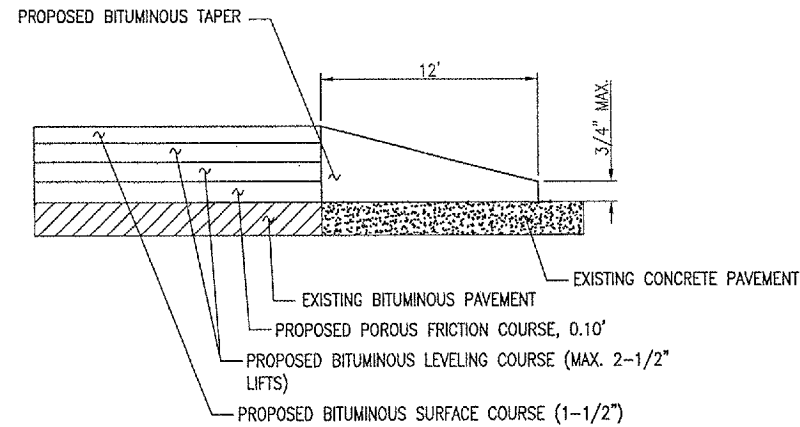
ALL PAVEMENT EDGES (LONGITUDINAL, RADIUS, AND PAVEMENT ENDS) MUST BE LEFT IN PROPER ALIGNMENT AS SHOWN ON THE PLANS. THIS MAY BE ACCOMPLISHED BY THE TRIMMING METHOD OUTLINED ABOVE OR AT THE CONTRACTOR'S OPTION BY SAWING AFTER THE PAVING HAS BEEN COMPLETED. NO ADDITIONAL COMPENSATION WILL BE MADE IF THE SAWING METHOD IS USED.



**PROPOSED TYPICAL SECTION A-A**  
NOT TO SCALE



**PROPOSED TYPICAL SECTION B-B**  
NOT TO SCALE



**PROPOSED TYPICAL SECTION C-C**  
NOT TO SCALE

BITUMINOUS TACK COAT SHALL BE APPLIED BETWEEN LIFTS OF PROPOSED PAVEMENT AT THE FOLLOWING RATES:

- ON EXISTING PRIOR TO PFC – 0.25 GAL/SY
  - 80% EMULSION/20% WATER
  - MINIMUM CURE TIME OF 48 HOURS
  - TACK MUST BE DRY TO THE SATISFACTION OF THE RESIDENT ENGINEER

- ON PFC PRIOR TO FIRST LIFT 401 – 0.25 GAL/SY
  - 80% EMULSION/20% WATER
  - MINIMUM CURE TIME OF 24 HOURS
  - TACK MUST BE DRY TO THE SATISFACTION OF THE RESIDENT ENGINEER

- ON 401 PRIOR TO EACH ADDITIONAL LIFT 401 – 0.15 GAL/SY
  - 50% EMULSION/50% WATER
  - MINIMUM CURE TIME OF 24 HOURS
  - TACK MUST BE DRY TO THE SATISFACTION OF THE RESIDENT ENGINEER

WATER SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION ALLOWED.

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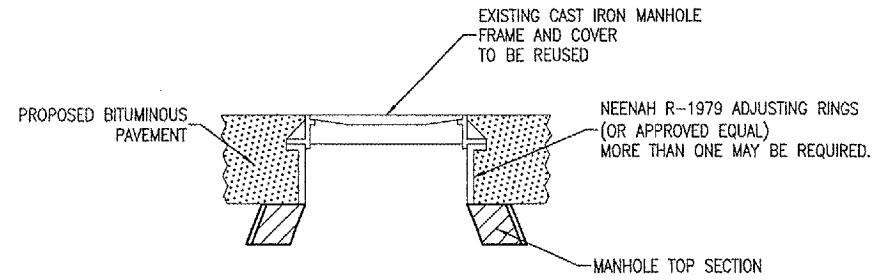
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| HEL Project No. | 803-059PND   |
| Filename        | R-521CON     |
| Scale           | NOT TO SCALE |
| Date            | 02/14/05     |
| LAYOUT          | JMB 02/14/05 |
| DRAWN           | JMB 02/14/05 |
| REVIEWED        | CAH ?        |



RECONSTRUCT WEST APRON  
PROPOSED TYPICAL SECTIONS



**AR751943 MANHOLE ADJUSTMENT NOTES**

THE EXISTING MANHOLES WILL BE ADJUSTED AS SHOWN IN THE DETAILS ON THIS SHEET AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

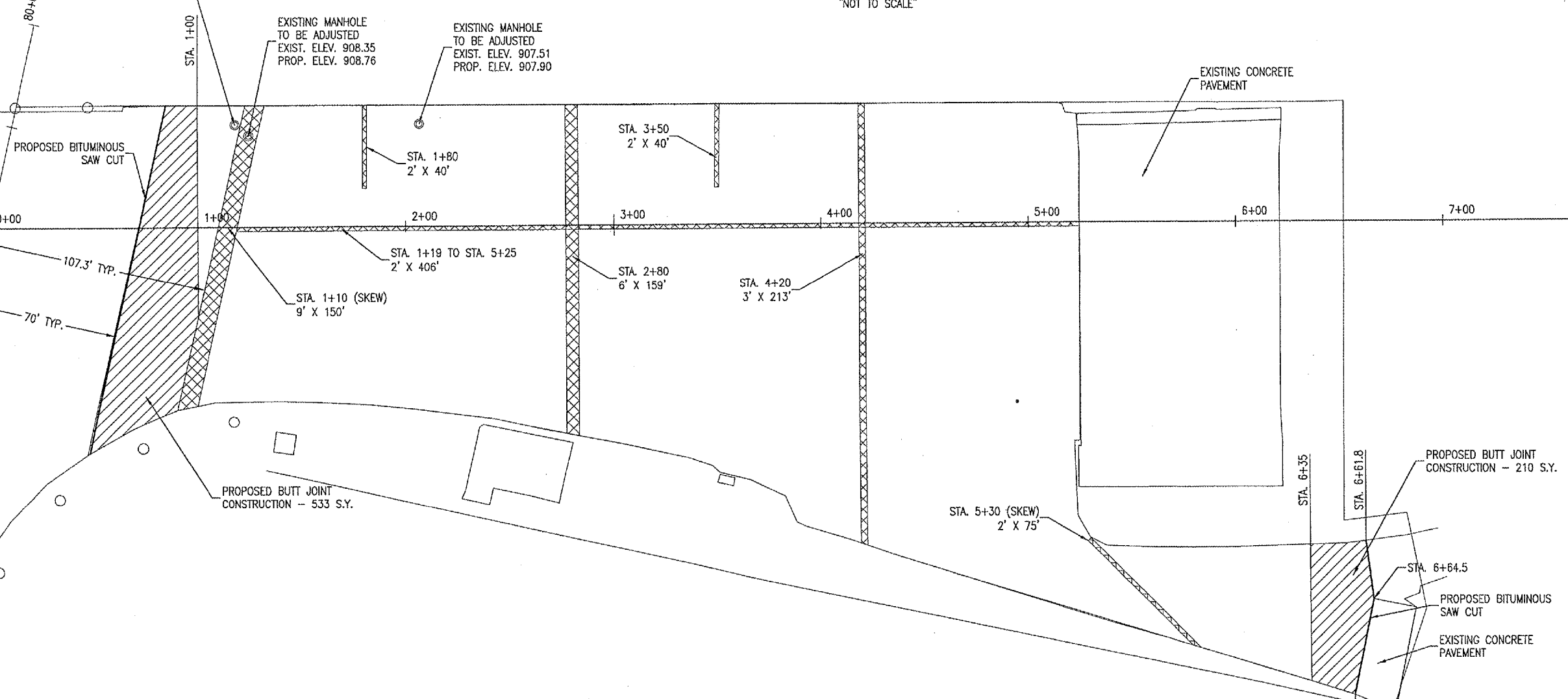
THE CONTRACTOR SHALL REMOVE THE EXISTING LID AND CONSTRUCT THE ADJUSTMENT TO THE PROPOSED ELEVATION. THE EXISTING LID WILL BE REUSED AND PLACED ON THE ADJUSTING RING(S) FOR EACH MANHOLE TO BE ADJUSTED

THE MANHOLE ADJUSTMENTS WILL BE PAID FOR UNDER ITEM:  
AR751943 "ADJUST MANHOLE" 3 EACH

EXISTING MANHOLE TO BE ADJUSTED  
EXIST. ELEV. 908.47  
PROP. ELEV. 908.87

EXISTING MANHOLE TO BE ADJUSTED  
EXIST. ELEV. 908.35  
PROP. ELEV. 908.76

EXISTING MANHOLE TO BE ADJUSTED  
EXIST. ELEV. 907.51  
PROP. ELEV. 907.90



**AR401655 BUTT JOINT CONSTRUCTION NOTES**

THE PROPOSED BUTT JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM AR401655 "BUTT JOINT CONSTRUCTION" AS STATED ON PAGE 295 OF THE SUPPLEMENTAL SPECIFICATION AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING BITUMINOUS PAVEMENT SURFACE BY MILLING IN ORDER TO CONSTRUCT A BUTT JOINT FOR THE TRANSITION OF THE PROPOSED BITUMINOUS OVERLAY AT THE LOCATIONS AS SHOWN ON THE PLANS.

THE PAVEMENT SURFACE WILL THEN BE MILLED TO A DEPTH OF 0.13 FT. AT THE BUTT END AND WILL TAPER TO 0 IN. AT THE OPPOSITE END. THE PLANER MUST BE CAPABLE OF MILLING THE SURFACE TO THE DESIRED ELEVATIONS AND GRADES.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

THE PAVEMENT MILLING WILL BE PAID FOR UNDER ITEM:  
AR401655 "BUTT JOINT CONSTRUCTION" 743 S.Y.

**REMOVE AND REPLACE BITUMINOUS PAVEMENT NOTES**

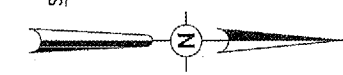
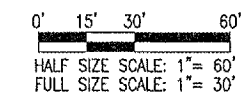
THE AREA DESIGNATED AS [X] ON THIS SHEET WILL HAVE THE EXISTING BITUMINOUS PAVEMENT REMOVED (FULL DEPTH) AND REPLACED WITH BITUMINOUS MATERIAL TO BE FLUSH WITH THE EXISTING PAVEMENT. THE AVERAGE DEPTH OF BITUMINOUS PAVEMENT REMOVAL IS ESTIMATED AT 6 INCHES. THE BITUMINOUS MATERIAL USED IN THE REPLACEMENT WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED REMOVAL AND REPLACEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

PAVEMENT AREAS TO BE REMOVED AND REPLACED WERE LOCATED IN A FEBRUARY 2005 SURVEY. ALL PAVEMENT AREAS TO BE REMOVED AND REPLACED WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

THE AREAS TO BE REPLACED SHALL BE SAWED FULL DEPTH. THE SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT REMOVAL AND REPLACEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

THE BITUMINOUS PAVEMENT REMOVAL AND REPLACEMENT WILL BE PAID FOR UNDER ITEM:  
AR401910 "REMOVE AND REPLACE BIT. PAVEMENT" 450 SQ. YDS.

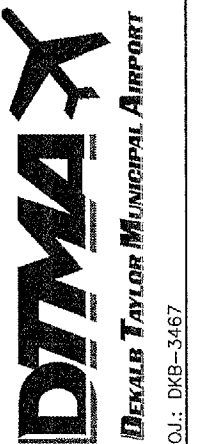


**LEGEND**

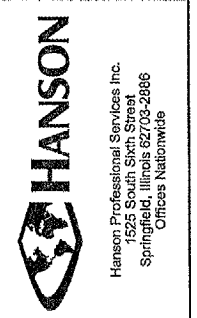
- [Solid line] EXISTING PAVEMENT
- [Hatched area] PROPOSED BITUMINOUS PAVEMENT MILLING
- [Cross-hatched area] PROPOSED PAVEMENT REMOVAL & REPLACEMENT
- [Dashed line] PROPOSED BITUMINOUS SAW CUT
- [Circle with dot] EXISTING MANHOLE (TO BE ADJUSTED)
- [Open circle] EXISTING STAKE MOUNTED TAXIWAY LIGHT

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| IEL Project No. | 803-056PND   |
| Plan Name       | R-111.PRP    |
| Scale           | 1"=30'       |
| Date            | 02/09/05     |
| LAYOUT          | JMB 02/09/05 |
| DRAWN           | JMB 02/09/05 |
| REVIEWED        | CAH ??/??/?? |



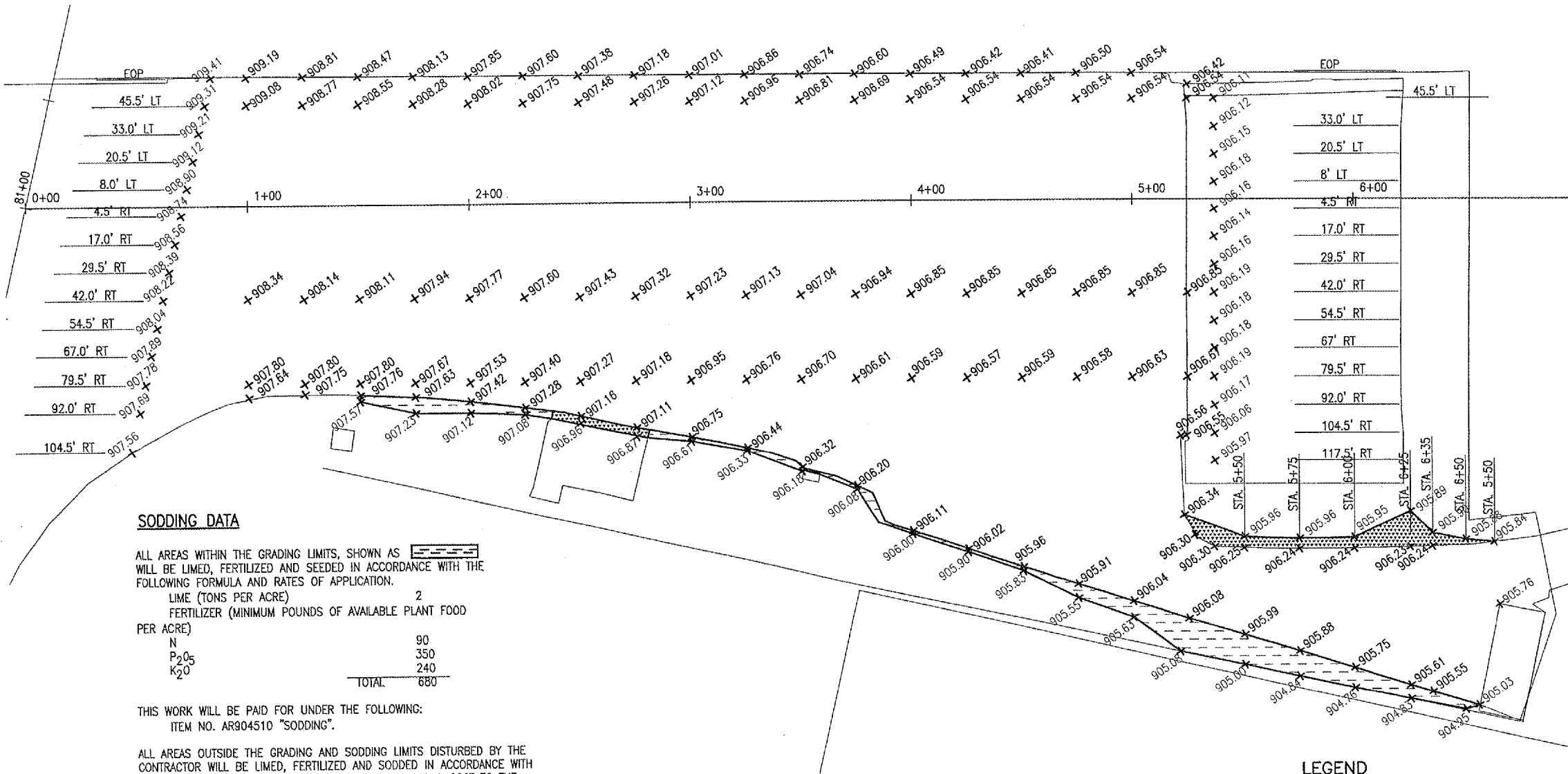
RECONSTRUCT WEST APRON  
PROPOSED PAVEMENT PREPARATION PLAN

**SHOULDER ADJUSTMENT NOTES**

WHERE THE PROPOSED PAVEMENT IS ADJACENT TO TURFED AREAS, EARTH SHOULDERS WILL BE CONSTRUCTED. THE EARTH SHOULDERS ARE SHOWN ON THE CROSS-SECTION SHEETS AS THE PROPOSED GRADING LIMITS. THE EARTH SHOULDERS WILL NOT REQUIRE COMPACTING, OTHER THAN LIGHT ROLLING AND SHAPING. THE MATERIAL FOR THE PROPOSED EARTH SHOULDERS WILL BE OBTAINED FROM OFF-SITE.

WHERE THE PROPOSED PAVEMENT IS ADJACENT TO A GRAVEL AREA, CRUSHED AGGREGATES SHOULDERS WILL BE CONSTRUCTED. THE AGGREGATE SHOULDERS ARE SHOWN ON THE PROPOSED CROSS-SECTION SHEETS AS THE PROPOSED GRADING LIMITS. THE AGGREGATE SHOULDERS WILL NOT REQUIRE COMPACTING, OTHER THAN LIGHT ROLLING AND SHAPING.

THE MATERIAL FOR THE PROPOSED SHOULDERS WILL BE PAID FOR UNDER:  
ITEM AR152480 "SHOULDER ADJUSTMENT" PER S.Y.



**SODDING DATA**

ALL AREAS WITHIN THE GRADING LIMITS, SHOWN AS [hatched symbol] WILL BE LIMED, FERTILIZED AND SEEDED IN ACCORDANCE WITH THE FOLLOWING FORMULA AND RATES OF APPLICATION.

|  |            |
|--|------------|
| LIME (TONS PER ACRE)   | 2          |
| FERTILIZER (MINIMUM POUNDS OF AVAILABLE PLANT FOOD PER ACRE) |            |
| N  | 90         |
| P <sub>2</sub> O <sub>5</sub>                                | 350        |
| K <sub>2</sub> O   | 240        |
| <b>TOTAL</b>   | <b>680</b> |

THIS WORK WILL BE PAID FOR UNDER THE FOLLOWING:  
ITEM NO. AR904510 "SODDING".

ALL AREAS OUTSIDE THE GRADING AND SODDING LIMITS DISTURBED BY THE CONTRACTOR WILL BE LIMED, FERTILIZED AND SODDED IN ACCORDANCE WITH THE REQUIREMENTS OF ITEM AR904510, AT NO ADDITIONAL COST TO THE CONTRACT.

THE SOIL SHALL BE THOROUGHLY LOOSENED TO A DEPTH NOT LESS THAN 5 INCHES.

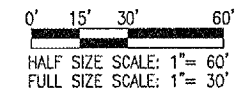
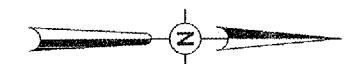
THE SOD BED SHALL BE SMOOTH AND TO GRADE PRIOR TO THE SODDING OPERATIONS. THE CONTRACTOR WILL USE DRAG BARS, HARROWS OR OTHER EQUIPMENT NECESSARY TO OBTAIN A SMOOTH GRADE TO THE SATISFACTION OF THE ENGINEER.

ALL ROCK, ASPHALT OR CONCRETE DEBRIS LEFT FROM THE PAVING OPERATION WILL BE DISPOSED OF OFF THE AIRPORT SITE.

THE PRIME CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATION OF ALL SUB-CONTRACTORS AS TO THEIR RESPONSIBILITIES PERTAINING TO THE SODDING OPERATIONS.

**LEGEND**

- [Solid line] EXISTING PAVEMENT
- [Dashed line] PROPOSED IMPROVEMENTS
- [Hatched pattern] PROPOSED EARTH SHOULDERS
- [Cross-hatched pattern] PROPOSED AGGREGATE SHOULDERS
- +900.00 PROPOSED ELEVATION
- +900.00 EXISTING ELEVATION



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**DIMA**  
DEKALB TAYLOR MUNICIPAL AIRPORT  
ILL. PROJ.: DKB-3467

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| HEL. Project No. 803-05APND |  |
| Filename R-191STK           |  |
| Scale 1"=30'                |  |
| Date 02/18/05               |  |
| LAYOUT JMB 02/18/05         |  |
| DRAWN JMB 02/18/05          |  |
| REVIEWED CAH ?              |  |

**HANSON**  
Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62703-2886  
Offices Nationwide

RECONSTRUCT WEST APRON  
PROPOSED STAKING PLAN

**TIE DOWN REMOVAL NOTES**

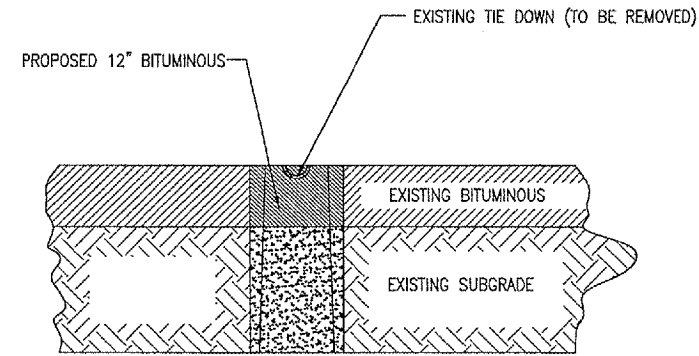
THE EXISTING TIE DOWNS LOCATED IN THE AREAS TO BE OVERLAID WILL BE REMOVED AND DISPOSED OF OFF THE AIRPORT SITE.

THE CONTRACTOR WILL REMOVE THE EXISTING CONCRETE TO A MINIMUM DEPTH OF 1' BELOW EXISTING GRADE. THE EXISTING METAL TIE DOWNS WILL BE CUT OFF AND REMOVED TO A DEPTH OF AT LEAST 1 FT BELOW EXISTING GRADE. REMOVAL OF THE TIE DOWN WILL NOT CAUSE ANY DISPLACEMENT OF THE SURROUNDING PAVEMENT. ANY DISPLACEMENT WILL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

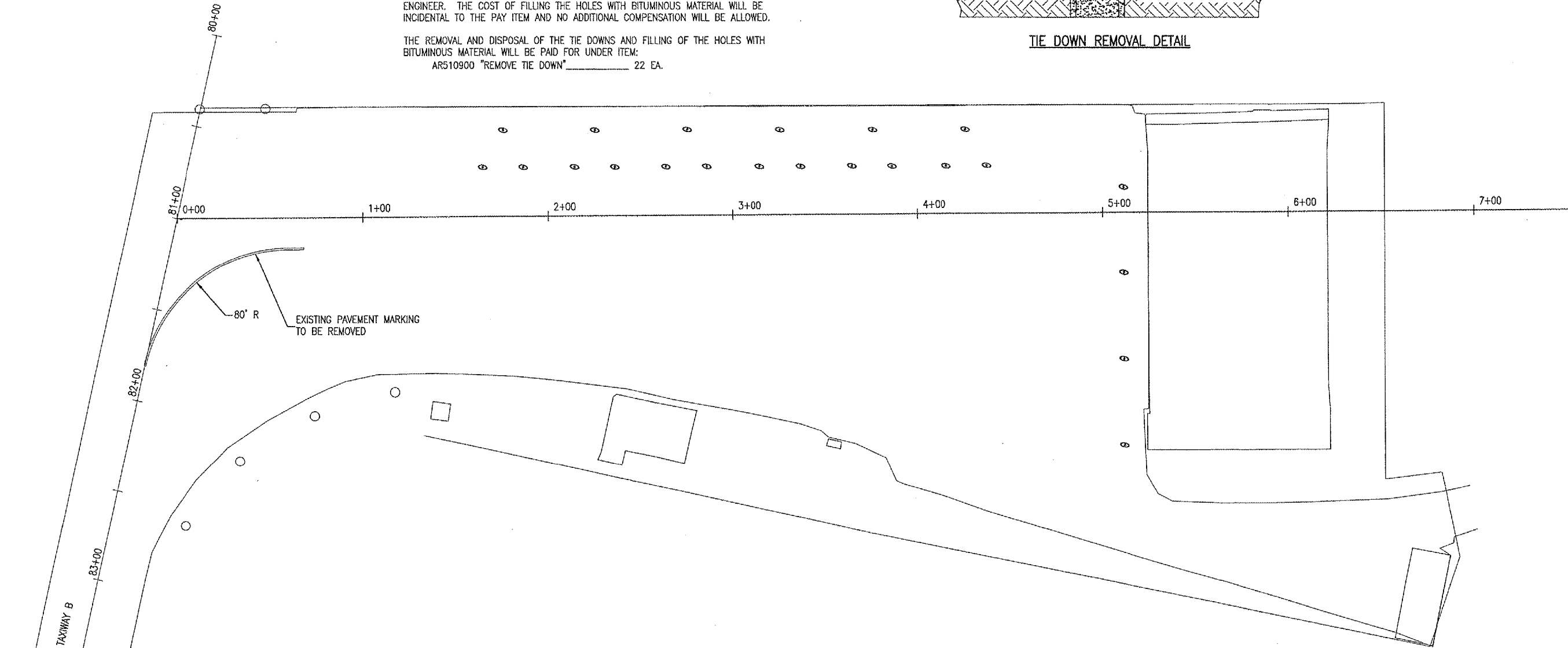
THE CONTRACTOR WILL FILL THE HOLE WITH AN APPROVED BITUMINOUS MIX WITH MAXIMUM 3" LIFTS. THE BITUMINOUS MATERIAL WILL BE COMPACTED TO THE SATISFACTION OF THE ENGINEER. THE COST OF FILLING THE HOLES WITH BITUMINOUS MATERIAL WILL BE INCIDENTAL TO THE PAY ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE REMOVAL AND DISPOSAL OF THE TIE DOWNS AND FILLING OF THE HOLES WITH BITUMINOUS MATERIAL WILL BE PAID FOR UNDER ITEM:

AR510900 "REMOVE TIE DOWN" \_\_\_\_\_ 22 EA.



**TIE DOWN REMOVAL DETAIL**



**MARKING REMOVAL NOTES**

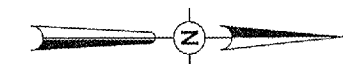
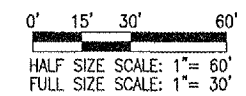
THE EXISTING PAVEMENT MARKING SHALL BE REMOVED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED ON PAGE 77 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE AREAS THAT ARE DESIGNATED FOR REMOVAL ARE SHOWN ON THE CONSTRUCTION PLANS.

ALL AREAS TO BE REMOVED ARE CALCULATED AREAS. ANY ADDITIONAL AREAS, DUE TO OVER SPRAY, SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE PROPOSED MARKING REMOVAL SHALL BE PAID FOR UNDER ITEM:

AR620900 "PAVEMENT MARKING REMOVAL" \_\_\_\_\_ 116 S.F.



**LEGEND**

- EXISTING PAVEMENT
- EXISTING MARKING (TO BE REMOVED)
- EXISTING TIE DOWN (TO BE REMOVED)
- EXISTING STAKE MOUNTED TAXIWAY LIGHT

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| <b>DIMA</b>  | <b>DEKALB TAYLOR MUNICIPAL AIRPORT</b>   |  |  |  |  |
| IL. PROJ.: DKB-3467  |  |  |  |  |  |
| HEL Project No. 803-05APND<br>Filename R-151MRK<br>Scale 1"=30'<br>Date 02/09/05 | LAYOUT JMB 02/09/05<br>DRAWN JMB 02/09/05<br>REVIEWED CAH ??/?/?   |  |  |  |  |
| <b>HANSON</b>  | Hanson Professional Services Inc.<br>1025 South Skyway<br>Springfield, Illinois 62703-2888<br>Offices Nationwide |  |  |  |  |
| <b>RECONSTRUCT<br/>WEST APRON</b>  | <b>EXISTING<br/>MARKING AND TIE DOWN<br/>PLAN</b>  |  |  |  |  |
| <b>8</b>   |  |  |  |  |  |
| 8 of 21 sheets   |  |  |  |  |  |



**TIE DOWN NOTES**

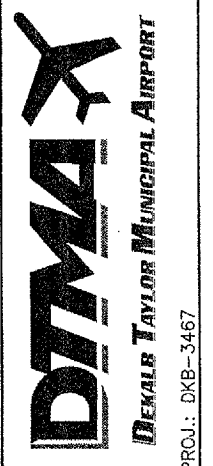
NEW TIE DOWNS WILL BE INSTALLED AT THE LOCATIONS SHOWN ON THIS SHEET IN ACCORDANCE WITH THE DETAILS ON THE FOLLOWING SHEET.

THE PERIMETER OF EACH PROPOSED TIE DOWN SHALL BE SAWED. TIE DOWN CASTINGS SHALL BE NEENAH R-3490A OR EQUAL.

CONCRETE SHALL MEET THE REQUIREMENTS OF 610 - STRUCTURAL CONCRETE.

THE PROPOSED TIE DOWN MARKING WILL BE IN ACCORDANCE WITH THE DETAIL ON THE FOLLOWING SHEET AND THE SPECIFICATIONS.

THE TIE DOWNS WILL BE PAID FOR UNDER ITEM:  
AR510510 "TIE DOWN" \_\_\_\_\_ 36 EA.

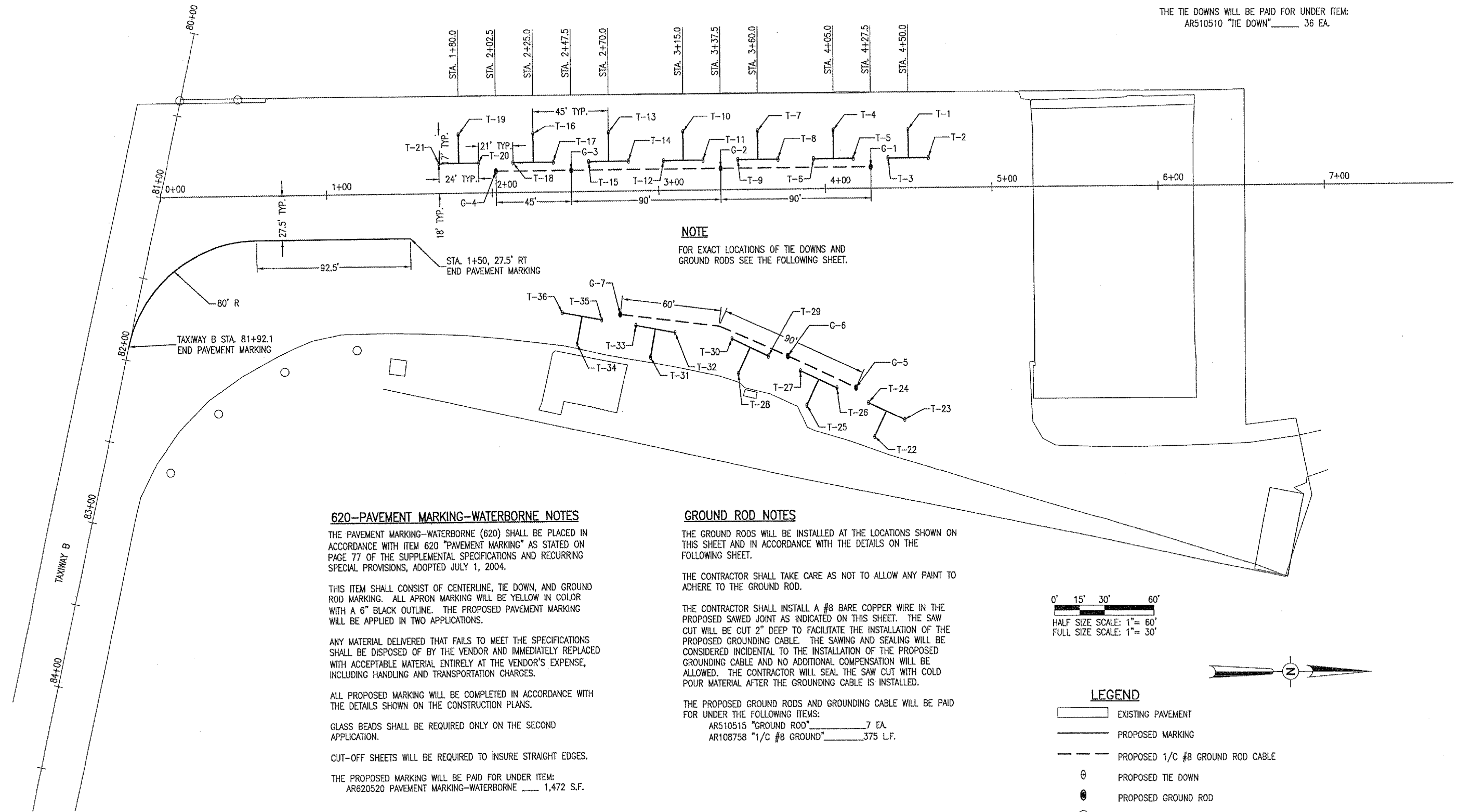


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| HEI Project No. | 803-05APND   |
| Filename        | R-152MRK     |
| Scale           | 1" = 30'     |
| Date            | 02/09/05     |
| LAYOUT          | JMB 02/09/05 |
| DRAWN           | JMB 02/09/05 |
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RECONSTRUCT WEST APRON  
PROPOSED MARKING AND TIE DOWN PLAN

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**NOTE**  
FOR EXACT LOCATIONS OF TIE DOWNS AND GROUND RODS SEE THE FOLLOWING SHEET.

**620-PAVEMENT MARKING-WATERBORNE NOTES**

THE PAVEMENT MARKING-WATERBORNE (620) SHALL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED ON PAGE 77 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM SHALL CONSIST OF CENTERLINE, TIE DOWN, AND GROUND ROD MARKING. ALL APRON MARKING WILL BE YELLOW IN COLOR WITH A 6" BLACK OUTLINE. THE PROPOSED PAVEMENT MARKING WILL BE APPLIED IN TWO APPLICATIONS.

ANY MATERIAL DELIVERED THAT FAILS TO MEET THE SPECIFICATIONS SHALL BE DISPOSED OF BY THE VENDOR AND IMMEDIATELY REPLACED WITH ACCEPTABLE MATERIAL ENTIRELY AT THE VENDOR'S EXPENSE, INCLUDING HANDLING AND TRANSPORTATION CHARGES.

ALL PROPOSED MARKING WILL BE COMPLETED IN ACCORDANCE WITH THE DETAILS SHOWN ON THE CONSTRUCTION PLANS.

GLASS BEADS SHALL BE REQUIRED ONLY ON THE SECOND APPLICATION.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

THE PROPOSED MARKING WILL BE PAID FOR UNDER ITEM:  
AR620520 PAVEMENT MARKING-WATERBORNE \_\_\_\_\_ 1,472 S.F.

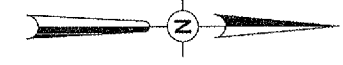
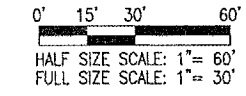
**GROUND ROD NOTES**

THE GROUND RODS WILL BE INSTALLED AT THE LOCATIONS SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE DETAILS ON THE FOLLOWING SHEET.

THE CONTRACTOR SHALL TAKE CARE AS NOT TO ALLOW ANY PAINT TO ADHERE TO THE GROUND ROD.

THE CONTRACTOR SHALL INSTALL A #8 BARE COPPER WIRE IN THE PROPOSED SAWED JOINT AS INDICATED ON THIS SHEET. THE SAW CUT WILL BE CUT 2" DEEP TO FACILITATE THE INSTALLATION OF THE PROPOSED GROUNDING CABLE. THE SAWING AND SEALING WILL BE CONSIDERED INCIDENTAL TO THE INSTALLATION OF THE PROPOSED GROUNDING CABLE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CONTRACTOR WILL SEAL THE SAW CUT WITH COLD POUR MATERIAL AFTER THE GROUNDING CABLE IS INSTALLED.

THE PROPOSED GROUND RODS AND GROUNDING CABLE WILL BE PAID FOR UNDER THE FOLLOWING ITEMS:  
AR510515 "GROUND ROD" \_\_\_\_\_ 7 EA.  
AR108758 "1/C #8 GROUND" \_\_\_\_\_ 375 L.F.



**LEGEND**

- EXISTING PAVEMENT
- PROPOSED MARKING
- PROPOSED 1/C #8 GROUND ROD CABLE
- PROPOSED TIE DOWN
- PROPOSED GROUND ROD
- EXISTING STAKE MOUNTED LIGHT

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**DIMAX**  
 DEKALB TAYLOR MUNICIPAL AIRPORT  
 IL. PROJ.: DKB-3467

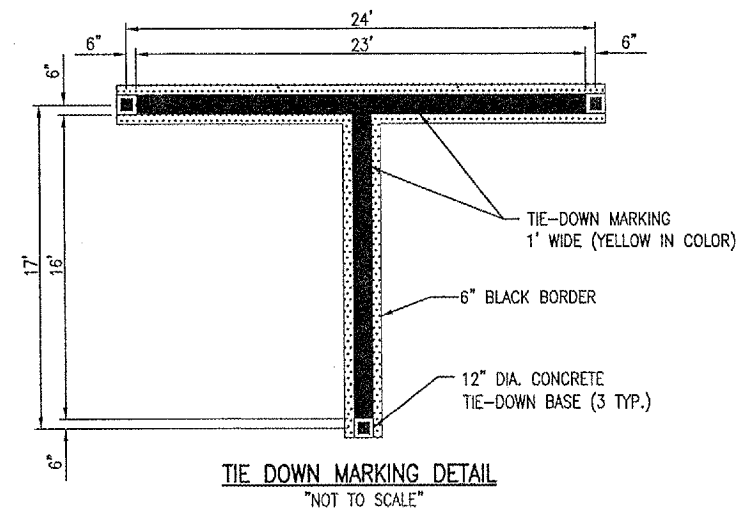
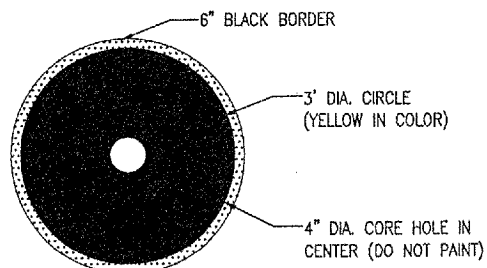
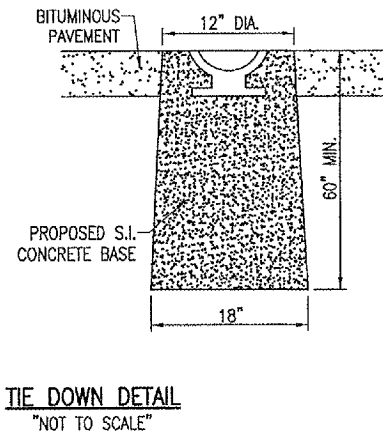
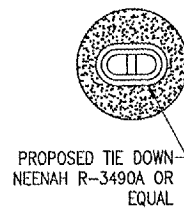
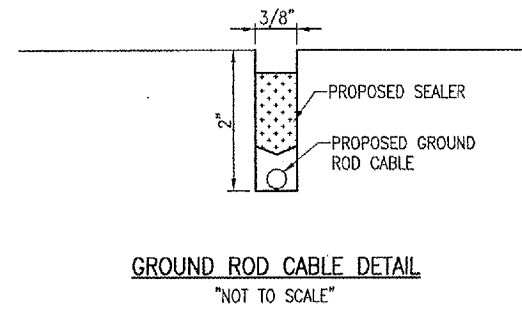
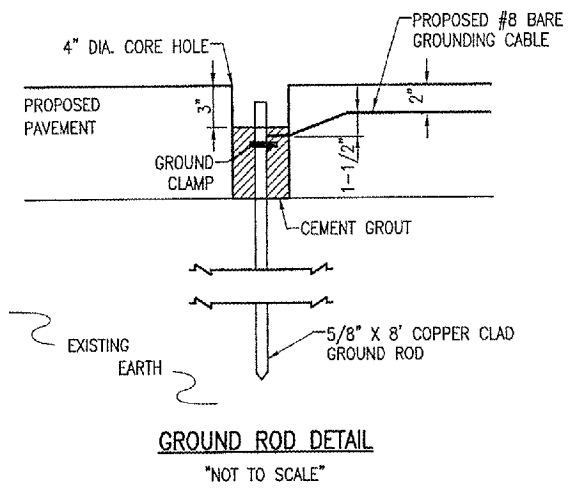
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| HEL Project No. | 803-05APND   |
| Filename        | R=552MRK     |
| Scale           | NOT TO SCALE |
| Date            | 02/10/05     |
| LAYOUT          | JMB 02/10/05 |
| DRAWN           | JMB 02/10/05 |
| REVIEWED        | CAH ?        |

**HANSON**  
 Hanson Professional Services Inc.  
 4525 South Sixth Street  
 Springfield, Illinois 62709-2886  
 Offices Nationwide

RECONSTRUCT WEST APRON  
 PROPOSED MARKING AND TIE DOWN DETAILS

TIE DOWN AND GROUND ROD LOCATIONS

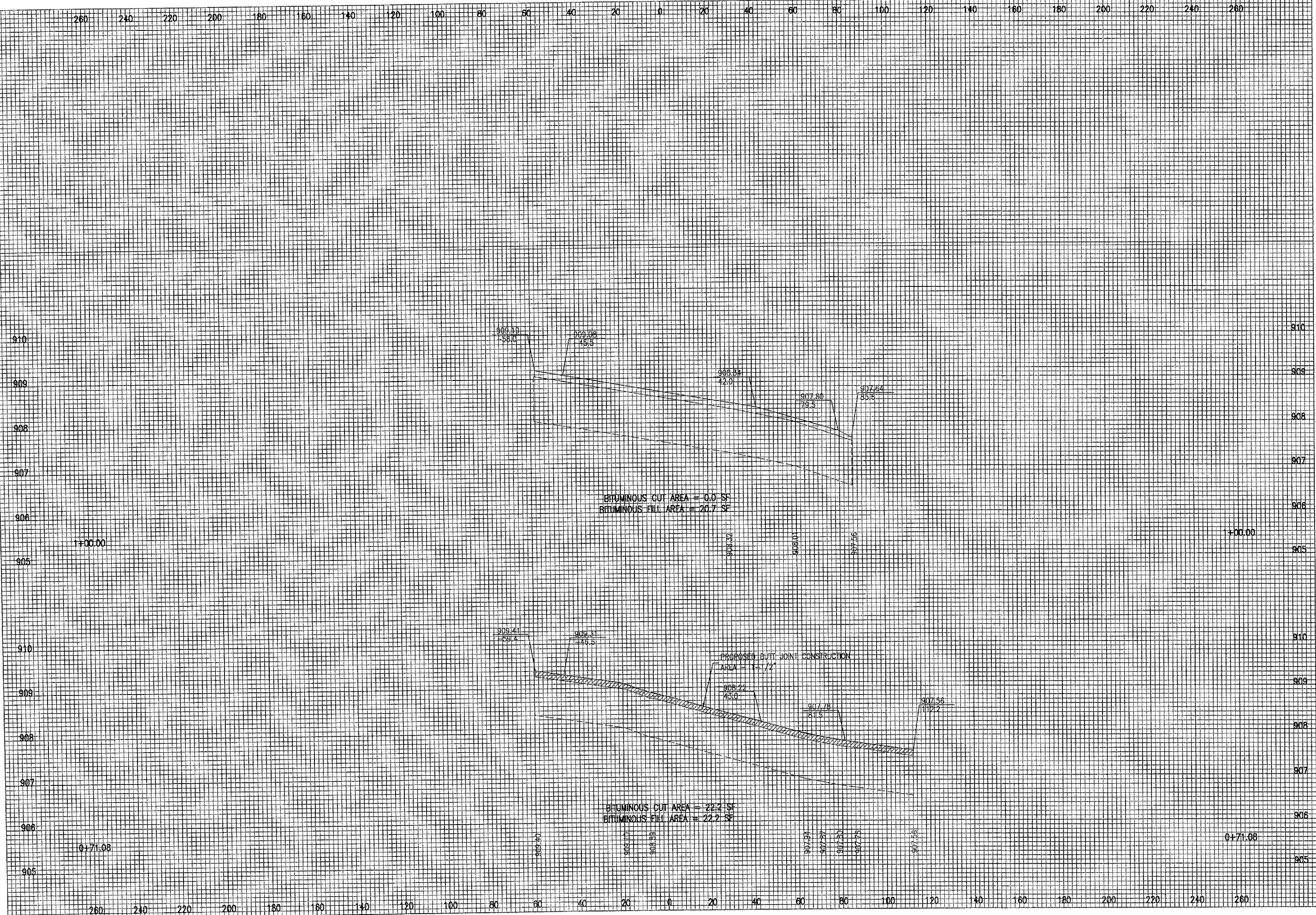
| POINT | DESCRIPTION | STATION | OFFSET     | NORTHING     | EASTING     |
|-------|-------------|---------|------------|--------------|-------------|
| T-1   | TIE DOWN    | 4+50.00 | 35.00' LT  | 1917711.8084 | 877626.8118 |
| T-2   | TIE DOWN    | 4+62.00 | 18.00' LT  | 1917727.2306 | 877640.7817 |
| T-3   | TIE DOWN    | 4+38.00 | 18.00' LT  | 1917703.8096 | 877646.0217 |
| T-4   | TIE DOWN    | 4+05.00 | 35.00' LT  | 1917667.8941 | 877636.6369 |
| T-5   | TIE DOWN    | 4+17.00 | 18.00' LT  | 1917683.3163 | 877650.6068 |
| T-6   | TIE DOWN    | 3+93.00 | 18.00' LT  | 1917659.8953 | 877655.8468 |
| T-7   | TIE DOWN    | 3+60.00 | 35.00' LT  | 1917623.9798 | 877646.4620 |
| T-8   | TIE DOWN    | 3+72.00 | 18.00' LT  | 1917639.4020 | 877660.4319 |
| T-9   | TIE DOWN    | 3+48.00 | 18.00' LT  | 1917615.9810 | 877665.6719 |
| T-10  | TIE DOWN    | 3+15.00 | 35.00' LT  | 1917580.0655 | 877656.2871 |
| T-11  | TIE DOWN    | 3+27.00 | 18.00' LT  | 1917595.4876 | 877670.2569 |
| T-12  | TIE DOWN    | 3+03.00 | 18.00' LT  | 1917572.0667 | 877675.4970 |
| T-13  | TIE DOWN    | 2+70.00 | 35.00' LT  | 1917536.1511 | 877666.1122 |
| T-14  | TIE DOWN    | 2+82.00 | 18.00' LT  | 1917551.5733 | 877680.0820 |
| T-15  | TIE DOWN    | 2+58.00 | 18.00' LT  | 1917528.1524 | 877685.3221 |
| T-16  | TIE DOWN    | 2+25.00 | 35.00' LT  | 1917492.2368 | 877675.9373 |
| T-17  | TIE DOWN    | 2+37.00 | 18.00' LT  | 1917507.6590 | 877689.9071 |
| T-18  | TIE DOWN    | 2+13.00 | 18.00' LT  | 1917484.2380 | 877695.1472 |
| T-19  | TIE DOWN    | 1+80.00 | 35.00' LT  | 1917448.3225 | 877685.7624 |
| T-20  | TIE DOWN    | 1+92.00 | 18.00' LT  | 1917463.7447 | 877699.7322 |
| T-21  | TIE DOWN    | 1+68.00 | 18.00' LT  | 1917440.3237 | 877704.9723 |
| T-22  | TIE DOWN    | 4+28.00 | 149.66' RT | 1917730.6529 | 877811.8240 |
| T-23  | TIE DOWN    | 4+46.16 | 139.51' RT | 1917746.1591 | 877797.9475 |
| T-24  | TIE DOWN    | 4+24.51 | 129.15' RT | 1917722.7702 | 877792.5862 |
| T-25  | TIE DOWN    | 3+87.40 | 130.24' RT | 1917686.7986 | 877801.7340 |
| T-26  | TIE DOWN    | 4+05.56 | 120.09' RT | 1917702.3049 | 877787.8575 |
| T-27  | TIE DOWN    | 3+83.91 | 109.73' RT | 1917678.9160 | 877782.4762 |
| T-28  | TIE DOWN    | 3+46.81 | 110.82' RT | 1917642.9444 | 877791.6440 |
| T-29  | TIE DOWN    | 3+64.89 | 110.62' RT | 1917658.3806 | 877777.7468 |
| T-30  | TIE DOWN    | 3+43.24 | 90.27' RT  | 1917634.9717 | 877772.3655 |
| T-31  | TIE DOWN    | 2+93.92 | 100.48' RT | 1917589.0773 | 877793.0993 |
| T-32  | TIE DOWN    | 3+08.86 | 86.15' RT  | 1917600.5215 | 877775.8538 |
| T-33  | TIE DOWN    | 2+85.13 | 81.40' RT  | 1917576.5269 | 877776.3606 |
| T-34  | TIE DOWN    | 2+49.71 | 92.09' RT  | 1917544.1012 | 877794.5634 |
| T-35  | TIE DOWN    | 2+64.65 | 77.76' RT  | 1917555.5453 | 877777.3179 |
| T-36  | TIE DOWN    | 2+41.12 | 73.01' RT  | 1917531.5507 | 877777.8247 |
| G-1   | GROUND ROD  | 4+27.50 | 13.00' LT  | 1917694.6546 | 877653.1936 |
| G-2   | GROUND ROD  | 3+37.50 | 13.00' LT  | 1917606.8260 | 877672.8438 |
| G-3   | GROUND ROD  | 2+47.50 | 13.00' LT  | 1917518.9974 | 877692.4940 |
| G-4   | GROUND ROD  | 2+02.50 | 13.00' LT  | 1917475.0830 | 877702.3191 |
| G-5   | GROUND ROD  | 4+17.19 | 120.11' RT | 1917713.6586 | 877785.3391 |
| G-6   | GROUND ROD  | 3+76.60 | 100.69' RT | 1917669.8044 | 877775.2492 |
| G-7   | GROUND ROD  | 2+75.92 | 74.67' RT  | 1917565.8734 | 877771.8419 |



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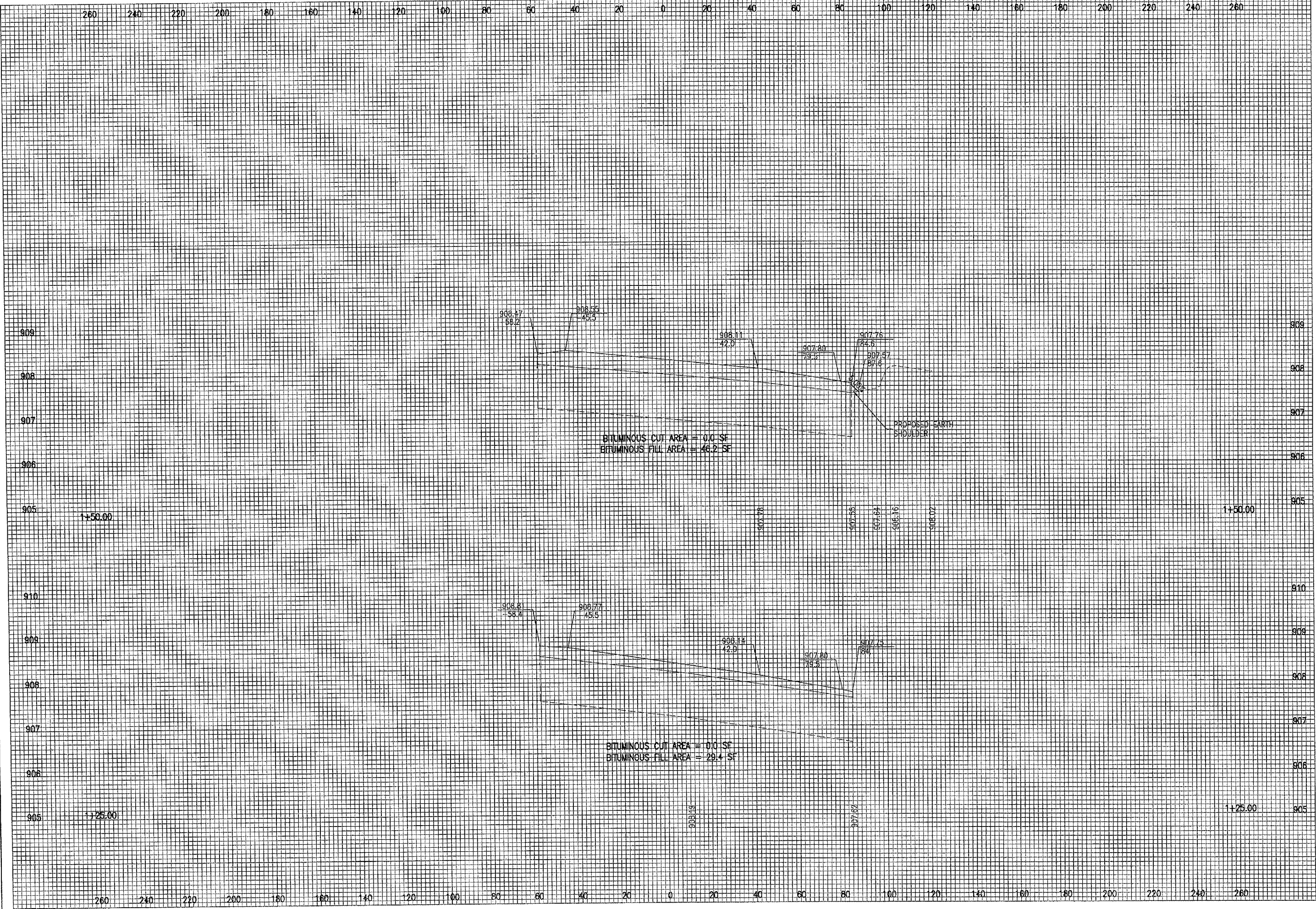
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| DRAWN           | MEW                   |
| REVIEWED        | CAH                   |
|                 | 02/23/05              |
|                 | 02/23/05              |
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RECONSTRUCT WEST APRON  
 PROPOSED CROSS-SECTIONS  
 STA. 0+71.08 TO STA. 1+00

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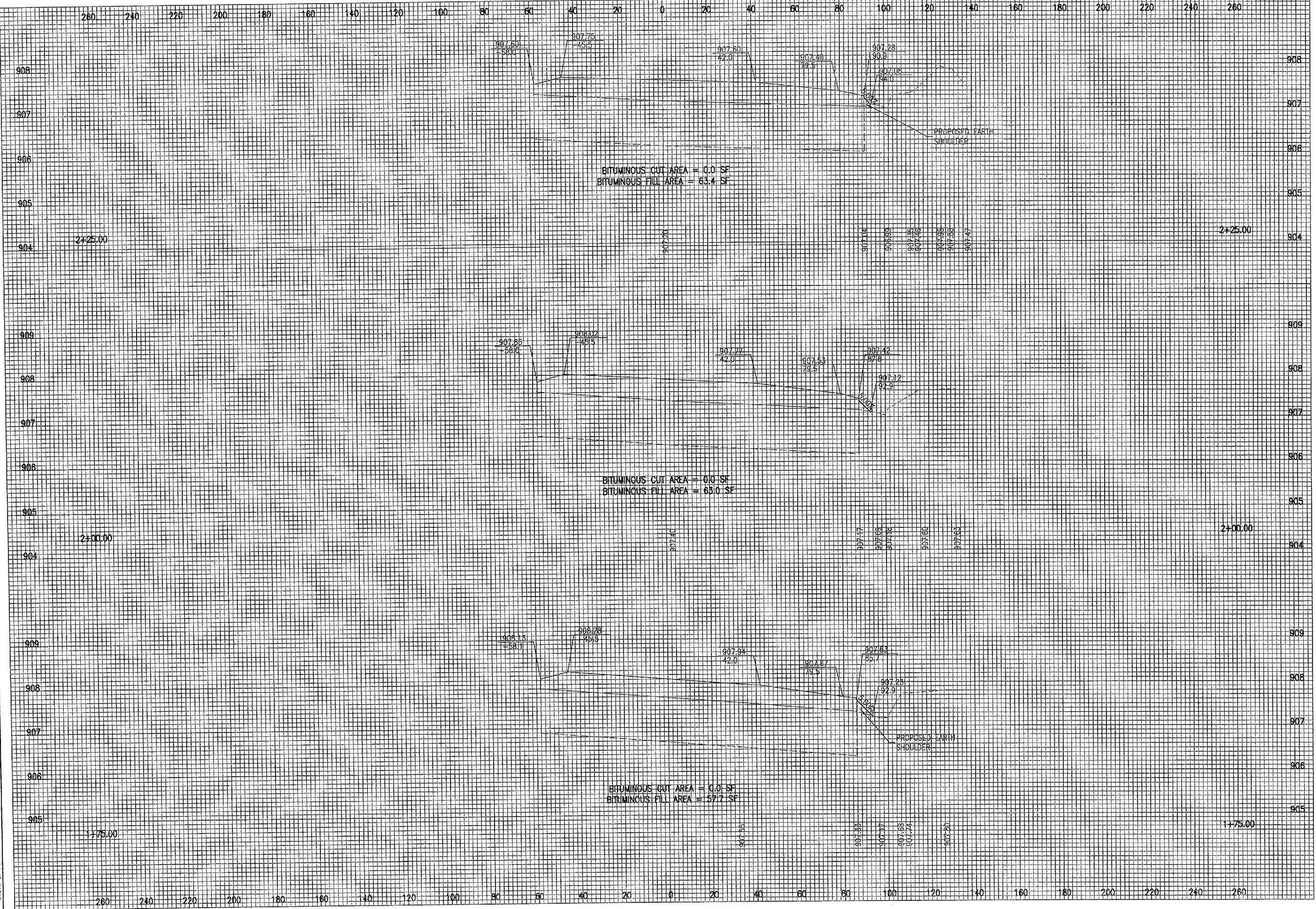
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| REVIEWED        | CAH ?                 |



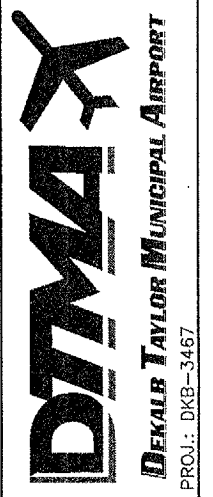
RECONSTRUCT WEST APRON  
 PROPOSED CROSS-SECTIONS  
 STA. 1+25 TO STA. 1+50

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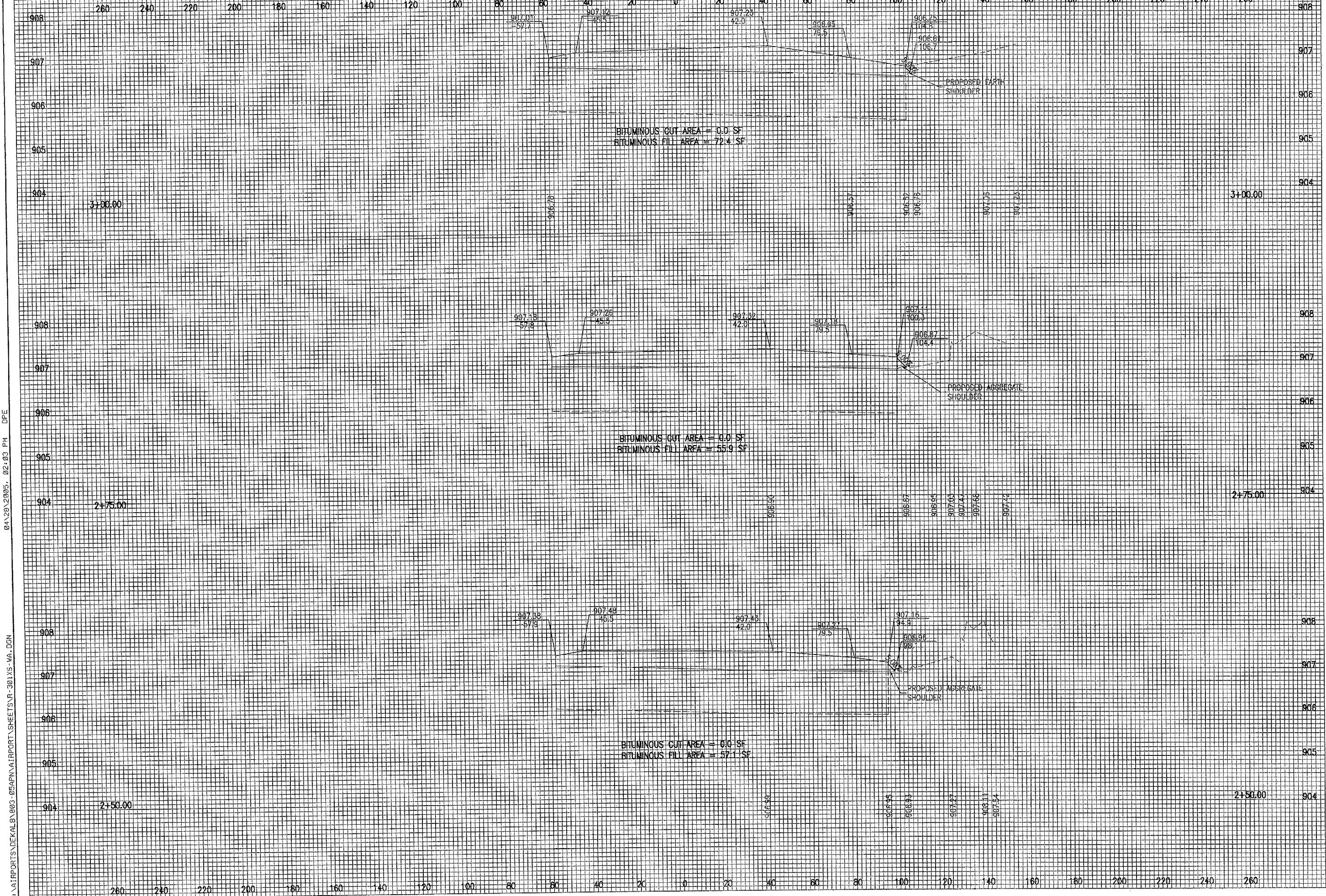
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| DRAWN           | MEW 02/23/05         |
| REVIEWED        | CAH ?                |



RECONSTRUCT WEST APRON  
 PROPOSED CROSS-SECTIONS  
 STA. 1+75 TO STA. 2+25



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**DIMA**  
**DEKALB TAYLOR MUNICIPAL AIRPORT**  
 ILL. PROJ.: DKB-3467

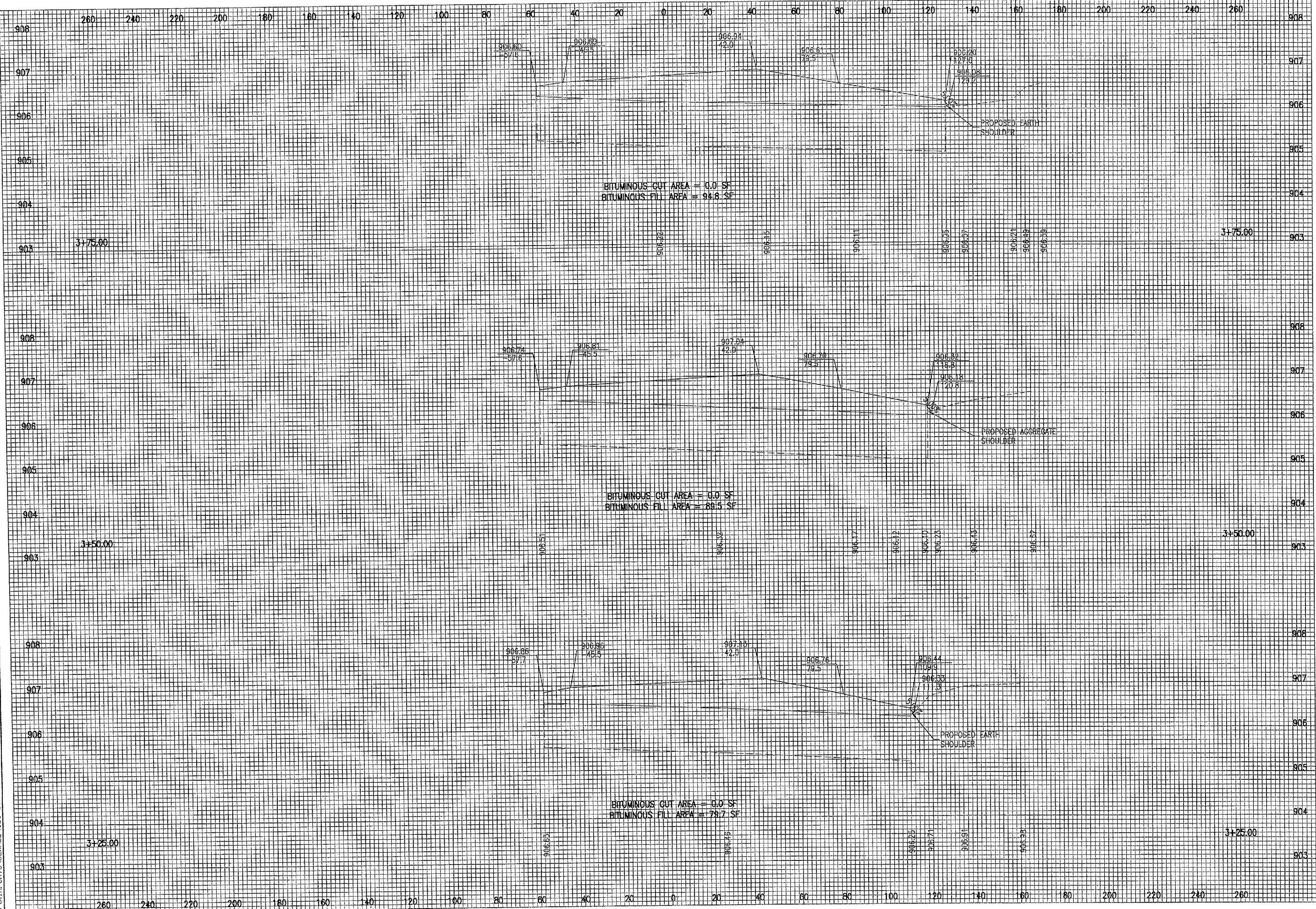
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| LAYOUT         | MEW 02/23/05         |
| DRAWN          | MEW 02/23/05         |
| REVIEWED       | CAH ?                |

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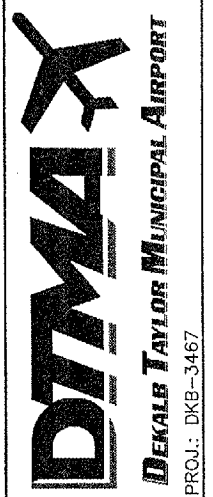
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 PROPOSED CROSS-SECTIONS  
 STA. 2+50 TO STA. 3+00

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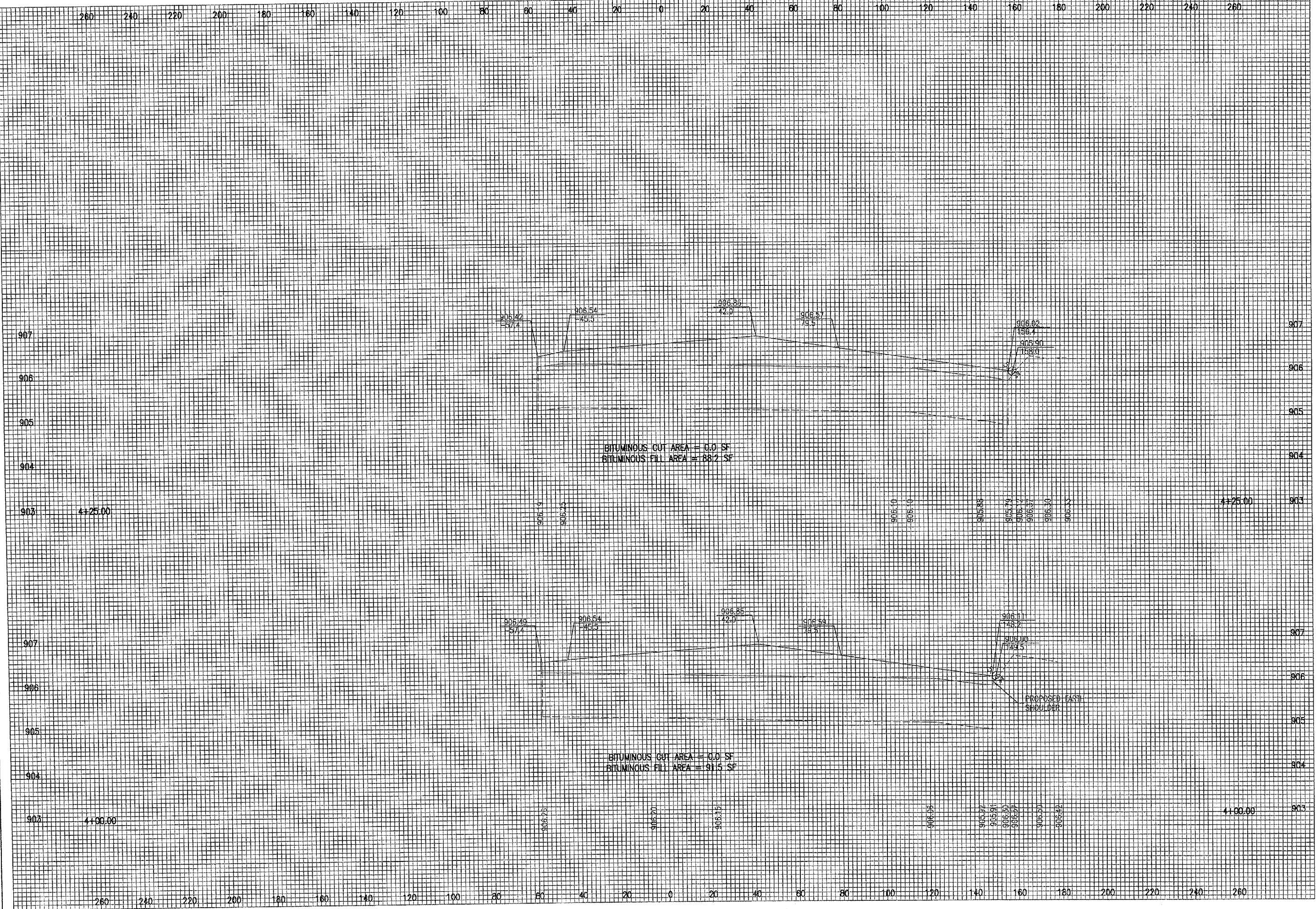
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| DRAWN          | NEW 02/23/05         |
| REVIEWED       | CAH ?                |



RECONSTRUCT WEST APRON  
 PROPOSED CROSS-SECTIONS  
 STA. 3+25 TO STA. 3+75

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**DIMAX**  
**DEKALB TAYLOR MUNICIPAL AIRPORT**  
 IL PROJ.: DKB-3467

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| DRAWN           | MEW                   |
| REVIEWED        | CAH                   |

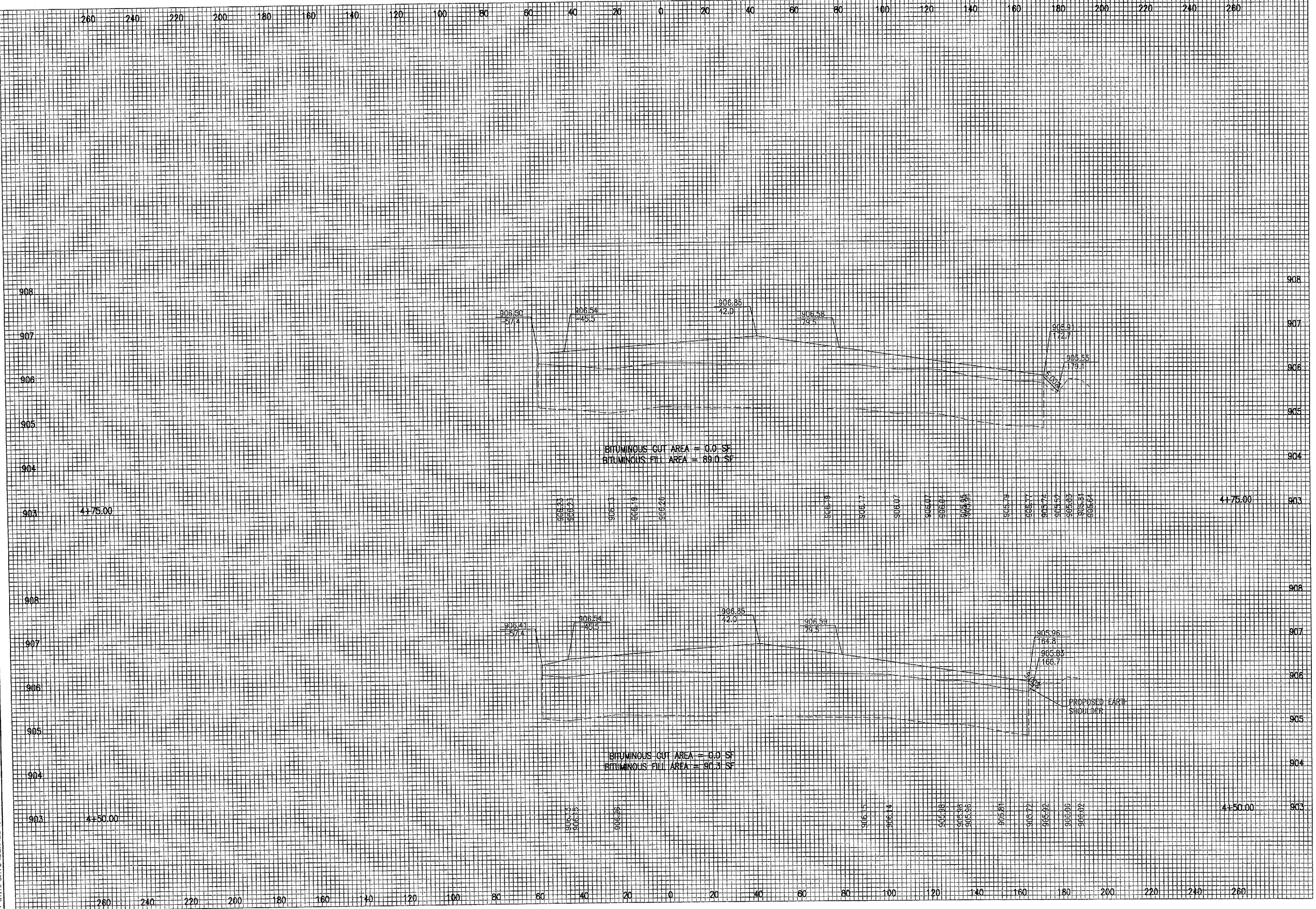
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 Hanson Professional Services Inc.  
 1805 S. State Street  
 Springfield, Illinois 62703-2886  
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**RECONSTRUCT WEST APRON**  
**PROPOSED CROSS-SECTIONS**  
 STA. 4+00 TO STA. 4+25



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ILL. PROJ.: DKB-3467

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| DRAWN                      | MEW                       | 02/23/05                | ?                         |
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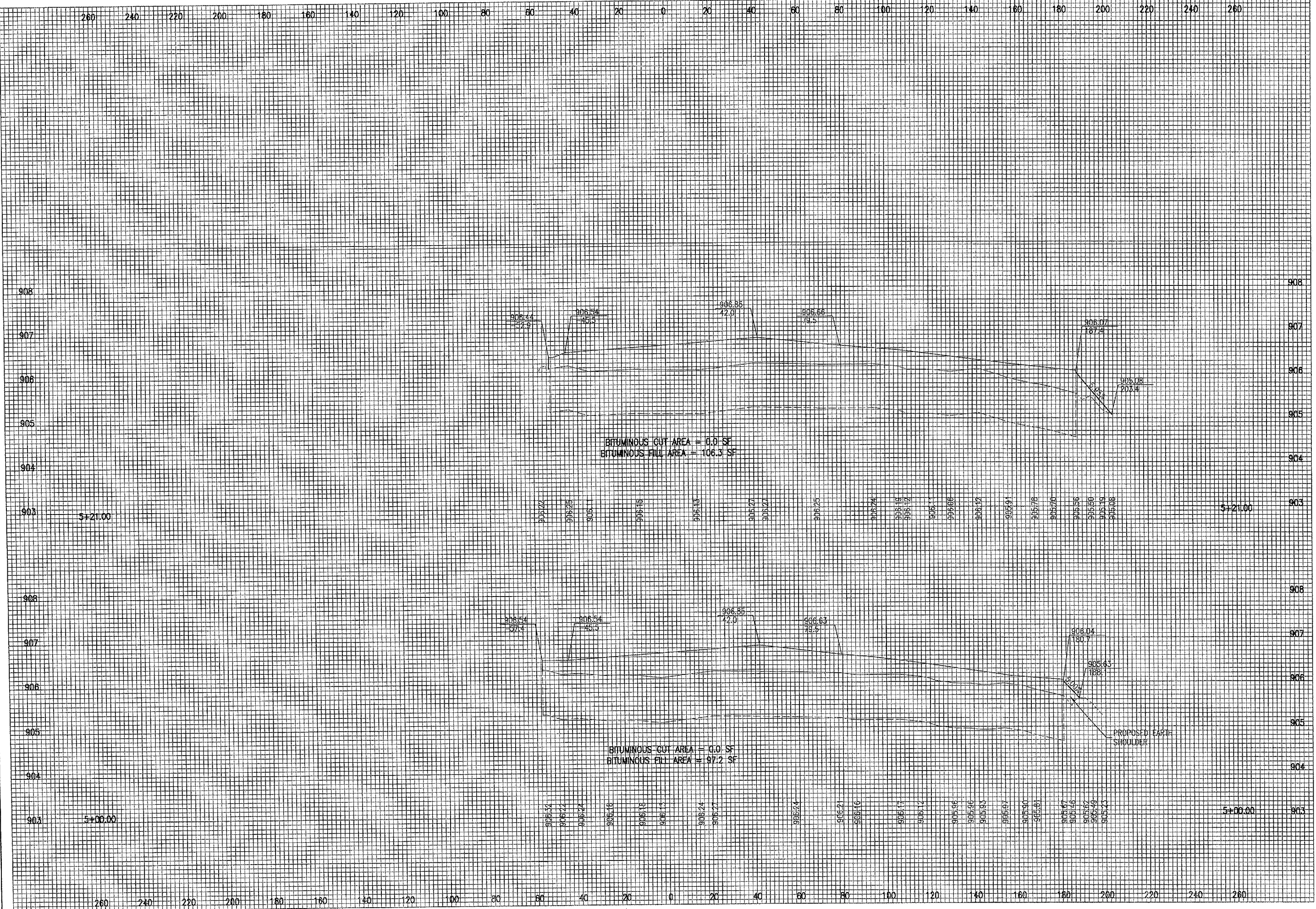


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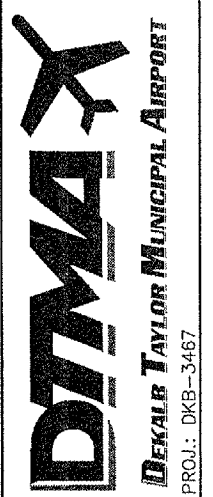
PROPOSED CROSS-SECTIONS STA. 4+50 TO STA. 4+75

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| DRAWN                      | MEW                       | 02/23/05               | ?                         |
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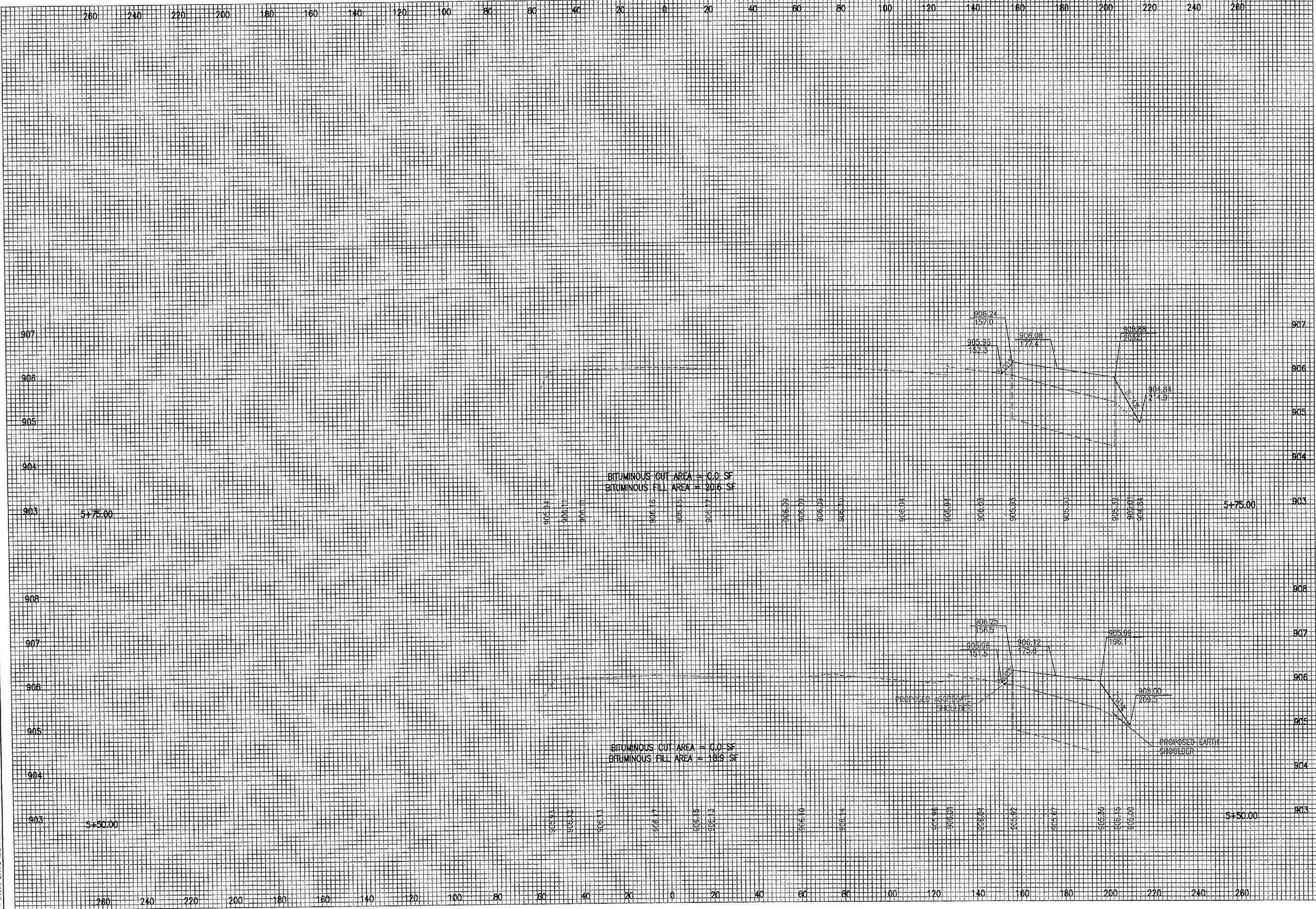


**RECONSTRUCT WEST APRON**

**PROPOSED CROSS-SECTIONS STA. 5+00 TO STA. 5+21**

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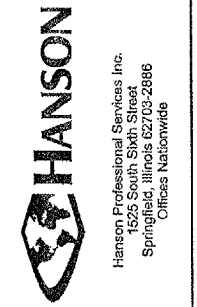
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| HEI Project No. | 803-05APND            |
| Filename        | r-301.xs-wa.dgn       |
| Scale           | 1" = 1' V, 1" = 20' H |
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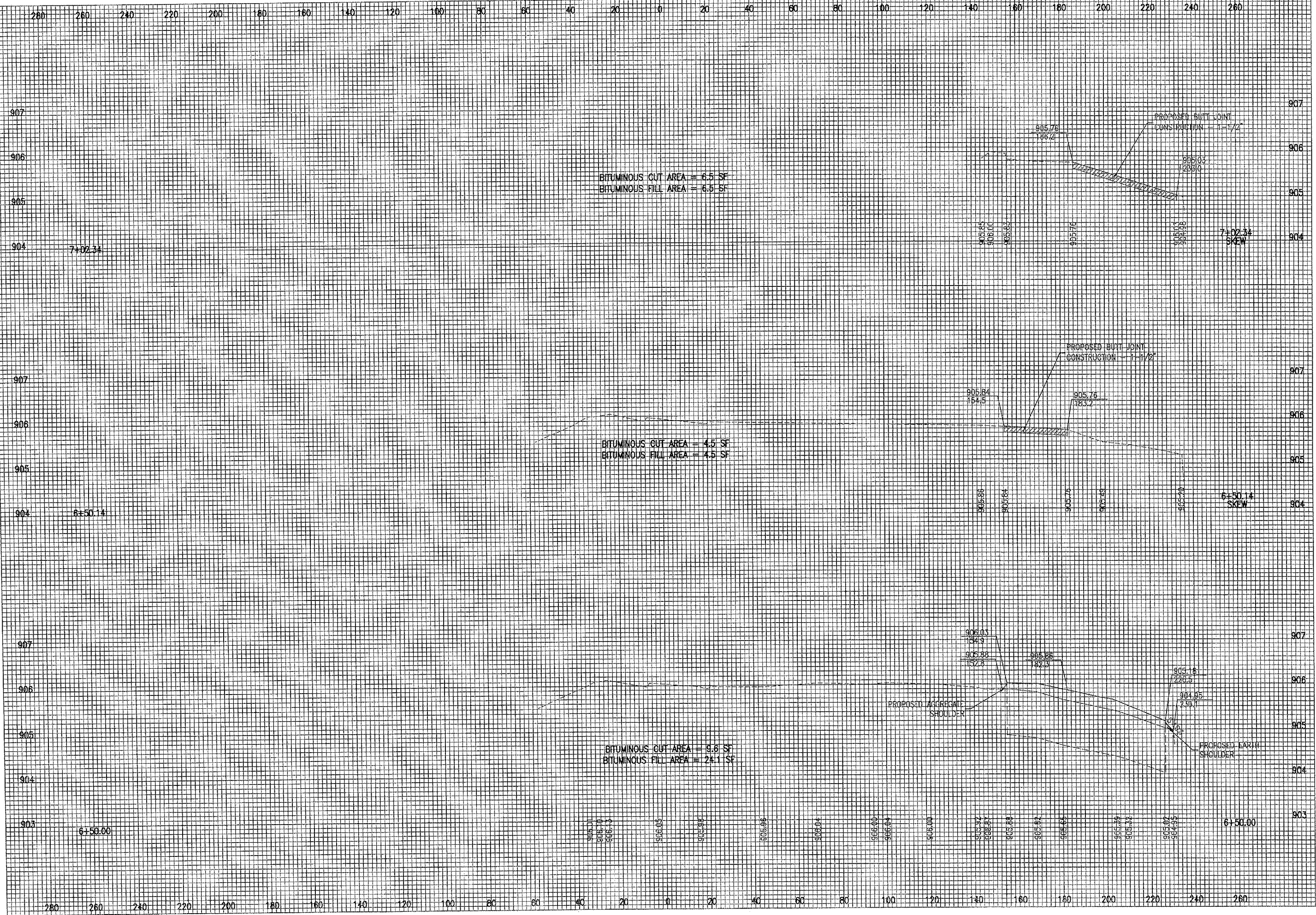


RECONSTRUCT WEST APRON  
PROPOSED CROSS-SECTIONS  
STA. 5+50 TO STA. 5+75



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**DIMA**  
**DEKALB TAYLOR MUNICIPAL AIRPORT**  
 IL PROJ.: DK9-3467

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| Date            | 04\28\2005, 02:12 PM  |
| LAYOUT          | NEW                   |
| DRAWN           | MEW                   |
| REVIEWED        | CAH                   |
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**RECONSTRUCT WEST APRON**  
 PROPOSED CROSS-SECTIONS  
 STA. 6+50 TO STA. 7+02.34