

FOR
PEKIN MUNICIPAL AIRPORT
 CITY OF PEKIN
 PEKIN - TAZEWELL COUNTY - ILLINOIS

ILLINOIS PROJECT: C15-3434
 A.I.P. PROJECT: 3-17-0078-B8

SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	BID QUANTITY	AS BUILT QUANTITY
AR152480	SHOULDER ADJUSTMENT	SY	2,850	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	1,575	
AR401655	BUTT JOINT CONSTRUCTION	SY	775	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	1,000	
AR401920	REPLACE BITUMINOUS SURFACE	TON	1,100	
AR402622	POROUS FRICTION COURSE, 0.10'	SY	20,000	
AR603510	BITUMINOUS TACK COAT	GAL	3,200	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	6,250	
AR800420	REFLECTIVE CRACK CONTROL - ISAC	LF	5,700	

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6	CONSTRUCTION ACTIVITY PLAN PHASE 2
7	CONSTRUCTION ACTIVITY PLAN PHASE 3
8	PROJECT IMPROVEMENTS SHEET 1 / STA. 12+12.50 TO STA. 38+50
9	PROJECT IMPROVEMENTS SHEET 2 / STA. 38+50 TO STA. 51+80
10	TYPICAL SECTIONS SHEET 1
11	TYPICAL SECTIONS SHEET 2
12	MARKING PLAN SHEET 1 / STA. 12+12.50 TO STA. 38+50
13	MARKING PLAN SHEET 2 / STA. 38+50 TO STA. 51+80

REHABILITATE AND MARK THE
 PARALLEL TAXIWAY INCLUDING
 CONNECTING TAXIWAYS

APRIL 22, 2005

CALL J.U.L.I.E.
 BEFORE EXCAVATING
 1-800-892-0123

PEKIN MUNICIPAL AIRPORT

TOWNSHIP: T 24 N
 RANGE: R 5 W
 COUNTY: TAZEWELL
 TOWNSHIP: CINCINNATI

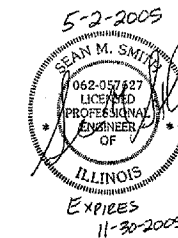
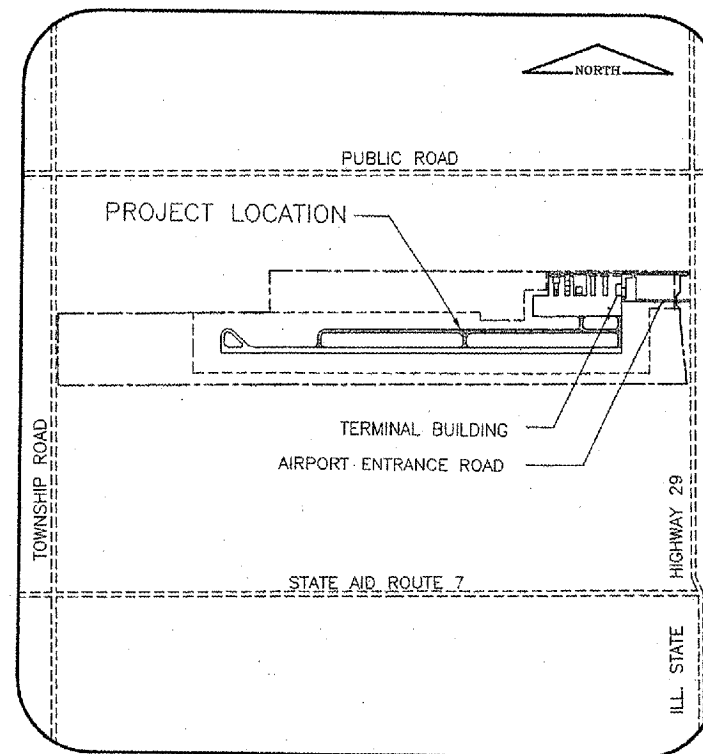
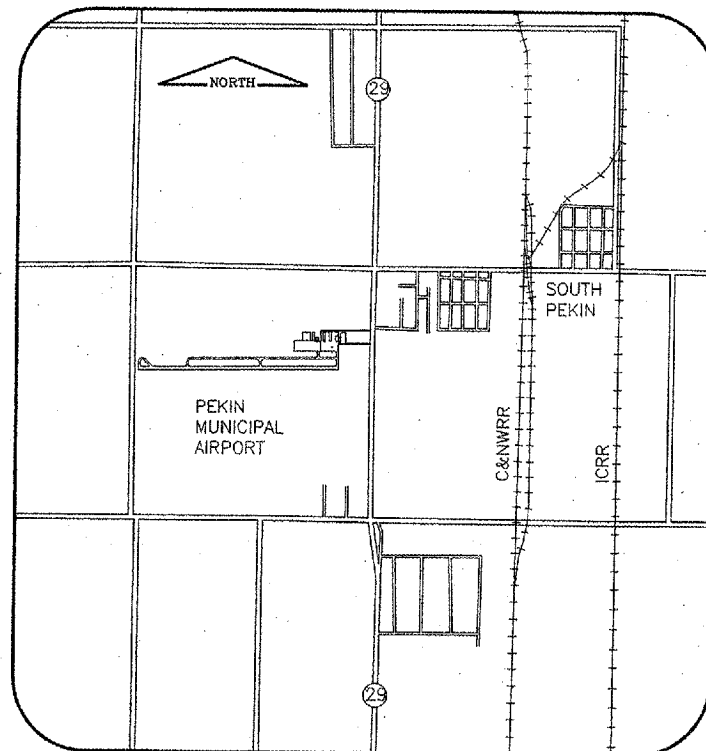
GEOMETRIC DESIGN DATA

DESIGN GROUP - GROUP II
 WINGSPAN - UP TO BUT NOT INCLUDING 79 FT.
 DESIGN APPROACH CATEGORY - B

STRUCTURAL DESIGN DATA

GROSS WEIGHT - 25,000 LBS.
 SINGLE WHEEL GEAR

MAXIMUM EQUIPMENT HEIGHT = 25 FEET



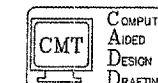
CITY OF PEKIN AIRPORT COMMISSION

APPROVED *Peter Kalman*
 DATE 4-17-05 2005

APPROVED *[Signature]*
 DATE 4-18- 2005

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SUBMITTED BY *[Signature]*
 DATE 05-02- 2005
 CM&T JOB NUMBER 04069-02-00



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 PLOTTED: 05/18/2005
 SCALE: 1" = 200'
 LAST UPDATE: SMS

GENERAL NOTES

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN OR AS DIRECTED BY THE AIRPORT MANAGER. THE UNICOM FREQUENCY IS 122.8 MEGAHERTZ.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE EITHER IN RADIO CONTACT OR UNDER CONTROL OF A FLAGMAN IN RADIO CONTACT. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE RADIO COMMUNICATION TRAINING CLASS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CLOSING AND OPENING PAVEMENTS AND CONSTRUCTION SEQUENCING LIES WITH THE FAA. MODIFICATIONS OF THE CONSTRUCTION ACTIVITY PLAN MUST BE APPROVED BY THE FAA, AND TYPICALLY REQUIRES A MINIMUM 60 DAY PROCESS.
4. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS OUTSIDE THE RUNWAY SAFETY AREA (RSAs) AND OBSTACLE FREE ZONES (OFAs). EQUIPMENT AND MATERIALS SHALL NOT BE STORED ON CLOSED RUNWAYS OR TAXIWAYS. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25 FEET. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE AIRPORT PART 77 SURFACES.
5. BROKEN OR WASTE CONCRETE (BITUMINOUS AND PCC) SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 200 FEET FROM THE CENTERLINE OF AN ACTIVE RUNWAY AND 65 FEET FROM THE CENTERLINE OF AN ACTIVE TAXIWAY.
7. ALL PAVEMENTS, DRIVES AND OTHER AREAS USED BY THE CONTRACTOR FOR HAUL ROADS AND STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT MANAGER. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF & AGRICULTURAL AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE COMPLETELY RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT MANAGER.
9. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING THE CROSSING VEHICLE.
10. REFER TO THE SEQUENCE OF CONSTRUCTION SHEETS AND THE SPECIFICATIONS FOR REQUIREMENTS CONCERNING COORDINATION OF CONSTRUCTION ACTIVITIES.
11. THE FIRE/CRASH/RESCUE VEHICLES SHALL HAVE COMPLETE ACCESS TO THE ENTIRE AIRFIELD INCLUDING THE CLOSURE AREAS.
12. THE CONTRACTOR SHALL NOTIFY THE AIRPORT MANAGER FIVE (5) FULL WORKING DAYS PRIOR TO CLOSING ANY PAVEMENTS.
13. AT THE PRECONSTRUCTION MEETING, THE CONTRACTOR SHALL SUPPLY THE AIRPORT MANAGER WITH PROPOSED CLOSURE AND PHASING DATES FOR REVIEW AND APPROVAL. THE RESIDENT ENGINEER SHALL KEEP THE AIRPORT MANAGER ADVISED OF ANY PROPOSED CHANGES IN CLOSURE AND PHASING DATES.
14. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT. THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, MEN AND EQUIPMENT.
15. CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE GUIDELINES PRESENTED IN FAA ADVISORY CIRCULAR AC 150/5370-2E OR ITS LATEST REVISION.
16. THE CONTRACTOR SHALL PROVIDE BARRICADES AT THE LOCATIONS SHOWN IN THE PLANS. BARRICADES SHALL BE AT A 15-FOOT SPACING, WITH ONE BARRICADE ON THE CENTERLINE. BARRICADES SHALL HAVE FLAGS AND LIGHTS AS SHOWN. THE BARRICADES SHALL BE LIGHTED WITH A STEADY BURNING OR FLASHING RED LIGHT.
17. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
18. WHEN ANY VEHICLE IS REQUIRED TO TRAVEL OVER ANY PORTION OF THE AIRCRAFT MOVEMENT AREA OR RUNWAY APPROACH AREA, THE VEHICLE SHALL BE PROPERLY IDENTIFIED TO OPERATE IN THE AREA.
 - A. DURING DAYLIGHT HOURS, MARK VEHICLES WITH A FLAG ON A STAFF SO ATTACHED TO THE VEHICLE SO THAT THE FLAG WILL BE READILY VISIBLE. THE FLAG SHOULD BE NOT LESS THAN 3- FEET SQUARE CONSISTING OF A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN ONE FOOT ON EACH SIDE AND DISPLAYED IN FULL VIEW ABOVE THE VEHICLE.
 - B. DURING NIGHTTIME OR LOW-VISIBILITY OPERATIONS, MARK VEHICLES WITH FLASHING YELLOW BEACONS.
 - C. MARK CONSTRUCTION VEHICLES REQUIRING AN ESCORT WITH A FLAG ON A STAFF. THE FLAG SHALL BE AS DESCRIBED ABOVE.
 - D. MARK AND IDENTIFY VEHICLES IN ACCORDANCE WITH AC 150/5210-5, PAINTING, MARKING AND LIGHTING OF VEHICLES USED ON AN AIRPORT.

CONTRACTOR'S ACCESS

- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
- A) THE CONTRACTORS ACCESS TO THE WORK SHALL BE AS SHOWN ON THE SITE PLAN DRAWING.
 - B) THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT MANAGER.
 - C) ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - D) NO PERSONAL VEHICLES SHALL BE ALLOWED BEYOND THE CONTRACTOR'S PARKING AREA.
 - E) THE CONTRACTOR SHALL PROVIDE CONSTRUCTION ENTRANCE SIGNAGE AS REQUIRED BY TAZWELL COUNTY HIGHWAY DEPARTMENT.
 - F) THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.

HAUL ROAD NOTES

THE CONTRACTOR SHALL MAINTAIN A CONSTRUCTION HAUL ROAD FROM THE BORROW AREA TO THE SITE AT THE APPROXIMATE LOCATION SHOWN. THE CONTRACTOR SHALL SUBMIT A HAUL ROAD PLAN FOR APPROVAL AT THE PRECONSTRUCTION CONFERENCE. FINAL APPROVAL OF THE HAUL ROAD ALIGNMENT LIES WITH THE CITY OF PEKIN. GRADING, DRAINAGE AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO MAINTAIN THE HAUL ROAD SUITABLE TO SUPPORT THE CONTRACTOR'S OPERATIONS THROUGHOUT THE DURATION OF THE CONTRACT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK. THE CONTRACTOR SHALL RESTORE THE HAUL ROAD TO ORIGINAL CONDITION AT THE COMPLETION OF THE PROJECT.

UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE.

UNICOM FREQUENCY IS 122.8 MEGAHERTZ

PN003

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UPDATE BY: adraughan
SURVEY BOOK #
DATE: Mon 5/2/05 9:22am
XREF DWG: C:\info.dwg
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REVISIONS

NUMBER	BY	DATE

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AT FULL SCALE (34X22).

PEKIN MUNICIPAL AIRPORT
PEKIN, TAZEWELL COUNTY, ILLINOIS

REHABILITATE AND MARK THE PARALLEL TAXIWAY

GENERAL NOTES

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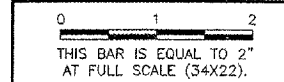
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A.I.P. PROJECT: 3-17-0078-88

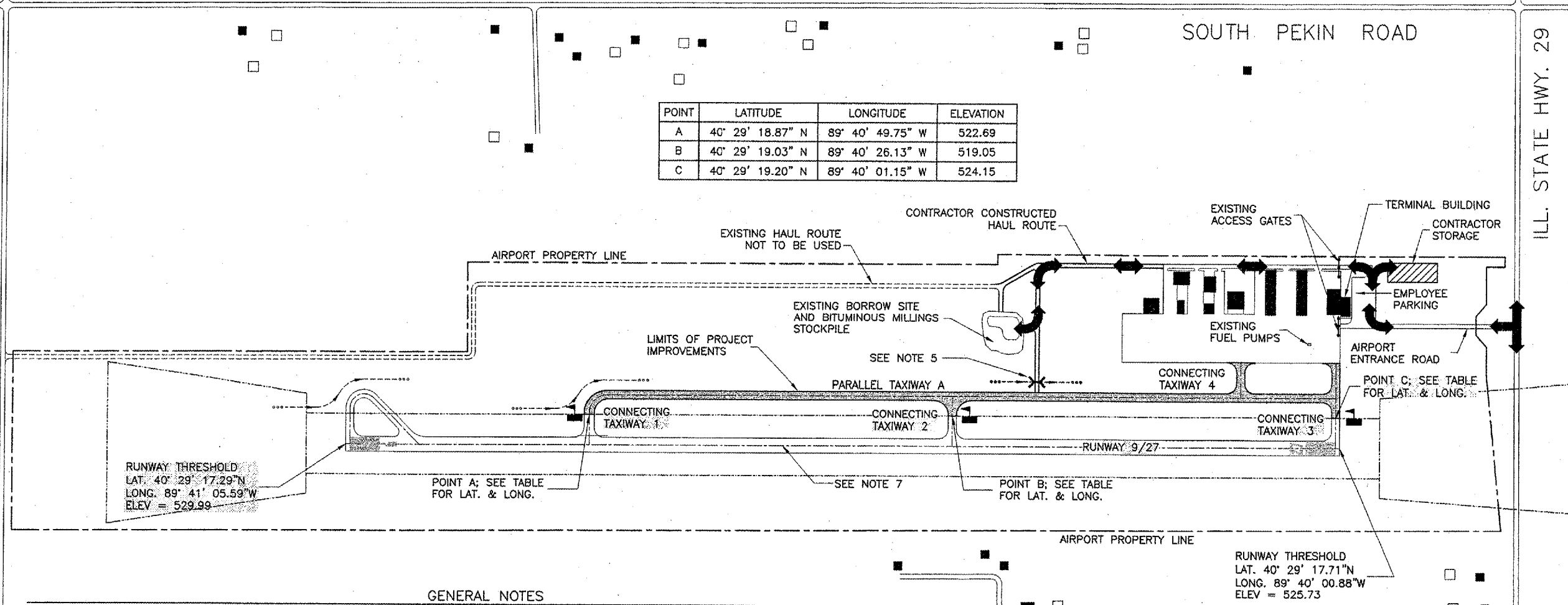
SHEET 2 OF 13 SHEETS



REVISIONS		
NUMBER	BY	DATE



POINT	LATITUDE	LONGITUDE	ELEVATION
A	40° 29' 18.87" N	89° 40' 49.75" W	522.69
B	40° 29' 19.03" N	89° 40' 26.13" W	519.05
C	40° 29' 19.20" N	89° 40' 01.15" W	524.15



RUNWAY THRESHOLD
 LAT. 40° 29' 17.29" N
 LONG. 89° 41' 05.59" W
 ELEV = 529.99

POINT A; SEE TABLE FOR LAT. & LONG.

SEE NOTE 7

POINT B; SEE TABLE FOR LAT. & LONG.

POINT C; SEE TABLE FOR LAT. & LONG.

RUNWAY THRESHOLD
 LAT. 40° 29' 17.71" N
 LONG. 89° 40' 00.88" W
 ELEV = 525.73

GENERAL NOTES

1. THE CONTRACTOR SHALL PROVIDE AN ACCESS ROAD AND HAUL ROAD OF HIS OWN STRUCTURE TO MAINTAIN HIS ACCESS TO THE PROJECT SITE AS HE DEEMS NECESSARY. INACCESSIBILITY OF THE SITE DURING CONSTRUCTION WILL NOT BE CONSIDERED AS A BASIS FOR THE EXTENSION OF TIME IF THE CONTRACT CALENDAR DAYS ARE EXCEEDED. CONTRACTOR'S HAUL ROAD SHALL BE REGRADED AND RETURNED UPON COMPLETION OF WORK. AT THE OPTION OF THE AIRPORT, THE HAUL ROAD MAY BE ALLOWED TO REMAIN IN-PLACE.
2. THE CONTRACTOR SHALL USE THE EXISTING GATES FOR AIRFIELD ACCESS. ALL GATES SHALL BE SECURED AT ALL TIMES AND LOCKED AT THE END OF EACH WORK DAY. ANY FINES IMPOSED ON THE AIRPORT DUE TO CONTRACTOR'S SECURITY VIOLATIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
3. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIAL AT THE LOCATIONS SHOWN ON THIS SHEET. THE MAXIMUM HEIGHT OF MATERIAL, EQUIPMENT OR STOCKPILE SHALL BE 25 FEET.
4. ACCESS ROUTES ARE TO BE ESTABLISHED BY THE CONTRACTOR AS SHOWN ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLANS.
5. CONTRACTOR SHALL INSTALL 20 L.F. OF 18-INCH DIAMETER CORRUGATED METAL PIPE AT THE LOCATION SHOWN IN ORDER TO CONSTRUCT THE CONTRACTOR'S ACCESS ROAD ACROSS AN EXISTING DRAINAGE SWALE. UPON COMPLETION OF CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL REMOVE THE 18-INCH C.M.P. AND REGRADE THE SWALE TO DRAIN. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
6. SEE CONSTRUCTION ACTIVITY PLANS FOR PROJECT PHASING AND DETAILED HAUL ROUTES.
7. THE CONTRACTOR SHALL BE ALLOWED TO USE RUNWAY 9/27 AS A HAUL ROAD AT THE DISCRETION OF THE RESIDENT ENGINEER PROVIDED THE CONTRACTOR HAVE A FLAGGER IN CONTACT WITH AIR TRAFFIC AT ALL TIMES. FLAGGER LOCATIONS SHALL BE AS SHOWN.

LEGEND

- CONTRACTOR'S ACCESS AND HAUL ROUTE
- CONTRACTOR STAGING AND STORAGE AREA
- LIMITS OF PROJECT IMPROVEMENTS
- FLAGGER REQUIRED IF RUNWAY IS TO BE USED AS A HAUL ROUTE
- 18-INCH CMP

PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS
 REHABILITATE AND MARK THE PARALLEL TAXIWAY
 SITE PLAN

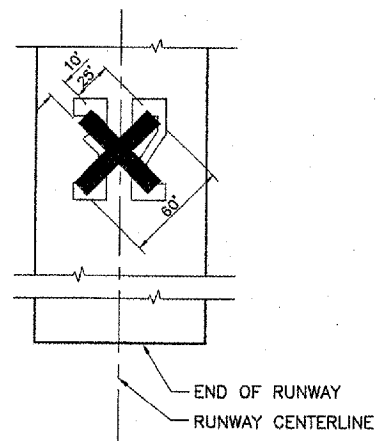
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JOB No:	04069-02-00
IL PROJECT:	C15-3434
A.I.P. PROJECT:	3-17-0078-B8
SHEET	3 OF 13 SHEETS

REVISIONS

NUMBER	BY	DATE

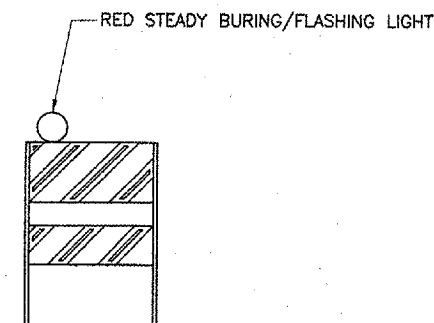
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CLOSED RUNWAY MARKER DETAIL
 N.T.S.

RUNWAY CLOSURE MARKER NOTES

1. MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE PAINTED PLYWOOD, PLASTIC OR OTHER APPROVED MATERIAL. SNOW FENCE IS NOT PERMITTED.
3. CONTRACTOR SHALL MAINTAIN MARKERS AS DIRECTED BY THE RESIDENT ENGINEER.
4. COST OF FURNISHING, INSTALLING AND MAINTAINING, RELOCATING AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS.
6. CLOSED RUNWAY MARKERS TO BE REMOVED AT THE END OF EACH WORKING DAY WHEN THE RUNWAY IS TO BE OPENED TO TRAFFIC.



BARRICADE DETAIL
 N.T.S.

BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT MAXIMUM 15' INTERVALS.
5. BARREL TYPE BARRICADES MAY NOT BE USED.

PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS
 REHABILITATE AND MARK THE PARALLEL TAXIWAY
 CONSTRUCTION ACTIVITY PLAN
 GENERAL DETAILS

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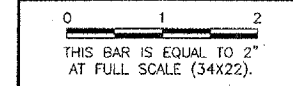
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 SHEET 4 OF 13 SHEETS

PN003

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 UPDATE BY: adraughan
 SURVEY BOOK #
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**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**

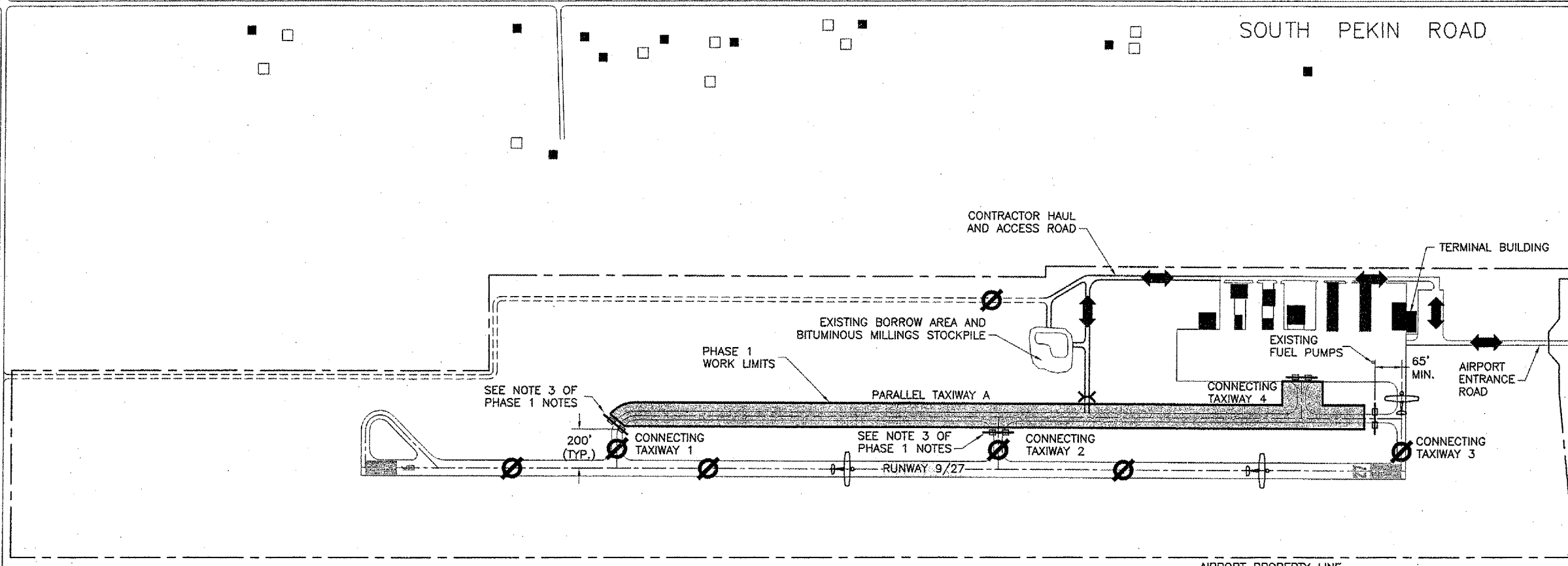
**REHABILITATE AND MARK THE PARALLEL TAXIWAY
 CONSTRUCTION ACTIVITY PLAN
 PHASE 1**

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A.I.P. PROJECT:	3-17-0078-BB
SHEET	5 OF 13 SHEETS

SOUTH PEKIN ROAD

ILL. STATE HWY. 29



PHASE 1 NOTES

- ACCESS FOR PHASE 1 SHALL BE AS SHOWN.
- ALL ITEMS OF WORK IN PHASE 1 SHALL BE COMPLETED BEFORE PHASE 2 MAY BE STARTED. PHASES 1 AND 2 SHALL NOT BE CONSTRUCTED CONCURRENTLY.
- BARRICADES ON CONNECTING TAXIWAYS 1 AND 2 SHALL BE PLACED ON THE SIDE OF THE RUNWAY HOLD POSITION MARKING CLOSEST TO PARALLEL TAXIWAY A.

LEGEND

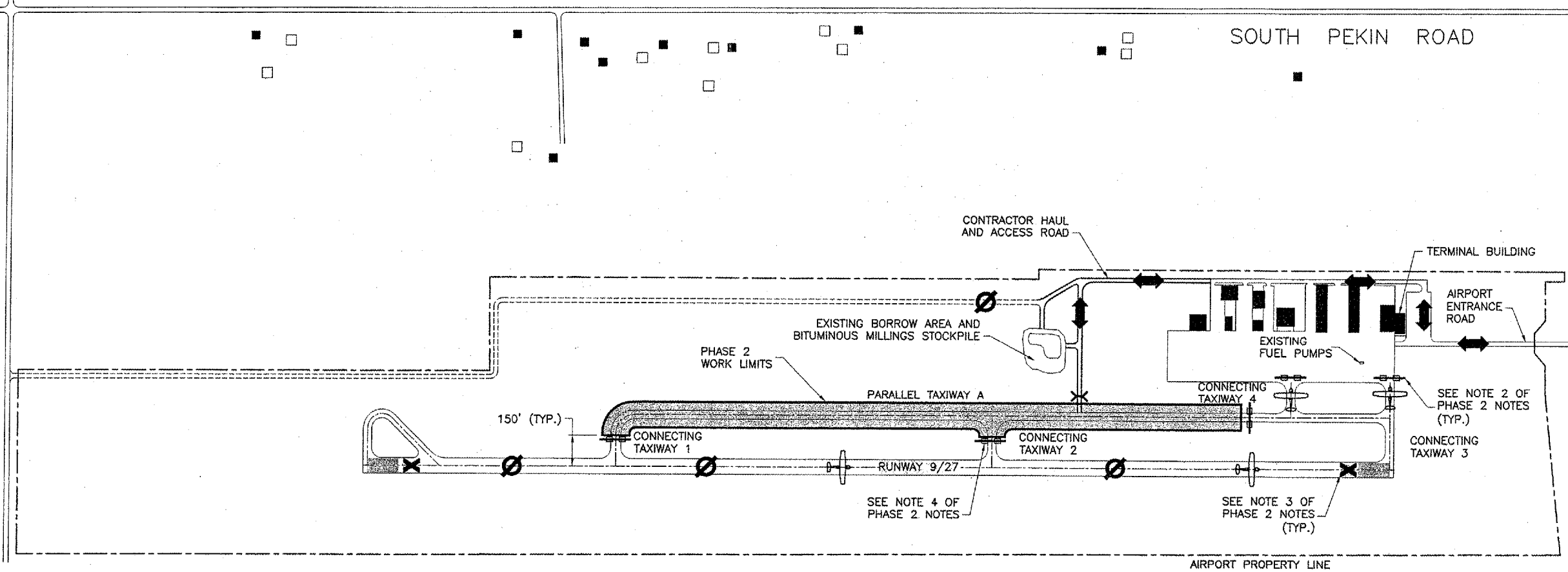
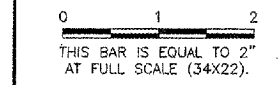
- BARRICADE LINE
- CONTRACTOR ACCESS
- PAVEMENT OFF LIMITS TO CONTRACTOR
- AIRCRAFT ACCESS
- CONTRACTOR WORK AREA

AIRFIELD CLOSURE TABLE - PHASE 1

AIRFIELD PAVEMENTS OPEN TO TRAFFIC	CLOSED TO TRAFFIC	
	LONG-TERM	DAILY
RUNWAY 9/27 CONNECTING TAXIWAY NO. 3 TURNAROUND TAXIWAY	CONNECTING TAXIWAY NO. 1 CONNECTING TAXIWAY NO. 2 CONNECTING TAXIWAY NO. 4 PARALLEL TAXIWAY A FROM STA. 15+00 TO STA. 51+00	



REVISIONS		
NUMBER	BY	DATE



PHASE 2 NOTES

- ACCESS FOR PHASE 2 SHALL BE AS SHOWN.
- THE BARRICADES AT THE ENTRANCE TO CONNECTING TAXIWAYS 3 AND 4 FROM THE APRON TO BE PLACED BEFORE WORK BEGINS FOR THE DAY AND REMOVED AT THE END OF THE DAY. THESE BARRICADES TO BE PLACED ALONG THE EDGE OF THE APRON AT EACH ENTRANCE.
- THE CONTRACTOR SHALL PLACE THE RUNWAY CLOSURE MARKERS OVER THE RUNWAY DESIGNATION MARKING BEFORE WORKS BEGINS FOR THE DAY AND REMOVE THEM AT THE END OF THE DAY.
- BARRICADES TO BE PLACED AT THE ENTRANCE TO CONNECTING TAXIWAY 1 AND 2 FROM RUNWAY 9/27 TO BE PLACED A MINIMUM OF 150' FROM RUNWAY 9/27 CENTERLINE AT THE END OF THE WORK DAY AND WHEN THE RUNWAY IS OPEN TO AIR TRAFFIC. THE BARRICADES MAY BE MOVED DURING THE DAY TO FACILITATE CONSTRUCTION OPERATIONS PROVIDED THE RUNWAY IS CLOSED.
- THE CONTRACTOR SHALL HAVE A MAXIMUM OF 7 CALENDAR DAYS TO COMPLETE PHASE 2.
- ALL OPEN EXCAVATIONS WITHIN 150 FEET OF THE RUNWAY 9/27 CENTERLINE MUST BE BACKFILLED TO WITHIN 3 INCHES OF THE SURROUNDING SURFACE BEFORE RUNWAY 9/27 MAY BE REOPENED TO TRAFFIC. IF BACKFILL IS NOT FEASIBLE, THE EXCAVATIONS MAY BE COVERED WITH STEEL ROAD PLATES CAPABLE OF SUPPORTING A 25,000 POUND SINGLE WHEEL GEAR AIRCRAFT. THE GROSS WEIGHT ON ONE MAIN GEAR EQUALS 11,875 POUNDS. OPEN EXCAVATIONS INCLUDE ANY PAVEMENT REMOVAL FOR CRACK REPAIR AND SUBGRADE OR SHOULDER REMOVAL FOR CRACK REPAIR OPERATIONS. THE COST OF THIS WORK SHALL BE INCIDENTAL TO THE CRACK REPAIR OR OTHER ASSOCIATED ITEMS.

LEGEND

- BARRICADE LINE
- CONTRACTOR ACCESS
- PAVEMENT OFF LIMITS TO CONTRACTOR
- AIRCRAFT ACCESS
- CONTRACTOR WORK AREA

AIRFIELD CLOSURE TABLE - PHASE 2

AIRFIELD PAVEMENTS OPEN TO TRAFFIC	CLOSED TO TRAFFIC	
	LONG-TERM	DAILY
RUNWAY 9/27 AND TURNAROUND TAXIWAY, 3:30 PM TO 7 AM	CONNECTING TAXIWAY NO. 1 CONNECTING TAXIWAY NO. 2 PARALLEL TAXIWAY A FROM STA. 15+00 TO STA. 51+00	AIRFIELD CLOSED 7AM TO 3:30 PM
CONNECTING TAXIWAY NO. 3 CONNECTING TAXIWAY NO. 4		

ILL. STATE HWY. 29

PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS

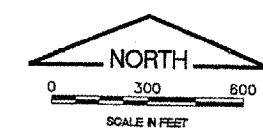
REHABILITATE AND MARK THE PARALLEL TAXIWAY

CONSTRUCTION ACTIVITY PLAN
 PHASE 2

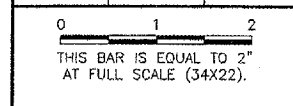
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IL PROJECT:	C15-3434
A.I.P. PROJECT:	3-17-0078-88
SHEET	6 OF 13 SHEETS



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**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**

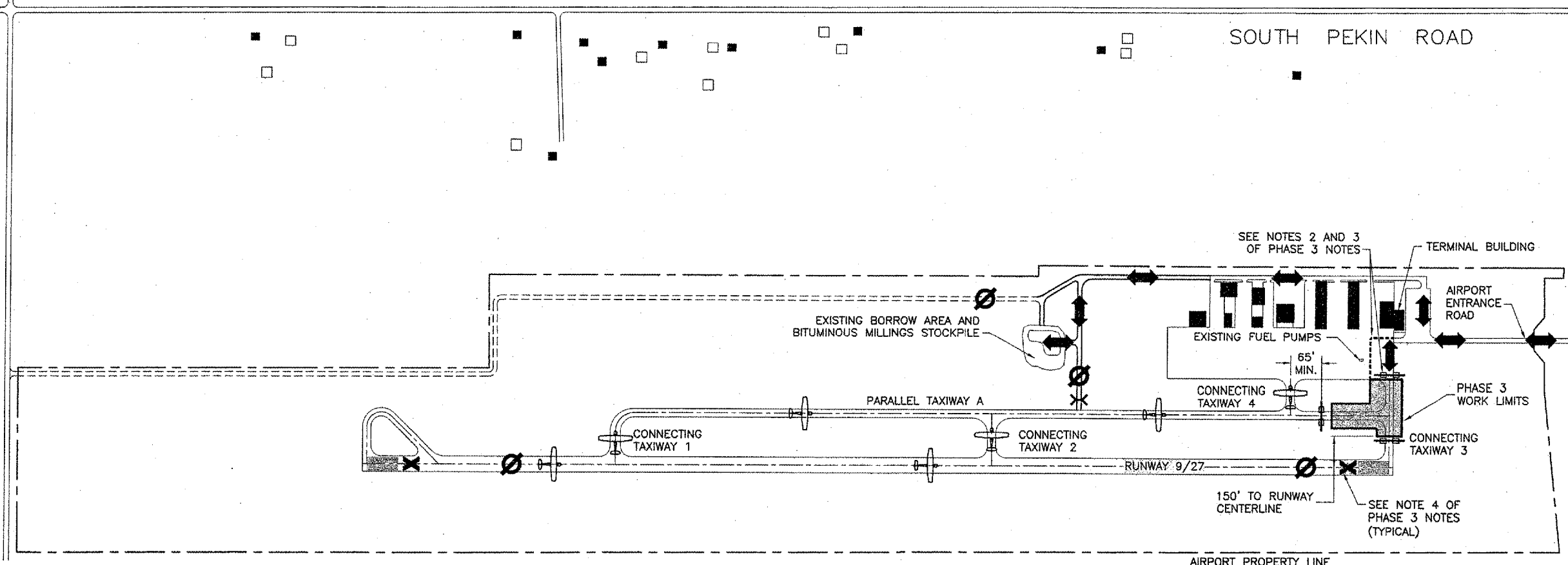
REHABILITATE AND MARK THE PARALLEL TAXIWAY

**CONSTRUCTION ACTIVITY PLAN
 PHASE 3**

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SHEET 7 OF 13 SHEETS	

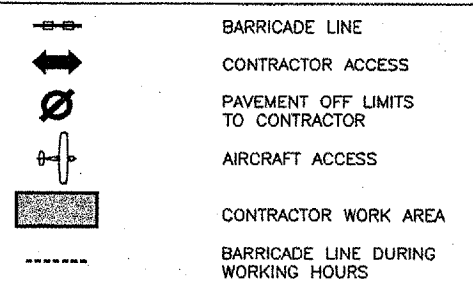
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PHASE 3 NOTES

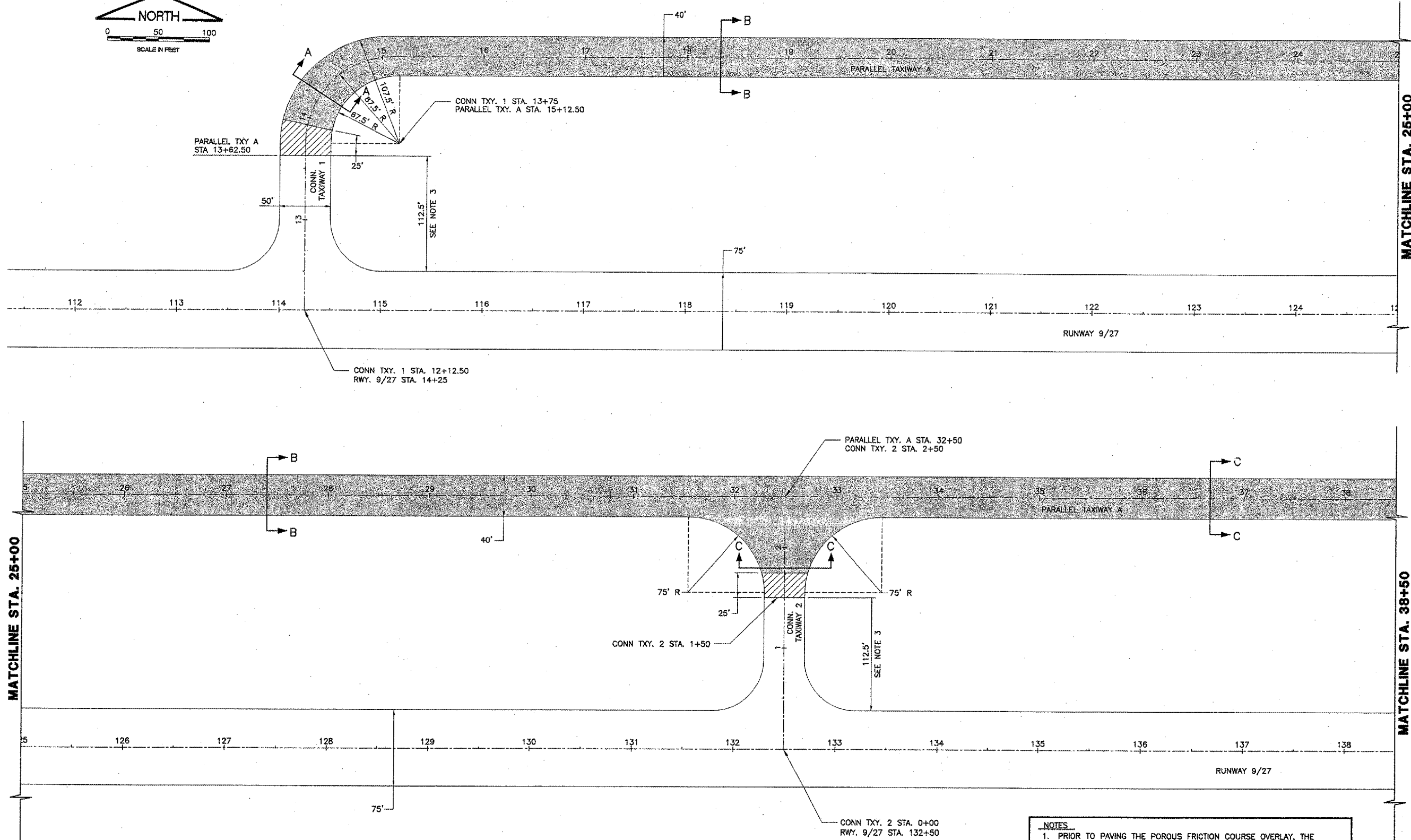
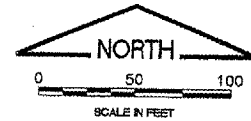
- ACCESS FOR PHASE 3 SHALL BE AS SHOWN.
- THE CONTRACTOR SHALL BE ALLOWED ACCESS THROUGH THE AIRCRAFT PARKING RAMP AS SHOWN ONLY DURING THIS PHASE. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS INSIDE THESE LIMITS UNLESS PRIOR PERMISSION IS GRANTED BY THE AIRPORT.
- THE CONTRACTOR SHALL MAINTAIN A LINE OF BARRICADES AROUND HIS ACCESS AREA ON THE APRON TO PREVENT ACCESS BY AIRCRAFT TO THE WORKSITE DURING THE DAY. AT THE END OF THE WORKING DAY, BARRICADES SHALL BE RELOCATED TO THE ENTRANCE TO CONNECTING TAXIWAY 3 ALONG THE EDGE OF THE APRON AS SHOWN.
- THE CONTRACTOR SHALL PLACE THE RUNWAY CLOSURE MARKERS OVER THE RUNWAY DESIGNATION MARKING BEFORE WORKS BEGINS FOR THE DAY AND REMOVE THEM AT THE END OF THE DAY.
- BARRICADES TO BE PLACED AT THE ENTRANCE TO CONNECTING TAXIWAY 3 FROM RUNWAY 9/27 TO BE PLACED A MINIMUM OF 150' FROM RUNWAY 9/27 CENTERLINE AT THE END OF THE WORK DAY AND WHEN THE RUNWAY IS OPEN TO AIR TRAFFIC. THE BARRICADES MAY BE MOVED DURING THE DAY TO FACILITATE CONSTRUCTION OPERATIONS PROVIDED THE RUNWAY IS CLOSED.
- ALL OPEN EXCAVATIONS WITHIN 150 FEET OF THE RUNWAY 9/27 CENTERLINE MUST BE BACKFILLED TO WITHIN 3 INCHES OF THE SURROUNDING SURFACE BEFORE RUNWAY 9/27 MAY BE REOPENED TO TRAFFIC. IF BACKFILL IS NOT FEASIBLE, THE EXCAVATIONS MAY BE COVERED WITH STEEL ROAD PLATES CAPABLE OF SUPPORTING A 25,000 POUND SINGLE WHEEL GEAR AIRCRAFT. THE GROSS WEIGHT ON ONE MAIN GEAR EQUALS 11,875 POUNDS. OPEN EXCAVATIONS INCLUDE ANY PAVEMENT REMOVAL FOR CRACK REPAIR AND SUBGRADE OR SHOULDER REMOVAL FOR CRACK REPAIR OPERATIONS. THE COST OF THIS WORK SHALL BE INCIDENTAL TO THE CRACK REPAIR OR OTHER ASSOCIATED ITEMS.
- THE CONTRACTOR SHALL HAVE A MAXIMUM OF 7 CALENDAR DAYS TO COMPLETE PHASE 3.

LEGEND



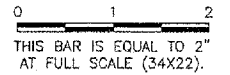
AIRFIELD CLOSURE TABLE - PHASE 3

AIRFIELD PAVEMENTS OPEN TO TRAFFIC	CLOSED TO TRAFFIC	
	LONG-TERM	DAILY
RUNWAY 9/27 AND TURNAROUND TAXIWAY, 3:30 PM TO 7 AM	CONNECTING TAXIWAY NO. 3 PARALLEL TAXIWAY A FROM STA. 48+50 TO STA. 51+80	AIRFIELD CLOSED 7AM TO 3:30 PM
CONNECTING TAXIWAY NO. 1 CONNECTING TAXIWAY NO. 2 CONNECTING TAXIWAY NO. 4		



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**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**

REHABILITATE AND MARK THE PARALLEL TAXIWAY

**PROJECT IMPROVEMENTS SHEET 1
 STA. 12+12.50 TO STA. 38+50**

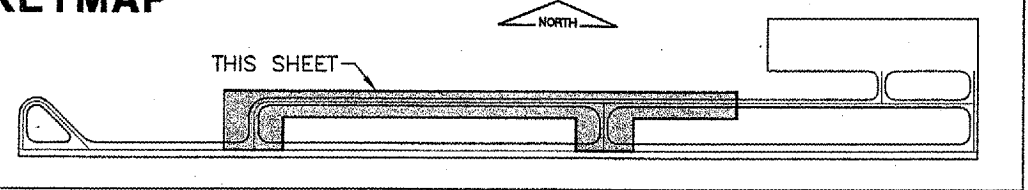
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IL PROJECT: C15-3434
 A.I.P. PROJECT: 3-17-0078-BB
 SHEET 8 OF 13 SHEETS

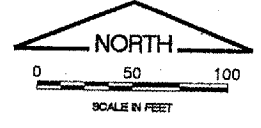
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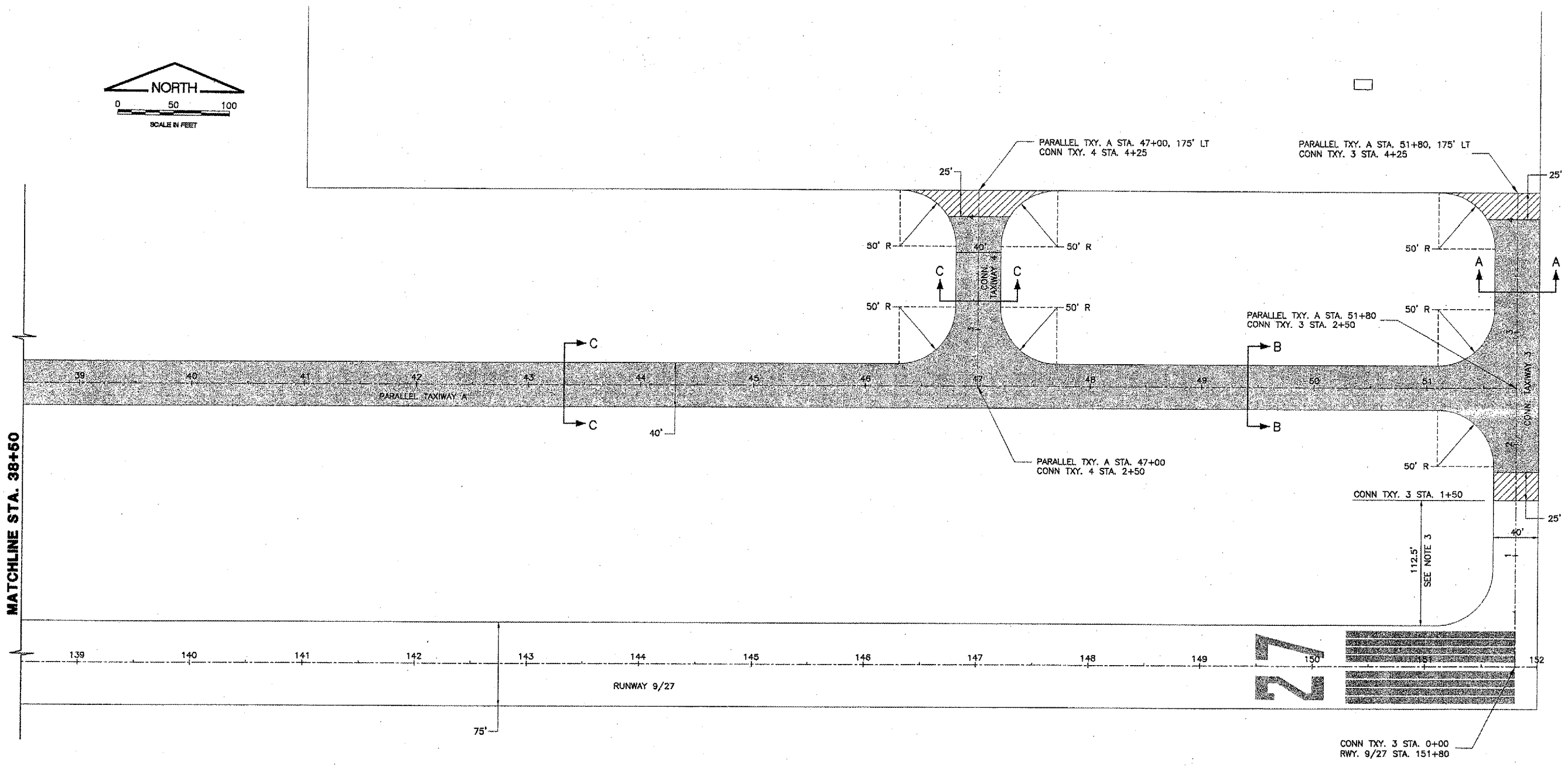
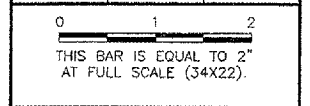
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	CRACK REPAIR AT LOCATIONS DETERMINED BY ENGINEER AND 0.10' PFC OVERLAY
	BUTT JOINT CONSTRUCTION AND 0.10' PFC OVERLAY

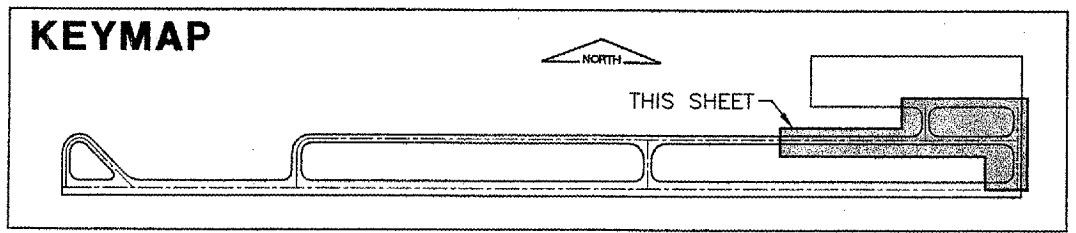
- NOTES**
1. PRIOR TO PAVING THE POROUS FRICTION COURSE OVERLAY, THE EXISTING TURF ALONG THE EDGE OF TAXIWAY IMPROVEMENTS SHALL BE PEELED BACK A DISTANCE OF THREE (3) FEET.
 2. AT THE COMPLETION OF PAVING, THE TURF ALONG THE EDGE OF TAXIWAY IMPROVEMENTS SHALL BE RESTORED. A 1 1/2" DROP OFF IS REQUIRED BETWEEN THE EDGE OF PAVEMENT ELEVATION AND TOP OF THE SHOULDER EMBANKMENT MATERIAL. THE SHOULDER EMBANKMENT SHALL BE GRADED TO DRAIN WITH A SLOPE OF 3-5%.
 3. THE PROJECT IMPROVEMENTS TO BEGIN AT THE TRANSVERSE JOINT OF THE EXISTING BUTT JOINT FROM A PREVIOUS PROJECT. ADJUST DISTANCE AS NECESSARY.
 4. TYPICAL SECTIONS SHOWN ON SHEET 10.



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MATCHLINE STA. 38+50



LEGEND

	CRACK REPAIR AT LOCATIONS DETERMINED BY ENGINEER AND 0.10' PFC OVERLAY
	BUTT JOINT CONSTRUCTION AND 0.10' PFC OVERLAY

- NOTES**
1. PRIOR TO PAVING THE POROUS FRICTION COURSE OVERLAY, THE EXISTING TURF ALONG THE EDGE OF TAXIWAY IMPROVEMENTS SHALL BE PEELED BACK A DISTANCE OF THREE (3) FEET.
 2. AT THE COMPLETION OF PAVING, THE TURF ALONG THE EDGE OF TAXIWAY IMPROVEMENTS SHALL BE RESTORED. A 1" DROP OFF IS REQUIRED BETWEEN THE EDGE OF PAVEMENT ELEVATION AND TOP OF THE SHOULDER EMBANKMENT MATERIAL. THE SHOULDER EMBANKMENT SHALL BE GRADED TO DRAIN WITH A SLOPE OF 3-5%.
 3. THE PROJECT IMPROVEMENTS TO BEGIN AT THE TRANSVERSE JOINT OF THE EXISTING BUTT JOINT FROM A PREVIOUS PROJECT. ADJUST DISTANCE AS NECESSARY.
 4. TYPICAL SECTIONS SHOWN ON SHEET 10.

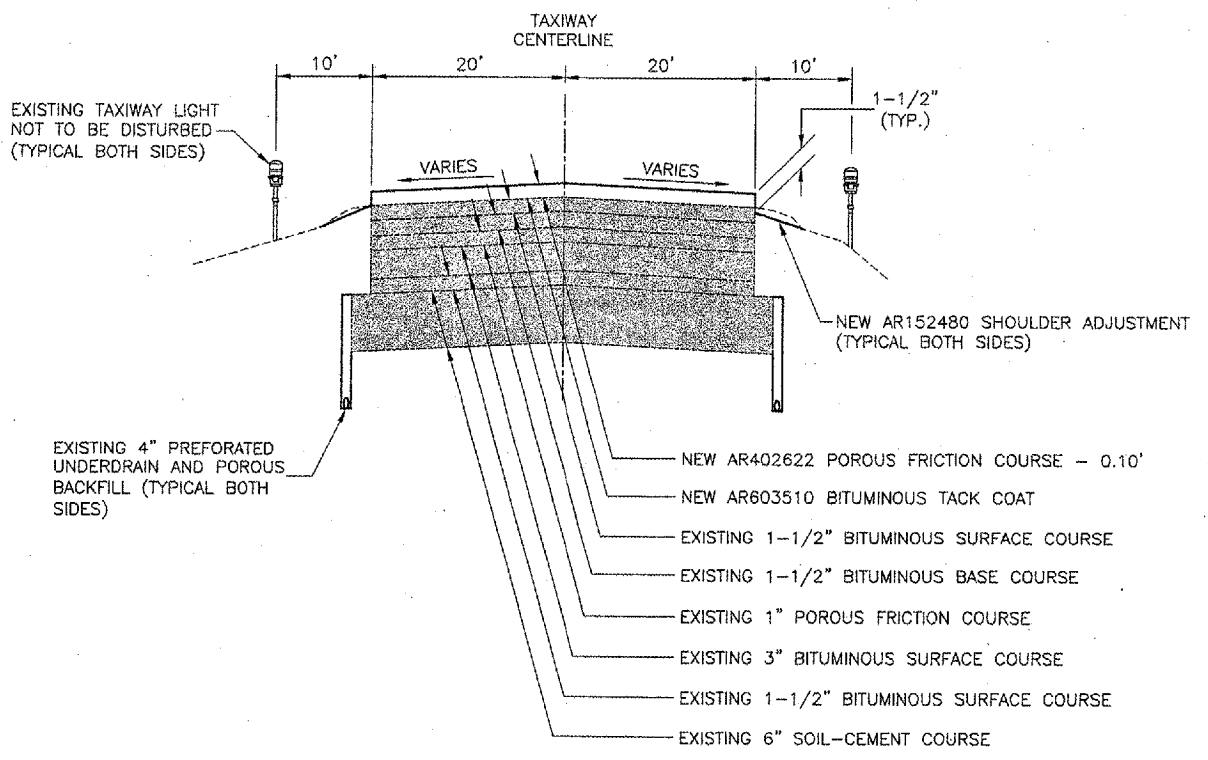
**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**

REHABILITATE AND MARK THE PARALLEL TAXIWAY

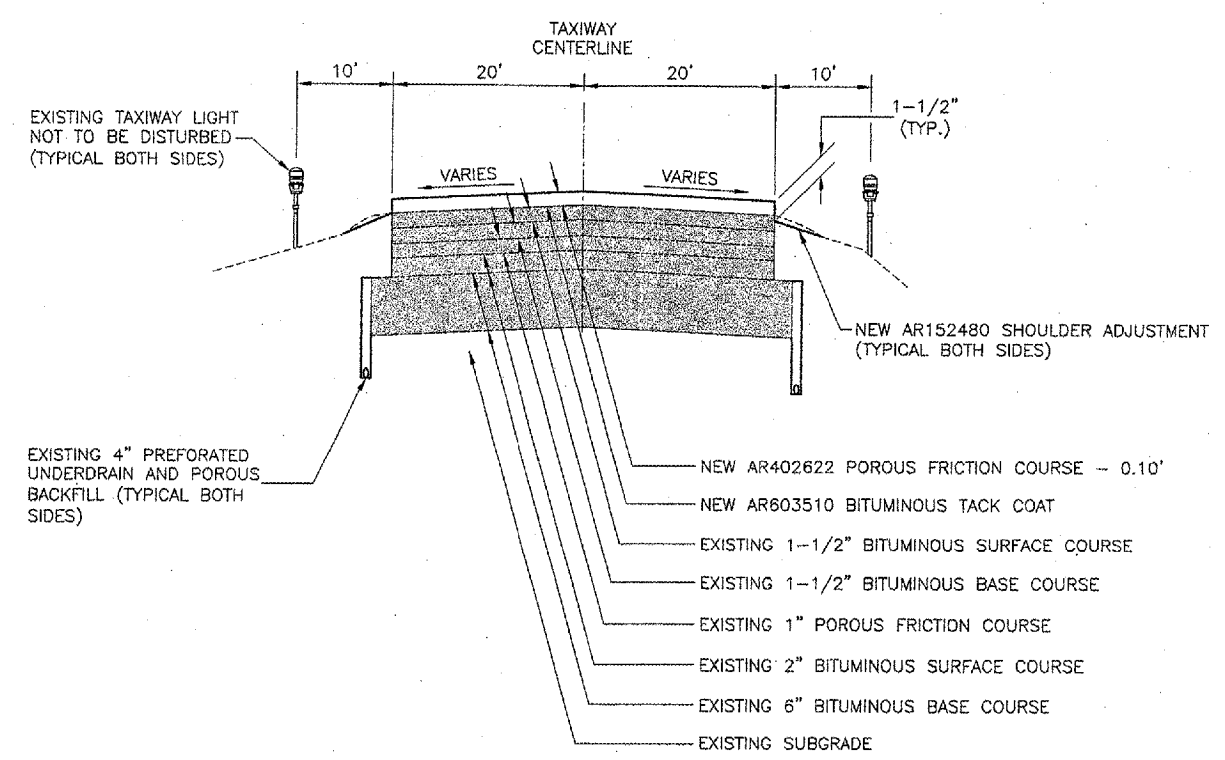
**PROJECT IMPROVEMENTS SHEET 2
 STA 38+50 TO STA 51+80**

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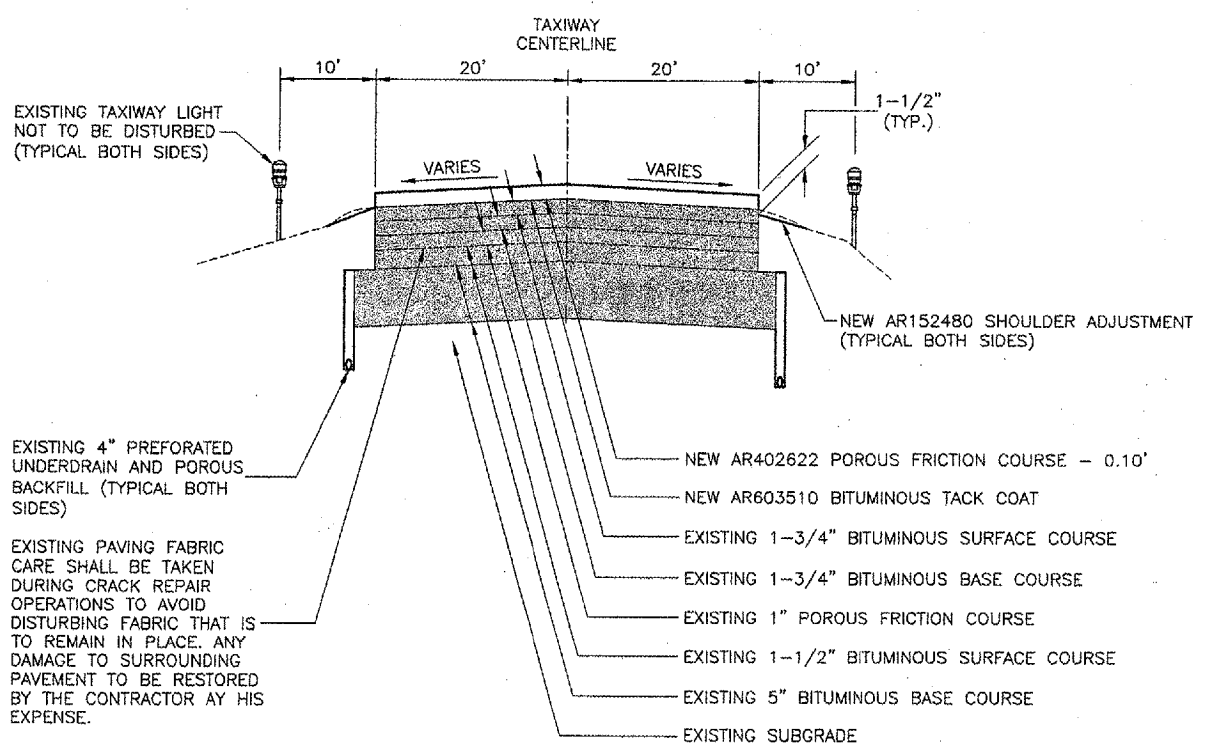
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SHEET	9 OF 13 SHEETS



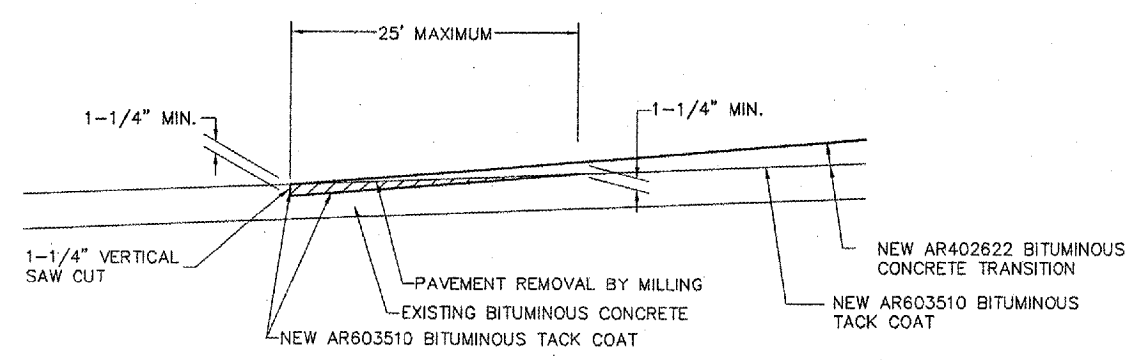
TYPICAL SECTION
 CONNECTING TAXIWAYS 1 AND 3
 N.T.S.



PARALLEL TAXIWAY STA. 15+00 TO STA. 32+30
 PARALLEL TAXIWAY STA. 47+20 TO STA. 51+60
 N.T.S.

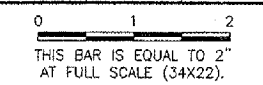


PARALLEL TAXIWAY STA. 32+30 TO STA. 47+20
 CONNECTING TAXIWAYS 2 AND 4
 N.T.S.



AR401655 BUTT JOINT DETAIL
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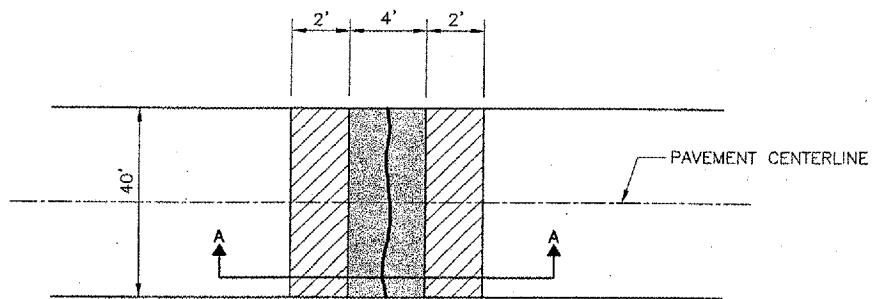
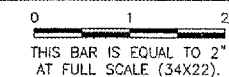


**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**
REHABILITATE AND MARK THE PARALLEL TAXIWAY
TYPICAL SECTIONS SHEET 1

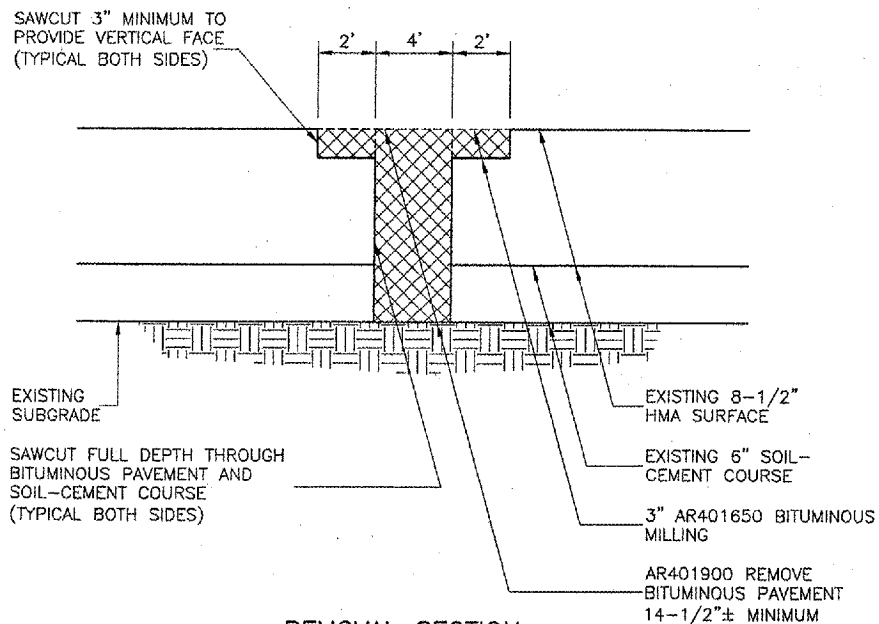
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SHEET 10 OF 13 SHEETS	

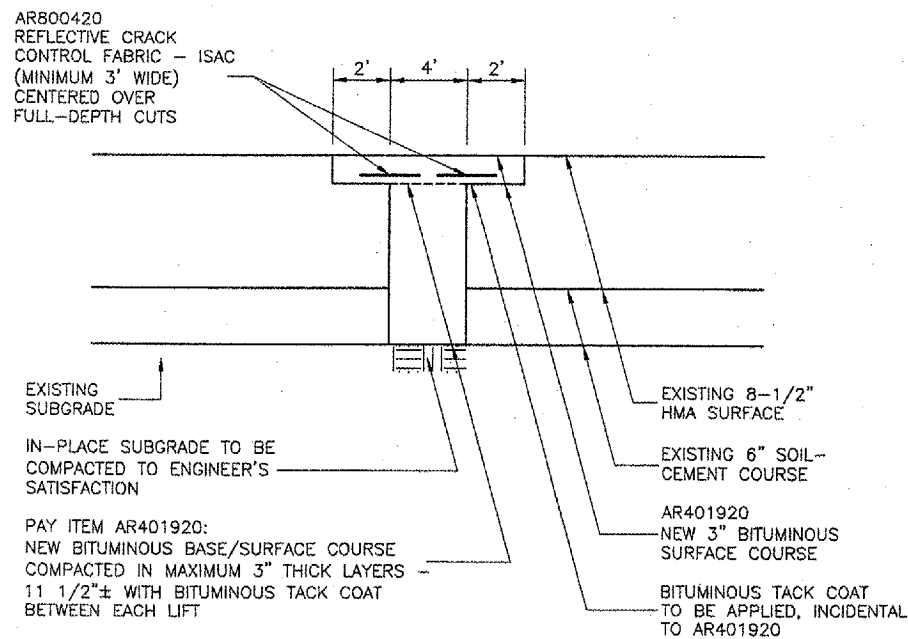
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PLAN VIEW
N.T.S.

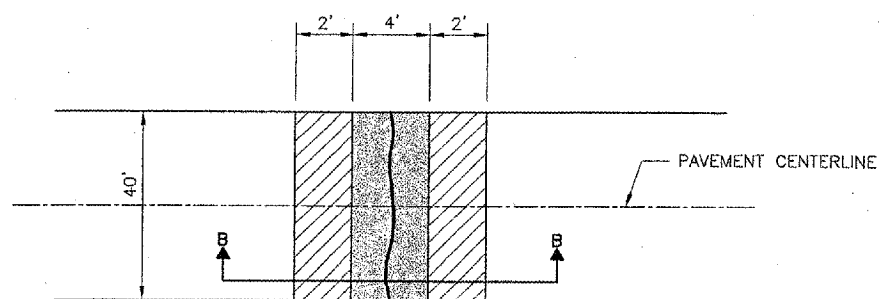


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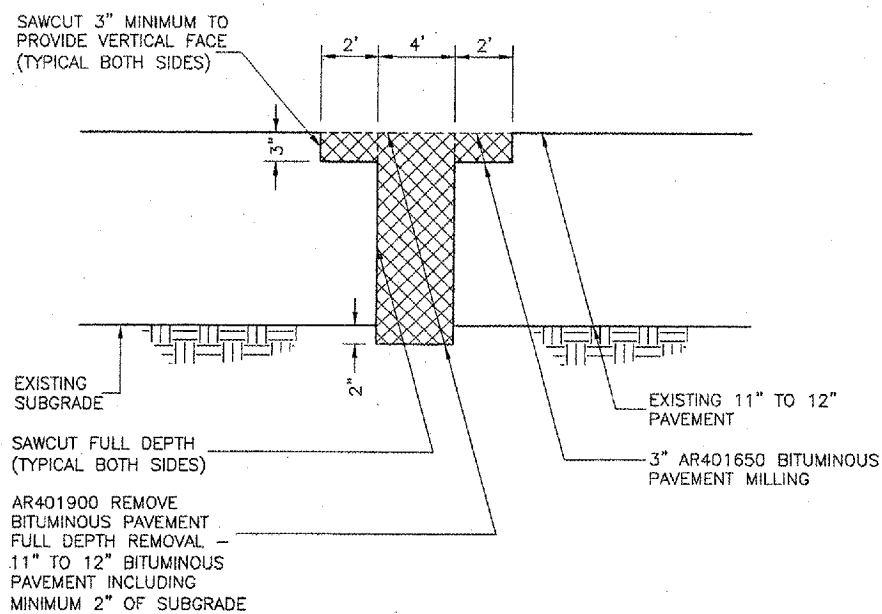


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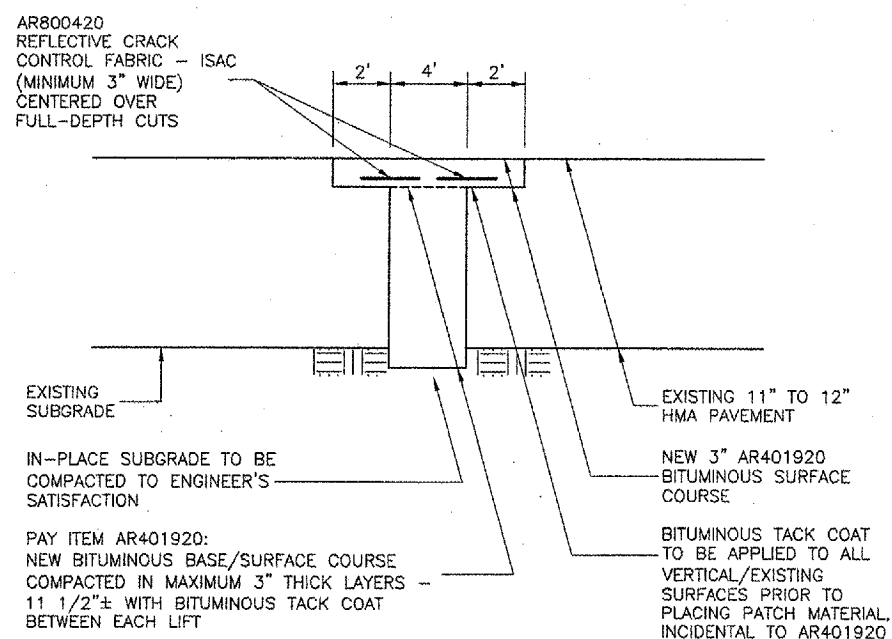
BITUMINOUS CRACK REPAIR - METHOD A DETAIL
CONNECTING TAXIWAYS 1 AND 3
N.T.S.



PLAN VIEW
N.T.S.

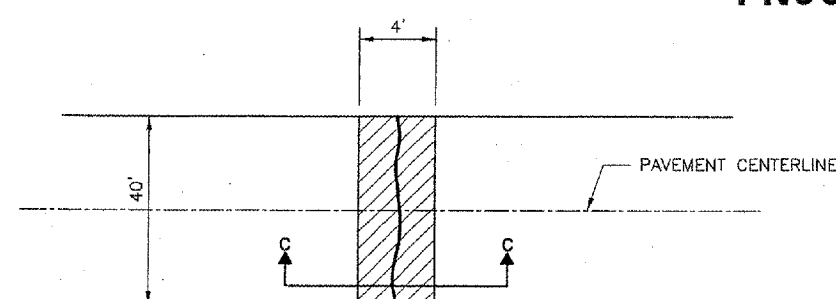


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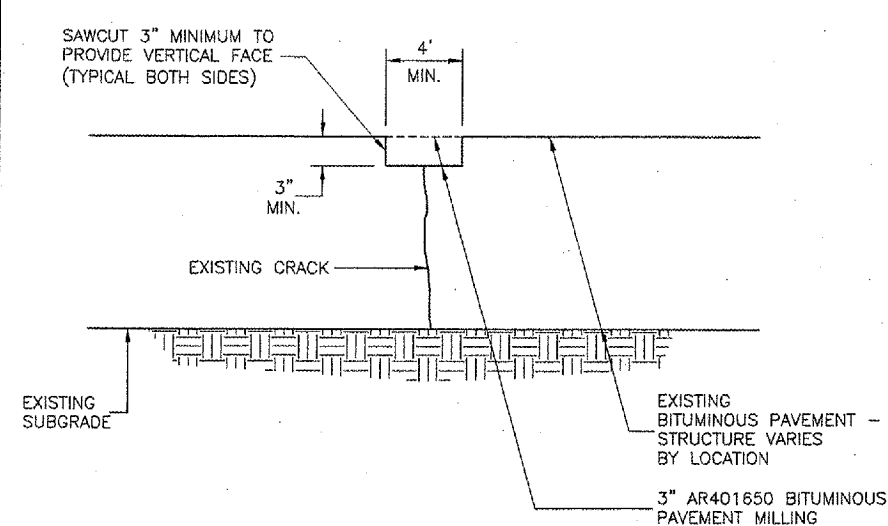


B REPLACEMENT SECTION B
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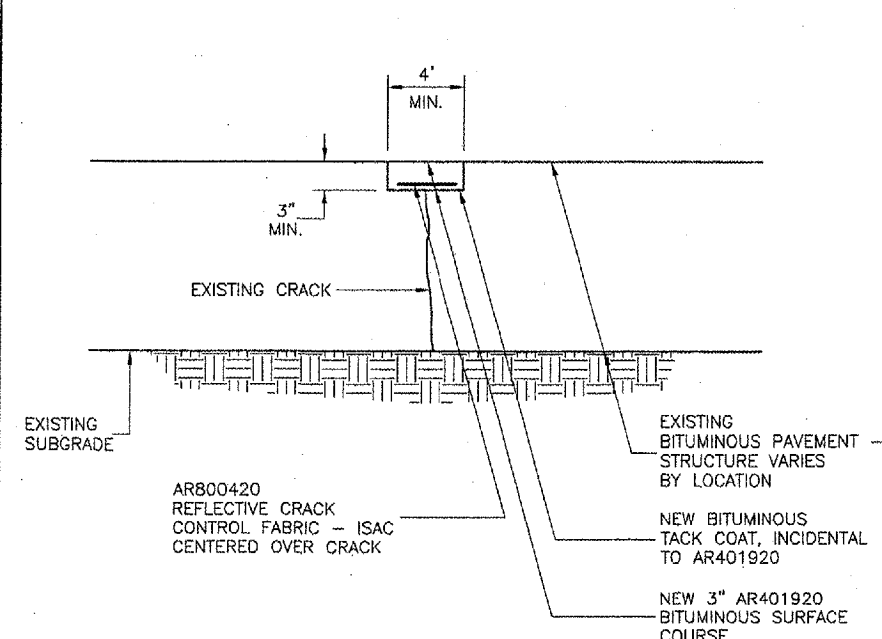
BITUMINOUS CRACK REPAIR - METHOD B DETAIL
CONNECTING TAXIWAYS 2 AND 4
N.T.S.



PLAN VIEW
N.T.S.



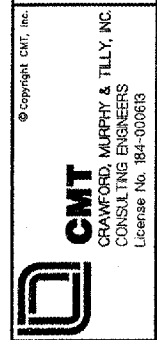
C REMOVAL SECTION C
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C REPLACEMENT SECTION C
N.T.S.

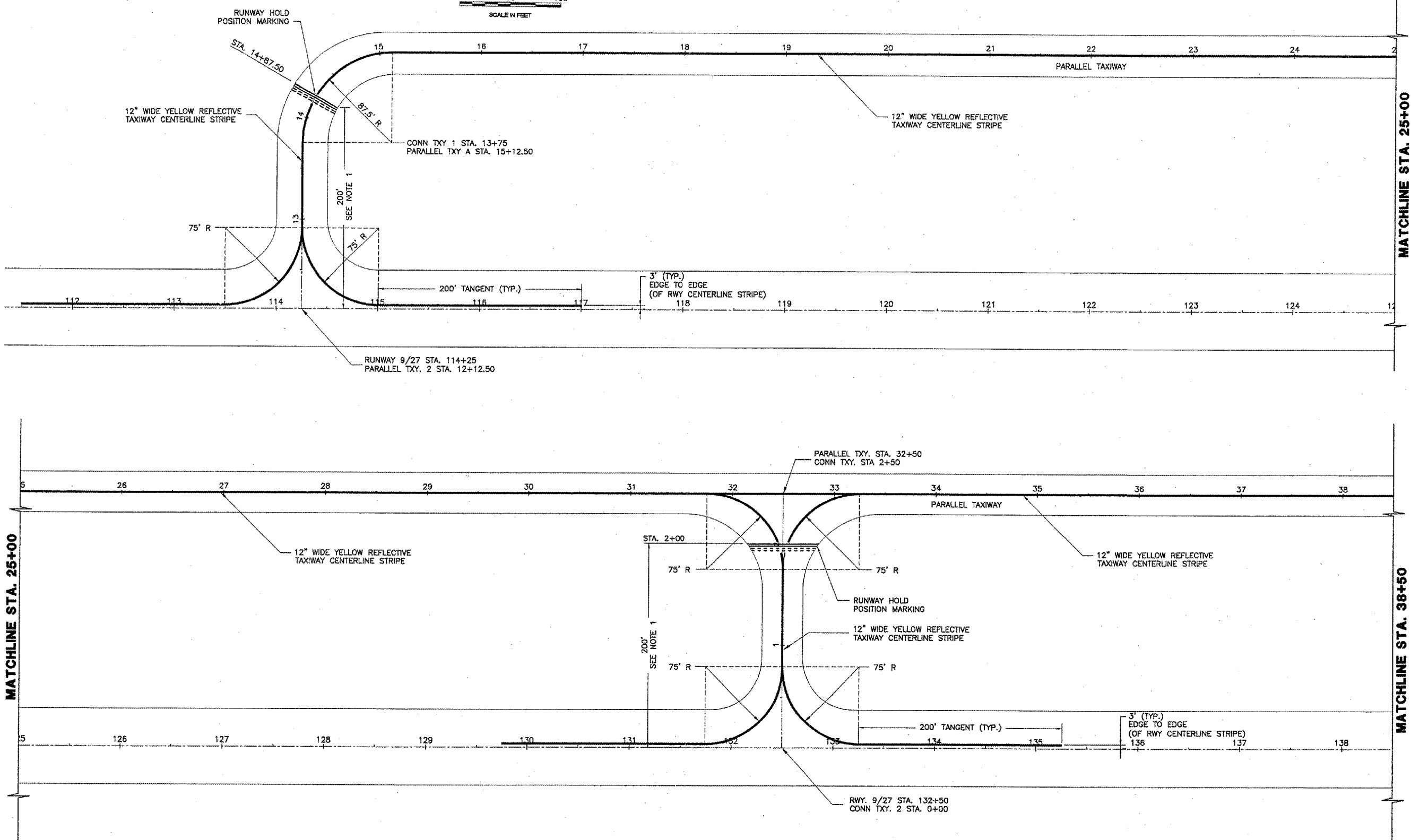
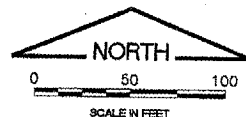
BITUMINOUS CRACK REPAIR - METHOD C DETAIL
N.T.S.

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 PEKIN, TAZEWELL COUNTY, ILLINOIS
 REHABILITATE AND MARK THE PARALLEL TAXIWAY
 TYPICAL SECTIONS SHEET 2



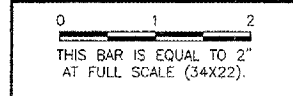
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SHEET 11 OF 13 SHEETS	

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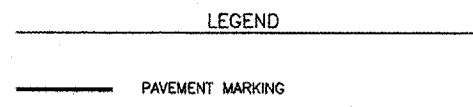
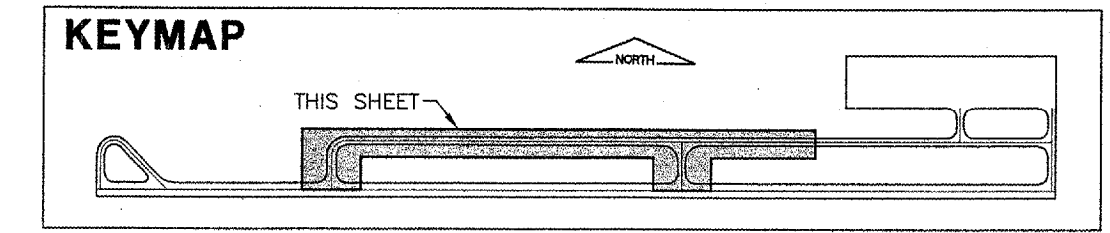
**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**

REHABILITATE AND MARK THE PARALLEL TAXIWAY

**MARKING PLAN SHEET 1
 STA 12+12.50 TO STA 38+50**

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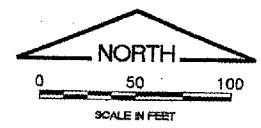


- NOTES**
- CONTRACTOR TO LOCATE HOLDLINE MARKINGS WITH RESPECT TO RUNWAY CENTERLINE AT START OF PROJECT. PRIOR TO PAVING. UPON COMPLETION OF PROJECT, HOLD SIDE OF HOLDLINE MARKINGS SHALL BE REMARKED AT ORIGINAL LOCATION. HOLDLINE MARKINGS SHALL HAVE 12-INCH WIDE STRIPES, AS SHOWN IN DETAIL.

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 SHEET 12 OF 13 SHEETS

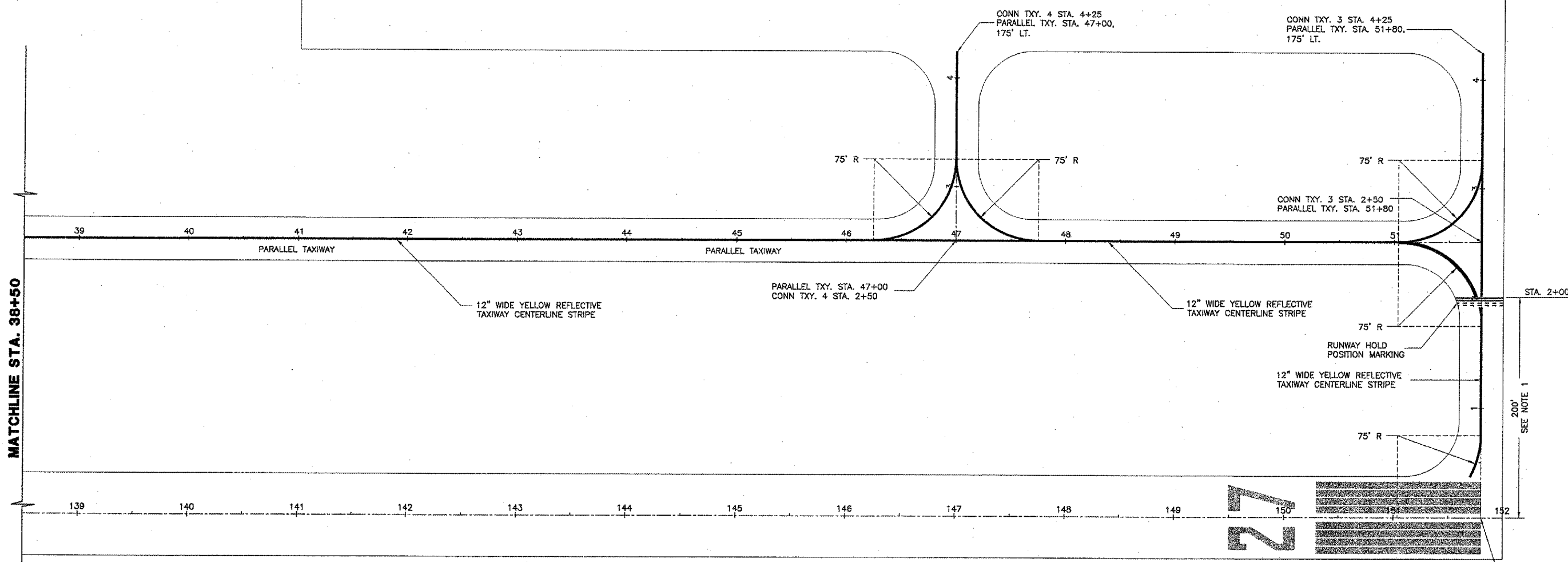
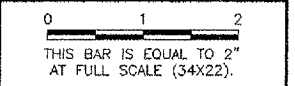
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LEGEND
 ——— PAVEMENT MARKING



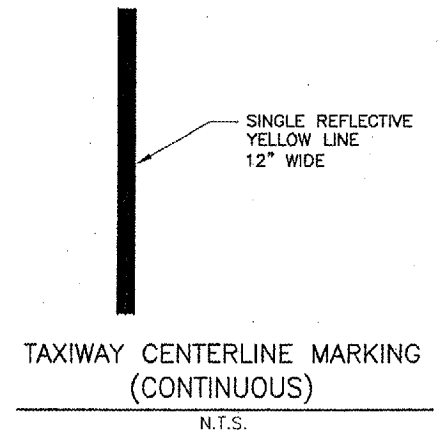
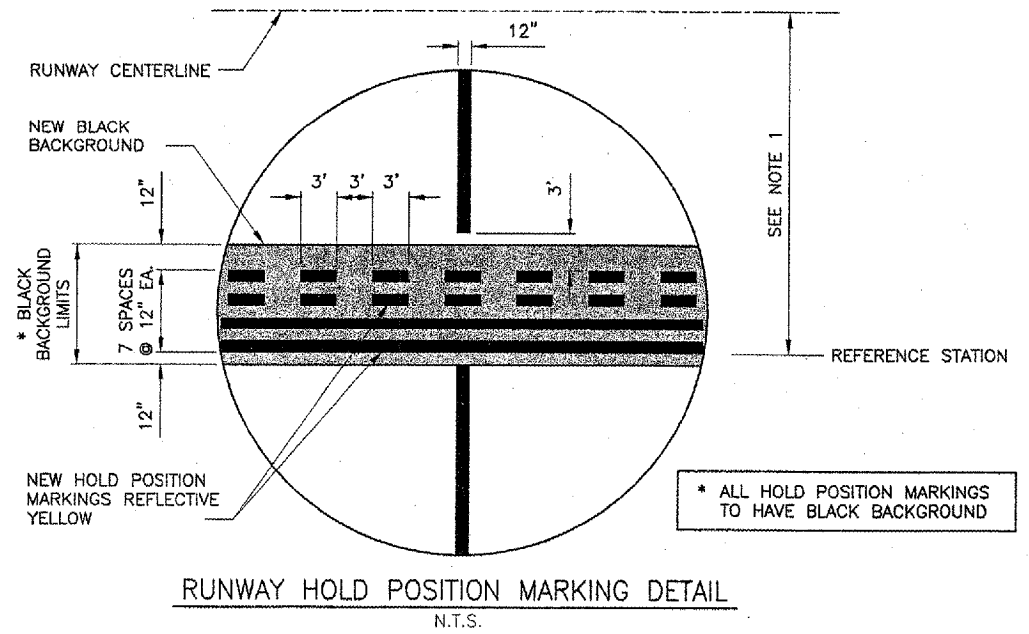
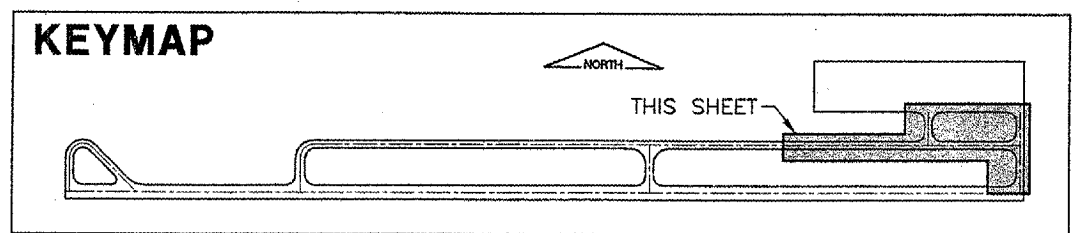
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MATCHLINE STA. 38+50

200' SEE NOTE 1



PEKIN MUNICIPAL AIRPORT
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REHABILITATE AND MARK THE PARALLEL TAXIWAY
MARKING PLAN SHEET 2
STA 38+50 TO STA 51+80

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SHEET 13 OF 13 SHEETS