

SCOPE OF WORK

THIS PROJECT CONSISTS OF THE CONSTRUCTION OF A NEW 3,500 FT. x 60 FT. EAST-WEST RUNWAY WITH A CONNECTING TAXIWAY SYSTEM. ASSOCIATED WORK WILL INCLUDE BUILDING DEMOLITION, CLEARING, GRUBBING, GRADING, DRAINAGE, PAVING, LIGHTING, MARKING, SEEDING, AND MULCHING, AND THE REMOVAL AND REPLACEMENT OF THE AIRPORT'S ELECTRICAL VAULT.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI TRUCK.

HAUL ROUTE AND VEHICLE PARKING

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND PARKING AREA AS SHOWN ON THIS SHEET. THE PROPOSED PARKING AREA WILL BE APPROXIMATELY 200' X 200'. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL RESTORE IT TO ITS ORIGINAL STATE. RESTORATION OF THE HAUL ROUTE AND PARKING AREA WILL BE CONSIDERED INCIDENTAL TO ITEM AR150540 - HAUL ROUTE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THIS AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

NO UNPROTECTED TRENCHES OR HOLES, WITHIN 200' OF 18-36 WHEN IT IS CLOSED AND BEING READIED TO REOPEN, WILL REMAIN OPEN OVERNIGHT. IF A HOLE OR TRENCH IS TO REMAIN OPEN IT MUST BE PROTECTED WITH BARRICADES OR CONES AT THE DISCRETION OF THE RESIDENT ENGINEER.

RUNWAY AND TAXIWAY CLOSURE

RUNWAY 18-36 WILL REQUIRE CLOSURE ANYTIME WORK IS BEING COMPLETED WITHIN 200-FT OF ITS CENTERLINE. A TAXIWAY WILL REQUIRE CLOSURE ANYTIME WORK IS COMPLETED WITHIN 66 FT OF THE TAXIWAY CENTERLINE. THE CONTRACTOR WILL BE REQUIRED TO NOTIFY THE AIRPORT MANAGER NO LESS THAN 48 HOURS IN ADVANCE OF THE INTENDED CLOSURE TO ENSURE AN ADEQUATE AMOUNT OF TIME IS AVAILABLE TO ISSUE A NOTICE TO AIRMEN (NOTAM) REGARDING THE CLOSURE AND THE EXISTENCE OF EQUIPMENT IN THE AREA. THE NOTAM WILL BE ISSUED BY AIRPORT PERSONNEL. DURING TIMES OF CLOSURE THE WORK WITHIN THE 200-FT OFFSET AREA IS TO BE EXPEDITED TO MINIMIZE THE LENGTH OF THE CLOSURE, AND NO CLOSURE OF RUNWAY 18-36 WILL BE ALLOWED FROM 5 P.M. EACH FRIDAY UNTIL THE FOLLOWING MONDAY AT 7 A.M. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

STEPS TO OPEN RUNWAY OR TAXIWAY

- 1) THE PAVEMENT WILL BE SWEEP BY A POWER BROOM TO REMOVE ANY AND ALL DEBRIS FROM THE PAVEMENT.
- 2) THE AREA WITHIN 200 FEET OF A RUNWAY CENTERLINE AND 66 FEET OF A TAXIWAY CENTERLINE MUST BE SMOOTH GRADED WITH A 1% - 5% SLOPE AWAY FROM THE PAVEMENT EDGE. THE PAVEMENT EDGE DROP-OFF CAN NOT EXCEED 3 INCHES. NO HOLES OR MOUNDS WILL BE PERMITTED WITHIN THE AREA.
- 3) ALL PERSONNEL AND EQUIPMENT WILL BE OUT OF THE 200 FEET AND 66 FEET LIMITS.
- 4) ALL RUNWAY AND TAXIWAY LIGHTING CIRCUITS WILL BE OPERATIONAL.
- 5) RUNWAY AND TAXIWAY CENTERLINE MARKING, IF OBLITERATED, WILL BE REMARKED.
- 6) THE CLOSED PAVEMENTS WILL BE VISUALLY INSPECTED BY AIRPORT PERSONNEL PRIOR TO OPENING. THE CONTRACTOR WILL MAKE ANY CORRECTIONS REQUIRED AS A RESULT OF THE INSPECTION.

150-ENGINEER'S FIELD OFFICE NOTES

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 168 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

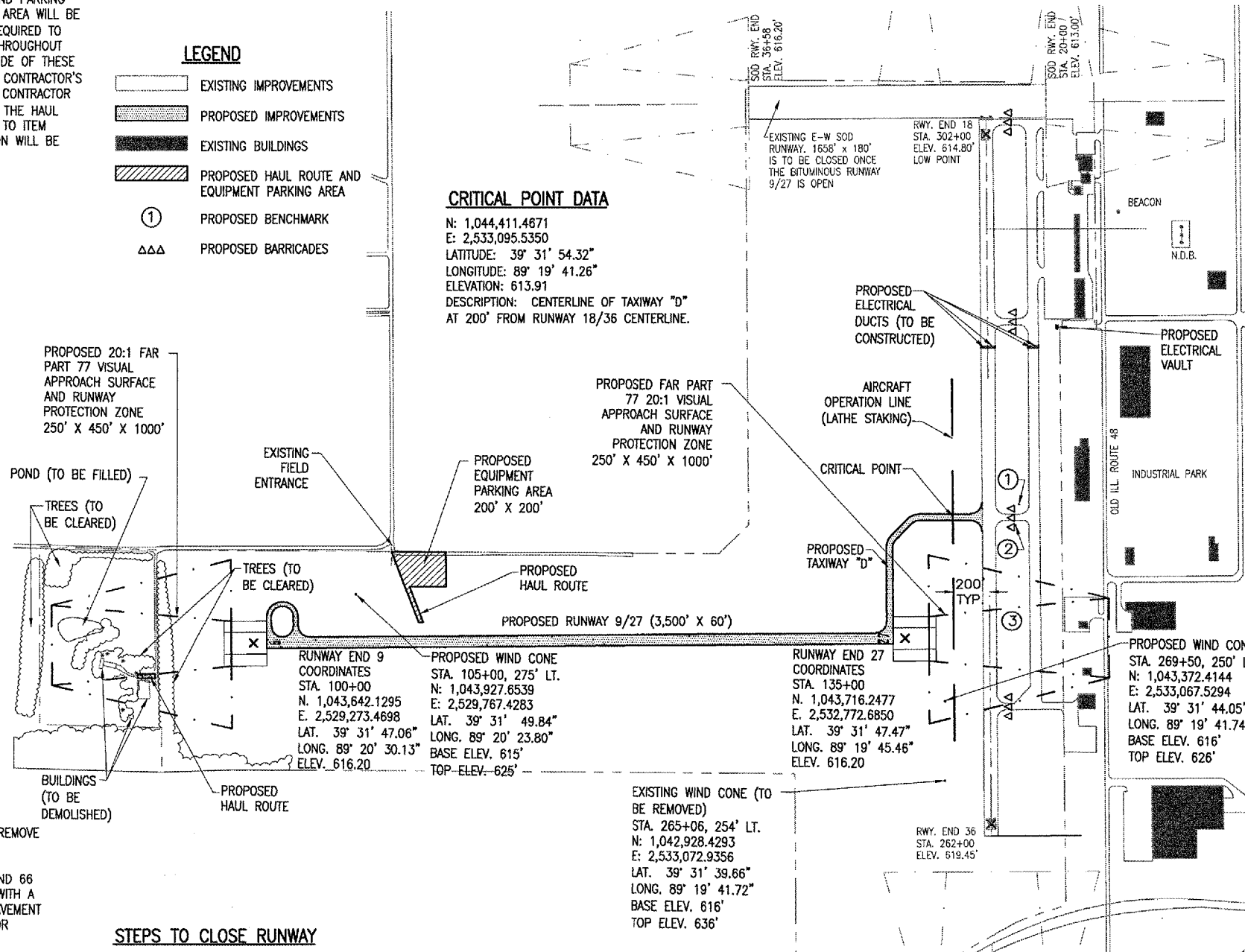
THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS: AR150510 ENGINEER'S FIELD OFFICE 1 L.S.

LEGEND

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA
- PROPOSED BENCHMARK
- PROPOSED BARRICADES

CRITICAL POINT DATA

N: 1,044,411.4671
 E: 2,533,095.5350
 LATITUDE: 39° 31' 54.32"
 LONGITUDE: 89° 19' 41.26"
 ELEVATION: 613.91
 DESCRIPTION: CENTERLINE OF TAXIWAY "D" AT 200' FROM RUNWAY 18/36 CENTERLINE.

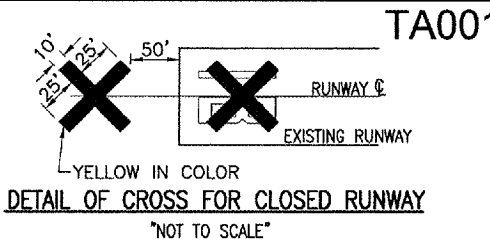


BENCHMARK DATA

NO.	DESCRIPTION	ELEV.
1	"TAYPORT", STAINLESS STEEL ROD	614.22
2	CHISELED "□" ON SW CORNER OF CONC. PAD, "36-18" SIGN	616.05
3	CHISELED "□" ON SW CORNER OF CONC. PAD OF DROP INLET	616.05

EROSION CONTROL

THIS PROJECT WILL DISTURB MORE THAN 1 ACRE OF LAND, THEREFORE AN N.P.D.E.S. PERMIT WILL BE REQUIRED.



NOTE:

COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE RESIDENT ENGINEER. THE CROSSES WILL BE PLACED OVER THE NUMERALS AND SECURED IN A MANNER APPROVED BY THE RESIDENT ENGINEER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

WHEN THE EAST-WEST RUNWAY BEGINS TO LOOK LIKE A RUNWAY, CROSSES WILL BE PLACED 50' OFF THE END OF THE EAST-WEST RUNWAY.

J.U.L.I.E. INFORMATION

COUNTY: CHRISTIAN
 CITY: TAYLORVILLE
 TOWNSHIP: TAYLORVILLE
 SECTION NO.: 32
 ADDRESS: TAYLORVILLE MUNICIPAL AIRPORT
 R.R. 3, BOX 364
 TAYLORVILLE, IL 62568

UTILITY NOTE

THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND AGENCIES WHICH HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) TO ACCOMPLISH THE ABOVE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY ALL UNDERGROUND NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UNDERGROUND IMPROVEMENTS WILL BE LOCATED AT THE CONTRACTOR'S OWN EXPENSE PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.

PROPOSED SAFETY PLAN

GENERAL - THE TAYLORVILLE MUNICIPAL AIRPORT IS COMPRISED OF A NORTH-SOUTH RUNWAY AND A SOD STRIP. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING THE NORTH-SOUTH RUNWAY ANY TIME THE CONTRACTOR IS WORKING WITHIN 200' OF THE RUNWAY CENTERLINE. THE RUNWAY WILL BE CLOSED ONLY DURING THE CONSTRUCTION DAY. AT THE END OF EACH CONSTRUCTION DAY THE CONTRACTOR WILL SMOOTH GRADE ALL AREAS WITHIN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER AND RE-OPEN THE RUNWAY. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3) FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (122.80 MHz.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE TAYLORVILLE MUNICIPAL AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

BARRICADES, TRAFFIC CONES, AND LATHE STAKING

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES, TRAFFIC CONES, AND LATHE STAKING AS SHOWN AND AT THE DISCRETION OF THE RESIDENT ENGINEER. THE BARRICADES ON THE TAXIWAYS WILL BE REMOVED WHILE RUNWAY 18-36 IS OPEN AND ERECTED UPON THE RUNWAY CLOSURE. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR STEADY BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE CONTRACTOR WILL BE REQUIRED TO PLACE LATHE STAKING AT THE AIRCRAFT OPERATIONAL LIMIT, THE 200-FT OFFSET FROM RUNWAY 18-36, TO PROVIDE A VISUAL INDICATION TO THE WORKERS AND DRIVERS IN THE AREA THE BOUNDARY TO WHICH THEY MUST REMAIN BEHIND WHILE RUNWAY 18-36 IS ACTIVE. THE BARRICADES, CONES, AND LATHE STAKING, THEIR MAINTENANCE, PLACEMENT, AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

DATE	REVISION

TAYLORVILLE MUNICIPAL AIRPORT
 TAYLORVILLE, ILLINOIS
 A.I.P. PROJ.: 3-17-0100-07
 I.L. PROJ.: TAZ-3166

FILE PROJECT NO.	DATE	BY
828-03RWYCD_0800	02/05/04	RAW
R-003SEY.DWG	02/05/04	RAW
Scale: 1" = 400'	03/14/05	CAH



CONSTRUCT
 RUNWAY 9-27
 PROPOSED
 SAFETY
 PLAN

MAY 04, 2005 9:00 AM RAW I:\AIRPORTS\TAYLORVILLE\828-03RWYCD\AIRPORT_SHEETS\R-003SEY.DWG - SAFETY