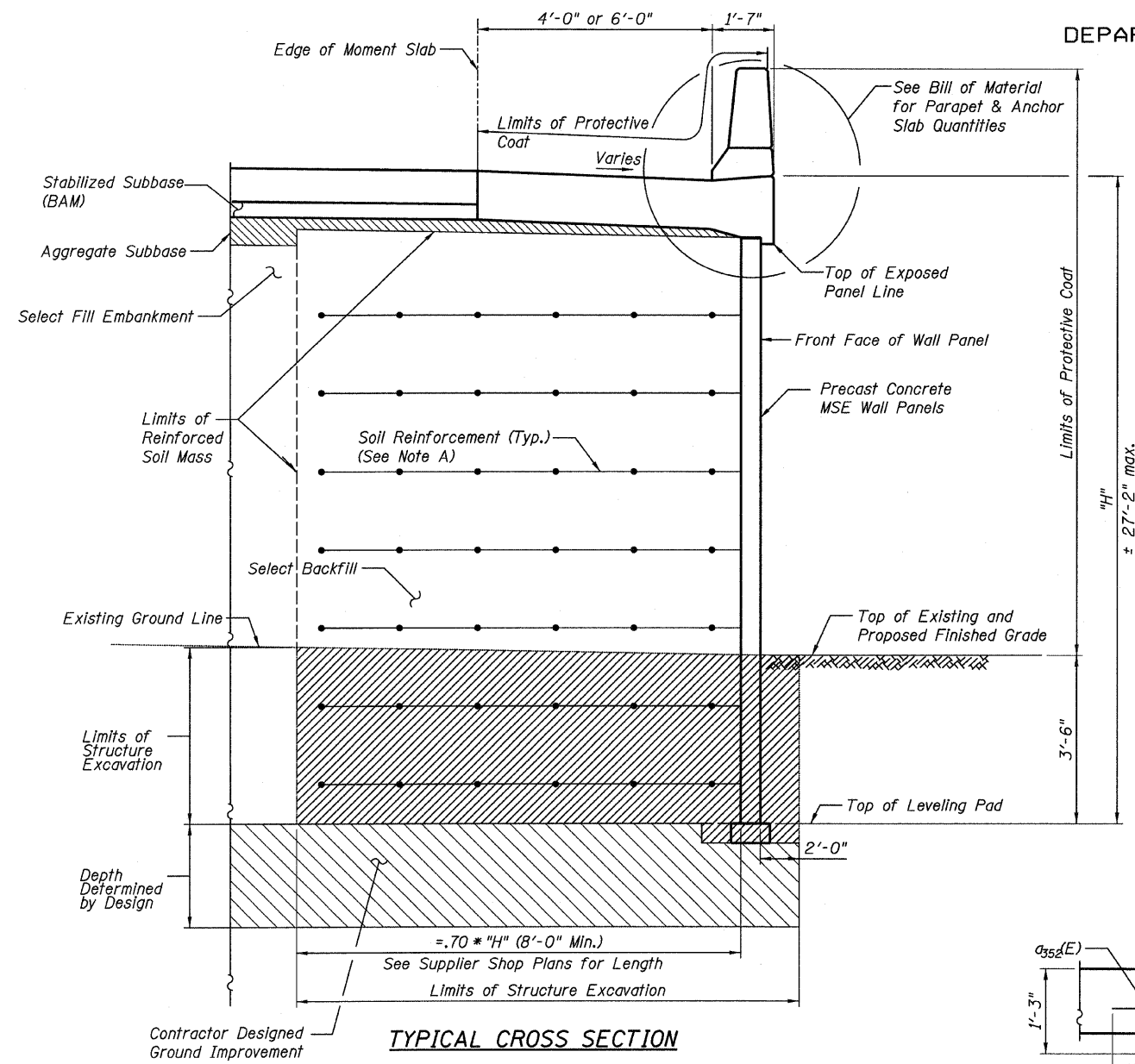
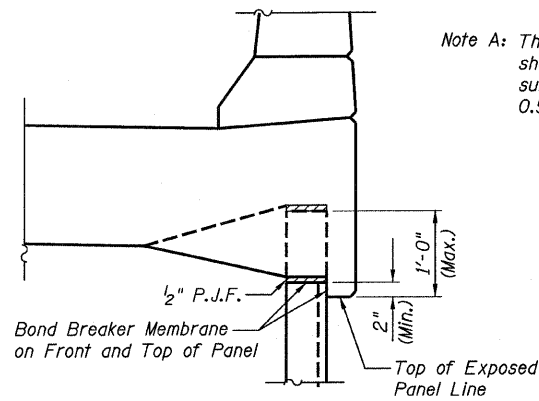


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

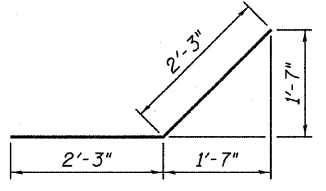


TYPICAL CROSS SECTION

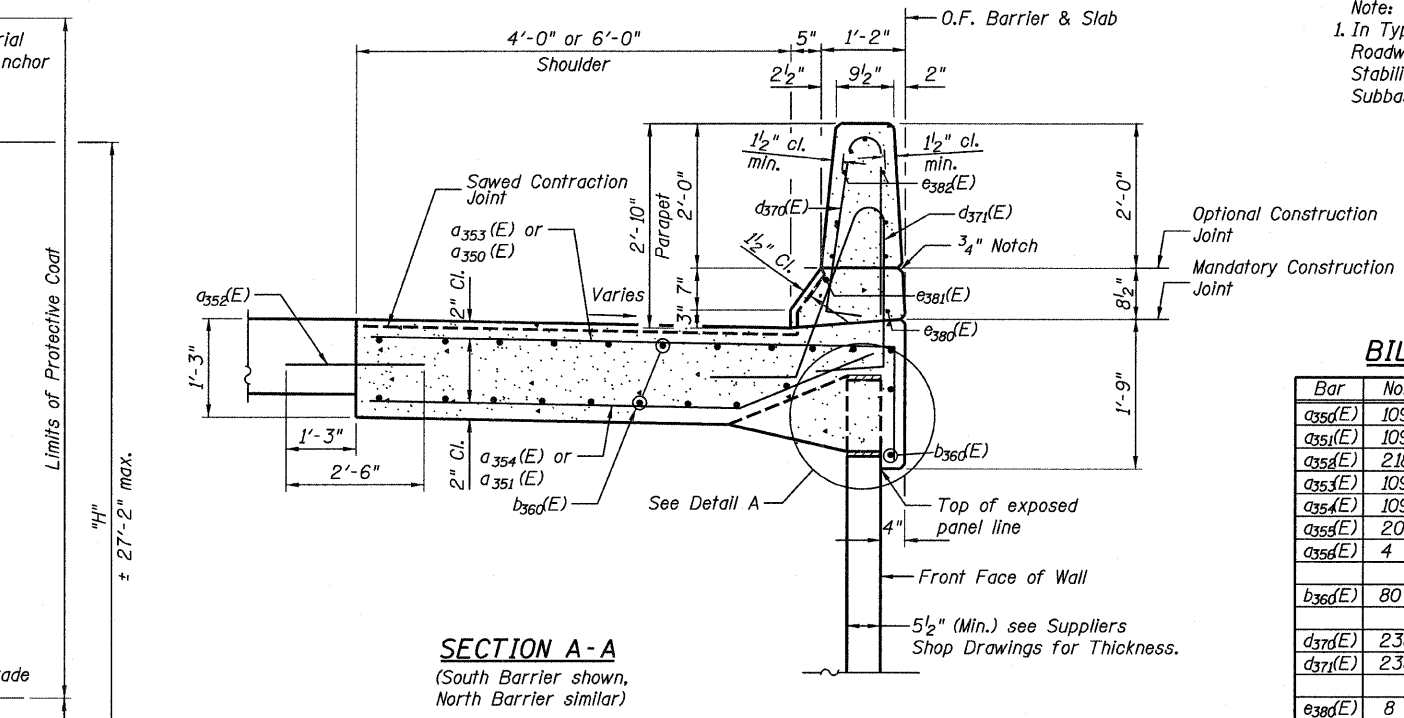
Note A: The M.S.E. Wall Suppliers internal stability design shall account for the anchorage slab's bearing pressure surcharge of 1.0 ksf and a horizontal sliding force of 0.5 k/ft of wall.



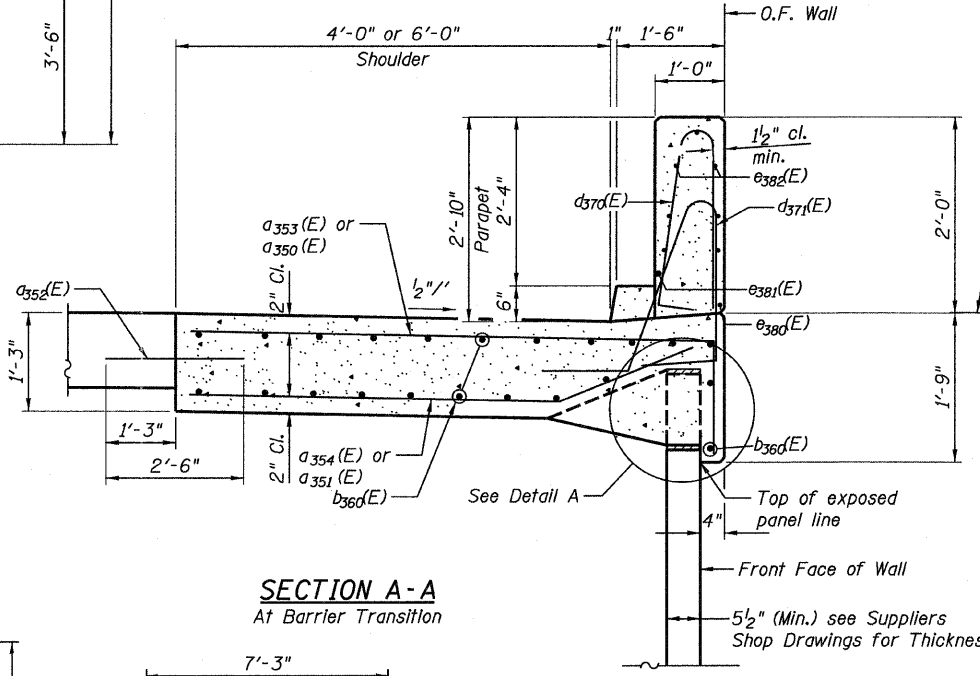
DETAIL A



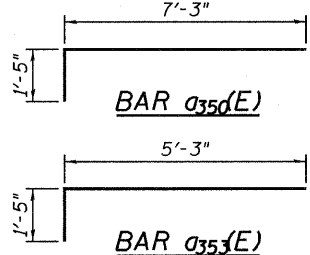
BAR a356(E)



SECTION A-A
(South Barrier shown, North Barrier similar)

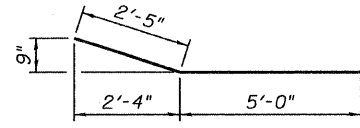


SECTION A-A
At Barrier Transition

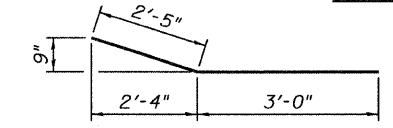


BAR a350(E)

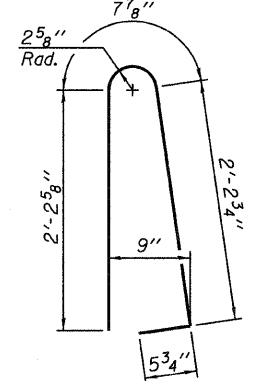
BAR a351(E)



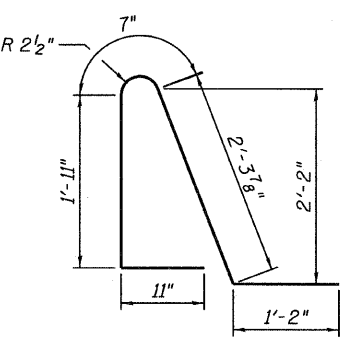
BAR a351(E)



BAR a354(E)



BAR d370(E)



BAR d371(E)

RAMP 3 MOMENT SLAB
SECTIONS AND BAR LIST
STRUCTURE NO. 016-1306

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a350(E)	109	#6	8'-8"	—
a351(E)	109	#6	7'-5"	—
a352(E)	218	#6	2'-6"	—
a353(E)	109	#6	6'-8"	—
a354(E)	109	#6	5'-5"	—
a355(E)	20	#6	5'-6"	—
a356(E)	4	#6	4'-6"	—
b360(E)	80	#4	29'-0"	—
d370(E)	238	#5	5'-7"	U
d371(E)	238	#5	6'-11"	U
e380(E)	8	#4	28'-3"	—
e381(E)	8	#8	30'-4"	—
e382(E)	84	#4	14'-8"	—
e383(E)	28	#4	8'-10"	—
Reinforcement Bars, Epoxy Coated		Pound	12,070	
Concrete Superstructure		Cu. Yds.	102.8	

Bars indicated thus 1 x 15- #5 etc. indicates 1 line of bars with 15 lengths per line.

Note:
1. In Typical Cross Section, See Roadway plans for payment of Stabilized Subbase, Aggregate Subbase and Embankment.

TYLIN INTERNATIONAL

DESIGNED	EKH	REVISIONS	
CHECKED	AMD, MMB	NAME	DATE
DRAWN	EKH		
CHECKED	AMD, MMB		
DATE	03/25/2011		

SHEET NO. 5	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	0711.2R & 1011.1BR	COOK	741	588
9 SHEETS	CONTRACT NO. 60999				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

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