

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**
FAU 0298 BLACK ROAD AT FAU 0396 RIVER ROAD
INTERSECTION IMPROVEMENTS (3R)

SECTION 09-00028-00-CH
PROJECT: M-9003 (805)
JOB NO. C-91-726-09
VILLAGE OF SHOREWOOD
WILL COUNTY

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 1
FED. ROAD DIST. NO. 1	ILLINOIS C-91-726-09	FED AID PROJECT M-9003 (805)		

CONTRACT NO.: 63562

TRAFFIC DATA

RIVER ROAD
POSTED SPEED = 40 MPH
DESIGN SPEED = 45 MPH
EXISTING ADT = 11,000 (2009)
PROPOSED ADT = 12,500 (2012)
URBAN COLLECTOR

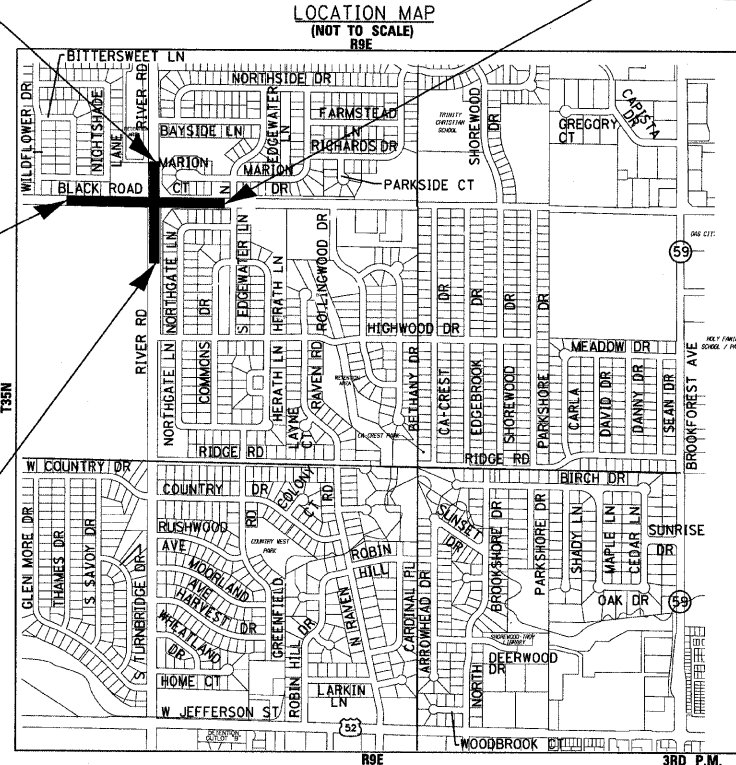
BLACK ROAD
POSTED SPEED = 40 MPH
DESIGN SPEED = 45 MPH
EXISTING ADT = 8,700 (2009)
PROPOSED ADT = 9,800 (2012)
URBAN COLLECTOR

RIVER ROAD (FAU 0396)
END IMPROVEMENTS
STA 116 + 81

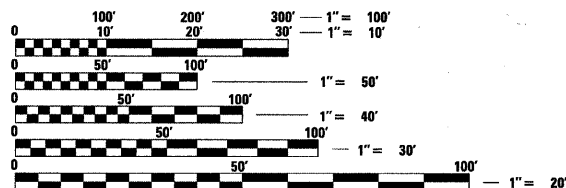
BLACK ROAD (FAU 0298)
END IMPROVEMENTS
STA 220 + 12

BLACK ROAD (FAU 0298)
BEGIN IMPROVEMENTS
STA 205 + 49

RIVER ROAD (FAU 0396)
BEGIN IMPROVEMENTS
STA 107 + 82



TROY TOWNSHIP
GROSS LENGTH OF IMPROVEMENT = 2,362 LF OR 0.447 MILES
NET LENGTH OF IMPROVEMENT = 2,362 LF OR 0.447 MILES



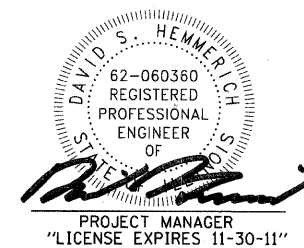
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. X2190710



CONTACT JULIE AT 811 OR 800-892-0123 WITH THE FOLLOWING:
COUNTY = WILL
CITY-TOWNSHIP = SHOREWOOD-TROY
SEC. & 1/4 SEC. NO. = SW4, SE5, NE8, NW9
48 HOURS (2 working days) BEFORE YOU DIG

BAXTER & WOODMAN, INC.
STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
LICENSE NO. - 184-001121 - EXPIRES 4/30/2011



PROJECT MANAGER
"LICENSE EXPIRES 11-30-11"



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 3/11/2011
Paul F. Carroll
VILLAGE OF SHOREWOOD, VILLAGE ADMINISTRATOR

PASSED 2-15-2011
Cheryl Chastain
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW FEBRUARY 15, 2011
Diana M. O'Keefe
DEPUTY DIRECTOR OF
HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

CONTRACT NO. 63562

B&W PROJECT NO.: 090491

DATE: 1-31-11

Copyright © 2010 by Baxter & Woodman, Inc.
STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
EXPIRES 4/30/2011
PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847)705-4406, SCHAUMBURG, IL

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-05	TEMPORARY EROSION CONTROL SYSTEMS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
602011-02	CATCH BASIN TYPE C
602301-03	INLET TYPE A
602401-03	MANHOLE TYPE A
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS TYPE I
604051-03	FRAME AND GRATE TYPE II
604056-03	FRAME AND GRATE TYPE 11V
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-04	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701-07	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001-02	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877011-04	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
878001-08	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS

COMMITMENTS

- EXISTING LANDSCAPING ON THE NORTHEAST AND SOUTHEAST CORNERS NOT IN CONFLICT WITH ANY PROPOSED IMPROVEMENTS SHALL BE PRESERVED FROM HARM. LANDSCAPING TO BE RELOCATED SHALL BE REPLACED TO MATCH THE EXISTING LAYOUT AS MUCH AS PRACTICAL.

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, BENCHMARKS AND COMMITMENTS
3	GENERAL NOTES
4 -8	SUMMARY OF QUANTITIES
9 -10	TYPICAL SECTIONS, STRUCTURAL DESIGN DATA AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS
11	SCHEDULE OF QUANTITIES
12	ALIGNMENT AND TIES
13-15	GEOMETRIC PLAN AND PROFILE: BLACK ROAD
16-18	GEOMETRIC PLAN AND PROFILE: RIVER ROAD
19	MAINTENANCE OF TRAFFIC STAGING NOTES
20	SUGGESTED MAINTENANCE OF TRAFFIC BLACK ROAD RECONSTRUCTION (STAGE III)
21-22	EROSION CONTROL PLAN
23-25	DRAINAGE AND UTILITY PLAN AND PROFILE: BLACK ROAD
26-28	DRAINAGE AND UTILITY PLAN AND PROFILE: RIVER ROAD
29-30	PAVEMENT MARKING PLAN
31-36	DISTRICT 1 TRAFFIC SIGNAL DESIGN DETAILS (SHTS 1 THRU 6)
37	LED INTERNALLY ILLUMINATED STREET NAME SIGN
38	TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN BLACK ROAD AND RIVER ROAD
39	TEMPORARY CABLE PLAN AND TEMPORARY PHASE DESIGNATION DIAGRAM BLACK ROAD AND RIVER ROAD
40	TRAFFIC SIGNAL INSTALLATION PLAN BLACK ROAD AND RIVER ROAD
41	SCHEDULE OF QUANTITIES, CABLE PLAN AND PHASE DESIGNATION DIAGRAM BLACK ROAD AND RIVER ROAD
42-45	LIGHTING PLANS BLACK ROAD AND RIVER ROAD
46	SINGLE LINE DIAGRAM
47-48	STREET LIGHTING DETAILS
49	MISCELLANEOUS DETAILS
50	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)
51	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
52	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
53	COMBINATION LIGHTING AND TRAFFIC POLE MOUNTED ELECTRIC SERVICE BOX DETAIL (BE-230)
54	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
55	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
56	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
57	ARTERIAL ROAD INFORMATION SIGN (TC-22)
58-60	CROSS SECTIONS - BLACK ROAD
61-63	CROSS SECTIONS - RIVER ROAD

BENCHMARK LIST

B.M. #6	RAILROAD SPIKE IN PPUC ON SOUTH SIDE OF BLACK STREET ON WEST LEG OF PROJECT. ELEV = 621.37
B.M. #7	RAILROAD SPIKE IN POWER POLE AT SOUTHWEST CORNER OF BLACK ROAD AND RIVER ROAD. (TO BE RELOCATED) ELEV = 619.74
B.M. #8	RAILROAD SPIKE IN POWER POLE ON WEST SIDE OF RIVER ROAD NORTH OF BLACK ROAD. ELEV = 616.64
B.M. #9	RAILROAD SPIKE IN POWER POLE ON EAST SIDE OF RIVER ROAD APPROXIMATELY 700' SOUTH OF BLACK ROAD. ELEV = 607.53
B.M. #10	RAILROAD SPIKE IN POWER POLE ON SOUTH SIDE OF BLACK ROAD APPROXIMATELY 700' EAST OF RIVER ROAD. ELEV = 618.99

ALL BENCHMARKS ON NAVD 88 VERTICAL DATUM.

COPYRIGHT © 2008 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 08-00121 - EXPIRES 7/30/2011
 2/27/2011 2:58:58 PM
 ...\\pilot01\pilot\pilot\PH2-Index-Stnds.sht
 ..\\PLOT\090491-PH2-Index-Stnds.sht
 ..\\C:\Users\ldh\Documents\090491-PH2-Index-Stnds.sht



DESIGNED - DSH	REVISED - 1-31-11 PER IDOT
DRAWN - CJC	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-Index-Stnds.sht

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT**

**INDEX OF SHEETS, HIGHWAY STANDARDS,
BENCHMARKS AND COMMITMENTS**

SCALE: NONE

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298 0396	09-00028-00-CH	WILL	63	2
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
2. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY OR SPRINKLER SYSTEM THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.
4. THE CONTRACTOR SHALL NOTIFY THE VILLAGE PUBLIC WORKS ADMINISTRATOR AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
5. THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE ENGINEER RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
6. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
7. EXISTING PAVEMENT, CURB AND GUTTER, BIKE PATH AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
8. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
9. THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCH ABOVE THE GUTTER FLAG.
10. THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL.
11. A ½-INCH THICK EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE CONCRETE SIDEWALK AND CURB. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE SIDEWALK.
12. B-6.12 CURB AND GUTTER SHALL BE TAPERED TO EXISTING CURB AND GUTTER IN A FIVE (5) FOOT LENGTH WHEREVER THE PROPOSED CURB AND GUTTER MEETS THE EXISTING, WITH AN EXPANSION JOINT PLACED AT THE START OF THE TAPER.

13. ALL POSTS, RAILROAD TIES, AND DECORATIVE TIMBER IN CONFLICT WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED AND RELOCATED AS DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION AND SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION. EVERY EFFORT SHALL BE MADE BY THE CONTRACTOR WHEN REMOVING THESE ITEMS TO PRESERVE THEM FROM HARM. ITEMS NOT RELOCATED SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR.
14. PRIOR TO CONSTRUCTION OF ANY PROPOSED UTILITIES, THE CONTRACTOR SHALL EXCAVATE AND LOCATE THE EXISTING UTILITIES TO VERIFY THEIR LOCATION, SIZE, AND DEPTH TO INSURE THAT GRADE CONFLICTS WILL NOT OCCUR. THE COST OF THIS EXPLORATION SHALL BE PAID FOR AS EXPLORATION TRENCH, SPECIAL.
15. CONNECTION OF PROPOSED STORM SEWER INTO EXISTING STORM SEWER OR EXISTING STORM SEWER STRUCTURES SHALL BE INCLUDED IN THE COST OF STORM SEWERS.
16. CONNECTION OF EXISTING STORM SEWER INTO PROPOSED STORM SEWER STRUCTURES SHALL BE INCLUDED IN THE COST OF THE STORM SEWER STRUCTURE. ANY ADDITIONAL STORM SEWER PIPE REQUIRED TO MAKE THE CONNECTION SHALL BE OF THE SAME SIZE AND MATERIAL TYPE AS THE EXISTING STORM SEWER AND SHALL BE INCLUDED IN THE COST OF THE STORM SEWER STRUCTURE.
17. IF ANY STORM SEWER LATERALS ARE FOUND DURING CONSTRUCTION AND ARE NOT IDENTIFIED ON THE PLANS, THEY SHALL BE CONNECTED TO THE PROPOSED STORM SEWER SYSTEM AND INCLUDED IN THE COST OF THE STORM SEWER CONSTRUCTION.
18. STORM STRUCTURE OFFSET LOCATIONS ARE TO THE EDGE OF PAVEMENT IF THE STRUCTURE IS IN THE CURB LINE OR TO THE CENTER OF STRUCTURE IF THE STRUCTURE IS NOT IN THE CURBLINE.
19. FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF COST OF THE STRUCTURE.
20. ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN THE ITEM "MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS."
21. THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY.
22. FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.
23. PIPE UNDERDRAINS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAIL ON THE PLANS AND SHALL INCLUDE EXCAVATION, CONNECTIONS TO EXISTING OR PROPOSED STORM PIPES, DRAINAGE STRUCTURES OR PIPE DRAINS; GEOTECHNICAL FABRIC SOCK; AND CA-11 TRENCH BACKFILL TO THE BOTTOM OF THE HOT-MIX ASPHALT BASE COURSE.
24. DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A CAST-IN-PLACE "DURALAST" 24" WIDE CAST IRON PLATES AS MANUFACTURED BY EAST JORDAN IRONWORKS OR AN APPROVED EQUAL. THE PANEL SHALL BE CAST IRON AND COMPLY WITH ADA REQUIREMENTS. THE DOMES LOCATED ON THE PANEL SHALL PARALLEL THE PAVEMENT CROSS WALK WITH THE CLOSEST EDGE LOCATED AT THE BACK OF CURB. A 2' X 4' CONFIGURATION SHALL BE PLACED AT 6' CROSSWALKS AND A 2' X 8' CONFIGURATION SHALL BE PLACED AT 10' CROSSWALKS. THE PANEL SHALL BE POWDER-COATED A RED COLOR THAT SHALL BE APPROVED BY THE ENGINEER. INSTALLATION SHALL BE IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS.
25. FULL-DEPTH SAW CUTS SHALL BE MADE ALONG THE EXISTING EDGE OF PAVEMENT WHERE CURB AND GUTTER IS TO BE REMOVED TO ENSURE A NEAT VERTICAL FACE BETWEEN THE EXISTING AND PROPOSED. THESE SAW CUTS SHALL BE INCLUDED IN THE COST OF COMBINATION CURB AND GUTTER REMOVAL.
26. INLINE CONNECTIONS OF PROPOSED STORM SEWER TO EXISTING STORM SEWER TO REMAIN SHALL BE MADE WITH A RUBBER MISSION COUPLING WITH STAINLESS STEEL BANDS. THIS WORK WILL BE INCLUDED IN THE PROPOSED STORM SEWER.

COPYRIGHT © 2008 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - BA-40027 - EXPIRES 4/20/2011
 2/4/12/11
 ...\\deltor\lhw\app\pfe22x34.dwg
 ..\\P\015\09008\06\01\01
 ..C:\p\015\09008\06\01\01\01



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED - 2-14-11 PER IDOT
DATE - 12/6/10	FILE - 090491-PH2-Gen ntes.sht

**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

GENERAL NOTES

SCALE: _____ STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298 0396	09-00028-00-GH	WILL	63	3
C-91-726-09			CONTRACT NO. 63562	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(385)				

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE			NON-PARTICIPATING QUANTITY
				ROADWAY 0003	SIGNALS/LIGHTS 0021	TRAINEES 0042	
				QUANTITY	QUANTITY	QUANTITY	
20200100	EARTH EXCAVATION	CU YD	1,934	1,934			
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	2,266	2,266			
20800150	TRENCH BACKFILL	CU YD	59	59			
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	1,305	1,305			
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	6,000	6,000			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	75	75			
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	75	75			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	75	75			
25200110	SODDING, SALT TOLERANT	SQ YD	6,000	6,000			
25200200	SUPPLEMENTAL WATERING	UNIT	90	90			
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	124	124			
28000305	TEMPORARY DITCH CHECKS	FOOT	42	42			
28000400	PERIMETER EROSION BARRIER	FOOT	635	635			
28000500	INLET AND PIPE PROTECTION	EACH	1	1			
28000510	INLET FILTERS	EACH	33	33			
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	481	481			
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	652	652			
35501320	HOT-MIX ASPHALT BASE COURSE, 9"	SQ YD	3,559	3,559			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	14	14			
40600300	AGGREGATE (PRIME COAT)	TON	52	52			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	3	3			
40600895	CONSTRUCTING TEST STRIP	EACH	1	1			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	76	76			
40600990	TEMPORARY RAMP	SQ YD	395	395			
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	34	34			
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	64	64			
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1,120	1,120			
42001300	PROTECTIVE COAT	SQ YD	966	966			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	4,325	4,290	35		
42400800	DETECTABLE WARNINGS	SQ FT	96	96			

* INDICATES SPECIALTY ITEM

COPYRIGHT © 2005 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 384-00021 - EXPIRES 4/30/2011
 2/25/2011
 \\PLOT5\WORK\PH2\PH2.tbl
 \\C:\PLOT5\WORK\PH2\PH2.tbl
 \\C:\PLOT5\WORK\PH2\PH2.tbl



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-S00.shx

**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

SUMMARY OF QUANTITIES

SCALE: NONE STA. TO STA.

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 4
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(805)				

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE			NON-PARTICIPATING QUANTITY
				ROADWAY 0003	SIGNALS/LIGHTS 0021	TRAINEES 0042	
				QUANTITY	QUANTITY	QUANTITY	
44000100	PAVEMENT REMOVAL	SQ YD	1,019	1,019			
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	9,418	9,418			
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1,866	1,866			
44000600	SIDEWALK REMOVAL	SQ FT	3,459	3,459			
44002214	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3 1/2"	SQ YD	157	157			
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	8	8			
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	34	34			
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	35	35			
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	55	55			
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	243	243			
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	1	1			
54247100	GRATING FOR CONCRETE FLARED END SECTION 15"	EACH	1	1			
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	19	19			
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	73	73			
550A2320	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 12"	FOOT	89	89			
55100500	STORM SEWER REMOVAL 12"	FOOT	162	162			
55100700	STORM SEWER REMOVAL 15"	FOOT	32	32			
56400100	FIRE HYDRANTS TO BE MOVED	EACH	3	3			
60107600	PIPE UNDERDRAINS 4"	FOOT	125	125			
60207005	CATCH BASINS, TYPE C, TYPE 1 FRAME, CLOSED LID	EACH	1	1			
60207905	CATCH BASINS, TYPE C, TYPE 11 FRAME AND GRATE	EACH	2	2			
60207915	CATCH BASINS, TYPE C, TYPE 11V FRAME AND GRATE	EACH	2	2			
60236800	INLETS, TYPE A, TYPE 11 FRAME AND GRATE	EACH	3	3			
60236825	INLETS, TYPE A, TYPE 11V FRAME AND GRATE	EACH	2	2			
60255500	MANHOLES TO BE ADJUSTED	EACH	2	2			
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	1	1			
60500050	REMOVING CATCH BASINS	EACH	2	2			
60500060	REMOVING INLETS	EACH	5	5			
67100100	MOBILIZATION	L SUM	1	1			
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1			

* INDICATES SPECIALTY ITEM

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC. ... \photos\ver\PH22X24.dwg
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM ... \PLOTS\090491-PH2-101
 LICENSE NO. 0298-0396 - EXPIRES 7/31/2011
 40834 PM
 SURVEY\DRAWINGS\WORKS\090491-PH2-101.dwg



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-500.sh+

VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT

SUMMARY OF QUANTITIES

SCALE: NONE STA. TO STA.

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 5
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003385				

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE			NON-PARTICIPATING QUANTITY
				ROADWAY 0003 QUANTITY	SIGNALS/LIGHTS 0021 QUANTITY	TRAINEES 0042 QUANTITY	
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1			
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	6,447	6,447			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	5,680	5,680			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	2,610	2,610			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	328	328			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9,592	9,592			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2,706	2,706			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	837	837			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	166	166			
78300100	PAVEMENT MARKING REMOVAL	SQ FT	728	728			
* 80400100	ELECTRICAL SERVICE INSTALLATION	EACH	1		1		
* 80400200	ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1		1		
* 81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	37		37		
* 81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	35		35		
* 81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	20		20		
* 81018700	CONDUIT PUSHED, 3" DIA., GALVANIZED STEEL	FOOT	535		535		
* 81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	435		435		
* 81400100	HANDHOLE	EACH	3		3		
* 81400300	DOUBLE HANDHOLE	EACH	1		1		
* 81400730	HANDHOLE, COMPOSITE CONCRETE	EACH	6		6		
* 81603070	UNIT DUCT, 600V, 2-1C NO.2, 1/C NO. 4 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	5,084		5,084		
* 81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	4,482		4,482		
* 81702450	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 10	FOOT	854		854		
* 82103250	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO-CELL CONTROL, 250 WATT	EACH	4		4		
* 82500350	LIGHTING CONTROLLER, BASE MOUNTED, 240VOLT, 100AMP	EACH	1		1		
* 83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	80		80		
* 84200804	REMOVAL OF POLE FOUNDATION	EACH	4		4		
* 8440116	RELOCATE EXISTING LIGHTING UNIT SPECIAL	EACH	4		4		

* INDICATES SPECIALTY ITEM

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 08-000211, EXPIRES 4/30/2011
 46253737.W
 12/27/2011
 ...:\p01\dr\va-vp022334.dwt
 ...:\p1\ots\090491-ph2-500.sht
 ...:\p1\ots\090491-ph2-500.sht



DESIGNED	DSH	REVISED	- 1-19-11 PER VILLAGE
DRAWN	CJC	REVISED	- 1-31-11 PER IDOT
CHECKED	LDH	REVISED	-
DATE	12/6/10	FILE	- 090491-PH2-500.sht

VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT

SUMMARY OF QUANTITIES

SCALE: NONE

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298 0396	09-00028-00-CH	WILL	63	6
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-9003(385)		

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE			NON-PARTICIPATING QUANTITY
				ROADWAY 0003 QUANTITY	SIGNALS/LIGHTS 0021 QUANTITY	TRAINEES 0042 QUANTITY	
* 85700205	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1		1		
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1,262		1,262		
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2,344		2,344		
* 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1,471		1,471		
* 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1,519		1,519		
* 87301295	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 20 3C	FOOT	300		300		
* 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	40		40		
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT, GROUNDING CONDUCTOR, NO. 6 1C	FOOT	505		505		
* 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4		4		
* 87702930	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 40 FT.	EACH	1		1		
* 87702950	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 44 FT.	EACH	3		3		
* 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	16		16		
* 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4		4		
* 87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	52		52		
* 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4		4		
* 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4		4		
* 88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	4		4		
* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8		8		
* 88200110	TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	8		8		
* 88700200	LIGHT DETECTOR	EACH	2		2		
* 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1		1		
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	8		8		
* 89500400	RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	EACH	1		1		
* 89502500	REMOVE TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1		1		
Z0001050	AGGREGATE SUBGRADE 12"	SQ YD	4,174	4,174			
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	104	104			
* Z0033022	CLEAN AND RELAMP EXISTING LUMINAIRE	EACH	18		18		
* Z0033072	VIDEO VEHICLE DETECTION SYSTEM	EACH	1		1		
Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	349	349			
Z0056610	STORM SEWER (WATER MAIN REQUIREMENTS) 15 INCH	FOOT	48	48			

* INDICATES SPECIALTY ITEM

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 384-00001 - EXPIRES 4/30/2011
 809322 ILL
 2/2/2011
 ...\\net101\proj\APR\PH2\224.dwg
 ...\\net101\proj\APR\PH2\110.dwg
 ...\\net101\proj\APR\PH2\110.dwg



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED - 2-14-11 PER IDOT
DATE - 12/6/10	FILE - 090491-PH2-S00.sht

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT**

SUMMARY OF QUANTITIES

SCALE: NONE STA. TO STA.

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 7
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003385				

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE			NON-PARTICIPATING QUANTITY
				ROADWAY 0003 QUANTITY	SIGNALS/LIGHTS 0021 QUANTITY	TRAINEES 0042 QUANTITY	
* Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1		1		
Z0076600	TRAINEES	HOUR	500			500	
* K0026610	TRANSPLANTED SALVAGED TREES	EACH	22	22			
* K0026840	SHRUBS TO BE RELOCATED	EACH	45	45			
* K0036120	MULCH PLACEMENT 4"	SQ YD	60	60			
* X0321973	MODIFY EXISTING SERVICE INSTALLATION	EACH	1		1		
X0322118	REMOVE CONCRETE FLARED END SECTIONS	EACH	1	1			
* 81500120	REMOVE EXISTING SERVICE INSTALLATION	EACH	8		8		
* 81009100	PAINT NEW COMBINATION MAST ARM AND POLE, OVER 40 FT	EACH	4		4		
* X0325037	PAINT NEW SIGNAL POST	EACH	4		4		
X0326864	BRICK SIDEWALK REMOVAL	SQ FT	275	275			
X0327008	REMOVE AND RELOCATE SIGN (SPECIAL)	EACH	1	1			
X0327036	BIKE PATH REMOVAL	SQ YD	614	614			
X2130010	EXPLORATION TRENCH, SPECIAL	FOOT	100	100			
X4060826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	821	821			
X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	3	3			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	5	5			
X6064200	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)	FOOT	2,145	2,145			
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1			
* 80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1		1		
* X8250500	LIGHTING UNIT COMPLETE, SPECIAL	EACH	4		4		
* 80200120	UNINTERRUPTIBLE POWER SUPPLY	EACH	1		1		
* X8900020	MAINTENANCE OF EXISTING TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1		1		
* XX005937	LED INTERNALLY ILLUMINATED STREET NAME SIGN	EACH	4				4

* INDICATES SPECIALTY ITEM

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-000217 - EXPIRES 4/30/2011
 40818 PM
 \\p01drf\ver\p0122x34.plt
 ..\P_015\080919-21D
 ..\P_015\080919-21D
 ..\P_015\080919-21D



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-S00.shp

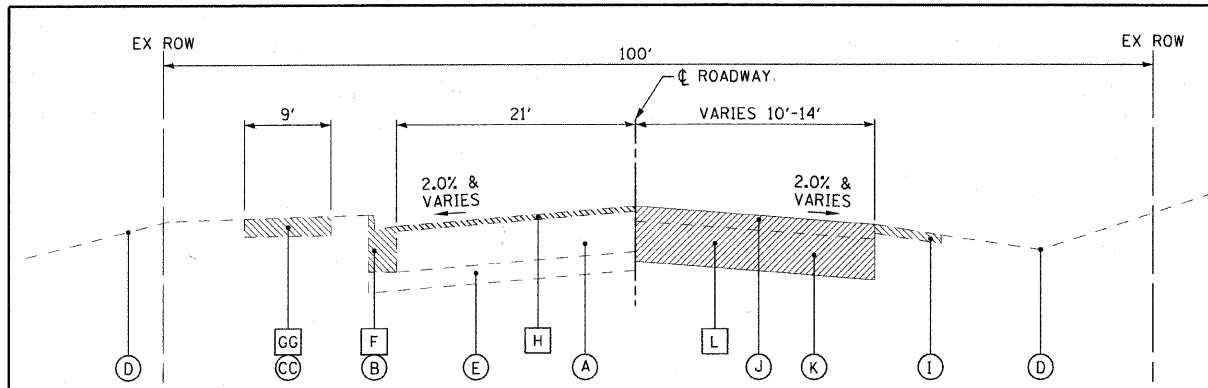
**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

SUMMARY OF QUANTITIES

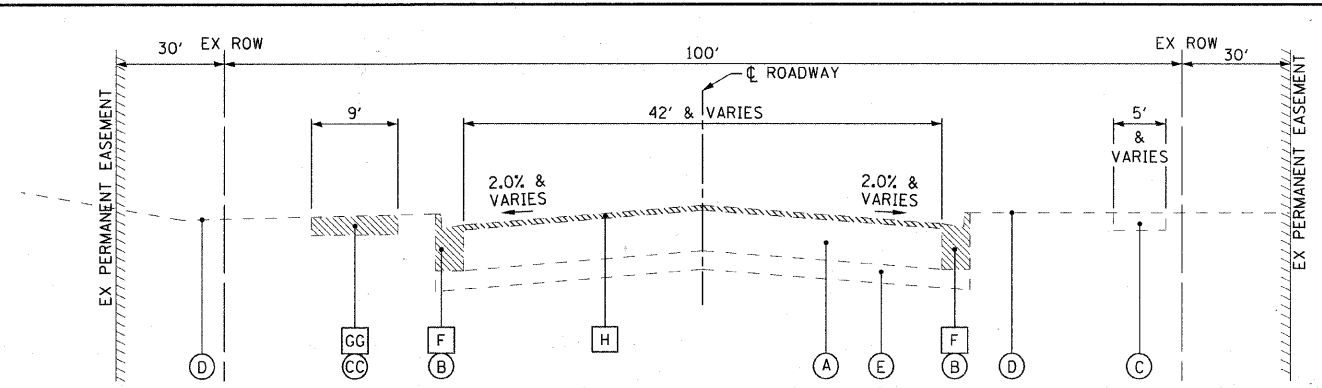
SCALE: NONE

STA. TO STA.

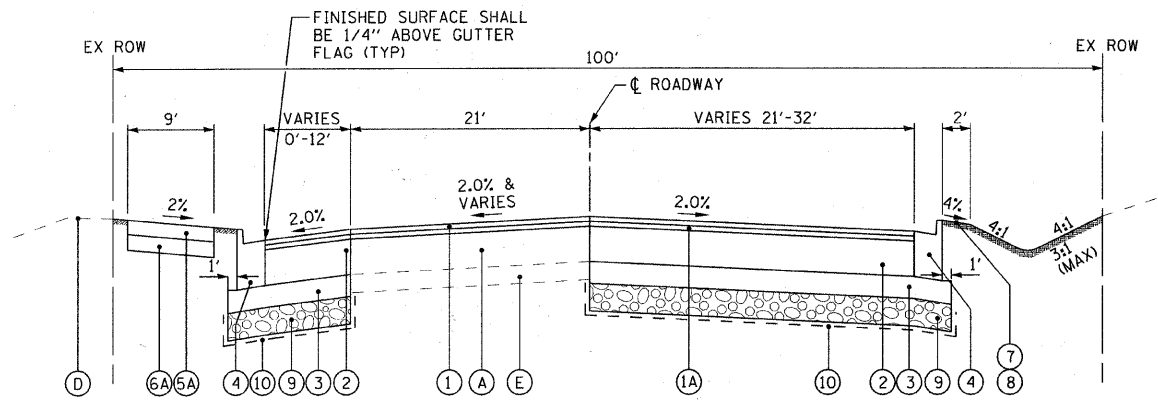
F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 8
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				



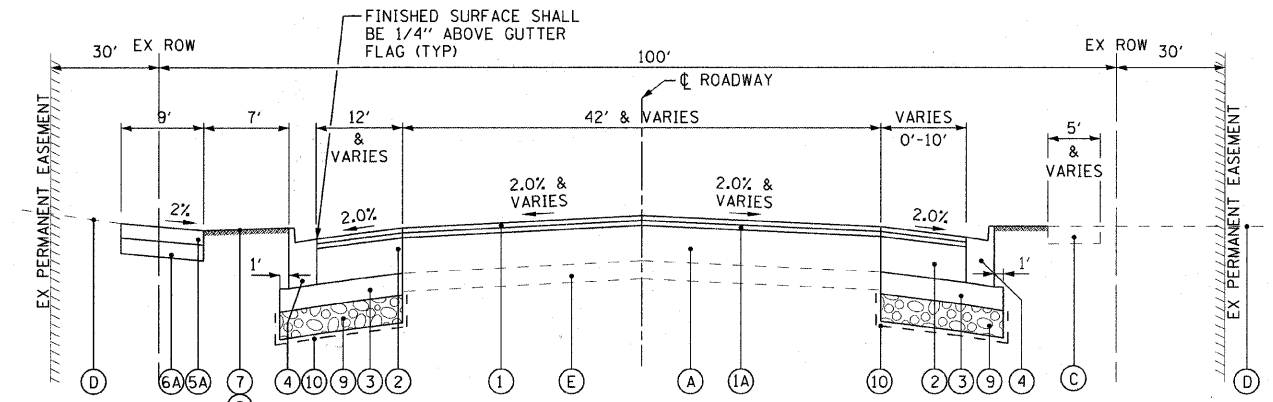
**EXISTING TYPICAL SECTION
BLACK ROAD
STA 208+29 TO STA 212+78**



**EXISTING TYPICAL SECTION
BLACK ROAD
STA 212+78 TO STA 220+12**



**PROPOSED TYPICAL SECTION
BLACK ROAD
STA 208+29 TO STA 212+78**



**PROPOSED TYPICAL SECTION
BLACK ROAD
STA 212+78 TO STA 220+12**

EXISTING LEGEND

- (A) EXISTING HOT-MIX ASPHALT BASE, BINDER & SURFACE COURSES, - 11.5"±
- (B) EXISTING CURB AND GUTTER
- (C) EXISTING CONCRETE SIDEWALK
- (CC) EXISTING HMA BIKE PATH
- (D) GROUND SURFACE (12" AVG. TOPSOIL DEPTH)
- (E) AGGREGATE BASE - 4" AND VARIES
- (F) COMBINATION CURB AND GUTTER REMOVAL
- (G) SIDEWALK REMOVAL
- (GG) BIKE PATH REMOVAL
- (H) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (I) AGGREGATE SHOULDER - 4"±
- (J) EXISTING HOT-MIX ASPHALT SURFACE - 4.5"
- (K) EXISTING AGGREGATE BASE - 8"
- (L) PAVEMENT REMOVAL
- [Hatched Box] ITEM TO BE REMOVED

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- (1A) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
- (2) HOT-MIX ASPHALT BASE COURSE, 9"
- (3) AGGREGATE SUBGRADE - 12"
- (4) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL) (11.5" MIN FLAG DEPTH)
- (5) PCC SIDEWALK - 5"
- (5A) HMA SURFACE COURSE, MIX "C", N50, 2" (ON BIKE PATH)
- (6) AGGREGATE BASE COURSE, TYPE B - 4"
- (6A) AGGREGATE BASE COURSE, TYPE B - 8" (BIKE PATH)
- (7) TOPSOIL FURNISH AND PLACE - 4"
- (8) SODDING, SALT TOLERANT
- (9) POROUS GRANULAR EMBANKMENT, SUBGRADE (AS DETERMINED BY THE ENGINEER)
- (10) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION (IN UNDERCUT AREAS)
- (11) AGGREGATE SHOULDERS, TYPE B-6"

POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN PROVIDED FOR USE AT LOCATIONS INDICATED FOR SOILS THAT ARE CONSIDERED TO BE UNSTABLE OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND IN CONJUNCTION WITH THE IDOT SUBGRADE STABILITY MANUAL (REV. 2005). IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED AS INDICATED IN THE CONTRACT PLANS, THEN THE QUANTITY OF PGES SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

STRUCTURAL DESIGN DATA

STREET	STRUCTURAL DESIGN TRAFFIC			STREET CLASS	TRAFFIC FACTOR	SSR	TEMP	STRAIN	AC	E AC	REQ'D HMA THICKNESS	MECHANISTIC PAVEMENT DESIGN
	PV	SU	MU									
RIVER ROAD	19,008	144	48	II	0.50	POOR	78.2°F	130	PG64-22	585	9 IN	1 1/2" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1" POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 9" HOT-MIX ASPHALT BASE COURSE 12" AGGREGATE SUBGRADE
BLACK ROAD	14,209	218	73	II	0.54	POOR	78.2°F	127	PG64-22	585	9.25 IN	1 1/2" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1" POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 9" HOT-MIX ASPHALT BASE COURSE 12" AGGREGATE SUBGRADE

COPYRIGHT © 2005 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00023 - EXPIRES 4/30/2011
 2/27/2011 10:06:15 AM
 ...:\PLOTS\09081-PH2-12\...
 ...:\PLOTS\09081-PH2-12\...
 ...:\PLOTS\09081-PH2-12\...



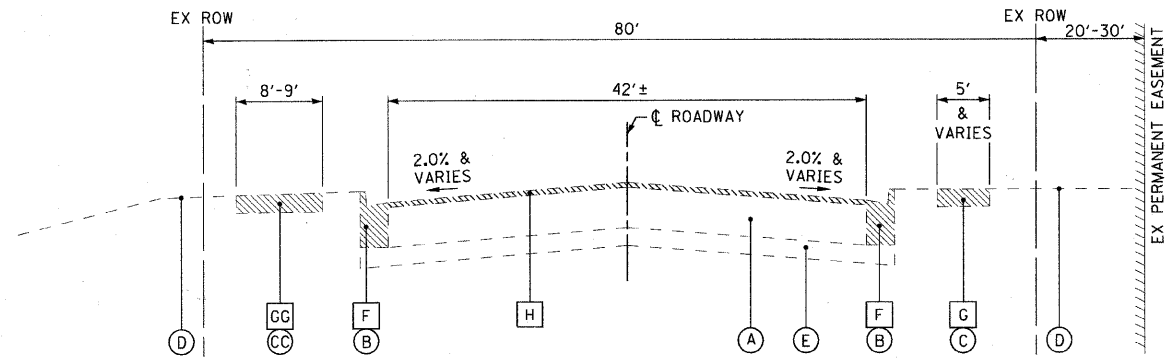
DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-1typsec.shp

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT**

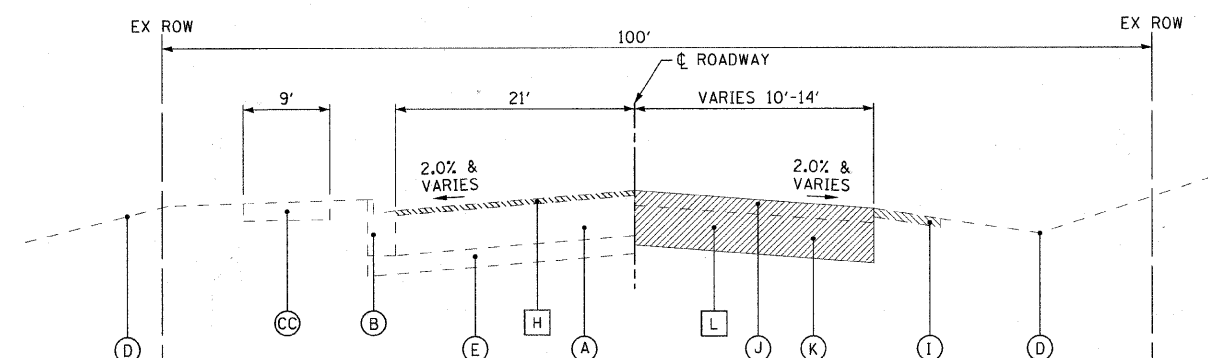
TYPICAL SECTIONS AND STRUCTURAL DESIGN DATA

SCALE: NONE STA. TO STA.

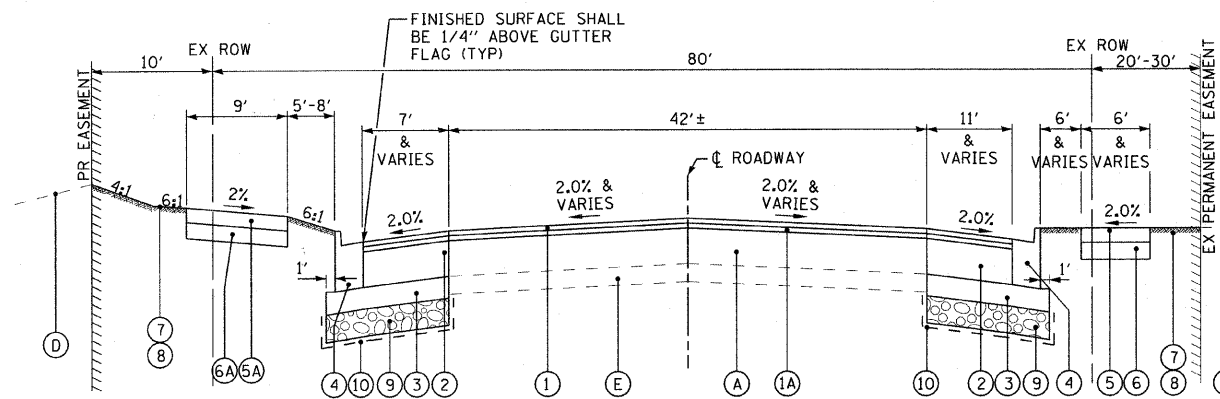
F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 9
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-900313851				



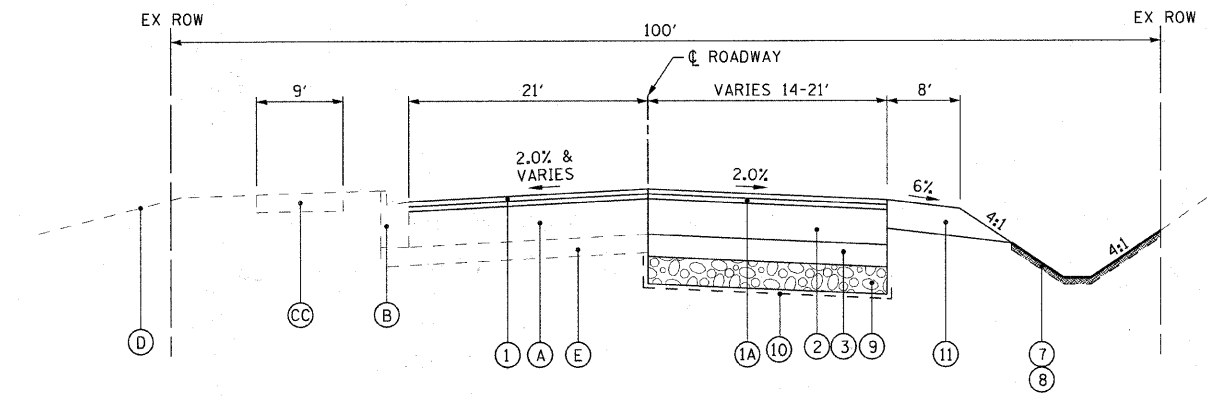
**EXISTING TYPICAL SECTION
RIVER ROAD
STA 107+82 TO STA 116+81**



**EXISTING TYPICAL SECTION
BLACK ROAD
STA 205+49 TO STA 208+29**



**PROPOSED TYPICAL SECTION
RIVER ROAD
STA 107+82 TO STA 116+81**



**PROPOSED TYPICAL SECTION
BLACK ROAD
STA 205+49 TO STA 208+29**

EXISTING LEGEND

- (A) EXISTING HOT-MIX ASPHALT BASE, BINDER & SURFACE COURSES, - 11.5"±
- (B) EXISTING CURB AND GUTTER
- (C) EXISTING CONCRETE SIDEWALK
- (CC) EXISTING HMA BIKE PATH
- (D) GROUND SURFACE (12" AVG. TOPSOIL DEPTH)
- (E) AGGREGATE BASE - 4" AND VARIES
- (F) COMBINATION CURB AND GUTTER REMOVAL
- (G) SIDEWALK REMOVAL
- (GG) BIKE PATH REMOVAL
- (H) HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- (I) AGGREGATE SHOULDER - 4"±
- (J) EXISTING HOT-MIX ASPHALT SURFACE - 4.5"
- (K) EXISTING AGGREGATE BASE - 8"
- (L) PAVEMENT REMOVAL
- [Hatched Box] ITEM TO BE REMOVED

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- (1A) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
- (2) HOT-MIX ASPHALT BASE COURSE, 9"
- (3) AGGREGATE SUBGRADE - 12"
- (4) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL) (11.5" MIN FLAG DEPTH)
- (5) PCC SIDEWALK - 5"
- (5A) HMA SURFACE COURSE, MIX "C", N50, 2" (ON BIKE PATH)
- (6) AGGREGATE BASE COURSE, TYPE B - 4"
- (6A) AGGREGATE BASE COURSE, TYPE B - 8" (BIKE PATH)
- (7) TOPSOIL FURNISH AND PLACE - 4"
- (8) SODDING, SALT TOLERANT
- (9) POROUS GRANULAR EMBANKMENT, SUBGRADE (AS DETERMINED BY THE ENGINEER)
- (10) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION (IN UNDERCUT AREAS)
- (11) AGGREGATE SHOULDERS, TYPE B-6"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS
THE CONTRACTOR SHALL PATCH BEFORE COMPLETING MILLING

MIXTURE TYPE	VOIDS
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm)	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75 N50	4% @ 50 GYR
RECONSTRUCTION	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm)	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75 N50	4% @ 50 GYR
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm) - 9" (IN 3 LIFTS)	4% @ 70 GYR
PAVEMENT WIDENING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm)	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75 N50	4% @ 50 GYR
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm) - 9" (IN 3 LIFTS)	4% @ 70 GYR
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19mm); TYPE I-IV - 8"	4% @ 70 GYR
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR
HOT-MIX ASPHALT BIKE PATHS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm)	4% @ 50 GYR

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LB/SO YD/IN
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

COPYRIGHT © 2008 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 18-0-00121 - EXPIRES 4/30/2011
 508824 AM
 2/27/2011
 PROJECT: VILLAGE OF SHOREWOOD, ILLINOIS - BLACK ROAD & RIVER ROAD INTERSECTION IMPROVEMENT
 DRAWING: TYPICAL SECTIONS AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-typsec.shd

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT**

**TYPICAL SECTIONS AND
HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

SCALE: _____ STA. _____ TO STA. _____

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 10
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

BUTT JOINTS

LOCATION	LENGTH (FT)	WIDTH (FT)	40600982 HMA SURF REM - BUTT JOINT (SY)	
BLACK ROAD (STA 205+49 LT)	4.5	21		11
BLACK ROAD (STA 220+12)	4.5	45		23
RIVER ROAD (STA 107+82)	4.5	42		21
RIVER ROAD (STA 116+81)	4.5	42		21
	TOTAL			76

EXISTING UTILITY STRUCTURE ADJUSTMENTS AND REMOVALS

STATION	OFFSET	TYPE	EXISTING LOCATION	60255500	60265700	60500050	60500060	X6026050	X6030310	
				MH TO BE ADJUSTED	VV TO BE ADJUSTED	REMOVING CATCH BASINS	REMOVING INLETS	SAN MH TO BE ADJUSTED	FRAMES & LIDS TO BE ADJ (SPL)	
BLACK ROAD										
211+86	21' LT	CATCH BASIN	CURBLINE			1				
212+34	19' LT	STORM MH	EX PAVEMENT						1	
212+36	28' RT	SAN MH	PARKWAY					1		
212+49	25' LT	VALVE VAULT	EX PAVEMENT						1	
213+01	21' LT	VALVE VAULT	EX PAVEMENT						1	
213+04	24' LT	VALVE VAULT	EX PAVEMENT						1	
213+36	23' LT	INLET	CURBLINE				1			
213+37	23' RT	INLET	CURBLINE				1			
215+38	27' LT	VALVE VAULT	BACK OF CURB		1					
220+04	28' LT	VALVE VAULT	EX PAVEMENT						1	
RIVER ROAD										
107+95	21' RT	STORM MH	CURBLINE	1						
110+27	21' RT	CATCH BASIN	CURBLINE			1				
111+76	21' RT	INLET	CURBLINE				1			
111+93	40' LT	SAN MH	PARKWAY					1		
112+93	26' RT	AT&T MH	PARKWAY	TO BE ADJUSTED BY UTILITY						
113+18	35' RT	SAN MH	EDGE OF SDWK					1		
114+28	21' RT	INLET	CURBLINE				1			
114+28	21' LT	INLET	CURBLINE				1			
114+28	44' LT	STORM MH	PRKWY (BURIED)	1						
TOTAL				2	1	2	5	3	5	

CLASS D PATCHES

LOCATION	44201737	44201741	44201745	44201747	44002214	40601005
	CL D PATCH 8" TYPE I (SY)	CL D PATCH 8" TYPE II (SY)	CL D PATCH 8" TYPE III (SY)	CL D PATCH 8" TYPE IV (SY)	HMA REM OVR PATCH 3.5" (SY)	HMA REPL OVR PATCH (TON)
BLACK ROAD						
STA 207+08 LT			17		19	4
STA 211+86 LT		14			17	4
STA 211+86 TO STA 212+34 LT			18		21	4
STA 212+34 LT		13			15	3
STA 213+36				25	30	6
STA 213+52				30	35	7
RIVER ROAD						
STA 110+27, 21' RT	2				3	1
STA 111+76, 21' RT	2				3	1
STA 114+28, 20.5' RT	2				3	1
STA 114+28, 20.5' LT	2				3	1
STA 114+81 LT		7			8	2
TOTALS	8	34	35	55	157	34

EARTH EXCAVATION TABLE

STATION BEGIN	STATION END	UNDERCUT AND PGES REPLACEMENT (CU YD)	TOPSOIL EXCAVATION (CU YD)	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (CU YD)	EARTH EXCAVATION (CU YD)	STORM SEWER EXCAVATION (CU YD)	TOTAL SUITABLE EXCAVATION (CU YD)	EXCAVATION TO BE USED IN EMBANKMENT (ADJUST FOR 20% SHRINKAGE) (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)
BLACK ROAD										
205+49	206+00	0.0	32.4	32.4	24.8	0.0	24.8	19.9	22.8	-2.9
206+00	207+00	4.0	74.4	78.4	49.8	0.0	49.8	39.9	61.4	-21.5
207+00	208+00	9.0	90.2	99.2	48.0	0.0	48.0	38.4	70.4	-32.0
208+00	209+00	17.0	118.3	135.3	56.3	0.0	56.3	45.1	65.5	-20.4
209+00	210+00	22.0	136.4	158.4	66.1	0.0	66.1	53.0	53.1	-0.1
210+00	211+00	30.0	146.2	176.2	72.2	0.0	72.2	57.8	46.7	11.1
211+00	212+00	27.0	166.4	193.4	105.0	12.4	117.4	94.0	49.5	44.5
212+00	212+58	20.0	125.2	145.2	180.6	35.8	216.4	173.2	13.9	159.3
213+00	213+50	15.0	91.6	106.6	140.5	2.0	142.5	114.1	3.1	111.0
213+50	214+00	10.0	68.2	78.2	92.4	1.8	94.2	75.4	5.4	70.0
214+00	215+00	17.0	107.5	124.5	155.1	0.0	155.1	124.2	6.9	117.3
215+00	216+00	12.0	72.5	84.5	97.1	0.0	97.1	77.7	6.3	71.4
216+00	217+00	4.0	36.8	40.8	41.3	0.0	41.3	33.1	6.3	26.8
217+00	217+37	0.0	2.8	2.8	3.8	0.0	3.8	3.1	0.8	2.3
SUBTOTAL		187	1269	1456	1134	52	1186	949	413	537
RIVER ROAD										
107+82	108+00	0.0	2.2	2.2	1.5	0.0	1.5	1.2	1.3	-0.1
108+00	109+00	0.0	48.7	48.7	38.4	0.0	38.4	30.7	15.3	15.4
109+00	110+00	6.0	76.7	82.7	88.4	0.0	88.4	70.8	15.1	55.7
110+00	110+50	12.0	37.9	49.9	56.2	1.9	58.1	46.6	5.5	41.1
110+50	111+00	25.0	43.5	68.5	60.1	0.0	60.1	48.2	10.2	38.0
111+00	111+50	35.0	54.7	89.7	64.1	0.0	64.1	51.3	20.6	30.7
111+50	112+00	40.0	106.6	146.6	101.4	3.6	105.0	84.0	22.9	61.1
112+00	113+00	35.0	58.2	93.2	109.5	0.0	109.5	87.7	1.6	86.1
113+00	113+50	7.0	81.3	88.3	128.3	0.0	128.3	102.7	4.5	98.2
113+50	114+00	2.0	54.8	56.8	61.6	0.0	61.6	49.3	7.9	41.4
114+00	114+50	0.0	40.7	40.7	41.9	4.1	46.0	36.9	6.9	30.0
114+50	115+00	0.0	27.2	27.2	31.1	0.0	31.1	24.9	3.2	21.7
115+00	115+71	0.0	15.1	15.1	17.5	0.0	17.5	14.1	2.1	12.0
SUBTOTAL		162	648	810	800	10	810	649	117	532
TOTALS		349	1917	2266	1934	62	1996	1598	530	1069

EARTH EXCAVATION 1934
 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL 2266
 POROUS GRANULAR EMBANKMENT, SUBGRADE 349
 EARTHWORK BALANCE 1069 EXCESS EARTH EXCAVATION

COPYRIGHT © 2009 BY BAYTES & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 184-C-0021 - EXPIRES 12/31/2011
 2/2/2011 4:28:31 PM
 G:\C-91-726-09\11\10\09049-BlackRiver-Ph2-CADD-SURVEY-DRAWINGS\09049-Ph2-Schedule_Qty.sht



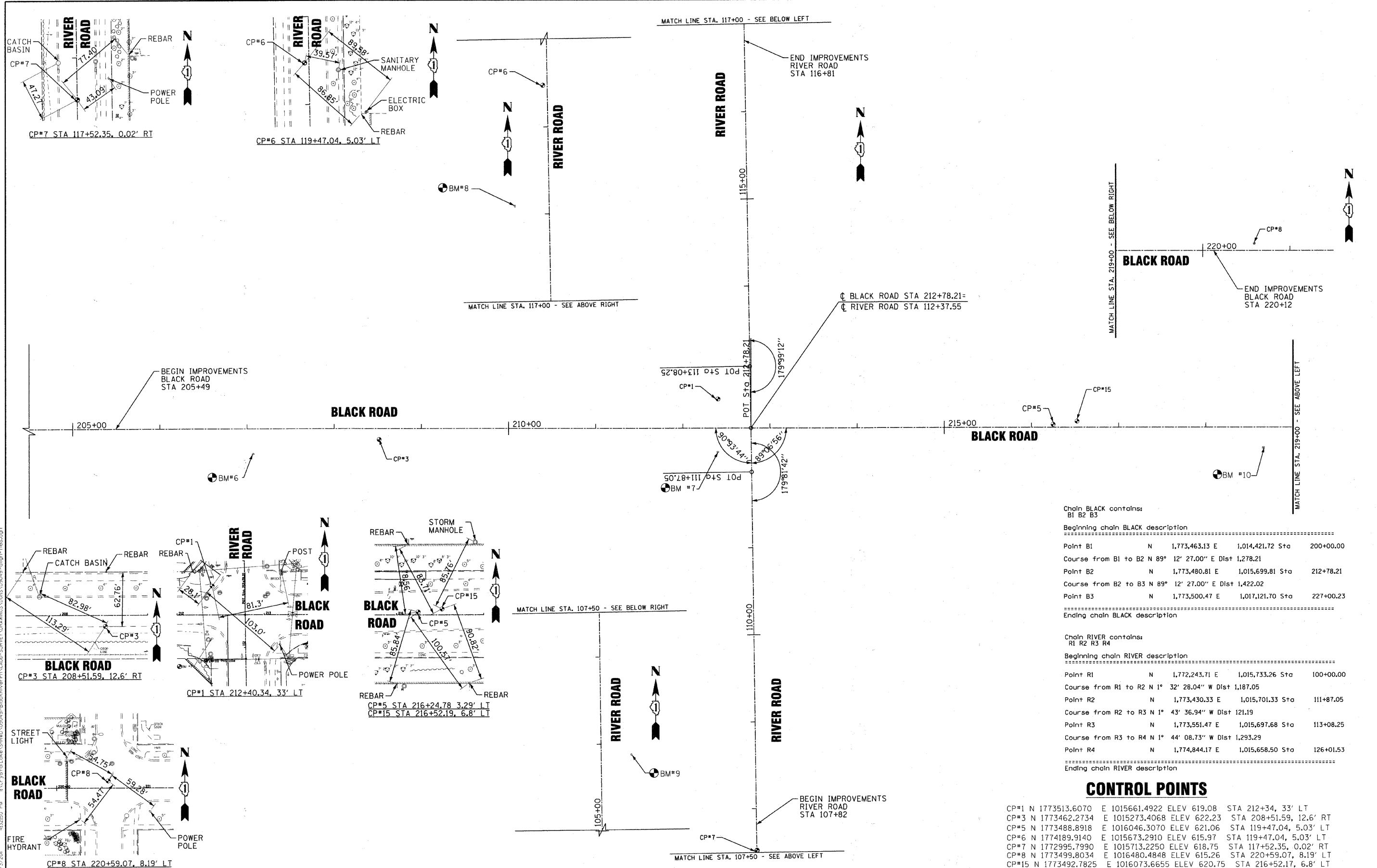
DESIGNED -	DSH	REVISED -	1-19-11 PER VILLAGE
DRAWN -	CJC	REVISED -	
CHECKED -	LDH	REVISED -	
DATE -	12/6/10	FILE -	090491-PH2-Schedule_Qty.sht

**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

SCHEDULE OF QUANTITIES

SCALE: STA. TO STA.

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 11
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				



Chain BLACK contains:
B1 B2 B3

Beginning chain BLACK description

Point B1	N	1,773,463.13 E	1,014,421.72 Sta	200+00.00
Course from B1 to B2	N 89° 12' 27.00" E Dist 1,278.21			
Point B2	N	1,773,480.81 E	1,015,699.81 Sta	212+78.21
Course from B2 to B3	N 89° 12' 27.00" E Dist 1,422.02			
Point B3	N	1,773,500.47 E	1,017,121.70 Sta	227+00.23

Ending chain BLACK description

Chain RIVER contains:
R1 R2 R3 R4

Beginning chain RIVER description

Point R1	N	1,772,243.71 E	1,015,733.26 Sta	100+00.00
Course from R1 to R2	N 1° 32' 28.04" W Dist 1,187.05			
Point R2	N	1,773,430.33 E	1,015,701.33 Sta	111+87.05
Course from R2 to R3	N 1° 43' 36.94" W Dist 121.19			
Point R3	N	1,773,551.47 E	1,015,697.68 Sta	113+08.25
Course from R3 to R4	N 1° 44' 08.73" W Dist 1,293.29			
Point R4	N	1,774,844.17 E	1,015,658.50 Sta	126+01.53

Ending chain RIVER description

CONTROL POINTS

CP#1	N	1773513.6070	E	1015661.4922	ELEV	619.08	STA	212+34, 33'	LT
CP#3	N	1773462.2734	E	1015273.4068	ELEV	622.23	STA	208+51.59, 12.6'	RT
CP#5	N	1773488.8918	E	1016046.3070	ELEV	621.06	STA	119+47.04, 5.03'	LT
CP#6	N	1774189.9140	E	1015673.2910	ELEV	615.97	STA	119+47.04, 5.03'	LT
CP#7	N	1772995.7990	E	1015713.2250	ELEV	618.75	STA	117+52.35, 0.02'	RT
CP#8	N	1773499.8034	E	1016480.4848	ELEV	615.26	STA	220+59.07, 8.19'	LT
CP#15	N	1773492.7825	E	1016073.6655	ELEV	620.75	STA	216+52.17, 6.8'	LT

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-000211 EXPIRES 7/30/2011
 12/27/2009 1:27:20 PM C:\projects\090091\fig1.dwg
 09/25/09 12:27:20 PM C:\projects\090091\fig1.dwg
 09/25/09 12:27:20 PM C:\projects\090091\fig1.dwg



DESIGNED	-	DSH	REVISED	-	1-31-11 PER 100T
DRAWN	-	CJC	REVISED	-	
CHECKED	-	LDH	REVISED	-	
DATE	-	12/6/10	FILE	-	090491-allgn-ties.dgn

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT**

ALIGNMENT AND TIES

SCALE: 1"=50'

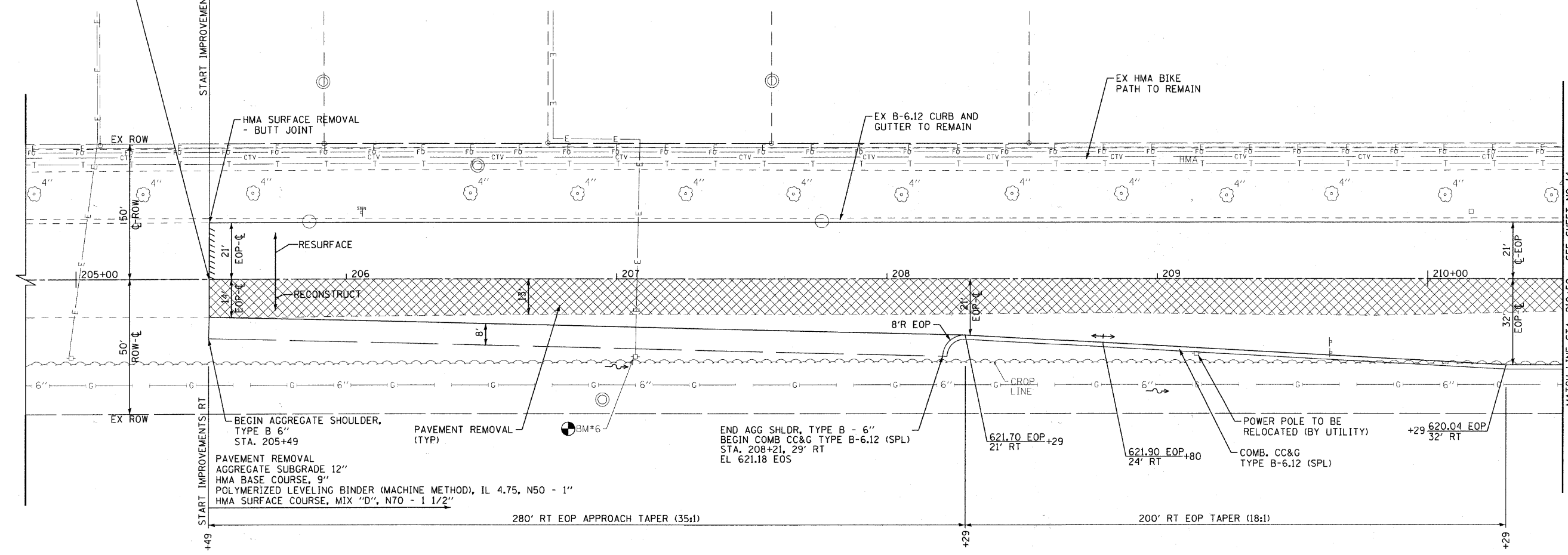
STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0258 0386	09-00028-00-CH	WILL	63	12
C-91-726-09		CONTRACT NO.		63562
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		M-9003(385)



BEGIN IMPROVEMENTS
BLACK ROAD
STA. 205+49

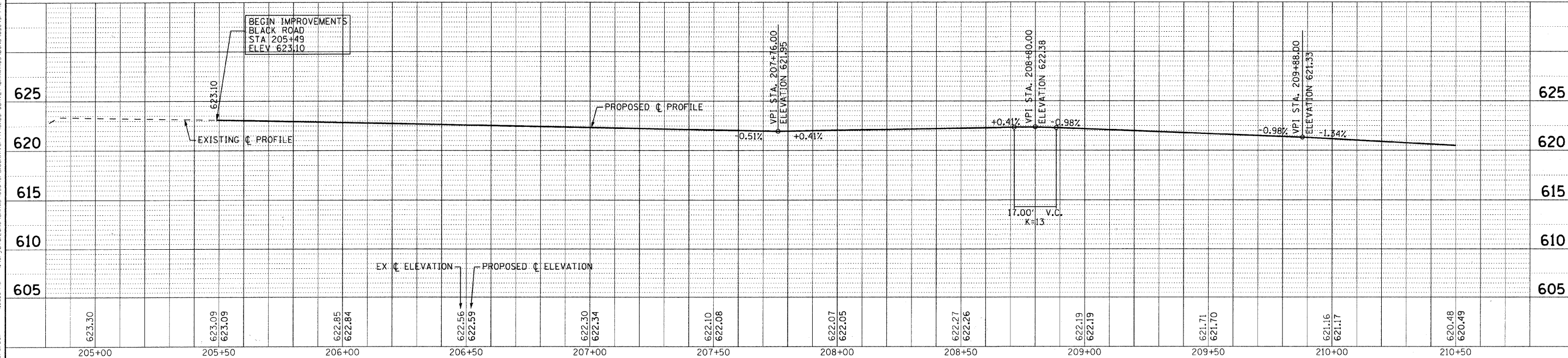
HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL. 4.75, N50 - 1"
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"



MATCH LINE STA. 210+50 - SEE SHEET NO. 14

BLACK ROAD

PAVEMENT REMOVAL



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - UKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-pp1.sht

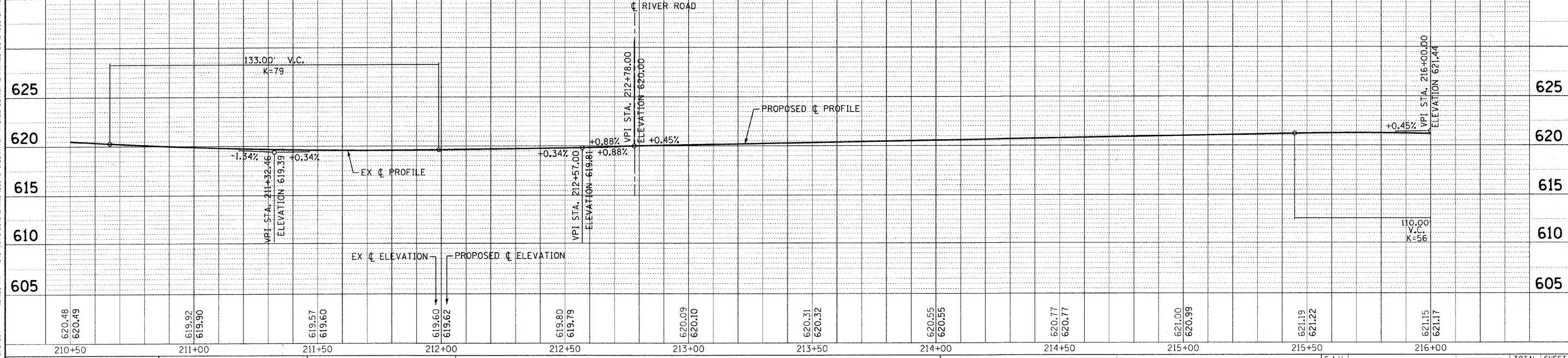
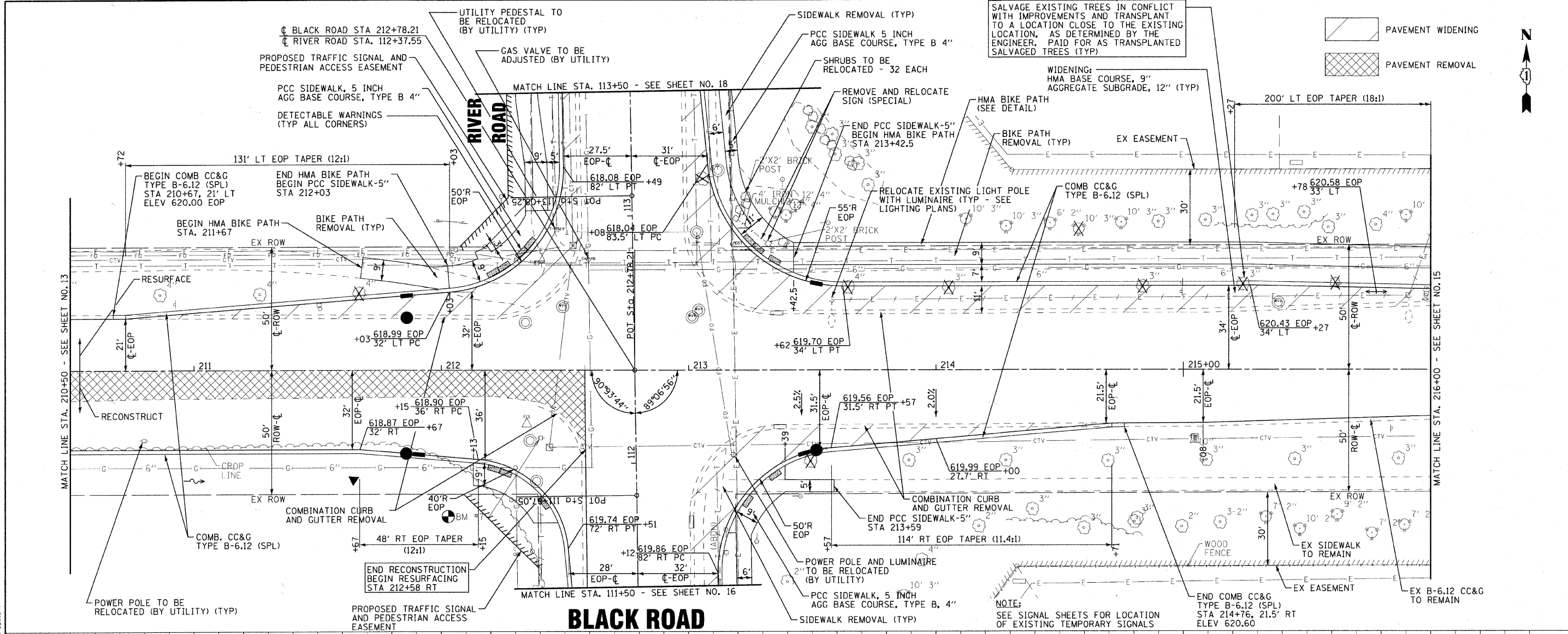
VILLAGE OF SHOREWOOD, ILLINOIS BLACK ROAD AND RIVER ROAD INTERSECTION IMPROVEMENTS

GEOMETRIC PLAN AND PROFILE BLACK ROAD

SCALE: H: 1"=20' V: 1"=5' STA. 205+00 TO STA. 210+50

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 13
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

CONTRACT NO. 63562 BY DAXTER CONSULTING ENGINEERS, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 034-0011 - EXPIRES 4/30/2011
 4518 B PM 5/23/2011
 C:\Users\ldh\Documents\090491-PH2-pp1.sht



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - LKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-pp2.sh1

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD AND RIVER ROAD
INTERSECTION IMPROVEMENTS**

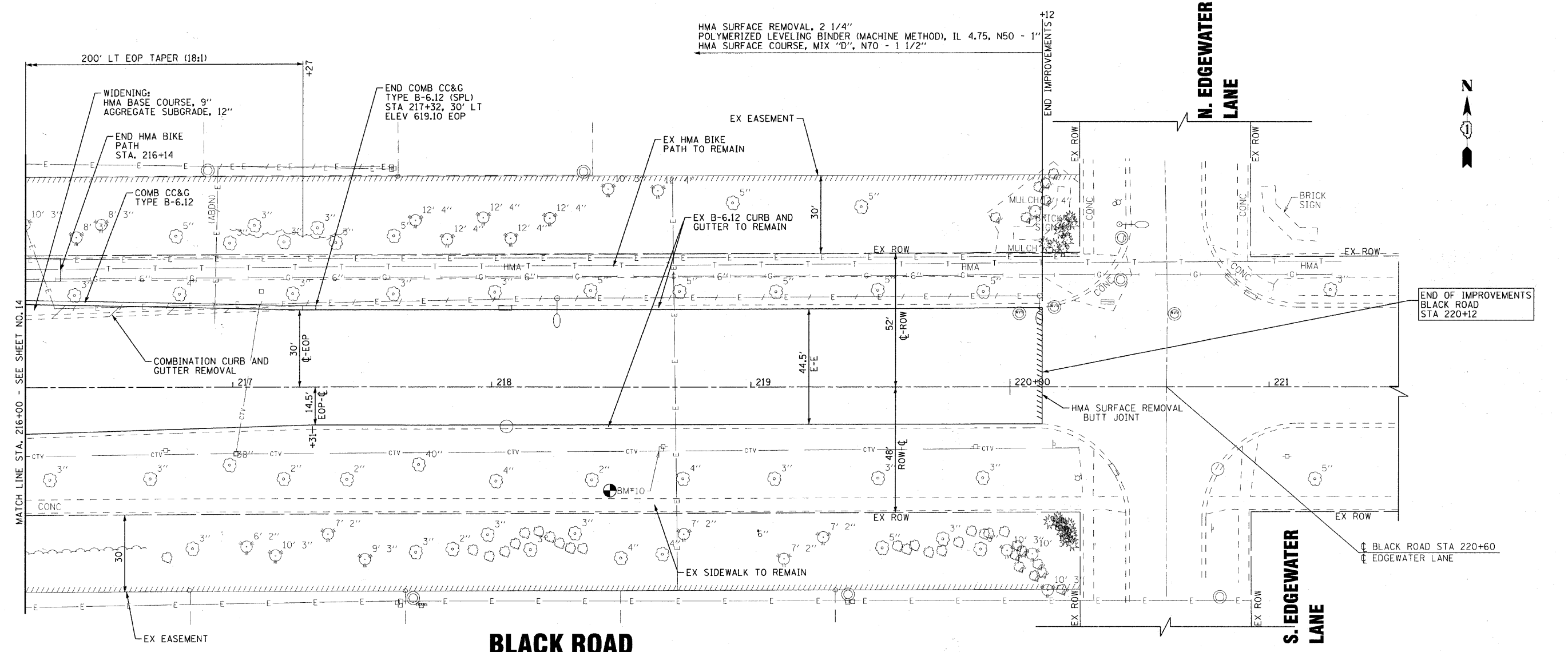
**GEOMETRIC PLAN AND PROFILE
BLACK ROAD**

SCALE: H: 1"=20' V: 1"=5' STA. 210+50 TO STA. 216+00

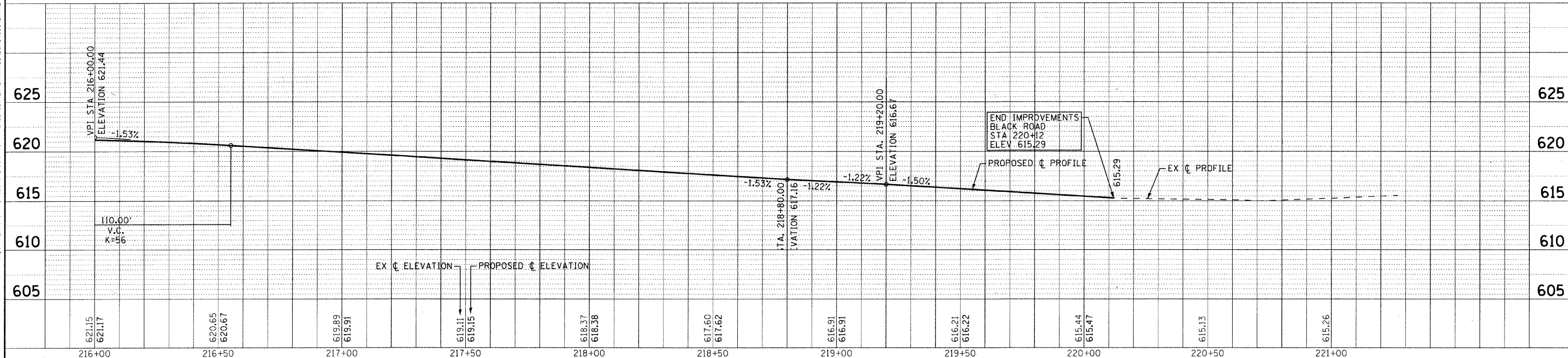
F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 14
C-91-726-09		CONTRACT NO. 63562		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(385)

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 0298-0396, EXPIRES 7/31/2011
 23040

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00291 - EXP. 12/31/2011
 2/23/2010 4:51:48 PM C:\CrystalLake\Shirley\090491-Ph2\Drawings\090491-Ph2-pp3.sht



HMA SURFACE REMOVAL, 2 1/4"
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50 - 1"
 HMA SURFACE COURSE, MIX "D", N70 - 1 1/2"



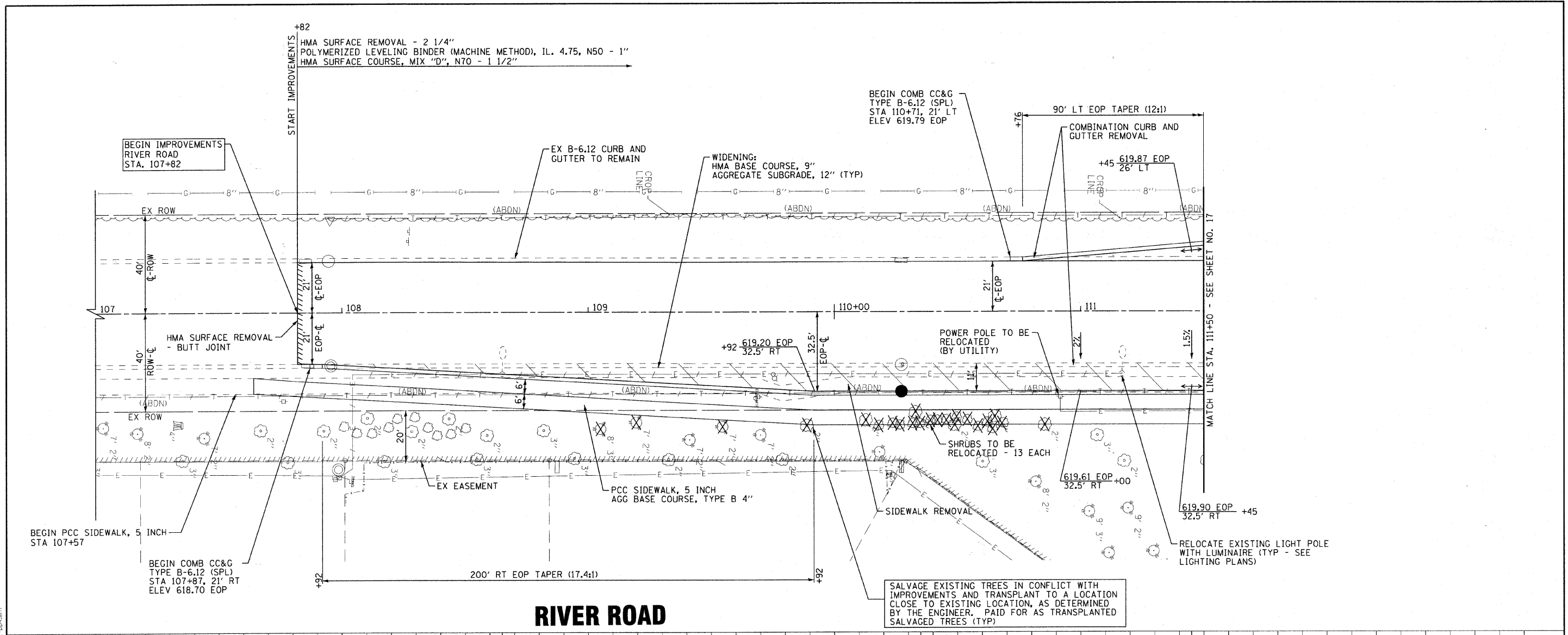
DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - UKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-Ph2-pp3.sht

VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD AND RIVER ROAD
INTERSECTION IMPROVEMENTS

GEOMETRIC PLAN AND PROFILE
BLACK ROAD

SCALE: H: 1"=20' V: 1"=5' STA. 216+00 TO STA. 221+00

F.A.U. FILE 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 15
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-900313851				



RIVER ROAD



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - UKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-pp4.sh1

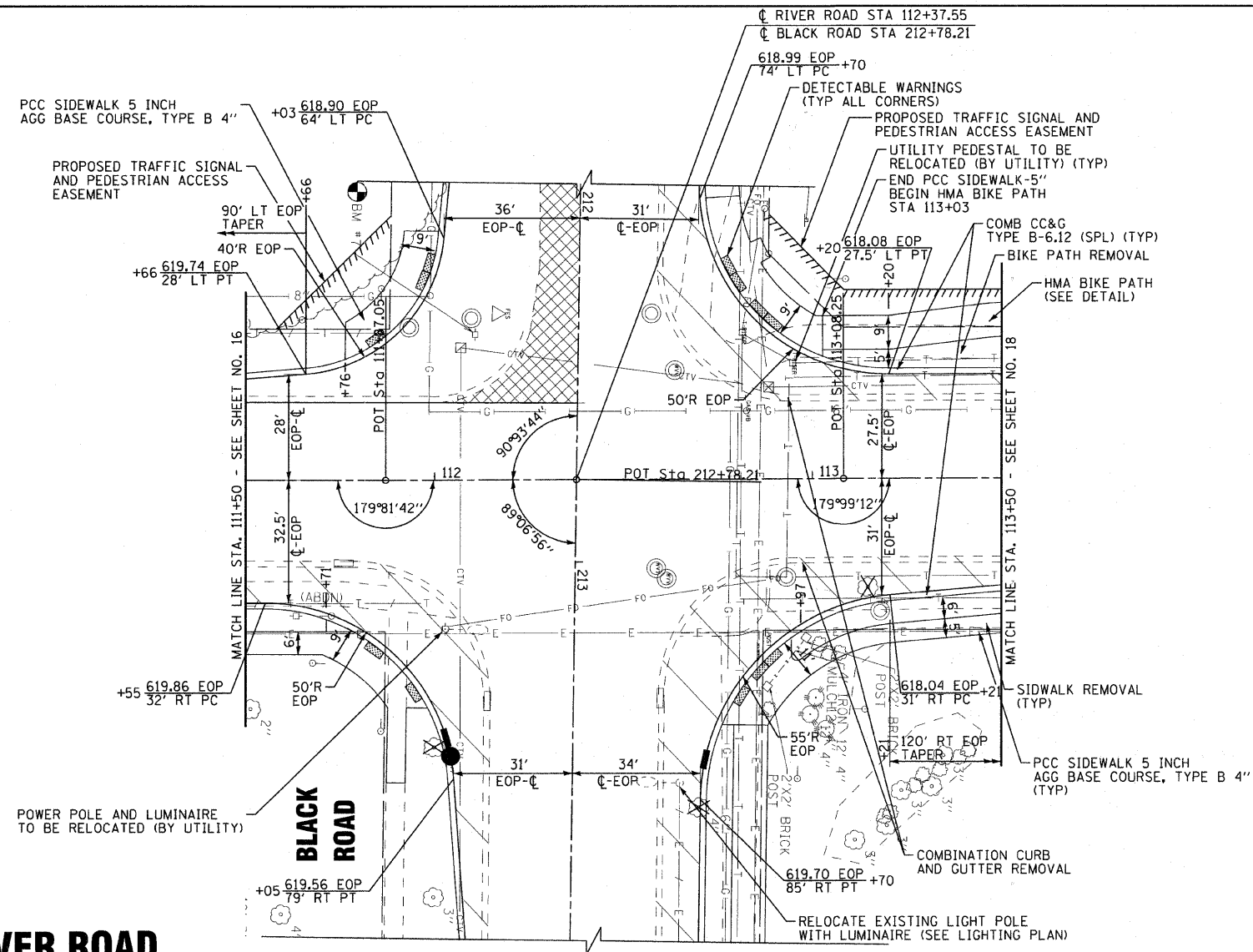
**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD AND RIVER ROAD
INTERSECTION IMPROVEMENTS**

**GEOMETRICS PLAN AND PROFILE
RIVER ROAD**

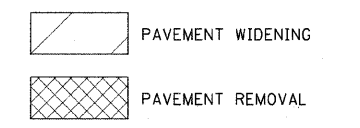
SCALE: H: 1"=20' V: 1"=5' STA. 107+00 TO STA. 111+50

F.A.U. R.T.E. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 16
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90033851				

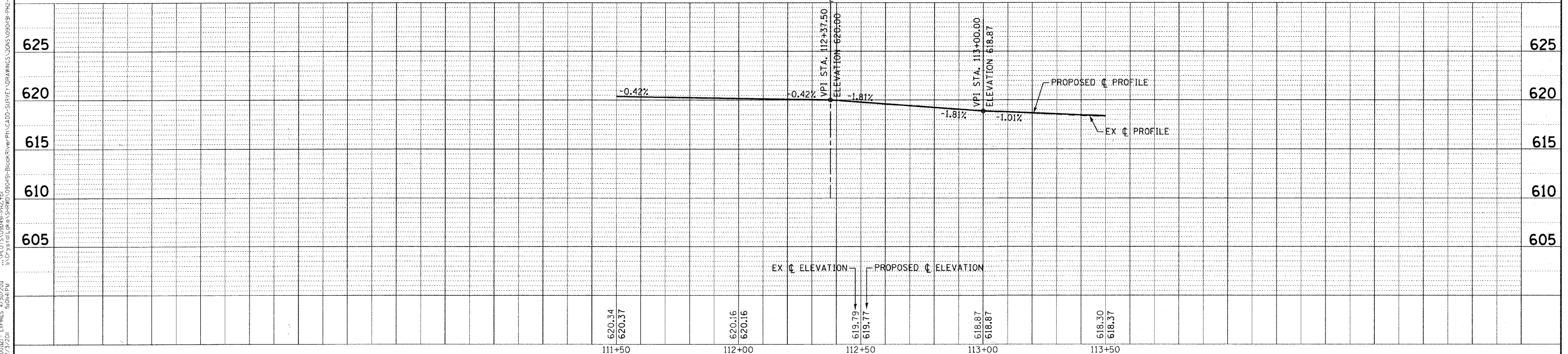
COPYRIGHT © 2010 BY WAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-000211 - EXPIRES 4/2/2011
 230400



NOTE:
SEE SIGNAL SHEET FOR LOCATIONS
OF EXISTING TEMPORARY SIGNALS.



RIVER ROAD



COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 0298-0396 - EXPIRES 7/31/2011
 2/23/2010



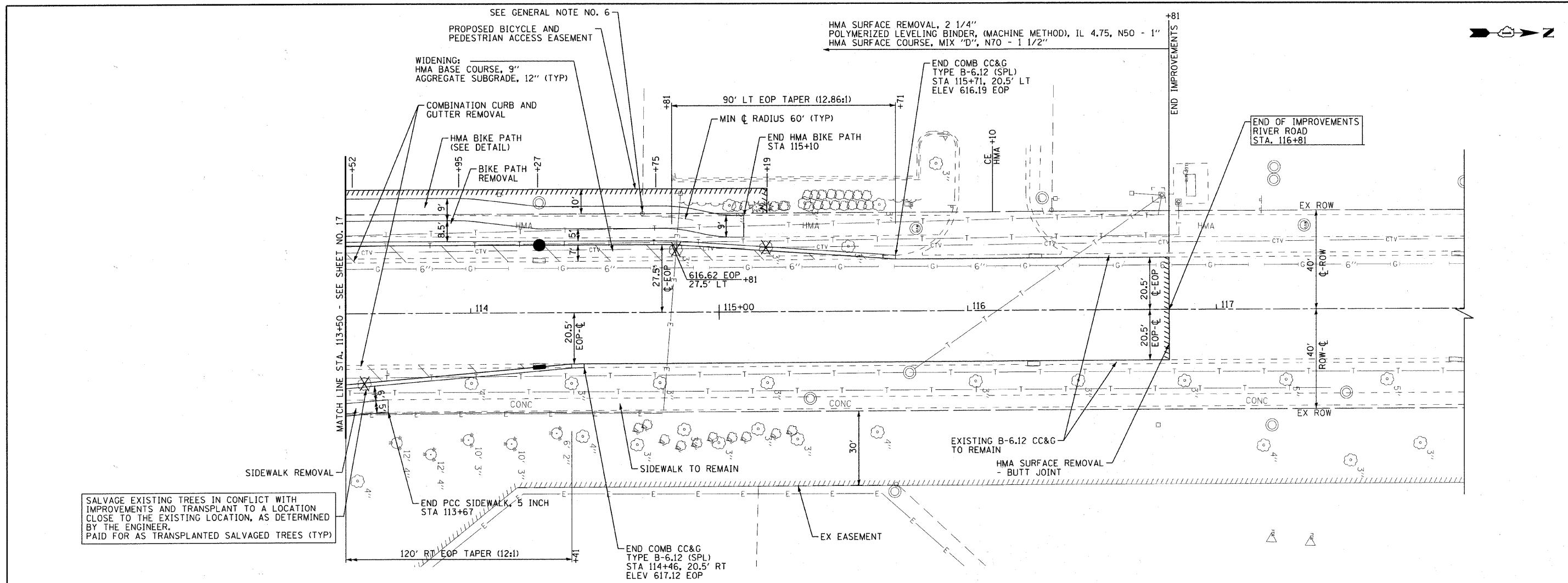
DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - LKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-pp5.shx

VILLAGE OF SHOREWOOD, ILLINOIS BLACK ROAD AND RIVER ROAD INTERSECTION IMPROVEMENTS

GEOMETRICS PLAN AND PROFILE RIVER ROAD

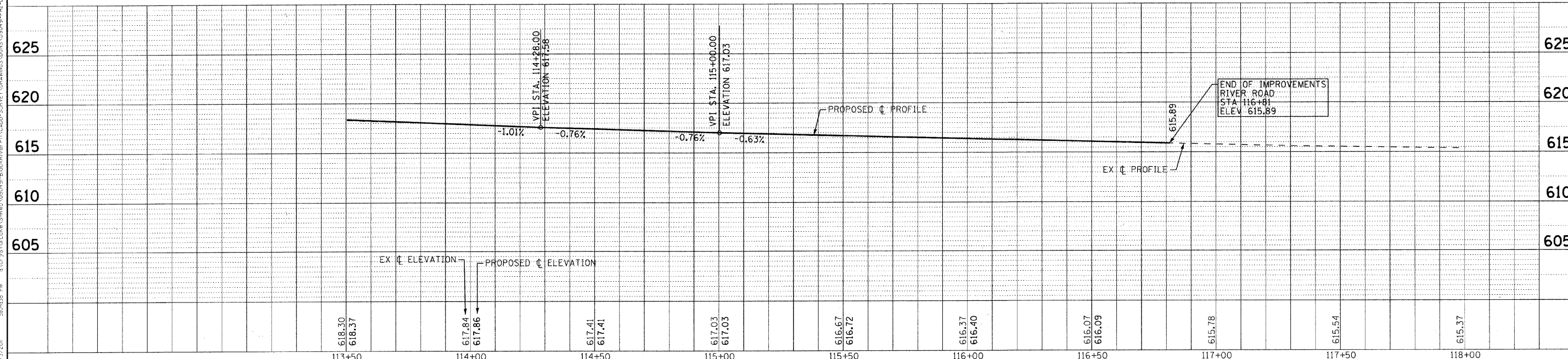
SCALE: H: 1"=20' V: 1"=5'
STA. 111+50 TO STA. 113+50

F.A.U. RTE. 0298-0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 17
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				



SALVAGE EXISTING TREES IN CONFLICT WITH IMPROVEMENTS AND TRANSPLANT TO A LOCATION CLOSE TO THE EXISTING LOCATION, AS DETERMINED BY THE ENGINEER. PAID FOR AS TRANSPLANTED SALVAGED TREES (TYP)

RIVER ROAD



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - UKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-pp6.shp

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD AND RIVER ROAD
INTERSECTION IMPROVEMENTS**

**GEOMETRIC PLAN AND PROFILE
RIVER ROAD**

SCALE: H: 1"=20' V: 1"=5'
STA. 113+50 TO STA. 118+00

F.A.U. R.T.E. 0298 0386	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 18
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

COPYRIGHT © 2010 BY BAKER & WOODMAN, INC.
 STATE OF ILLINOIS PROFESSIONAL DESIGNER
 LICENSE NO. 184-002111 - EXPIRES 4/30/2011
 2/7/2010 5:04:38 PM \\C:\Users\ldh\OneDrive\Baker\RiverPh\CAD\SURVEY\DRAWINGS\DWG\090491-PH2-pp6.shp

SUGGESTED STAGES OF CONSTRUCTION

PRIOR TO ANY CONSTRUCTION, INSTALL ALL NECESSARY TRAFFIC CONTROL.

PRIOR TO STAGE I, INSTALL ALL APPROPRIATE EROSION CONTROL MEASURES, INCLUDING PERIMETER EROSION BARRIER AND INLET FILTERS ON EXISTING ON-SITE AND DOWNSTREAM DRAINAGE STRUCTURES.

STAGE I:

1. CONSTRUCT PROPOSED STORM SEWER AND INSTALL INLET FILTERS ON ALL NEW OPEN-LIDDED STRUCTURES AND INSTALL PIPE PROTECTION AROUND THE ENDS OF FLARED END SECTIONS.
2. MOVE FIRE HYDRANTS.
3. REMOVE TREES, FENCES, BUSHES AND ANY OTHER LANDSCAPING IN CONFLICT WITH THE PROPOSED CONSTRUCTION.
4. RELOCATE EXISTING PEDESTRIAN PUSH BUTTON ON THE NORTHEAST CORNER (SEE TEMPORARY SIGNAL PLAN) TO THE EXISTING WOOD POLE.
5. DISASSEMBLE AND SALVAGE EXISTING STREET LIGHTS TO BE RELOCATED, PULL EXISTING WIRING, AND REMOVE CONCRETE FOUNDATIONS.
6. CLOSE SIDEWALKS/BIKE PATHS TO PEDESTRIAN/BICYCLE TRAFFIC.
7. BEGIN REMOVING SIDEWALK AND BIKE PATHS.

STAGE II:

1. REMOVE CURB AND GUTTER ON THE NORTHEAST, NORTHWEST, AND SOUTHEAST QUADRANTS OF THE INTERSECTION.
2. REMOVE VEGETATION, TOPSOIL AND COMPLETE EARTH EXCAVATION ON THE NORTH, SOUTH AND EAST LEGS.
3. GRADE AND COMPACT SUBGRADE, REPAIRING UNSTABLE AREAS.
4. INSTALL AGGREGATE SUBGRADE.
5. INSTALL CURB AND GUTTER.
6. BACKFILL BEHIND THE CURB AND INSTALL ANY NECESSARY TEMPORARY EROSION CONTROL SEEDING.
7. ADJUST EXISTING FRAMES AND LIDS LOCATED WITHIN THE WIDENING.
8. INSTALL HMA BASE COURSE.
9. INSTALL TEMPORARY RAMPS AT ALL LOCATIONS WHERE TRAFFIC WILL BE CROSSING BETWEEN THE BASE COURSE AND EXISTING PAVEMENT DURING STAGE III.

STAGE III:

1. SETUP SIGNAGE, REMOVE CONFLICTING PAVEMENT MARKING, AND SHIFT TRAFFIC WITH PAVEMENT MARKING TAPE AND BARRICADES ON THE EAST LEG FIRST IN ACCORDANCE WITH THE SUGGESTED MAINTENANCE OF TRAFFIC STAGING, SHEET 10.
2. ONCE TRAFFIC ON THE EAST LEG IS SHIFTED, SHIFT TRAFFIC ON THE WEST LEG IN ACCORDANCE WITH THE SUGGESTED MAINTENANCE OF TRAFFIC STAGING, SHEET 10.
3. ADJUST THE TEMPORARY SIGNAL VIDEO DETECTION ZONE FOR NEW TRAFFIC CONFIGURATION.
4. SAWCUT ALONG THE CENTERLINE OF THE WEST LEG.
5. REMOVE EXISTING CURB AND GUTTER ON THE SOUTHWEST CORNER.
6. REMOVE PAVEMENT ON THE SOUTH HALF OF THE WEST LEG, REMOVE VEGETATION AND TOPSOIL, AND COMPLETE EARTH EXCAVATION.
7. GRADE AND COMPACT SUBGRADE, REPAIRING UNSTABLE AREAS.
8. INSTALL AGGREGATE SUBGRADE.
9. INSTALL CURB AND GUTTER.
10. BACKFILL BEHIND THE CURB AND GUTTER AND BEGIN GRADING AND SHAPING DITCHES. INSTALL TEMPORARY DITCH CHECKS AND TEMPORARY EROSION CONTROL SEEDING ONCE DITCHES ARE COMPLETE (OR IF THEY WILL SIT IDLE FOR MORE THAN 7 DAYS).
11. INSTALL HMA BASE COURSE.
12. INSTALL TEMPORARY RAMPS AT ALL LOCATIONS WHERE TRAFFIC WILL BE CROSSING BETWEEN THE BASE COURSE AND EXISTING PAVEMENT.
13. INSTALL SHORT TERM PAVEMENT MARKING TO DELINEATE A LEFT TURN LANE.
14. REOPEN THE WHOLE WEST LEG TO TRAFFIC EXCEPT THE NEW RIGHT TURN LANE.
15. SHIFT WESTBOUND TRAFFIC BACK TO ORIGINAL CONFIGURATION, REMOVE CONFLICTING PAVEMENT MARKING TAPE AND SIGNS, AND RE-ESTABLISHING OLD LANE CONFIGURATION WITH SHORT-TERM MARKING.
16. ADJUST THE TEMPORARY SIGNAL VIDEO DETECTION ZONES FOR THE NEW CONFIGURATION AND ADD EB LEFT TURN PHASE TO THE CYCLE.

STAGE IV:

1. INSTALL ALL UNDERGROUND CONDUIT, HANDHOLES, AND FOUNDATIONS FOR THE PROPOSED TRAFFIC SIGNAL.
2. INSTALL ALL UNDERGROUND CONDUIT AND FOUNDATIONS FOR THE PROPOSED AND RELOCATED STREET LIGHTS.
3. BEGIN CONSTRUCTING SIDEWALKS, BIKE PATHS, AND DETECTABLE WARNINGS.
4. BEGIN FINISH GRADING OF PARKWAYS.

STAGE V:

1. INSTALL HMA PATCHES AS DETERMINED BY THE ENGINEER.
2. MILL THE EXISTING HMA SURFACE AND REMOVE TEMPORARY RAMPS.
3. INSTALL BUTT JOINTS AND INSTALL SHORT-TERM PAVEMENT MARKING TO DELINEATE LANES.
4. INSTALL LEVELING BINDER ON THE ENTIRE INTERSECTION AND INSTALL SHORT-TERM PAVEMENT MARKING.
5. OPEN NEW RIGHT-TURN LANES AND ADJUST TEMPORARY SIGNAL DETECTION ZONES ACCORDINGLY.
6. COMPLETE ANY REMAINING SIDEWALK AND BIKE PATHS.
7. INSTALL TOPSOIL AND BEGIN SEEDING AND/OR SODDING.
8. INSTALL ANY AVAILABLE TRAFFIC SIGNAL EQUIPMENT AND STREET LIGHTING HARDWARE.
9. INSTALL HMA SURFACE COURSE AND INSTALL SHORT TERM PAVEMENT MARKING.
10. INSTALL PERMANENT PAVEMENT MARKING AND REMOVE SHORT-TERM MARKING.
11. REOPEN SIDEWALK AND BIKE PATHS TO PEDESTRIAN AND BICYCLE TRAFFIC AND ACTIVATE PUSH-BUTTONS AND PEDESTRIAN SIGNAL HEADS ON THE TEMPORARY SIGNAL.
12. COMPLETE SEEDING AND/OR SODDING.
13. UPON SATISFACTORY COMPLETION OF THE ABOVE WORK, WORK SHALL BE SUSPENDED UNTIL REMAINING SIGNAL EQUIPMENT AND STREET LIGHTING MATERIALS ARE RECEIVED BY THE CONTRACTOR, AS DETERMINED BY THE ENGINEER.

STAGE VI:

1. NOTIFICATION SHALL BE GIVEN TO THE ENGINEER WHEN REMAINING SIGNAL AND STREET LIGHTING EQUIPMENT IS RECEIVED. THE WORK SUSPENSION SHALL BE LIFTED ON A DATE AGREED TO BY THE ENGINEER, CONTRACTOR, AND VILLAGE.
2. REMAINING SIGNAL AND STREET LIGHTING EQUIPMENT SHALL BE INSTALLED AND TESTED.
3. SWITCH SIGNAL OPERATION FROM THE TEMPORARY SIGNAL TO THE NEW SIGNAL EQUIPMENT.
4. REMOVE THE TEMPORARY TRAFFIC SIGNAL EQUIPMENT.
5. COMPLETE ANY REMAINING RESTORATION.
6. COMPLETE PUNCH LIST ITEMS, REMOVE EROSION CONTROL (ONCE SOD/SEED ESTABLISHES) AND REMOVE TRAFFIC CONTROL.
7. FINAL COMPLETION WITHIN THE NUMBER OF WORKING DAYS PROVIDED IN THE CONTRACT.

TRAFFIC STAGING GENERAL NOTES

1. THE CONTRACTOR SHALL SUBMIT A PRE-PLANNED SEQUENCE OF WORK AT THE PRECONSTRUCTION MEETING FOR REVIEW AND APPROVAL. WORK SHALL BE SCHEDULED TO MINIMIZE INCONVENIENCE TO LOCAL TRAFFIC AND MAINTAIN A REASONABLE LEVEL OF CONSTRUCTION EFFICIENCY. THE VILLAGE RESERVES THE RIGHT TO RESTRICT WORK ON ANY AREA IF TRAFFIC CONTROL OPERATIONS BECOME UNACCEPTABLE.
2. ALL STREETS MUST BE OPEN TO TWO-WAY TRAFFIC AT THE END OF EACH DAY. 10-FOOT MINIMUM LANE WIDTHS ARE REQUIRED FOR ALL OPEN LANES.
3. ACCESS FOR LOCAL TRAFFIC, MAIL SERVICE AND EMERGENCY VEHICLES MUST BE MAINTAINED AT ALL TIMES.
4. PROVIDE 24 HOUR ADVANCED NOTIFICATION TO POLICE AND FIRE WHEN ROADS ARE TO BE TEMPORARILY CLOSED OR WILL HAVE LIMITED ACCESS.
5. STAGE II MUST BE COMPLETED UP THRU BASE COURSE PRIOR TO SETTING TRAFFIC CONTROL FOR STAGE III.
6. A MINIMUM OF 72 HOURS OF CURING TIME SHALL BE GIVEN TO ALL CONCRETE WORK PRIOR TO BACKFILLING AND CROSSING WITH EQUIPMENT.
7. ADJUSTMENTS NECESSARY TO THE TEMPORARY VIDEO DETECTION ZONES AND SIGNAL PHASING DURING CONSTRUCTION SHALL BE INCLUDED IN THE COST OF MAINTENANCE OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.
8. STEPS 1 AND 2 IN STAGE IV MAY BE COMPLETED DURING STAGES II AND III AS APPROPRIATE, BUT MUST BE COMPLETED PRIOR TO STEPS 3 AND 4 OF STAGE IV.
9. ACTIVATING TEMPORARY PUSH BUTTONS AND TEMPORARY PEDESTRIAN SIGNAL HEADS SHALL BE INCLUDED IN THE COST OF MAINTENANCE OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.
10. SEEDING AND/OR SODDING SHALL BE COMPLETED AT THE APPROPRIATE TIME OF YEAR IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. IF STEP 12 OF STAGE V CANNOT BE COMPLETED DURING STAGE V, AS DETERMINED BY THE ENGINEER, IT MAY BE MOVED TO STAGE VI, WITH TEMPORARY SEEDING COMPLETED DURING STAGE V.
11. THE CONTRACTOR SHALL BE GRANTED A WORK SUSPENSION AT THE END OF STAGE V ONLY IF ALL WORK IN STAGES I THROUGH V HAS BEEN COMPLETED TO SATISFACTION OF THE ENGINEER AND VILLAGE. A WRITTEN REQUEST MUST BE MADE BY THE CONTRACTOR AND APPROVED IN WRITING FOR THE SUSPENSION TO TAKE EFFECT.
12. THE CONTRACTOR SHOULD PROVIDE FOR ENOUGH REMAINING WORKING DAYS TO COMPLETE ALL WORK IN STAGE VI WITHIN THE TOTAL ALLOTTED CONTRACT WORKING DAYS.
13. THE ENGINEER MUST BE NOTIFIED OF ANY CHANGES IN CONSTRUCTION STAGING.

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - BF-00021 - EXPIRES 12/31/2011
 PROJECT NO. - 090491-PH2-Mot-1.sht
 ...\\p1015\090491-PH2-Mot-1.sht
 ...\\p1015\090491-PH2-Mot-1.sht



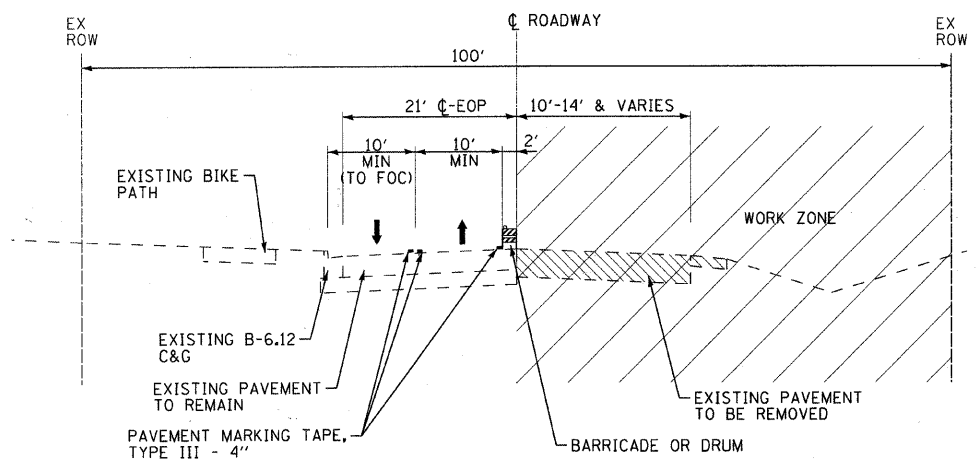
DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-Mot-1.sht

**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

MAINTENANCE OF TRAFFIC STAGING NOTES

SCALE: STA. TO STA.

F.A.U. RTE. 0298/0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 19
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-900313851				

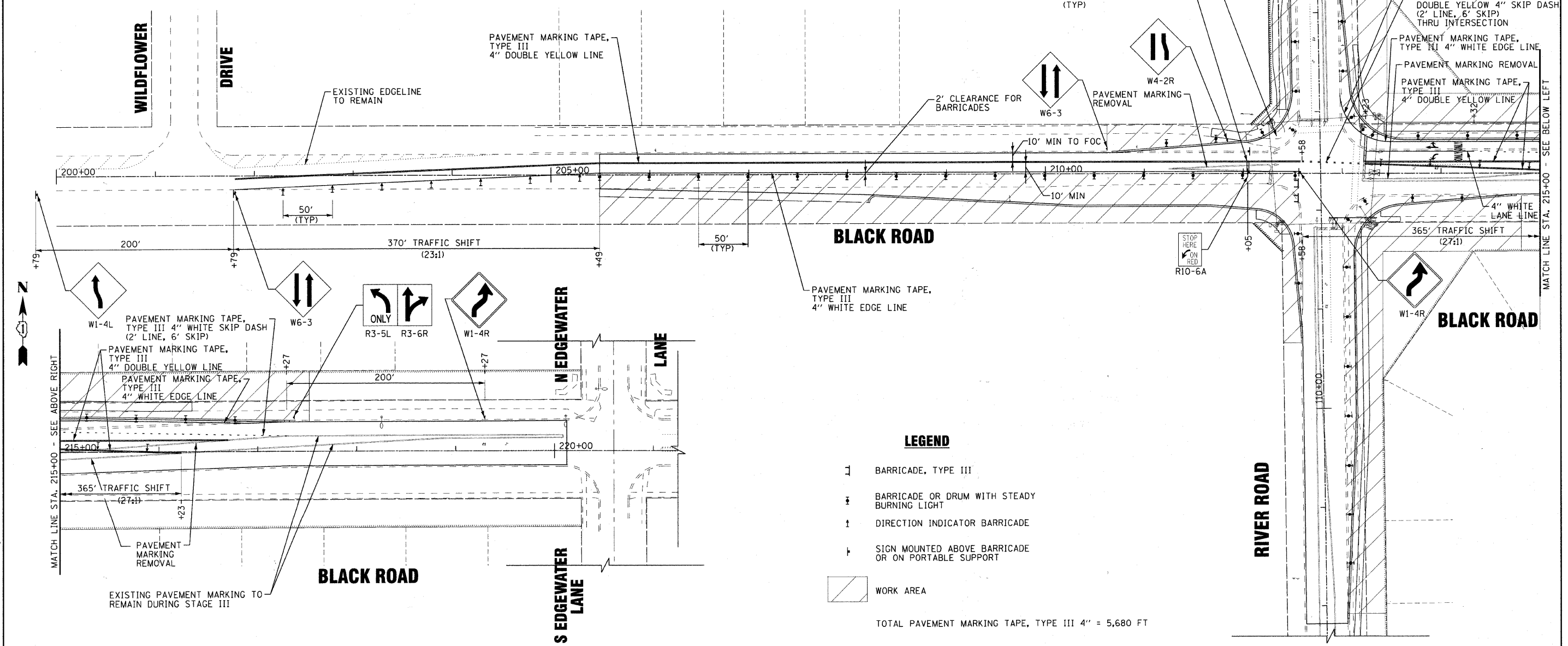


**STAGE III MAINTENANCE OF TRAFFIC
TYPICAL SECTION**

BLACK ROAD RECONSTRUCTION
STA 205+49 TO STA 212+58

NOTES:

1. WIDENING WORK ON THE NORTHWEST AND NORTHEAST CORNER MUST BE COMPLETED PRIOR TO THE RECONSTRUCTION OF BLACK ROAD.
2. "ROAD CONSTRUCTION AHEAD" (W20-1A) SIGNS (NOT SHOWN) SHALL BE PLACED 500' BEFORE THE "SHIFT LEFT" (W1-4L) SIGN ON THE WEST LEG AND 500' PRIOR TO THE RESURFACING LIMIT ON THE OTHER THREE LEGS.
3. MAINTAIN 10' MINIMUM LANE WIDTHS AT ALL TIMES.
4. PAVEMENT MARKING TAPE, BARRICADES, AND APPROPRIATE SIGNS SHALL BE REMOVED AND EASTBOUND LANE SHALL BE RE-OPENED PRIOR TO RESURFACING. UTILIZE SHORT-TERM PAVEMENT MARKING TO DELINEATE NEW LANES.
5. UPON DELINEATING NEW EASTBOUND LEFT TURN LANE PRIOR TO RESURFACING (STAGE V), UNCOVER AND ACTIVATE LEFT TURN ARROWS ON EXISTING TEMPORARY SIGNAL.
6. ALL SIGNS, BARRICADES AND MAINTENANCE OF TRAFFIC FOR STAGE II WILL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION, (SPECIAL). PAVEMENT MARKING TAPE, TYPE III-4" WILL BE PAID FOR SEPARATELY.
7. ADJUST LOCATION OF WESTBOUND TEMPORARY SIGNAL HEADS AS REQUIRED. INCLUDED IN MAINTENANCE OF EXISTING TEMPORARY TRAFFIC SIGNAL INSTALLATION.
8. UPON REMOVAL OF EASTBOUND SHIFT, SHIFT WESTBOUND TRAFFIC BACK TO ORIGINAL CONFIGURATION UTILIZING SHORT-TERM PAVEMENT MARKING.



LEGEND

- ⊥ BARRICADE, TYPE III
- ⊥ BARRICADE OR DRUM WITH STEADY BURNING LIGHT
- ⊥ DIRECTION INDICATOR BARRICADE
- ⊥ SIGN MOUNTED ABOVE BARRICADE OR ON PORTABLE SUPPORT
- ▨ WORK AREA

TOTAL PAVEMENT MARKING TAPE, TYPE III 4" = 5,680 FT

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 084-00021 - EXPIRES 7/30/2011
 PROJECT NO. - 09-00028-00-CH
 SHEET NO. - 63 OF 20



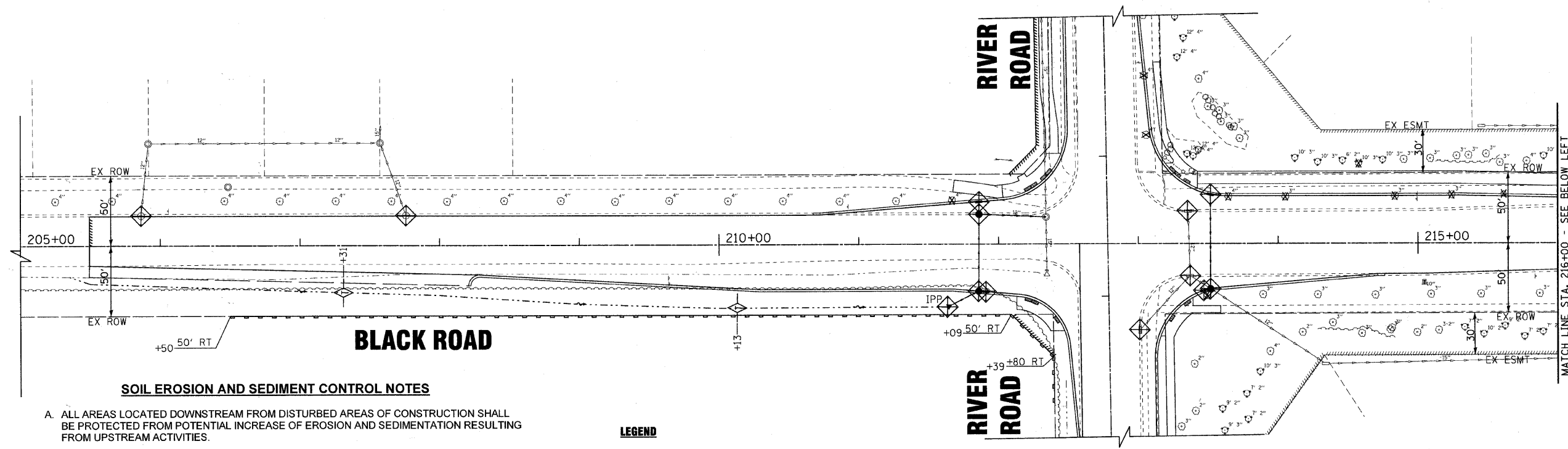
DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-Mot-2.sht

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT**

**SUGGESTED MAINTENANCE OF TRAFFIC
BLACK ROAD RECONSTRUCTION (STAGE III)**

SCALE: 1"=50' STA. 205+00 TO STA. 221+00

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 20
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

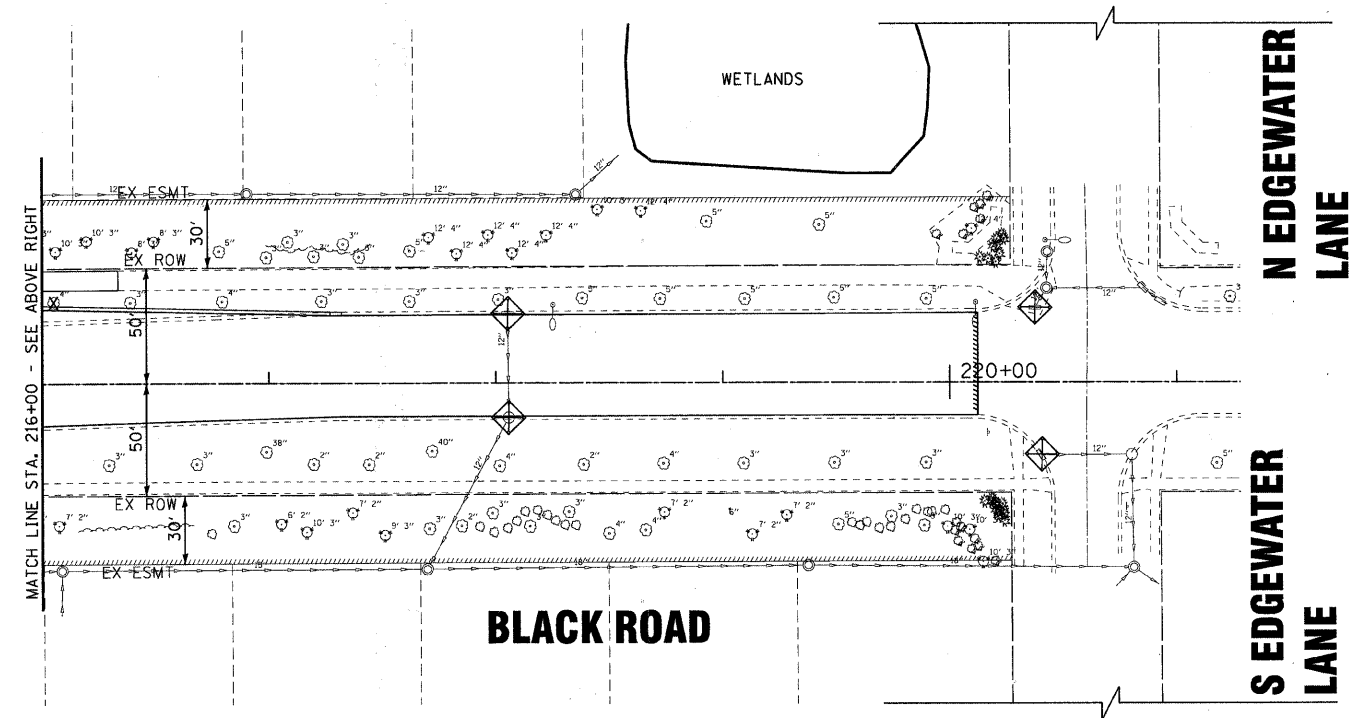


SOIL EROSION AND SEDIMENT CONTROL NOTES

- A. ALL AREAS LOCATED DOWNSTREAM FROM DISTURBED AREAS OF CONSTRUCTION SHALL BE PROTECTED FROM POTENTIAL INCREASE OF EROSION AND SEDIMENTATION RESULTING FROM UPSTREAM ACTIVITIES.
- B. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF CONSTRUCTION SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.
- C. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED AND FUNCTIONAL PRIOR TO THE START OF DISTURBANCE.
- D. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICAL IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED; BUT IN NO CASE SHALL THESE MEASURES BE INSTALLED MORE THAN 7 DAYS AFTER THE CONSTRUCTION IN THIS AREA TEMPORARILY OR PERMANENTLY CEASES.
- E. ALL STORM SEWER STRUCTURES THAT RECEIVE RUNOFF DURING CONSTRUCTION SHALL INCLUDE INLET PROTECTION FILTERS TO PREVENT DEBRIS AND EXCESSIVE SEDIMENT FROM ENTERING THE STORM SEWER SYSTEM. THESE PROTECTIVE MEASURES SHALL BE PROPERLY INSTALLED, MAINTAINED, AND REMOVED IN THEIR ENTIRETY AFTER THE AREA TRIBUTARY TO THE STORM STRUCTURE IS STABILIZED.
- F. DISCHARGES FROM DEWATERING OPERATIONS SHALL ENTER OR BE ROUTED TO A SEDIMENT AND EROSION CONTROL SYSTEM OR DEVICE.
- G. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT SHALL BE PROPERLY STABILIZED OR DISPOSED.
- H. REPAIR, REPLACE OR MAINTAIN EROSION AND SEDIMENT CONTROL STRUCTURES AFTER A RAINFALL EVENT OF 1/2 INCH OR MORE OVER A 24-HOUR PERIOD AND ON A BI-WEEKLY BASIS AS A MINIMUM.
- I. MAKE ADJUSTMENTS TO THE SEDIMENTATION AND EROSION CONTROL PLAN AND METHODS, AS NEEDED, TO ACCOMPLISH THE INTENDED PURPOSE.
- J. ALL ADJACENT ROADWAYS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY, AND CLEANED WHEN NECESSARY OR AS DETERMINED BY THE VILLAGE OF SHOREWOOD OR THE ENGINEER.
- K. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL AND/OR DEBRIS UPON OR INTO ANY CHANNEL, DITCH, SWALE OR ANY OTHER TEMPORARY OR PERMENT LOCATIONS MEANT TO CONVEY SITE DRAINAGE.
- L. IF THE CONTRACTOR IS NOTIFIED BY THE ENGINEER OF AN EROSION AND SEDIMENT CONTROL DEFICIENCY, THE DEFECIENCY MUST BE CORRECTED WITHIN 24-HOURS OF BEING NOTIFIED.

LEGEND

- PERIMETER EROSION BARRIER
- INLET FILTERS
- INLET AND PIPE PROTECTION
- TEMPORARY DITCH CHECKS



COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 0298-0396 - EXPIRES 12/31/2011
 PROJECT NO. 09-00028-00-CH
 DATE: 12/6/10



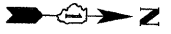
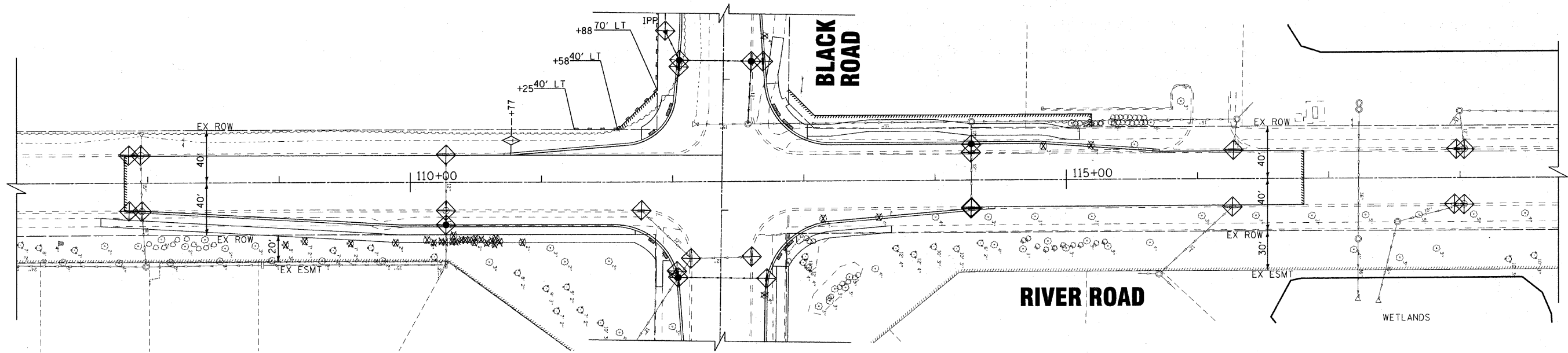
DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED - 2-14-11 PER IDOT
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-EROS1.shx

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT**

**EROSION CONTROL PLAN
BLACK ROAD**

SCALE: 1"=40'
STA. 205+00 TO STA. 221+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298-0396	09-00028-00-CH	WILL	63	21
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT N-300333851				



LEGEND

- PERIMETER EROSION BARRIER
- ◆ INLET FILTERS
- ◆ IPP INLET AND PIPE PROTECTION
- ◆ TEMPORARY DITCH CHECKS

COPYRIGHT © 2008 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-000211 - EXPIRES 6/30/2011
 2-27-2011
 3:24:58 PM
 \\C:\Users\jgibson\Documents\DRAWINGS\DNS\090091\BldgR\PH2\EROS2.dwg



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED -
CHECKED - DSH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-EROS2.shx

**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

**EROSION CONTROL PLAN
 RIVER ROAD**

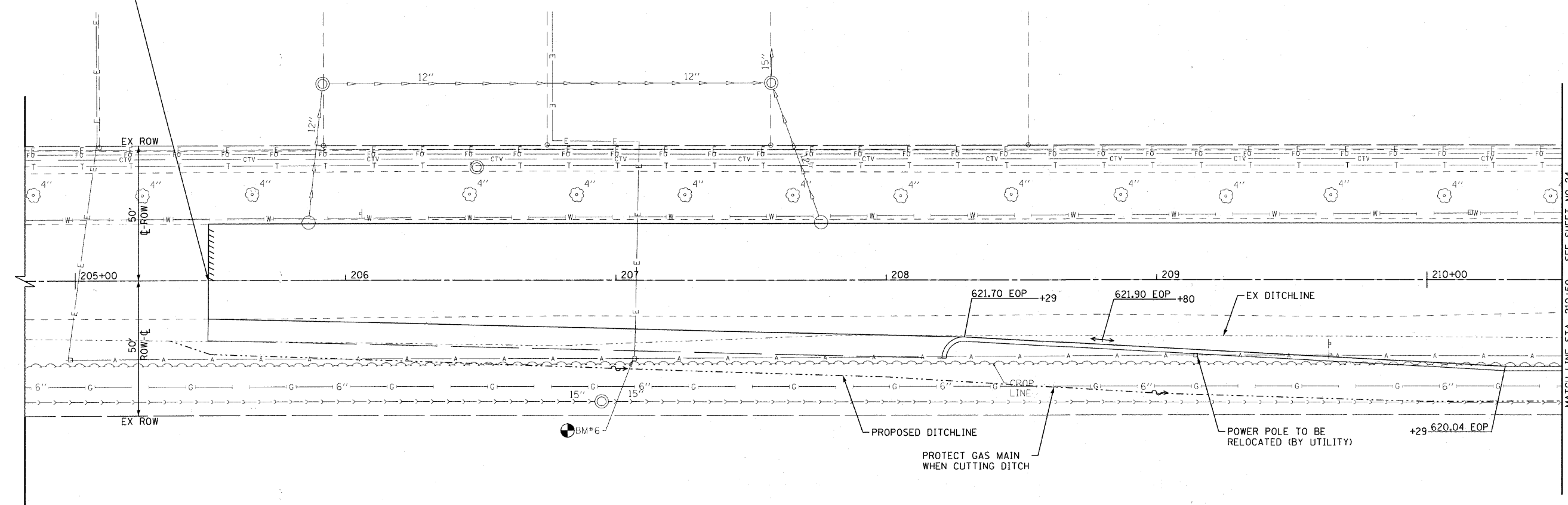
SCALE: 1"=40'

STA. 107+00 TO STA. 118+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298 0396	09-00028-00-CH	WILL	63	22
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			M-9003(385)	

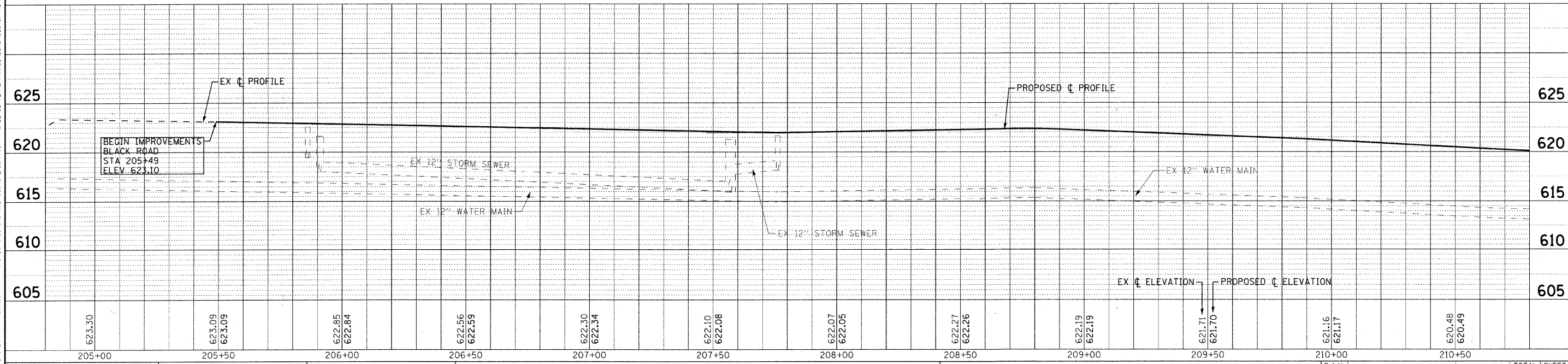


BEGIN IMPROVEMENTS
BLACK ROAD
STA. 205+49



MATCH LINE STA. 210+50 - SEE SHEET NO. 24

BLACK ROAD



	205+00	205+50	206+00	206+50	207+00	207+50	208+00	208+50	209+00	209+50	210+00	210+50
623.30	623.09 623.09	622.85 622.84	622.56 622.59	622.30 622.34	622.10 622.08	622.07 622.05	622.27 622.26	622.19 622.19	621.71 621.70	621.16 621.17	620.48 620.49	



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - LKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-DU1.sh+

VILLAGE OF SHOREWOOD, ILLINOIS BLACK ROAD AND RIVER ROAD INTERSECTION IMPROVEMENTS

DRAINAGE AND UTILITY BLACK ROAD

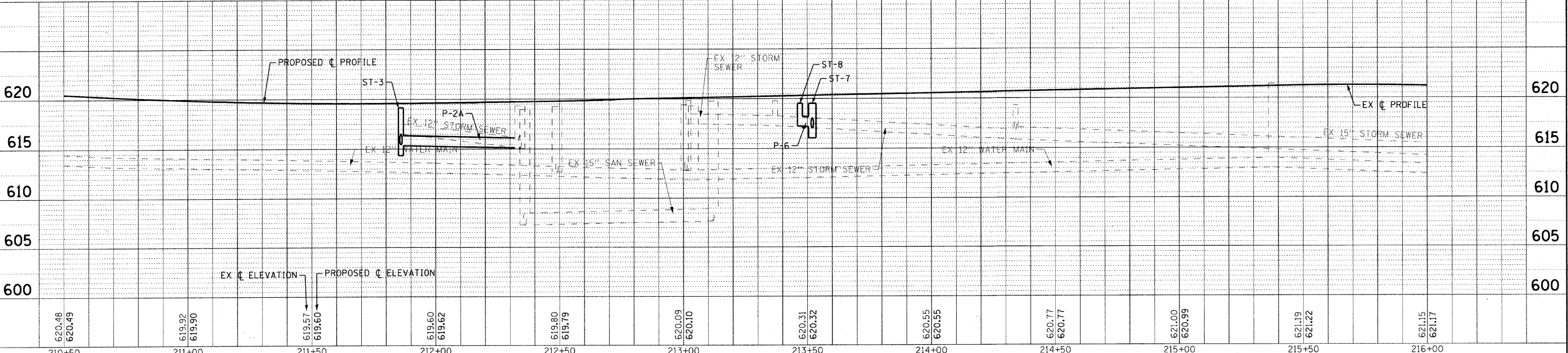
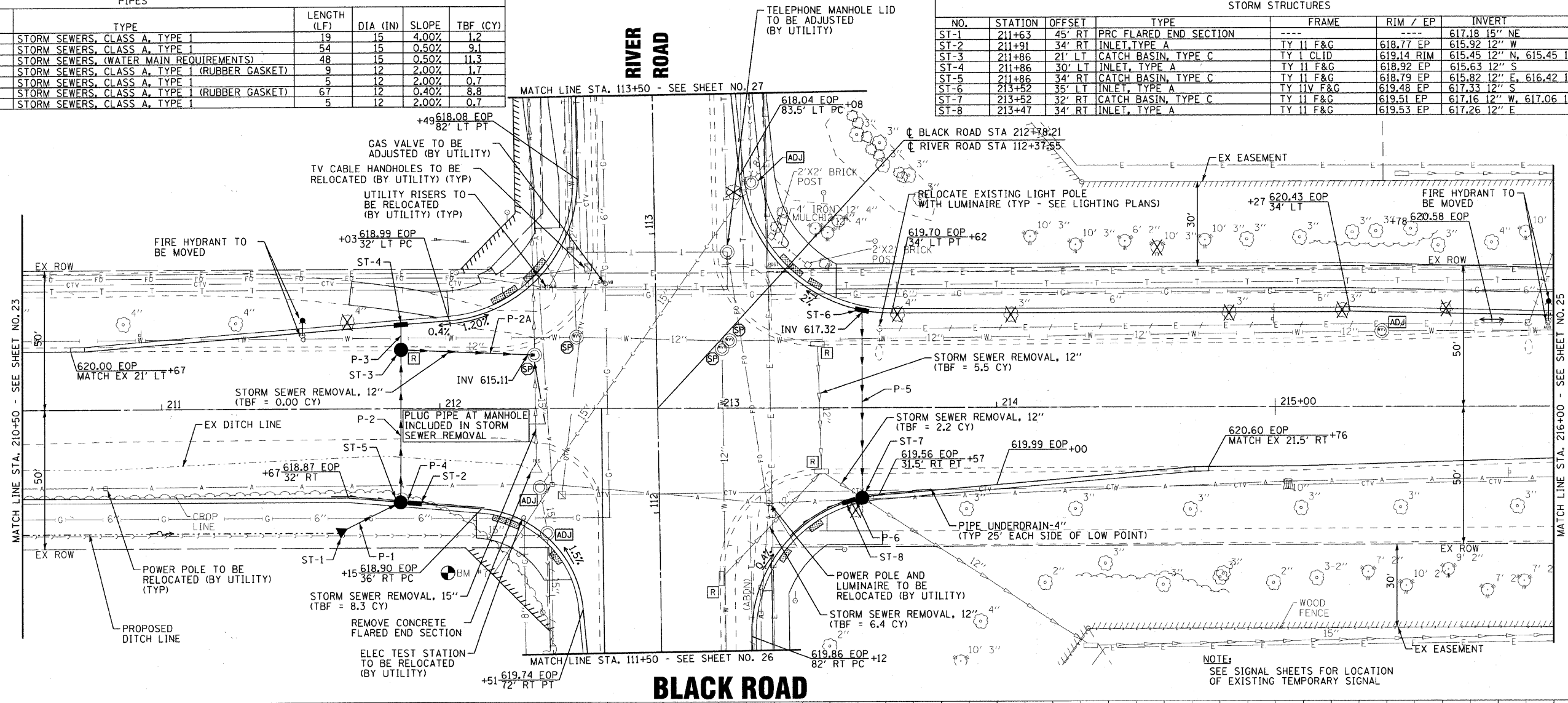
SCALE: H: 1"=20' V: 1"=5' STA. 205+00 TO STA. 210+50

F.A.U. RTE. 0298 0396	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	09-00028-00-CH	WILL	63	23
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

COPYRIGHT © 2010 BY BAXTER A. WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 0298-0396 - EXPIRES 4/30/2011
 2/27/2011 9:56:35 AM

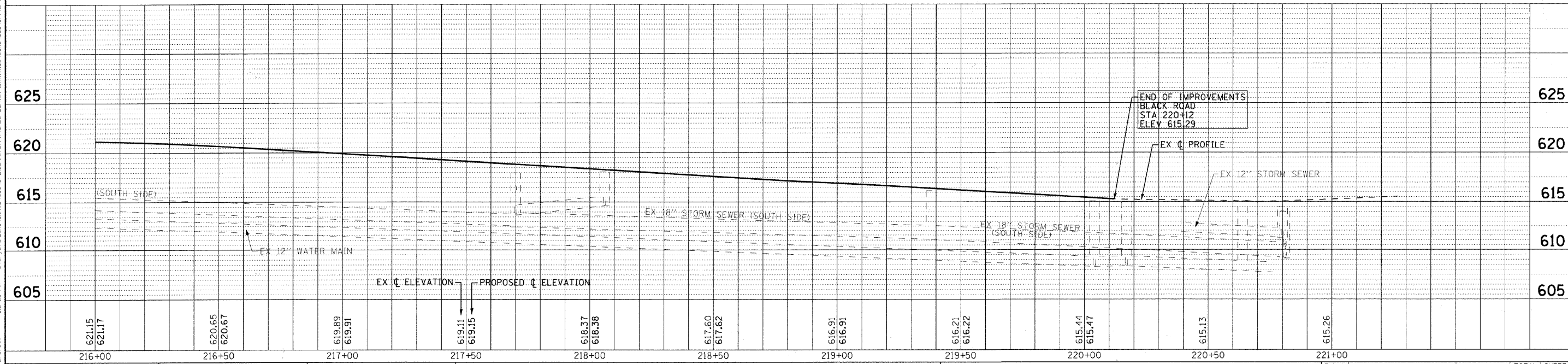
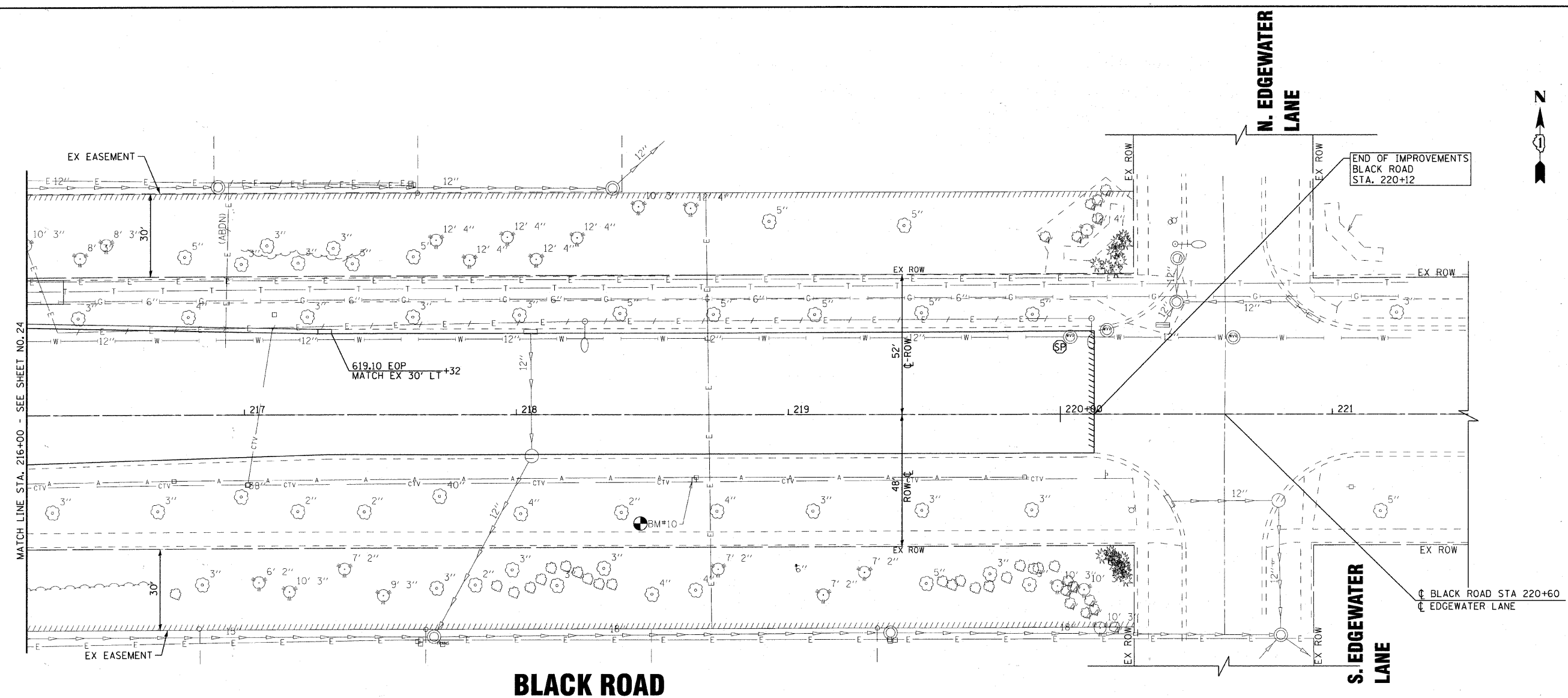
PIPES					
NO.	TYPE	LENGTH (LF)	DIA (IN)	SLOPE	TBF (CY)
P-1	STORM SEWERS, CLASS A, TYPE 1	19	15	4.00%	1.2
P-2	STORM SEWERS, CLASS A, TYPE 1	54	15	0.50%	9.1
P-2A	STORM SEWERS, (WATER MAIN REQUIREMENTS)	48	15	0.50%	11.3
P-3	STORM SEWERS, CLASS A, TYPE 1 (RUBBER GASKET)	9	12	2.00%	1.7
P-4	STORM SEWERS, CLASS A, TYPE 1	5	12	2.00%	0.7
P-5	STORM SEWERS, CLASS A, TYPE 1 (RUBBER GASKET)	67	12	0.40%	8.8
P-6	STORM SEWERS, CLASS A, TYPE 1	5	12	2.00%	0.7

STORM STRUCTURES						
NO.	STATION	OFFSET	TYPE	FRAME	RIM / EP	INVERT
ST-1	211+63	45' RT	PRC FLARED END SECTION	---	---	617.18 15" NE
ST-2	211+91	34' RT	INLET, TYPE A	TY 11 F&G	618.77 EP	615.92 12" W
ST-3	211+86	21' LT	CATCH BASIN, TYPE C	TY 1 CLID	619.14 RIM	615.45 12" N, 615.45 15" S, 615.35 15" E
ST-4	211+86	30' LT	INLET, TYPE A	TY 11 F&G	618.92 EP	615.63 12" S
ST-5	211+86	34' RT	CATCH BASIN, TYPE C	TY 11 F&G	618.79 EP	615.82 12" E, 616.42 15" SW, 615.72 15" N
ST-6	213+52	35' LT	INLET, TYPE A	TY 11V F&G	619.48 EP	617.33 12" S
ST-7	213+52	32' RT	CATCH BASIN, TYPE C	TY 11 F&G	619.51 EP	617.16 12" W, 617.06 12" N, 617.06 12" SE (EX)
ST-8	213+47	34' RT	INLET, TYPE A	TY 11 F&G	619.53 EP	617.26 12" E



	DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE	VILLAGE OF SHOREWOOD, ILLINOIS BLACK ROAD AND RIVER ROAD INTERSECTION IMPROVEMENTS	DRAINAGE AND UTILITY BLACK ROAD	F.A.U. R.T.E. 0298 0596	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 24
	DRAWN - UKB	REVISION			C-91-726-09	CONTRACT NO. 63562			
	CHECKED - LDH	REVISION			FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(385)				
	DATE - 12/6/10	FILE - 090491-PH2-DU2.shx			SCALE: H: 1"=20' V: 1"=5'				

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGNER
 LICENSE NO. 384-000281 - EXPIRES 4/30/2011
 2/27/2011 5:58:50 PM



DESIGNED - DSH	REVISED - 11-19-11 PER VILLAGE
DRAWN - LKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-DU3.sh1

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD AND RIVER ROAD
INTERSECTION IMPROVEMENTS**

**DRAINAGE AND UTILITY
BLACK ROAD**

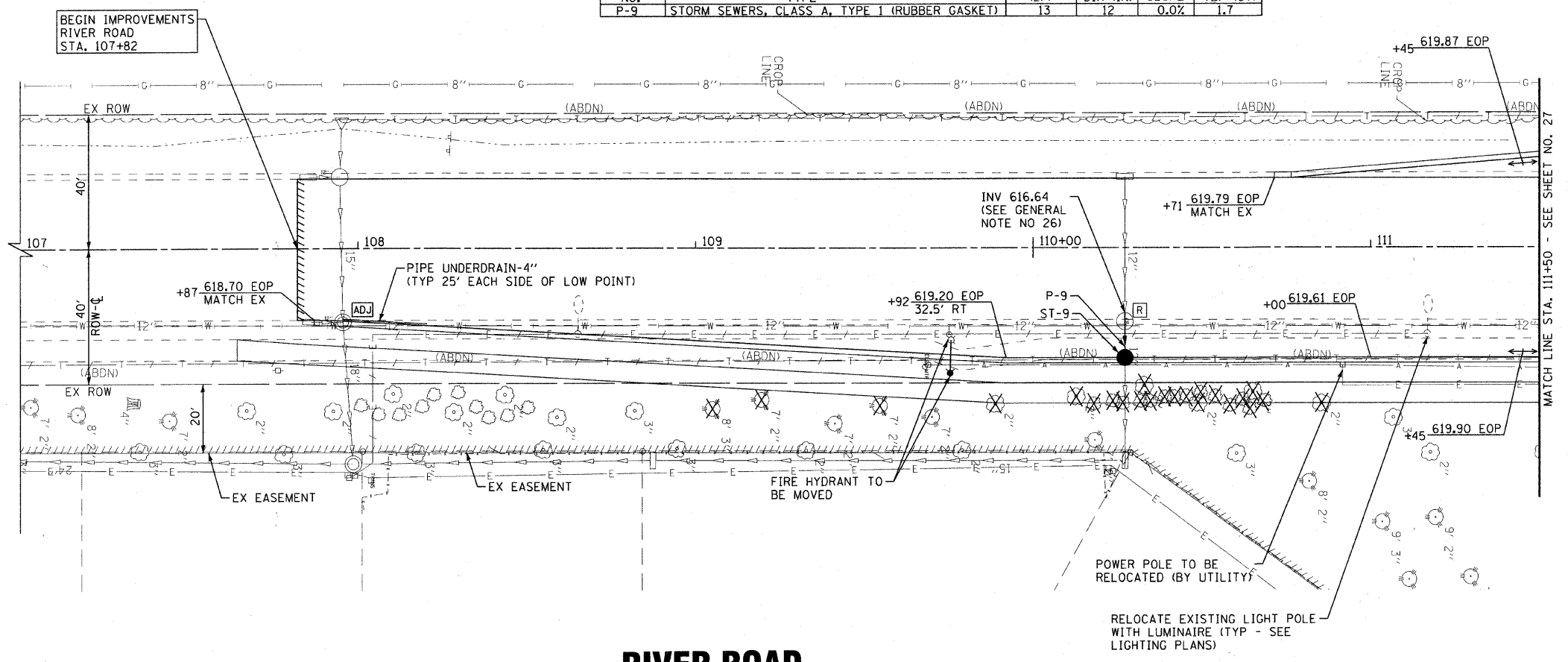
SCALE: H: 1"=20' V: 1"=5' STA. 216+00 TO STA. 221+00

F.A.U. RTE. 0398 0398	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 25
C-91-726-09		CONTRACT NO. 63562		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-90033951

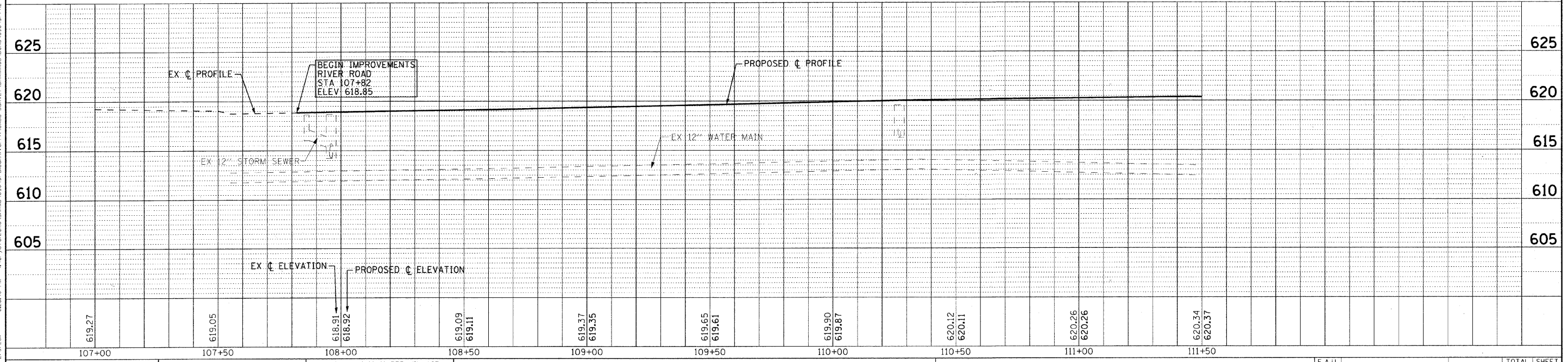
COPYRIGHT © 2010 BY DAKTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 384-00021 - EXPIRES 4/30/2011
 1500 Yorkwood Lane, Suite 100, Shorewood, IL 60054-3400
 TEL: 847.486.1000 FAX: 847.486.1001

STORM STRUCTURES						
NO.	STATION	OFFSET	TYPE	FRAME	RIM / EP	INVERT
ST-9	110+27	32.5' RT	CATCH BASIN, TYPE C	TYPE IIV F&G	619.39 EP	616.64 12" W, 616.64 12" E (EX) (FIELD VERIFY)

PIPES					
NO.	TYPE	LENGTH (LF)	DIA (IN)	SLOPE	TBF (CY)
P-9	STORM SEWERS, CLASS A, TYPE I (RUBBER GASKET)	13	12	0.0%	1.7



RIVER ROAD



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - UKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-DU4.shx

VILLAGE OF SHOREWOOD, ILLINOIS BLACK ROAD AND RIVER ROAD INTERSECTION IMPROVEMENTS

DRAINAGE AND UTILITY RIVER ROAD

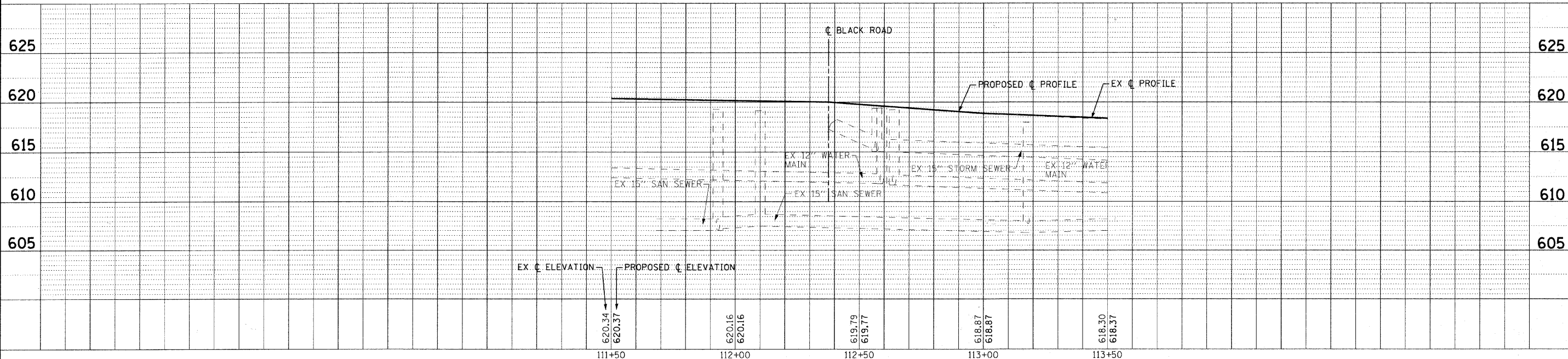
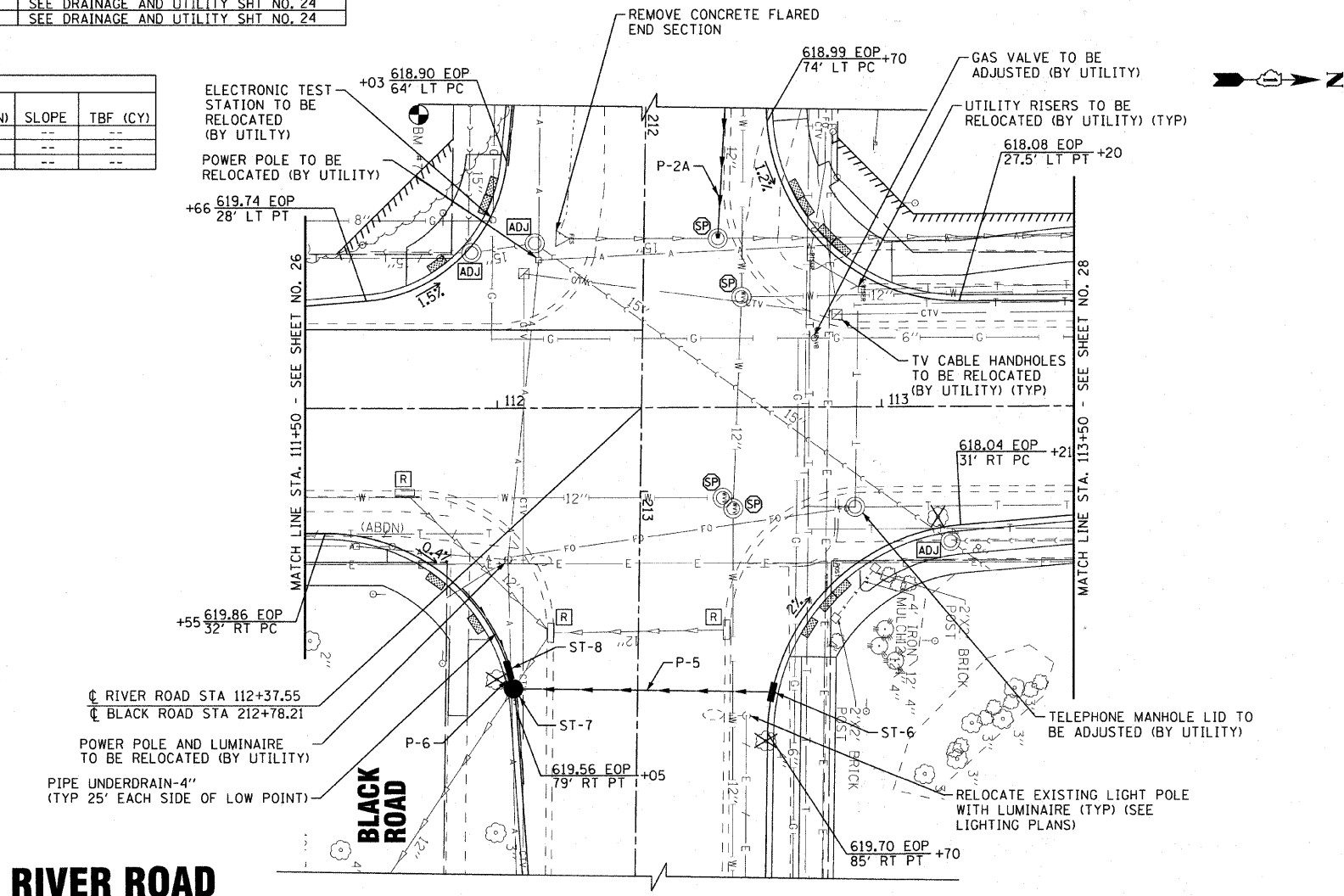
SCALE: H: 1"=20' V: 1"=5'
STA. 107+00 TO STA. 111+50

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 26
C-91-726-09		CONTRACT NO. 63562		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)

C:\WORK\09-00028-00-CH\09-00028-00-CH-01\09-00028-00-CH-01-01.dwg
 12/23/2010 2:23:20 PM
 WOODMAN CONSULTING ENGINEERS
 1000 W. WASHINGTON ST., SUITE 200
 DEERFIELD, IL 60015
 TEL: 847-434-8800 FAX: 847-434-8801
 WWW.WOODMANENGINEERS.COM

STORM STRUCTURES						
NO.	STATION	OFFSET	TYPE	FRAME	RIM / EP	INVERT
ST-6	--	--	--	--	--	SEE DRAINAGE AND UTILITY SHT NO. 24
ST-7	--	--	--	--	--	SEE DRAINAGE AND UTILITY SHT NO. 24
ST-8	--	--	--	--	--	SEE DRAINAGE AND UTILITY SHT NO. 24

PIPES						
NO.	TYPE	LENGTH (LF)	DIA (IN)	SLOPE	TBF (CY)	
P-2A	SEE DRAINAGE AND UTILITY SHT NO. 24	--	--	--	--	--
P-5	SEE DRAINAGE AND UTILITY SHT NO. 24	--	--	--	--	--
P-6	SEE DRAINAGE AND UTILITY SHT NO. 24	--	--	--	--	--



COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00021 - EXPIRES 4/30/2011
 2/23/2011 10:52:08 AM



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - UKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-0U5.sh+

VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD AND RIVER ROAD
INTERSECTION IMPROVEMENTS

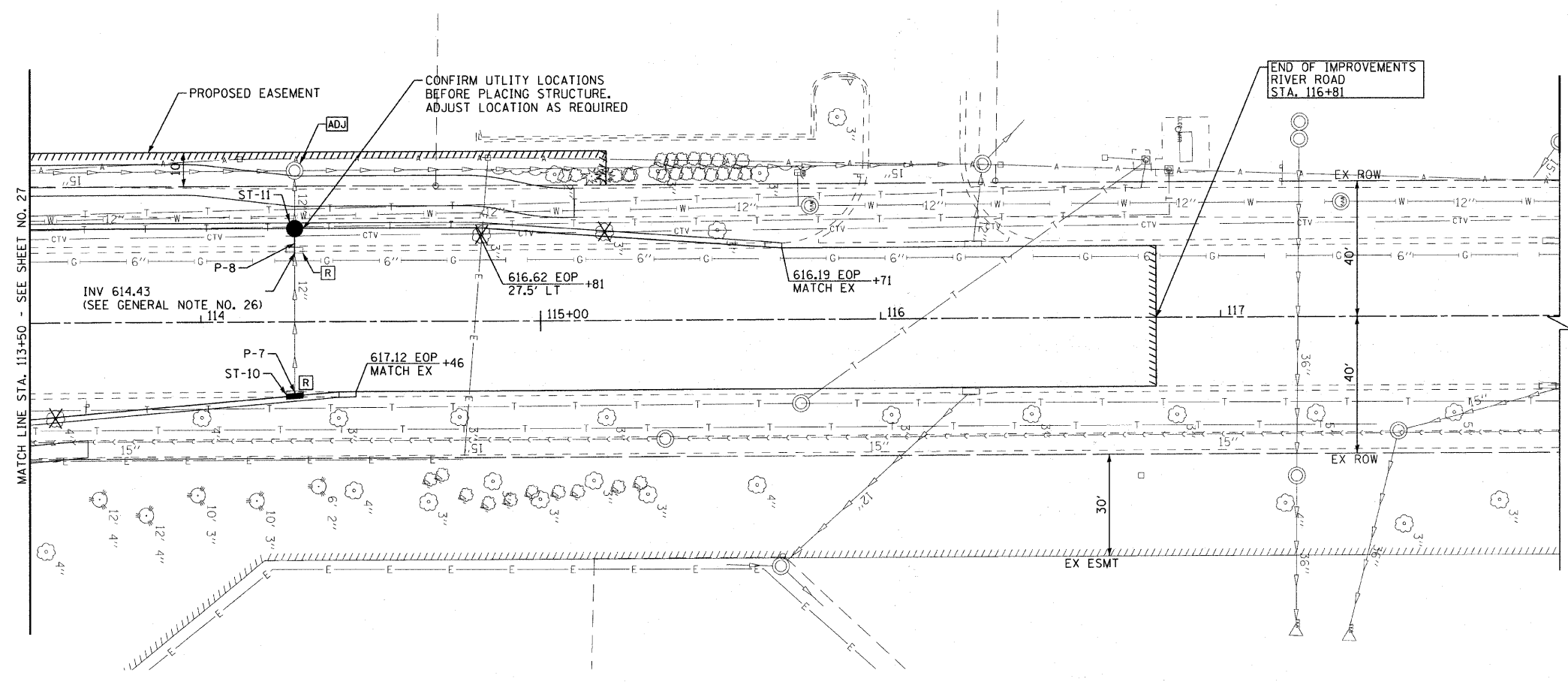
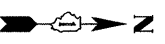
DRAINAGE AND UTILITY
RIVER ROAD

SCALE: H: 1"=20' V: 1"=5' STA. 111+50 TO STA. 113+50

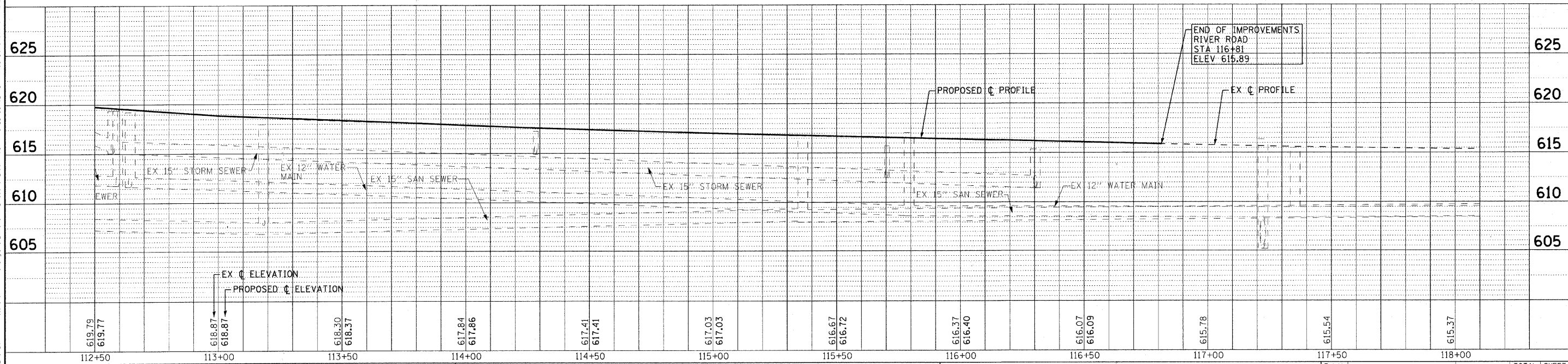
F.A.U. RTE. 0298 0386	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 27
C-91-726-09		CONTRACT NO. 63562		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)

STORM STRUCTURES						
NO.	STATION	OFFSET	TYPE	FRAME	RIM / EP	INVERT
ST-10	114+28	22' RT	INLET, TYPE A	TY 11V F&G	617.15 EP	614.92 12" W
ST-11	114+28	27.5' LT	CATCH BASIN, TYPE C	TY 11V F&G	617.13 EP	614.33 12" W (EX) 614.33 12" W (EX) (FIELD VERIFY)

PIPES						
NO.	TYPE	LENGTH (LF)	DIA (IN)	SLOPE	TBF (CY)	
P-7	STORM SEWERS, CLASS A, TYPE 1	2	12	1.00%	0.1	
P-8	STORM SEWERS, CLASS A, TYPE 1	7	12	1.64%	0.9	



RIVER ROAD



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - LKB	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-DUG.sh1

VILLAGE OF SHOREWOOD, ILLINOIS BLACK ROAD AND RIVER ROAD INTERSECTION IMPROVEMENTS

DRAINAGE AND UTILITY RIVER ROAD

SCALE: H: 1"=20' V: 1"=5'
STA. 112+50 TO STA. 118+00

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 28
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

COPYRIGHT © 2010 BY BAXTER A. WOODMAN INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 081-00021 - EXPIRES 7/30/2011
 2/2/2011 6:44:03 PM

COPYRIGHT © 2009 BY BAYTES & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-000231 - EXPIRES 4/30/2011
 626 S. PULASKI ST., SUITE 200, CHICAGO, IL 60607
 TEL: 312.467.1000 FAX: 312.467.1001
 WWW.BAYTESWOODMAN.COM

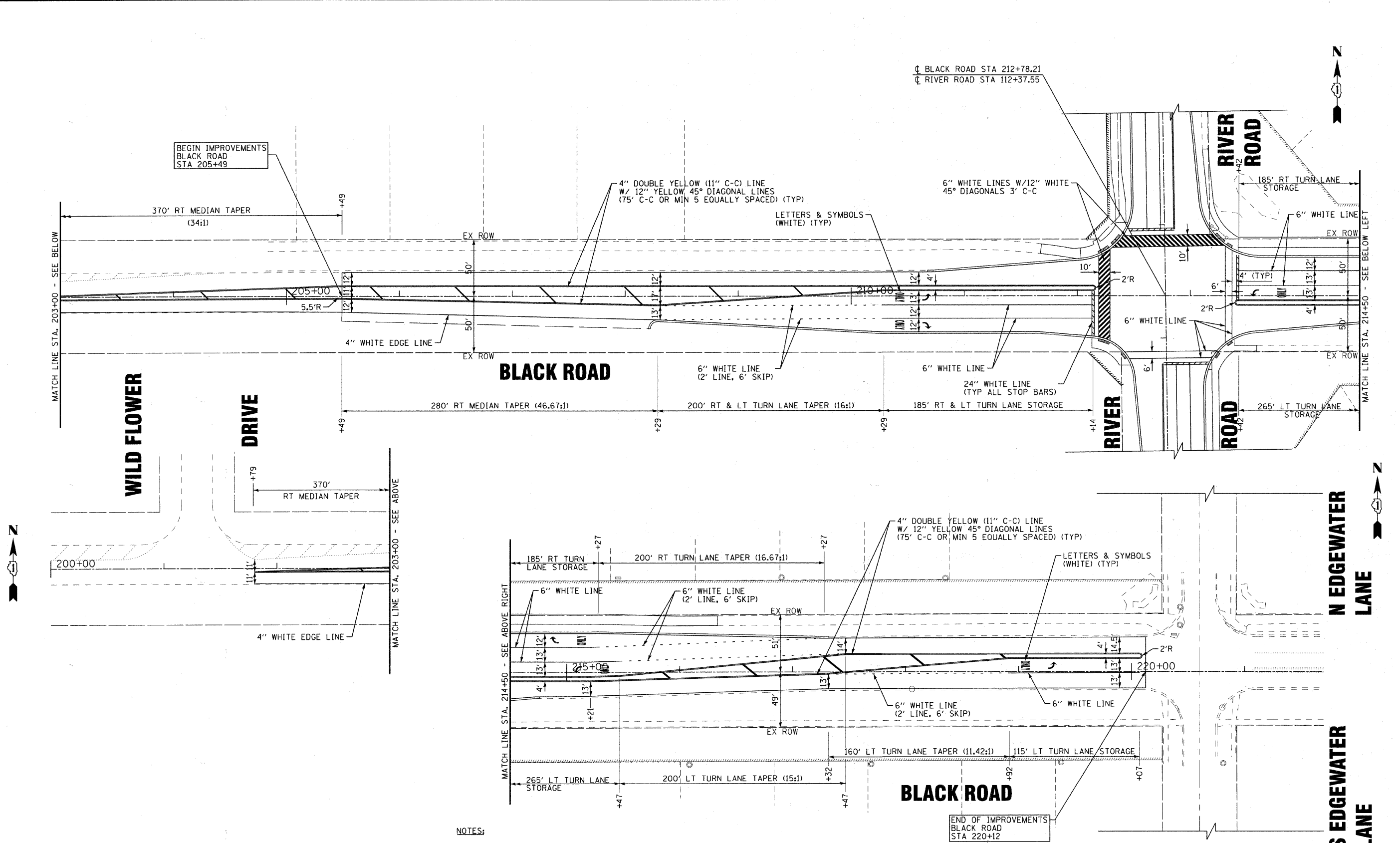


DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-PM1.shx

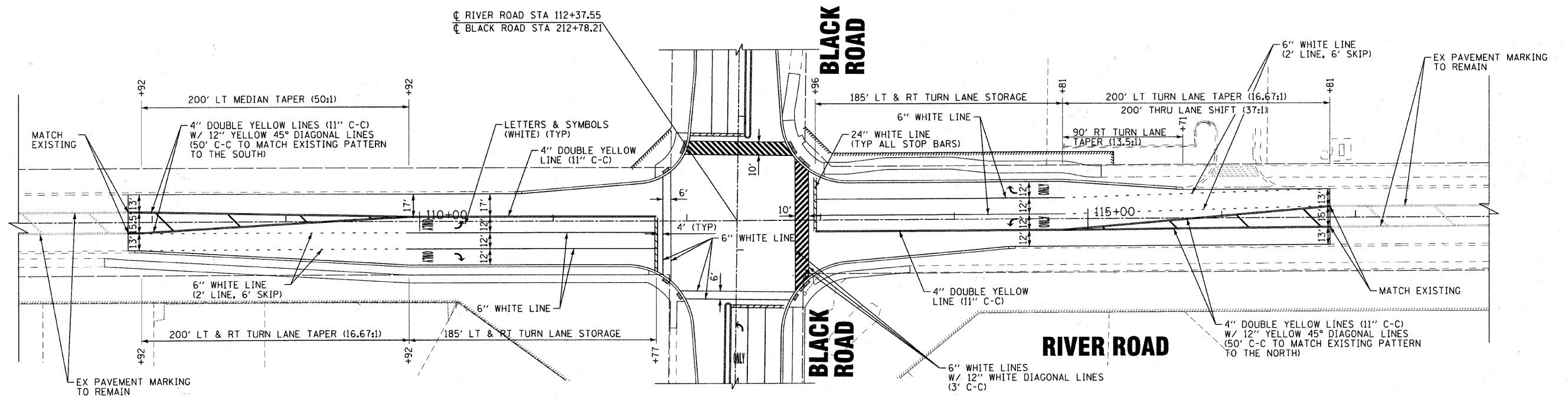
**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

**PAVEMENT MARKING PLAN
 BLACK ROAD**
 SCALE: 1"=40'
 STA. 200+00 TO STA. 221+00

F.A.U. R.T.E. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 29
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT W-9003(385)				



- NOTES:**
- ALL PAVEMENT MARKING IS THERMOPLASTIC PAVEMENT MARKING.
 - ALL PAVEMENT MARKING SHALL BE INSTALLED ACCORDING TO THE IDOT DISTRICT ONE DETAIL FOR TYPICAL PAVEMENT MARKINGS (TC-13) AND HIGHWAY STANDARD 780001.



NOTES:

1. ALL PAVEMENT MARKING IS THERMOPLASTIC PAVEMENT MARKING.
2. ALL PAVEMENT MARKING SHALL BE INSTALLED ACCORDING TO THE IDOT DISTRICT ONE DETAIL FOR TYPICAL PAVEMENT MARKINGS (TC-13) AND HIGHWAY STANDARD 780001.

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 184-00021 - EXPIRES 4/30/2011
 300 N. LAUREL STREET, SUITE 100, CHICAGO, IL 60610



DESIGNED - DSH	REVISED -
DRAWN - CJC	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-PM2.sht

**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

**PAVEMENT MARKING PLAN
 RIVER ROAD**

SCALE: 1"=40'

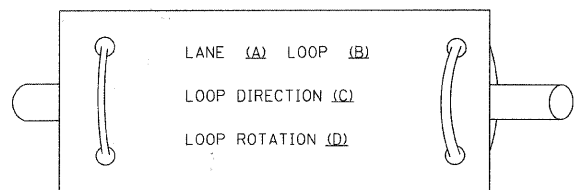
STA. 107+00 TO STA. 118+00

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 30
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

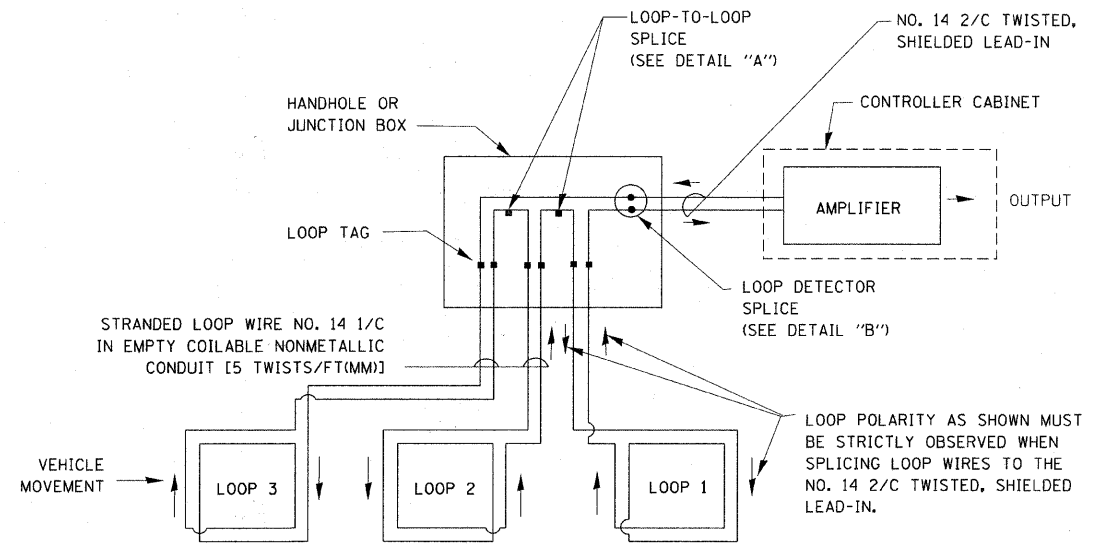
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

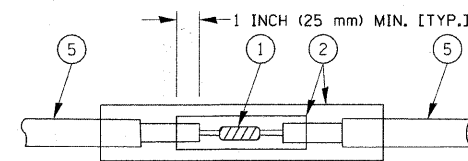


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

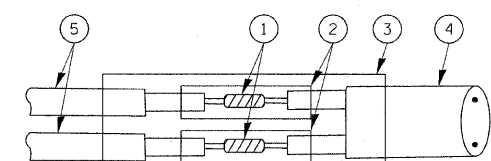


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

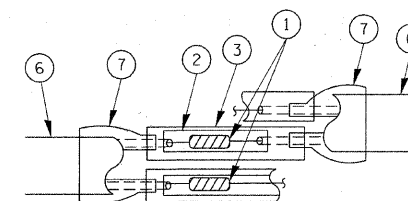


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

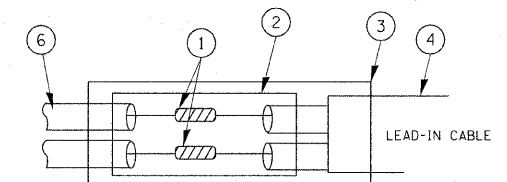


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



PREFORMED LOOP

**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

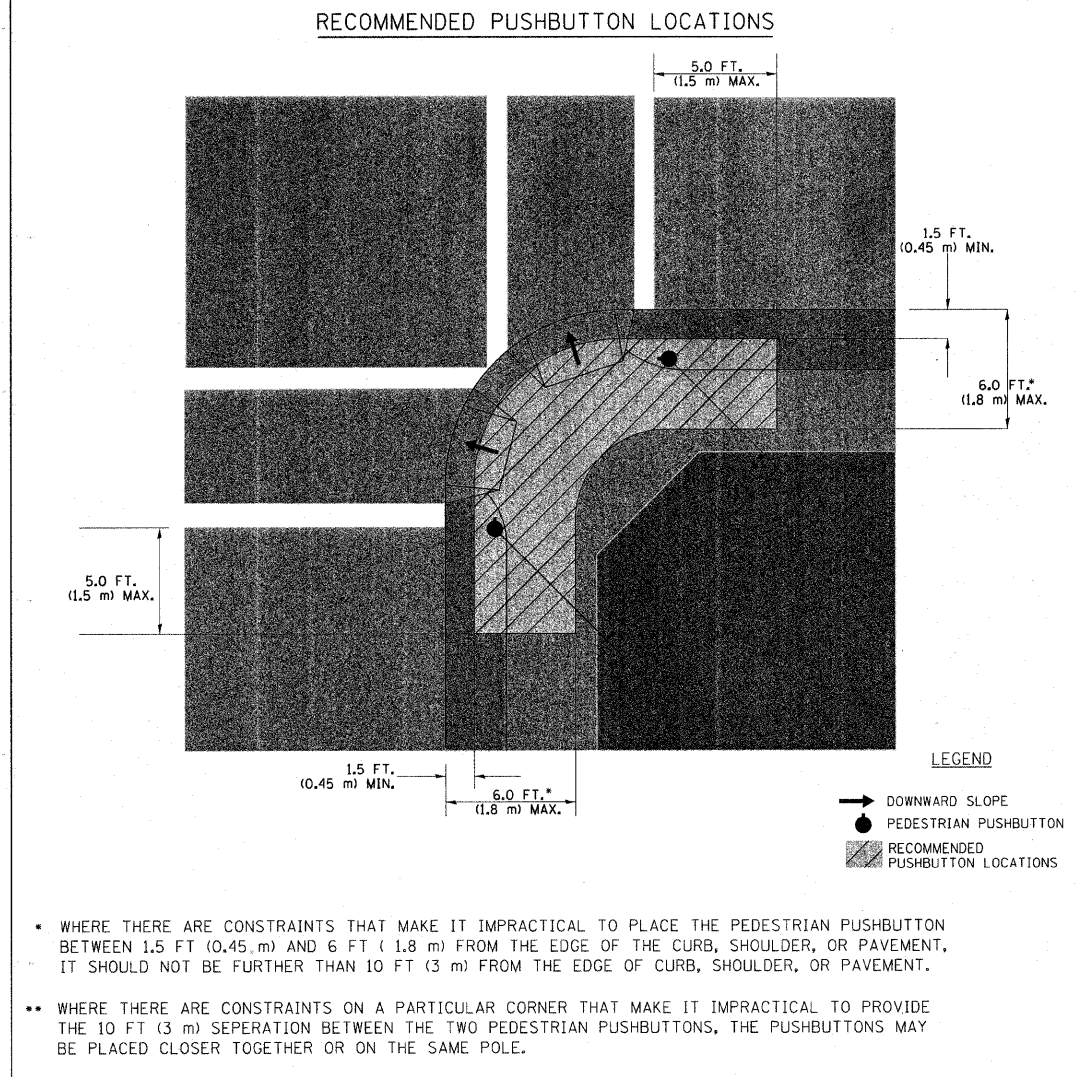
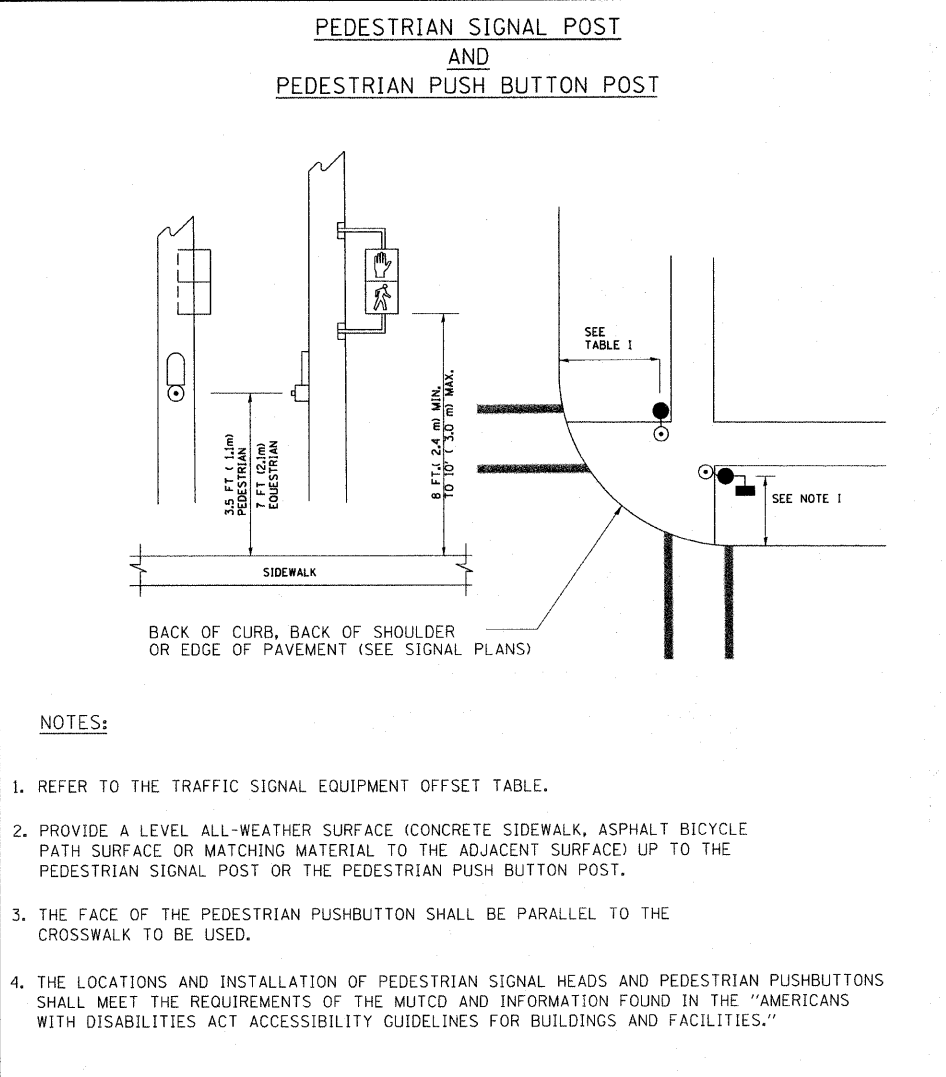
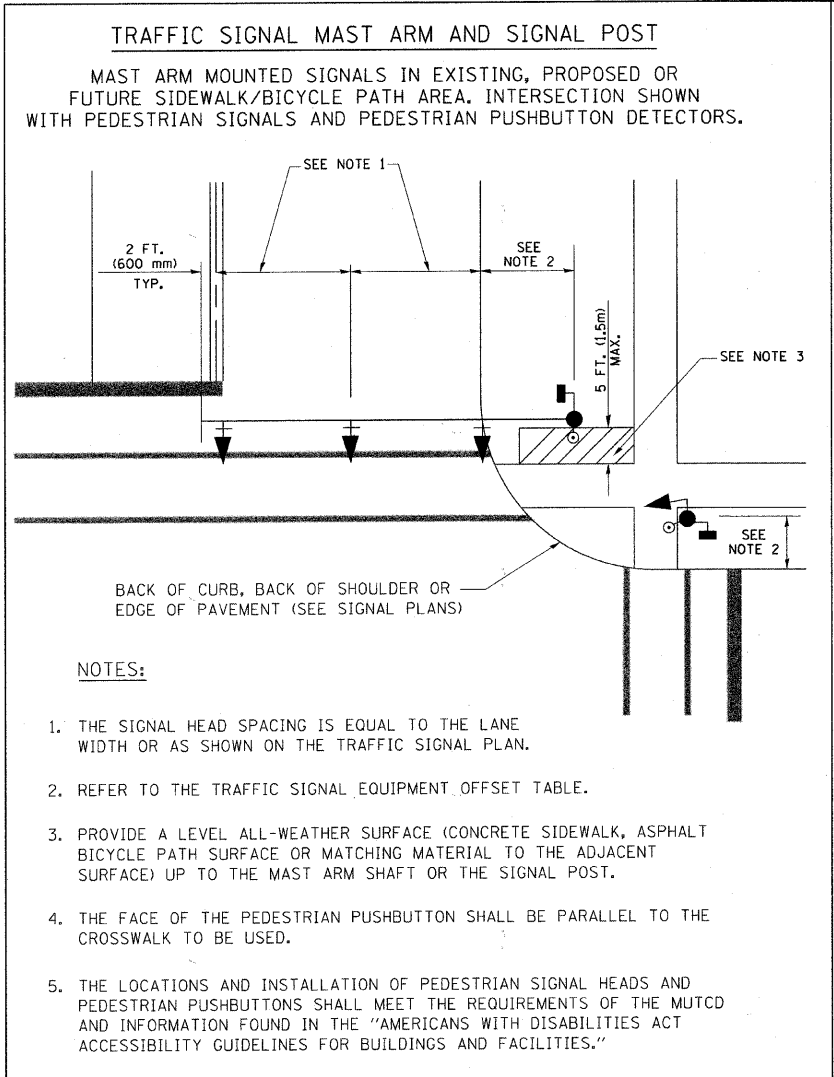
FILE NAME =	USER NAME = kanthaphixaybo	DESIGNED - DAD	REVISED -
c:\pwwork\PMIDOT\KANTHAPHIXAYBO\d01126	4\tracfile\legend.v7.dgn	DRAWN - BCK	REVISED -
	PLOT SCALE = 20.0000 "/ IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 10/6/2009	DATE - 10/28/09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: SHEET NO. 1 OF 6 SHEETS STA. TO STA.

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 31
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				



NOTES:

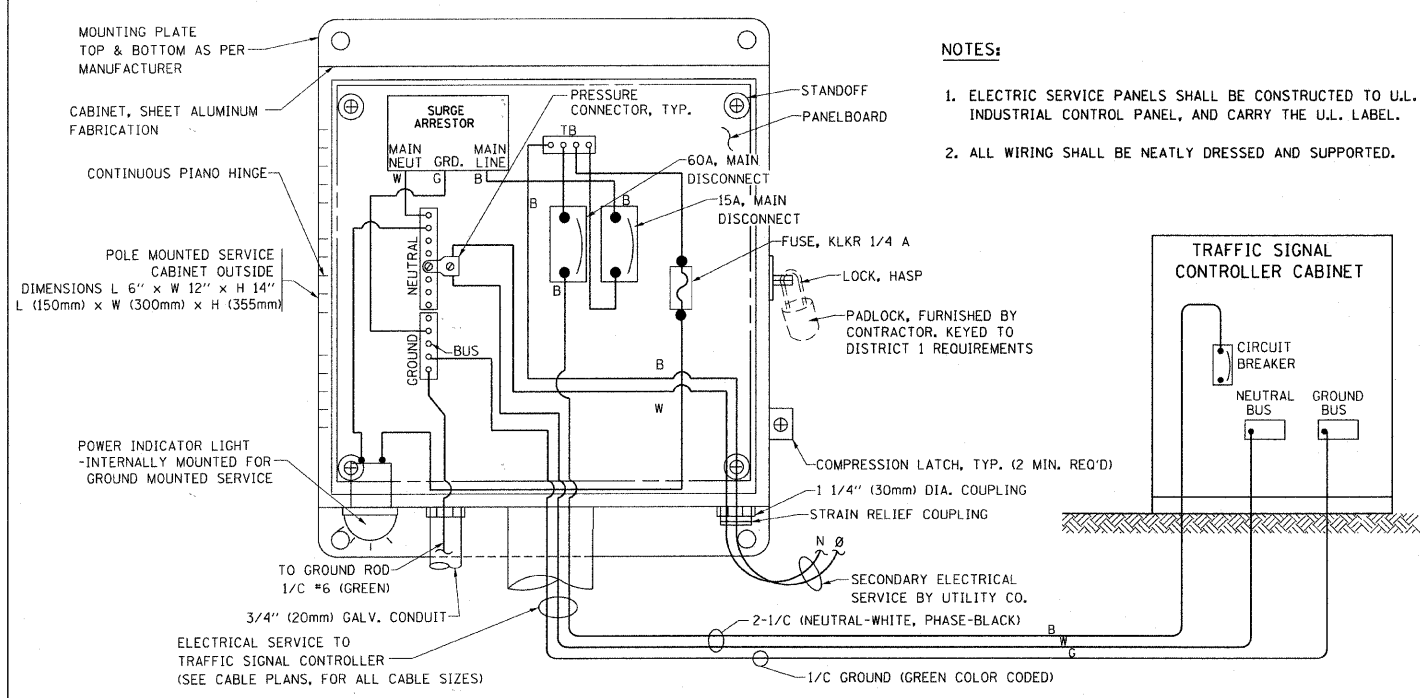
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

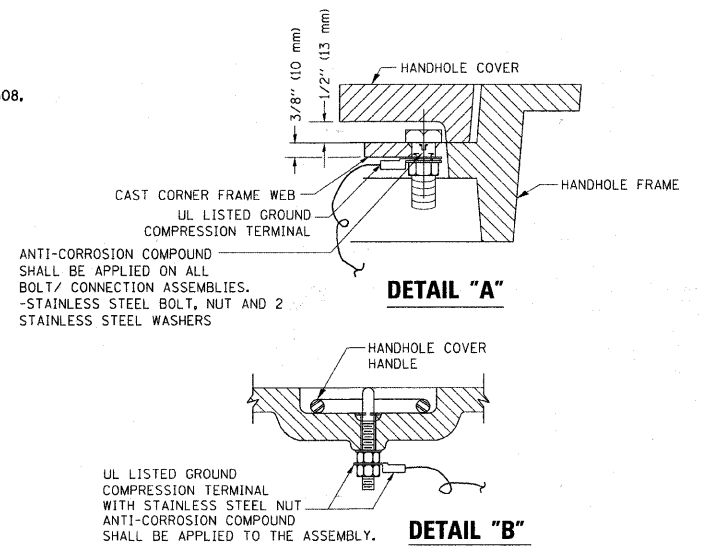
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

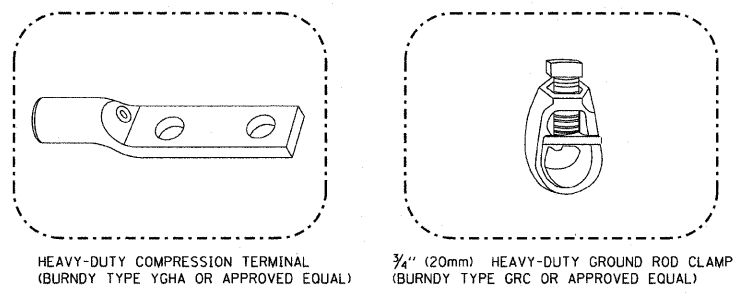
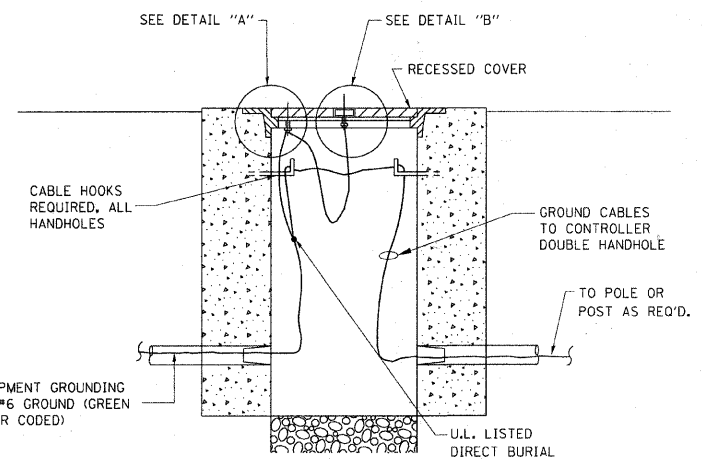
1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.



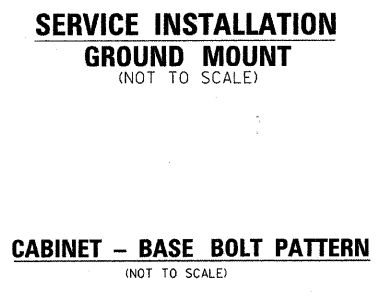
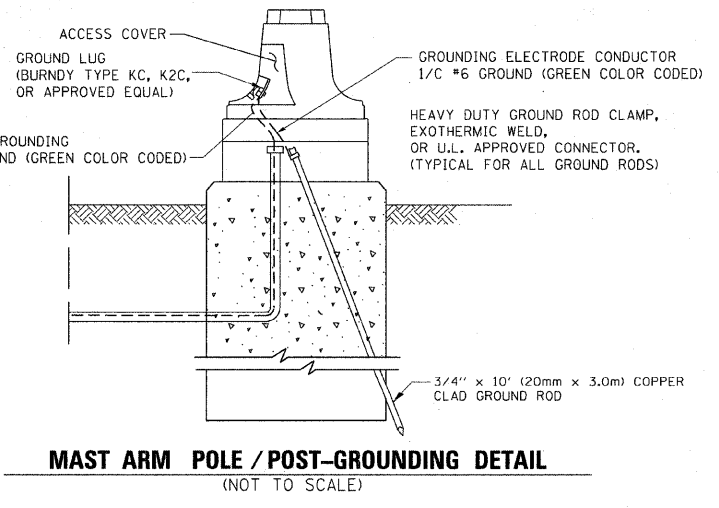
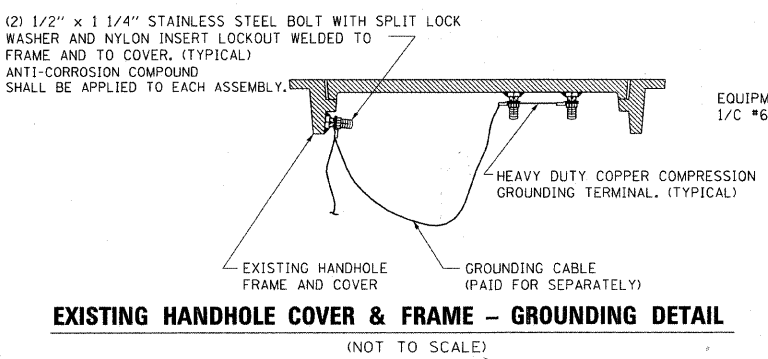
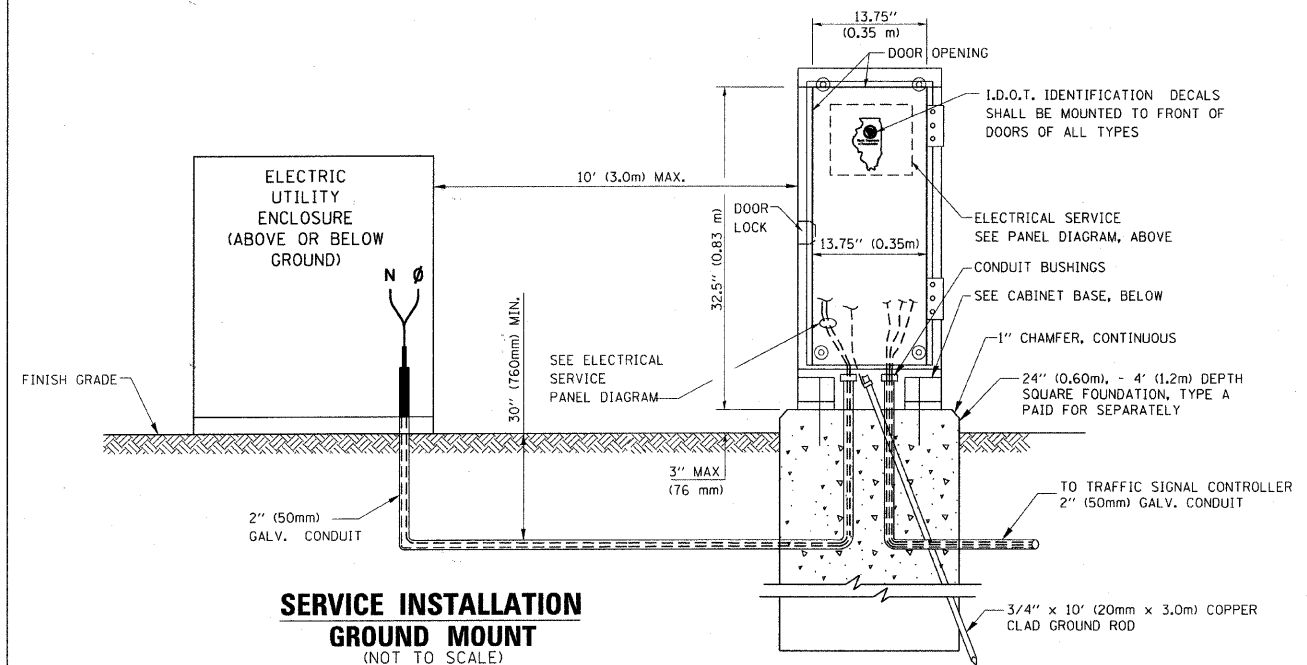
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



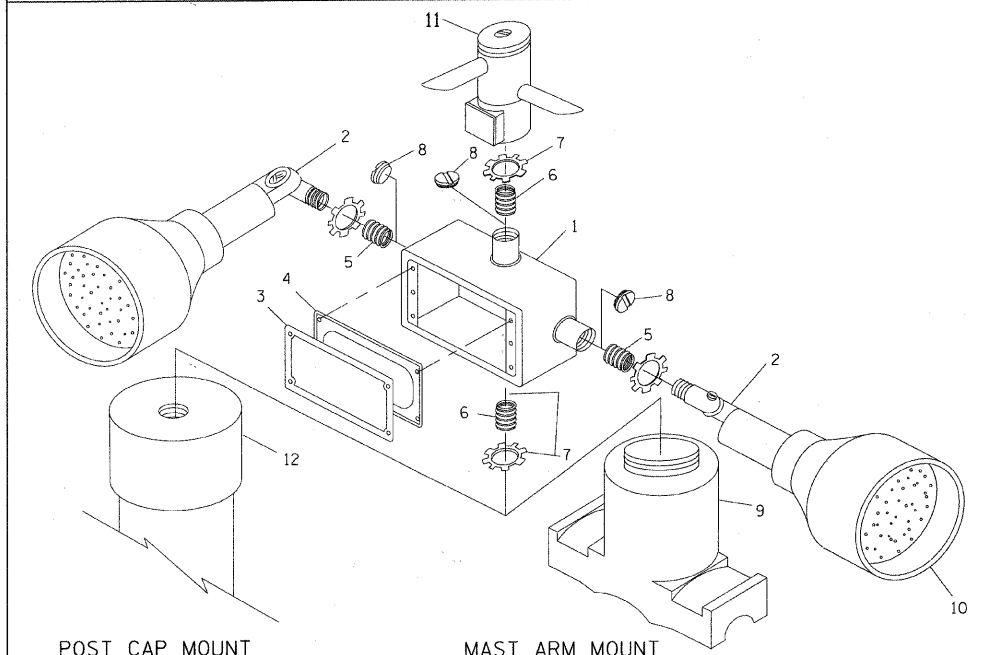
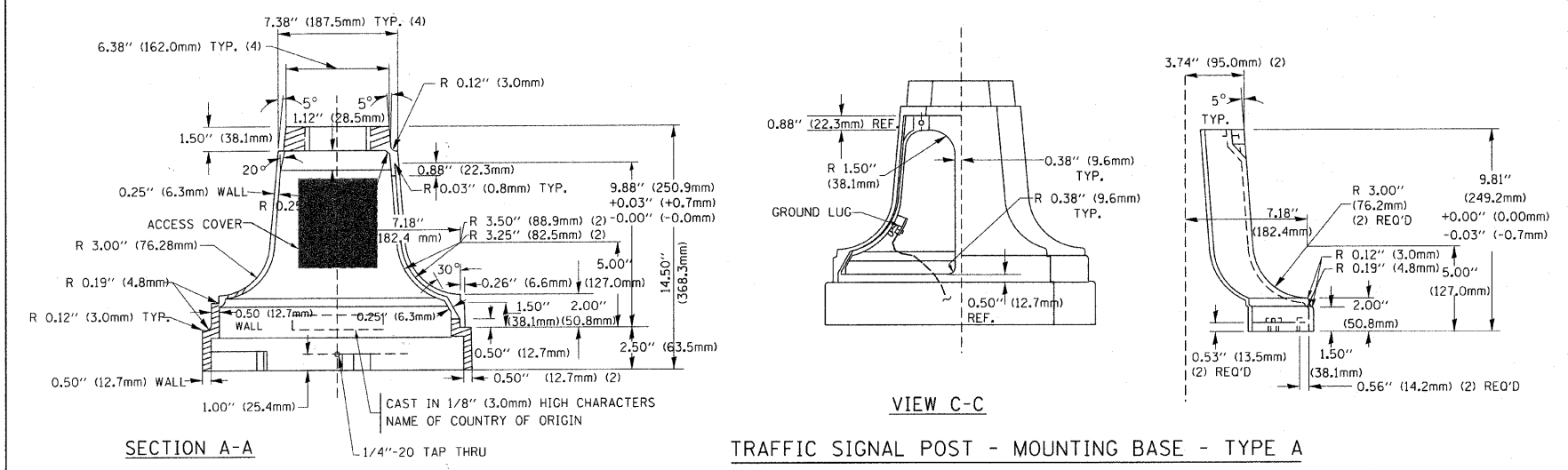
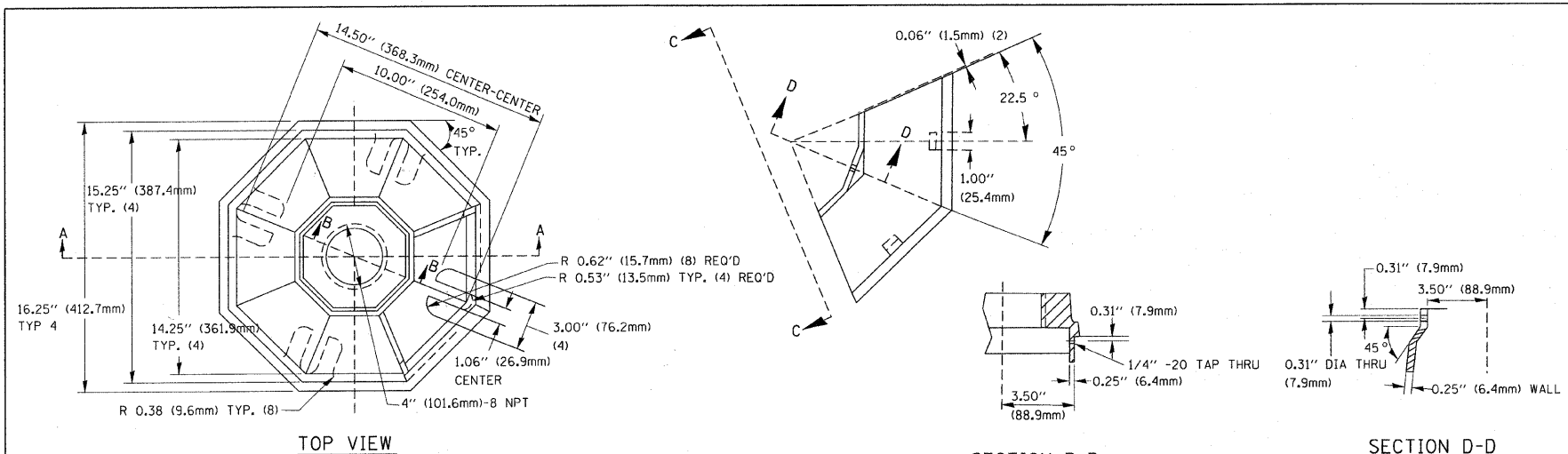
- NOTES:**
- GROUNDING SYSTEM**
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
 2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
 3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
 4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

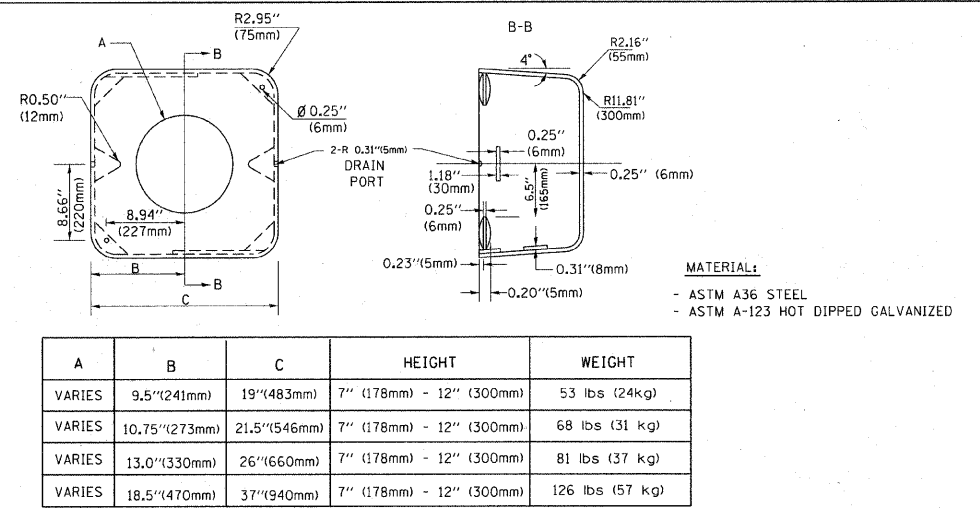


FILE NAME =	USER NAME = kanthaphxaybc	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.D. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 33
CONTRACT NO. 63562	SCALE: SHEET NO. 3 OF 6 SHEETS	DATE - 10/28/09	REVISED -			STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003 (385)			
PLOT SCALE = 20.0000 "/>										
PLOT DATE = 10/6/2009	CHECKED - DAD	DATE - 10/28/09	REVISED -							



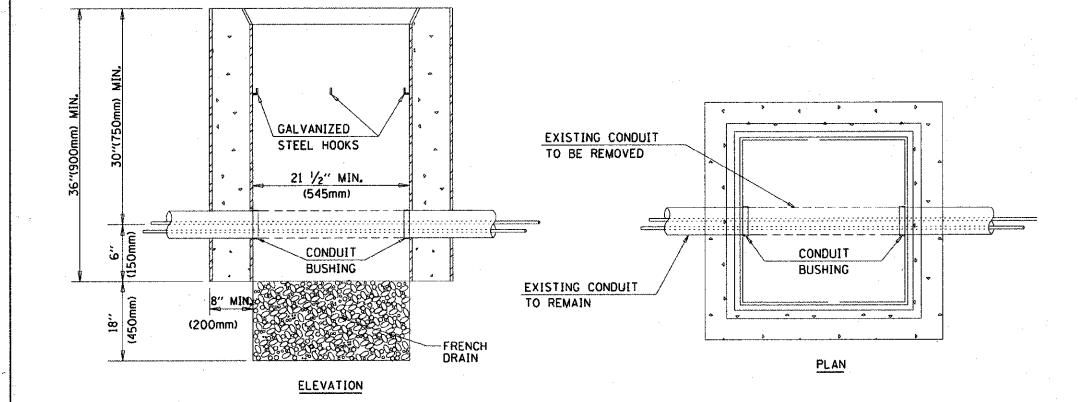
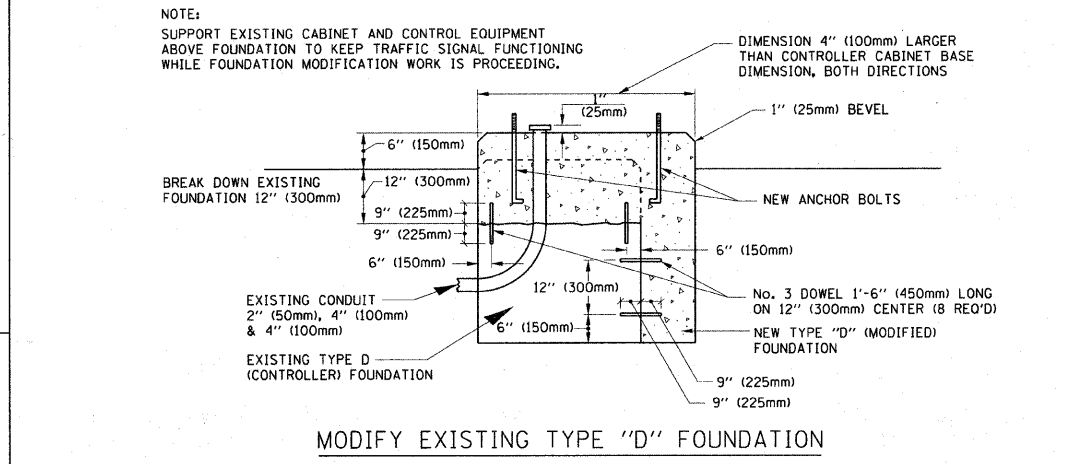
ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

- NOTES:**
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

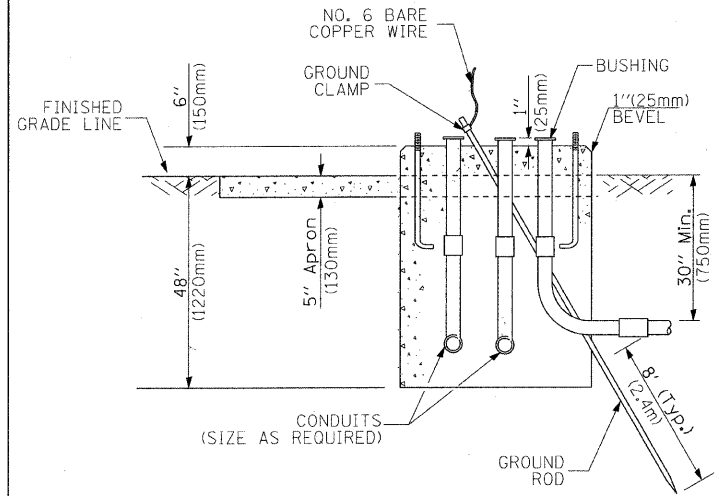
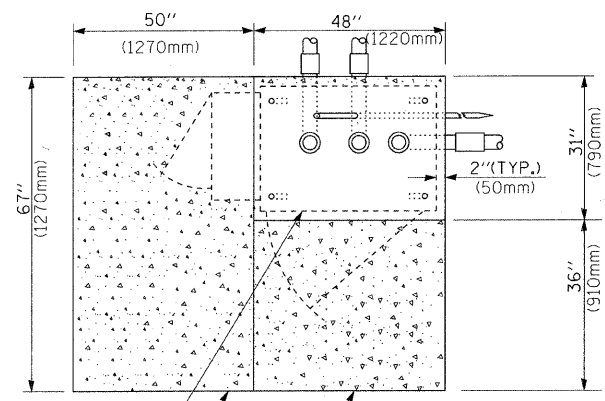


A	B	C	HEIGHT	WEIGHT
VARIES	9.5\"(241mm)	19\"(483mm)	7\"(178mm) - 12\"(300mm)	53 lbs (24kg)
VARIES	10.75\"(273mm)	21.5\"(546mm)	7\"(178mm) - 12\"(300mm)	68 lbs (31 kg)
VARIES	13.0\"(330mm)	26\"(660mm)	7\"(178mm) - 12\"(300mm)	81 lbs (37 kg)
VARIES	18.5\"(470mm)	37\"(940mm)	7\"(178mm) - 12\"(300mm)	126 lbs (57 kg)

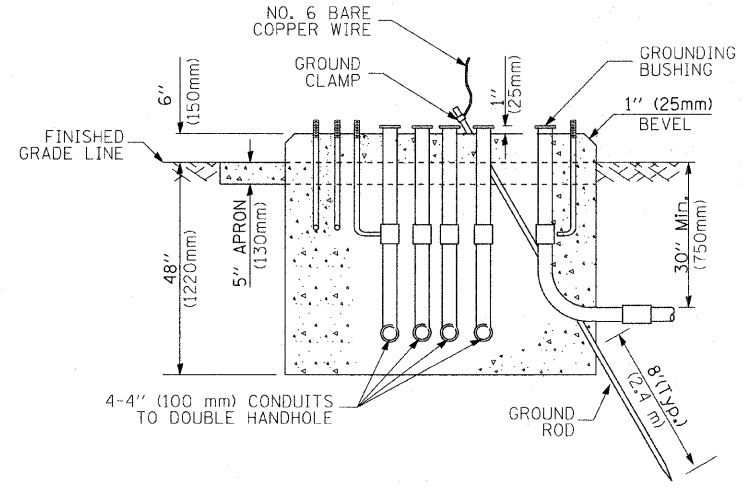
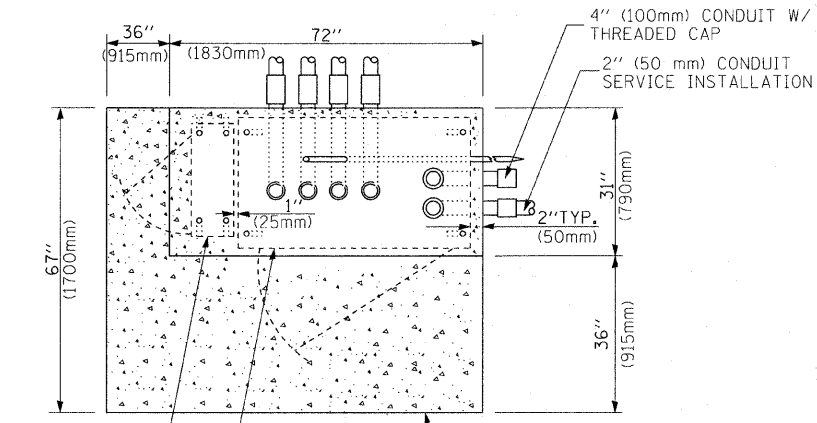
- NOTES:**
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
 - THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
 - THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



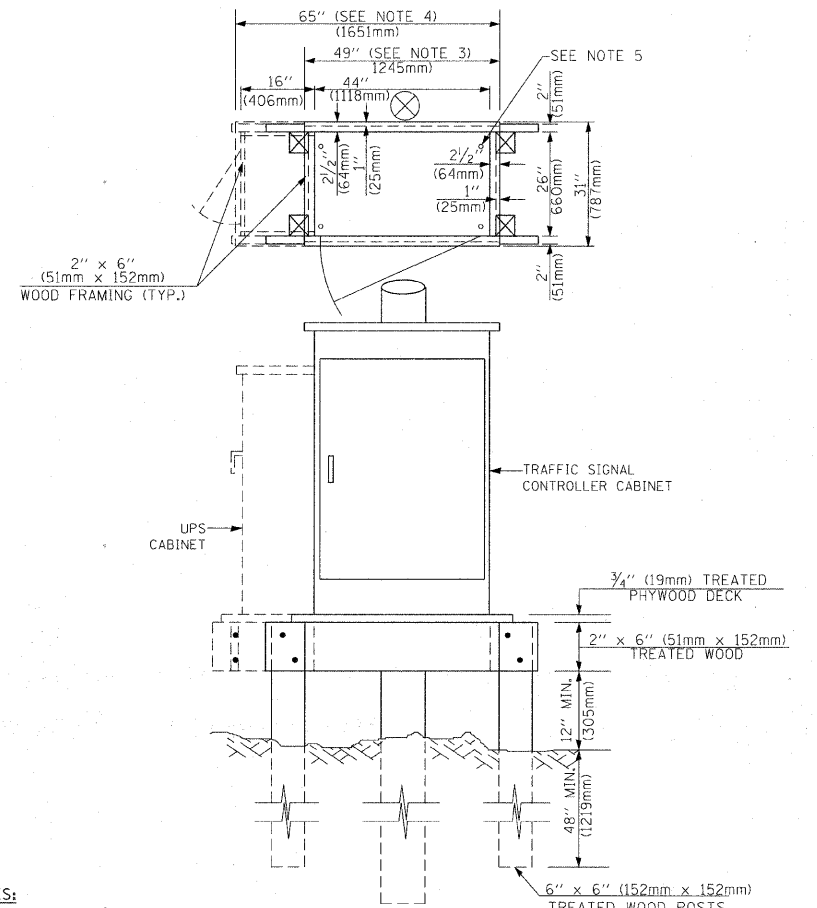
- NOTES:**
- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
 - REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.



**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



**TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



- NOTES:**
1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
 4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
 5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
 6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 56' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:**
1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
 2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
 3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
 4. For mast arm assemblies with dual arms refer to state standard 878001.

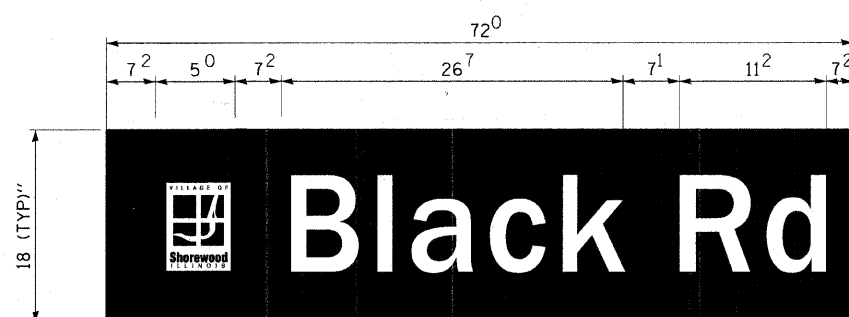
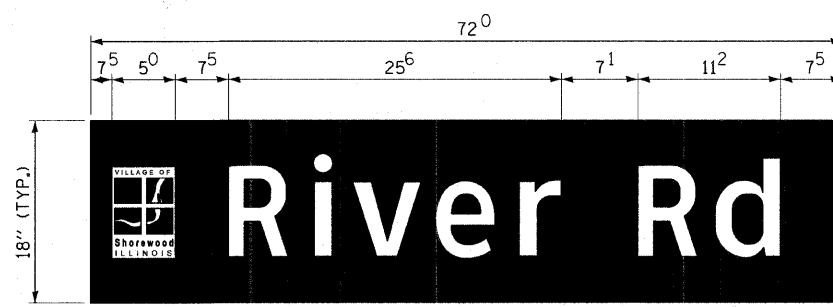
DEPTH OF MAST ARM FOUNDATIONS, TYPE E

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S	S	STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I	IP	ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM	R			STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	RL			SIGNAL POST AND FOUNDATION TO BE REMOVED			
GUY WIRE				ABANDON ITEM	A			INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				EXISTING INTERSECTION LOOP DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD				EXISTING PREFORMED INTERSECTION LOOP DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				PREFORMED SAMPLING (SYSTEM) DETECTOR			
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER							
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT							
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER							
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED							
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)							
MICROWAVE VEHICLE SENSOR											
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											

RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET		
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		



8" CLEARVIEW HIGHWAY FONT
 8" LOGO IN GREEN EC FILM OVER 3M DG CUBED REFLECTIVE FACE

- NOTES:
- THIS SHEET FOR INFORMATION ONLY. TO BE USED WITH INTERNALLY ILLUMINATED STREET NAME SIGNS MANUFACTURED BY TRAFFIC SIGNS, INC., OR APPROVED EQUAL.
 - SIGNS SHALL BE DUAL SIDED. FRONT AND BACK OF SIGN WILL BE THE SAME.

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 184-00023 - EXPIRES 4/30/2011
 2-2-2010
 ...\\plot1\river\river\PH2\22X34.sh
 ...\\PLOT5\090491-PH2.TIF
 C:\C:\PLOT5\090491-PH2.TIF



DESIGNED - DSH	REVISED -
DRAWN - CJC	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-SIGN.sh+

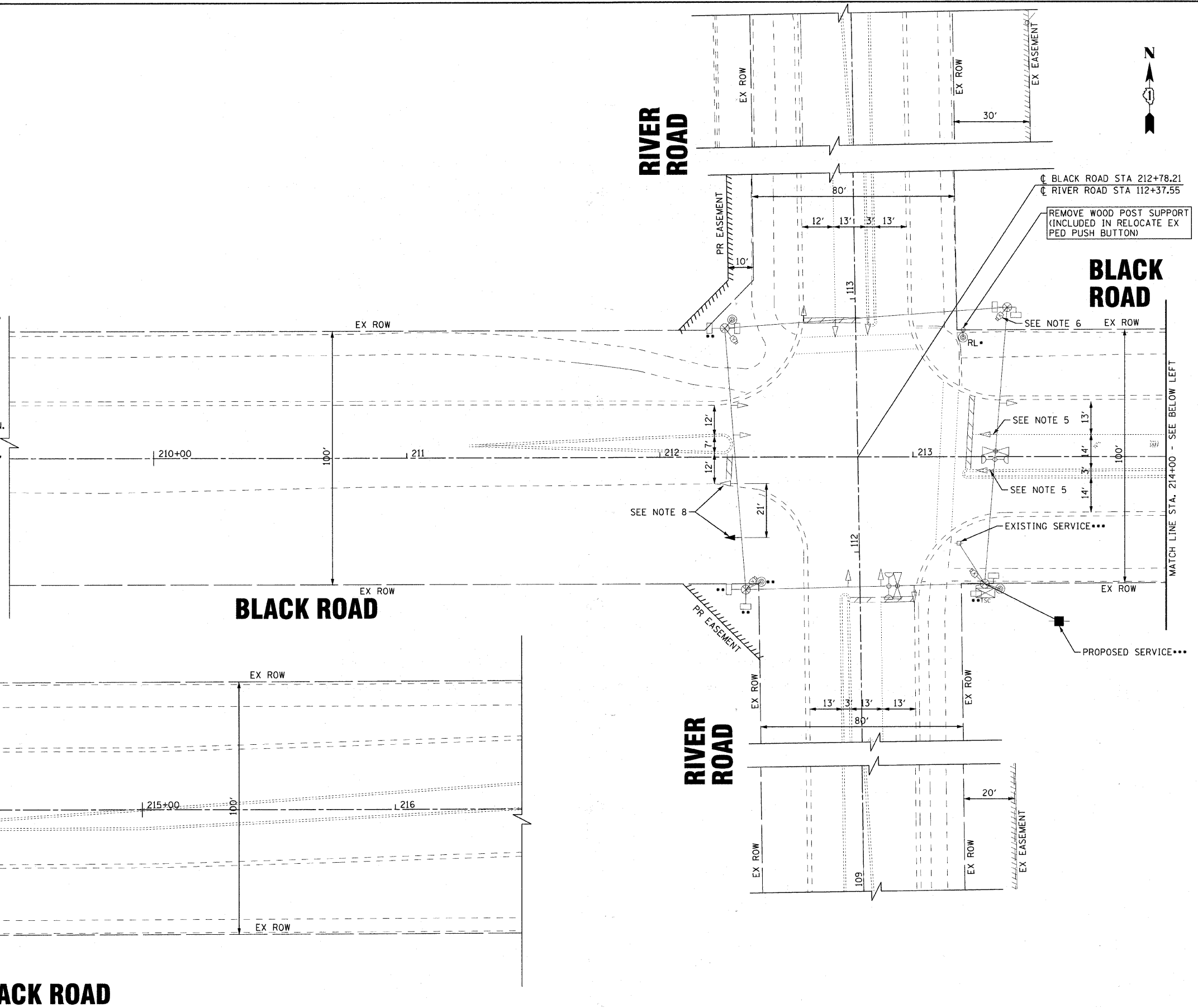
**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

LED INTERNALLY ILLUMINATED STREET NAME SIGN
 SCALE: NONE STA. TO STA.

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 37
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

TEMPORARY TRAFFIC SIGNAL NOTES

1. THE EXISTING SIGNAL IS A TEMPORARY SIGNAL TO BE MAINTAINED THROUGHOUT CONSTRUCTION.
2. THE EXISTING PEDESTRIAN PUSH-BUTTON ON THE NORTHEAST CORNER WILL NEED TO BE RELOCATED FROM THE EXISTING 4X4 WOODEN POST TO THE EXISTING WOODEN POLE OF THE TEMPORARY SIGNAL, INCLUDING REMOVING ALL CABLE FROM THE CONDUIT AND, IF NECESSARY, RE-RUNNING THE CABLE TO THE CONTROLLER VIA THE SPAN WIRE. THIS WORK WILL BE PAID FOR AS RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON.
3. THE EXISTING PEDESTRIAN SIGNAL HEADS FOR CROSSING THE WEST AND SOUTH LEGS WILL NEED TO BE ROTATED TO FACE THE CORRECT DIRECTION, CONNECTED AND ACTIVATED ONCE THE INTERSECTION IS OPENED UP TO PEDESTRIAN AND BICYCLE ACCESS ACROSS THESE LEGS. THE PUSH-BUTTON ON THE SOUTHWEST CORNER WILL ALSO NEED TO BE CONNECTED AND ACTIVATED. THIS WORK WILL BE INCLUDED IN THE COST OF MAINTENANCE OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.
4. THE EXISTING SIGNAL SERVICE IS LOCATED ON A POWER POLE TO BE RELOCATED. COORDINATION WILL BE REQUIRED WITH COM ED FOR PROPOSED LOCATION OF NEW SERVICE. SERVICE SHUTDOWN SHALL BE MINIMIZED AS MUCH AS PRACTICAL. THIS WORK WILL BE PAID FOR AS MODIFY EXISTING SERVICE INSTALLATION.
5. THE EXISTING TURN ARROWS ON THE EASTBOUND SIGNAL HEADS SHALL BE UNCOVERED, ACTIVATED, AND AN ACTUATED EASTBOUND LEFT TURN ARROW SHALL BE ADDED TO THE PHASE SEQUENCE WHEN THE WEST LEG IS FULLY OPENED TO TRAFFIC (SEE CONSTRUCTION STAGING) WITH A LEFT TURN LANE. THIS WORK SHALL BE INCLUDED IN THE COST OF MAINTENANCE OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.
6. VIDEO DETECTION ZONES FOR THE WEST LEG SHALL BE ADJUSTED FOR DIFFERENT TRAFFIC STAGES AND SHALL INCLUDE ADDING A LEFT TURN DETECTION ZONE ONCE AN EASTBOUND LEFT TURN LANE IS OPENED. EXISTING DETECTION ZONES ON THE OTHER THREE LEGS SHALL BE MAINTAINED; HOWEVER, DETECTION ZONES SHALL BE ADJUSTED WHEN ALL NEW LANES ARE OPENED TO TRAFFIC. THIS WORK SHALL BE INCLUDED IN THE COST OF MAINTENANCE OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.
7. UPON REMOVAL OF THE TEMPORARY SIGNAL, ALL TRAFFIC SIGNAL EQUIPMENT SHALL REMAIN THE PROPERTY OF THE VILLAGE OF SHOREWOOD, INCLUDING SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS, PEDESTRIAN PUSH-BUTTONS, VIDEO DETECTION EQUIPMENT, EMERGENCY VEHICLE PREEMPTION EQUIPMENT, WOOD POLES, SPAN WIRE, TETHER WIRE ALL CABLES AND THE CONTROLLER WITH CABINET. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK-UP OF ALL EQUIPMENT TO BE RETURNED TO THE VILLAGE. THIS WORK WILL BE PAID FOR AS REMOVE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
8. RELOCATE EB NEAR RIGHT SIGNAL HEAD TO EDGE OF PROPOSED RIGHT TURN LANE JUST PRIOR TO OPENING NEW EB LANES. INCLUDED IN THE COST OF MAINTENANCE OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.



COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - BR-00021 - EXPIRES 4/30/2011
 605044 PA
 2/27/2011
 C:\projects\090028\090028-00-CH\090028-00-CH-TS-PLAN.dwg
 090028-00-CH-TS-PLAN.dwg



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-TEMP-TS-PLAN.dwg

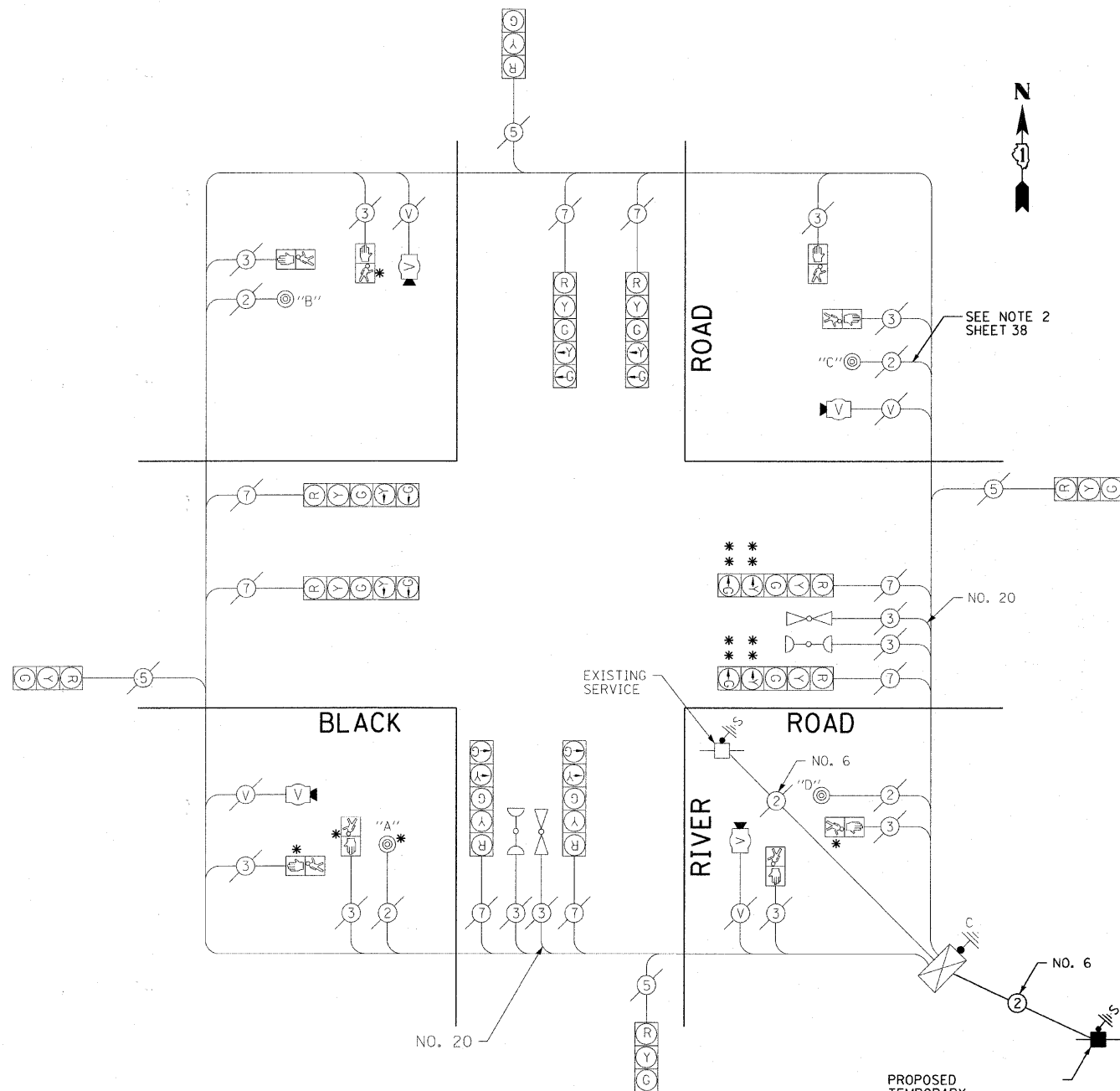
**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN
BLACK ROAD AND RIVER ROAD**

SCALE: 1"=20'

STA. TO STA.

F.A.U. R.T.E. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 38
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				



* SEE NOTE 3 SHEET 38
 ** SEE NOTE 5 SHEET 38

TEMPORARY CABLE PLAN

PEDESTRIAN PUSH BUTTON NOTES:
 PUSH BUTTON "A" SHALL PLACE A CALL IN PHASES 2 AND 4.
 PUSH BUTTON "B" SHALL PLACE A CALL IN PHASES 4 AND 6.
 PUSH BUTTON "C" SHALL PLACE A CALL IN PHASES 6 AND 8.
 PUSH BUTTON "D" SHALL PLACE A CALL IN PHASES 2 AND 8.

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	%OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12	135		0.50	810.00
(YELLOW)	12	135		0.25	405.00
(GREEN)	12	135		0.25	405.00
ARROW	16	135		0.10	216
PED. SIGNAL	8	90		1.00	720.00
CONTROLLER	1	100		1.00	100.00
ILLUM. SIGN	-	-	-	0.05	-
VIDEO DET	1	150		1.00	150.00
FLASHER				0.50	-
				TOTAL =	2806

ENERGY COSTS TO:
 VILLAGE OF SHOREWOOD
 1 TOWNE CENTER BOULEVARD
 SHOREWOOD, ILLINOIS 60404

ENERGY SUPPLY CONTACT: JIM CLOVER
 PHONE: (815) 724-5054
 COMPANY: COM. ED.

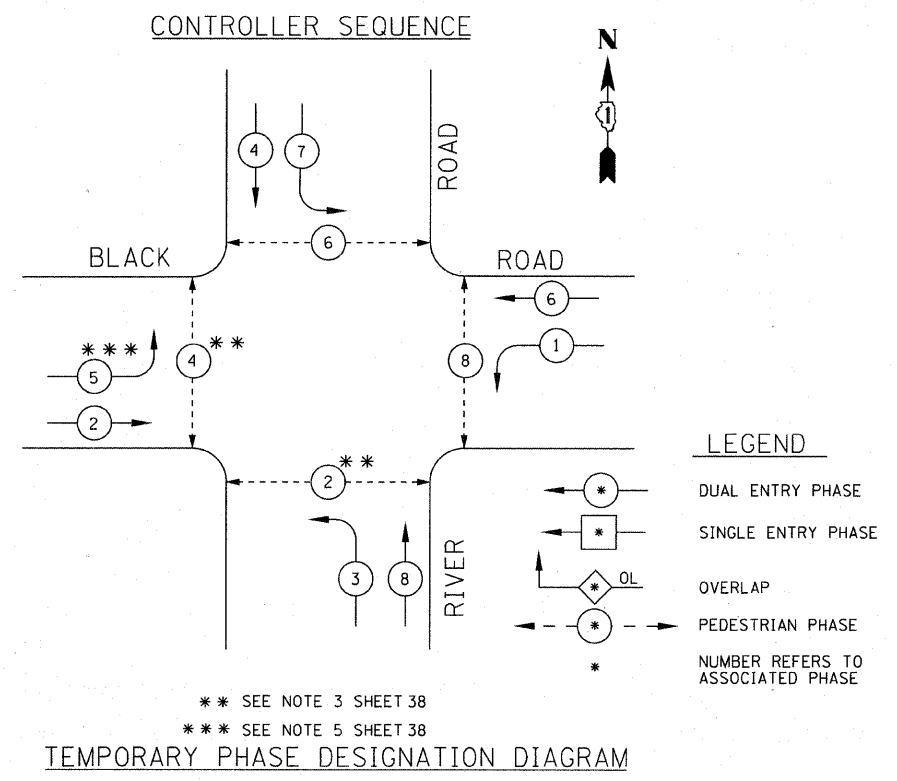


DESIGNED - DSH	REVISED -
DRAWN - MAC	REVISED -
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-ts-temp-cable.sht

**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

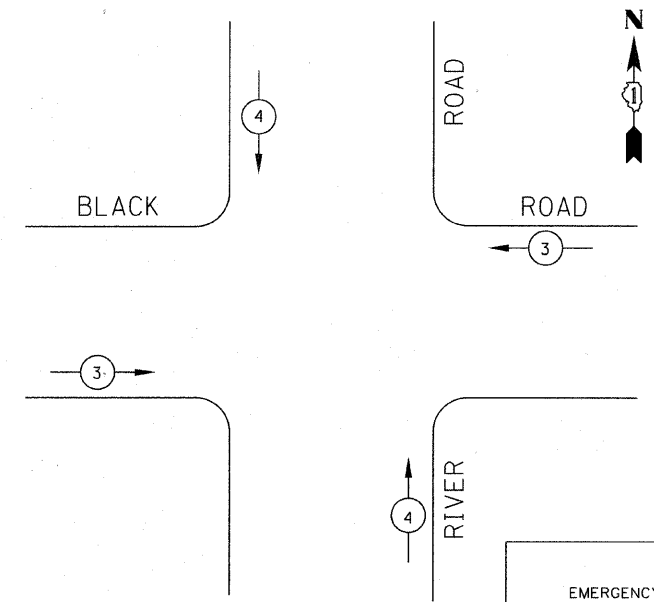
**TEMPORARY CABLE PLAN
 AND TEMPORARY PHASE DESIGNATION DIAGRAM
 BLACK ROAD AND RIVER ROAD**

SCALE: NONE STA. TO STA.



** SEE NOTE 3 SHEET 38
 *** SEE NOTE 5 SHEET 38

TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTION		
EMERGENCY VEHICLE PREEMPTION	3	4
MOVEMENT	← →	↑ ↓

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 384-000211 - EXPIRES 4/30/2011
 PROJECT NO. 090491-PH2-ts-temp-cable.sht
 DATE: 12/2/10

F.A.U. RTE. 0238 0356	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 39
C-91-726-09		CONTRACT NO. 63562		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT W-9003(385)

SCHEDULE OF QUANTITIES

PAY ITEM	UNIT	QUANTITY
PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	35
CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	37
CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	35
CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	20
CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	435
HANDHOLE	EACH	3
DOUBLE HANDHOLE	EACH	1
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 10	FOOT	854
TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	108
LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO-CELL CONTROL, 250 WATT	EACH	4
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1262
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2344
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1471
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1519
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 20 3C	FOOT	300
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	40
ELECTRIC CABLE IN CONDUIT, EQUIPMENT, GROUNDING CONDUCTOR, NO. 6 1C	FOOT	505
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 40 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 44 FT.	EACH	3
CONCRETE FOUNDATION, TYPE A	FOOT	16
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	52
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	4
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8
TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	8
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	8
RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	EACH	1
REMOVE TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
VIDEO VEHICLE DETECTION SYSTEM	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1
MODIFY EXISTING SERVICE INSTALLATION	EACH	1
PAINT NEW COMBINATION MAST ARM AND POLE, OVER 40 FT	EACH	4
PAINT NEW SIGNAL POST	EACH	4
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
UNINTERRUPTIBLE POWER SUPPLY	EACH	1
MAINTENANCE OF EXISTING TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
LED INTERNALLY ILLUMINATED STREET NAME SIGN	EACH	4

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	%OPERATION	
SIGNAL (RED)	16	17		0.50	136.00
(YELLOW)	16	25		0.25	100.00
(GREEN)	16	15		0.25	60.00
ARROW	16	12		0.10	19.20
PED. SIGNAL	8		25	1.00	200.00
CONTROLLER	1	100		1.00	100.00
ILLUM. SIGN	4		90	0.50	180.00
VIDEO DET.	1		150	1.00	150.00
LUMINAIRE	4	250		0.50	500.00
FLASHER				0.50	-
TOTAL =					1445.20

ENERGY COSTS TO:
VILLAGE OF SHOREWOOD
1 TOWN CENTER BOULEVARD
SHOREWOOD, ILLINOIS 60404

ENERGY SUPPLY CONTACT: JIM GLOVER
PHONE: (815) 724-5054
COMPANY: COM. ED.

	DESIGNED - DSH	REVISED -
	DRAWN - CJC	REVISED -
	CHECKED - LDH	REVISED -
	DATE - 12/6/10	FILE - 090491-PH2-ts-cable.sht

VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT

SCHEDULE OF QUANTITIES, CABLE PLAN AND
PHASE DESIGNATION DIAGRAM
BLACK ROAD AND RIVER ROAD

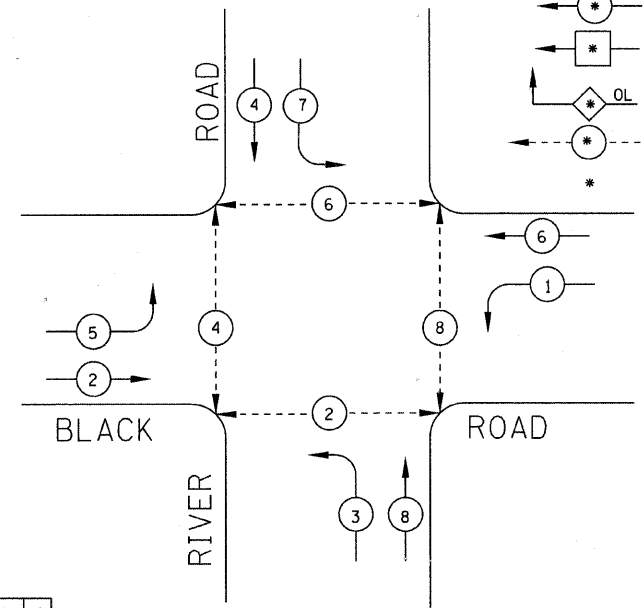
SCALE: NONE STA. TO STA.

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 41
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003385				

LEGEND

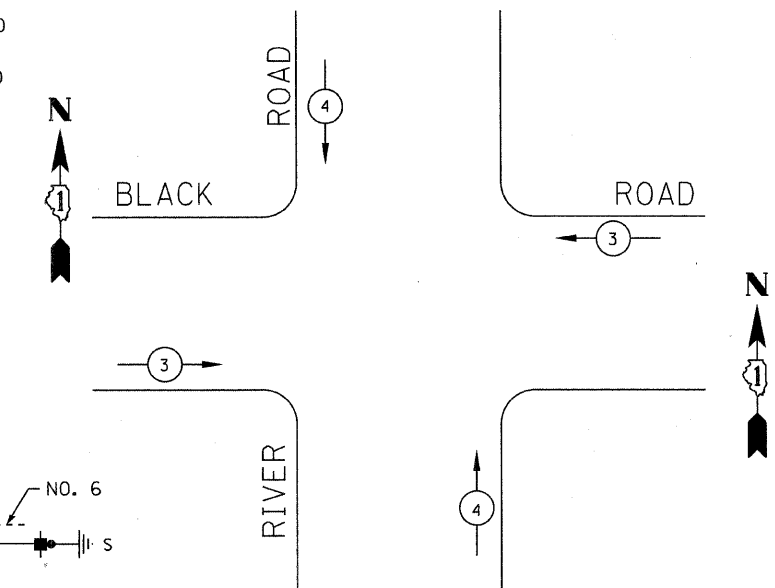
- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

PROPOSED CONTROLLER SEQUENCE



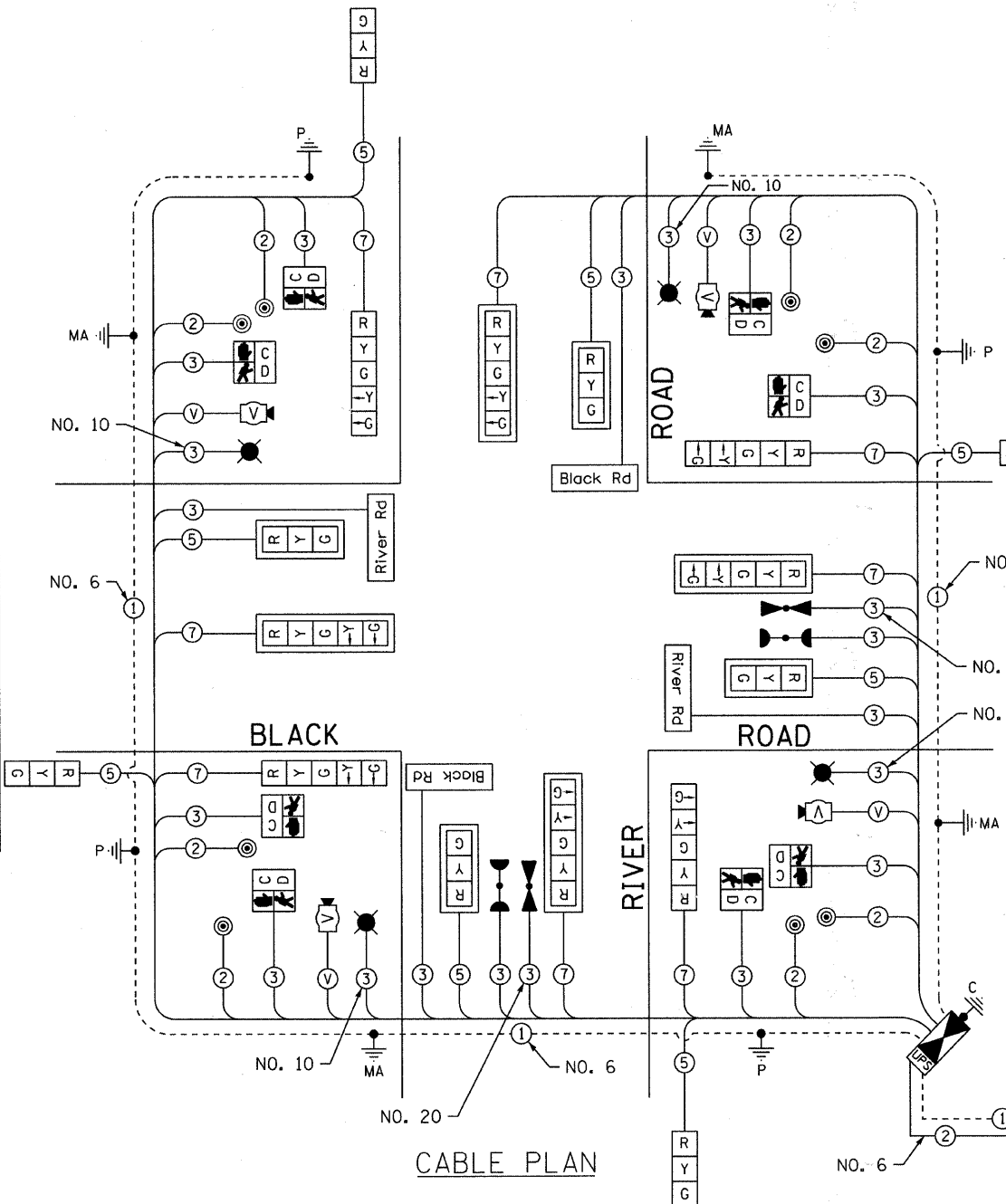
PHASE DESIGNATION DIAGRAM

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



- NOTES:
- THE PRIORITY CONTROL SYSTEM EQUIPMENT SHALL MATCH THE EXISTING EQUIPMENT TYPE ON ADJACENT SIGNAL SYSTEMS.

PROPOSED EMERGENCY VEHICLE PREEMPTION		
EMERGENCY VEHICLE PREEMPTION	3	4
MOVEMENT		



CABLE PLAN

CABLE PLAN LEGEND

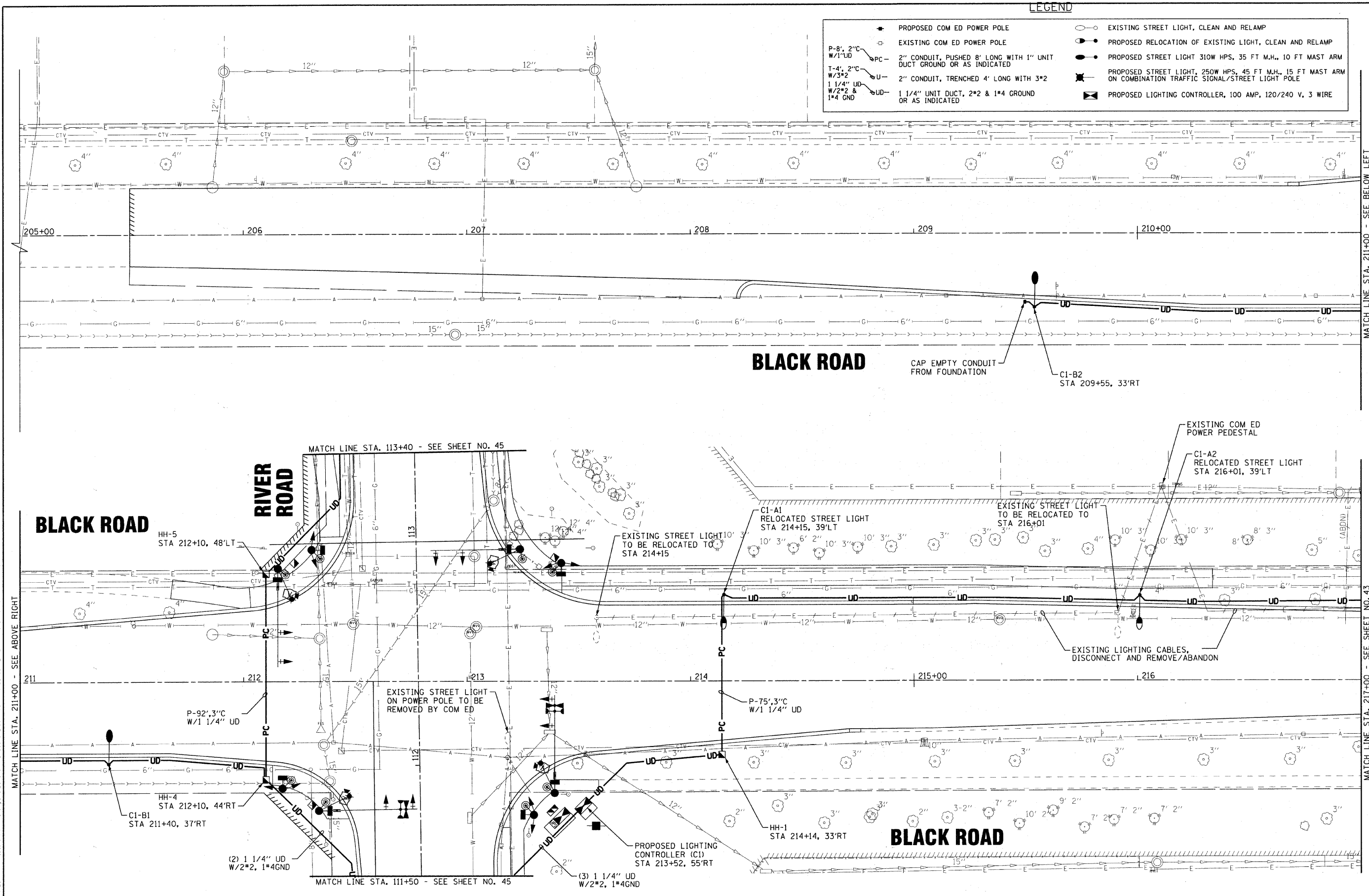
(NOTE: SEE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS SHEET FOR FULL LEGEND)

Black Rd LED INTERNALLY ILLUMINATED STREET NAME SIGNS

COPYRIGHT © 2008 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 034-000001-EXPIRES 12/31/2011
 PROJECT NO. 090491-PH2-ts-cable.sht

LEGEND

- PROPOSED COM ED POWER POLE
- EXISTING COM ED POWER POLE
- EXISTING STREET LIGHT, CLEAN AND RELAMP
- PROPOSED RELOCATION OF EXISTING LIGHT, CLEAN AND RELAMP
- PROPOSED STREET LIGHT 310W HPS, 35 FT M.H., 10 FT MAST ARM
- PROPOSED STREET LIGHT, 250W HPS, 45 FT M.H., 15 FT MAST ARM ON COMBINATION TRAFFIC SIGNAL/STREET LIGHT POLE
- PROPOSED LIGHTING CONTROLLER, 100 AMP, 120/240 V, 3 WIRE



COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 184-00021 - EXPIRES 4/30/2011
 PROJECT NO. - 090491-PH2-SL1-1
 DRAWN BY - MWH
 CHECKED BY - MDK
 DATE - 12/6/10



DESIGNED - MWH	REVISED -
DRAWN - CJC	REVISED -
CHECKED - MDK	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-SL1.shx

VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT

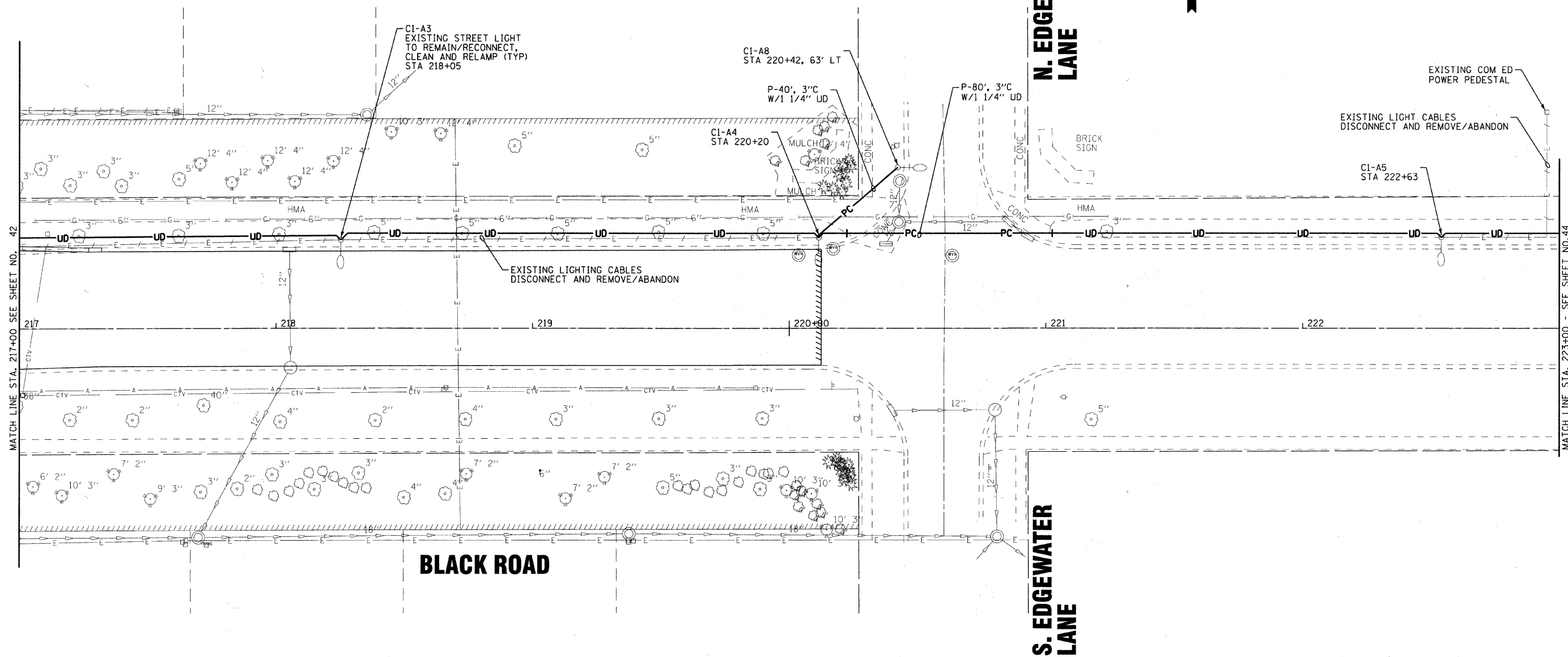
LIGHTING PLAN
BLACK ROAD

SCALE: 1"=20' STA. 205+00 TO STA. 217+00

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 42
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT W-9003(385)				

LEGEND

<ul style="list-style-type: none"> PROPOSED COM ED POWER POLE EXISTING COM ED POWER POLE 2" CONDUIT, PUSHED 8' LONG WITH 1" UNIT DUCT GROUND OR AS INDICATED 2" CONDUIT, TRENCHED 4' LONG WITH 3" 2 1 1/4" UNIT DUCT, 2*2 & 1*4 GND OR AS INDICATED 	<ul style="list-style-type: none"> EXISTING STREET LIGHT, CLEAN AND RELAMP PROPOSED RELOCATION OF EXISTING LIGHT, CLEAN AND RELAMP PROPOSED STREET LIGHT 310W HPS, 35 FT M.H., 10 FT MAST ARM PROPOSED STREET LIGHT, 250W HPS, 45 FT M.H., 15 FT MAST ARM ON COMBINATION TRAFFIC SIGNAL/STREET LIGHT POLE PROPOSED LIGHTING CONTROLLER, 100 AMP, 120/240 V, 3 WIRE
---	--



COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 186-000217 - EXPIRES 4/30/2011
 230 S. LAKE STREET, SUITE 200, CHICAGO, IL 60601
 PROJECT NO. 090491-PH2-SL2.sht

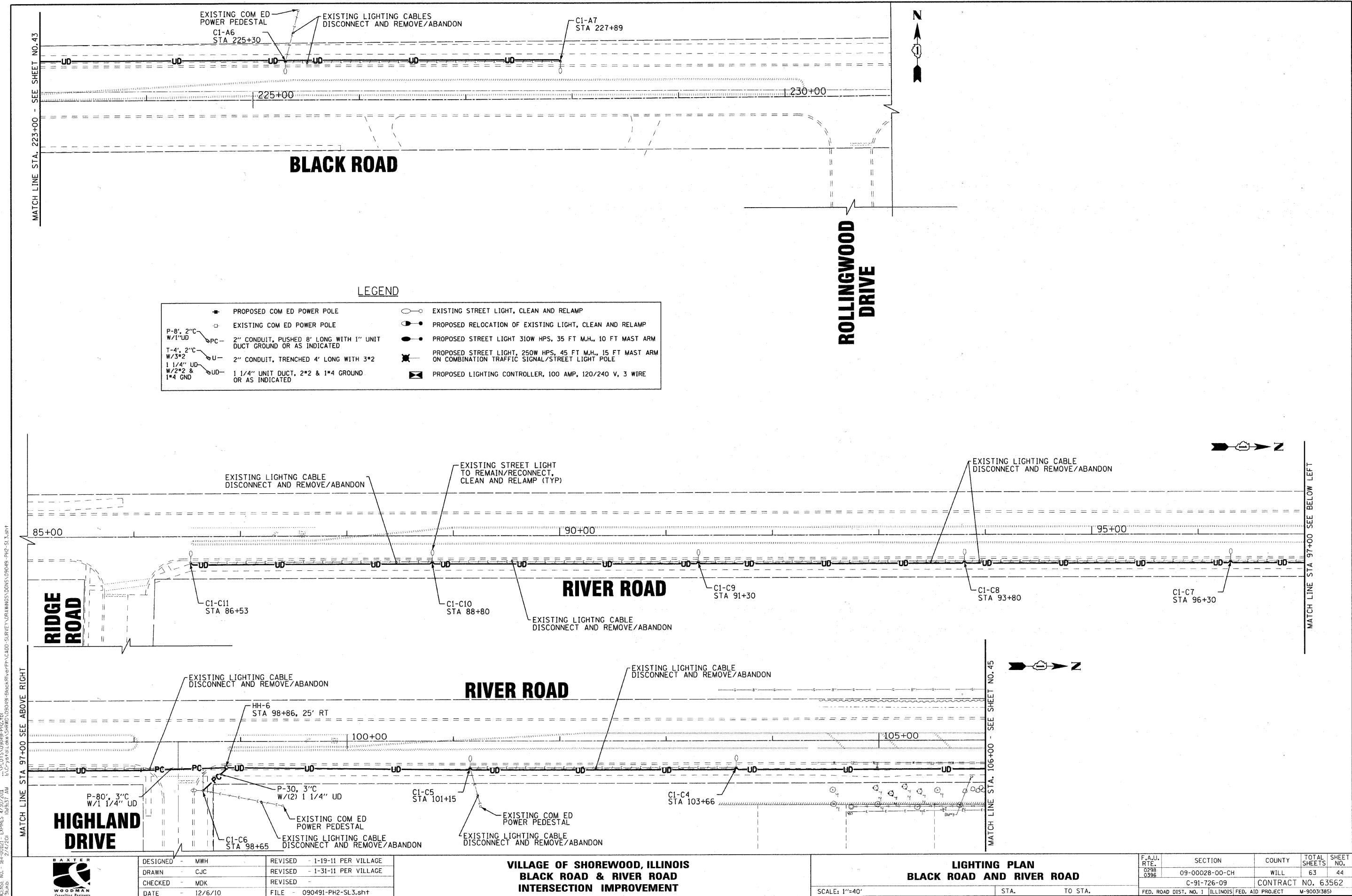


DESIGNED - MWH	REVISED - 1-31-11 PER VILLAGE
DRAWN - CJC	REVISED -
CHECKED - MDK	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-SL2.sht

VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT

LIGHTING PLAN
BLACK ROAD
 SCALE: 1"=20'
 STA. 217+00 TO STA. 223+00

F.A.U. R.T.E. 0298 0386	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 43
C-91-726-09		CONTRACT NO. 63562		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(385)



LEGEND

	PROPOSED COM ED POWER POLE		EXISTING STREET LIGHT, CLEAN AND RELAMP
	EXISTING COM ED POWER POLE		PROPOSED RELOCATION OF EXISTING LIGHT, CLEAN AND RELAMP
	2" CONDUIT, PUSHED 8' LONG WITH 1" UNIT DUCT GROUND OR AS INDICATED		PROPOSED STREET LIGHT 310W HPS, 35 FT M.H., 10 FT MAST ARM ON COMBINATION TRAFFIC SIGNAL/STREET LIGHT POLE
	2" CONDUIT, TRENCHED 4' LONG WITH 3*2		PROPOSED LIGHTING CONTROLLER, 100 AMP, 120/240 V, 3 WIRE
	1 1/4" UNIT DUCT, 2*2 & 1*4 GND OR AS INDICATED		

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 081-000211 - EXPIRES 4/30/2011
 22457 2/09



DESIGNED - MWH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED - 1-31-11 PER VILLAGE
CHECKED - MDK	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-SL3.shx

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT**

SCALE: 1"=40'

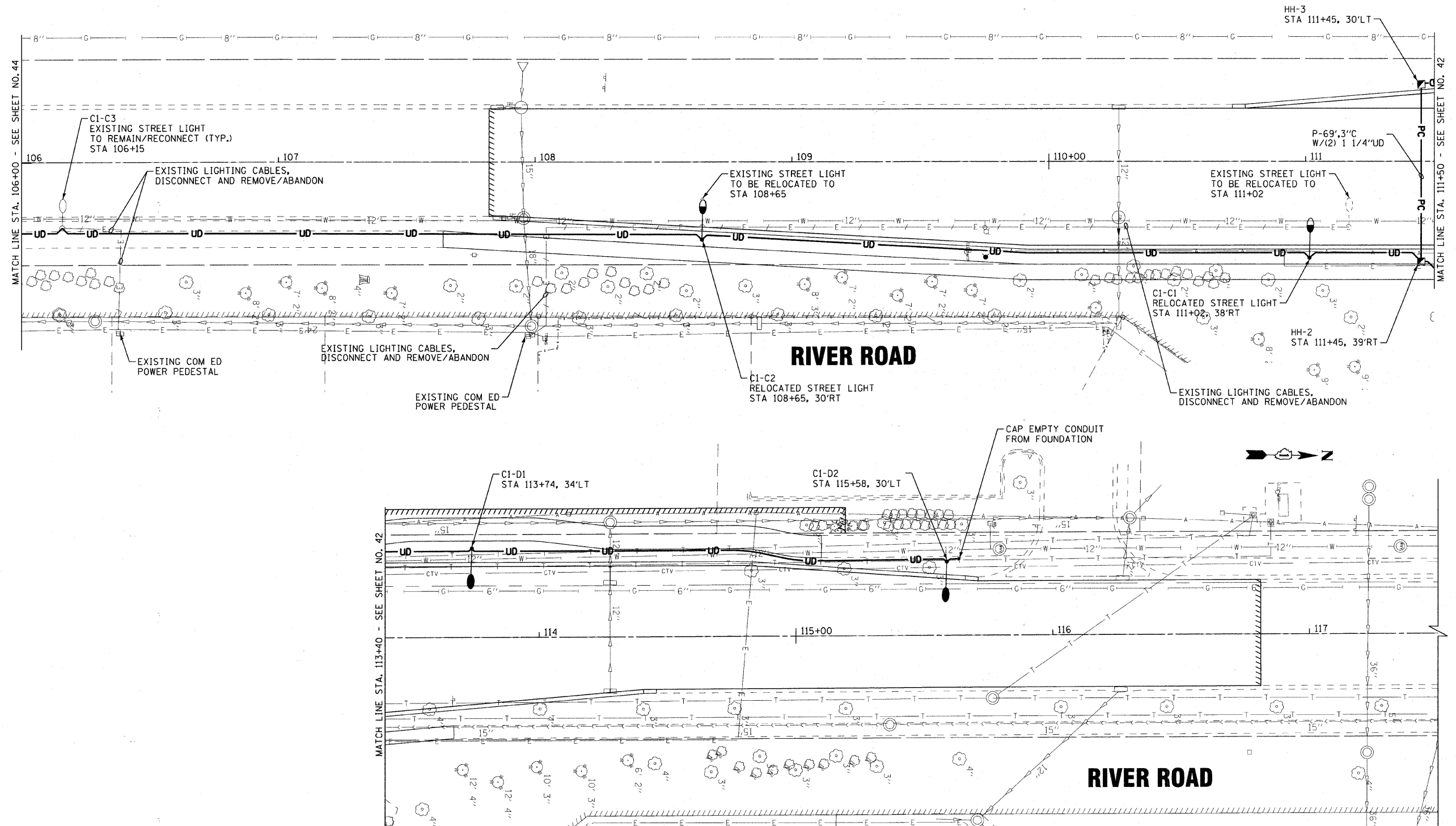
**LIGHTING PLAN
BLACK ROAD AND RIVER ROAD**

STA. TO STA.

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 44
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-900313851				

LEGEND

	PROPOSED COM ED POWER POLE		EXISTING STREET LIGHT, CLEAN AND RELAMP
	EXISTING COM ED POWER POLE		PROPOSED RELOCATION OF EXISTING LIGHT, CLEAN AND RELAMP
	PROPOSED STREET LIGHT 310W HPS, 35 FT M.H., 10 FT MAST ARM		PROPOSED STREET LIGHT, 250W HPS, 45 FT M.H., 15 FT MAST ARM ON COMBINATION TRAFFIC SIGNAL/STREET LIGHT POLE
	PROPOSED LIGHTING CONTROLLER, 100 AMP, 120/240 V, 3 WIRE		



COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 384-00212, EXPIRES 4/30/2011
 330 S. WASHINGTON ST., SUITE 200, CHICAGO, IL 60604
 TEL: 312.467.2000, FAX: 312.467.2001, WWW.B&W.COM



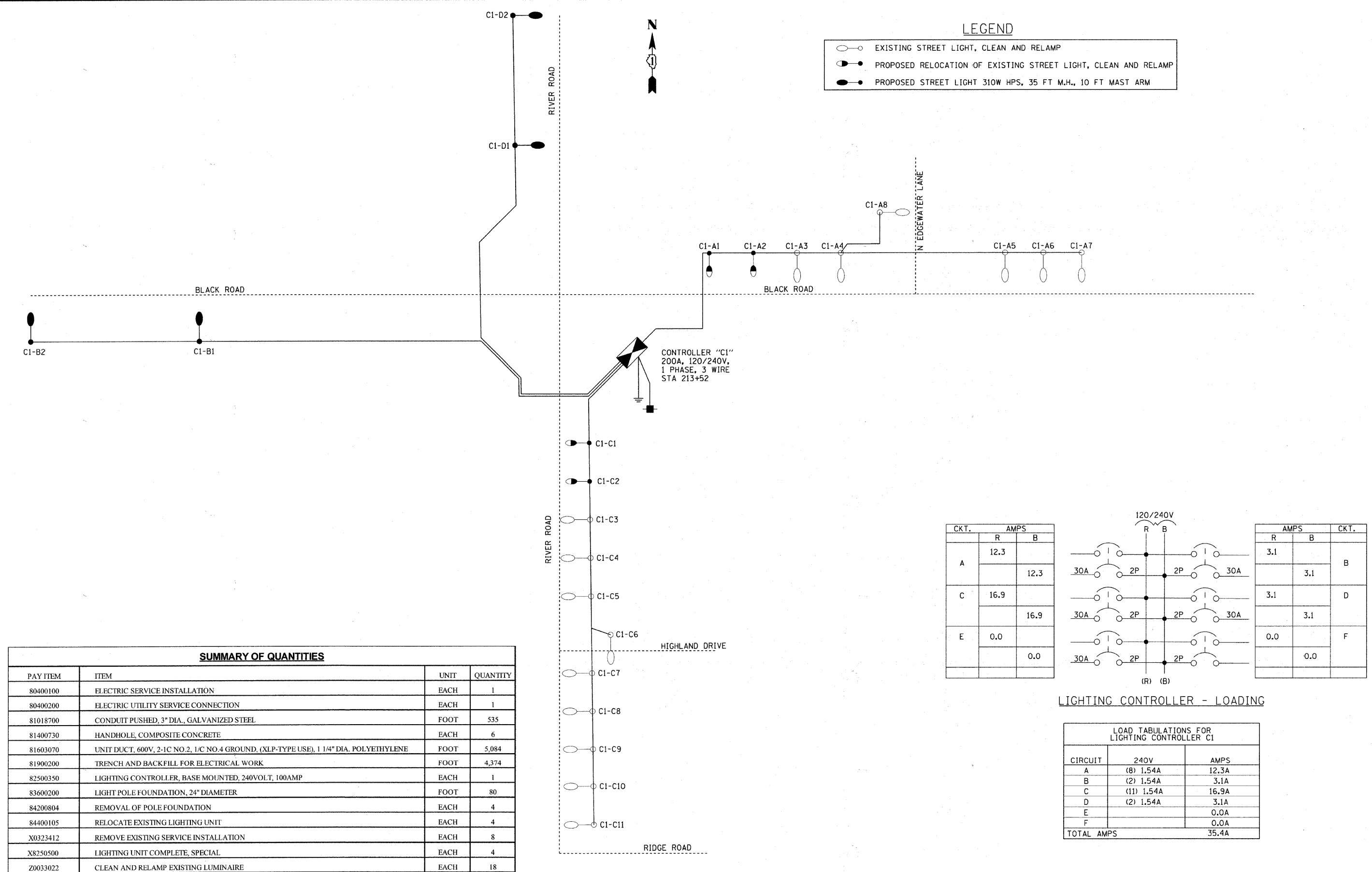
DESIGNED - MWH	REVISED - 1-31-11 PER VILLAGE
DRAWN - CJC	REVISED -
CHECKED - MDK	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-SL4.shx

VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT

LIGHTING PLAN
 RIVER ROAD
 SCALE: 1"=20'
 STA. 106+00 TO STA. 117+50

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 45
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC. ...
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM ...
 LICENSE NO. - BR-00021 - EXPIRES 4/30/2011 ...
 2/2/2011 10:24:53 AM ...



LEGEND

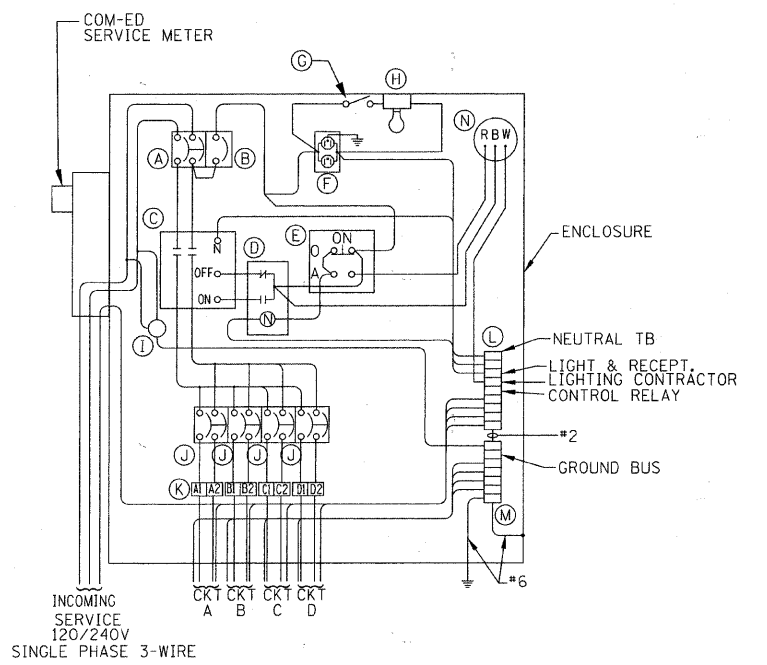
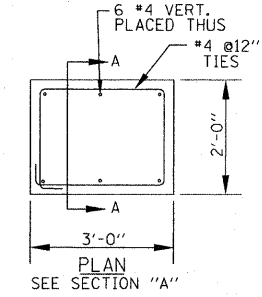
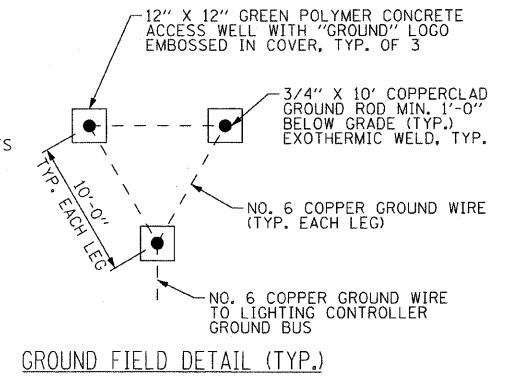
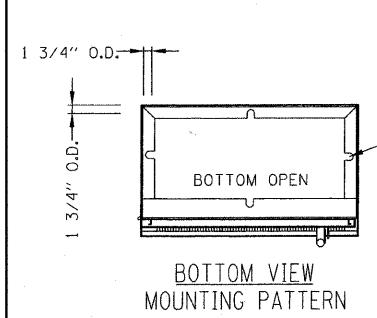
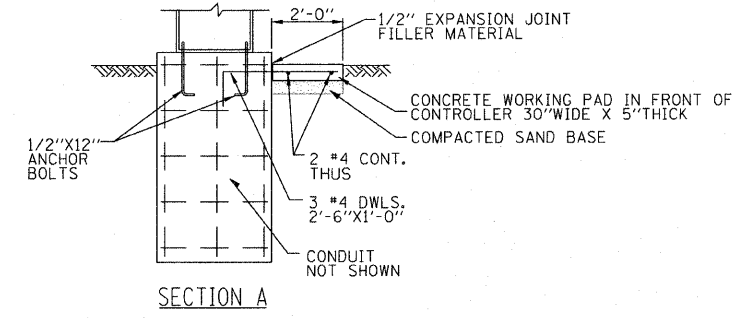
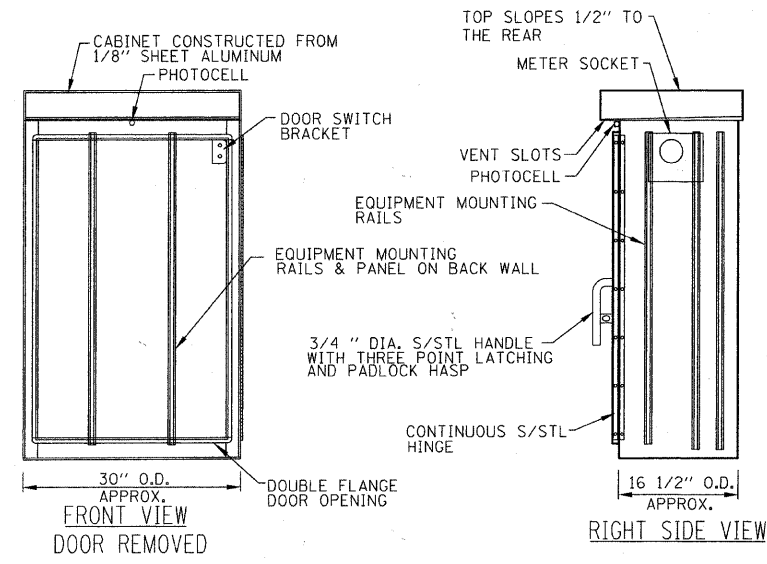
- EXISTING STREET LIGHT, CLEAN AND RELAMP
- PROPOSED RELOCATION OF EXISTING STREET LIGHT, CLEAN AND RELAMP
- PROPOSED STREET LIGHT 310W HPS, 35 FT M.H., 10 FT MAST ARM

SUMMARY OF QUANTITIES			
PAY ITEM	ITEM	UNIT	QUANTITY
80400100	ELECTRIC SERVICE INSTALLATION	EACH	1
80400200	ELECTRIC UTILITY SERVICE CONNECTION	EACH	1
81018700	CONDUIT PUSHED, 3" DIA., GALVANIZED STEEL	FOOT	535
81400730	HANDHOLE, COMPOSITE CONCRETE	EACH	6
81603070	UNIT DUCT, 600V, 2-1C NO.2, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	5,084
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	4,374
82500350	LIGHTING CONTROLLER, BASE MOUNTED, 240VOLT, 100AMP	EACH	1
83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	80
84200804	REMOVAL OF POLE FOUNDATION	EACH	4
84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	4
X0323412	REMOVE EXISTING SERVICE INSTALLATION	EACH	8
X8250500	LIGHTING UNIT COMPLETE, SPECIAL	EACH	4
20033022	CLEAN AND RELAMP EXISTING LUMINAIRE	EACH	18

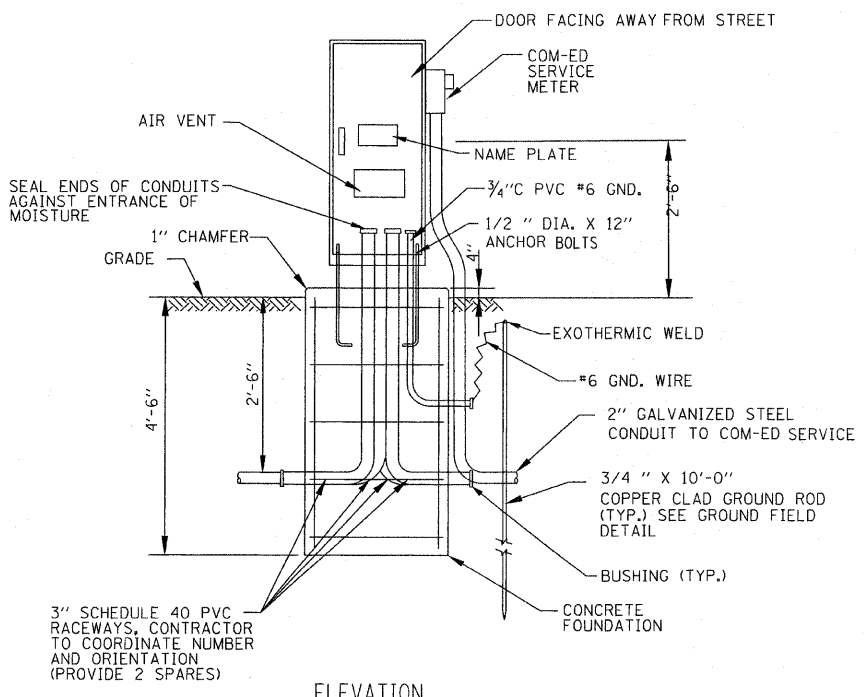
CKT.	AMPS		R	B	AMPS	CKT.
	R	B				
A	12.3				3.1	B
C	16.9				3.1	D
E	0.0				0.0	F
					0.0	

LIGHTING CONTROLLER - LOADING

LOAD TABULATIONS FOR LIGHTING CONTROLLER C1		
CIRCUIT	240V	AMPS
A	(8) 1.54A	12.3A
B	(2) 1.54A	3.1A
C	(11) 1.54A	16.9A
D	(2) 1.54A	3.1A
E		0.0A
F		0.0A
TOTAL AMPS		35.4A



LIGHTING CONTROLLER
WIRING DIAGRAM



LIGHTING CONTROLLER AND FOUNDATION DETAIL
NO SCALE

ITEM	QTY.	DESCRIPTION
A	1	MAIN CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, 2-POLE, 240 V. SINGLE-PHASE, 100A, BOLT-ON TYPE, TRIP INTERRUPTING RATING OF 22,000 RMS SYMMETRICAL AMPERES AT 240 V.
B	1	CONTROL CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, SINGLE-POLE, 120 V. SINGLE PHASE 20 A. BOLT-ON TYPE, TRIP INTERRUPTING RATING OF 14,000 RMS SYMMETRICAL AMPERES AT 120 V.
C	1	LIGHTING CONTACTOR MECHANICALLY HELD, SQUARE "D" IV010600V 200A, 2-POLE, 600 V. WITH 120 COIL OR EQUAL
D	1	120V OPERATED DPDT 60 HZ COIL, 600V, 2 N.O. & 2 N.C. CONTACTS
E	1	ON-OFF-AUTO 3-POSITION SELECTOR SWITCH GE CR104P HEAVY DUTY SWITCH, RATED FOR 10 A. AT 600 VAC.
F	1	GFCI RECEPTACLE, 120 V., 20 A. SPEC. GRADE, NEMA CONFIG. 5-20R IN WEATHERPROOF CAST ALUMINUM BOX W/ WEATHERPROOF COVER
G	1	SPDT LIMIT SWITCH WITH SIDE PUSH ROD PLUNGER RATED 20 A. AT 120 V. OR EQUAL
H	1	100 WATT LIGHT FIXTURE, VAPOR TIGHT, WITH GLOBE, GUARD AND CAST ALUMINUM MOUNTING BOX.
I	4	SECONDARY SURGE ARRESTER SQUARE D SDSA1175, 175 VAC PHASE-TO-GROUND MAXIMUM OR EQUAL
J	4	BRANCH CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, 2-POLE, 240 V. SINGLE-PHASE, 30A, TRIP INTERRUPTING RATING 22,000 RMS SYMMETRICAL AMPERES AT 240 V.
K	8	TERMINAL BLOCK RATED 600 V., 85 A.
L	1	COPPER NEUTRAL BUS
M	1	COPPER GROUND BUS
N	1	PHOTOCELL - 120V, BUTTON STYLE, DELAY TYPE, SPST

NOTES

- CABINET SHALL BE FABRICATED FROM 0.125-INCH SHEET ALUMINUM, ALLOY 5052 FORMED AND ARC WELDED ASSEMBLY WITH NEMA 3R RATING. MANUFACTURED BY SOUTHERN MFG. (DIV. OF ACCORD INDUSTRIES) AND AS COMPACT AS POSSIBLE.
- ALL SCREWS AND HARDWARE SHALL BE PLATED, GALVANIZED, OR MADE OF BRASS, ALUMINUM OR STAINLESS STEEL.
- NAME PLATE SHALL BE STEEL W/ENGRAVED 0.75-INCH HIGH LETTERS FILLED IN BLACK, "STREET LIGHTING"
- CABINET SHALL HAVE NATURAL ALUMINUM FINISH.
- ELECTRIC UTILITY METER BOX SHALL BE MOUNTED ON THE SIDE OF CONTROL CABINET AS SHOWN ON THE PANEL LAYOUT DIAGRAM.
- THE COMPLETED CONTROLLER AND ALL ELECTRICAL COMPONENTS SHALL BE U.L. LISTED AS AN ENCLOSED INDUSTRIAL CONTROL PANEL UNDER UL508A, AND SHOULD BE SERVICE ENTRANCE RATED.
- METAL MOUNTING PANEL SHALL BE #10 GAUGE GALVANIZED SHEET STEEL FLANGED BACK 0.75-INCHES I.D. ON 4 SIDES.
- CIRCUIT BREAKERS AND CONTACTOR AND OTHER COMPONENTS SHALL BE MOUNTED ON 0.125-INCH THICK GLASTIC INSULATION BACK PANEL.
- ALL DEVICES SHALL BE FRONT REMOVABLE.
- BUS BAR SHALL HAVE 22 LUG TERMINALS SIZED TO ACCOMMODATE REQUIRED WIRE SIZES. NEUTRAL BUS SHALL BE PAINTED WHITE. GROUND BUS SHALL BE PAINTED GREEN.
- ALL LUGS SHALL BE COPPER SCREWS AND CONNECTORS, SPRING HELD.
- ALL WIRING TERMINATIONS SHALL BE RATED NOT LESS THAN 75 DEGREE CENTIGRADE.
- ALL CONTROL WIRING SHALL BE 600V MACHINE TOOL WIRE TYPE MTW.
- ALL POWER WIRING SHALL BE 600V TYPE RHH/RHW (XLP-TYPE USE).
- A LAMINATED COPY OF THE CIRCUIT SCHEMATIC DIAGRAM SHALL BE ATTACHED TO THE INSIDE OF THE CONTROLLER.
- ALL 120 VOLT SYSTEM AND ALL CONTROL WIRING SHALL BE #12 AWG STRANDED UNLESS OTHERWISE INDICATED.
- ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.
- SERVICE EQUIPMENT SHALL BE MARKED TO IDENTIFY IT AS BEING SUITABLE FOR USE AS SERVICE EQUIPMENT.
- CONNECTION OF SURGE ARRESTOR TO LINE SIDE OF MAIN CIRCUIT BREAKER SHALL NOT BE "DOUBLE LUGGED".

COPYRIGHT © 2008 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00022 - EXPIRES 4/30/2011
 12/27/2011



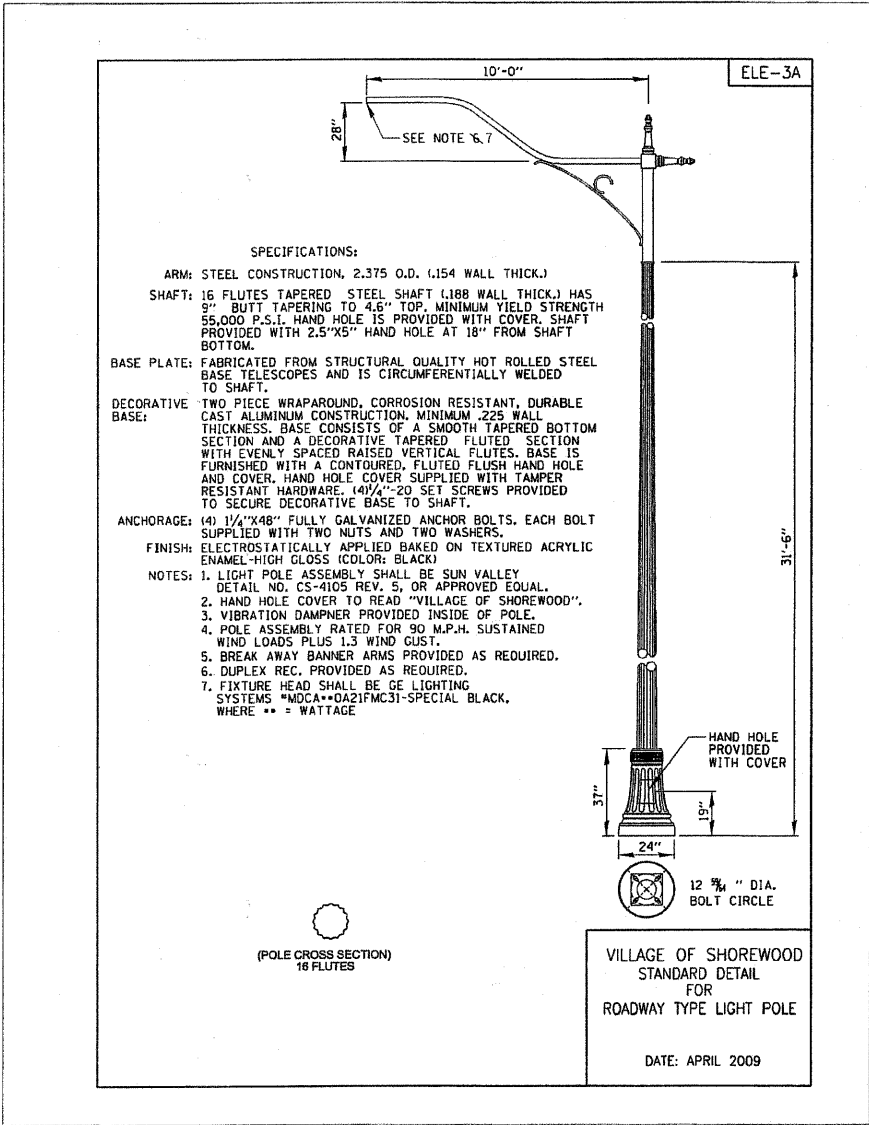
DESIGNED - MWH	REVISED -
DRAWN - CJC	REVISED -
CHECKED - MDK	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-LightngDet.sht

VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT

STREET LIGHTING DETAILS

SCALE: NONE STA. TO STA.

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 47
C-91-726-09			CONTRACT NO. 63562	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				

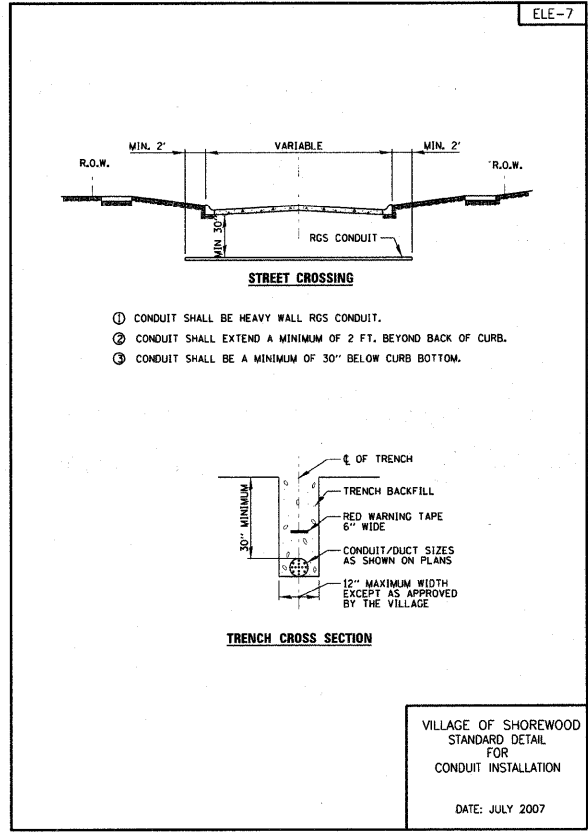
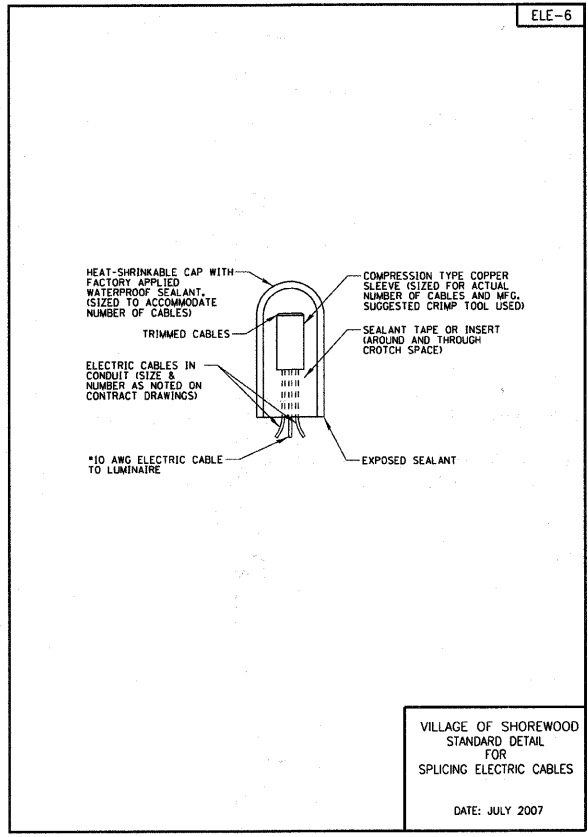
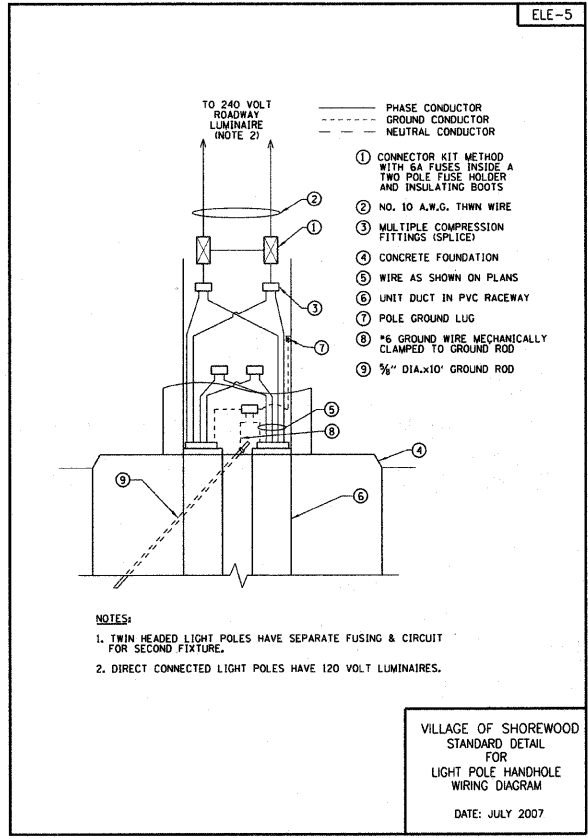
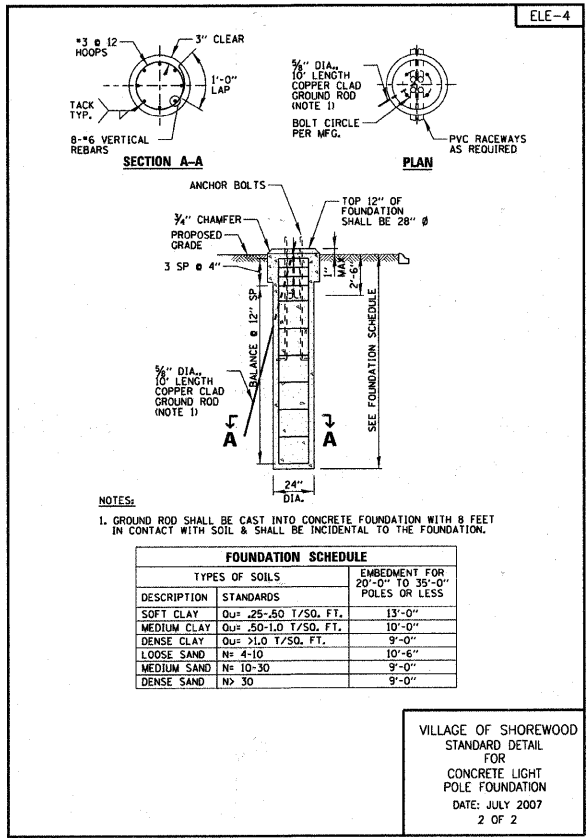


SPECIFICATIONS:

- ARM: STEEL CONSTRUCTION, 2.375 O.D. (.154 WALL THICK.)
- SHAFT: 16 FLUTES TAPERED STEEL SHAFT (188 WALL THICK.) HAS 9" BUTT TAPERING TO 4.6" TOP. MINIMUM YIELD STRENGTH 55,000 P.S.I. HAND HOLE IS PROVIDED WITH COVER. SHAFT PROVIDED WITH 2.5"x5" HAND HOLE AT 18" FROM SHAFT BOTTOM.
- BASE PLATE: FABRICATED FROM STRUCTURAL QUALITY HOT ROLLED STEEL BASE TELESCOPES AND IS CIRCUMFERENTIALLY WELDED TO SHAFT.
- DECORATIVE BASE: TWO PIECE WRAPAROUND, CORROSION RESISTANT, DURABLE CAST ALUMINUM CONSTRUCTION, MINIMUM .225 WALL THICKNESS. BASE CONSISTS OF A SMOOTH TAPERED BOTTOM SECTION AND A DECORATIVE TAPERED FLUTED SECTION WITH EVENLY SPACED RAISED VERTICAL FLUTES. BASE IS FURNISHED WITH A CONTOURED, FLUTED FLUSH HAND HOLE AND COVER. HAND HOLE COVER SUPPLIED WITH TAMPER RESISTANT HARDWARE. (4) 1/4"-20 SET SCREWS PROVIDED TO SECURE DECORATIVE BASE TO SHAFT.
- ANCHORAGE: (4) 1/4"x48" FULLY GALVANIZED ANCHOR BOLTS. EACH BOLT SUPPLIED WITH TWO NUTS AND TWO WASHERS.
- FINISH: ELECTROSTATICALLY APPLIED BAKED ON TEXTURED ACRYLIC ENAMEL-HIGH GLOSS (COLOR: BLACK)
- NOTES: 1. LIGHT POLE ASSEMBLY SHALL BE SUN VALLEY DETAIL NO. CS-4105 REV. 5, OR APPROVED EQUAL.
2. HAND HOLE COVER TO READ "VILLAGE OF SHOREWOOD".
3. VIBRATION DAMPNER PROVIDED INSIDE OF POLE.
4. POLE ASSEMBLY RATED FOR 90 M.P.H. SUSTAINED WIND LOADS PLUS 1.3 WIND GUST.
5. BREAK AWAY BANNER ARMS PROVIDED AS REQUIRED.
6. DUPLEX REC. PROVIDED AS REQUIRED.
7. FIXTURE HEAD SHALL BE GE LIGHTING SYSTEMS *MDCA**0A21FMC31-SPECIAL BLACK, WHERE ** = WATTAGE

NOTES:

- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS MORE THAN 4 IN. (100 mm) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105), NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 2H, AND WASHERS SHALL BE ACCORDING TO ASTM F 426.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MINIMUM COATING THICKNESS OF 150 UM (6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 0298-0396 - EXPIRES 4/30/2011
 4888PS AM
 1/27/2011
 ...:\p10101\09-00028-00-CH\09-00028-00-CH-01.dwg
 ...:\p10101\09-00028-00-CH\09-00028-00-CH-01.dwg



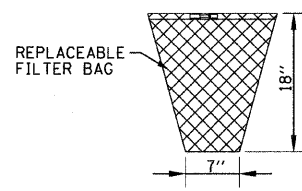
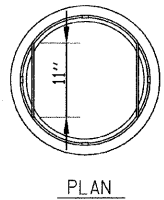
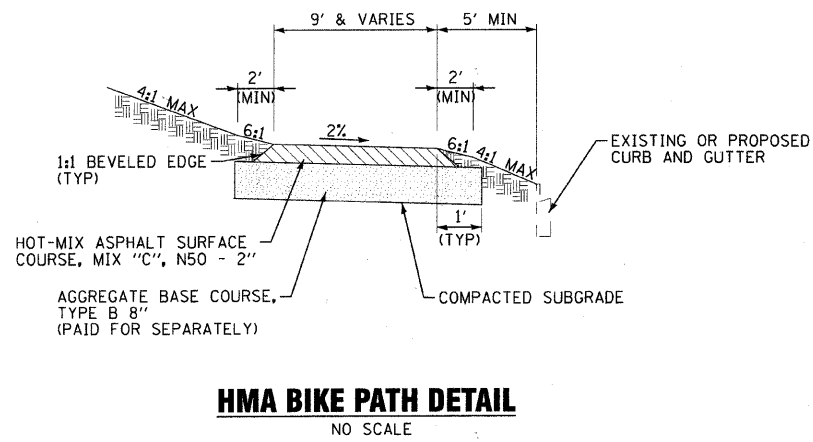
DESIGNED -	MWH	REVISED -	
DRAWN -	CJC	REVISED -	
CHECKED -	MDK	REVISED -	
DATE -	12/6/10	FILE -	090491-PH2-LightingDet.shp

**VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD & RIVER ROAD
INTERSECTION IMPROVEMENT**

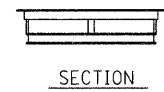
STREET LIGHTING DETAILS

SCALE: NONE STA. TO STA.

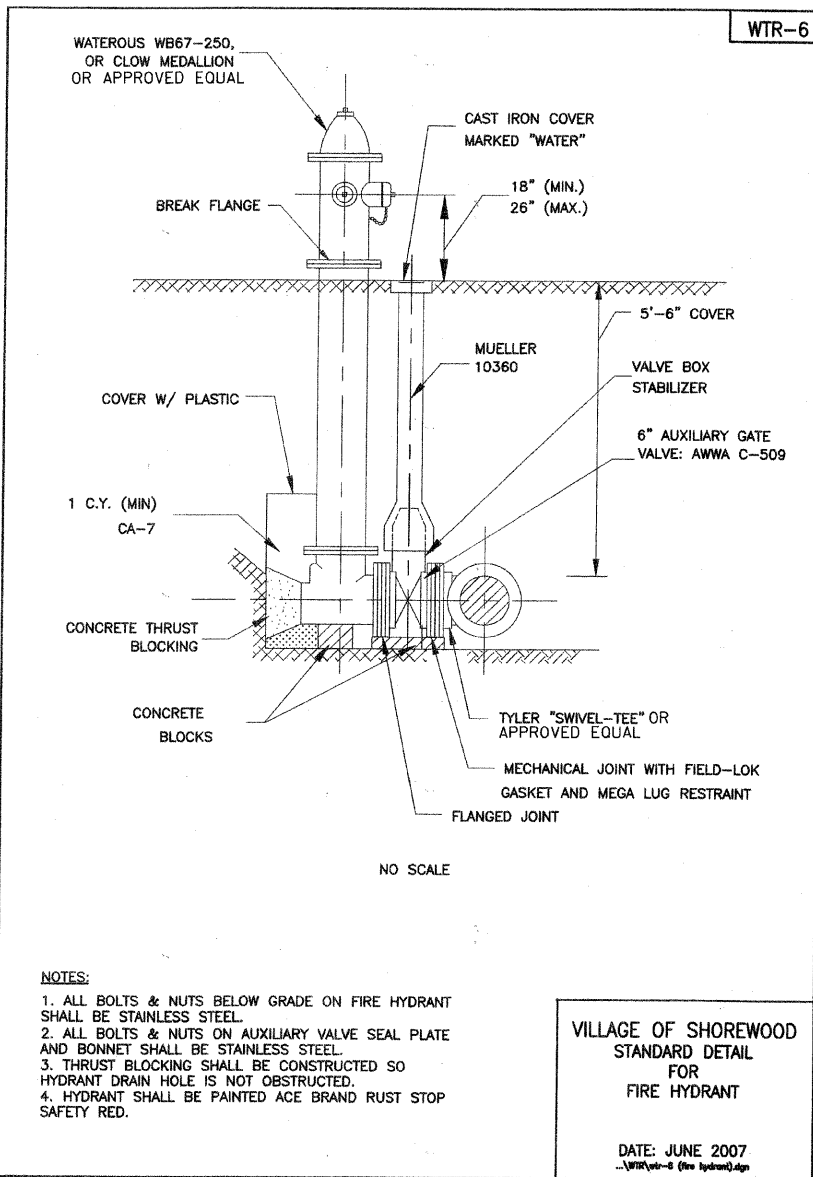
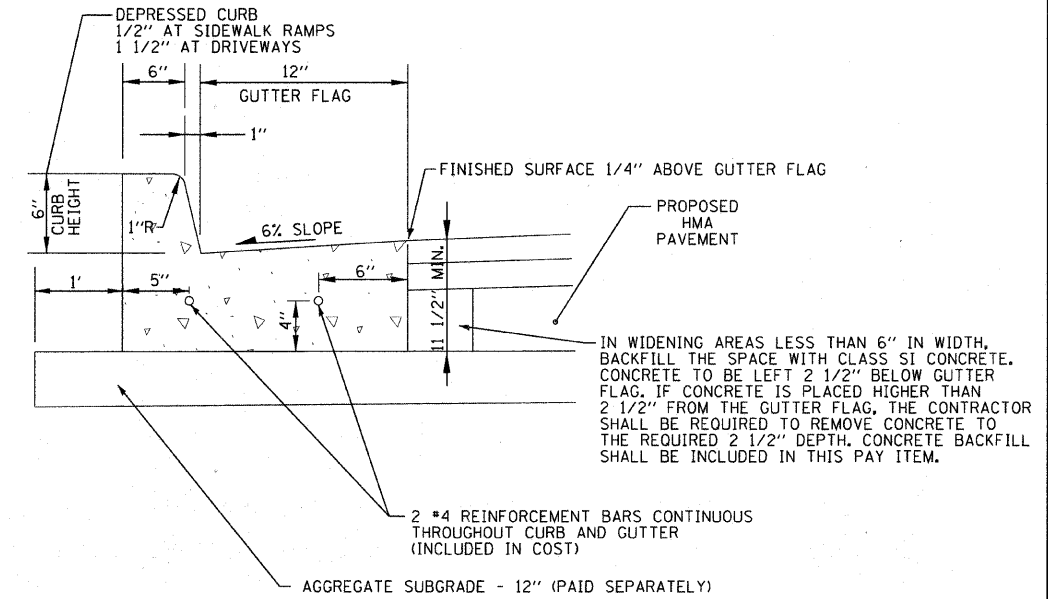
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
0298 0396	09-00028-00-CH	WILL	63 48
C-91-726-09		CONTRACT NO. 63562	
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-9003(385)	



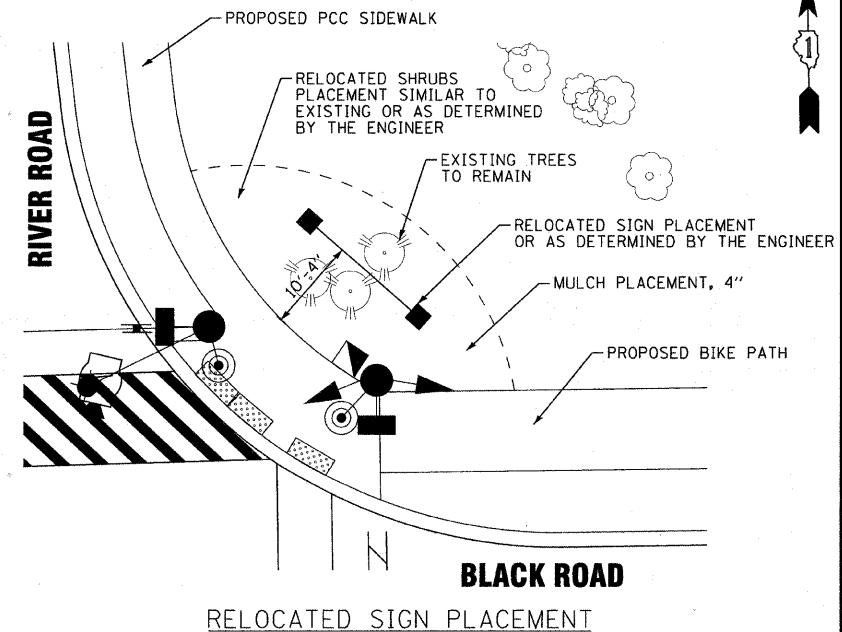
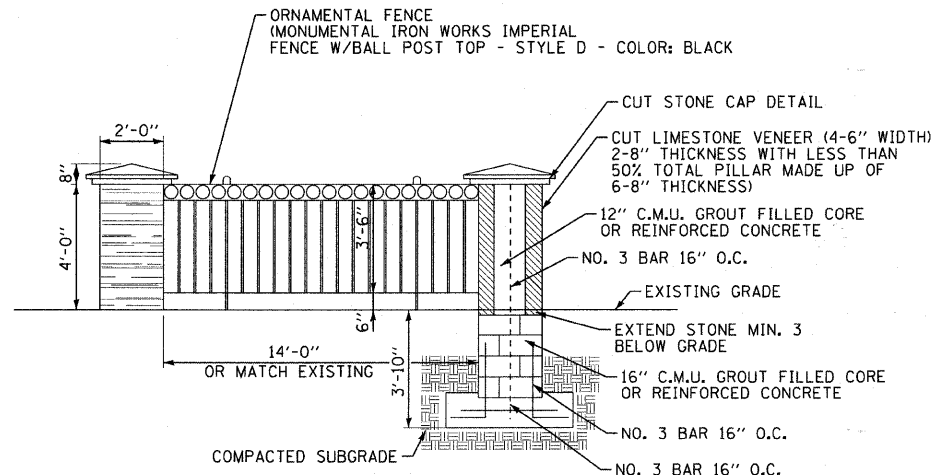
GENERAL NOTES:
 FRAME: TOP RING CONSTRUCTED FROM 1 1/4" X 1 1/4" X 1/8" ANGLE. BASE RING CONSTRUCTED OF 1 1/2" X 1/2" X 1/8" CHANNEL. HANDLES & SUSPENSION BRACKETS CONSTRUCTED FROM 1/4" X 1 1/4" FLAT. ALL STEEL CONFORMING TO ASTM-A36.
 REPLACEABLE BAG: CONSTRUCTED FROM 4 OZ./SQ. YD. NON-WOVEN POLYPROPYLENE GEOTEXTILE REINFORCED WITH POLYESTER MESH. CONNECTED TO BASE RING WITH STAINLESS STEEL STRAP & LOCK.



INLET FILTER
NO SCALE



- NOTES:
 1. ALL BOLTS & NUTS BELOW GRADE ON FIRE HYDRANT SHALL BE STAINLESS STEEL.
 2. ALL BOLTS & NUTS ON AUXILIARY VALVE SEAL PLATE AND BONNET SHALL BE STAINLESS STEEL.
 3. THRUST BLOCKING SHALL BE CONSTRUCTED SO HYDRANT DRAIN HOLE IS NOT OBSTRUCTED.
 4. HYDRANT SHALL BE PAINTED ACE BRAND RUST STOP SAFETY RED.



COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC. ...
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 064-00021 - EXPIRES 4/30/2011
 2/2/2011 8:26:28 PM



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - CJC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED -
DATE - 12/6/10	FILE - 090491-PH2-DETS.shx

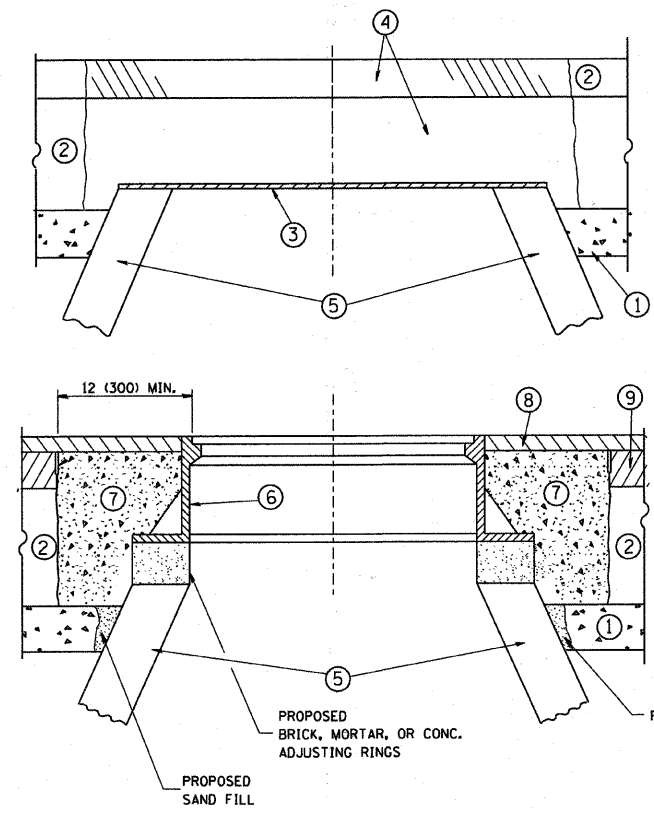
**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD & RIVER ROAD
 INTERSECTION IMPROVEMENT**

MISCELLANEOUS DETAILS

SCALE: NONE

STA. TO STA.

F.A.U. RTE. 0398 0398	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 49
C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

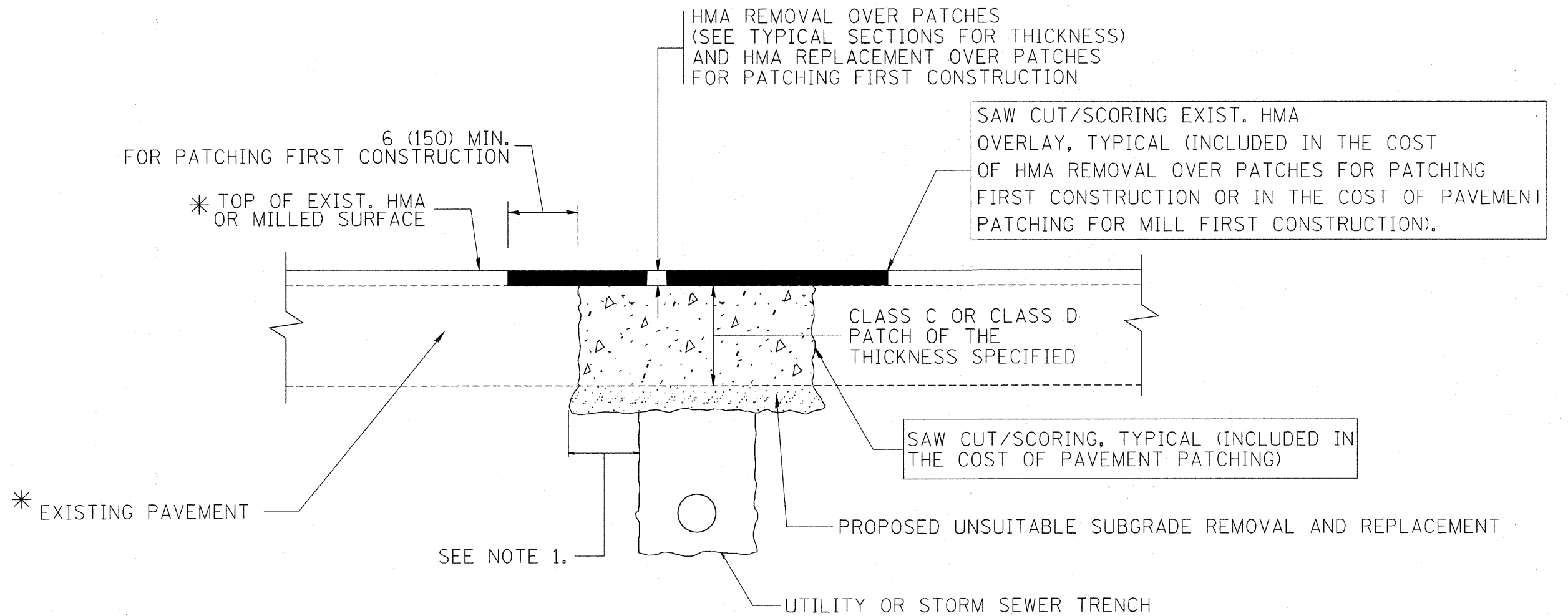
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN
C-91-726-09

FILE NAME = W:\diststd\22x34\ba08.dgn	USER NAME = gaglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 50
PLOT SCALE = 50.0000 "/ IN.	CHECKED -	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD600-03 (BD-8)		CONTRACT NO. 63562
PLOT DATE = 1/4/2008	DATE - 10-25-94	CHECKED -	REVISED - R. WIEDEMAN 05-14-04						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)		
		DATE - 10-25-94	REVISED - R. BORO 01-01-07								



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

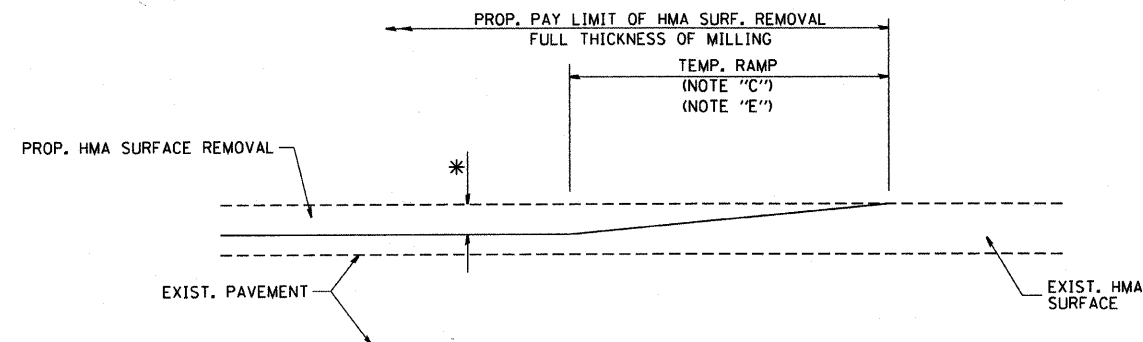
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

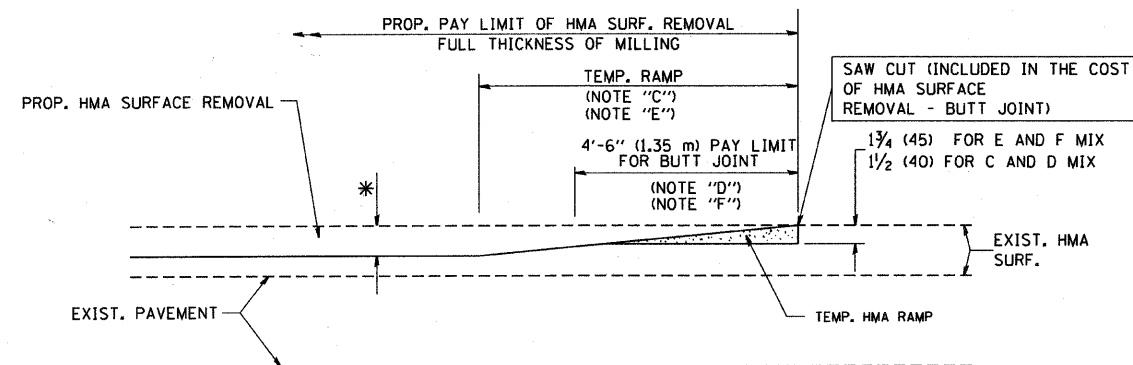
C-91-726-09

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT		F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 51	
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-04 (BD-22)		CONTRACT NO. 63562	
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - R. BORO 09-04-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003 (385)							
			REVISED - K. ENG 10-27-08									



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

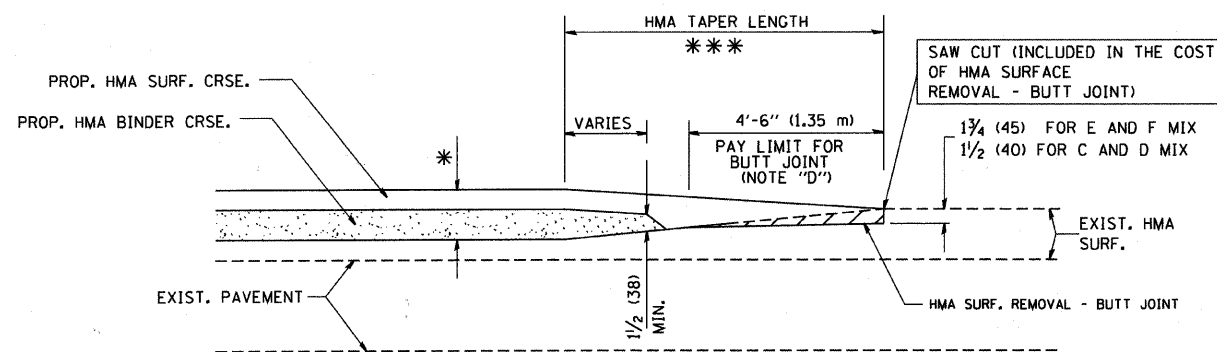
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

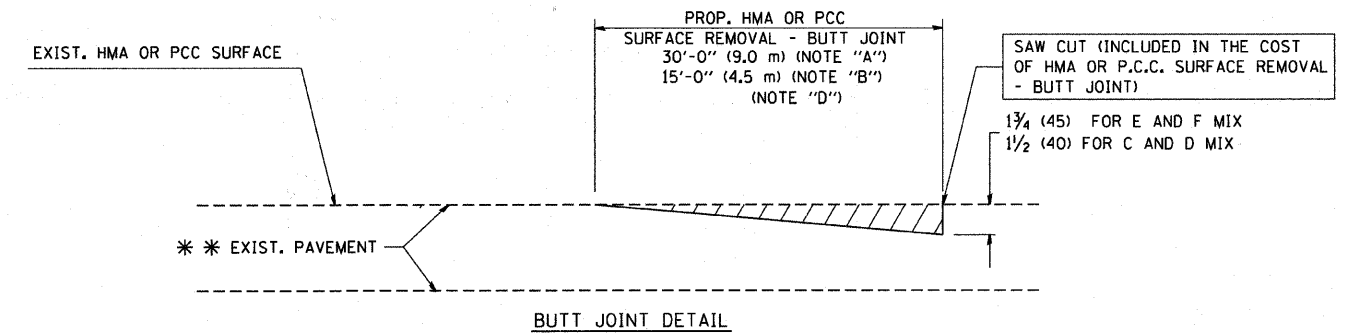
OPTION 2

TYPICAL TEMPORARY RAMP

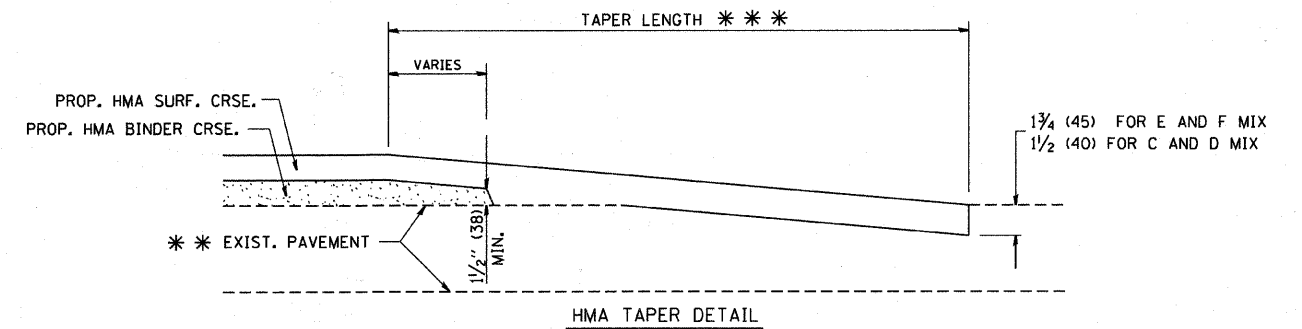


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

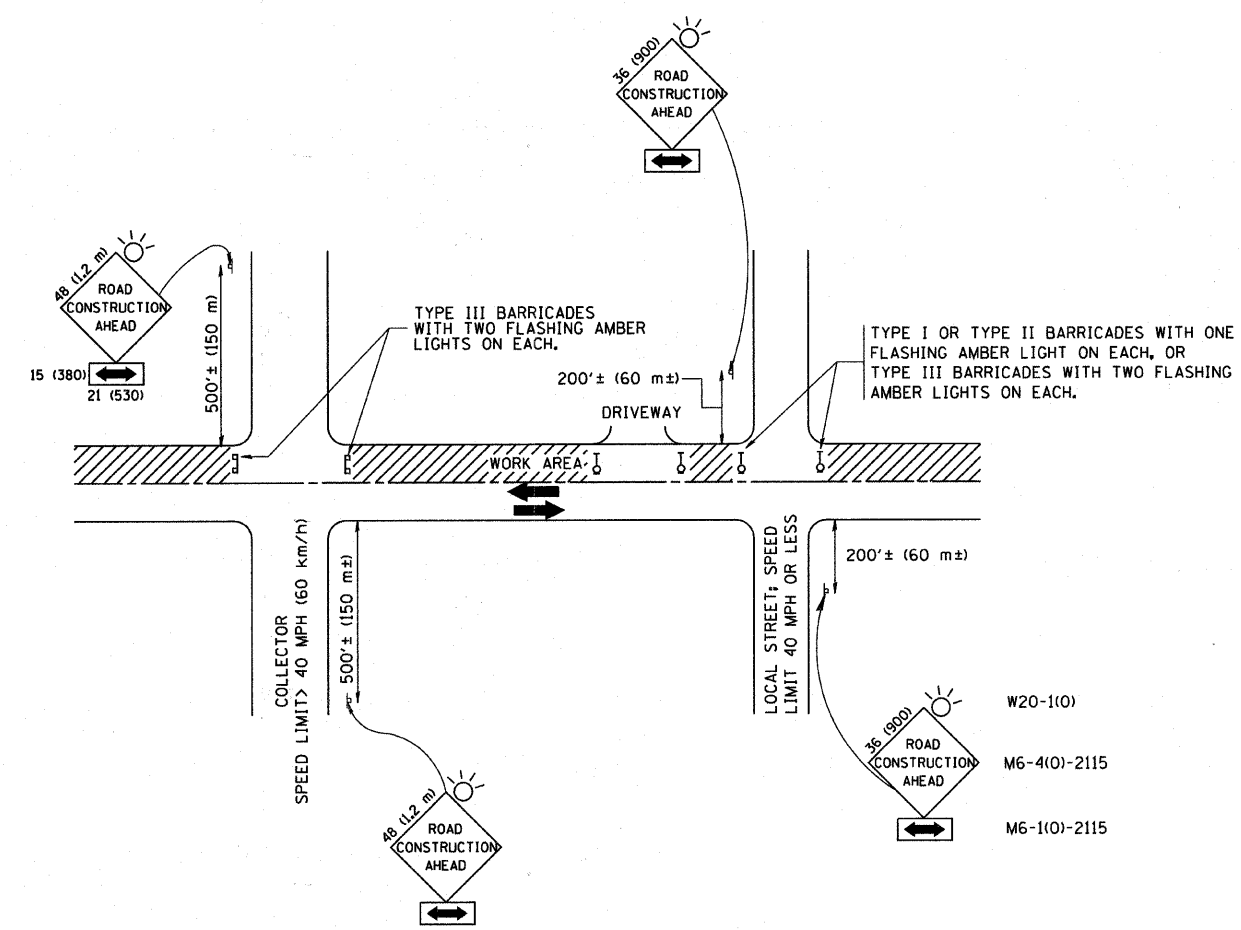
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
C-91-726-09

FILE NAME = W:\d\stetd\22x34\bd32.dgn	USER NAME = gaglioniob	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 0298 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 52
BD400-05 BD32			CONTRACT NO. 63562	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)				



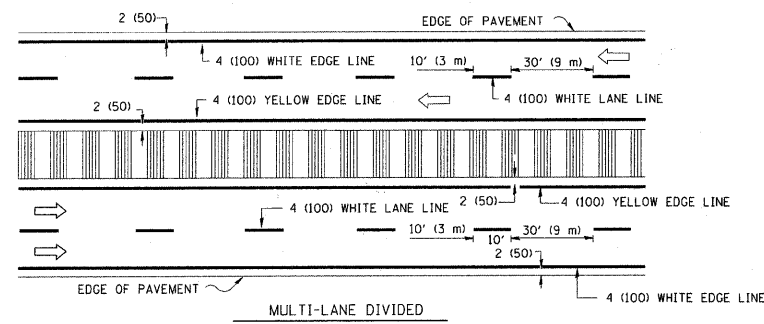
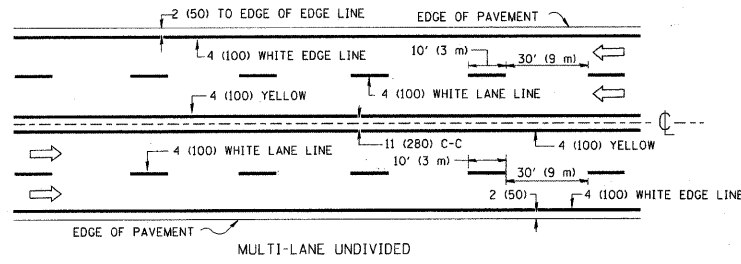
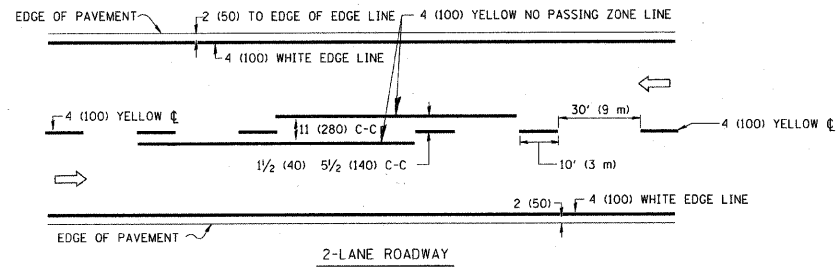
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
 USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

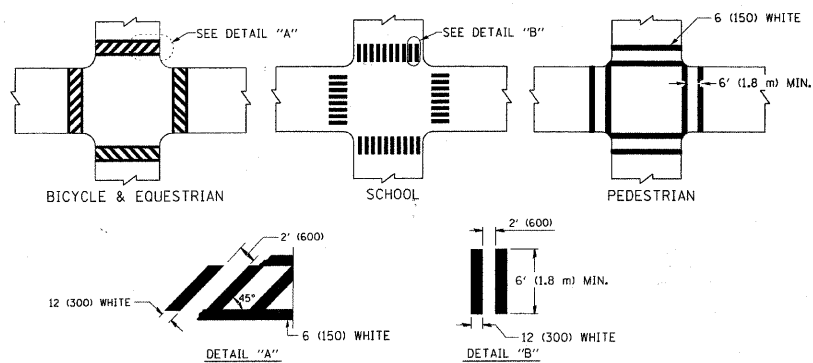
All dimensions are in millimeters (inches) unless otherwise shown.
C-91-726-09

FILE NAME = W:\diststd\22x34\tc10.dgn	USER NAME = gegl1enobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		F.A.U. RTE. 0239 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 54	
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - A. HOUSEH 03-06-96		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TC-10	CONTRACT NO. 63562	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(385)		
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - A. HOUSEH 10-15-96									
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00									

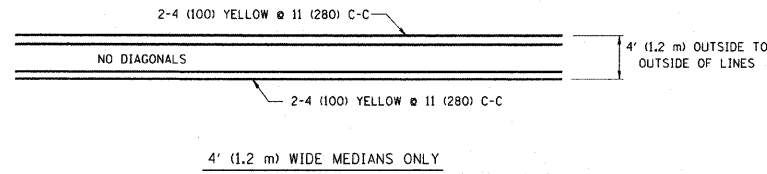


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

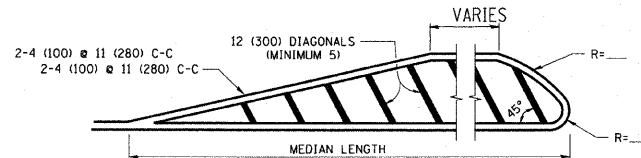
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

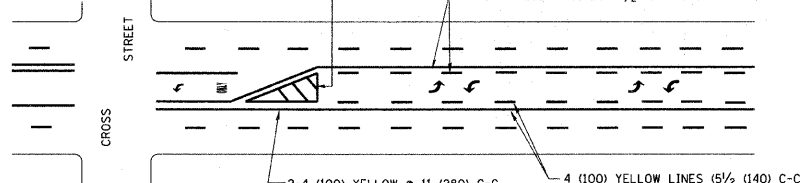


4' (1.2 m) WIDE MEDIANS ONLY

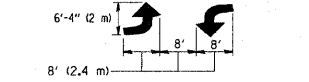


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

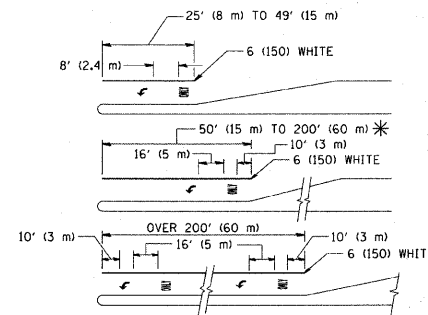


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

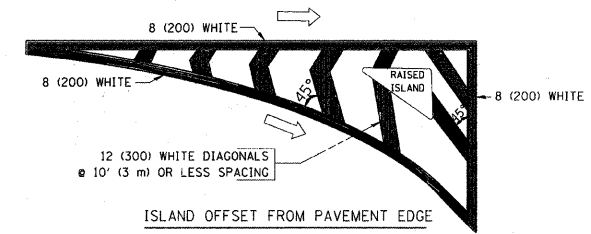
TYPICAL PAINTED MEDIAN MARKING



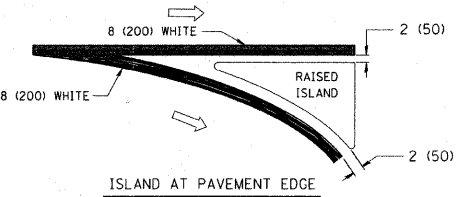
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

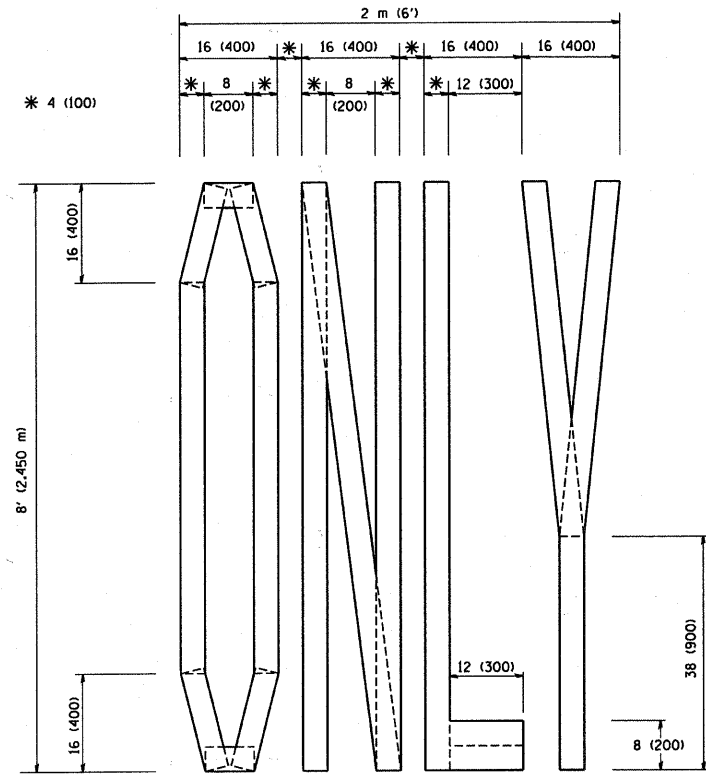
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

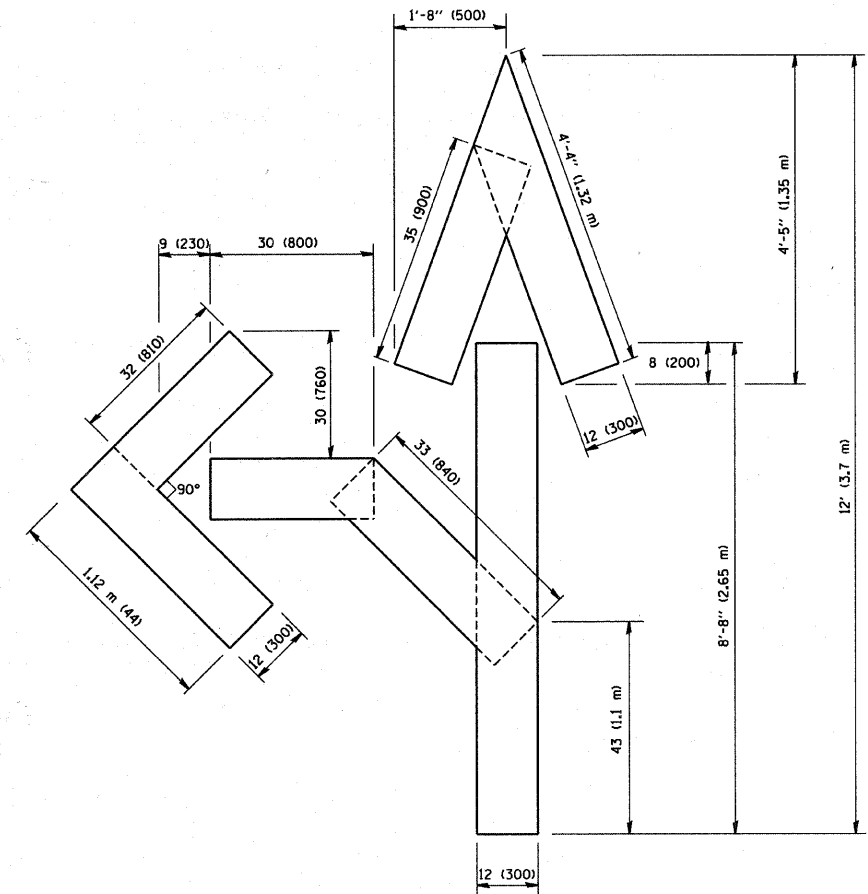
FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
ar:\pw\work\pedit\drivakosgn\d8108315\ta	3.dgn	DRAWN -	REVISED - C. JUCIUS 09-09-09
PLOT SCALE = 50,000' / 1 IN.	CHECKED -	DATE - 03-19-90	REVISED -
PLOT DATE = 9/9/2009			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

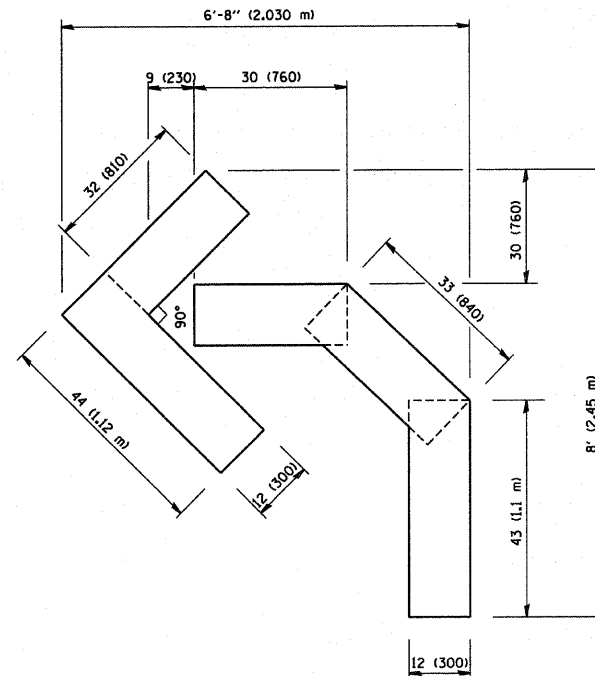
C-91-726-09		F.A.U. RTE. 10295 0396	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 55
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	CONTRACT NO. 63562	
		DISTRICT ONE TYPICAL PAVEMENT MARKINGS		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003(385)		



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

C-91-726-09

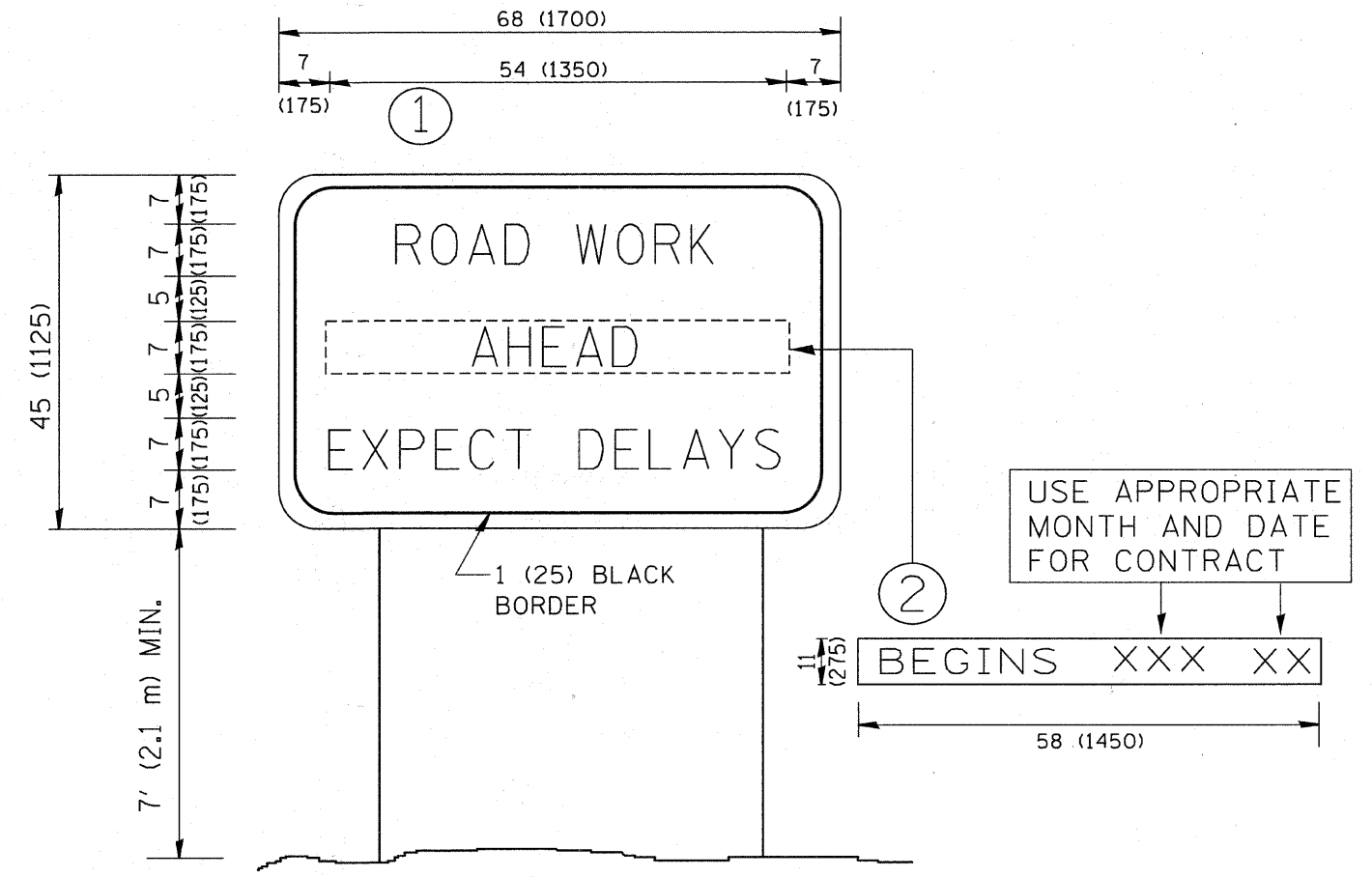
FILE NAME = W:\dststd\22x34\to16.dgn	USER NAME = geglanoht	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000" / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 0238 3586	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 56
TC-16		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003 (385)				



NOTES:

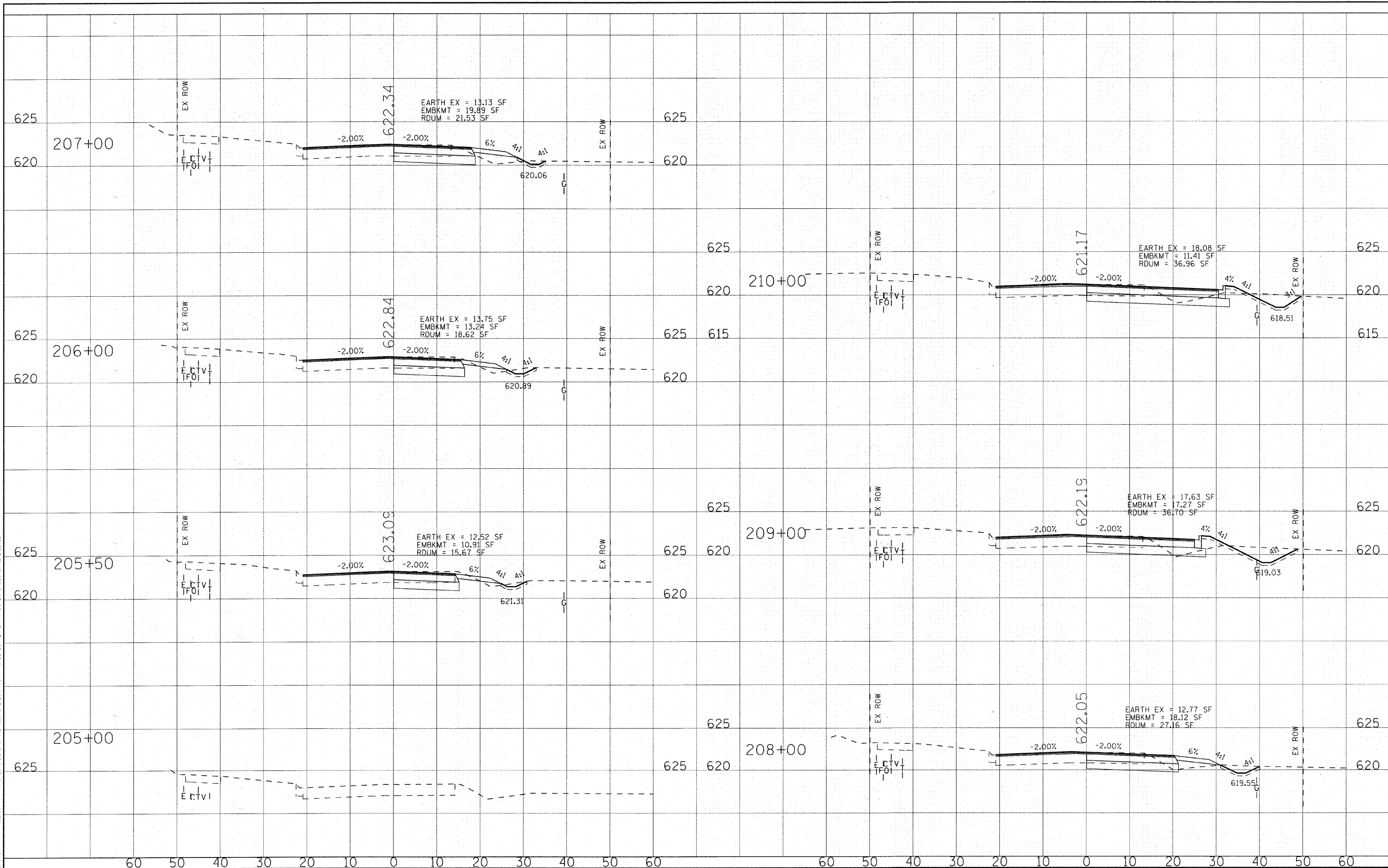
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

C-91-726-09

FILE NAME = W:\diststd\22x34\to22.dgn	USER NAME = geglienobt	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.U. RTE. 0228 0356	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 57
PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED - REVISED -	REVISED -	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	TC-22	CONTRACT NO. 63562	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003 (385)		
PLOT DATE = 1/4/2008	DATE -	REVISED -	REVISED -	STA.	TO STA.					

COPYRIGHT © 2009 BY BAXTER & BISHOPMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 84-00021 - EXPIRES 4/30/2011
 2800rd 7/2/2009 10:51:01 AM



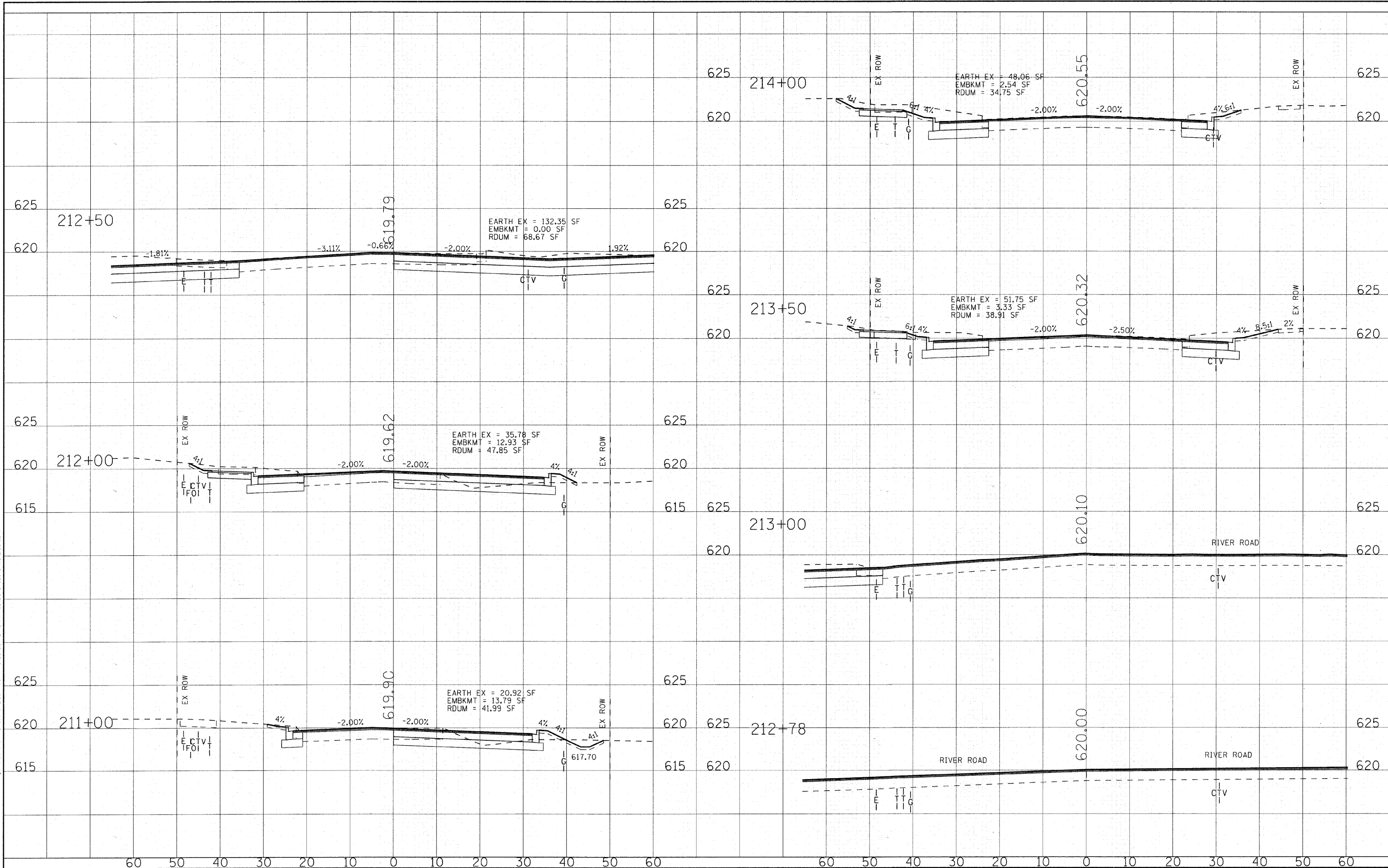
DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - MAC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED -
DATE - 12/6/10	REVISED -

VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD AND RIVER ROAD
IMPROVEMENTS

CROSS SECTIONS
BLACK ROAD
 SCALE: H: 1"=10' V: 1"=5'
 STA. 205+00 TO STA. 210+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298	09-00028-00-CH	WILL	63	58
JOB NO. C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-9003 (385)				

COPYRIGHT © 2008 BY BLATTER & BERGMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 08-00021 - EXPIRES 7/30/2011
 154445 PM
 I:\Projects\09-00028-00-CH\Drawings\09-00028-00-CH-03-01-01-01.dwg
 2/2/2011 10:44:45 AM



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - MAC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED -
DATE - 12/6/10	REVISED -

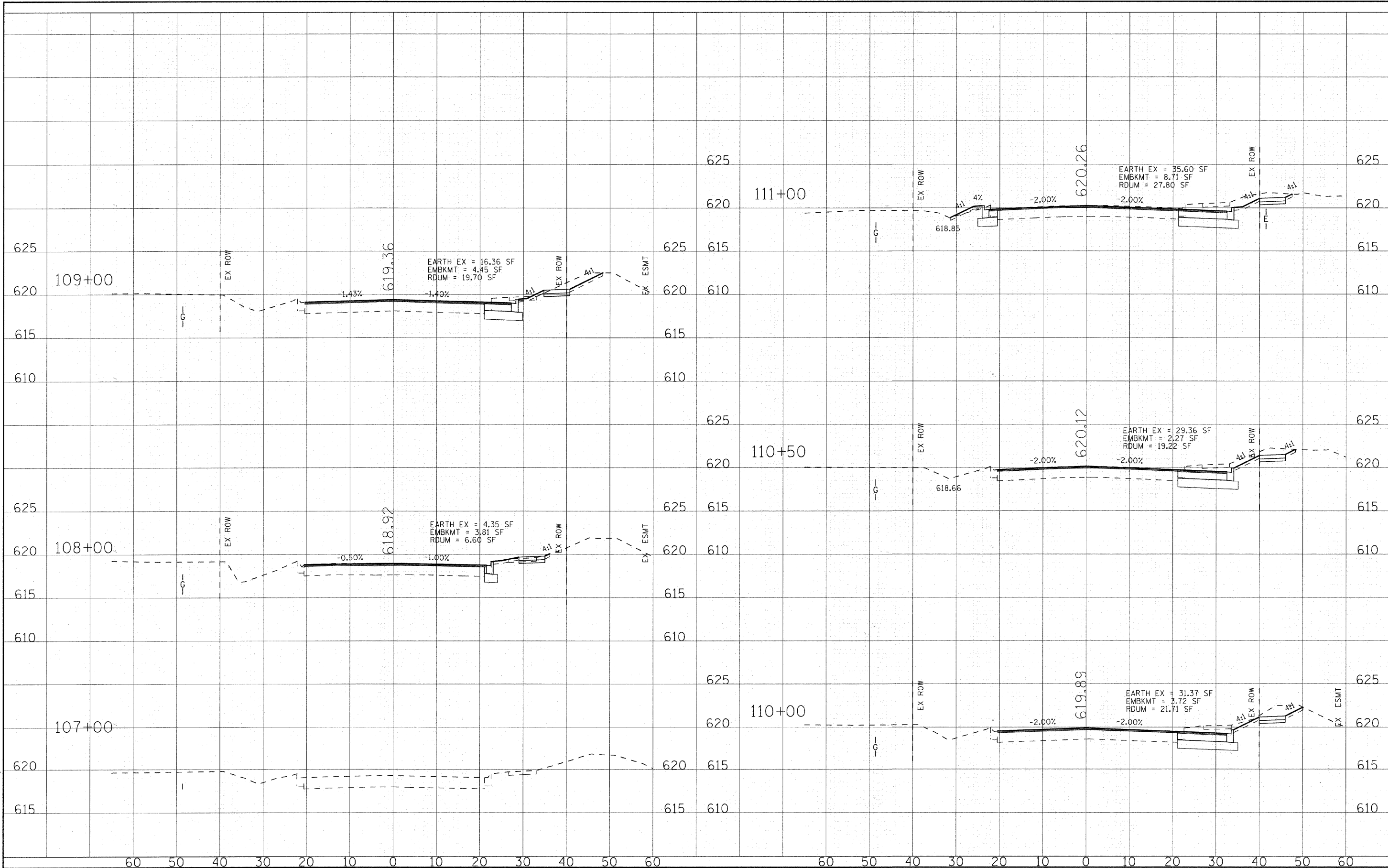
**VILLAGE OF SHOREWOOD, ILLINOIS
 BLACK ROAD AND RIVER ROAD
 IMPROVEMENTS**

**CROSS SECTIONS
 BLACK ROAD**

SCALE: H: 1"=10' V: 1"=5'
 STA. 211+00 TO STA. 214+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0938	09-00028-00-CH	WILL	63	59
JOB NO. C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-9003 (385)				

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 084-002121 - EXPIRES 4/30/2011
 2/27/2011 8:08:08 PM



DESIGNED - DSH	REVISED - 1-19-11 PER VILLAGE
DRAWN - MAC	REVISED - 1-31-11 PER IDOT
CHECKED - LDH	REVISED -
DATE - 12/6/10	REVISED -

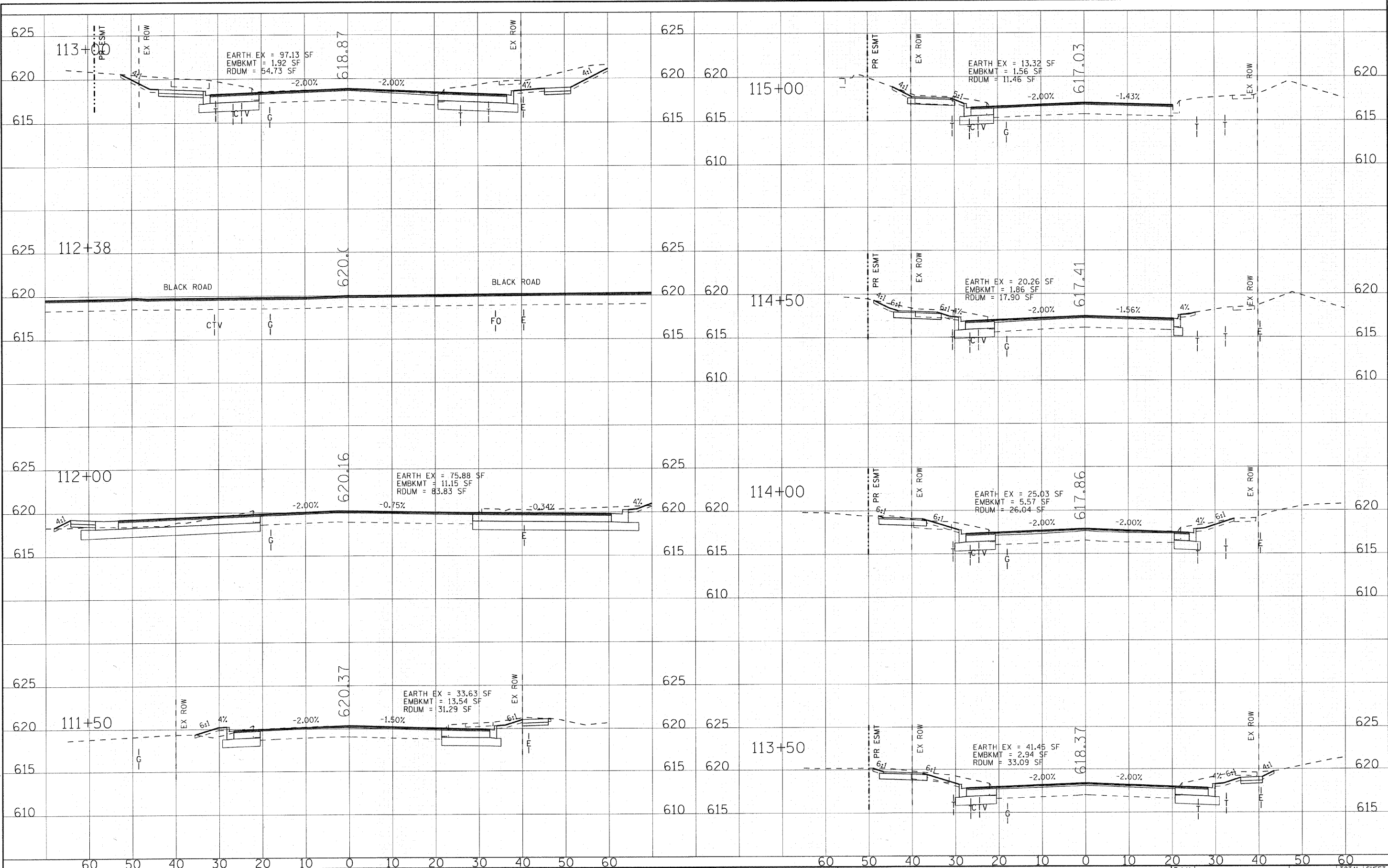
VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD AND RIVER ROAD
IMPROVEMENTS

CROSS SECTIONS
RIVER ROAD

SCALE: H: 1"=10' V: 1"=5' STA. 107+00 TO STA. 111+00

F.A.U. RTE. 0438 0439	SECTION 09-00028-00-CH	COUNTY WILL	TOTAL SHEETS 63	SHEET NO. 61
JOB NO. C-91-726-09		CONTRACT NO. 63562		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-9003 (385)				

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 081-000271 - EXPIRES 4/30/2011
 22400
 80266 PM
 I:\Crystal\090498\090498-BlackRoad\RiverRd\Drawings\090498-RiverRd.dwg



DESIGNED -	DSH	REVISED -	1-19-11 PER VILLAGE
DRAWN -	MAC	REVISED -	1-31-11 PER IDOT
CHECKED -	LDH	REVISED -	
DATE -	12/6/10	REVISED -	

VILLAGE OF SHOREWOOD, ILLINOIS
BLACK ROAD AND RIVER ROAD
IMPROVEMENTS

SCALE: H: 1"=10' V: 1"=5'

CROSS SECTIONS
RIVER ROAD

STA. 111+50 TO STA. 115+00

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0258 0386	09-00028-00-CH	WILL	63	62
JOB NO. C-91-726-09			CONTRACT NO. 63562	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT NO. M-9003 (385)				

